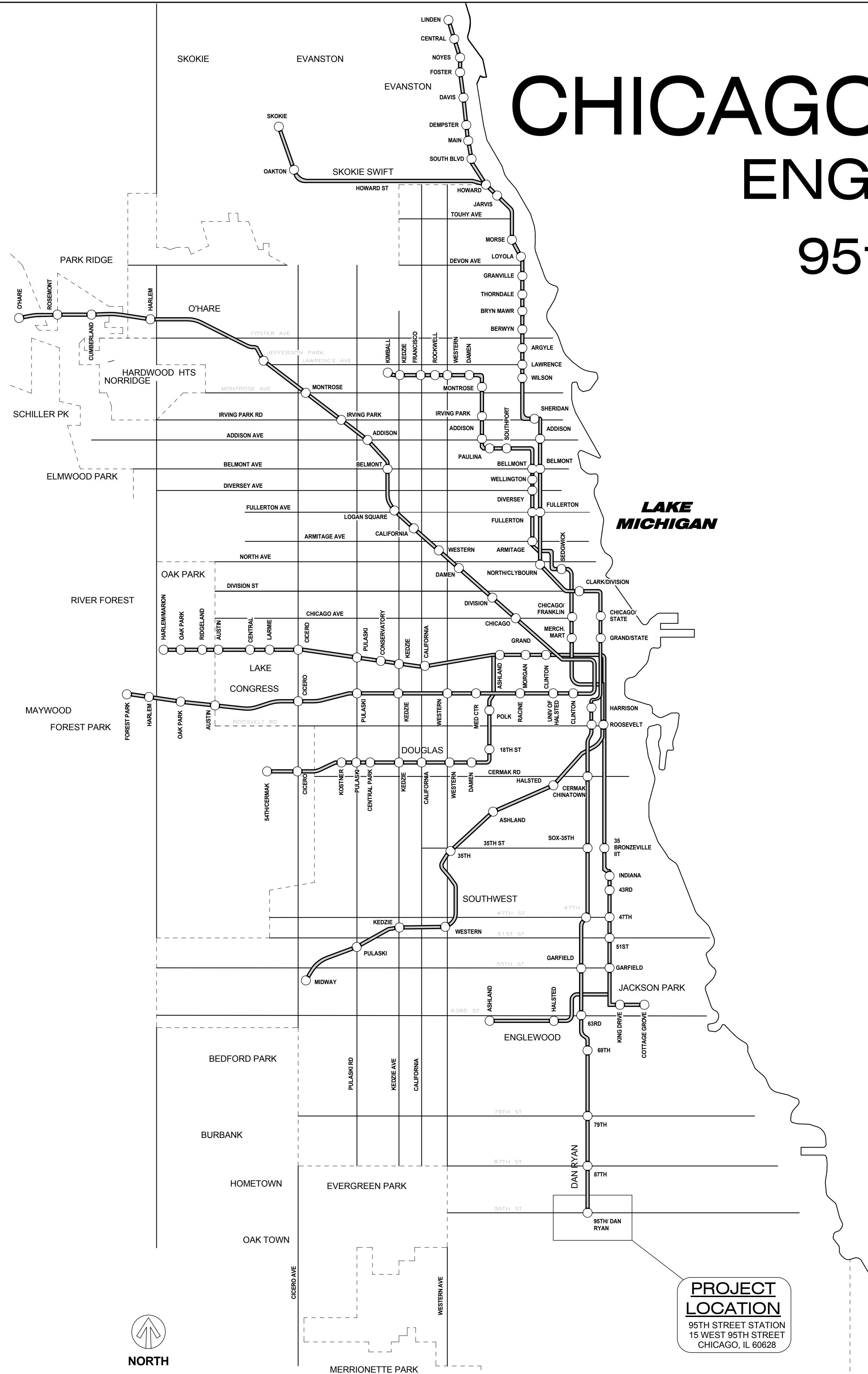
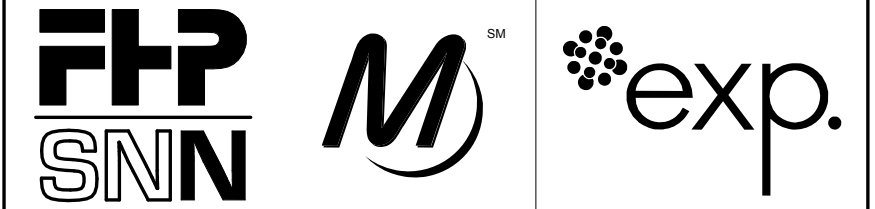


# CHICAGO TRANSIT AUTHORITY ENGINEERING DEPARTMENT

## 95th Terminal Improvement NORTH BUS BRIDGE FINAL DESIGN 2012-0021.02



**NOT FOR  
CONSTRUCTION**



**SENSITIVE SECURITY INFORMATION**


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\_\_\_\_\_  
CHIEF INFRASTRUCTURE OFFICER

\_\_\_\_\_  
VICE PRESIDENT, INFRASTRUCTURE

\_\_\_\_\_  
CHIEF ENGINEER, INFRASTRUCTURE

I HAVE PREPARED OR CAUSED TO BE PREPARED UNDER MY DIRECT SUPERVISION, THE ATTACHED PLANS AND SPECIFICATIONS AND STATE THAT TO THE BEST OF MY KNOWLEDGE AND BELIEF TO THE EXTENT OF MY OBLIGATION, THEY ARE IN COMPLIANCE WITH THE ENVIRONMENTAL BARRIERS ACT (Illinois Rev. Stat. 1985, ch. 111 / Pars. 3711et seq. amended) AND THE ILLINOIS ACCESSIBILITY CODE, ILL. ADM. CODE 601.

 THIS IS TO CERTIFY THAT THESE DRAWINGS HAVE BEEN PREPARED UNDER MY DIRECT SUPERVISION AND TO THE BEST OF MY KNOWLEDGE AND BELIEF ARE IN COMPLIANCE WITH ALL CODES AND BUILDING ORDINANCES OF THE CITY OF CHICAGO, STATE OF ILLINOIS.

MARK	DATE	DESCRIPTION
	05/23/18	FINAL DESIGN
	03/07/18	INTERMEDIATE DESIGN

LOCATION IDENTIFIER: **DR095**

**COVER SHEET**

**G-001**

5/24/2018 10:12:20 AM

W:\Revit Projects 2015\COVER SHEET AND INDEX FILE\_NBB - INTERMEDIATE DESIGN\_morenos-01.rvt

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•	S-302	95TH ST. NORTH BUS BRIDGE GENERAL DATA	5/23/18
•	S-303	95TH ST. NORTH BUS BRIDGE EXISTING STRUCTURE REMOVAL PLAN AND ELEVATION	5/23/18
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•	S-316A	95TH ST. NORTH BUS BRIDGE DRAINAGE SCUPPER DS-33	5/23/18
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•	S-329	95TH ST. NORTH BUS BRIDGE BEARING DETAILS (SHEET 1 OF 3)	5/23/18
•	S-329A	95TH ST. NORTH BUS BRIDGE BEARING DETAILS (SHEET 2 OF 3)	5/23/18
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•	S-334	95TH ST. NORTH BUS BRIDGE EAST & WEST ABUTMENT EXCAVATION & CELLULAR CONCRETE	5/23/18
•	S-335	95TH ST. NORTH BUS BRIDGE EAST & WEST EXISTING WALL EXCAVATION & CELLULAR CONCRETE	5/23/18
•	S-336	95TH ST. NORTH BUS BRIDGE PIER PLAN AND ELEVATION (SHEET 1 OF 2)	5/23/18
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•	S-338	95TH ST. NORTH BUS BRIDGE PIER SECTIONS AND DETAILS	5/23/18

NOT FOR CONSTRUCTION



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**95th Terminal Improvement**  
 14 West 95th Street (North Terminal)  
 15 West 95th Street (South Terminal)  
 Chicago, IL 60628

IN CHARGE	J. JAKALSKI
APPROVED BY	M. VIAMONTES
CHECKED BY	J. STRYKER
DESIGNED BY	T. HOEPF
DRAWN BY	S.MORENO
PROJECT NO.	
FILE NAME	DR095_A-TerminalCENTRAL.rvt

MARK	DATE	DESCRIPTION
	05/23/18	FINAL DESIGN
	03/07/18	INTERMEDIATE DESIGN

LOCATION IDENTIFIER: **DR095**

**INDEX OF DRAWINGS**

**G-002**

BENCH MARK: CITY OF CHICAGO BENCHMARK NO. 551. BENCH MONUMENT LOCATED ABOUT 50 FEET SOUTH OF THE SOUTH LINE OF W. 95TH ST., ABOUT 11 FEET EAST OF THE WEST LINE OF S. PERRY AVE. ELEVATION 16.092.

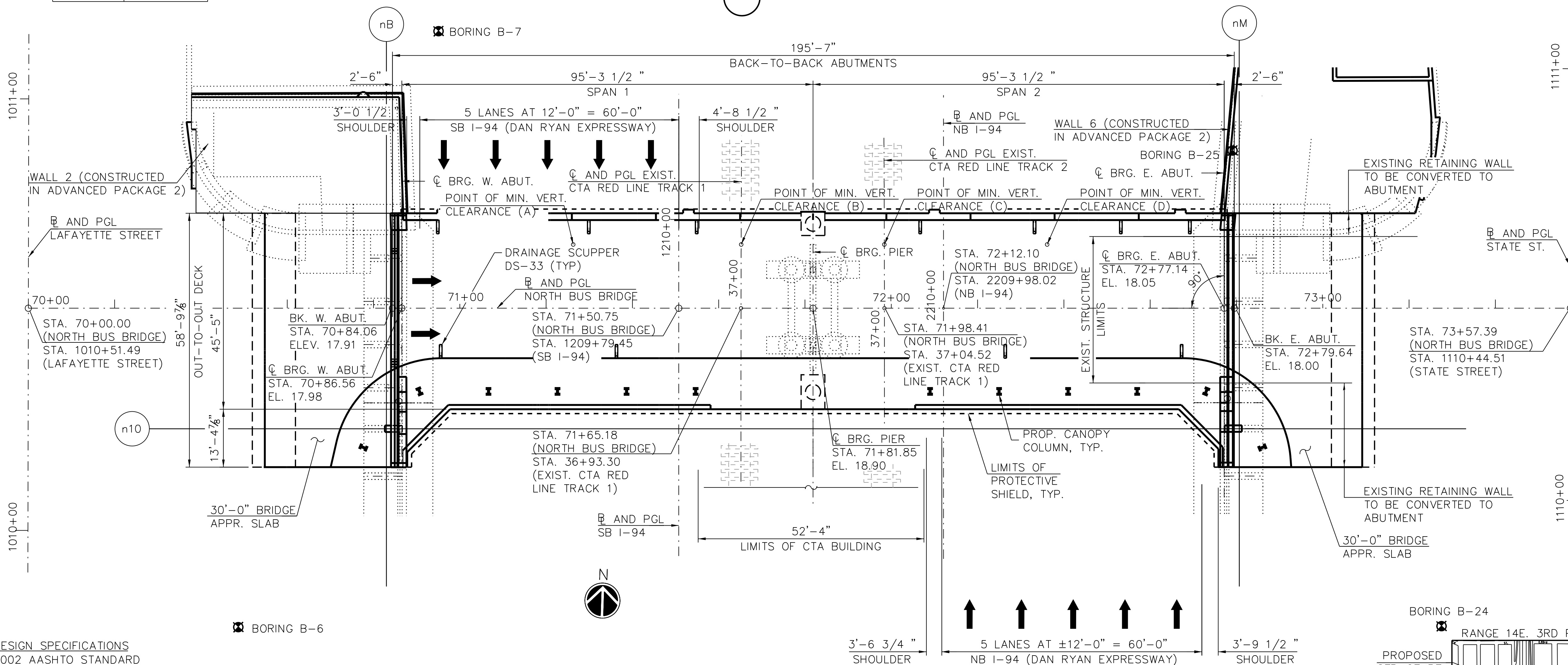
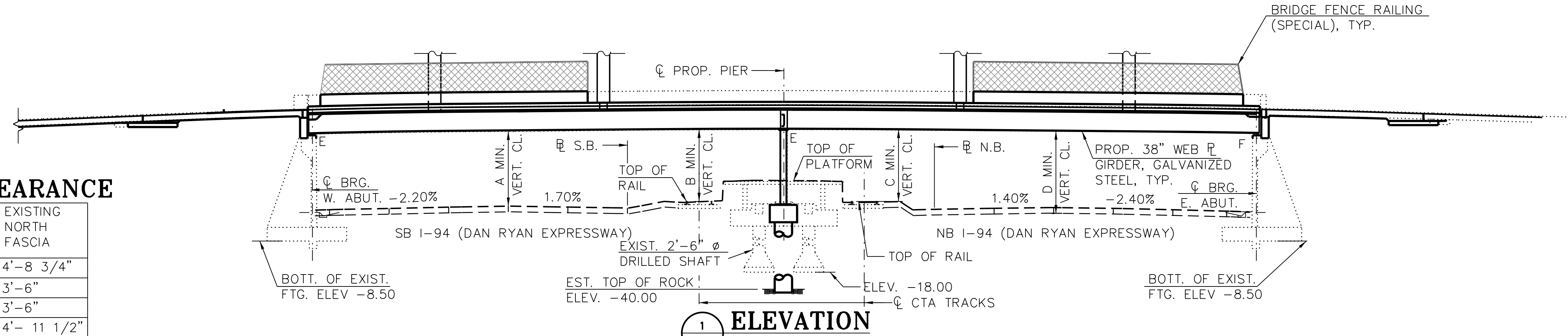
EXISTING STRUCTURE: THE NORTH BUS BRIDGE WAS ORIGINALLY CONSTRUCTED IN 1968 AS A TWO-SPAN STRUCTURE CARRYING TWO EASTBOUND LANES OF BUS TRAFFIC OVER I-94 DAN RYAN EXPRESSWAY AND CTA. IMPROVEMENTS WERE PERFORMED ON THE SUPERSTRUCTURE AND SUBSTRUCTURE IN 2003. THE BRIDGE HAS AN OVERALL LENGTH OF APPROXIMATELY 190'-7" (C TO C BEARING 95'-3 1/2" / 95'-3 1/2" SPANS), AN OVERALL WIDTH OF 35'-7" (OUT-TO-OUT SUPERSTRUCTURE) AND CONSISTS OF A REINFORCED CONCRETE DECK WITH OVERLAY AND SIDEWALK SUPPORTED ON STEEL PLATE-GIRDER I-BEAMS. THE SUBSTRUCTURE CONSISTS OF A STEEL PIER ON DRILLED SHAFTS AND CONCRETE ABUTMENTS ON SPREAD FOOTINGS WITH COUNTERFORT WALLS. THE BRIDGE IS ORIENTED IN THE EAST-WEST DIRECTION WITH NO SKEW. THE BRIDGE WILL BE WIDENED. THE EXISTING BRIDGE DECK WILL BE REMOVED AND REPLACED WITH A WIDENED DECK WITH A WIDTH OF 45'-5" SUPPORTED ON EXISTING GIRDERS AND NEW GIRDERS. PORTIONS OF THE EXISTING ABUTMENTS, SOUTHWEST RETAINING WALL AND SOUTHEAST RETAINING WALL WILL BE REMOVED AND RECONSTRUCTED TO SUPPORT THE NEW SUPERSTRUCTURE. A PORTION OF THE SOIL BEHIND THE ABUTMENTS AND RETAINING WALLS WILL BE REMOVED AND REPLACED WITH CELLULAR CONCRETE. THE CENTER PIER WILL BE EXTENDED AND SUPPORTED ON NEW DRILLED SHAFTS. STAGING CONSTRUCTION IS NOT REQUIRED. HOWEVER, THE NORTH BUS BRIDGE WILL BE CLOSED TO PUBLIC TRAFFIC DURING CONSTRUCTION.

**NOT FOR CONSTRUCTION**



**MIN. CLEARANCE**

LOCATION	EXISTING NORTH FASCIA
A	14'-8 3/4"
B	13'-6"
C	13'-6"
D	14'- 11 1/2"



**2 PLAN**

**DESIGN SPECIFICATIONS**  
 2002 AASHTO STANDARD SPECIFICATIONS FOR HIGHWAY BRIDGES, 17TH EDITION  
**ORIGINAL CONSTRUCTION (1968)**  
 AMERICAN ASSOCIATION OF STATE HIGHWAY OFFICIALS STANDARD SPECIFICATIONS FOR HIGHWAY BRIDGES, 9TH EDITION DATED 1965  
**LOADING HS20-44**  
 ALLOW 25#/SQ. FT. FOR FUTURE WEARING SURFACE.

**DESIGN STRESSES**  
**FIELD UNITS**  
 F<sub>C</sub> = 5,000 PSI  
 F<sub>Y</sub> = 60,000 PSI (REINFORCEMENT)  
 F<sub>Y</sub> = 50,000 PSI (M270 GRADE 50)  
**EXISTING UNITS**  
 F<sub>C</sub> = 4,000 PSI  
 F<sub>Y</sub> = 40,000 PSI (REINFORCEMENT)  
 F<sub>Y</sub> = 36,000 PSI (STRUCTURAL STEEL)

**SEISMIC DATA**  
 SEISMIC PERFORMANCE CATEGORY (SPC) = A  
 HORIZONTAL BEDROCK ACCELERATION COEFFICIENT (A) = 0.04G  
 SITE COEFFICIENT = 1.0

**NOTES:**  
 1. FOR PROPOSED CANOPY COLUMNS, SEE ARCHITECTURAL PLANS.  
 2. FOR PROFILE GRADE LINES, SEE SHEET NBB-S-302.

**cta CHICAGO TRANSIT AUTHORITY ENGINEERING**

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**95th Terminal Improvement**  
 14 West 95th Street (North Terminal)  
 Chicago, IL 60628

<b>IN CHARGE</b>	S. KADERBEK
<b>APPROVED BY</b>	D.ZROKA
<b>CHECKED BY</b>	P.MAID
<b>DESIGNED BY</b>	L.SOMMER
<b>DRAWN BY</b>	T.SLEDGE
<b>PROJECT NO.</b>	2012-0021
<b>FILE NAME</b>	File Name

MARK	DATE	DESCRIPTION
	05/23/18	FINAL DESIGN
	03/07/18	INTERMEDIATE DESIGN

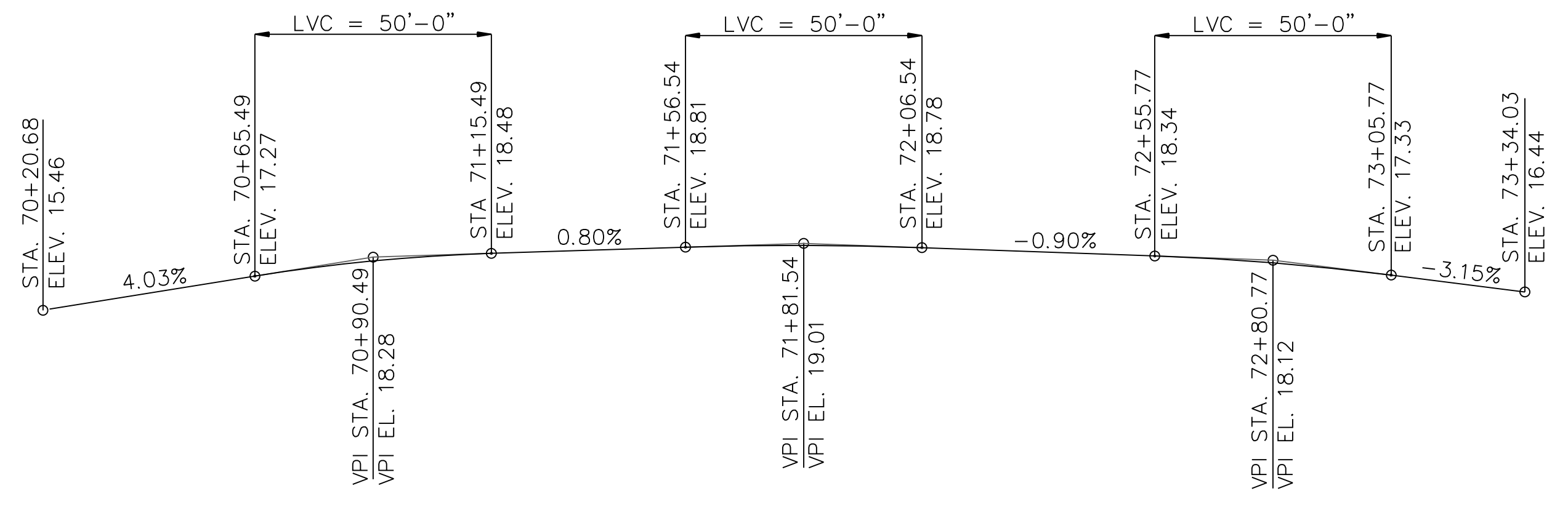
**LOCATION IDENTIFIER: DR095**  
**95TH ST. NORTH BUS BRIDGE**  
**GENERAL PLAN AND ELEVATION**

**NBB-S-301**

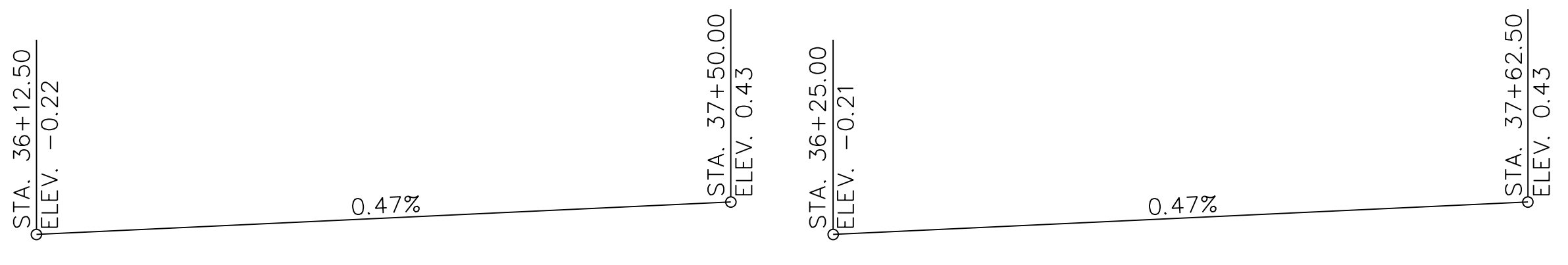
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 PLOT DATE = 5/24/2018

vscorp

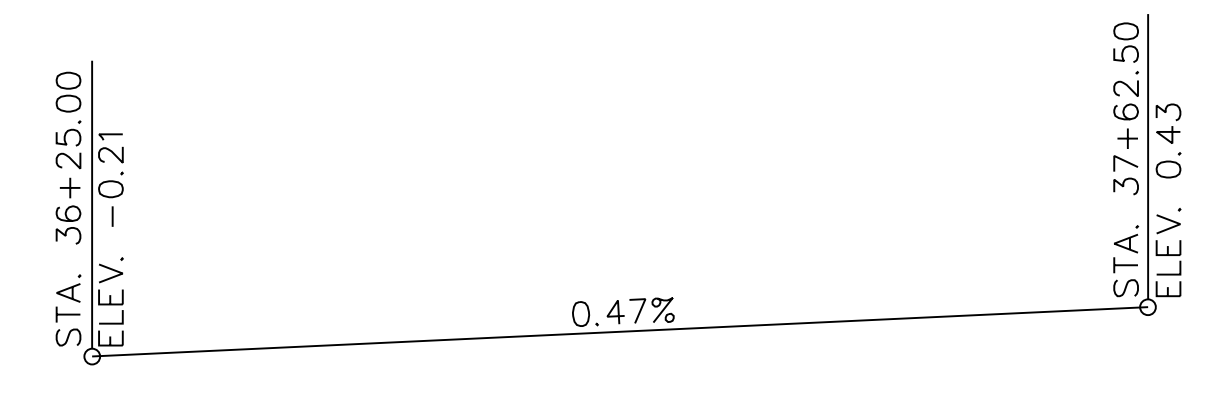
vscorppa



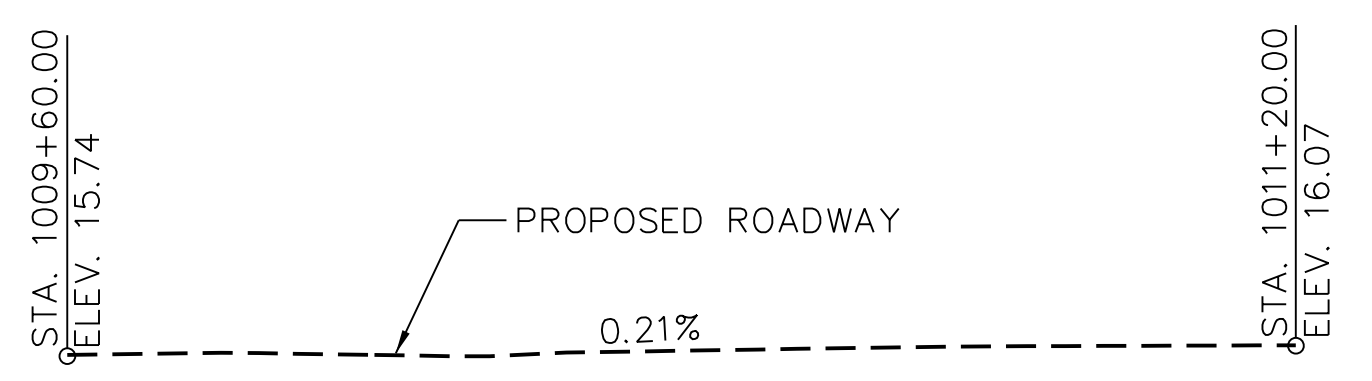
**3 PROFILE GRADE**  
(ALONG  $\bar{N}$  NORTH BUS BRIDGE)



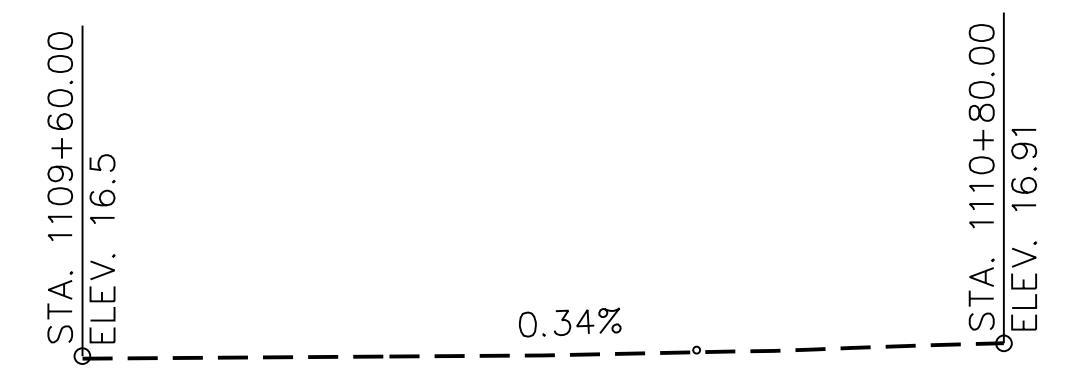
**4 PROFILE GRADE**  
(ALONG  $\bar{C}$  TRACK 1)



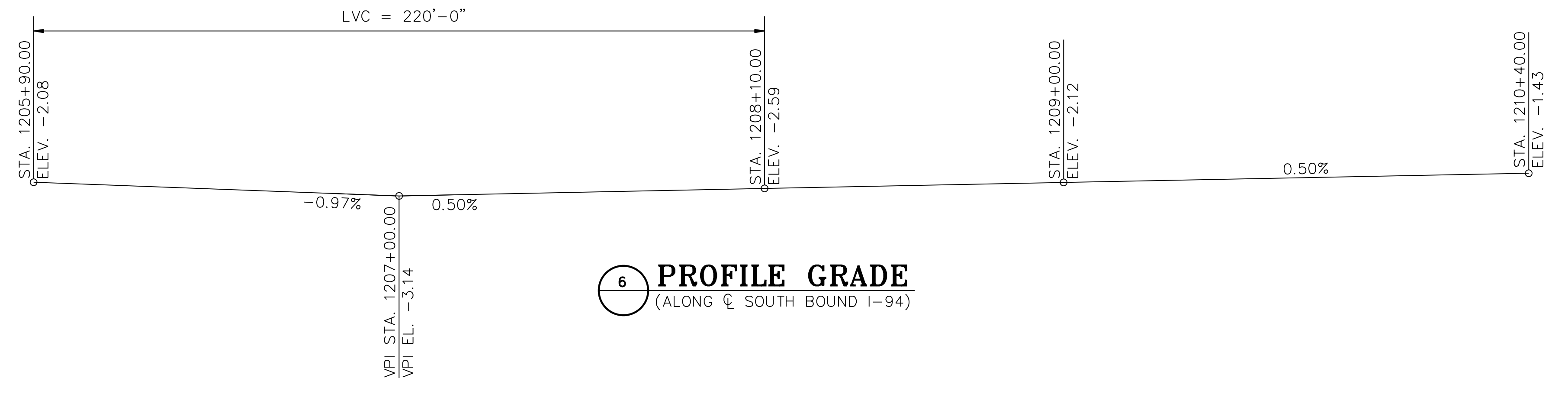
**5 PROFILE GRADE**  
(ALONG  $\bar{C}$  TRACK 2)



**1 PROFILE GRADE**  
(ALONG  $\bar{C}$  LAFAYETTE STREET)



**2 PROFILE GRADE**  
(ALONG  $\bar{C}$  STATE STREET)

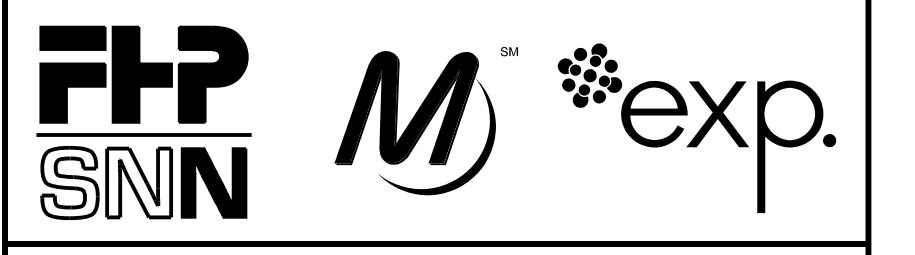


**6 PROFILE GRADE**  
(ALONG  $\bar{C}$  SOUTH BOUND I-94)



**7 PROFILE GRADE**  
(ALONG  $\bar{C}$  NORTH BOUND I-94)

**NOT FOR CONSTRUCTION**



**cta CHICAGO TRANSIT AUTHORITY ENGINEERING**

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14 West 95th Street (North Terminal)  
Chicago, IL 60628

<b>IN CHARGE</b>	S. KADERBEK
<b>APPROVED BY</b>	D.ZROKA
<b>CHECKED BY</b>	P.MAID
<b>DESIGNED BY</b>	L.SOMMER
<b>DRAWN BY</b>	T.SLEDGE
<b>PROJECT NO.</b>	2012-0021
<b>FILE NAME</b>	File Name

MARK	DATE	DESCRIPTION
	05/23/18	FINAL DESIGN
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LOCATION IDENTIFIER: **DR095**

95TH ST. NORTH BUS BRIDGE  
GENERAL DATA

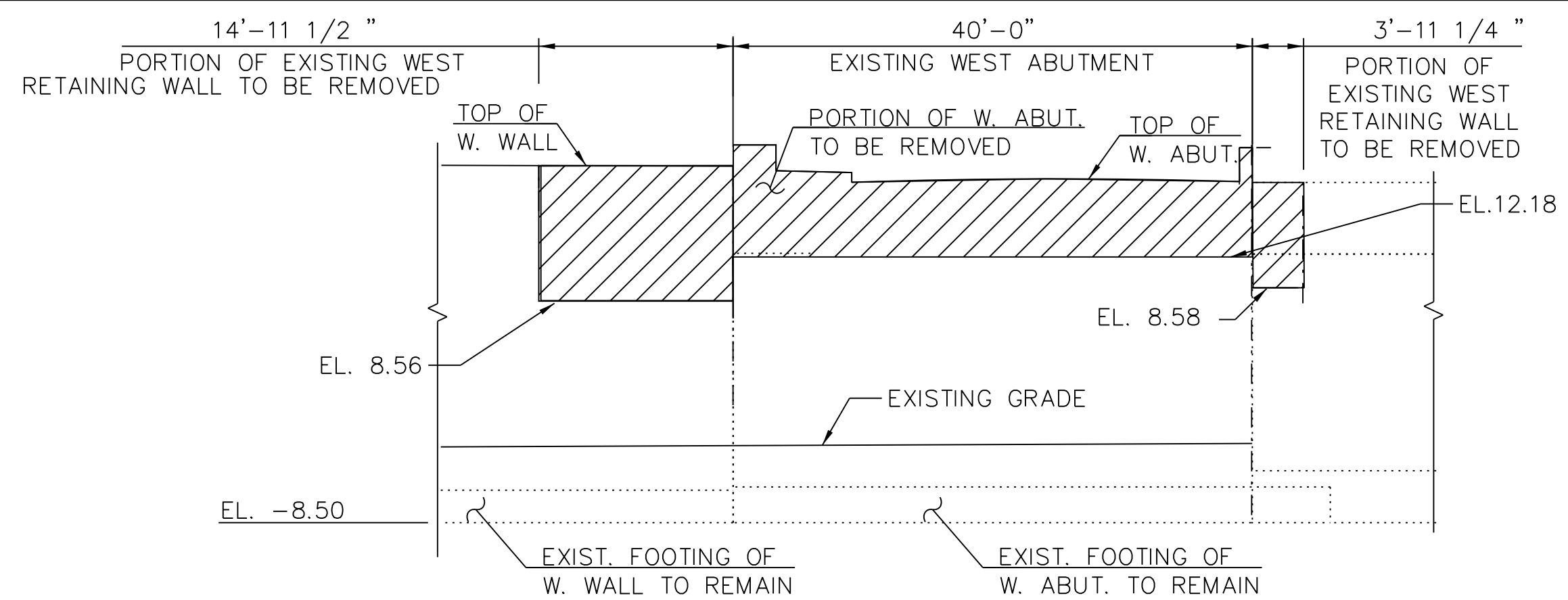
**NBB-S-302**

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PLOT DATE = 5/24/2018

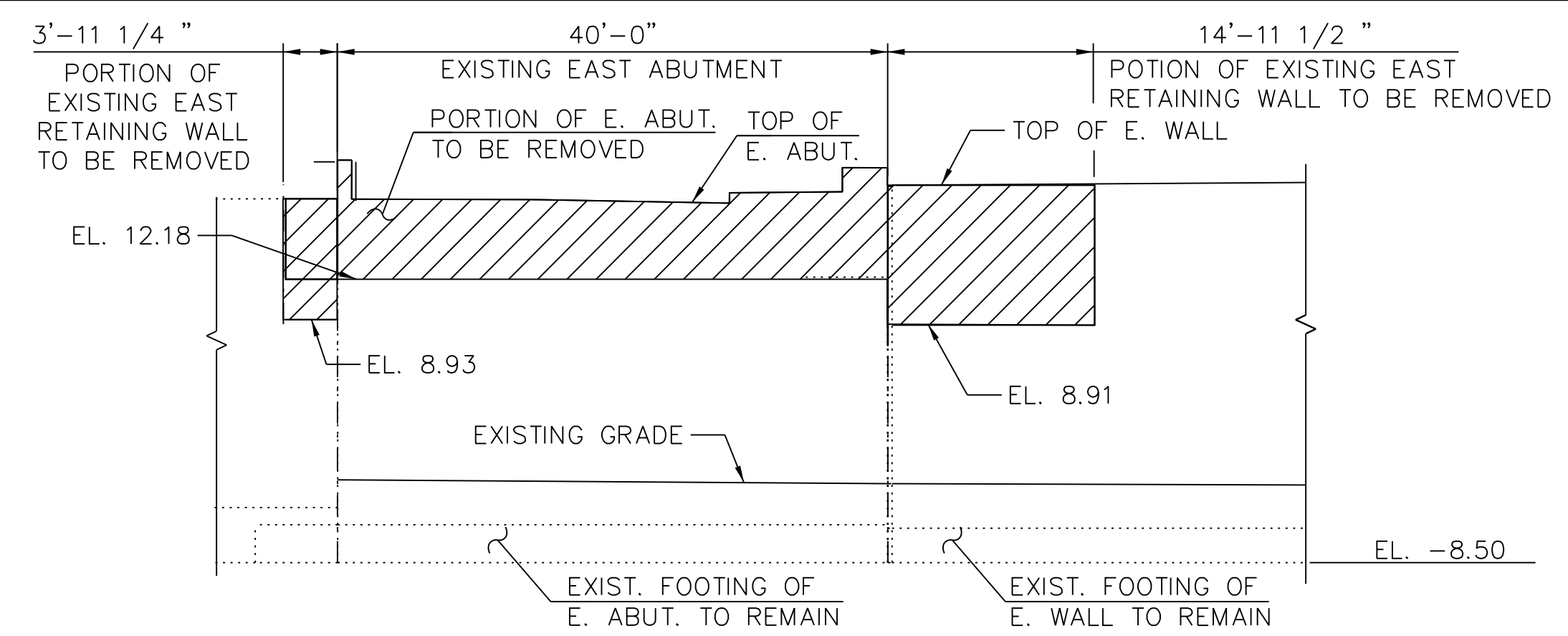




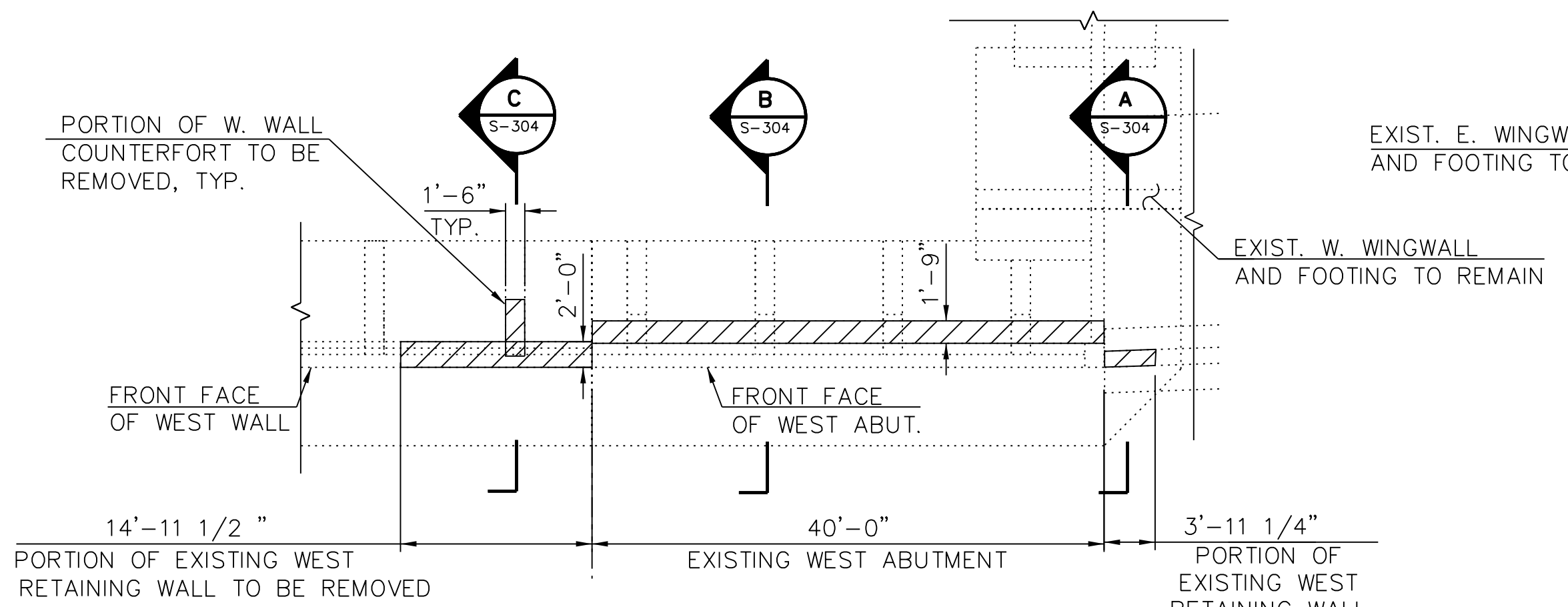
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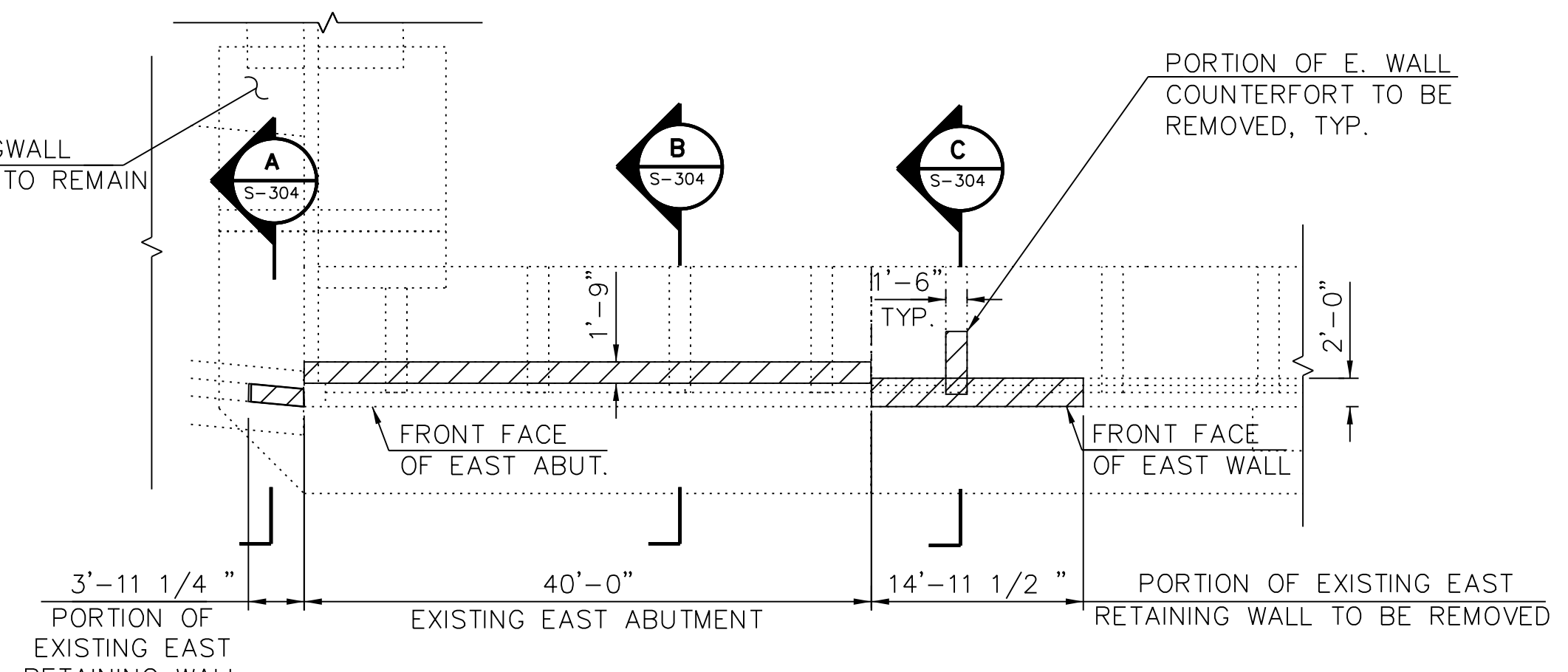
**1 WEST ABUTMENT AND WEST WALL ELEVATION**  
(LOOKING WEST)



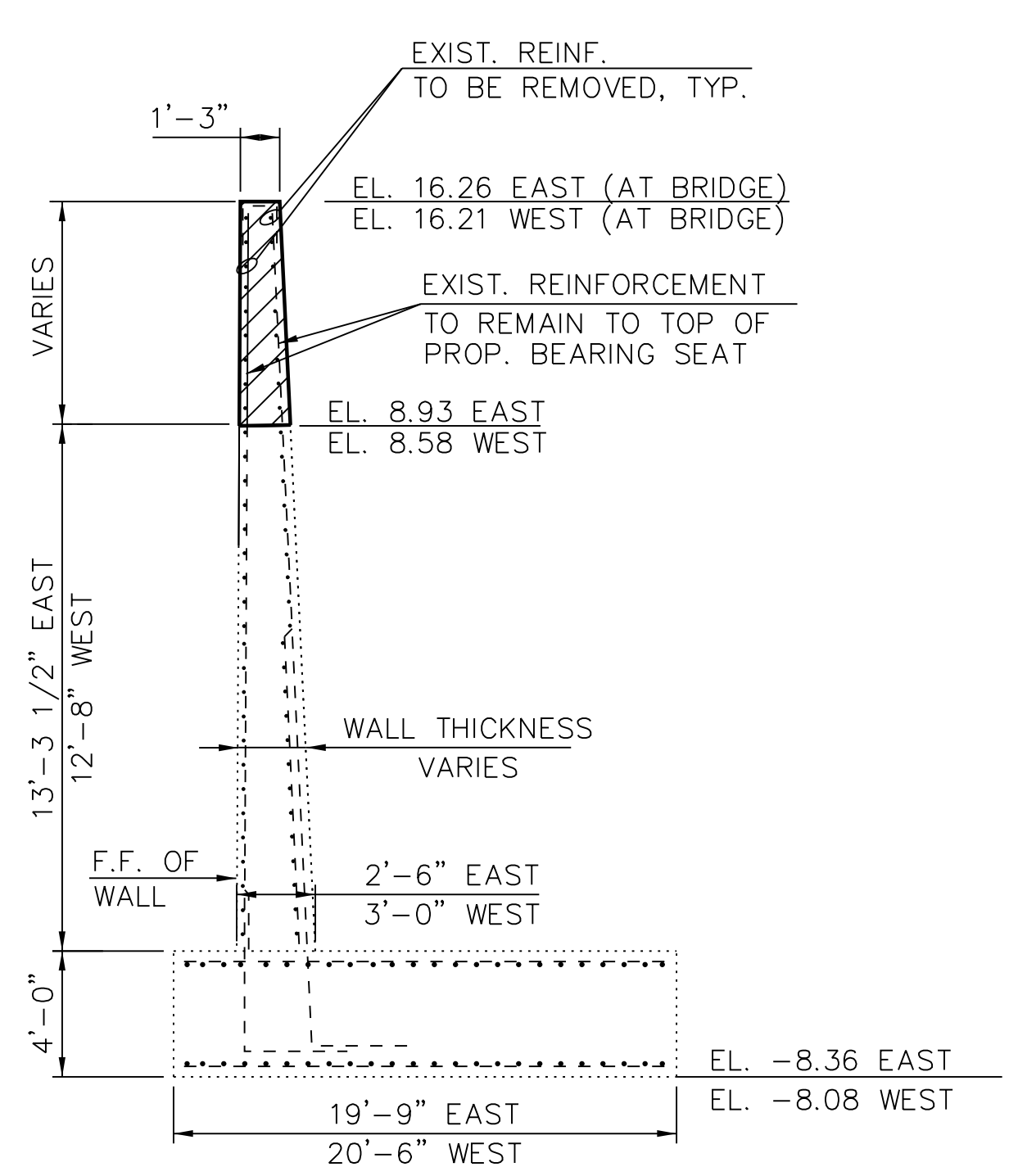
**2 EAST ABUTMENT AND EAST WALL ELEVATION**  
(LOOKING EAST)



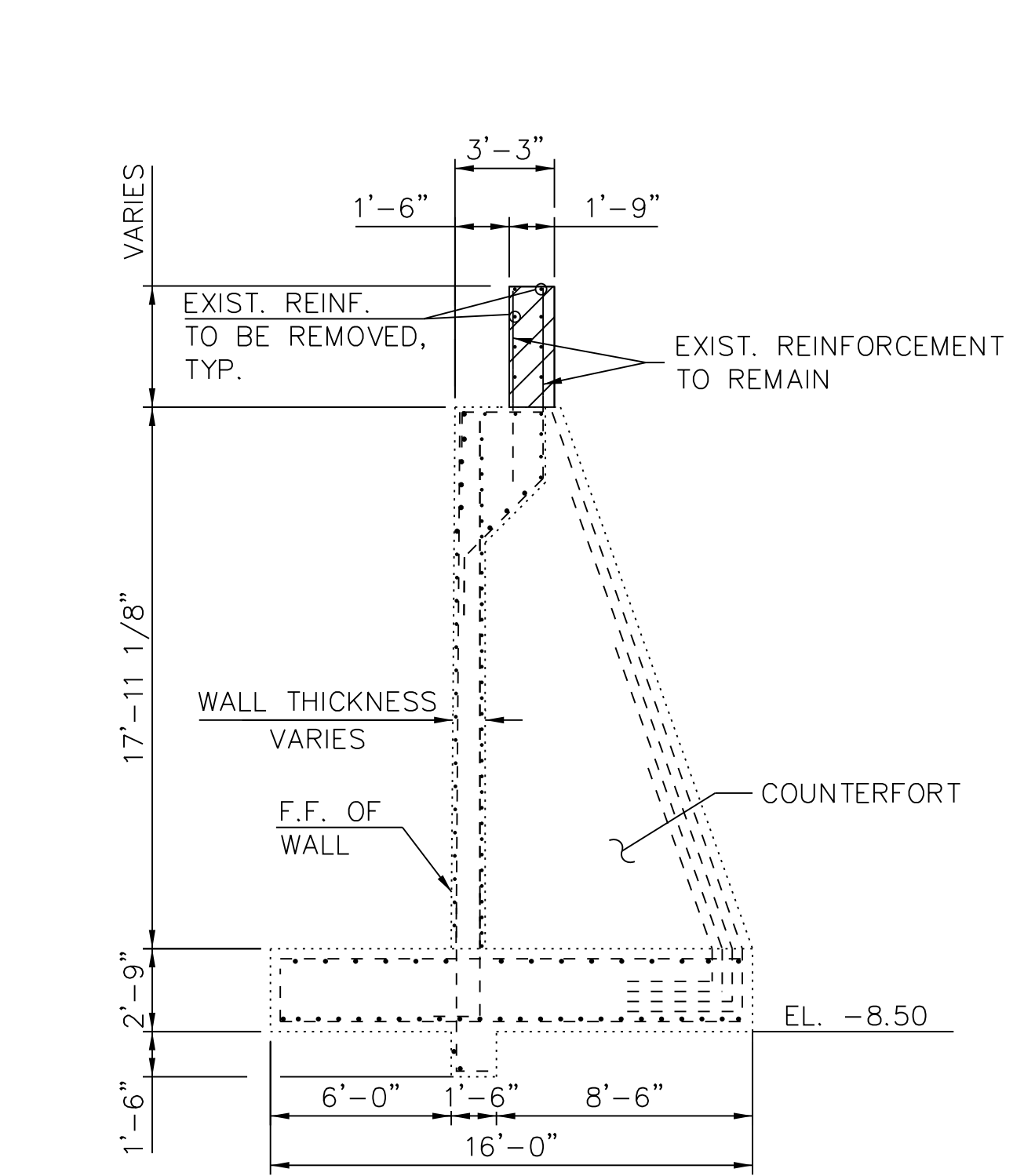
**3 WEST ABUTMENT AND WEST WALL PLAN**



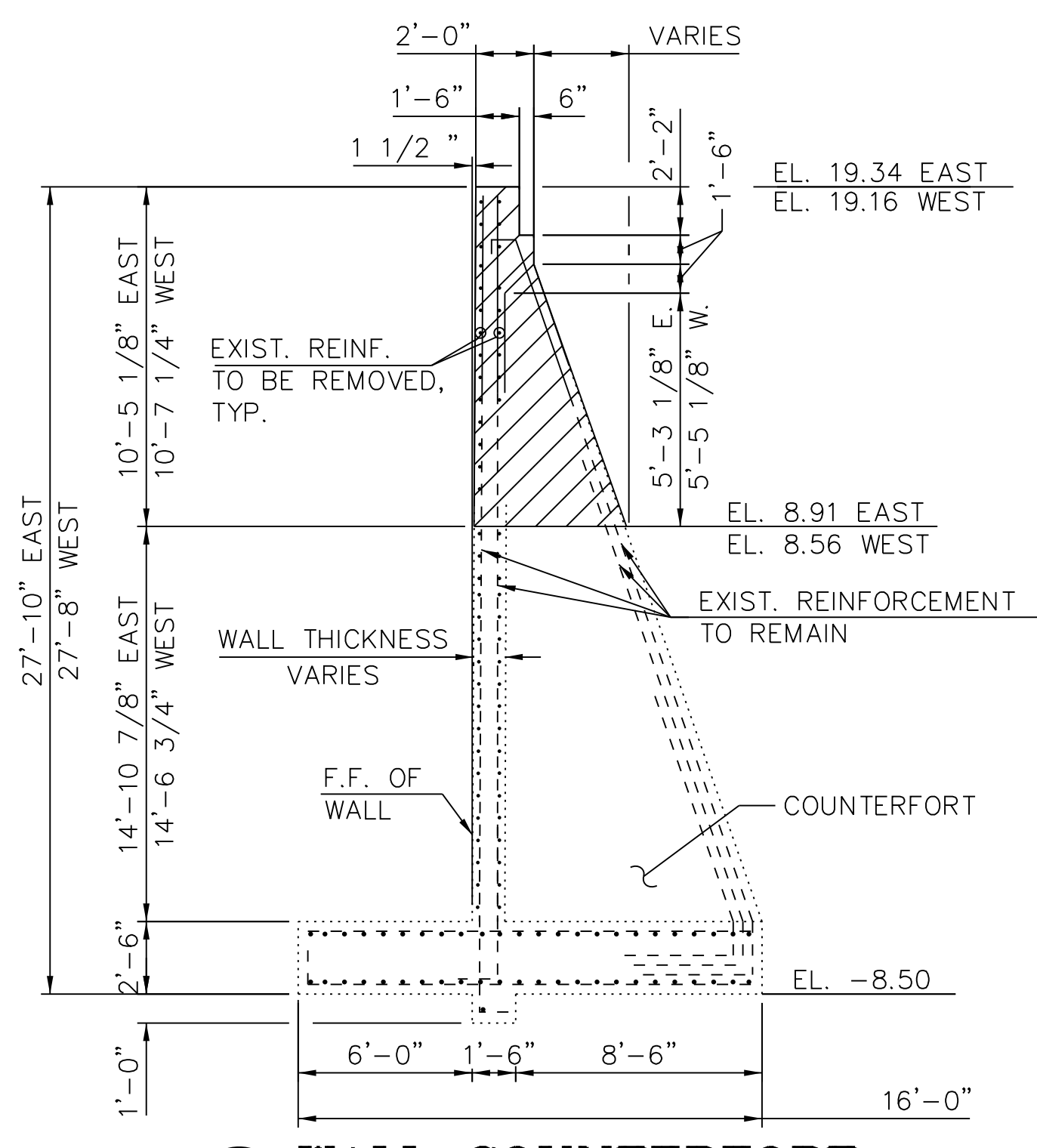
**4 EAST ABUTMENT AND EAST WALL PLAN**



**A WALL 2 / WALL 6**



**B ABUTMENT COUNTERFORT**



**C WALL COUNTERFORT**

LEGEND:  
 CONCRETE REMOVAL

- NOTES:
- DIMENSIONS SHOWN HAVE BEEN TAKEN FROM HISTORICAL DESIGN DRAWING AND MAY NOT REPRESENT "AS BUILT" CONDITIONS. THE CONTRACTOR SHALL VERIFY ALL DIMENSIONS IN THE FIELD. VARIATION IN THE FIELD DIMENSIONS SHALL NOT WARRANT ADDITIONAL COMPENSATION FOR REMOVAL OF EXISTING STRUCTURES. SEE SPECIAL PROVISIONS.
  - THE CONTRACTOR IS RESPONSIBLE TO PROTECT THE TRACKS AND HIGHWAY DURING REMOVAL OF THE SUPERSTRUCTURE AND SUBSTRUCTURE ELEMENTS FROM FALLING OBJECTS AND DEBRIS. ANY DAMAGES TO THE HIGHWAY OR CTA TRACKS BY THE CONTRACTOR DURING CONSTRUCTION SHALL BE THE CONTRACTOR'S RESPONSIBILITY AND SHALL BE REPAIRED AT NO INCREASE TO THE GMP.
  - EXISTING UTILITIES IN CONFLICT WITH NEW CONSTRUCTION SHALL BE ABANDONED OR RELOCATED ACCORDING TO DIRECTIONS GIVEN ON THE UTILITY PLANS.

**NOT FOR CONSTRUCTION**



**cta CHICAGO TRANSIT AUTHORITY ENGINEERING**

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**95th Terminal Improvement**  
 14 West 95th Street (North Terminal)  
 Chicago, IL 60628

IN CHARGE	S. KADERBEK
APPROVED BY	D.ZROKA
CHECKED BY	P.MAID
DESIGNED BY	L.SOMMER
DRAWN BY	T.SLEDGE
PROJECT NO.	2012-0021
FILE NAME	File Name

MARK	DATE	DESCRIPTION
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	03/07/18	INTERMEDIATE DESIGN

LOCATION IDENTIFIER: **DR095**  
 95TH ST. NORTH BUS BRIDGE  
 EXISTING STRUCTURE REMOVAL  
 DETAILS

**NBB-S-304**

FILE NAME = 070\_DR095\_S-304.dgn  
 PLOT DATE = 5/24/2018







**GIRDER 1-PROPOSED**

LOCATION	STATION	OFFSET	THEORETICAL GRADE ELEVATIONS	THEORETICAL GRADE ELEVATIONS ADJUSTED FOR DEAD LOAD DEFLECTION
BK. W. ABUT.	70+84.06	-19.58	17.61	17.61
☉ EXP. JOINT	70+85.64	-19.58	17.66	17.66
☉ BRG. W. ABUT.	70+86.56	-19.58	17.68	17.68
A	70+96.56	-19.58	17.92	17.94
B	71+06.56	-19.58	18.09	18.13
C	71+16.56	-19.58	18.20	18.26
D	71+26.56	-19.58	18.28	18.34
E	71+36.56	-19.58	18.36	18.41
F	71+46.56	-19.58	18.44	18.48
G	71+56.56	-19.58	18.52	18.55
H	71+66.56	-19.58	18.58	18.59
I	71+76.56	-19.58	18.61	18.61
☉ BRG. PIER	71+81.85	-19.58	18.61	18.61
J	71+91.85	-19.58	18.59	18.59
K	72+01.85	-19.58	18.53	18.55
L	72+11.85	-19.58	18.44	18.47
M	72+21.85	-19.58	18.35	18.39
N	72+31.85	-19.58	18.27	18.32
O	72+41.85	-19.58	18.18	18.24
P	72+51.85	-19.58	18.09	18.14
Q	72+61.85	-19.58	17.99	18.02
R	72+71.85	-19.58	17.85	17.86
☉ BRG. E. ABUT.	72+77.14	-19.58	17.76	17.76
☉ EXP. JOINT	72+78.06	-19.58	17.74	17.74
BK. E. ABUT.	72+79.64	-19.58	17.71	17.71

**GIRDER EX-1**

LOCATION	STATION	OFFSET	THEORETICAL GRADE ELEVATIONS	THEORETICAL GRADE ELEVATIONS ADJUSTED FOR DEAD LOAD DEFLECTION
BK. W. ABUT.	70+84.06	-14.75'	17.69	17.69
☉ EXP. JOINT	70+85.64	-14.75'	17.73	17.73
☉ BRG. W. ABUT.	70+86.56	-14.75'	17.76	17.76
A	70+96.56	-14.75'	17.99	18.01
B	71+06.56	-14.75'	18.16	18.20
C	71+16.56	-14.75'	18.27	18.32
D	71+26.56	-14.75'	18.35	18.40
E	71+36.56	-14.75'	18.43	18.48
F	71+46.56	-14.75'	18.51	18.55
G	71+56.56	-14.75'	18.59	18.61
H	71+66.56	-14.75'	18.65	18.66
I	71+76.56	-14.75'	18.68	18.68
☉ BRG. PIER	71+81.85	-14.75'	18.68	18.68
J	71+91.85	-14.75'	18.66	18.66
K	72+01.85	-14.75'	18.60	18.62
L	72+11.85	-14.75'	18.52	18.55
M	72+21.85	-14.75'	18.43	18.47
N	72+31.85	-14.75'	18.34	18.39
O	72+41.85	-14.75'	18.25	18.30
P	72+51.85	-14.75'	18.16	18.20
Q	72+61.85	-14.75'	18.06	18.09
R	72+71.85	-14.75'	17.92	17.93
☉ BRG. E. ABUT.	72+77.14	-14.75'	17.83	17.83
☉ EXP. JOINT	72+78.06	-14.75'	17.81	17.81
BK. E. ABUT.	72+79.64	-14.75'	17.78	17.78

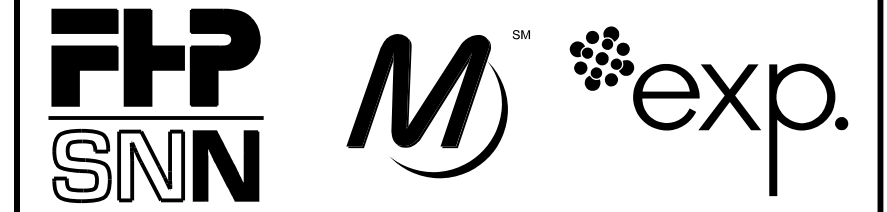
**GIRDER EX-2**

LOCATION	STATION	OFFSET	THEORETICAL GRADE ELEVATIONS	THEORETICAL GRADE ELEVATIONS ADJUSTED FOR DEAD LOAD DEFLECTION
BK. W. ABUT.	70+84.06	-8.92'	17.77	17.77
☉ EXP. JOINT	70+85.64	-8.92'	17.82	17.82
☉ BRG. W. ABUT.	70+86.56	-8.92'	17.84	17.84
A	70+96.56	-8.92'	18.08	18.11
B	71+06.56	-8.92'	18.25	18.30
C	71+16.56	-8.92'	18.36	18.43
D	71+26.56	-8.92'	18.44	18.51
E	71+36.56	-8.92'	18.52	18.59
F	71+46.56	-8.92'	18.60	18.65
G	71+56.56	-8.92'	18.68	18.71
H	71+66.56	-8.92'	18.74	18.76
I	71+76.56	-8.92'	18.77	18.77
☉ BRG. PIER	71+81.85	-8.92'	18.77	18.77
J	71+91.85	-8.92'	18.75	18.76
K	72+01.85	-8.92'	18.69	18.71
L	72+11.85	-8.92'	18.60	18.64
M	72+21.85	-8.92'	18.51	18.57
N	72+31.85	-8.92'	18.43	18.50
O	72+41.85	-8.92'	18.34	18.41
P	72+51.85	-8.92'	18.25	18.31
Q	72+61.85	-8.92'	18.15	18.19
R	72+71.85	-8.92'	18.01	18.03
☉ BRG. E. ABUT.	72+77.14	-8.92'	17.92	17.92
☉ EXP. JOINT	72+78.06	-8.92'	17.90	17.90
BK. E. ABUT.	72+79.64	-8.92'	17.87	17.87

**GIRDER EX-3**

LOCATION	STATION	OFFSET	THEORETICAL GRADE ELEVATIONS	THEORETICAL GRADE ELEVATIONS ADJUSTED FOR DEAD LOAD DEFLECTION
BK. W. ABUT.	70+84.06	-3.08'	17.86	17.86
☉ EXP. JOINT	70+85.64	-3.08'	17.91	17.91
☉ BRG. W. ABUT.	70+86.56	-3.08'	17.93	17.93
A	70+96.56	-3.08'	18.17	18.20
B	71+06.56	-3.08'	18.34	18.39
C	71+16.56	-3.08'	18.44	18.51
D	71+26.56	-3.08'	18.52	18.59
E	71+36.56	-3.08'	18.60	18.67
F	71+46.56	-3.08'	18.68	18.73
G	71+56.56	-3.08'	18.76	18.79
H	71+66.56	-3.08'	18.83	18.85
I	71+76.56	-3.08'	18.86	18.86
☉ BRG. PIER	71+81.85	-3.08'	18.86	18.86
J	71+91.85	-3.08'	18.83	18.84
K	72+01.85	-3.08'	18.78	18.80
L	72+11.85	-3.08'	18.69	18.73
M	72+21.85	-3.08'	18.60	18.66
N	72+31.85	-3.08'	18.51	18.58
O	72+41.85	-3.08'	18.42	18.49
P	72+51.85	-3.08'	18.33	18.39
Q	72+61.85	-3.08'	18.24	18.28
R	72+71.85	-3.08'	18.10	18.12
☉ BRG. E. ABUT.	72+77.14	-3.08'	18.00	18.00
☉ EXP. JOINT	72+78.06	-3.08'	17.99	17.99
BK. E. ABUT.	72+79.64	-3.08'	17.96	17.96

**NOT FOR CONSTRUCTION**



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**95th Terminal Improvement**  
 14 West 95th Street (North Terminal)  
 Chicago, IL 60628

<b>IN CHARGE</b>	<b>S. KADERBEK</b>
<b>APPROVED BY</b>	<b>D.ZROKA</b>
<b>CHECKED BY</b>	<b>P.MAID</b>
<b>DESIGNED BY</b>	<b>L.SOMMER</b>
<b>DRAWN BY</b>	<b>T.SLEDGE</b>
<b>PROJECT NO.</b>	<b>2012-0021</b>
<b>FILE NAME</b>	<b>File Name</b>

MARK	DATE	DESCRIPTION
	05/23/18	FINAL DESIGN
	03/07/18	INTERMEDIATE DESIGN

LOCATION IDENTIFIER: **DR095**

95TH ST. NORTH BUS BRIDGE  
 TOP OF SLAB ELEVATIONS I

**NBB-S-307**

vscorpp

FILE NAME = 070\_DR095\_S-307.dgn  
 PLOT DATE = 5/24/2018



**ROADWAY CROWN, BASELINE AND PGL  
NORTH BUS BRIDGE**

LOCATION	STATION	OFFSET	THEORETICAL GRADE ELEVATIONS	THEORETICAL GRADE ELEVATIONS ADJUSTED FOR DEAD LOAD DEFLECTION
BK. W. ABUT.	70+84.06	0.00'	17.91	17.91
☉ EXP. JOINT	70+85.64	0.00'	17.95	17.95
☉ BRG. W. ABUT.	70+86.56	0.00'	17.98	17.98
A	70+96.56	0.00'	18.21	18.24
B	71+06.56	0.00'	18.38	18.43
C	71+16.56	0.00'	18.49	18.56
D	71+26.56	0.00'	18.57	18.64
E	71+36.56	0.00'	18.65	18.72
F	71+46.56	0.00'	18.73	18.78
G	71+56.56	0.00'	18.81	18.84
H	71+66.56	0.00'	18.87	18.89
I	71+76.56	0.00'	18.90	18.90
☉ BRG. PIER	71+81.85	0.00'	18.90	18.90
J	71+91.85	0.00'	18.88	18.89
K	72+01.85	0.00'	18.82	18.84
L	72+11.85	0.00'	18.74	18.78
M	72+21.85	0.00'	18.65	18.71
N	72+31.85	0.00'	18.56	18.63
O	72+41.85	0.00'	18.47	18.54
P	72+51.85	0.00'	18.38	18.44
Q	72+61.85	0.00'	18.28	18.32
R	72+71.85	0.00'	18.14	18.16
☉ BRG. E. ABUT.	72+77.14	0.00'	18.05	18.05
☉ EXP. JOINT	72+78.06	0.00'	18.03	18.03
BK. E. ABUT.	72+79.64	0.00'	18.00	18.00

**GIRDER EX-5**

LOCATION	STATION	OFFSET	THEORETICAL GRADE ELEVATIONS	THEORETICAL GRADE ELEVATIONS ADJUSTED FOR DEAD LOAD DEFLECTION
BK. W. ABUT.	70+84.06	8.58'	17.78	17.78
☉ EXP. JOINT	70+85.64	8.58'	17.82	17.82
☉ BRG. W. ABUT.	70+86.56	8.58'	17.85	17.85
A	70+96.56	8.58'	18.08	18.11
B	71+06.56	8.58'	18.25	18.30
C	71+16.56	8.58'	18.36	18.43
D	71+26.56	8.58'	18.44	18.51
E	71+36.56	8.58'	18.52	18.59
F	71+46.56	8.58'	18.60	18.65
G	71+56.56	8.58'	18.68	18.71
H	71+66.56	8.58'	18.74	18.76
I	71+76.56	8.58'	18.77	18.77
☉ BRG. PIER	71+81.85	8.58'	18.77	18.77
J	71+91.85	8.58'	18.75	18.76
K	72+01.85	8.58'	18.70	18.72
L	72+11.85	8.58'	18.61	18.65
M	72+21.85	8.58'	18.52	18.58
N	72+31.85	8.58'	18.43	18.50
O	72+41.85	8.58'	18.34	18.41
P	72+51.85	8.58'	18.25	18.31
Q	72+61.85	8.58'	18.15	18.19
R	72+71.85	8.58'	18.01	18.03
☉ BRG. E. ABUT.	72+77.14	8.58'	17.92	17.92
☉ EXP. JOINT	72+78.06	8.58'	17.90	17.90
BK. E. ABUT.	72+79.64	8.58'	17.87	17.87

**GIRDER EX-4**

LOCATION	STATION	OFFSET	THEORETICAL GRADE ELEVATIONS	THEORETICAL GRADE ELEVATIONS ADJUSTED FOR DEAD LOAD DEFLECTION
BK. W. ABUT.	70+84.06	2.75'	17.87	17.87
☉ EXP. JOINT	70+85.64	2.75'	17.91	17.91
☉ BRG. W. ABUT.	70+86.56	2.75'	17.94	17.94
A	70+96.56	2.75'	18.17	18.20
B	71+06.56	2.75'	18.34	18.39
C	71+16.56	2.75'	18.45	18.52
D	71+26.56	2.75'	18.53	18.60
E	71+36.56	2.75'	18.61	18.68
F	71+46.56	2.75'	18.69	18.74
G	71+56.56	2.75'	18.77	18.80
H	71+66.56	2.75'	18.83	18.85
I	71+76.56	2.75'	18.86	18.86
☉ BRG. PIER	71+81.85	2.75'	18.86	18.86
J	71+91.85	2.75'	18.84	18.85
K	72+01.85	2.75'	18.78	18.80
L	72+11.85	2.75'	18.70	18.74
M	72+21.85	2.75'	18.61	18.67
N	72+31.85	2.75'	18.52	18.59
O	72+41.85	2.75'	18.43	18.50
P	72+51.85	2.75'	18.34	18.40
Q	72+61.85	2.75'	18.24	18.28
R	72+71.85	2.75'	18.10	18.12
☉ BRG. E. ABUT.	72+77.14	2.75'	18.01	18.01
☉ EXP. JOINT	72+78.06	2.75'	17.99	17.99
BK. E. ABUT.	72+79.64	2.75'	17.96	17.96

**GIRDER EX-6**

LOCATION	STATION	OFFSET	THEORETICAL GRADE ELEVATIONS	THEORETICAL GRADE ELEVATIONS ADJUSTED FOR DEAD LOAD DEFLECTION
BK. W. ABUT.	70+84.06	14.42'	17.69	17.69
☉ EXP. JOINT	70+85.64	14.42'	17.74	17.74
☉ BRG. W. ABUT.	70+86.56	14.42'	17.76	17.76
A	70+96.56	14.42'	18.00	18.02
B	71+06.56	14.42'	18.17	18.21
C	71+16.56	14.42'	18.27	18.32
D	71+26.56	14.42'	18.35	18.40
E	71+36.56	14.42'	18.43	18.48
F	71+46.56	14.42'	18.51	18.55
G	71+56.56	14.42'	18.59	18.61
H	71+66.56	14.42'	18.66	18.67
I	71+76.56	14.42'	18.69	18.69
☉ BRG. PIER	71+81.85	14.42'	18.69	18.69
J	71+91.85	14.42'	18.66	18.66
K	72+01.85	14.42'	18.61	18.63
L	72+11.85	14.42'	18.52	18.55
M	72+21.85	14.42'	18.43	18.47
N	72+31.85	14.42'	18.34	18.39
O	72+41.85	14.42'	18.25	18.30
P	72+51.85	14.42'	18.16	18.20
Q	72+61.85	14.42'	18.07	18.10
R	72+71.85	14.42'	17.93	17.94
☉ BRG. E. ABUT.	72+77.14	14.42'	17.83	17.83
☉ EXP. JOINT	72+78.06	14.42'	17.82	17.82
BK. E. ABUT.	72+79.64	14.42'	17.79	17.79

**NOT FOR  
CONSTRUCTION**



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**95th Terminal Improvement**  
14 West 95th Street (North Terminal)  
Chicago, IL 60628

<b>IN CHARGE</b>	<b>S. KADERBEK</b>
<b>APPROVED BY</b>	<b>D.ZROKA</b>
<b>CHECKED BY</b>	<b>P.MAID</b>
<b>DESIGNED BY</b>	<b>L.SOMMER</b>
<b>DRAWN BY</b>	<b>T.SLEDGE</b>
<b>PROJECT NO.</b>	<b>2012-0021</b>
<b>FILE NAME</b>	<b>File Name</b>

MARK	DATE	DESCRIPTION
	05/23/18	FINAL DESIGN
	03/07/18	INTERMEDIATE DESIGN

LOCATION IDENTIFIER: **DR095**

95TH ST. NORTH BUS BRIDGE  
TOP OF SLAB ELEVATIONS II

**NBB-S-308**

vscorp

FILE NAME = 070\_DR095\_S-308.dgn  
PLOT DATE = 5/24/2018

vscorp

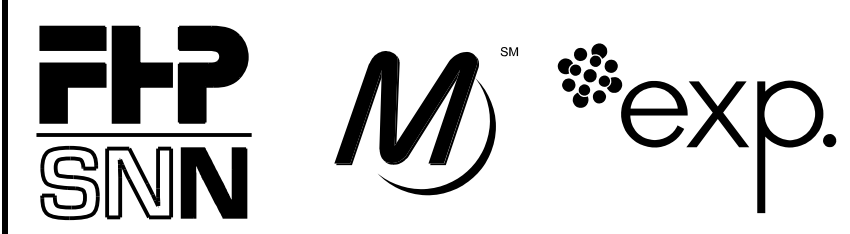
**NOT FOR CONSTRUCTION**

### GIRDER 2-PROPOSED

LOCATION	STATION	OFFSET	THEORETICAL GRADE ELEVATIONS	THEORETICAL GRADE ELEVATIONS ADJUSTED FOR DEAD LOAD DEFLECTION
BK. W. ABUT.	70+84.06	17.42	17.65	17.65
☉ EXP. JOINT	70+85.64	17.42	17.69	17.69
☉ BRG. W. ABUT.	70+86.56	17.42	17.72	17.72
A	70+96.56	17.42	17.95	18.02
B	71+06.56	17.42	18.12	18.24
C	71+16.56	17.42	18.23	18.38
D	71+26.56	17.42	18.31	18.46
E	71+36.56	17.42	18.39	18.53
F	71+46.56	17.42	18.47	18.57
G	71+56.56	17.42	18.55	18.61
H	71+66.56	17.42	18.61	18.64
I	71+76.56	17.42	18.64	18.64
☉ BRG. PIER	71+81.85	17.42	18.64	18.64
J	71+91.85	17.42	18.62	18.63
K	72+01.85	17.42	18.56	18.60
L	72+11.85	17.42	18.48	18.56
M	72+21.85	17.42	18.39	18.51
N	72+31.85	17.42	18.30	18.45
O	72+41.85	17.42	18.21	18.36
P	72+51.85	17.42	18.12	18.26
Q	72+61.85	17.42	18.02	18.11
R	72+71.85	17.42	17.88	17.88
☉ BRG. E. ABUT.	72+77.14	17.42	17.79	17.79
☉ EXP. JOINT	72+78.06	17.42	17.77	17.77
BK. E. ABUT.	72+79.64	17.42	17.74	17.74

### GIRDER 3-PROPOSED

LOCATION	STATION	OFFSET	THEORETICAL GRADE ELEVATIONS	THEORETICAL GRADE ELEVATIONS ADJUSTED FOR DEAD LOAD DEFLECTION
BK. W. ABUT.	70+84.06	21.42	17.59	17.59
☉ EXP. JOINT	70+85.64	21.42	17.63	17.63
☉ BRG. W. ABUT.	70+86.56	21.42	17.66	17.66
A	70+96.56	21.42	17.89	17.96
B	71+06.56	21.42	18.06	18.18
C	71+16.56	21.42	18.17	18.32
D	71+26.56	21.42	18.25	18.40
E	71+36.56	21.42	18.33	18.47
F	71+46.56	21.42	18.41	18.51
G	71+56.56	21.42	18.49	18.55
H	71+66.56	21.42	18.55	18.58
I	71+76.56	21.42	18.58	18.58
☉ BRG. PIER	71+81.85	21.42	18.58	18.58
J	71+91.85	21.42	18.56	18.57
K	72+01.85	21.42	18.50	18.54
L	72+11.85	21.42	18.42	18.50
M	72+21.85	21.42	18.33	18.45
N	72+31.85	21.42	18.24	18.39
O	72+41.85	21.42	18.15	18.30
P	72+51.85	21.42	18.06	18.20
Q	72+61.85	21.42	17.96	18.05
R	72+71.85	21.42	17.82	17.82
☉ BRG. E. ABUT.	72+77.14	21.42	17.73	17.73
☉ EXP. JOINT	72+78.06	21.42	17.71	17.71
BK. E. ABUT.	72+79.64	21.42	17.68	17.68



### BEAM 4-PROPOSED

LOCATION	STATION	OFFSET	THEORETICAL GRADE ELEVATIONS	THEORETICAL GRADE ELEVATIONS ADJUSTED FOR DEAD LOAD DEFLECTION
BK W. ABUT	70+84.06	26.60	17.51	17.51
CL EXP JT	70+85.64	26.60	17.55	17.55
CL W. ABUT	70+86.56	26.60	17.58	17.58
END	70+91.46	26.60	17.70	17.74

### BEAM 6-PROPOSED

LOCATION	STATION	OFFSET	THEORETICAL GRADE ELEVATIONS	THEORETICAL GRADE ELEVATIONS ADJUSTED FOR DEAD LOAD DEFLECTION
START	72+72.30	26.26	17.74	17.77
CL E. ABUT	72+77.14	26.26	17.66	17.66
CL EXP JT	72+78.06	26.26	17.64	17.64
BK E. ABUT	72+79.64	26.26	17.61	17.61

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IN CHARGE	S. KADERBEK
APPROVED BY	D.ZROKA
CHECKED BY	P.MAID
DESIGNED BY	L.SOMMER
DRAWN BY	T.SLEDGE
PROJECT NO.	2012-0021
FILE NAME	File Name
05/23/18	FINAL DESIGN
03/07/18	INTERMEDIATE DESIGN
MARK	DATE DESCRIPTION

### BEAM 5-PROPOSED

LOCATION	STATION	OFFSET	THEORETICAL GRADE ELEVATIONS	THEORETICAL GRADE ELEVATIONS ADJUSTED FOR DEAD LOAD DEFLECTION
BK W. ABUT	70+84.06	34.29	17.39	17.39
CL EXP JT	70+85.64	32.70	17.46	17.46
CL W. ABUT	70+86.56	31.79	17.50	17.50
END	70+96.94	21.42	17.90	17.97

### BEAM 7-PROPOSED

LOCATION	STATION	OFFSET	THEORETICAL GRADE ELEVATIONS	THEORETICAL GRADE ELEVATIONS ADJUSTED FOR DEAD LOAD DEFLECTION
START	72+67.45	21.42	17.89	17.95
CL E. ABUT	72+77.14	31.10	17.58	17.58
CL EXP JT	72+78.06	32.02	17.55	17.55
BK E. ABUT	72+79.64	33.60	17.50	17.50

NOTE: THEORETICAL GRADE ELEVATIONS OF GIRDERS AND BEAMS LOCATED BELOW SIDEWALK ARE GIVEN ASSUMING TOP OF DECK MAINTAINS 1.5% CROSS SLOPE BELOW SIDEWALK.

LOCATION IDENTIFIER: **DR095**  
**95TH ST. NORTH BUS BRIDGE**  
**TOP OF SLAB ELEVATIONS III**  
**NBB-S-309**

FILE NAME = 070\_DR095\_S-309.dgn  
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PLOT DATE = 5/24/2018

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**95th Terminal Improvement**  
14 West 95th Street (North Terminal)  
Chicago, IL 60628

IN CHARGE	S. KADERBEK
APPROVED BY	D.ZROKA
CHECKED BY	P.MAID
DESIGNED BY	L.SOMMER
DRAWN BY	T.SLEDGE
PROJECT NO.	2012-0021
FILE NAME	File Name

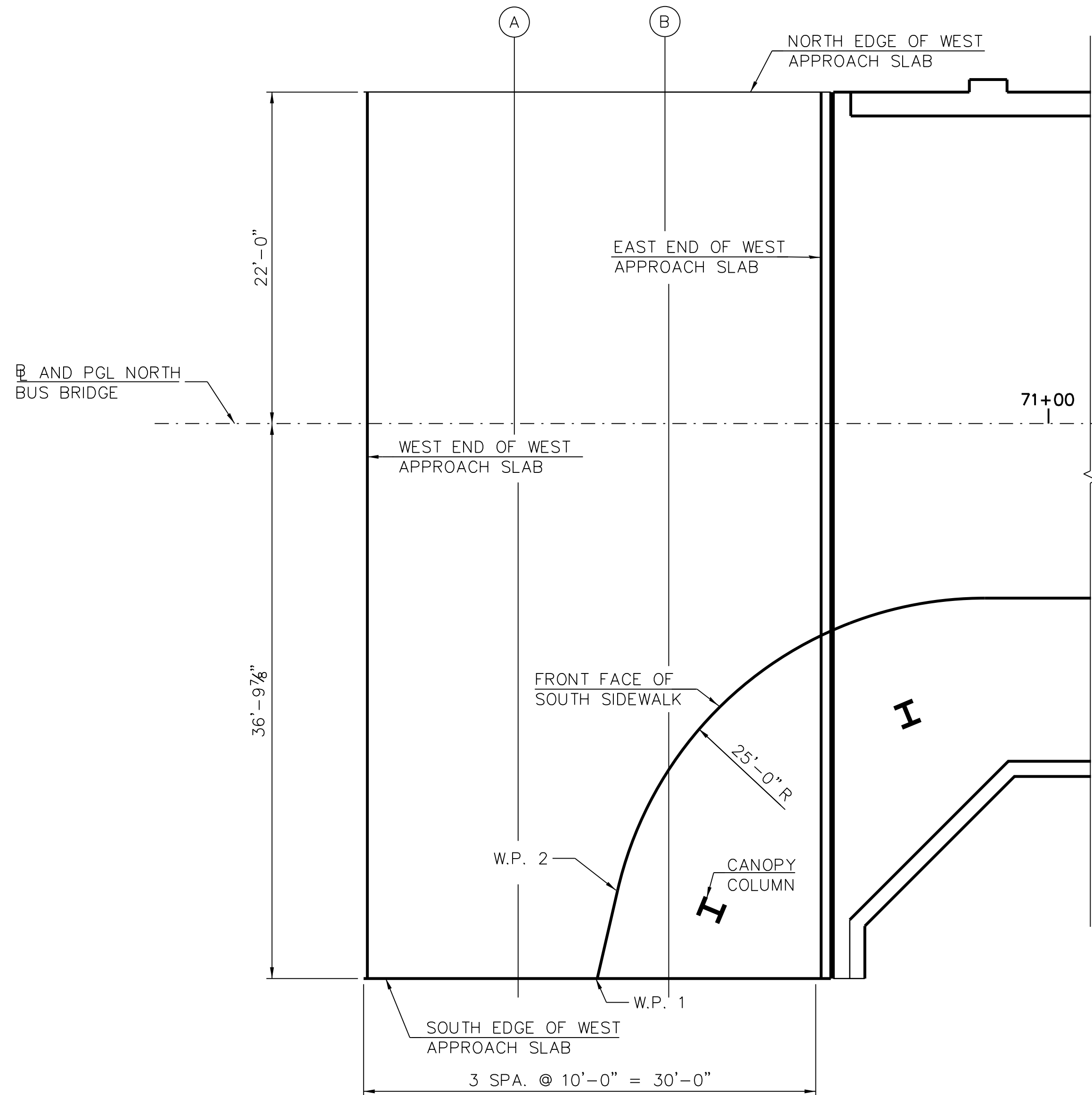
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MARK	DATE	DESCRIPTION

LOCATION IDENTIFIER: **DR095**

95TH ST. NORTH BUS BRIDGE

**NBB-S-310**

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1 PLAN

**NORTH EDGE OF WEST APPROACH SLAB**

LOCATION	STATION	OFFSET (FT)	THEORETICAL GRADE ELEVATIONS
W. END APPROACH SLAB	70+54.56	-22.00	16.75
A	70+64.56	-22.00	17.07
B	70+74.56	-22.00	17.36
E. END APPROACH SLAB	70+84.56	-22.00	17.59

**BASELINE AND PGL (NORTH BUS BRIDGE)**

LOCATION	STATION	OFFSET (FT)	THEORETICAL GRADE ELEVATIONS
W. END APPROACH SLAB	70+54.56	0.00	16.84
A	70+64.56	0.00	17.24
B	70+74.56	0.00	17.61
E. END APPROACH SLAB	70+84.56	0.00	17.92

**FRONT FACE OF SOUTH SIDEWALK**

LOCATION	STATION	OFFSET (FT)	THEORETICAL GRADE ELEVATIONS	TOP OF SIDEWALK ELEVATIONS
W.P. 1	70+70.07	36.83	17.60	18.29
W.P. 2	70+71.40	31.04	17.54	18.28
B	70+74.81	22.97	17.48	18.28
E. END APPROACH SLAB	70+84.56	14.24	17.71	18.56

**SOUTH EDGE OF WEST APPROACH SLAB**

LOCATION	STATION	OFFSET	THEORETICAL GRADE ELEVATIONS	TOP OF SIDEWALK ELEVATIONS
W. END APPROACH SLAB	70+54.56	36.82	17.14	-
A	70+64.56	36.82	17.44	-
B	70+74.56	36.82	17.41	18.34
E. END APPROACH SLAB	70+84.56	36.82	17.37	18.50

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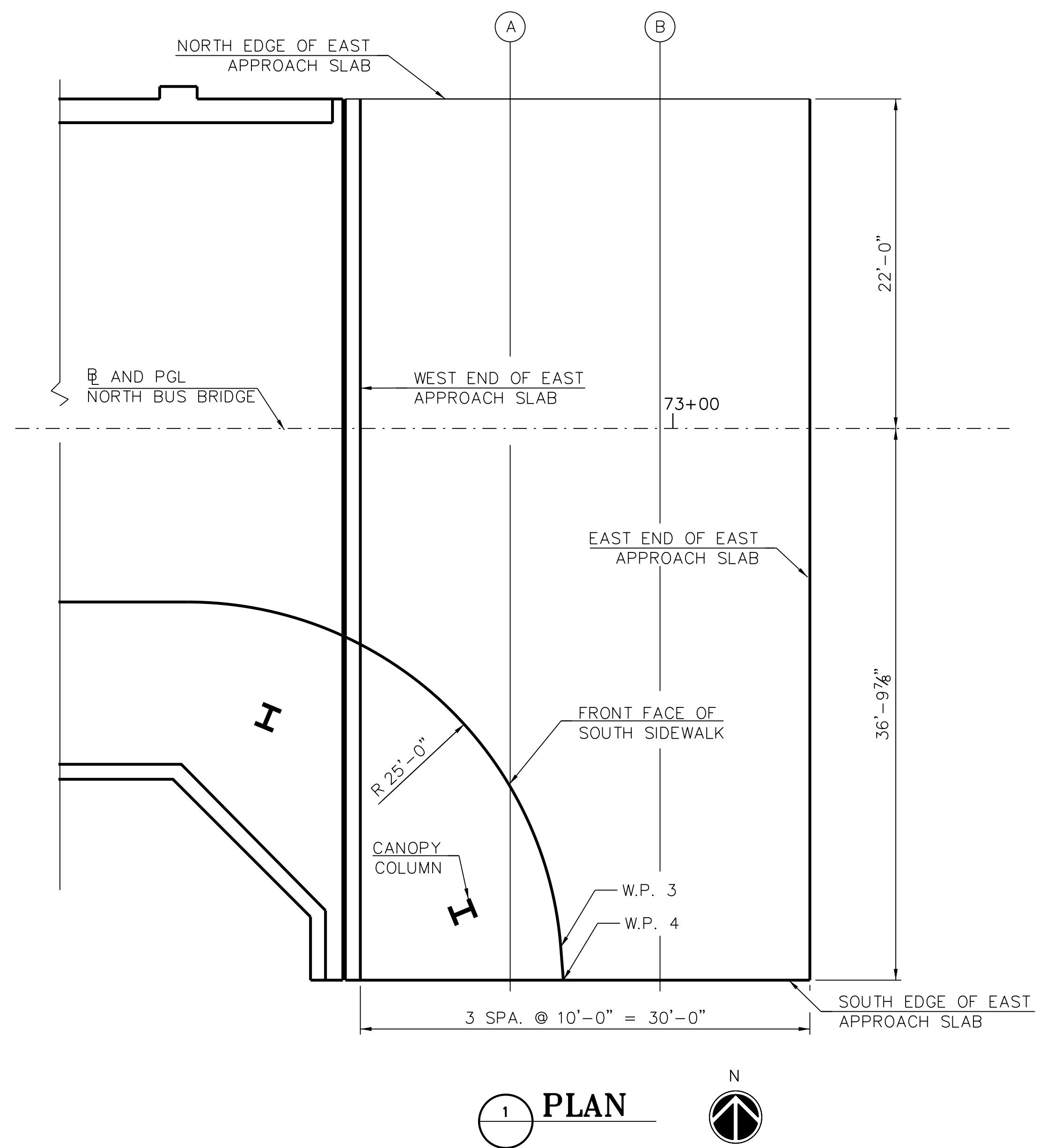
LOCATION IDENTIFIER: **DR095**

95TH ST. NORTH BUS BRIDGE  
 TOP OF WEST APPROACH  
 SLAB ELEVATIONS

**NBB-S-311**

FILE NAME = 070\_DR095\_S-311.dgn  
 PLOT DATE = 5/24/2018

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1 PLAN

**NORTH EDGE OF EAST APPROACH SLAB**

LOCATION	STATION	OFFSET (FT.)	THEORETICAL GRADE ELEVATIONS
W. END APPROACH SLAB	72+79.14	-22.00	17.68
A	72+89.14	-22.00	17.53
B	72+99.14	-22.00	17.36
E. END APPROACH SLAB	73+09.14	-22.00	17.14

**BASELINE AND PGL (NORTH BUS BRIDGE)**

LOCATION	STATION	OFFSET (FT.)	THEORETICAL GRADE ELEVATIONS
W. END APPROACH SLAB	72+79.14	0.00	18.01
A	72+89.14	0.00	17.79
B	72+99.14	0.00	17.53
E. END APPROACH SLAB	73+09.14	0.00	17.22

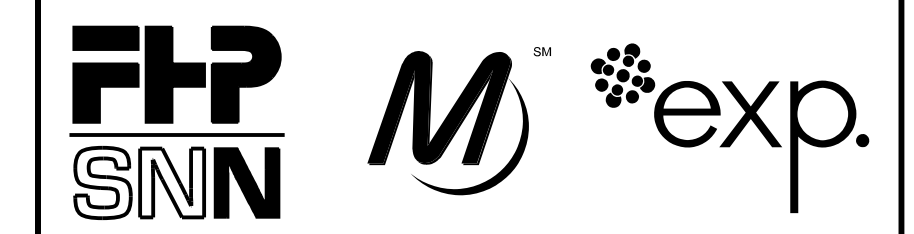
**FRONT FACE OF SOUTH SIDEWALK**

LOCATION	STATION	OFFSET (FT.)	THEORETICAL GRADE ELEVATIONS	TOP OF SIDEWALK ELEVATIONS
W. END APPROACH SLAB	72+79.14	14.41	17.79	18.58
A	72+89.14	23.90	17.63	18.34
W.P. 3	72+92.52	34.62	17.68	18.36
W.P. 4	72+92.69	36.82	17.69	18.37

**SOUTH EDGE OF EAST APPROACH SLAB**

LOCATION	STATION	OFFSET (FT.)	THEORETICAL GRADE ELEVATIONS	TOP OF SIDEWALK ELEVATIONS
W. END APPROACH SLAB	72+79.14	36.82	17.46	18.56
A	72+89.14	36.82	17.51	18.41
B	72+99.14	36.82	17.55	-
E. END APPROACH SLAB	73+09.14	36.82	17.34	-

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LOCATION IDENTIFIER: **DR095**

95TH ST. NORTH BUS BRIDGE  
 TOP OF EAST APPROACH  
 SLAB ELEVATIONS

**NBB-S-312**

FILE NAME = 070\_DR095\_S-312.dgn  
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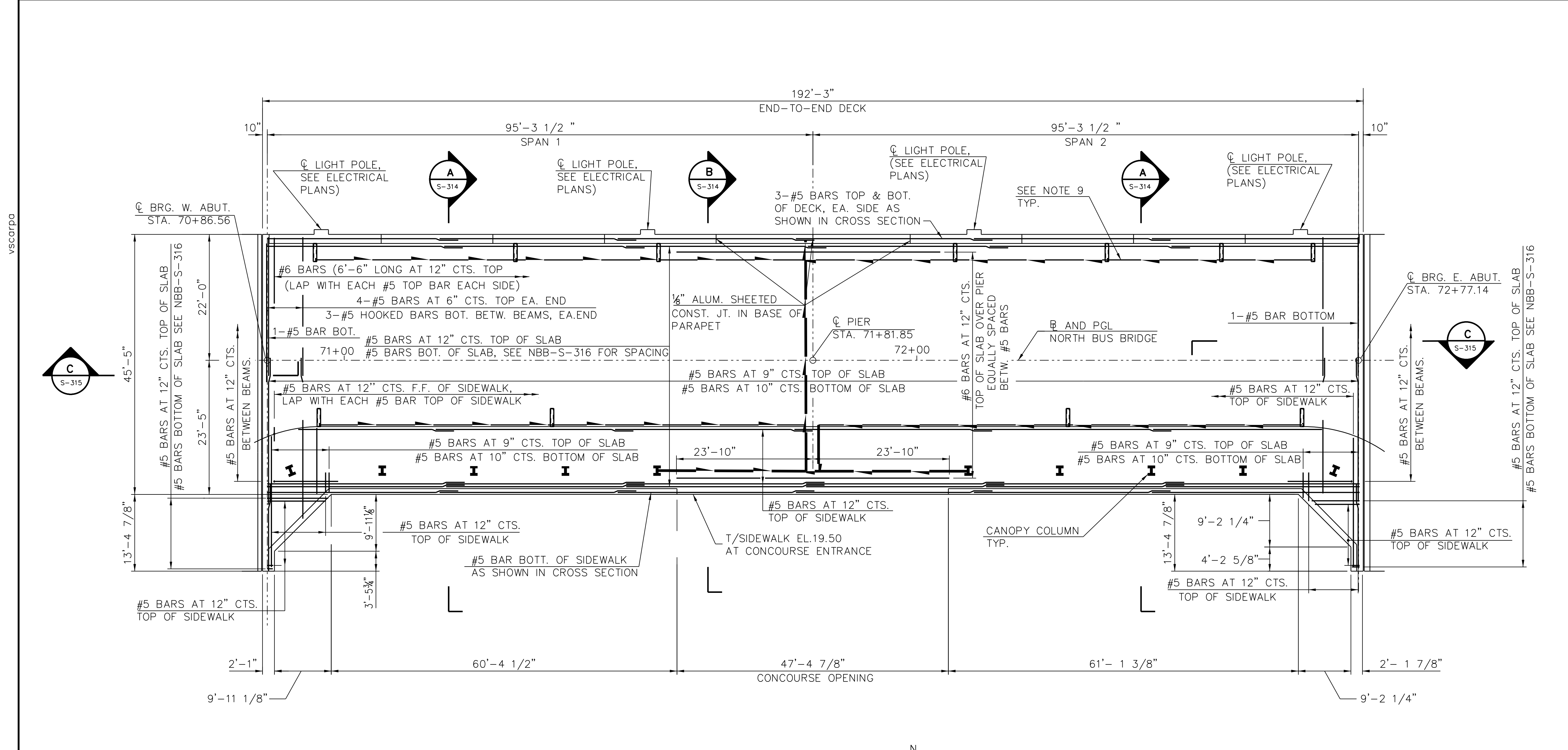
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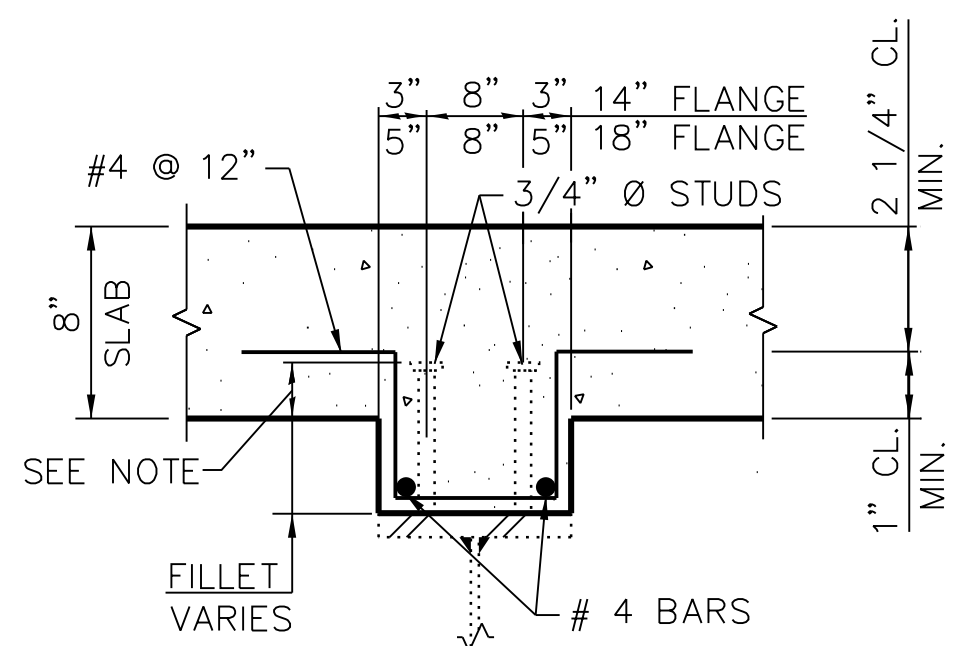
LOCATION IDENTIFIER: **DR095**

**95TH ST. NORTH BUS BRIDGE DECK PLAN**

**NBB-S-313**



**1 DECK PLAN**



**SECTION SHOWING EXISTING FILLET**

**NOTE:**  
ADD REINFORCEMENT SHOWN AT EXISTING GIRDERS WHERE CONTRACTOR HAS DETERMINED IN THE FIELD THAT THE EXISTING SHEAR STUD DOES NOT PROTRUDE INTO PROPOSED DECK A MINIMUM 2"

**NOTES:**

- FOR SECTIONS A & B, SEE SHEET NBB-S-314. FOR SECTION C, SEE NBB-S-315
- FOR APPROACH SLAB DETAILS, SEE SHEETS NBB-S-317 THRU NBB-S-320.
- FOR EXPANSION JOINT BETWEEN DECK AND CONCOURSE, SEE ARCHITECTURAL DRAWINGS.
- FOR DECK DETAILS AND REINFORCEMENT AT CANOPY COLUMNS SEE SHEET NBB-S-318.
- TOP AND BOTTOM LONGITUDINAL BARS SHALL NOT BE LAPPED AT THE SAME LOCATIONS IN THE DECK, NOR SHALL THE TOP AND BOTTOM TRANSVERSE BARS BE LAPPED AT THE SAME LOCATION.
- FOR PARAPET ELEVATIONS AND DETAILS, SEE SHEET NBB-S-315.
- OR SECTIONS THRU PARAPETS, PARTIAL DETAIL AT LIGHT POLE, BAR DIAGRAMS AND MINIMUM BAR LAPS, SEE SHEET NBB-S-316.
- FOR DRAINAGE SCUPPER AND CLOSED DRAINAGE SYSTEM DETAILS, SEE NBB-S-316A AND NBB-S-316B.

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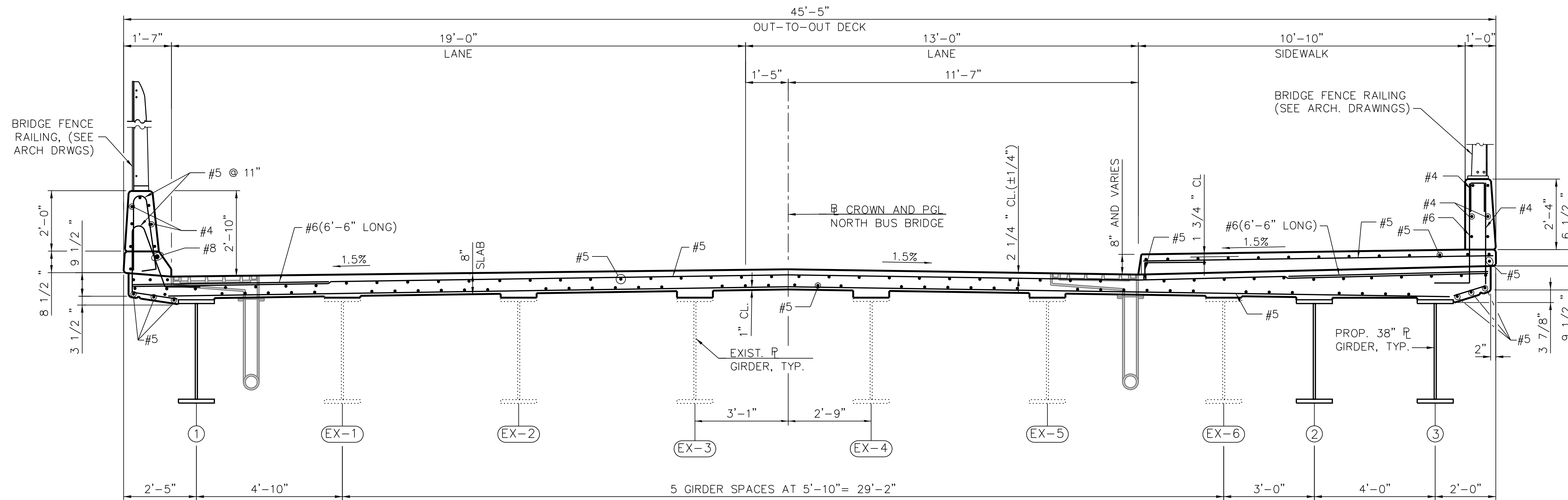
IN CHARGE	S. KADERBEK
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DESIGNED BY	L.SOMMER
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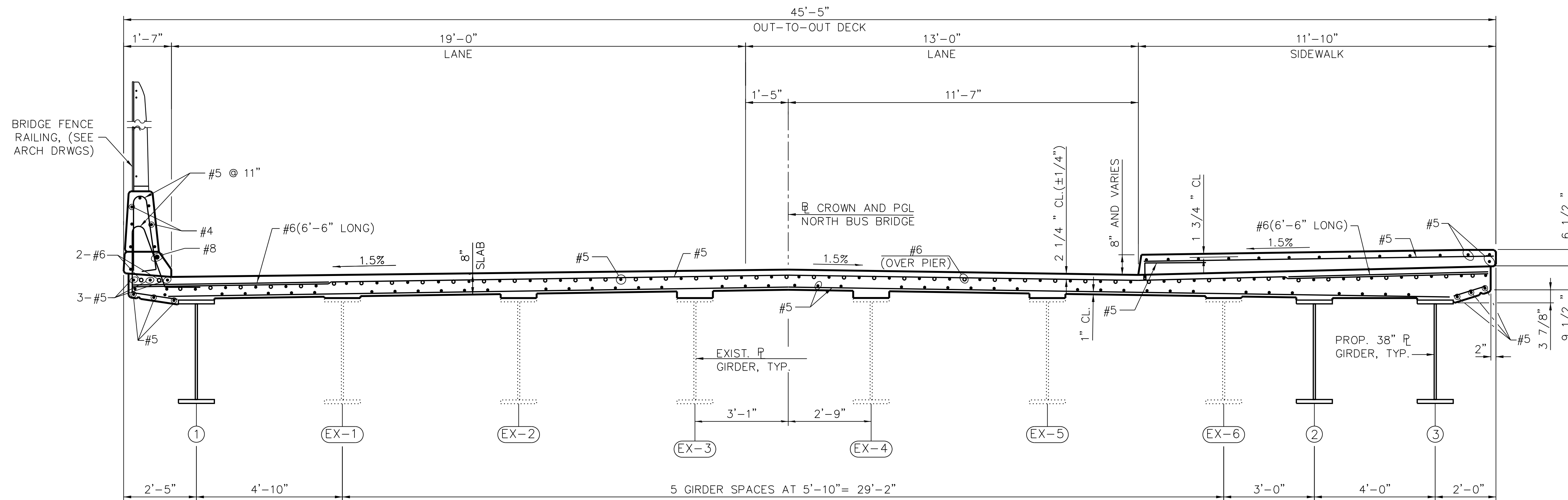
LOCATION IDENTIFIER: **DR095**

**95TH ST. NORTH BUS BRIDGE DECK CROSS SECTION**

**NBB-S-314**



**A DECK CROSS SECTION**  
(AT MIDSPAN)



**B DECK CROSS SECTION**  
(AT PIER)

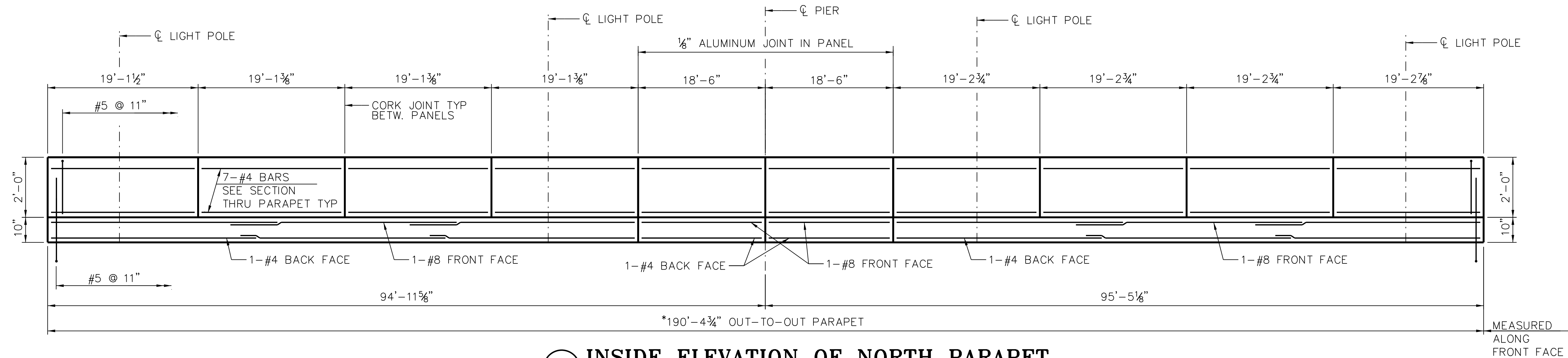
NOTE:  
1. FOR NOTES, SEE SHEET S-313.

NOTE:  
ELECTRICAL CONDUITS LOCATED IN BAYS EX-1 TO EX-2 AND EX-5 TO EX-6 TO BE CLAMPED WITH UNISTRUT CLAMP (OR APPROVED EQUAL) TO DIAPHRAGMS.

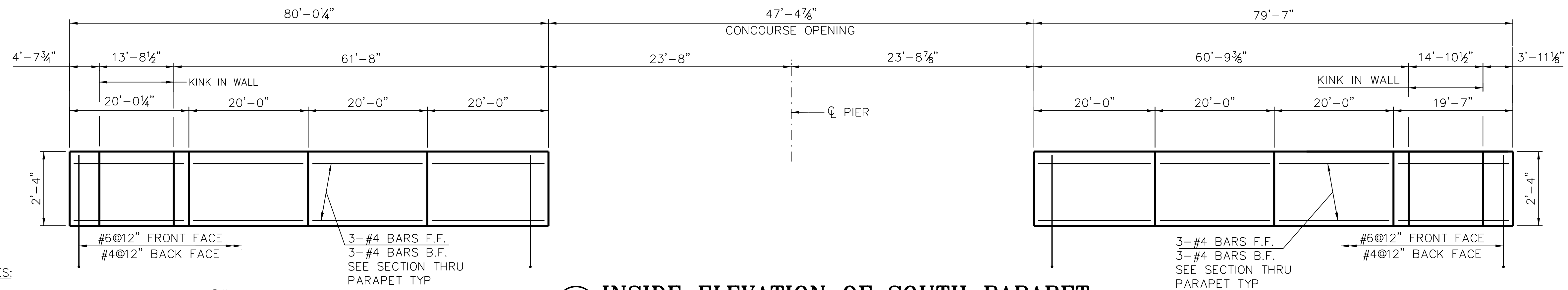
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PLOT DATE = 5/24/2018

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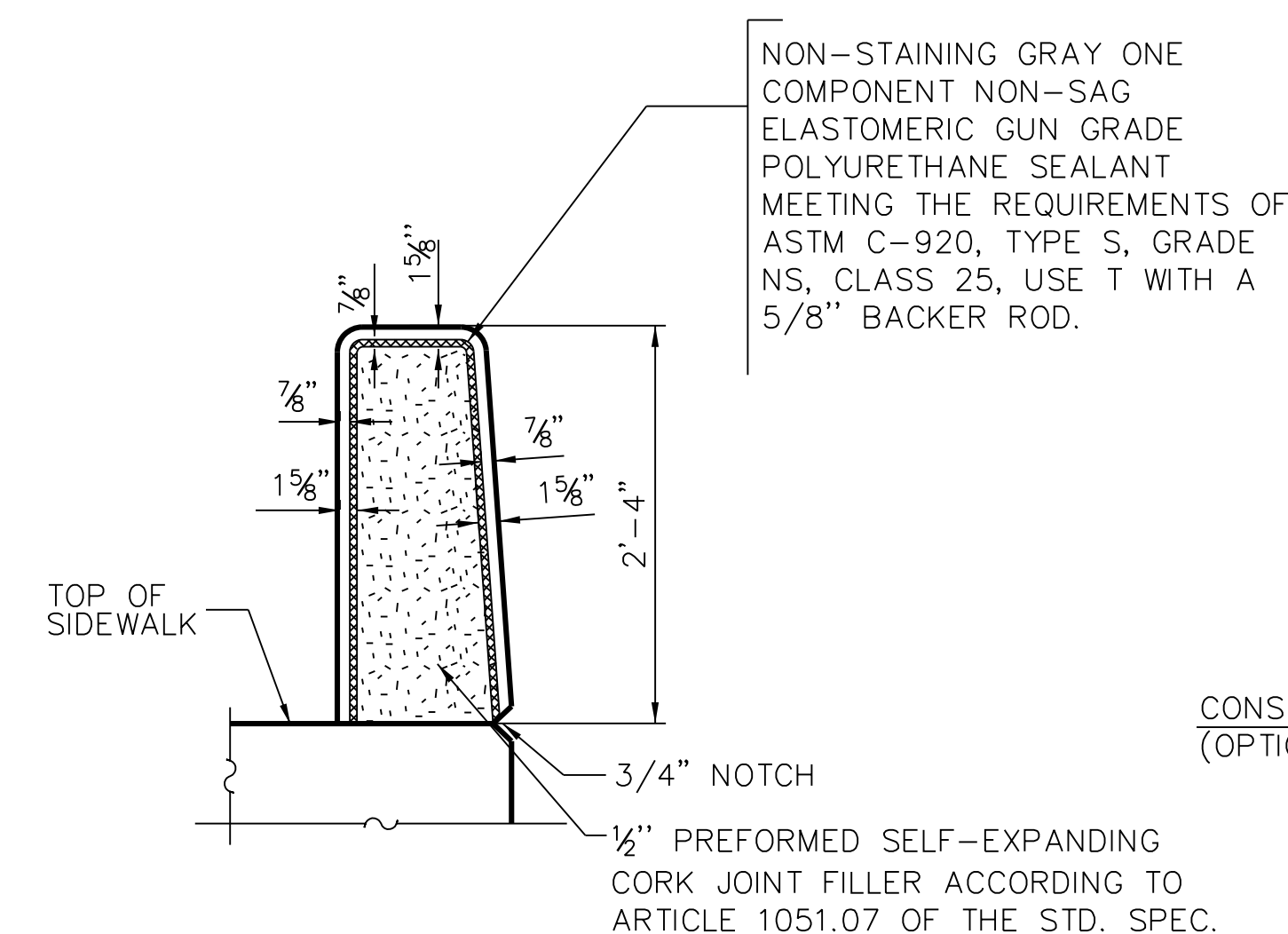


**1 INSIDE ELEVATION OF NORTH PARAPET**  
 (DIMENSION SHOWN MEASURED ALONG INSIDE FACE OF PARAPET LOOKING NORTH)  
 \*WALL LENGTH TO BE FIELD VERIFIED, END OF WALL TO MATCH FACE OF ADJACENT MOMENT SLAB PARAPET.

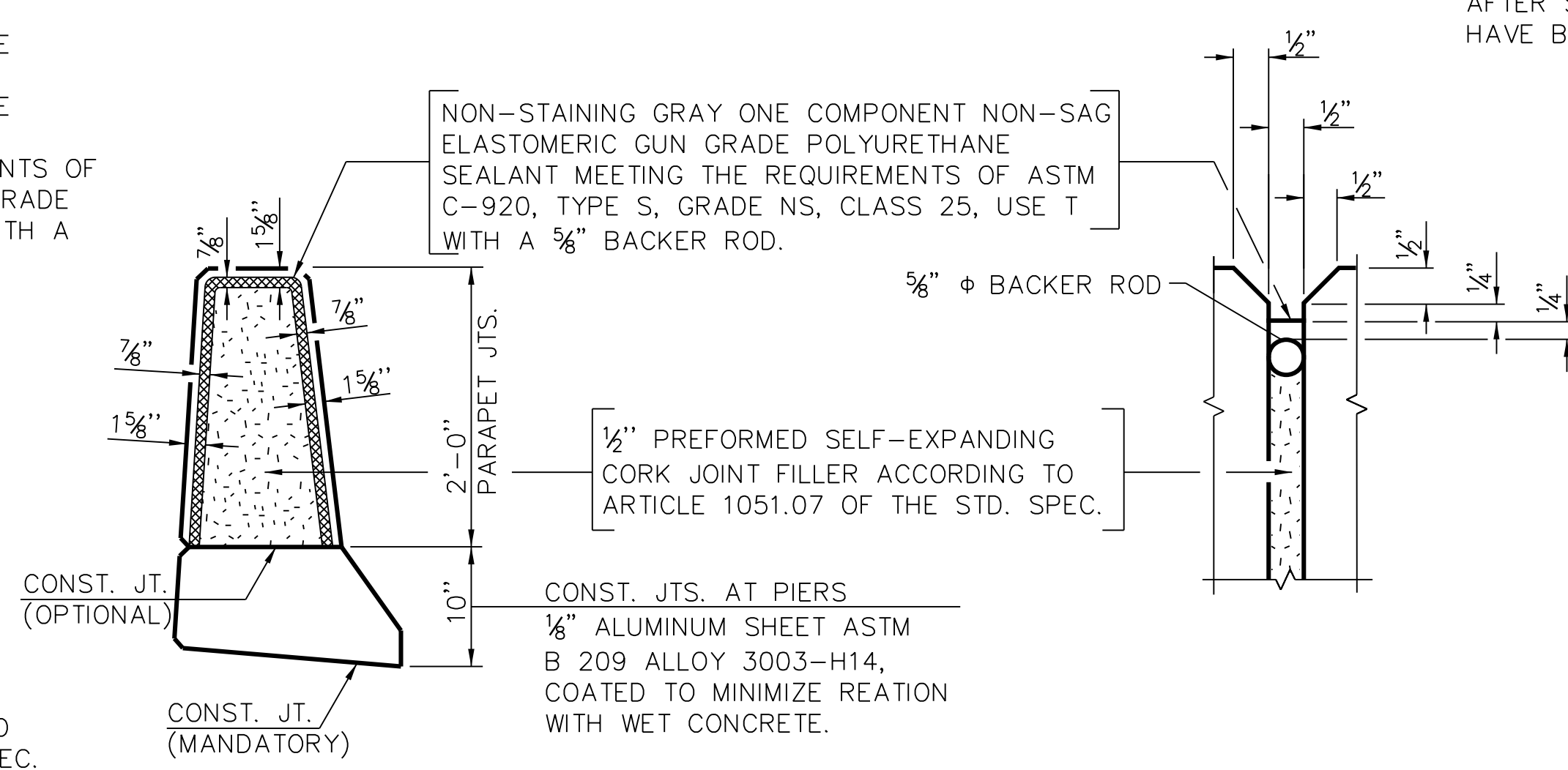


**2 INSIDE ELEVATION OF SOUTH PARAPET**  
 (DIMENSION SHOWN MEASURED ALONG INSIDE FACE OF PARAPET LOOKING SOUTH)

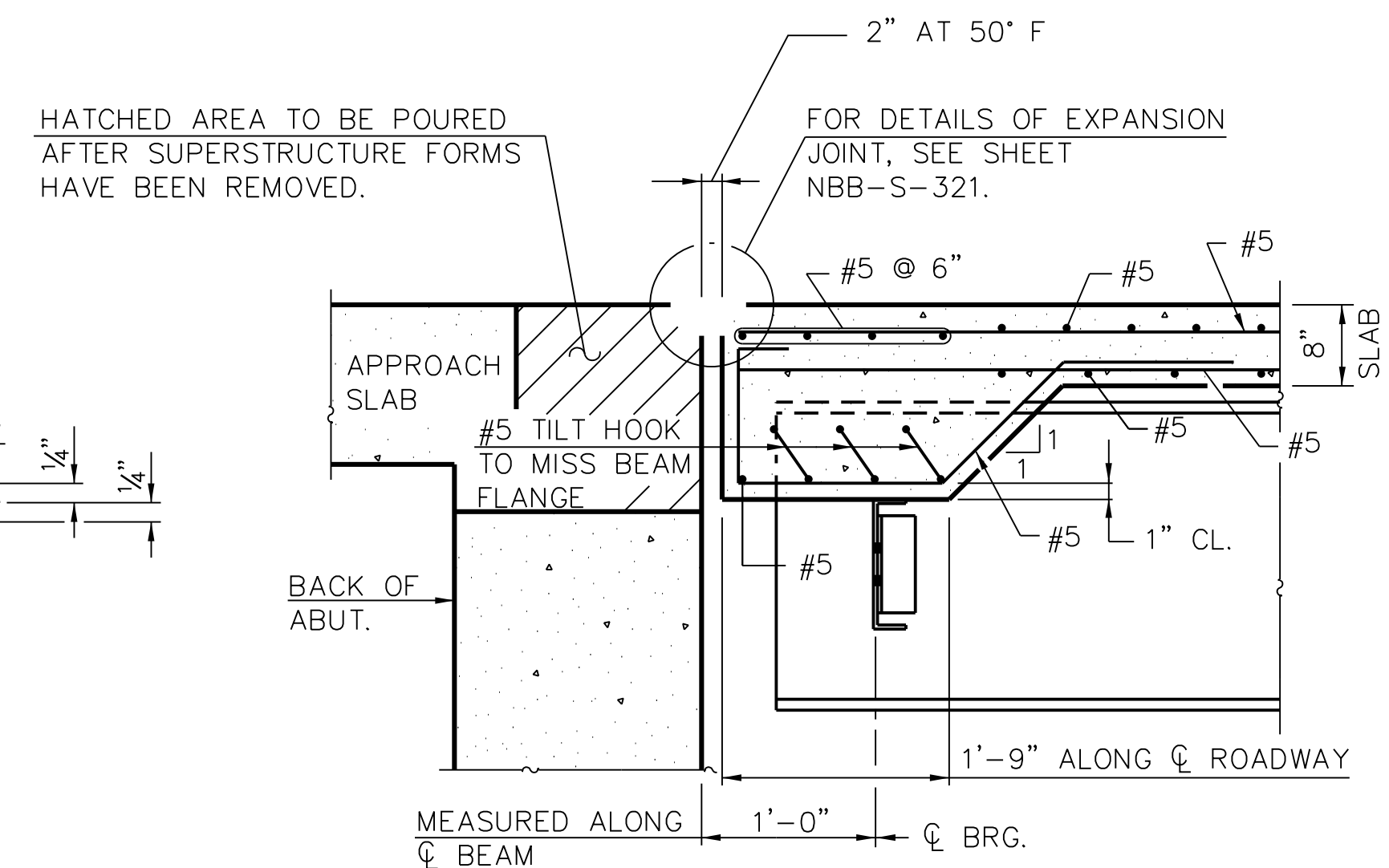
- NOTES:
- ALL EDGES SHALL BE CHAMFERED 3/4".
  - FOR BRIDGE FENCE RAILING, SEE ARCHITECTURAL PLANS.



**3 SOUTH PARAPET JOINT DETAILS**



**4 NORTH PARAPET JOINT DETAILS**



**5 SECTION C-C**

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LOCATION IDENTIFIER: **DR095**

95TH ST. NORTH BUS BRIDGE  
 PARAPET ELEVATIONS AND DETAILS

**NBB-S-315**

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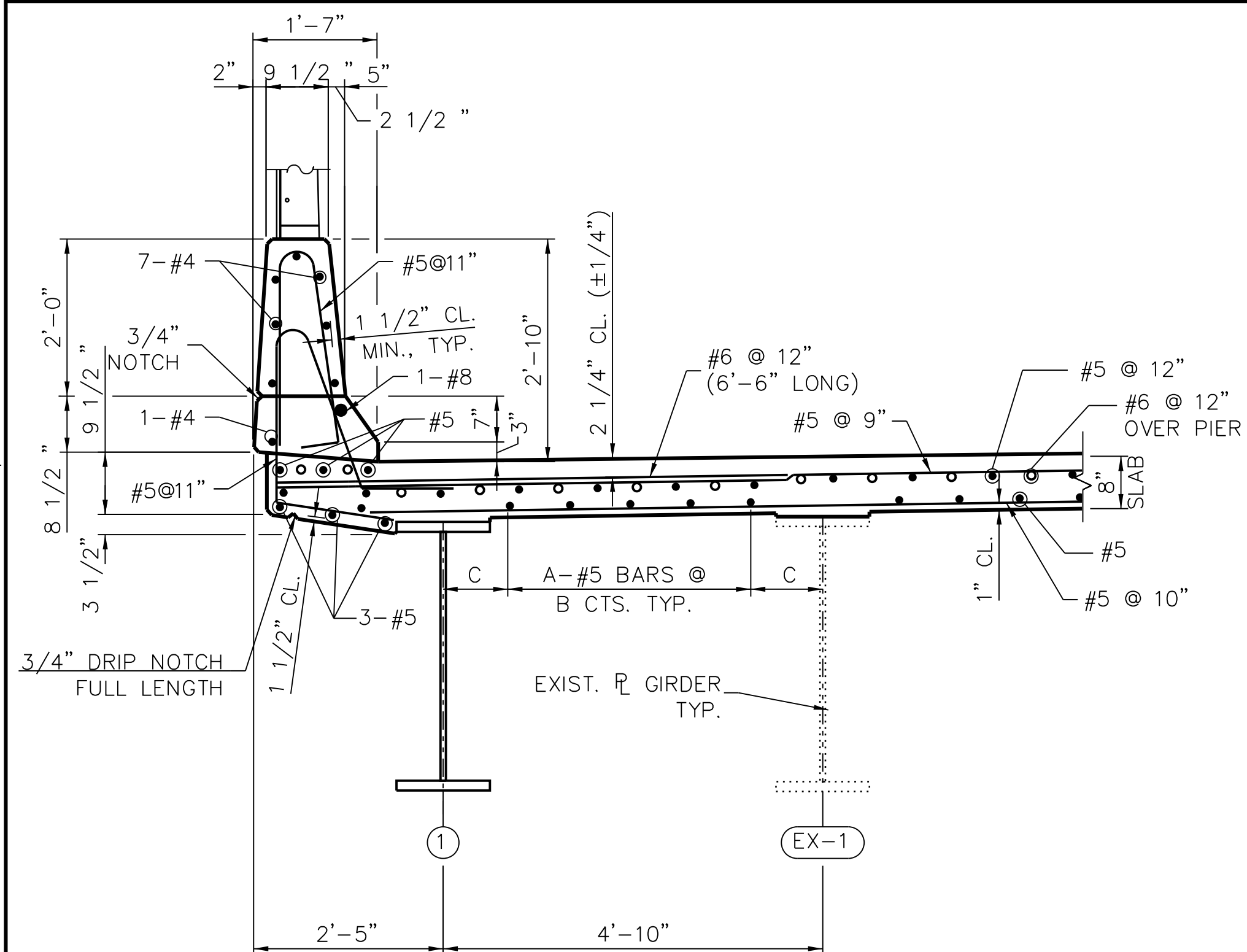
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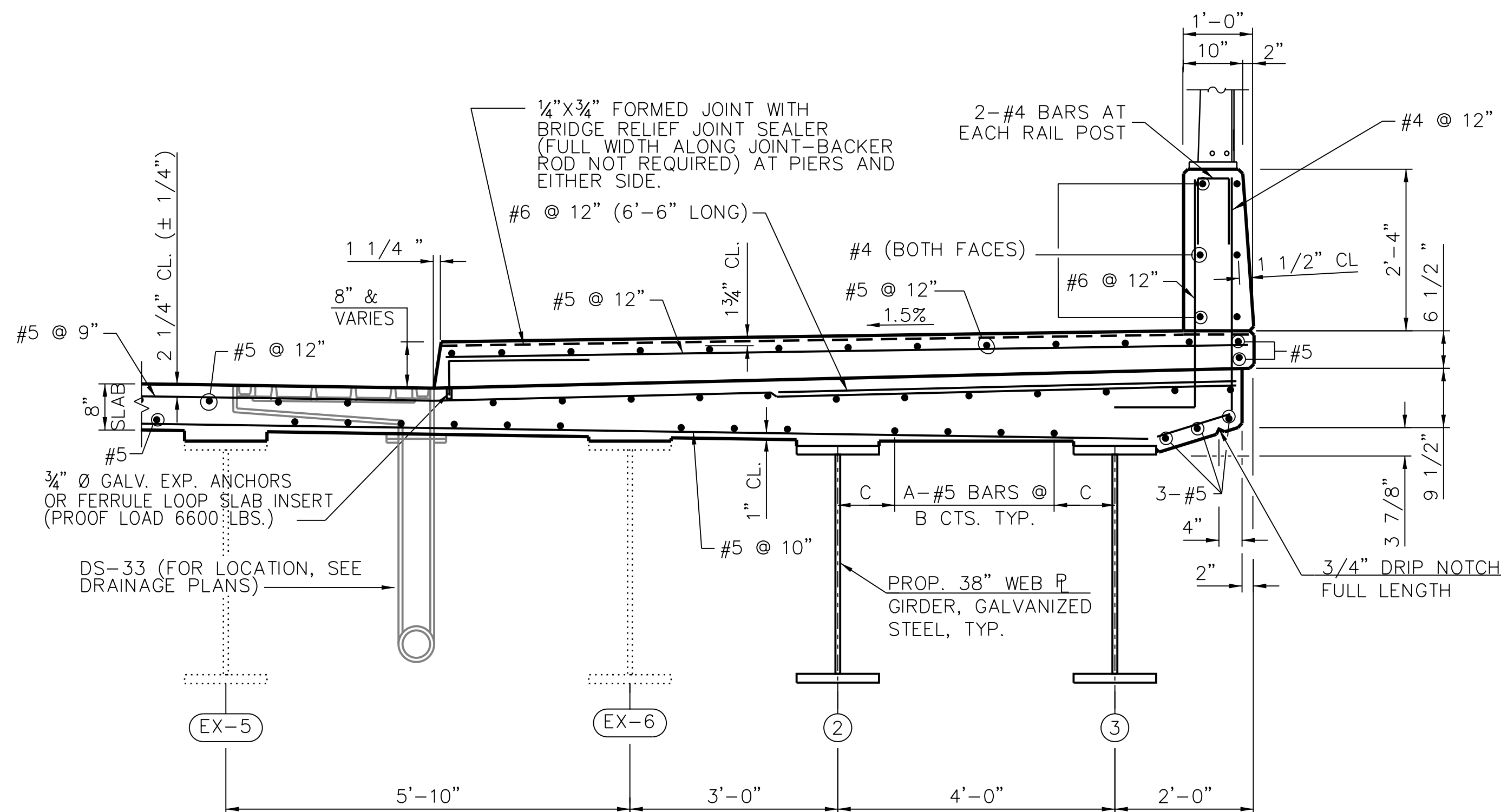
LOCATION IDENTIFIER: **DR095**

95TH ST. NORTH BUS BRIDGE DECK DETAILS

**NBB-S-316**



**1 SECTION THRU NORTH PARAPET AT PIER**

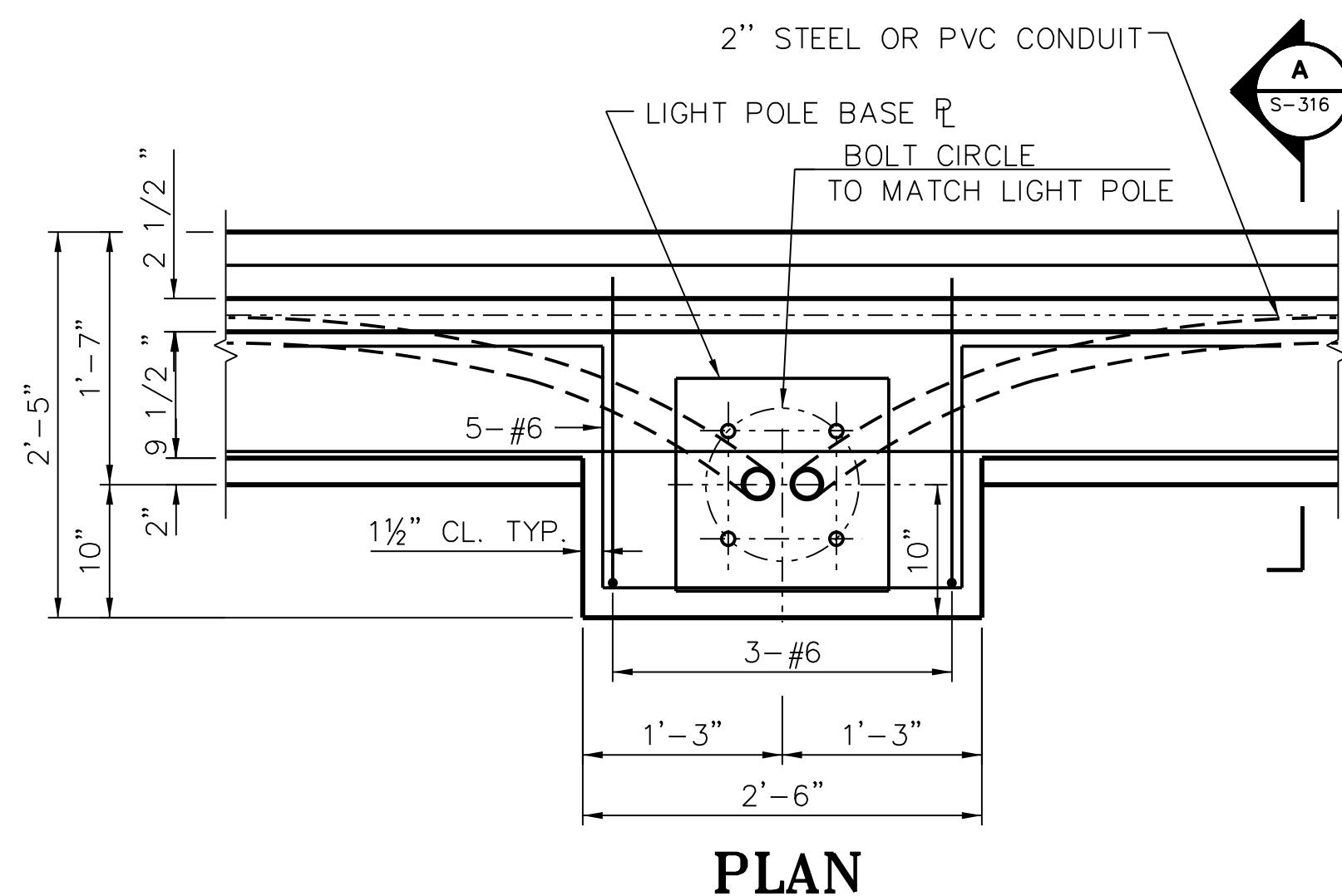


**2 SECTION THRU SOUTH PARAPET AT MID SPAN**

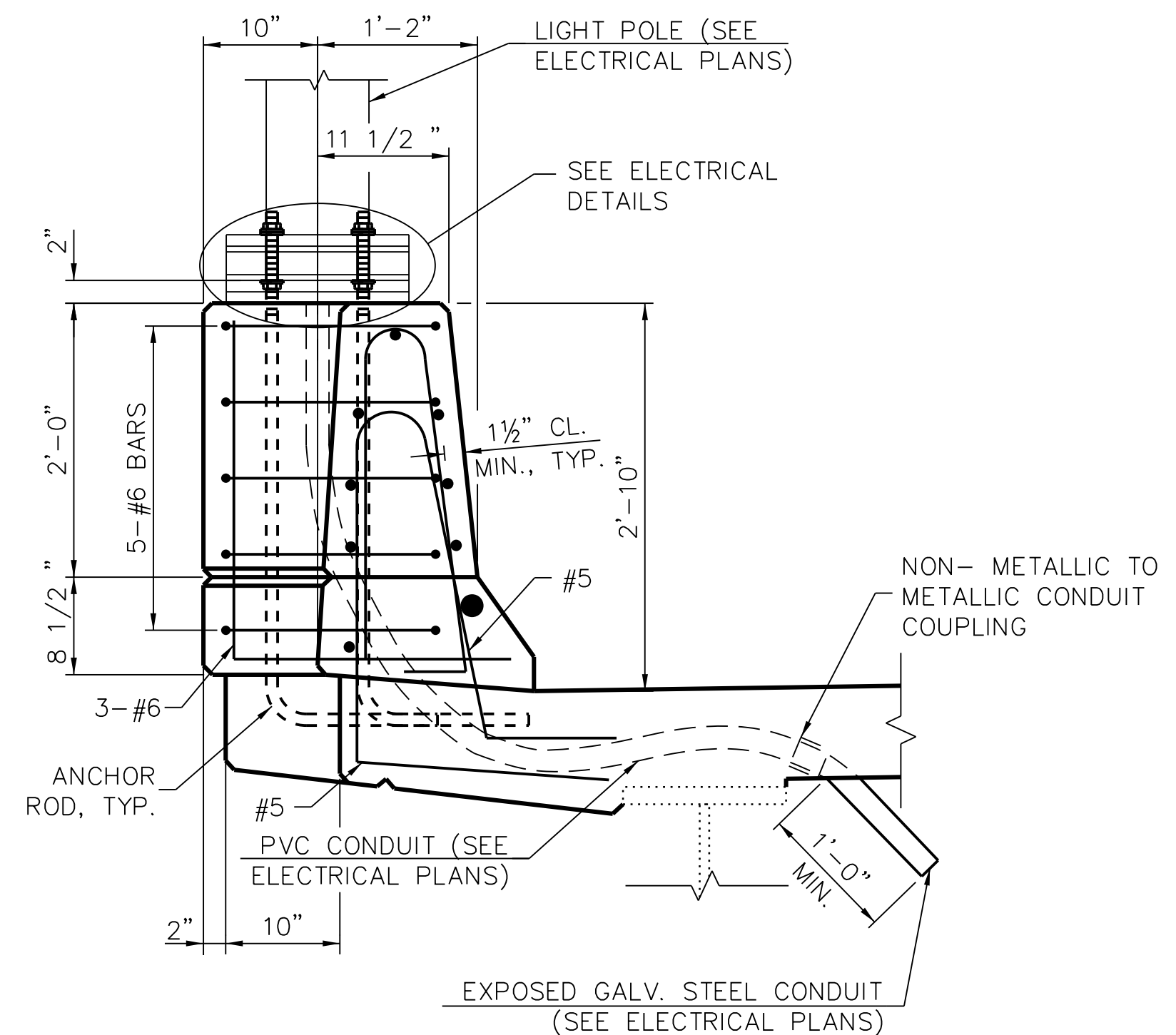
\* FOR CONDUIT SUPPORT DETAILS SEE SHEET S-213A

**BOTTOM LONGITUDINAL REINFORCEMENT TABLE**

BEAM LOCATION	A	B	C
1 TO EX-1	4	11"	12 1/2"
EX-1 TO EX-6	5	11 1/2"	12"
EX-6 TO 2	3	6"	12"
2 TO 3	4	8"	12"
3 TO 5	5	9 1/2"	12 1/8"
3 TO 7	4	11"	12 1/2"(+)



**PLAN**

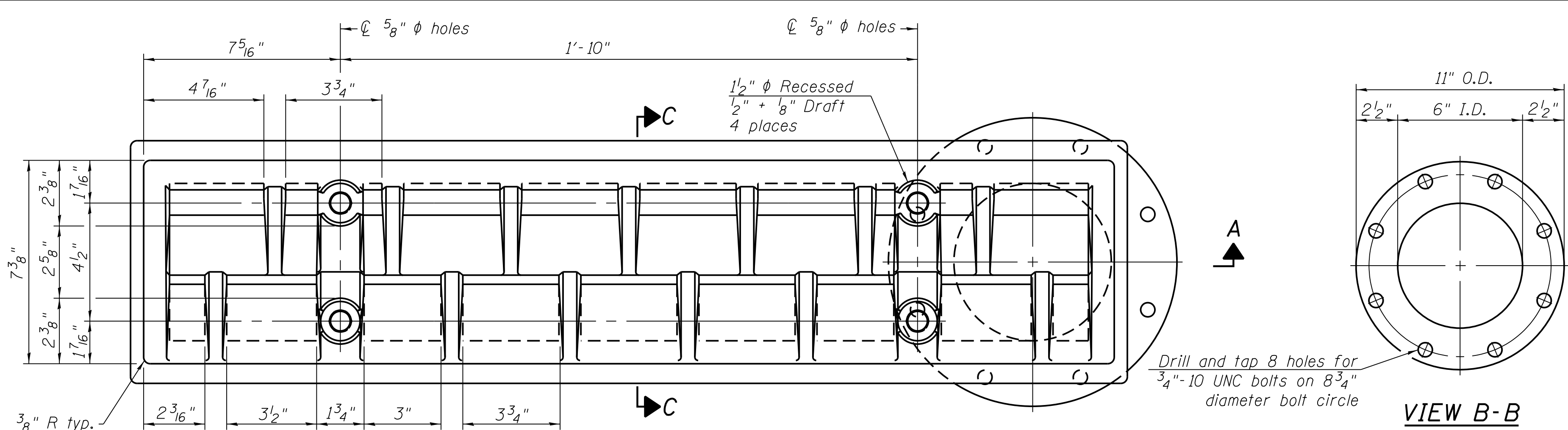


**A SECTION**

**PARAPET DETAIL AT LIGHTPOLE**

NOTE:  
ELECTRICAL CONDUITS LOCATED IN BAYS EX-1 TO EX-2 AND EX-5 TO EX-6 TO BE CLAMPED WITH UNISTRUT CLAMP (OR APPROVED EQUAL) TO DIAPHRAGMS.

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Notes:  
 Refer to Drainage system specification, section 33 41 10.  
 All cast iron parts shall be gray iron conforming to the requirements of AASHTO M 105, Class 35B.  
 Bolts, anchor studs, washers and nuts shall conform to the requirements of ASTM A 307 and shall be galvanized according to AASHTO M 232.  
 As an alternate, bolts, anchor studs, washers and nuts may be stainless steel according to Article 1006.29(d) of the IDOT Standard Specifications.  
 Structural steel weldments of equal sections and of the same configuration may be substituted for the cast iron scupper frame. Fillet or full penetration welds shall be used for the weldments. Details shall be submitted to the Engineer for approval. Structural steel weldments shall not be substituted for the cast iron scupper grate. Structural steel frames and downspouts shall be galvanized according to AASHTO M111.  
 The Contractor shall take appropriate measures to assure that Protective Coat is not applied to the scupper.  
 Alternate fiberglass downspout conforming to ASTM D 2996 with a short-time rupture strength hoop tensile stress of 30,000 psi min. may be used in lieu of the cast iron or steel equivalent.

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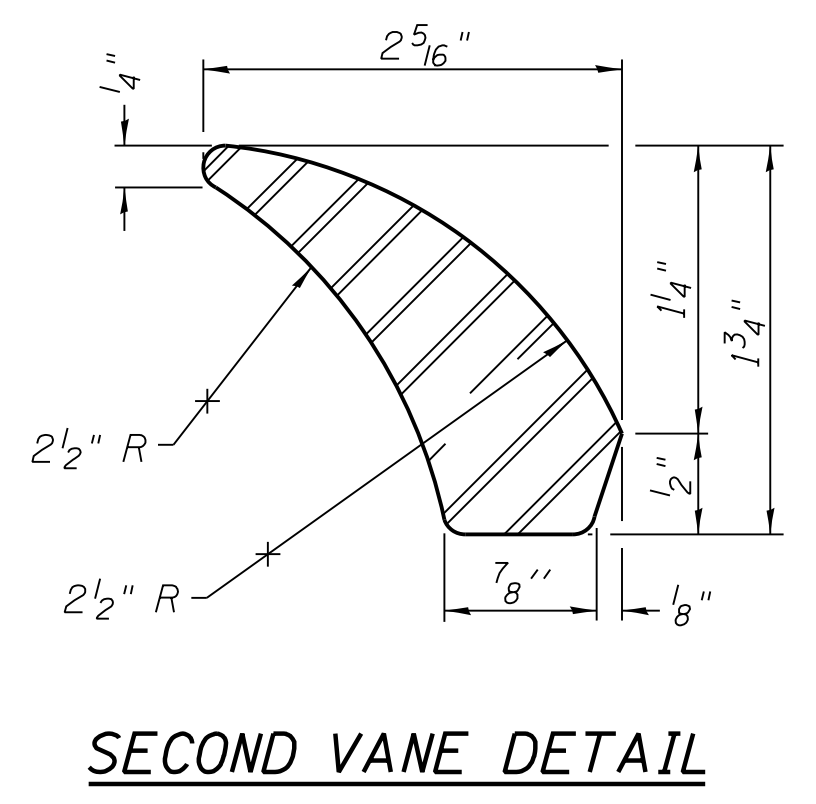
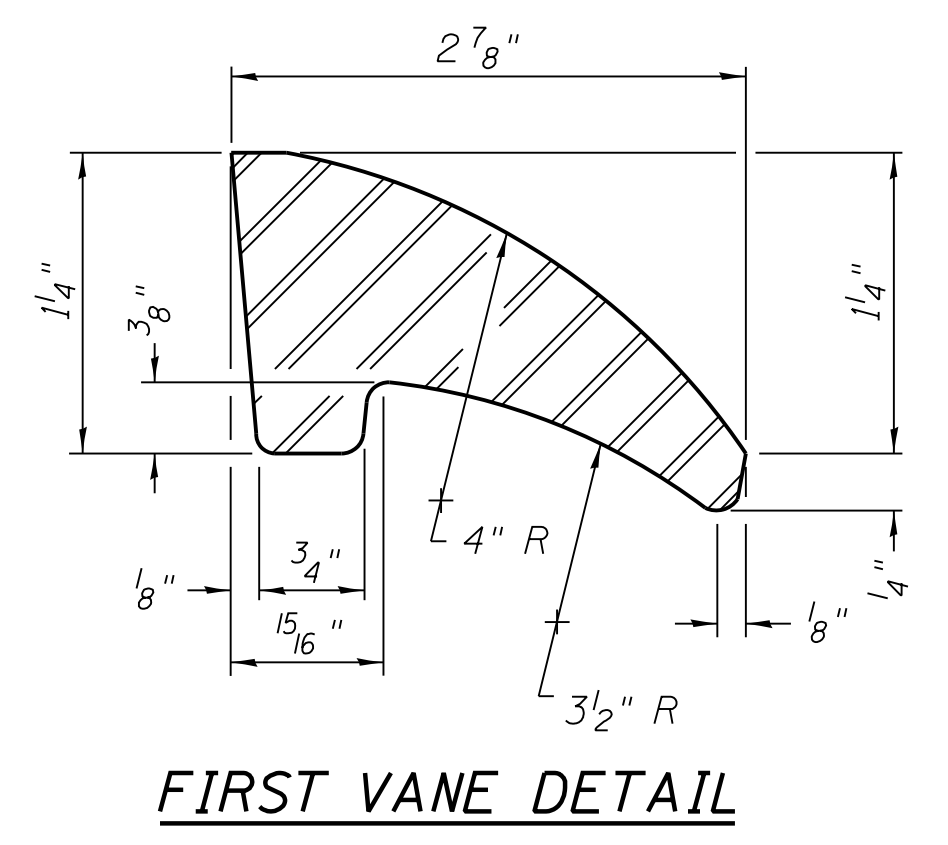
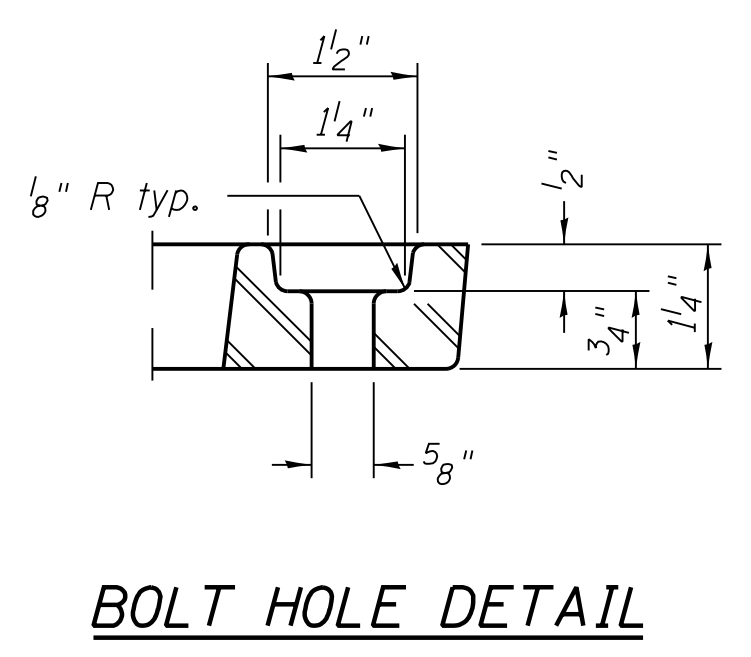
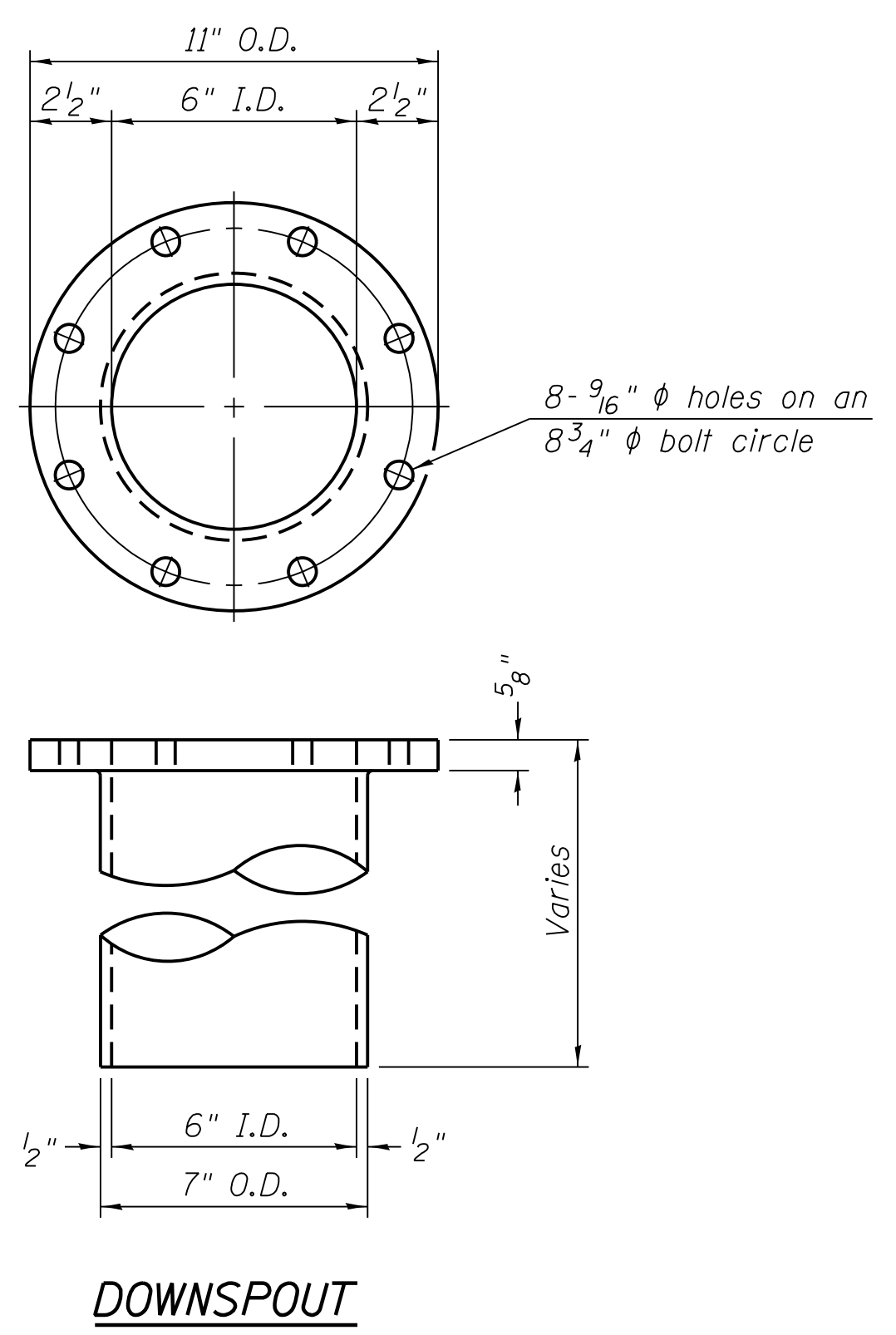
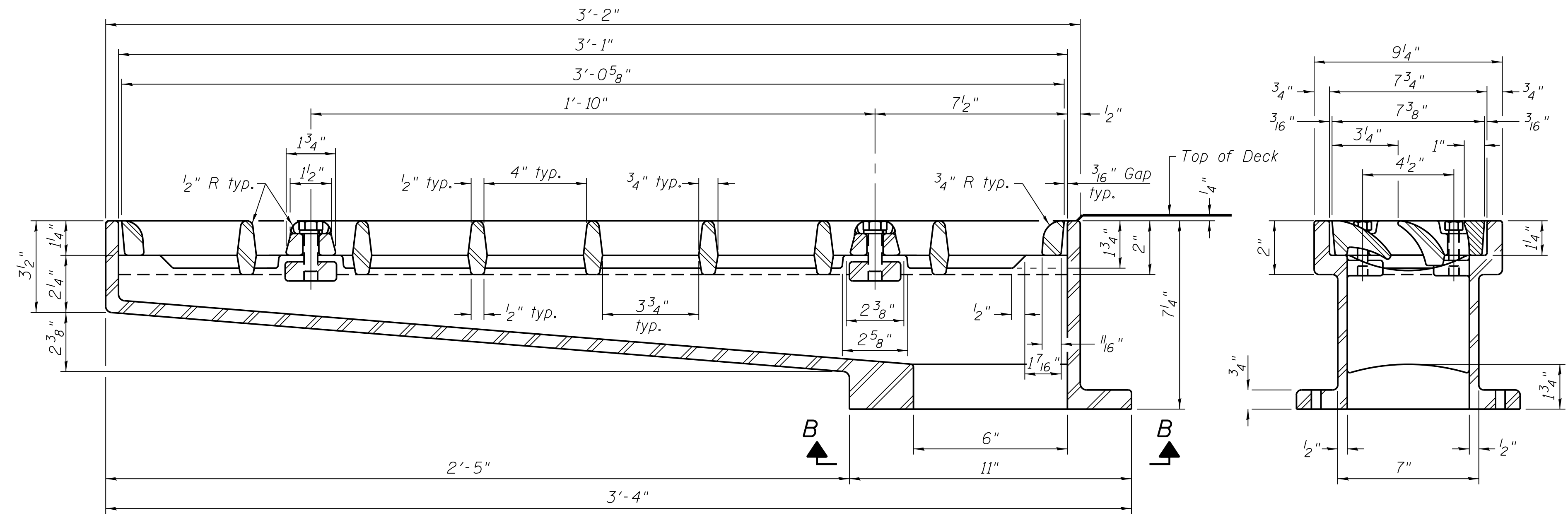
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DRAWN BY	T.SLEDGE
PROJECT NO.	2012-0021
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	03/07/18	INTERMEDIATE DESIGN

LOCATION IDENTIFIER: **DR095**  
 95TH ST. NORTH BUS BRIDGE  
 DRAINAGE SCUPPER,  
 DS-33

**NBB-S-316A**



FILE NAME = 070\_DR095\_S-316A.dgn  
 PLOT DATE = 5/24/2018



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**95th Terminal Improvement**  
 14 West 95th Street (North Terminal)  
 Chicago, IL 60628

IN CHARGE	S. KADERBEK
APPROVED BY	D.ZROKA
CHECKED BY	P.MAID
DESIGNED BY	L.SOMMER
DRAWN BY	T.SLEDGE
PROJECT NO.	2012-0021
FILE NAME	File Name

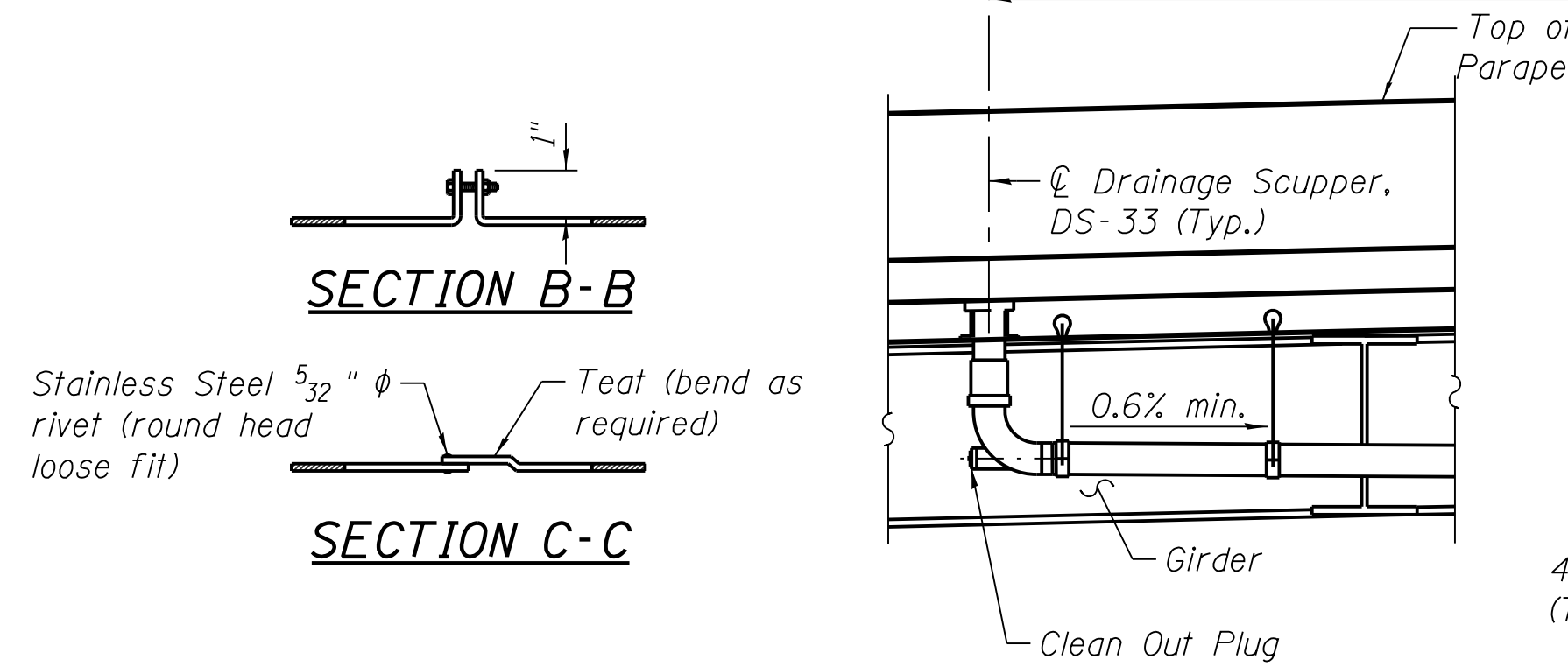
MARK	DATE	DESCRIPTION
	05/23/18	FINAL DESIGN
	03/07/18	INTERMEDIATE DESIGN

LOCATION IDENTIFIER: **DR095**

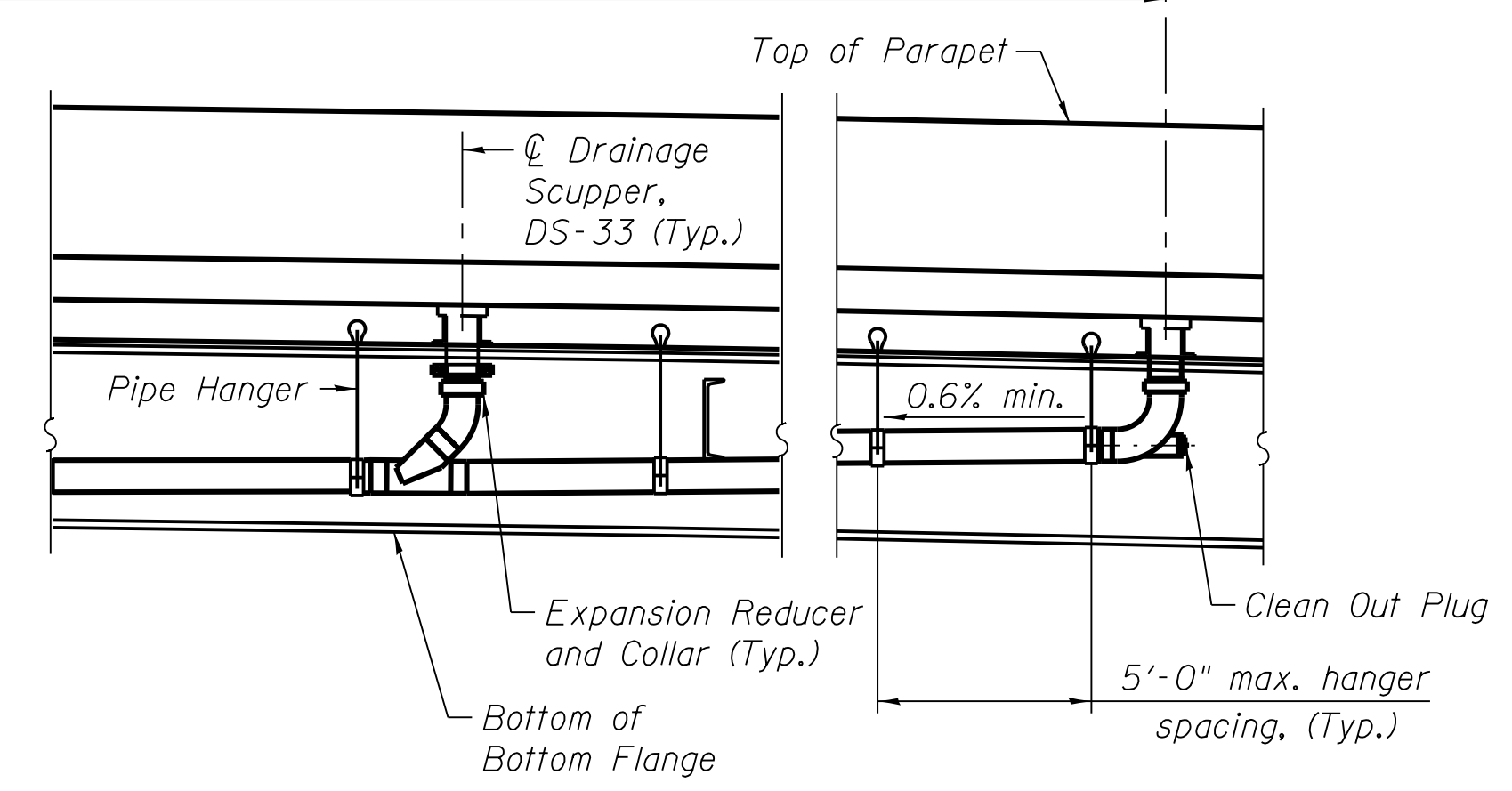
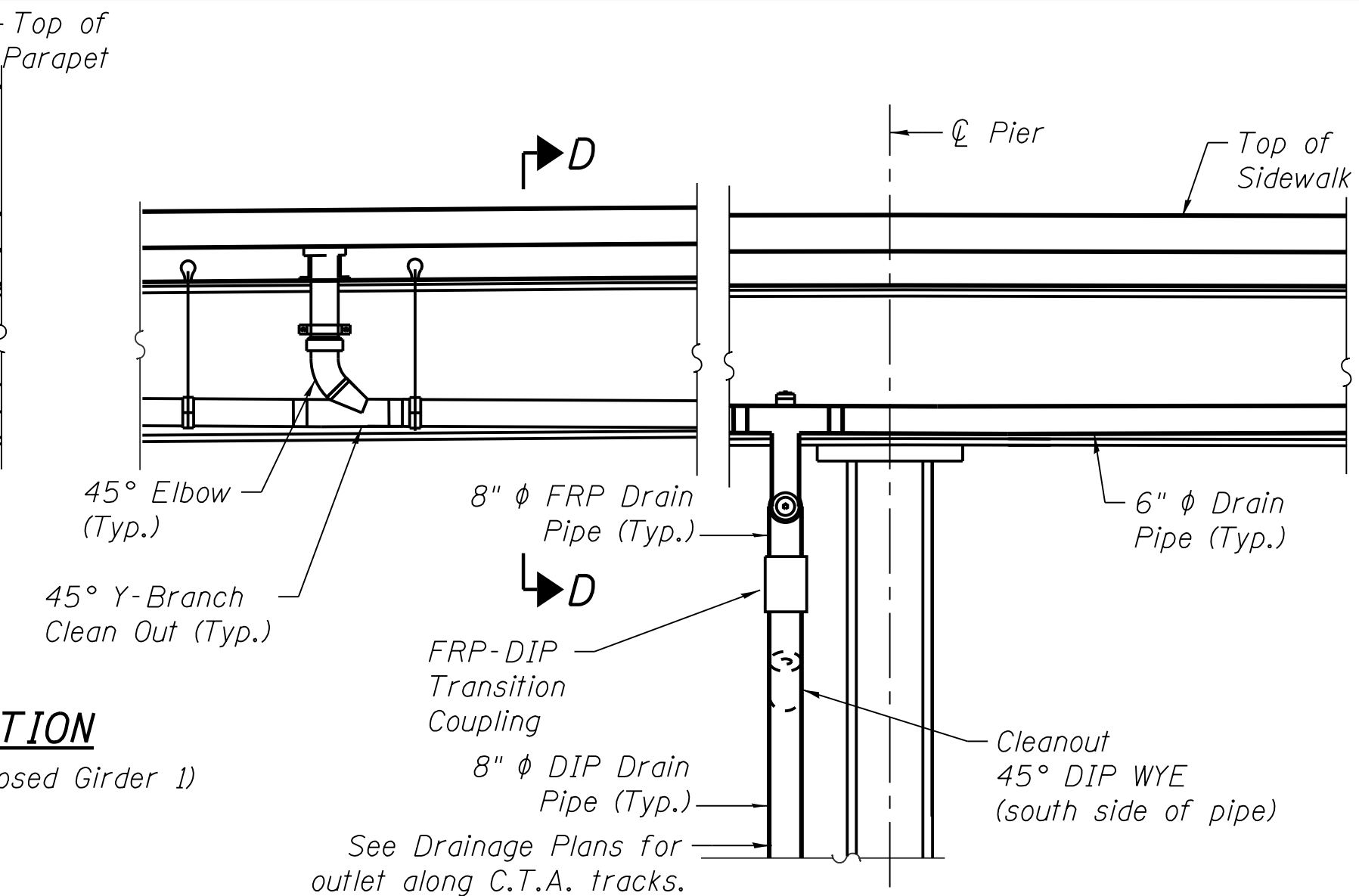
95TH ST. NORTH BUS BRIDGE  
 CLOSED DRAINAGE SYSTEM

**NBB-S-316B**

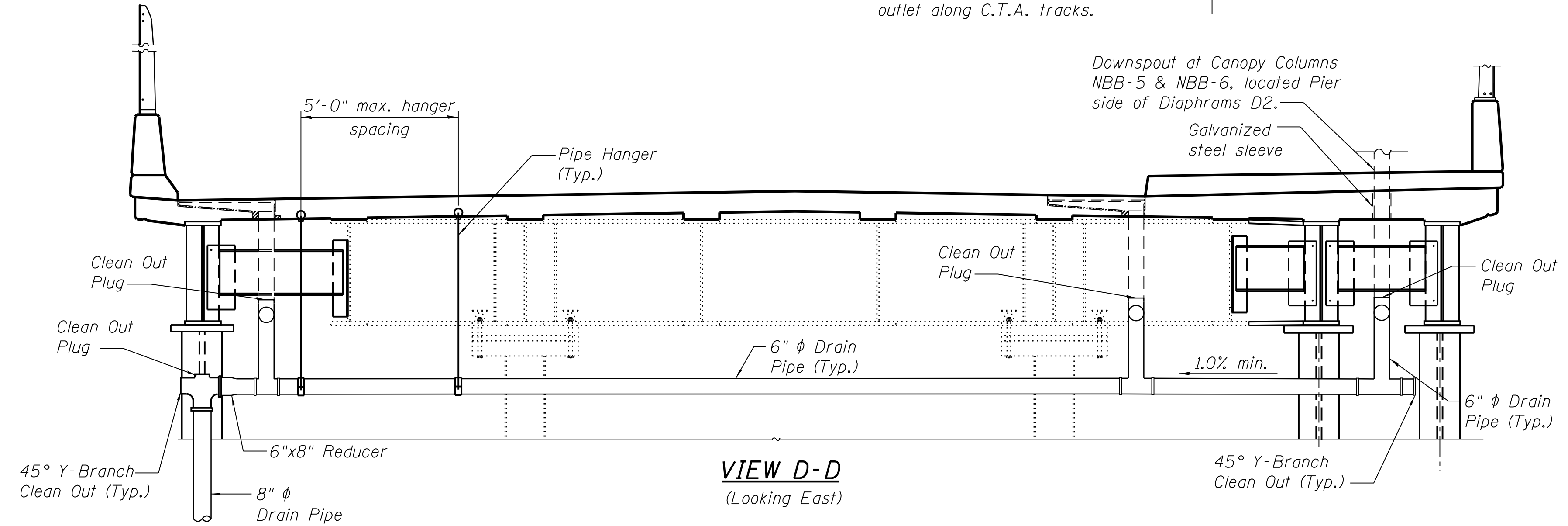
See Sheet NBB-S-313 and Drainage Plans for Spacing



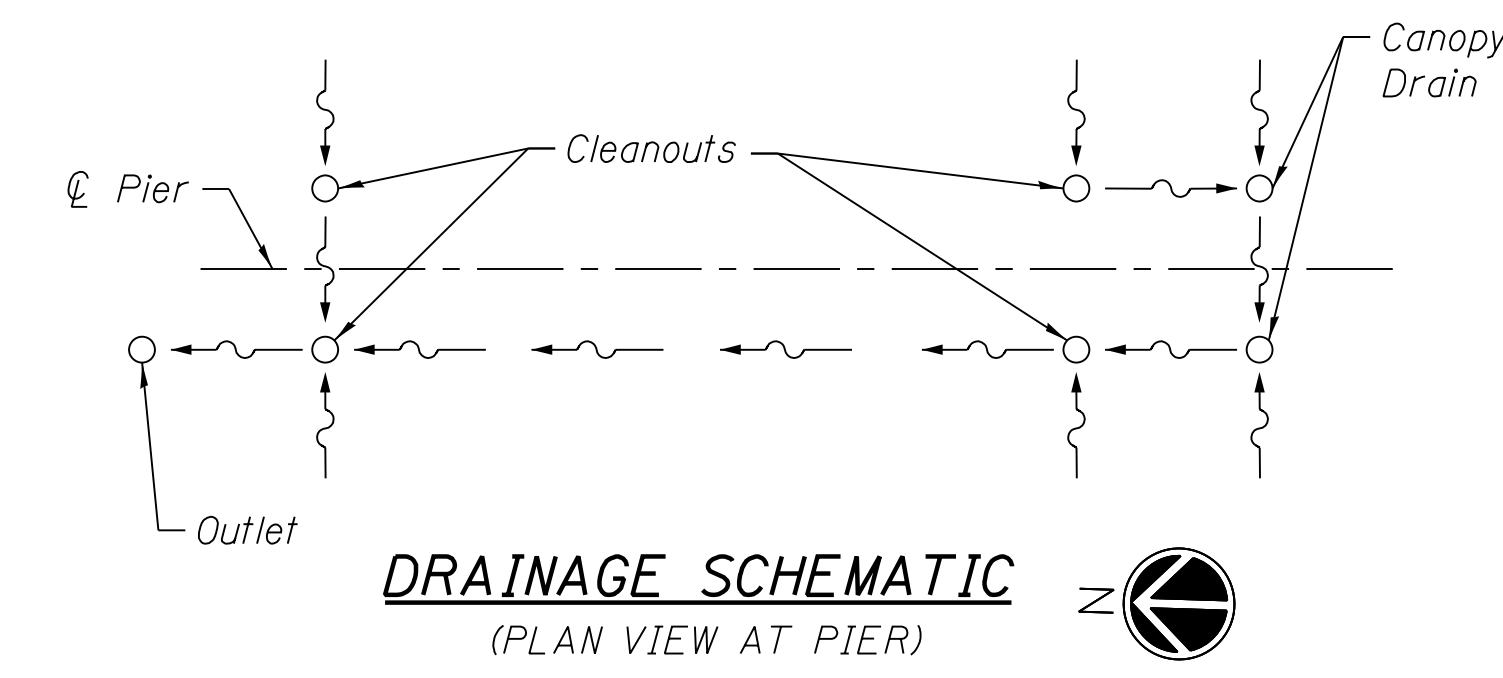
**SPAN 1 ELEVATION**  
 (Looking North Along Proposed Girder 1)



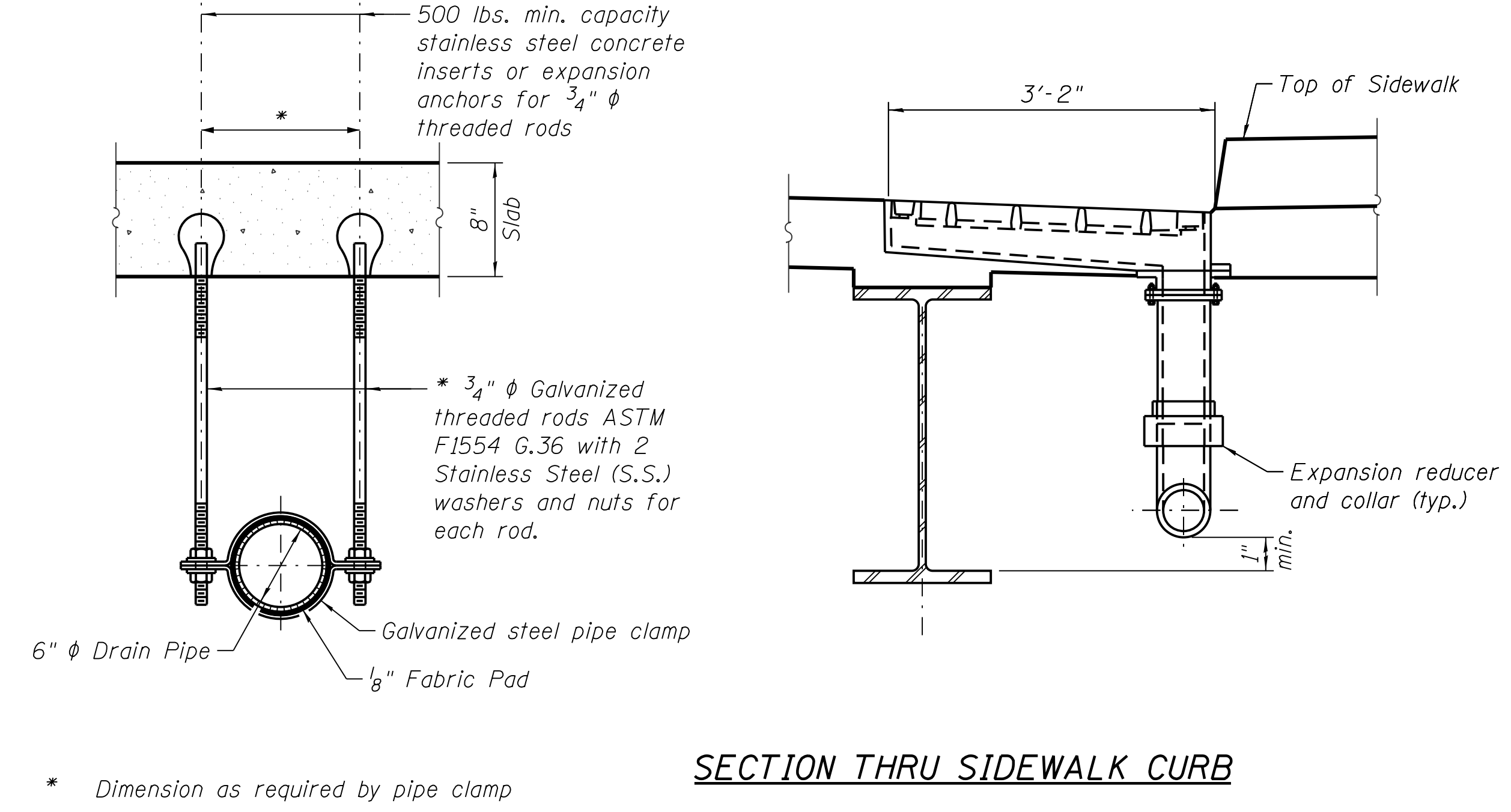
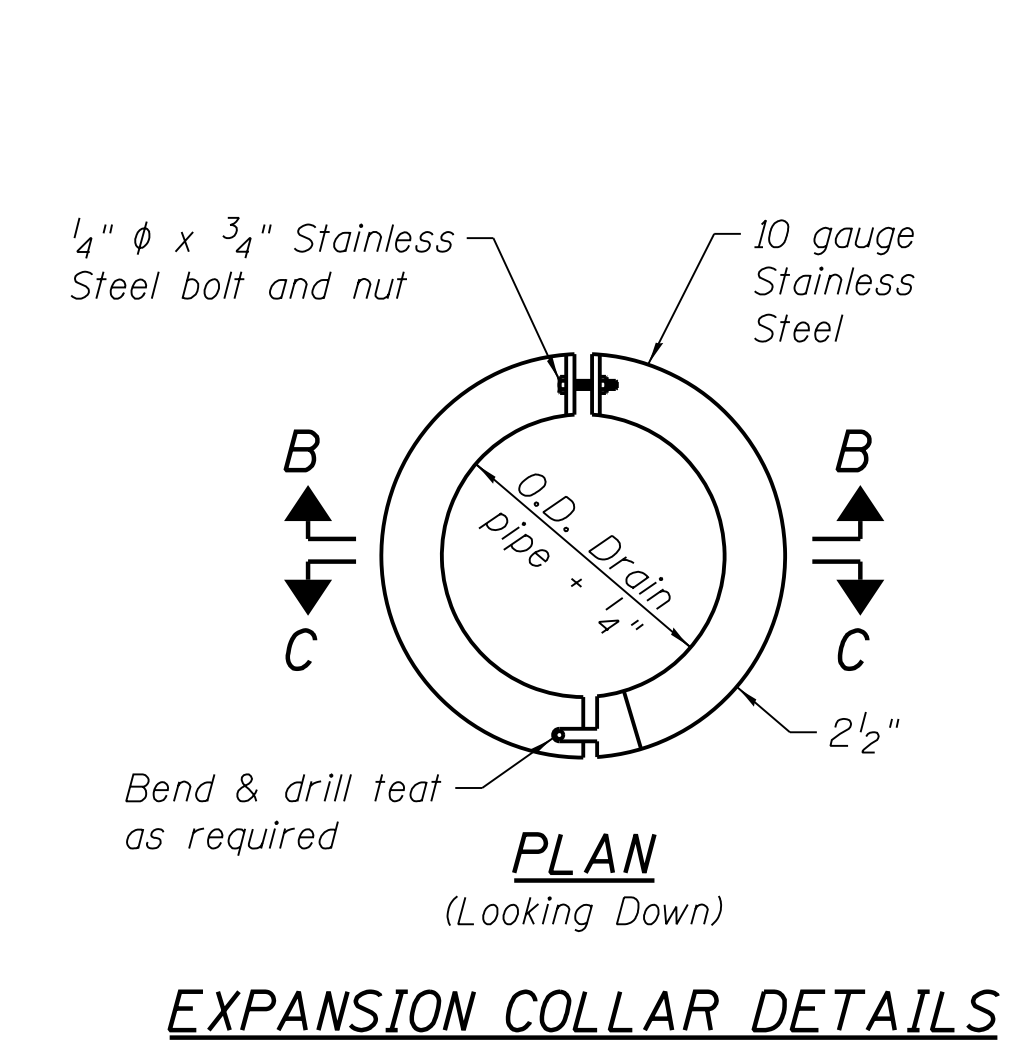
**SPAN 2 ELEVATION**  
 (Looking North Along Proposed Girder 1)



**VIEW D-D**  
 (Looking East)



**DRAINAGE SCHEMATIC**  
 (PLAN VIEW AT PIER)



**SECTION THRU SIDEWALK CURB**

- Notes:
- See sheet NBB-S-316A for drainage scupper details.
  - Scuppers & bridge drainage system shall be located clear of all diaphragms.
  - Fiberglass pipe shall conform to ASTM D 2996, RTRP short-time rupture strength hoop tensile stress of 30,000 p.s.i. minimum.
  - Pipe supports shall be provided on all pipes at 5'-0" max. spacing.
  - All pipe hangers, supports and hardware shall be hot-dipped galvanized in accordance with AASHTO M232 (ASTM A153).
  - All bolts, nuts and washers shall be Stainless Steel, Type 304.
  - Refer to Drainage system specification, section 33 41 10.

FILE NAME = 070\_DR095\_S-316B.dgn  
 PLOT DATE = 5/24/2018

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**95th Terminal Improvement**  
14 West 95th Street (North Terminal)  
Chicago, IL 60628

IN CHARGE	S. KADERBEK
APPROVED BY	D.ZROKA
CHECKED BY	P.MAID
DESIGNED BY	L.SOMMER
DRAWN BY	T.SLEDGE
PROJECT NO.	2012-0021
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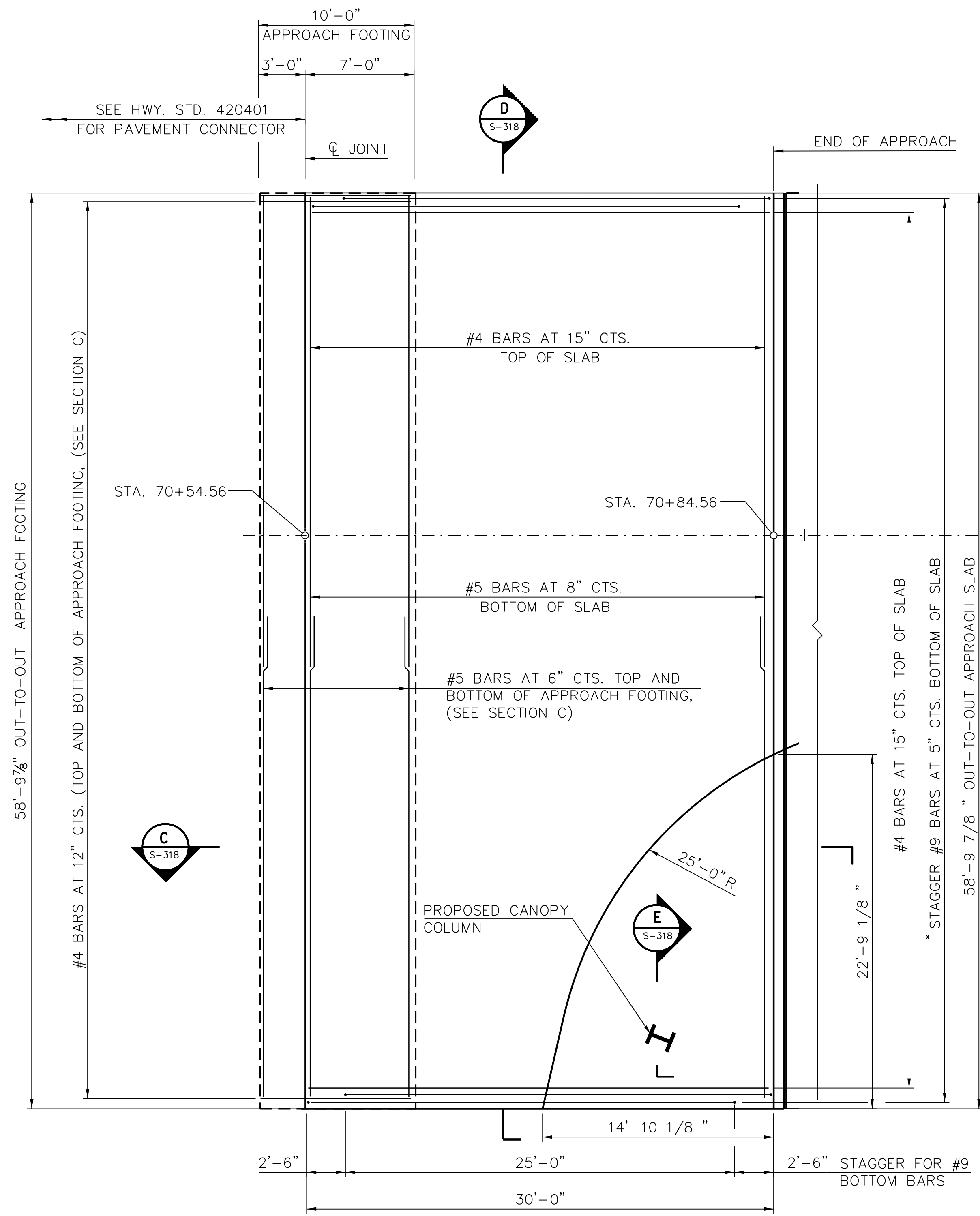
LOCATION IDENTIFIER: DR095

95TH ST. NORTH BUS BRIDGE  
WEST APPROACH SLAB PLAN

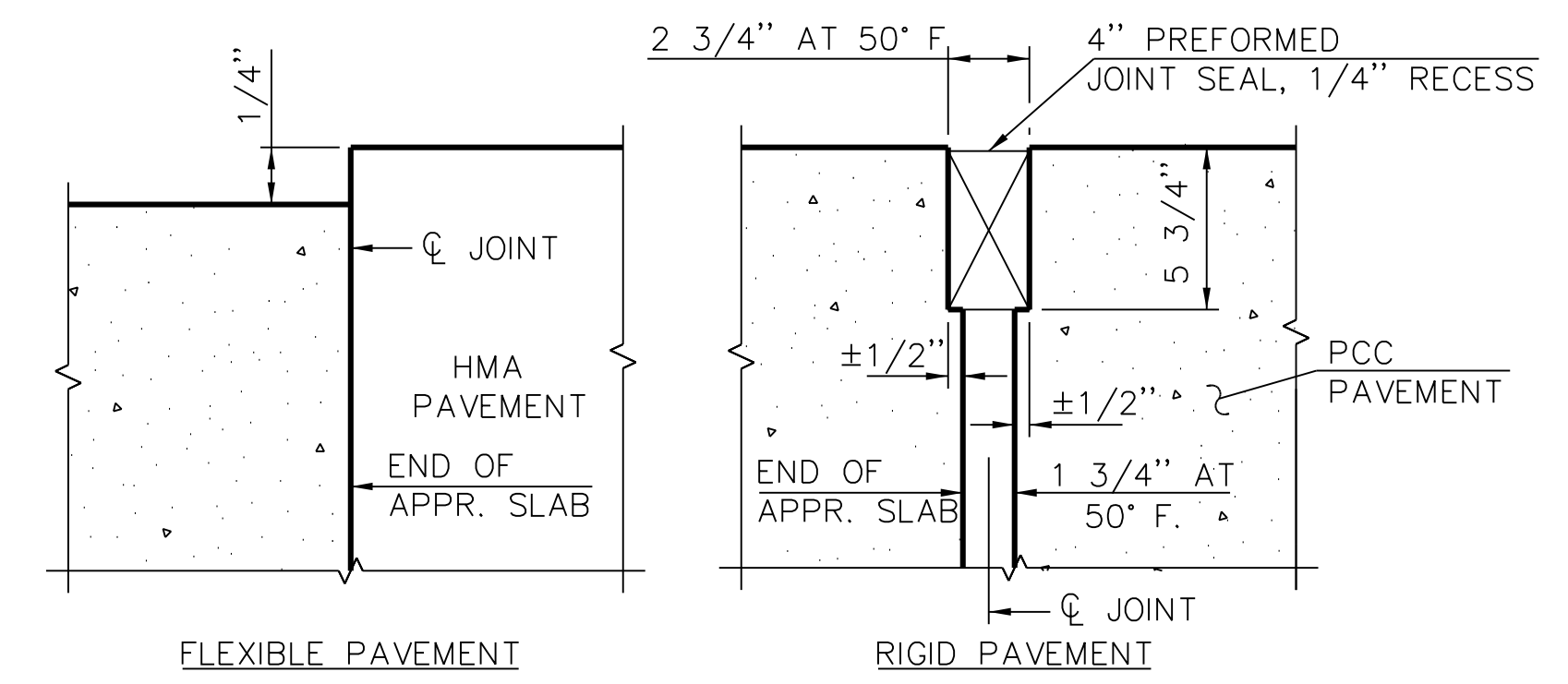
**NBB-S-317**

**NOTES:**

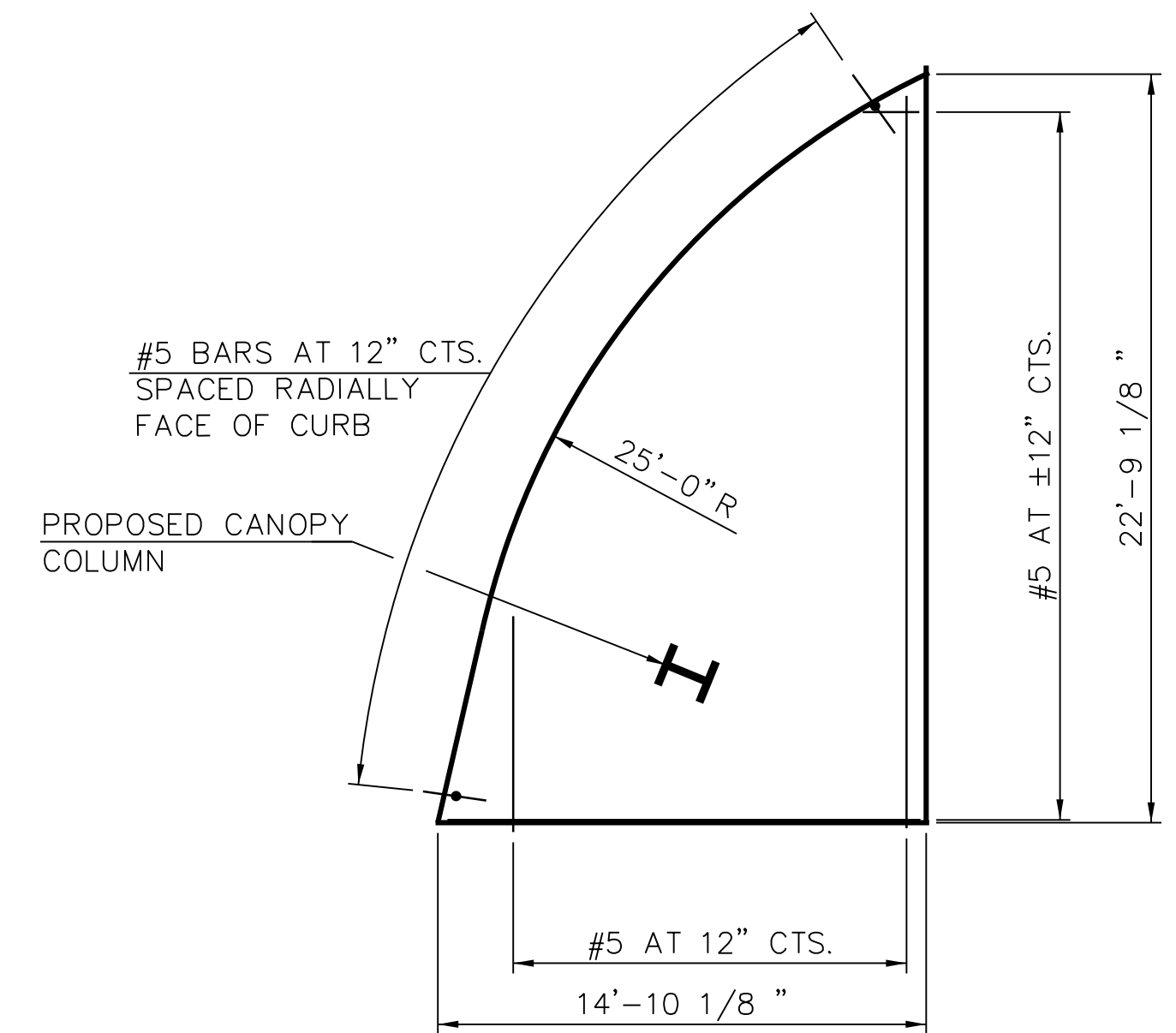
- SEE SHEET NBB-S-318 FOR SECTIONS C, D, AND E.
- FOR ADDITIONAL REINFORCEMENT AT CANOPY COLUMNS, SEE SHEET NBB-S-318.



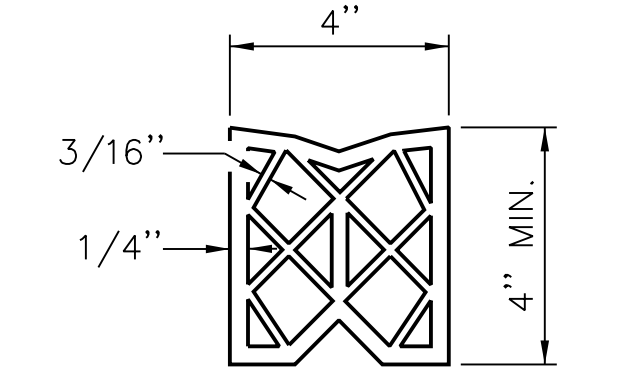
**1 PLAN**  
(SIDEWALK REINFORCEMENT NOT SHOWN FOR CLARITY)



**2 DETAIL A**



**4 SW SIDEWALK**



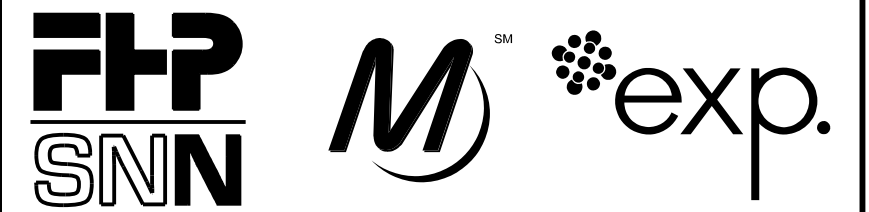
**3 PREFORMED JOINT SEAL**

\* TILT #9 STAGGER BARS AS REQUIRED TO MAINTAIN CLEARANCE.

FILE NAME = 070\_DR095\_S-317.dgn  
PLOT DATE = 5/24/2018

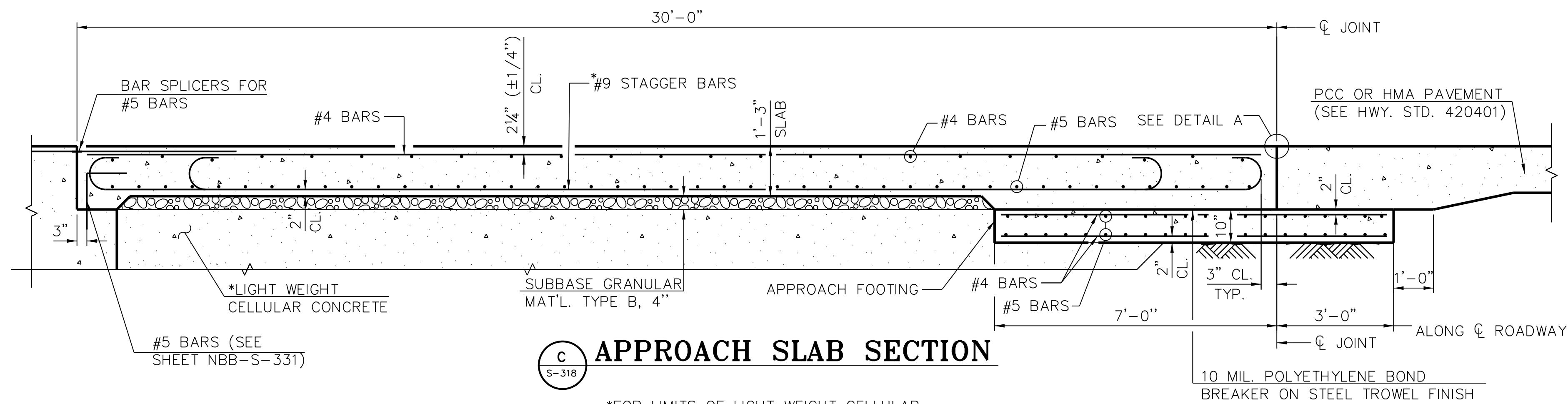
vscorppa

**NOT FOR CONSTRUCTION**



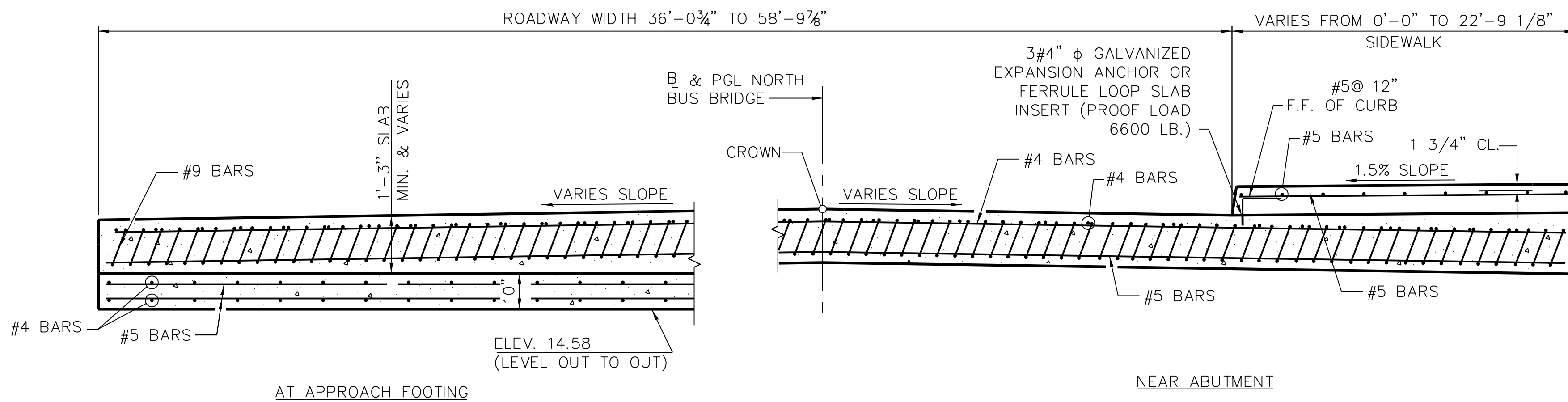
**NOTES:**

1. FOR DETAIL A, SEE SHEET NBB-S-317.
2. THE APPROACH FOOTING MAXIMUM APPLIED SERVICE BEARING PRESSURE (QMAX) = 2.0 KSF.
3. FOR SUBBASE GRANULAR MATERIAL AND LIGHTWEIGHT CELLULAR CONCRETE, SEE SHEETS NBB-S-334 AND NBB-S-335.



**C APPROACH SLAB SECTION**

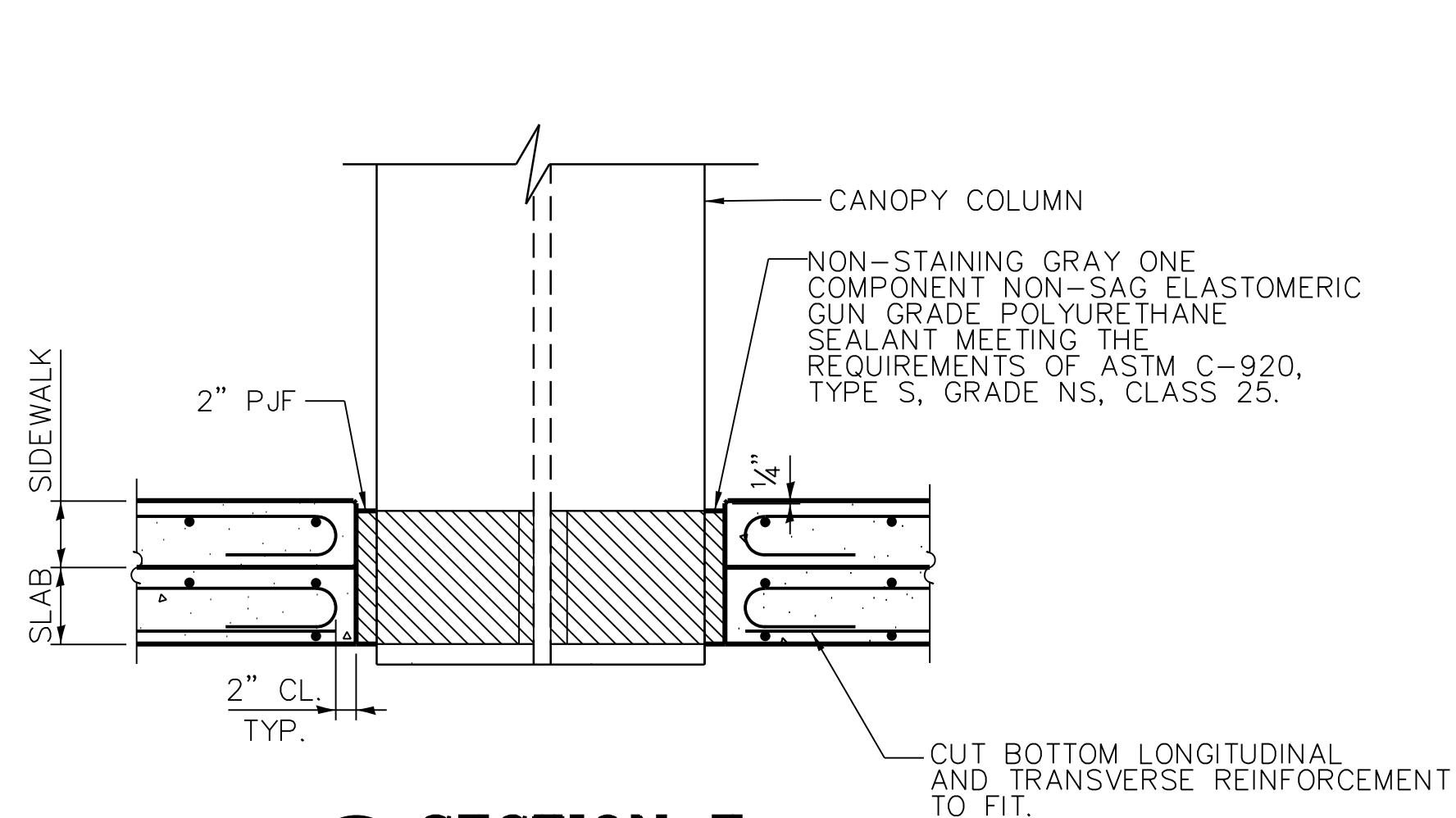
\*FOR LIMITS OF LIGHT WEIGHT CELLULAR CONCRETE SEE NBB-S-334 AND NBB-S-335.



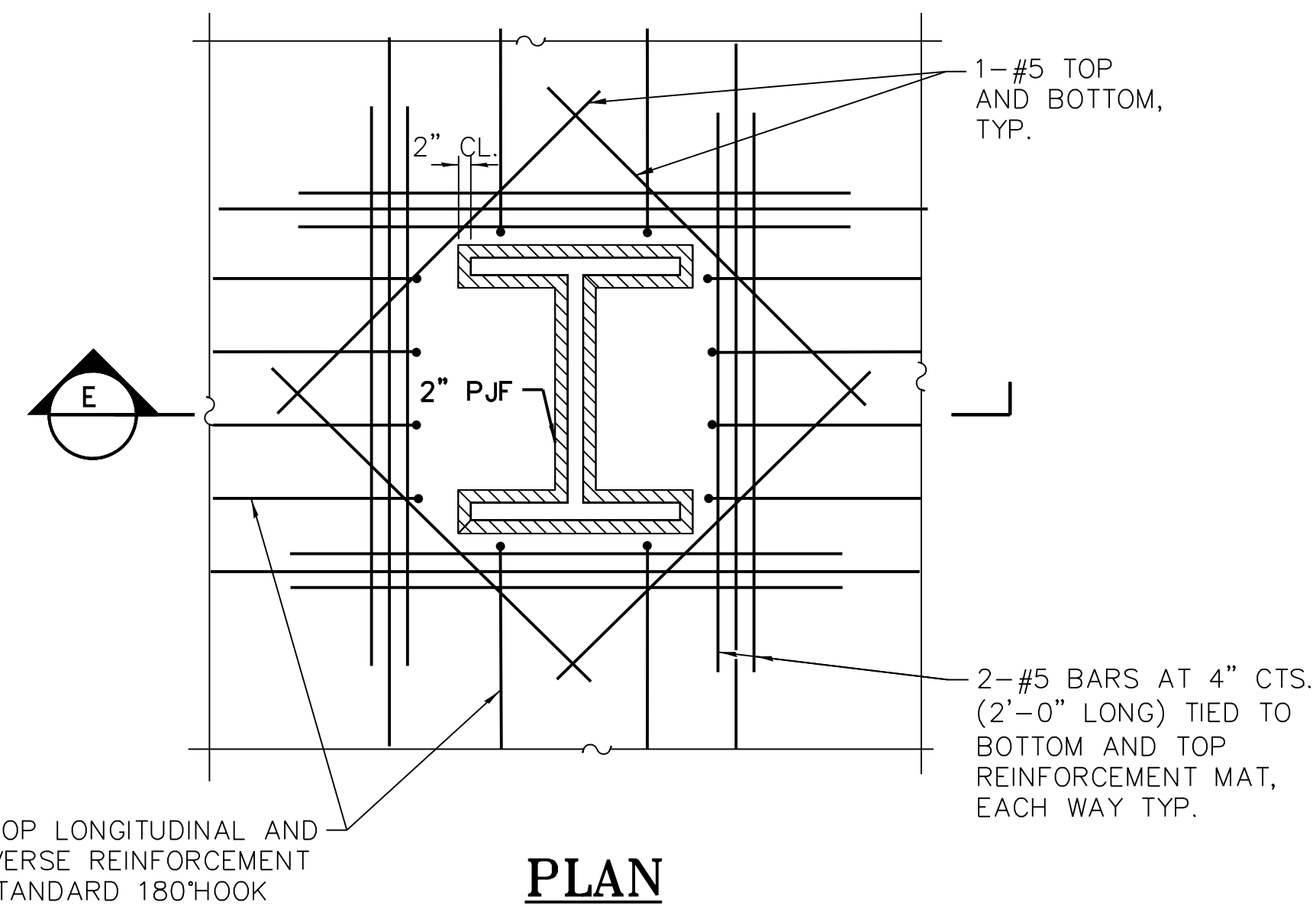
**D APPROACH SLAB SECTION**

(SEE PLAN FOR DIMENSIONS NOT SHOWN)

\* TILT #9 STAGGER BARS AS REQUIRED TO MAINTAIN CLEARANCE.



**E SECTION E**



**PLAN**

**CHICAGO TRANSIT AUTHORITY ENGINEERING**

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Chicago, IL 60628

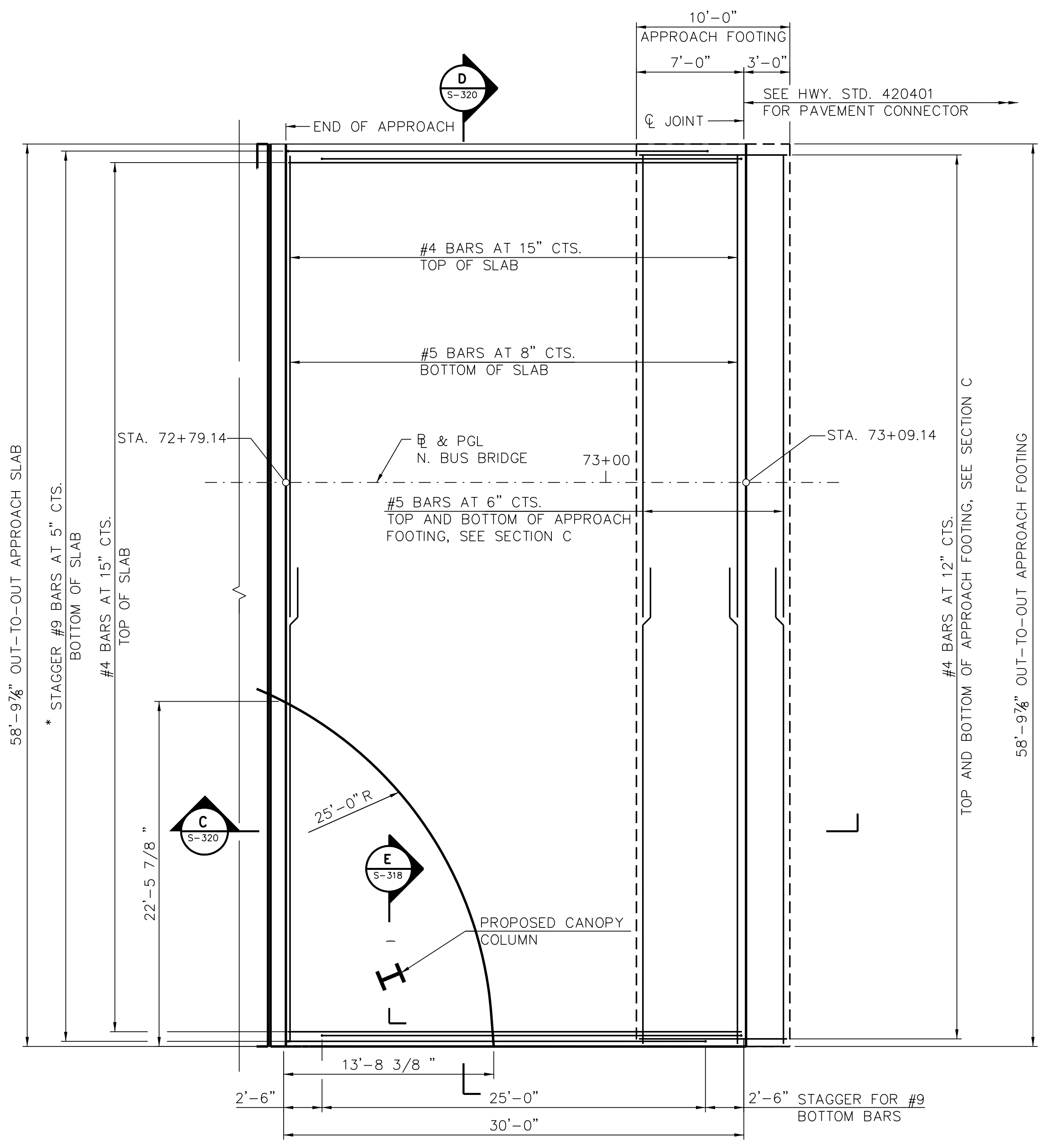
IN CHARGE	S. KADERBEK
APPROVED BY	D.ZROKA
CHECKED BY	P.MAID
DESIGNED BY	L.SOMMER
DRAWN BY	T.SLEDGE
PROJECT NO.	2012-0021
FILE NAME	File Name

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LOCATION IDENTIFIER: **DR095**  
**95TH ST. NORTH BUS BRIDGE WEST APPROACH SLAB SECTIONS AND DETAILS**

**NBB-S-318**

vscorppa

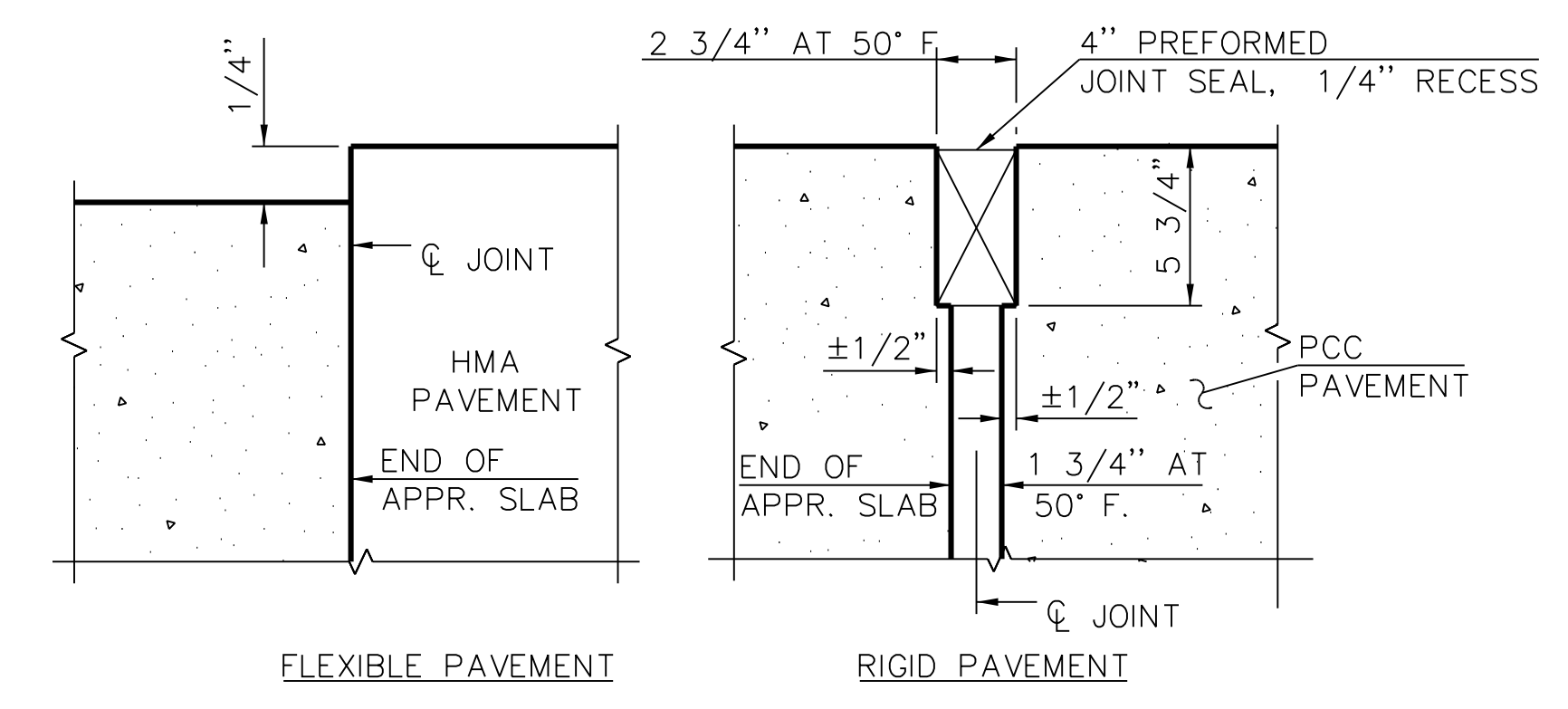


**1 PLAN**  
(SIDEWALK REINFORCEMENT NOT SHOWN FOR CLARITY)

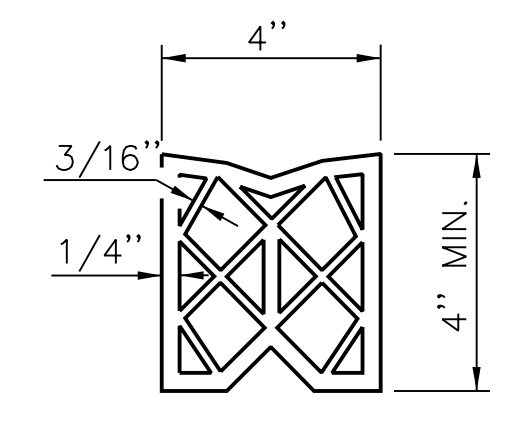
\* TILT #9 STAGGER BARS AS REQUIRED TO MAINTAIN CLEARANCE.

**NOTES:**

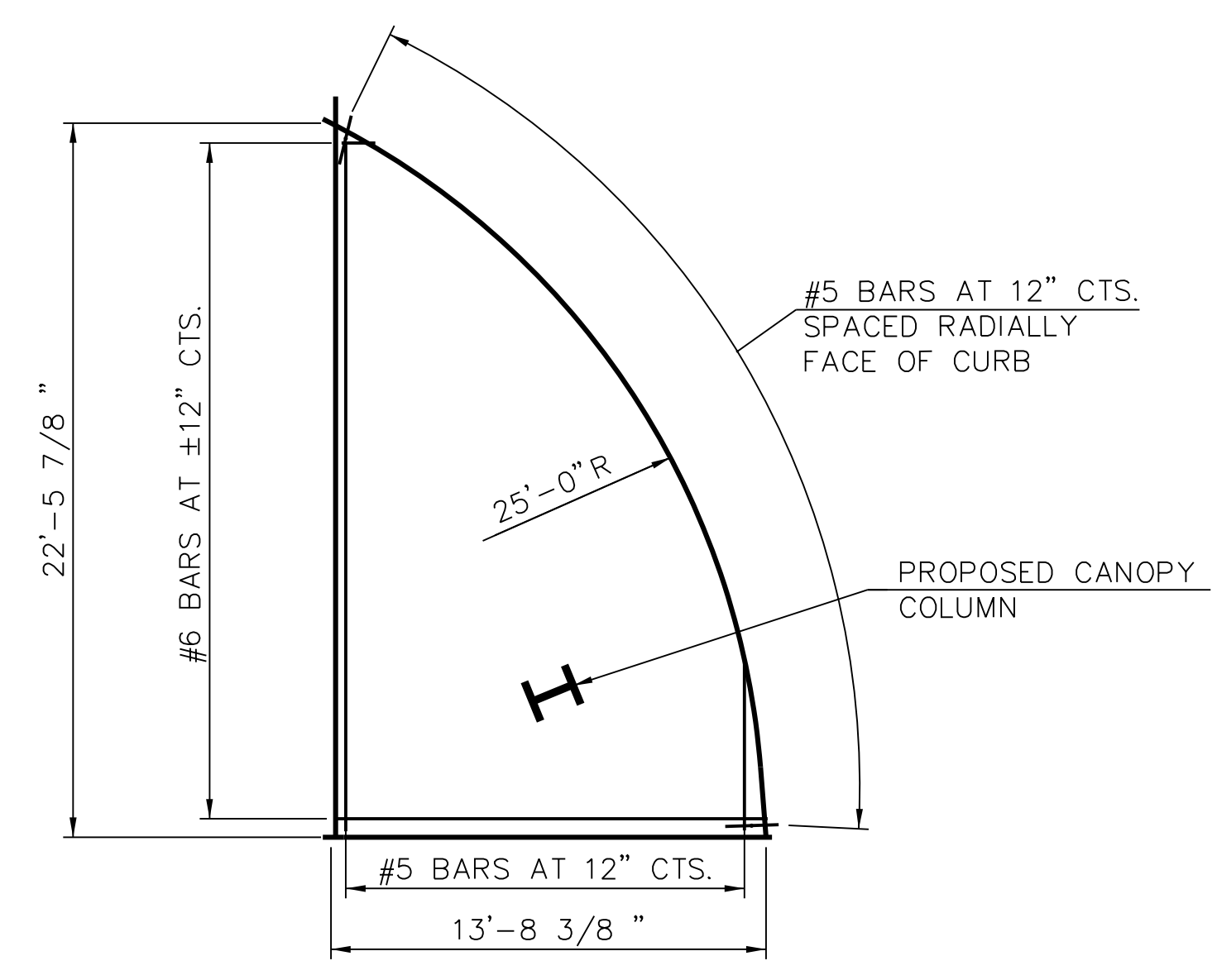
1. SEE SHEET NBB-S-320 FOR SECTIONS C AND D AND NBB-S-318 FOR SECTION E.
2. FOR ADDITIONAL REINFORCEMENT AT CANOPY COLUMNS, SEE SHEET NBB-S-318.



**2 DETAIL A**



**4 PREFORMED JOINT SEAL**



**3 SOUTH EAST SIDEWALK PLAN**

**NOT FOR CONSTRUCTION**



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**95th Terminal Improvement**  
14 West 95th Street (North Terminal)  
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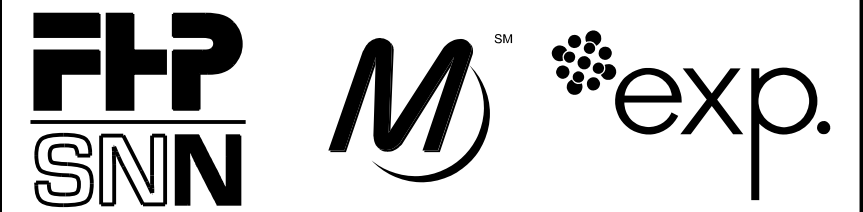
LOCATION IDENTIFIER: DR095

95TH ST. NORTH BUS BRIDGE  
EAST APPROACH SLAB PLAN

**NBB-S-319**

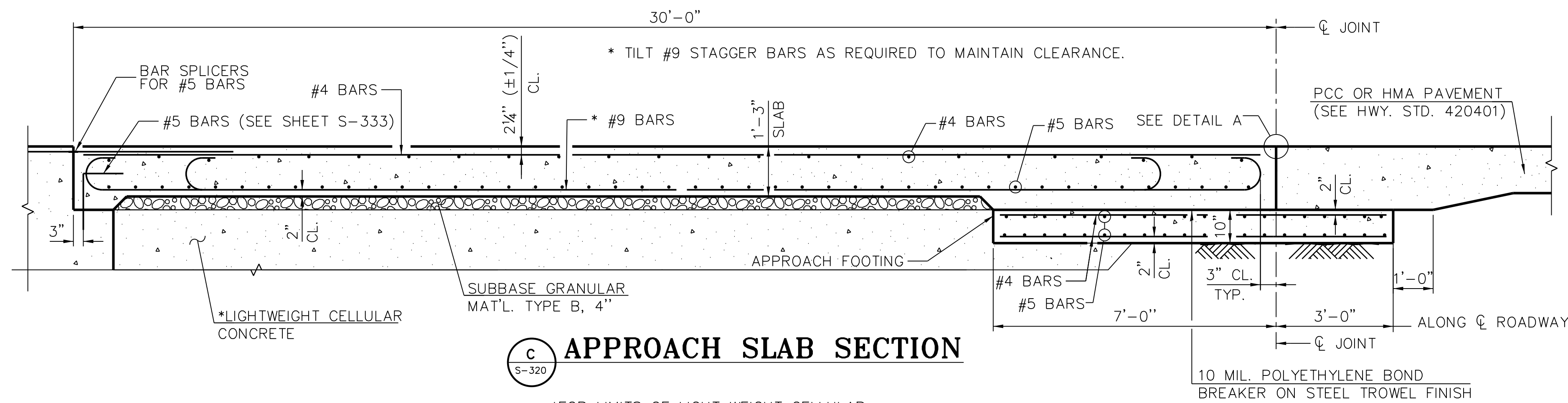
FILE NAME = 070\_DR095\_S-319.dgn  
PLOT DATE = 5/24/2018

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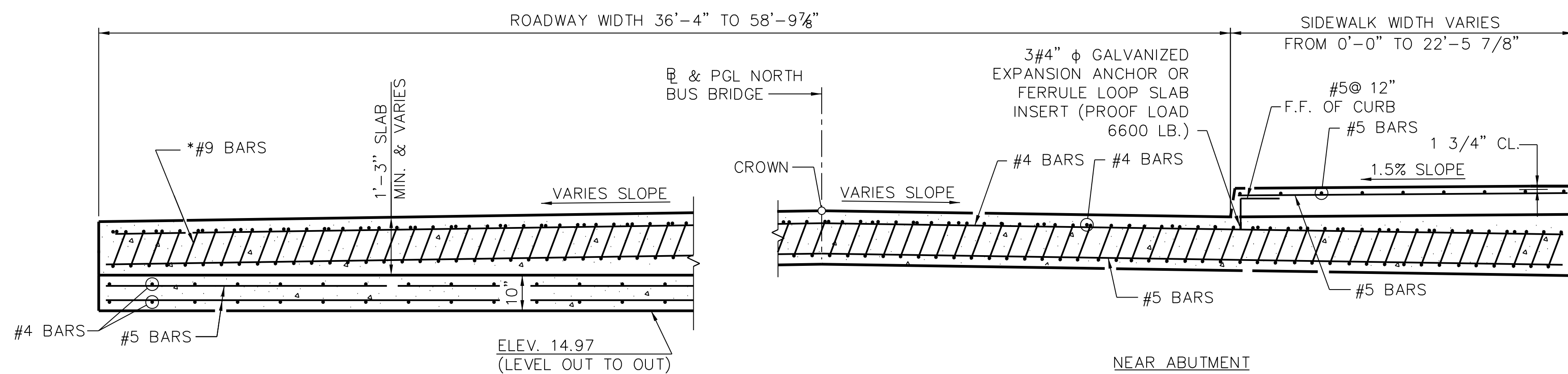
**NOTES:**

1. FOR DETAIL A, SEE SHEET NBB-S-319.
2. THE APPROACH FOOTING MAXIMUM APPLIED SERVICE BEARING PRESSURE (QMAX) = 2.0 KSF.
3. FOR SUBBASE GRANULAR MATERIAL AND LIGHTWEIGHT CELLULAR CONCRETE, SEE SHEETS NBB-S-334 AND NBB-S-335.



**C APPROACH SLAB SECTION**  
5-320

\*FOR LIMITS OF LIGHT WEIGHT CELLULAR CONCRETE SEE NBB-S-334 AND NBB-S-335.



**D APPROACH SLAB SECTION**  
5-320 (SEE PLAN FOR DIMENSIONS NOT SHOWN)

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**95th Terminal Improvement**  
14 West 95th Street (North Terminal)  
Chicago, IL 60628

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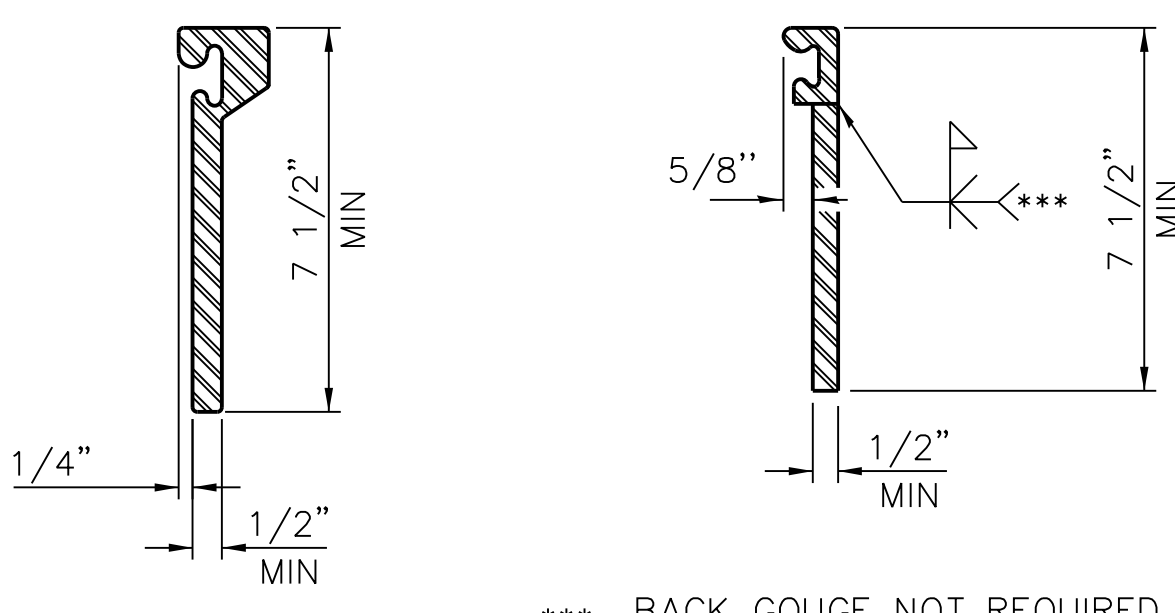
LOCATION IDENTIFIER: **DR095**

95TH ST. NORTH BUS BRIDGE  
EAST APPROACH SLAB  
SECTIONS AND DETAILS

**NBB-S-320**

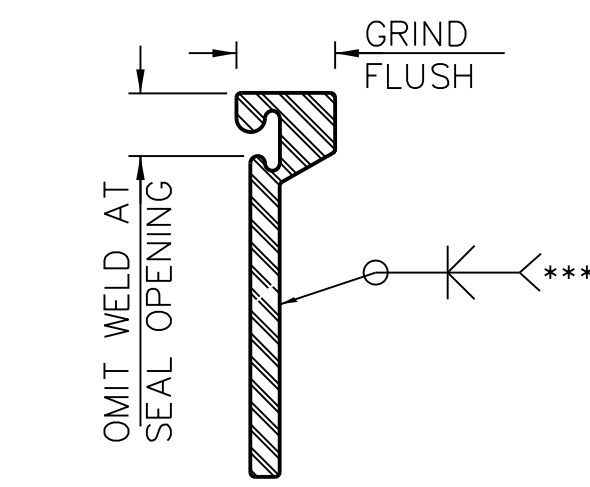


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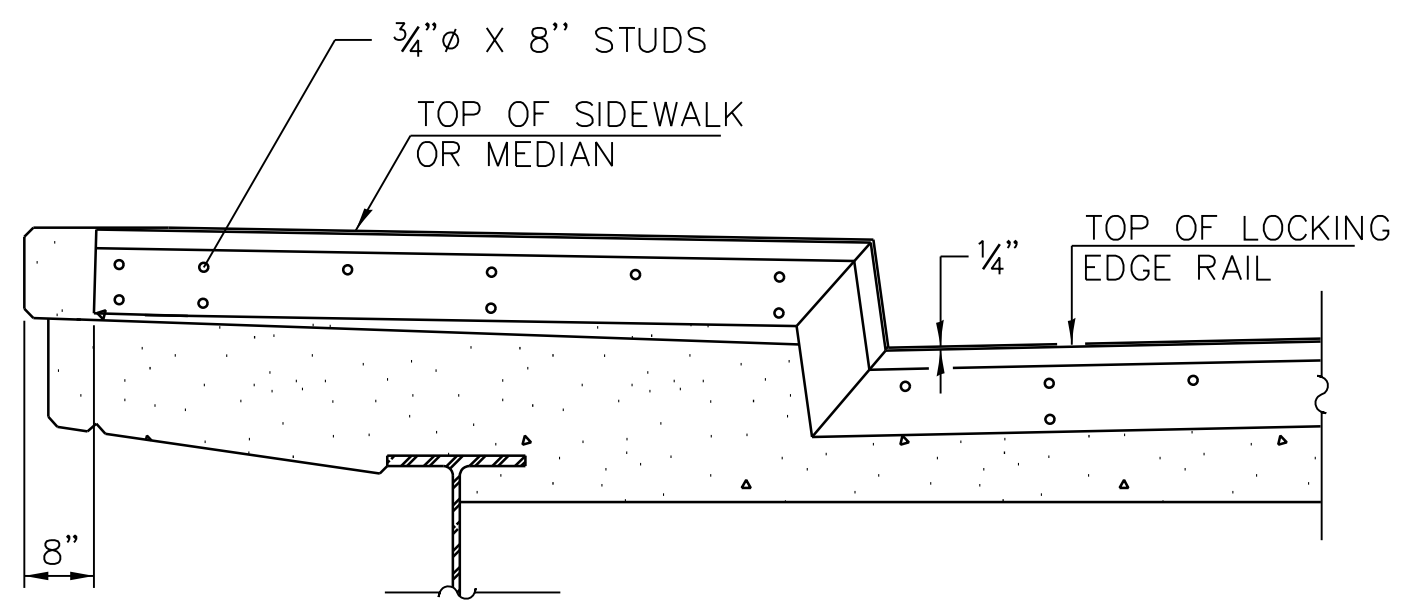
**1 ROLLED EXTRUDED RAIL**  
LOCKING EDGE RAILS

**2 WELDED RAIL**  
LOCKING EDGE RAIL SPLICE



THE INSIDE OF THE LOCKING EDGE RAIL GROOVE SHALL BE FREE OF WELD RESIDUE. ROLLED RAIL SHOWN, WELDED RAIL SIMILAR.

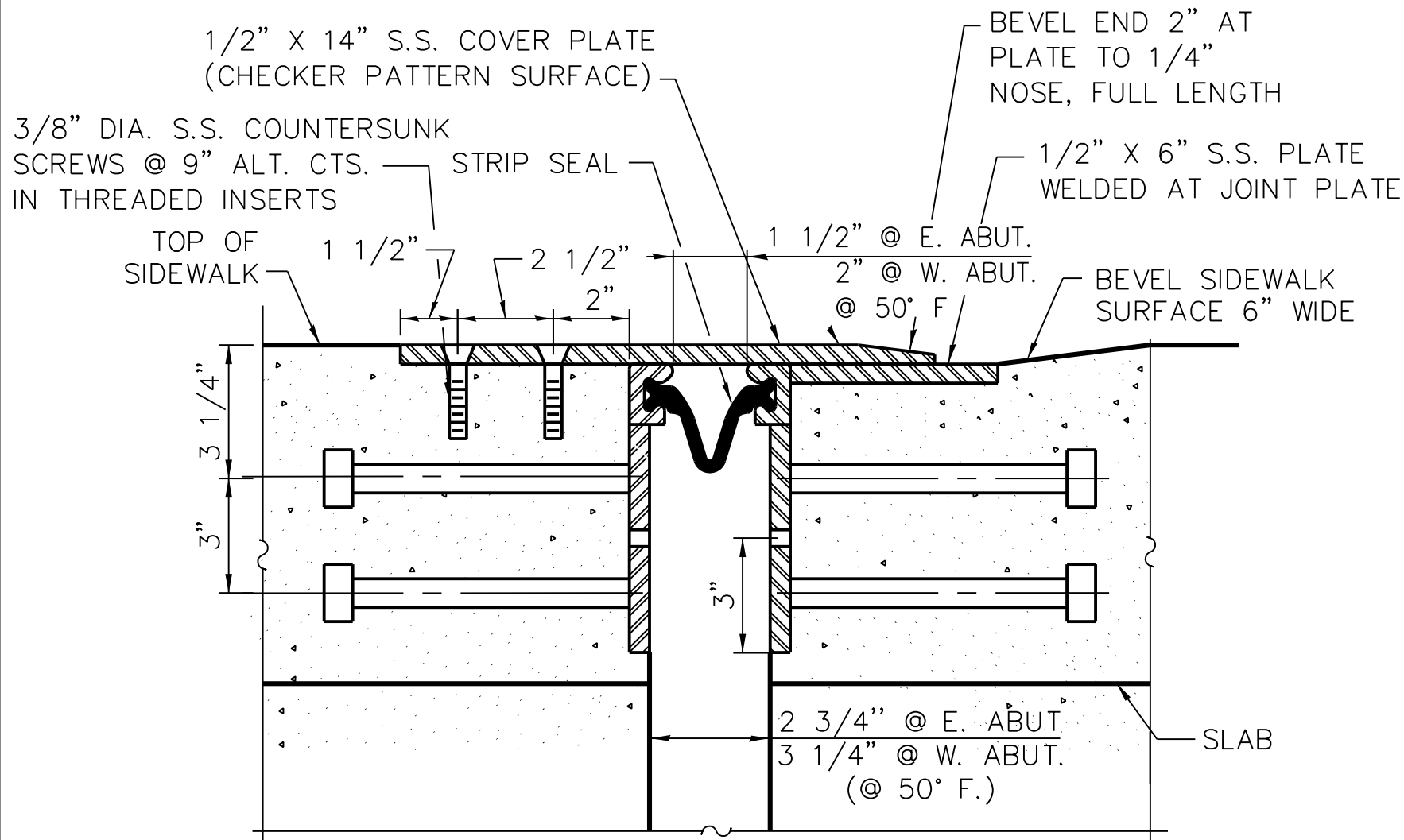
**3 LOCKING EDGE RAIL SPLICE**  
LOCKING EDGE RAIL SPLICE



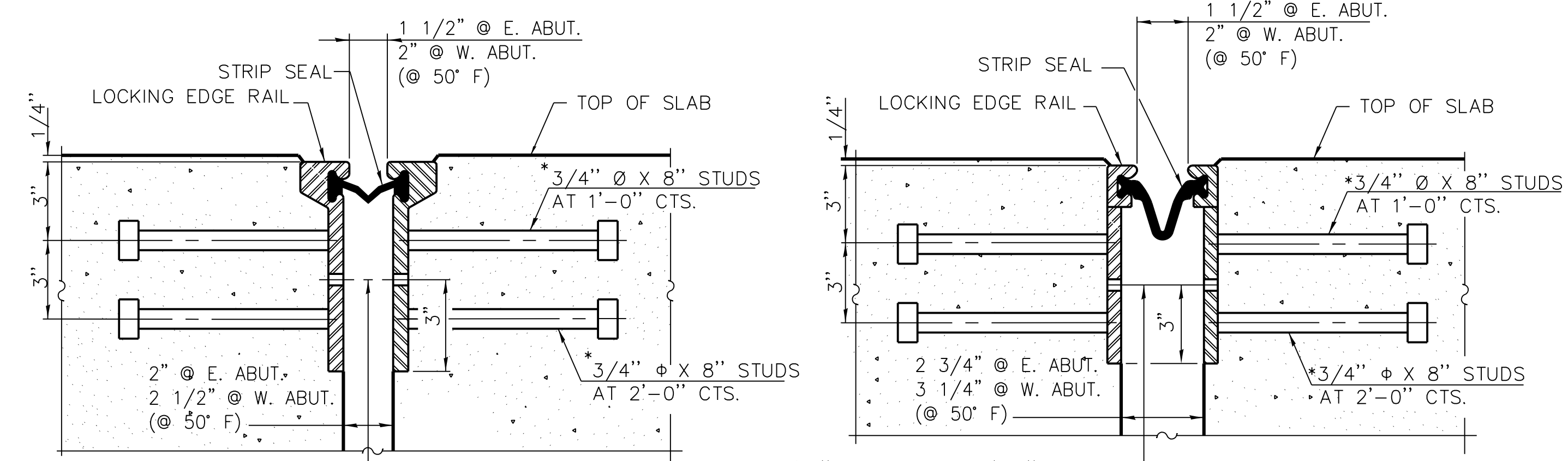
**4 TYPICAL END TREATMENT AT SIDEWALK OR MEDIAN**

**NOTES:**

1. THE STRIP SEAL SHALL BE MADE CONTINUOUS AND SHALL HAVE A MINIMUM THICKNESS OF 1/4". THE CONFIGURATION OF THE STRIP SEAL SHALL MATCH THE CONFIGURATION OF THE LOCKING EDGE RAILS. OPEN OR "WEBBED" STRIP SEAL GLAND CONFIGURATIONS ARE NOT PERMITTED. THE GLAND SHALL BE SIZED FOR A MAXIMUM RATED MOVEMENT OF 4 INCHES.
2. THE LOCKING EDGE RAILS DEPICTED ARE CONCEPTUAL ONLY, EXCEPT FOR THE MINIMUM DIMENSIONS SHOWN. THE ACTUAL CONFIGURATION OF THE LOCKING EDGE RAILS AND MATCHING STRIP SEAL MAY VARY FROM MANUFACTURER TO MANUFACTURER. FLANGED EDGE RAILS WILL NOT BE ALLOWED. LOCKING EDGE RAILS MAY BE SPLICED AT SLOPE DISCONTINUITIES.
3. THE MANUFACTURER'S RECOMMENDED INSTALLATION METHODS SHALL BE FOLLOWED.
4. THE JOINT OPENING AND DECK DIMENSIONS DETAILED ON THE SUPERSTRUCTURE ARE BASED ON A ROLLED RAIL EXPANSION JOINT. IF THE CONTRACTOR ELECTS TO USE THE WELDED RAIL EXPANSION JOINT, THE OPENING AND DECK DIMENSIONS SHALL BE MODIFIED ACCORDING TO THE DIMENSIONS DETAILED ON THIS SHEET. REQUIRED MODIFICATIONS SHALL BE MADE AT NO ADDITIONAL COST.
5. ALL STEEL COMPONENTS SHALL BE GALVANIZED AFTER FABRICATION ACCORDING TO ARTICLE 520.03 OF THE IDOT STANDARD SPECIFICATIONS.
6. MAXIMUM SPACE BETWEEN RAIL SEGMENTS SHALL BE 3/16", SEALED WITH A SUITABLE SEALANT. JOINTS IN RAILS WITHIN 10 FT. OF CURBS SHALL BE WELDED.
7. ALL STAINLESS STEEL COVER PLATES TO BE STAINLESS DIAMOND FLOOR PLATE CONFORMING TO THE REQUIREMENTS OF ASTM A793, TYPE 304.



**5 SECTION THRU WELDED RAIL JOINT AT SIDEWALK**

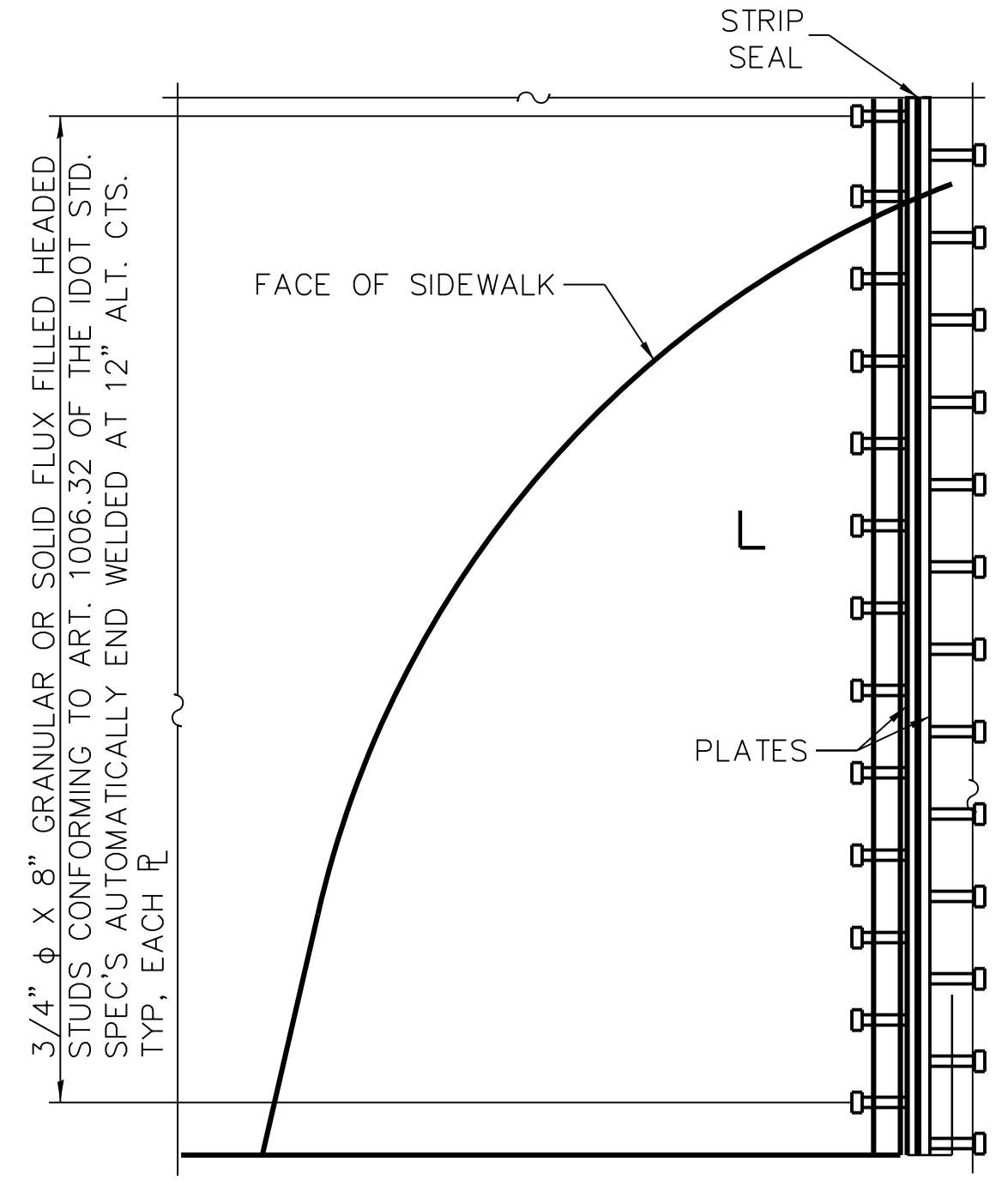


7/16"  $\phi$  HOLES AT 4'-0" CTS. FOR 3/8"  $\phi$  BOLTS. ALL BOLTS SHALL BE BURNED, SAWED, OR CHIPPED OFF FLUSH WITH THE PLATES AFTER FORMS ARE REMOVED, TYP.

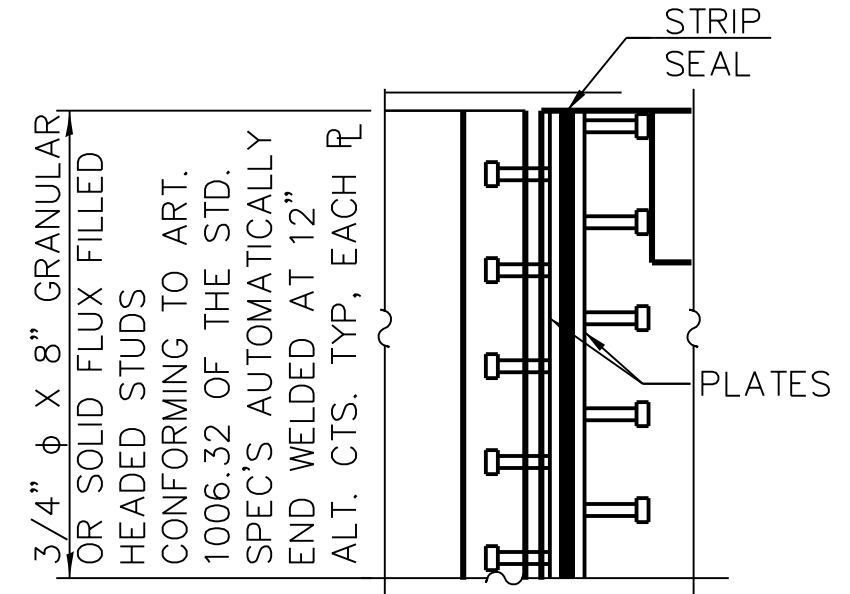
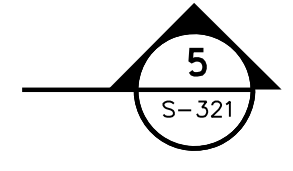
\* GRANULAR OR SOLID FLUX FILLED HEADED STUDS CONFORMING TO ARTICLE 1006.32 OF THE IDOT STD. SPECS., AUTOMATICALLY END WELDED.

**6 SECTION THRU ROLLED RAIL JOINT**

**7 SECTION THRU WELDED RAIL JOINT**  
SCALE: N.T.S.



**8 PLAN AT SIDEWALK**  
(SW SHOWN, SE OPPOSITE HAND)



**9 PLAN AT NORTH END**  
(NW SHOWN, NE OPPOSITE HAND)

**NOT FOR CONSTRUCTION**



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<b>APPROVED BY</b>	D.ZROKA
<b>CHECKED BY</b>	P.MAID
<b>DESIGNED BY</b>	L.SOMMER
<b>DRAWN BY</b>	T.SLEDGE
<b>PROJECT NO.</b>	2012-0021
<b>FILE NAME</b>	File Name

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LOCATION IDENTIFIER: DR095

95TH ST. NORTH BUS BRIDGE  
PREFORMED JOINT STRIP SEAL

**NBB-S-321**

FILE NAME = 070\_DR095\_S-321.dgn  
PLOT DATE = 5/24/2018

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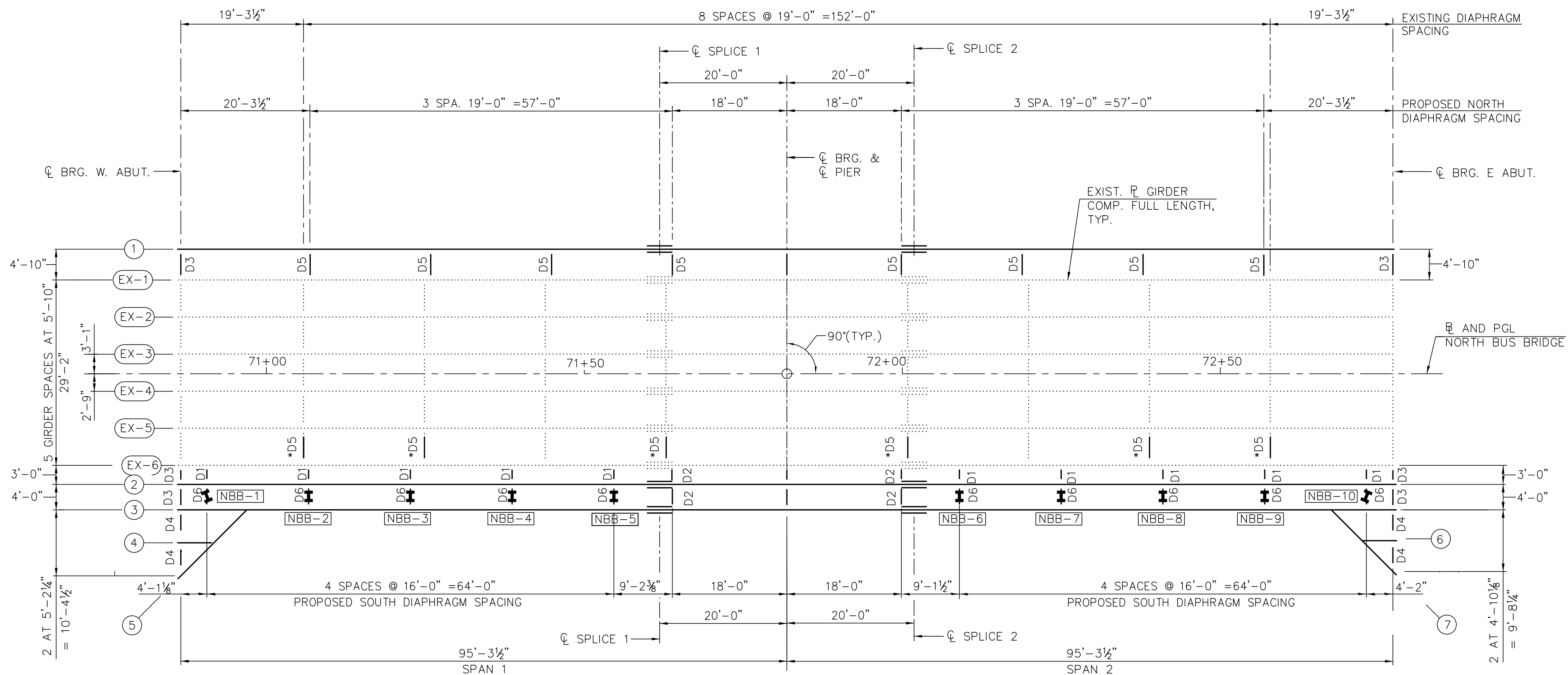
<b>IN CHARGE</b>	<b>S. KADERBEK</b>
<b>APPROVED BY</b>	<b>D.ZROKA</b>
<b>CHECKED BY</b>	<b>P.MAID</b>
<b>DESIGNED BY</b>	<b>L.SOMMER</b>
<b>DRAWN BY</b>	<b>T.SLEDGE</b>
<b>PROJECT NO.</b>	<b>2012-0021</b>
<b>FILE NAME</b>	<b>File Name</b>

MARK	DATE	DESCRIPTION
	05/23/18	FINAL DESIGN
	03/07/18	INTERMEDIATE DESIGN

LOCATION IDENTIFIER: **DR095**

**95TH ST. NORTH BUS BRIDGE FRAMING PLAN**

**NBB-S-322**



**1 FRAMING PLAN**

**NOTES:**

- ALL PLATES OF THE PROPOSED GIRDERS, INCLUDING BEARING STIFFENERS AND SPLICE PLATES, SHALL BE AASHTO M270, GRADE 50.
- LOAD CARRYING COMPONENTS DESIGNATED "NTR" SHALL CONFORM TO THE IMPACT TESTING REQUIREMENT, ZONE 2.
- ALL STRUCTURAL STEEL SHALL BE HOT-DIPPED GALVANIZED.
- GIRDERS HAVE BEARING STIFFENERS AND CONNECTION PLATES AS REQUIRED BY DESIGN. ADDITIONAL STIFFENERS MAY BE ADDED AT THE CONTRACTOR'S EXPENSE AS NECESSARY TO PREVENT DISTORTION OF THE GIRDERS DURING GALVANIZING. THE CONTRACTOR SHALL COORDINATE WITH THE FABRICATOR AND THE GALVANIZER TO DETERMINE IF ADDITIONAL STIFFENERS ARE NECESSARY, AND WHERE THESE SHOULD BE PLACED. ANY PROPOSED CHANGES SHALL BE SUBMITTED TO THE ENGINEER FOR APPROVAL PRIOR TO MAKING ANY CHANGES.
- TEMPORARY STIFFENER ANGLES SHALL BE BOLTED TO EACH SIDE OF THE SPLICE ENDS OF EACH GIRDER SEGMENT TO PREVENT DISTORTION DURING GALVANIZING. TEMPORARY STIFFENERS ANGLES SHALL BOLT OR FIT TIGHT AGAINST TOP & BOTTOM FLANGES AND INCLUDE SPACER TUBES TO MINIMIZE DAMAGE TO GALVANIZING DURING REMOVAL.
- ALL DIAPHRAGMS SHALL BE INSTALLED AS STEEL IS ERECTED AND SECURED WITH ERECTION PINS AND BOLTS EXCEPT AS OTHERWISE NOTED. INDIVIDUAL DIAPHRAGMS AT SUPPORTS MAY BE TEMPORARILY DISCONNECTED TO INSTALL BEARING ANCHOR RODS.
- FOR DIAPHRAGM DETAILS, SEE SHEETS NBB-S-326 & NBB-S-326A.
- FOR FIELD SPLICE DETAILS, SEE SHEET NBB-S-326.
- SEE SPECIAL PROVISION FOR PAINTING EXISTING STRUCTURE.
- FOR CONNECTION AT PIER SEE NBB-S-336 AND NBB-S-337.

\* EXISTING DIAPHRAGM TO BE REMOVED TO PROVIDE CLEARANCE FOR DRAINAGE SYSTEM. DIAPHRAGMS TO BE REPLACED BY DIAPHRAGM D5. PROPOSED DIAPHRAGM MAY BE OFFSET FROM EXISTING LOCATION BY NO MORE THAN ONE (1) FOOT TO FACILITATE INSTALLATION.

CANOPY LOCATION	STATION	OFFSET	BASELINE
NBB-1	70+90.65	19.2917	Ⓜ N. BUS BRIDGE
NBB-2	71+06.65	19.2917	Ⓜ N. BUS BRIDGE
NBB-3	71+22.65	19.2917	Ⓜ N. BUS BRIDGE
NBB-4	71+38.65	19.2917	Ⓜ N. BUS BRIDGE
NBB-5	71+54.65	19.2917	Ⓜ N. BUS BRIDGE
NBB-6	72+08.98	19.2917	Ⓜ N. BUS BRIDGE
NBB-7	72+24.98	19.2917	Ⓜ N. BUS BRIDGE
NBB-8	72+40.98	19.2917	Ⓜ N. BUS BRIDGE
NBB-9	72+56.98	19.2917	Ⓜ N. BUS BRIDGE
NBB-10	72+72.98	19.2917	Ⓜ N. BUS BRIDGE

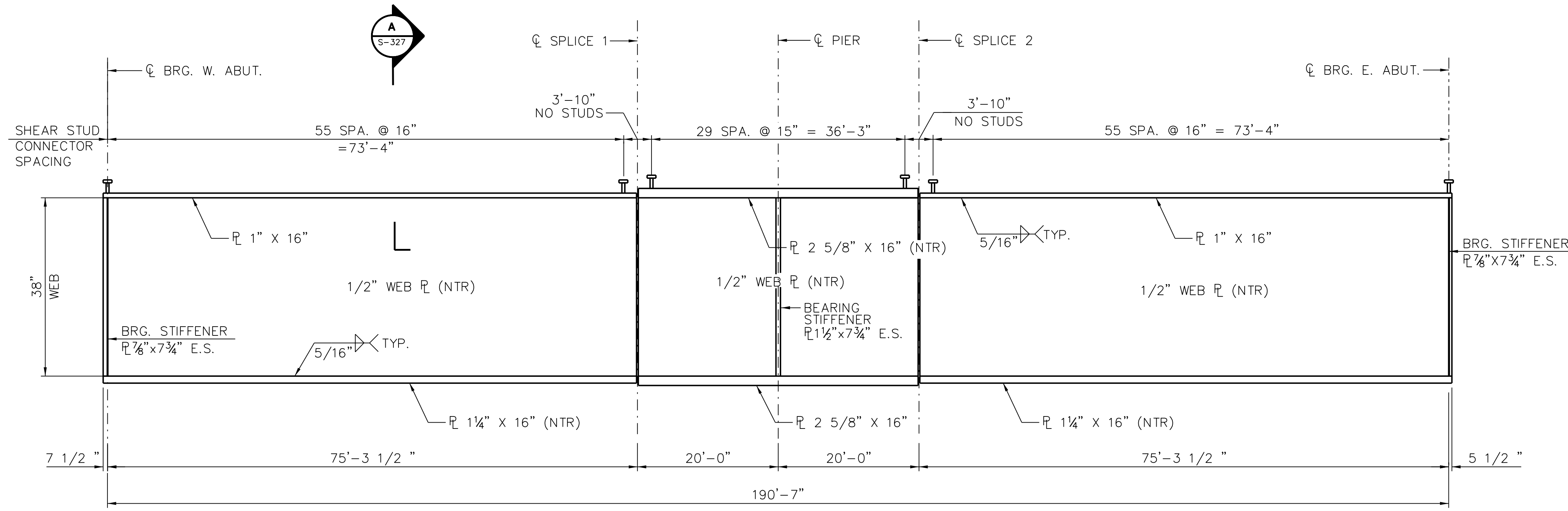
**LEGEND:**

**I** CANOPY NO  
FOR CANOPY COLUMN DETAILS, SEE ARCHITECTURAL PLANS.

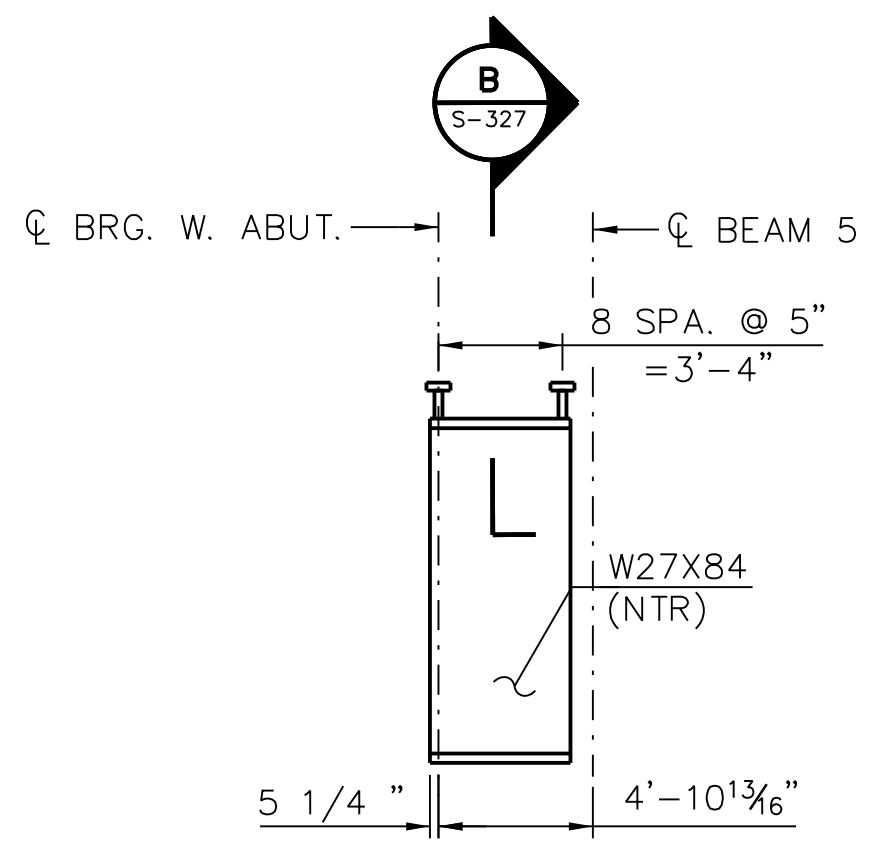
vscorppa

FILE NAME = 070\_DR095\_S-322.dgn  
PLOT DATE = 5/24/2018

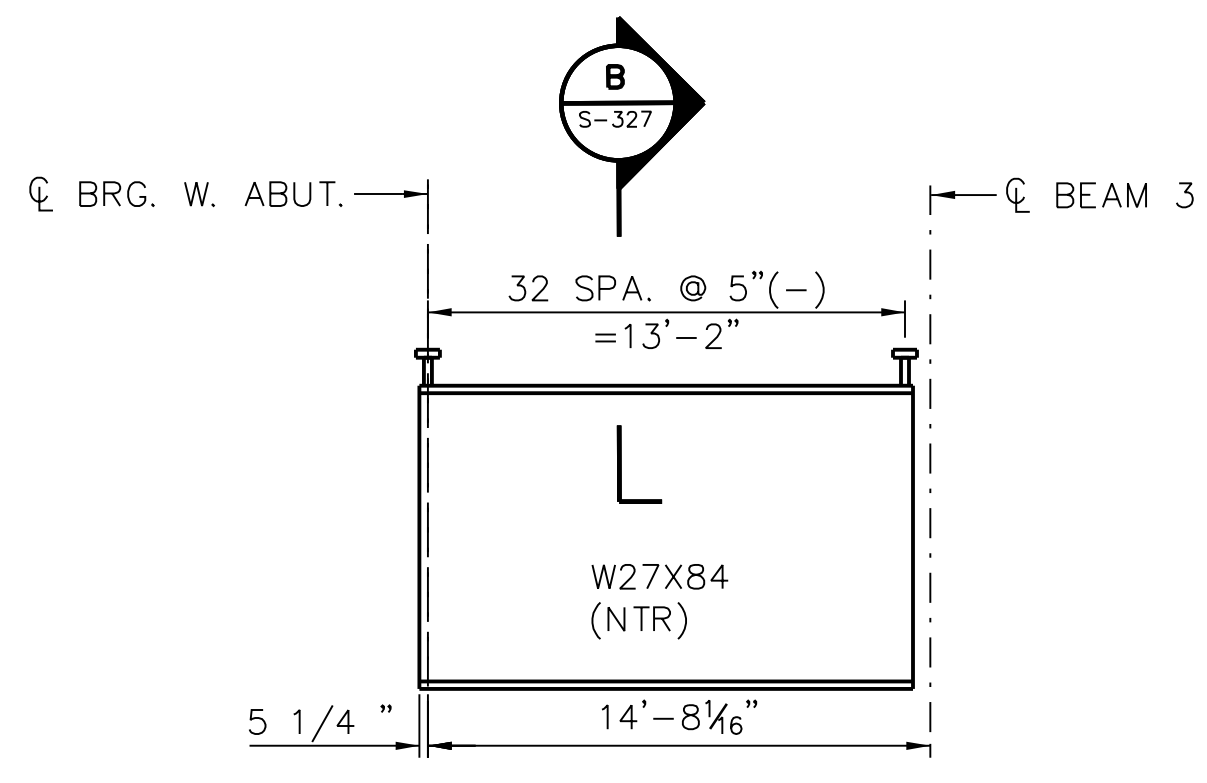
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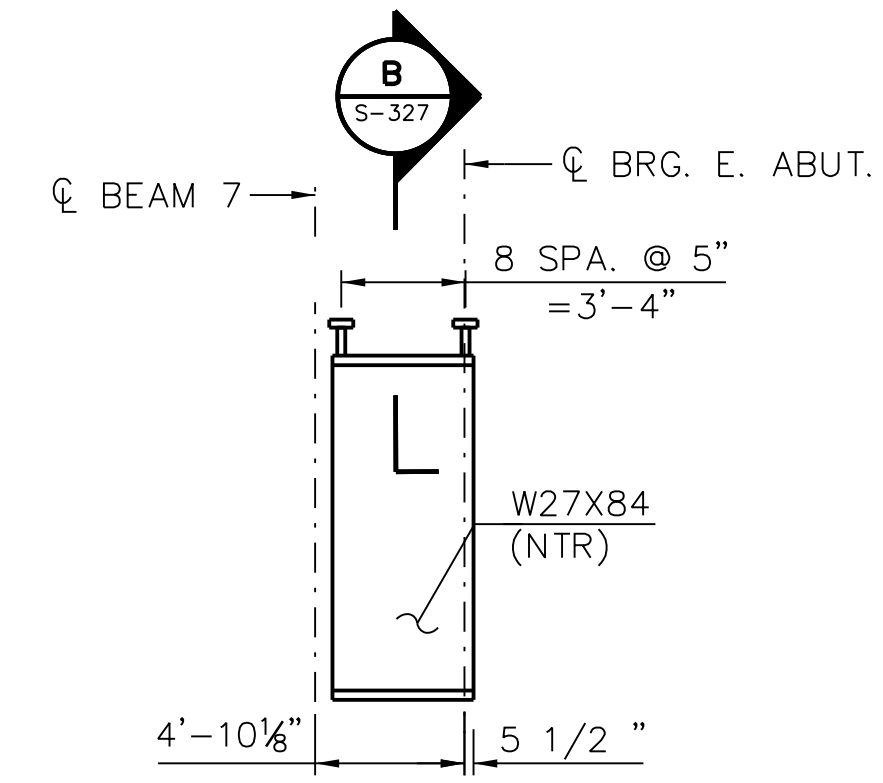
1 PROP. GIRDERS 1 THRU 3 ELEVATION



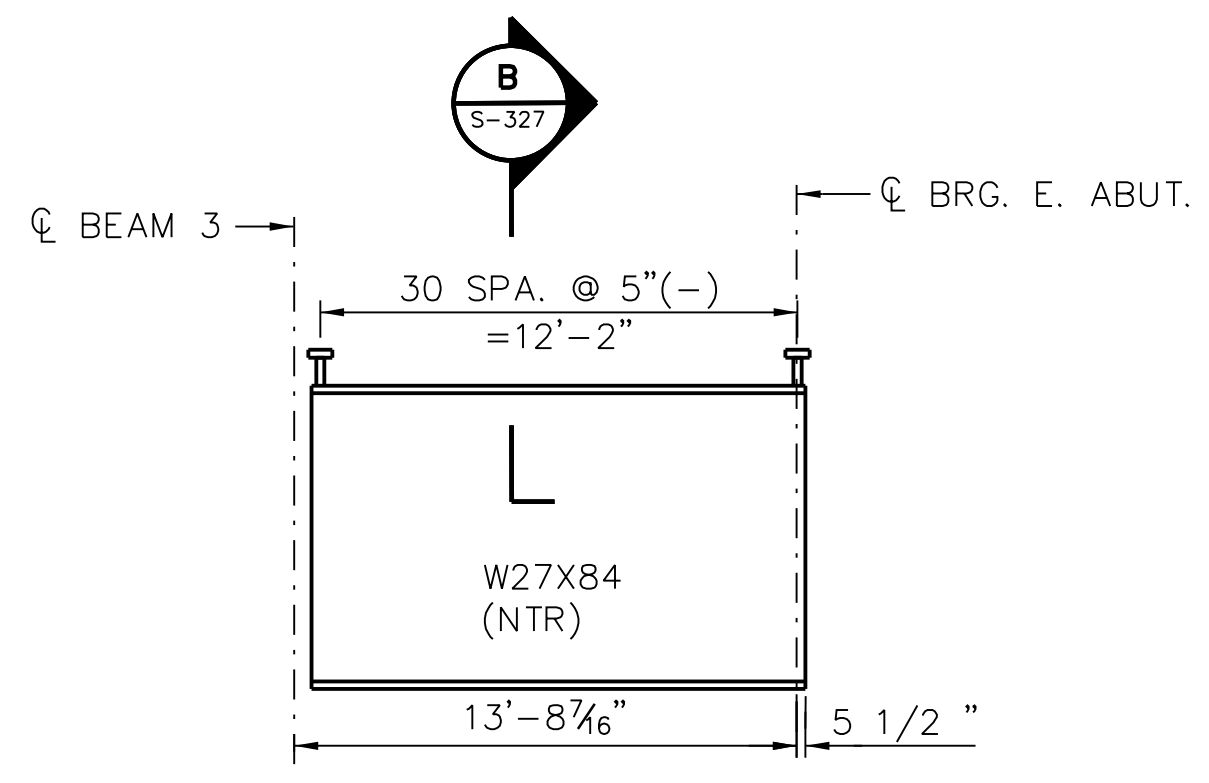
3 BEAM 4 ELEVATION



2 BEAM 5 ELEVATION



4 BEAM 6 ELEVATION



7 BEAM 7 ELEVATION

NOTE:  
FOR SPLICE DETAILS, SEE SHEET NBB-S-326.  
FOR BEARING STIFFENERS AND SHEAR STUD DETAILS  
SEE SHEET NBB-S-327.

**NOT FOR CONSTRUCTION**



**cta** CHICAGO TRANSIT AUTHORITY ENGINEERING

**SENSITIVE SECURITY INFORMATION**  
WARNING: THIS RECORD CONTAINS SENSITIVE SECURITY INFORMATION THAT IS CONTROLLED UNDER 49 CFR PARTS 15 AND 1550. NO PART OF THIS RECORD MAY BE DISCLOSED TO PERSONS WITHOUT A "NEED TO KNOW" AS DEFINED IN 49 CFR PARTS 15 AND 1550, EXCEPT WITH THE WRITTEN PERMISSION OF THE ADMINISTRATOR OF THE TRANSPORTATION SECURITY ADMINISTRATION OR THE SECRETARY OF TRANSPORTATION. UNAUTHORIZED RELEASE MAY RESULT IN CIVIL PENALTY OR OTHER ACTION. FOR U.S. GOVERNMENT AGENCIES, PUBLIC DISCLOSURE IS GOVERNED BY E.O.S. 13526 AND 49 CFR PARTS 15 AND 1550.

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**95th Terminal Improvement**  
14 West 95th Street (North Terminal)  
Chicago, IL 60628

IN CHARGE	S. KADERBEK
APPROVED BY	D.ZROKA
CHECKED BY	P.MAID
DESIGNED BY	L.SOMMER
DRAWN BY	T.SLEDGE
PROJECT NO.	2012-0021
FILE NAME	File Name

MARK	DATE	DESCRIPTION
	05/23/18	FINAL DESIGN
	03/07/18	INTERMEDIATE DESIGN

LOCATION IDENTIFIER: DR095

95TH ST. NORTH BUS BRIDGE  
PROPOSED GIRDERS ELEVATIONS

**NBB-S-323**

FILE NAME = 070\_DR095\_S-323.dgn  
PLOT DATE = 5/24/2018

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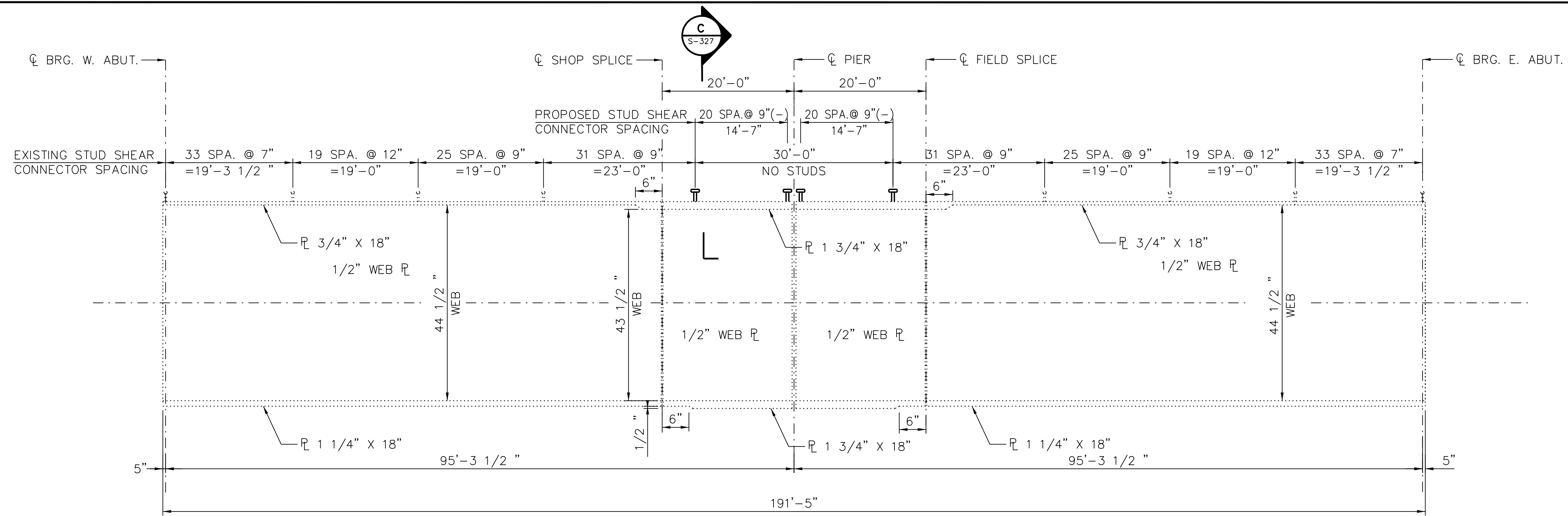
LOCATION IDENTIFIER: **DR095**

95TH ST. NORTH BUS BRIDGE  
EXISTING GIRDER ELEVATIONS

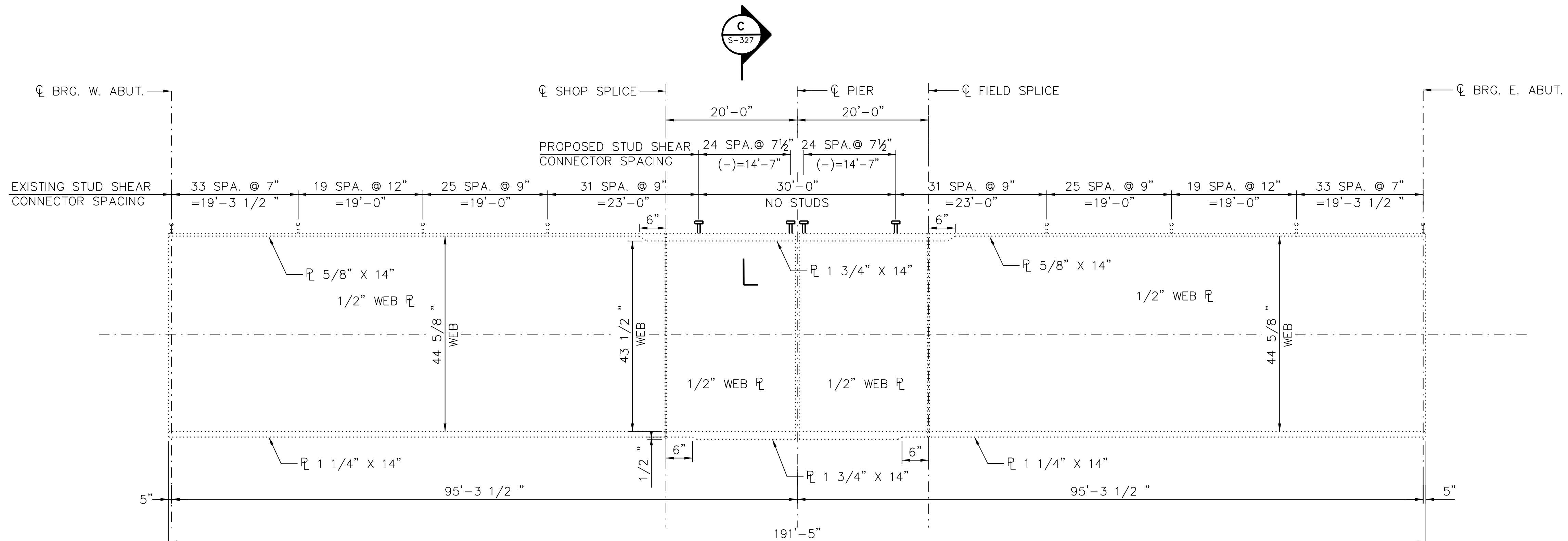
**NBB-S-324**

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FILE NAME = 070\_DR095\_S-324.dgn  
PLOT DATE = 5/24/2018



**1 EXISTING EXTERIOR GIRDER ELEVATION**  
GIRDERS EX-1 AND EX-6



**2 EXISTING INTERIOR GIRDER ELEVATION**  
GIRDERS EX-2 THRU EX-5

**NOT FOR CONSTRUCTION**



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LOCATION IDENTIFIER: **DR095**  
**95TH ST. NORTH BUS BRIDGE GIRDER MOMENT AND REACTION TABLES**

**NBB-S-325**

$I_s, S_s$ : Non-composite moment of inertia and section modulus of the steel section used for computing  $f_s$  (Total and Overload) due to non-composite dead loads (in.4 and in.3).  
 $I_c(n), S_c(n)$ : Composite moment of inertia and section modulus of the steel and deck based upon the modular ratio, "n", used for computing  $f_s$  (Total and Overload) due to short-term composite live loads (in.4 and in.3).  
 $I_c(3n), S_c(3n)$ : Composite moment of inertia and section modulus of the steel and deck based upon 3 times the modular ratio, "3n", used for computing  $f_s$  (Total and Overload) due to long-term composite (superimposed) dead loads (in.4 and in.3).  
 $Q$ : Un-factored non-composite dead load (kips/ft.).  
 $M_Q$ : Un-factored moment due to non-composite dead load (kip-ft.).  
 $s_Q$ : Un-factored long-term composite (superimposed) dead load (kips/ft.).  
 $M_sQ$ : Un-factored moment due to long-term composite (superimposed) dead load (kip-ft.).  
 $M_L$ : Un-factored live load moment (kip-ft.).  
 $M_{IM}$ : Un-factored moment due to impact (kip-ft.).  
 $M_E, M_s, M_w$ : Factored Moment due to Pedestrian Live Load, Snow Load, and Wind Load.  
 $M_a$ : Factored design moment (kip-ft.).  
 $1.3 [M_Q + M_sQ + \frac{5}{3} (M_L + M_I)]$   
 OR  $1.3 (M_Q + M_sQ + M_w) + 0.5 M_s$   
 OR  $1.3 (M_Q + M_sQ + M_E + .3M_w) + 1.6 M_s$   
 $M_u$ : Compact composite moment capacity according to AASHTO LFD 10.50.1.1 or compact non-composite moment capacity according to AASHTO LFD 10.48.1 (kip-ft.).  
 $f_s$  (Overload): Sum of stresses as computed from the moments below (ksi).  
 $M_Q + M_sQ + \frac{5}{3} (M_L + M_I)$   
 $M_Q + M_sQ + M_E + s \cdot w$   
 $f_s$  (Total): Sum of stresses as computed from the moments below on non-compact section (ksi).  
 $1.3 [M_Q + M_sQ + \frac{5}{3} (M_L + M_I)]$   
 OR  $1.3 (M_Q + M_sQ + M_w) + 0.5 M_s$   
 OR  $1.3 (M_Q + M_sQ + M_E + .3M_w) + 1.6 M_s$   
 VR: Maximum  $L_t$  + impact shear range within the composite portion of the span for stud shear connector design (kips).

	0.4 Sp. 1 or 0.6 Sp. 3	Pier
$I_s$	(in <sup>4</sup> ) 15,955	36,945
$I_c(n)$	(in <sup>4</sup> ) 34,161	
$I_c(3n)$	(in <sup>4</sup> ) 24,721	
$S_s$	(in <sup>3</sup> ) 849	1708
$S_c(n)$	(in <sup>3</sup> ) 1098	
$S_c(3n)$	(in <sup>3</sup> ) 999	
$Q$	(k/ft) .625	.821
$M_Q$	(k) 884	2165
$s_Q$	(k/ft) .297	.297
$M_sQ$	(k) 181	427
$M_L$	(k) 283	471
$M_w$	(k) 1255	2536
$M_s$	(k) 855	1753
$1.3(\frac{5}{3} M_Q)$	(k) 613	1,021
$1.3 M_w + .5M_s$	(k) 2,059	4,173
$1.3 (M_Q + .3M_w) + 1.6M_s$	(k) 2,225	4,406
$M_a$	(k) 3,610	7,776
$M_u$	(k) 8,831	
$f_s$ non-comp	(ksi) 12.5	15.2
$f_s$ (comp)	(ksi) 2.2	3.0
$f_s$ $L_t + s \cdot w$	(ksi) 18.7	23.8
$f_s$ (Overload)	(ksi) 33.4	42.0
$f_s$ (Total)	(ksi) 43.4	
VR	(k) 38.0	36.6

	Abut.	Pier
$R_Q$	(k) 75.9	209.3
$R_L + .3w \cdot s$	(k) 97.5	223.3
$R_{Total}$	(k) 173.4	432.6

	0.4 Sp. 1 or 0.6 Sp. 2	Pier
$I_s$	(in <sup>4</sup> ) 15,955	36,945
$I_c(n)$	(in <sup>4</sup> ) 34,161	
$I_c(3n)$	(in <sup>4</sup> ) 24,721	
$S_s$	(in <sup>3</sup> ) 849	1708
$S_c(n)$	(in <sup>3</sup> ) 1098	
$S_c(3n)$	(in <sup>3</sup> ) 999	
$Q$	(k/ft) .681	.772
$M_Q$	(k) 405	1087
$s_Q$	(k/ft) .297	.297
$M_sQ$	(k) 166	394
$M_L$	(k) 489	545
$M_{IM}$	(k) 112	125
$\frac{5}{3} [M_L + I]$	(k) 1002	1117
$M_a$	(k) 2,044	3,377
$M_u$	(k) 8,831	
$f_s$ non-comp	(ksi) 5.7	7.6
$f_s$ (comp)	(ksi) 2.0	2.8
$f_s$ $\frac{5}{3} [M_L + M_I]$	(ksi) 11.0	7.8
$f_s$ (Overload)	(ksi) 18.7	18.2
$f_s$ (Total)	(ksi) 24.3	
VR	(k) 38.0	36.6

	Abut.	Pier
$R_Q$	(k) 35.3	132.1
$R_L$	(k) 27.7	46.6
$R_I$	(k) 6.4	10.7
$R_{Total}$	(k) 69.4	189.4

\* Compact section  
 \*\* Braced non-compact and partially braced section

	0.4 Sp. 1 or 0.6 Sp. 2	Pier
$I_s$	(in <sup>4</sup> ) 16,567	28,525
$I_c(n)$	(in <sup>4</sup> ) 48,396	
$I_c(3n)$	(in <sup>4</sup> ) 34,597	
$S_s$	(in <sup>3</sup> ) 855	1214
$S_c(n)$	(in <sup>3</sup> ) 1227	
$S_c(3n)$	(in <sup>3</sup> ) 1123	
$Q$	(k/ft) .781	.872
$M_Q$	(k) 464	1079
$s_Q$	(k/ft) .297	.297
$M_sQ$	(k) 182	355
$M_L$	(k) 609	593
$M_{IM}$	(k) 138	135
$\frac{5}{3} [M_L + I]$	(k) 1245	1213
$M_a$	(k) 2,458	3,441
$M_u$	(k) 4,874	
$f_s$ non-comp	(ksi) 6.5	10.7
$f_s$ (comp)	(ksi) 1.9	3.5
$f_s$ $\frac{5}{3} [M_L + M_I]$	(ksi) 12.2	12.0
$f_s$ (Overload)	(ksi) 20.6	26.2
$f_s$ (Total)	(ksi) 26.8	
VR	(k) 45.6	44.0

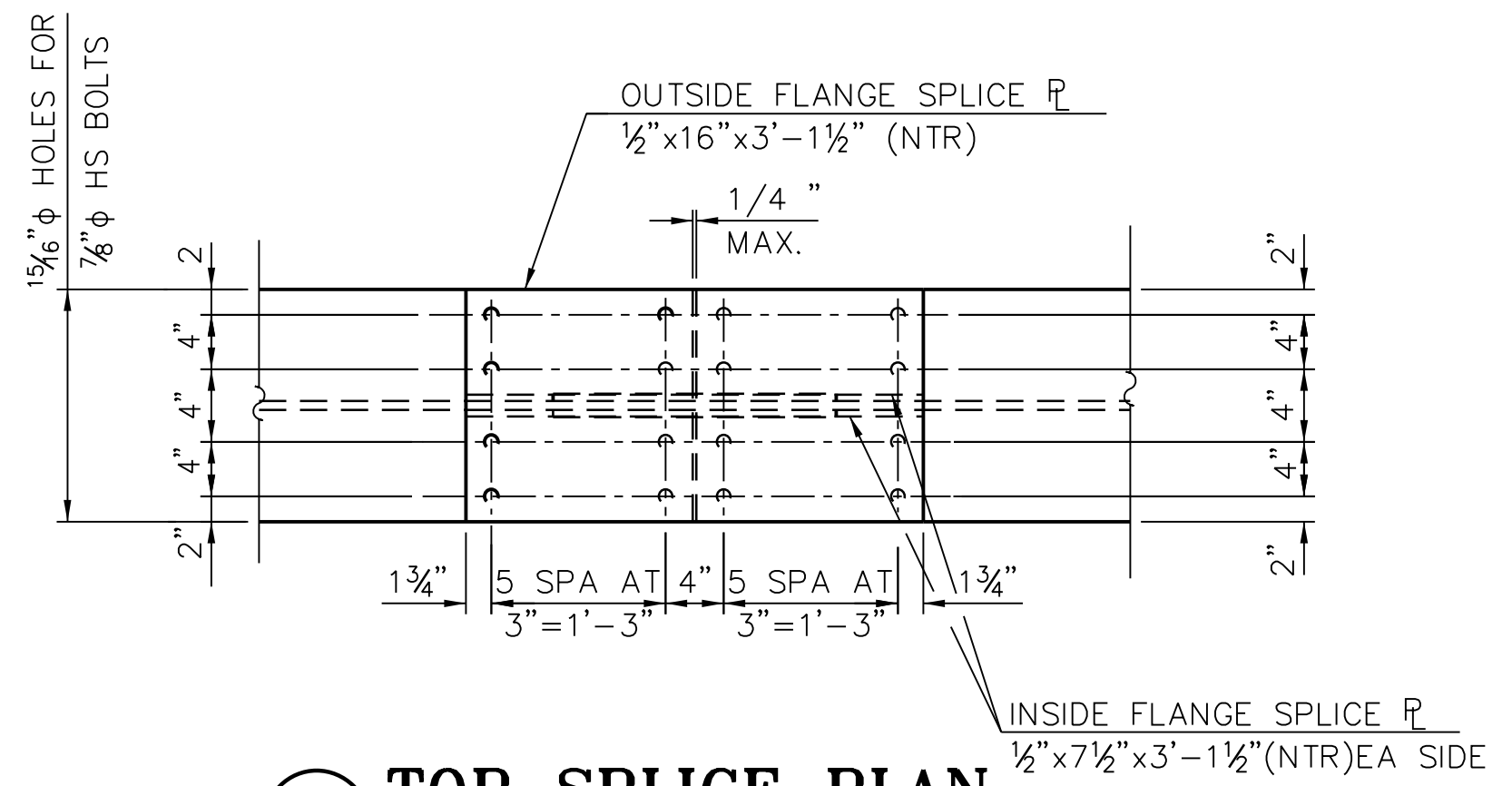
	Abut.	Pier
$R_Q$	(k) 38.3	136.3
$R_L$	(k) 33.4	55.2
$R_I$	(k) 7.6	12.5
$R_{Total}$	(k) 79.3	204.0

vscarpa

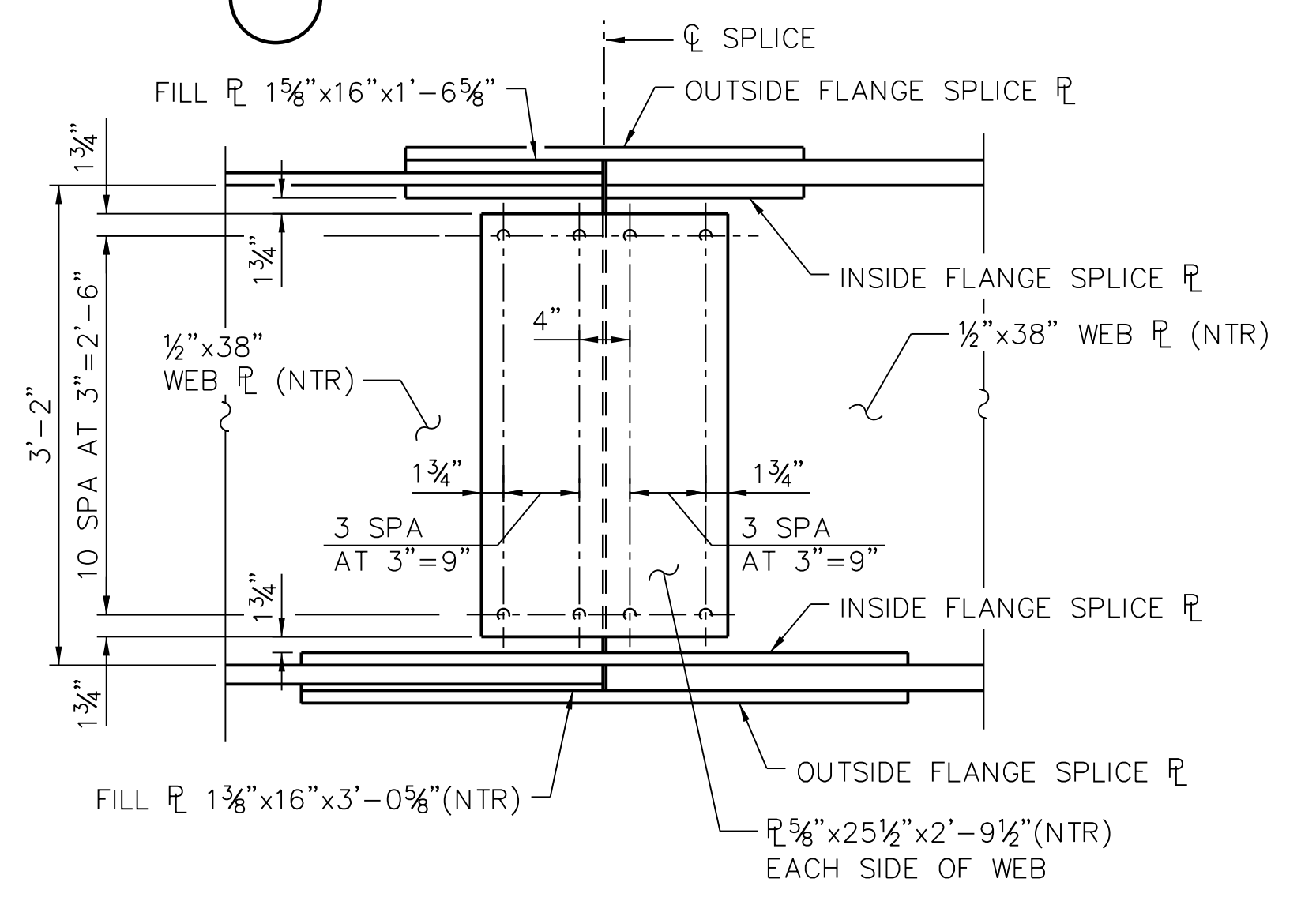
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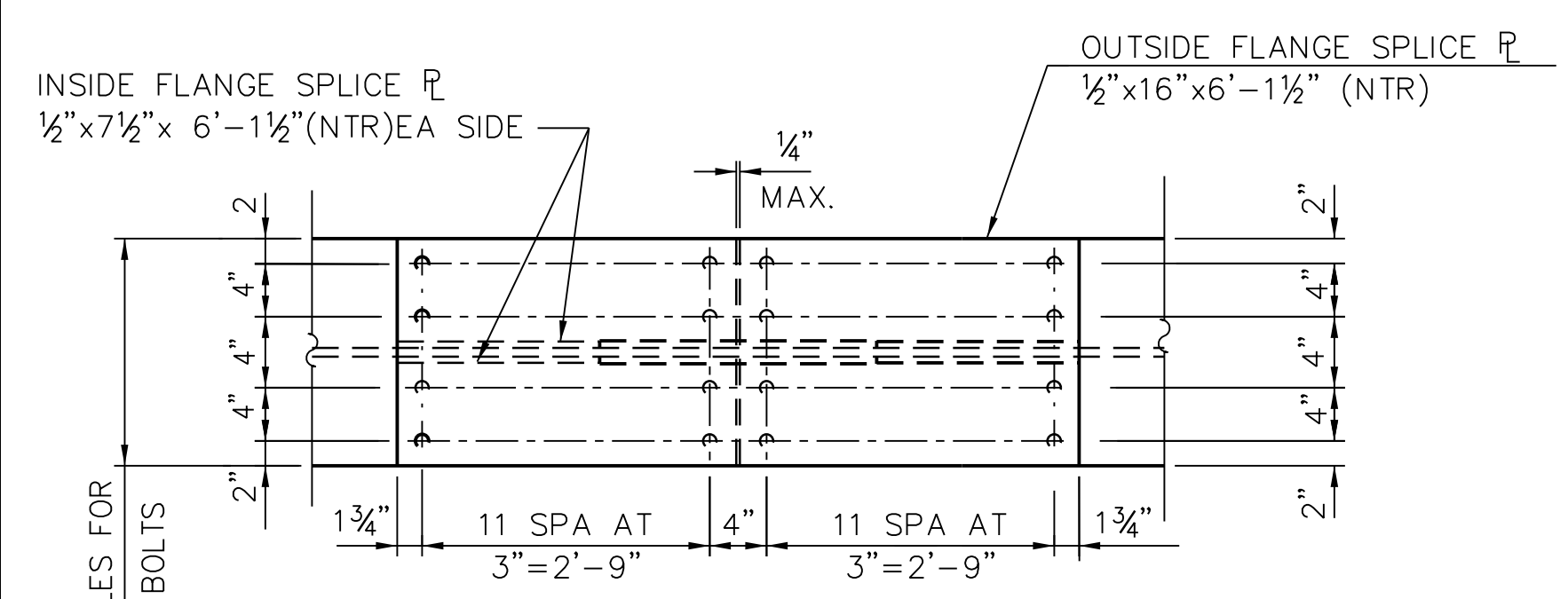
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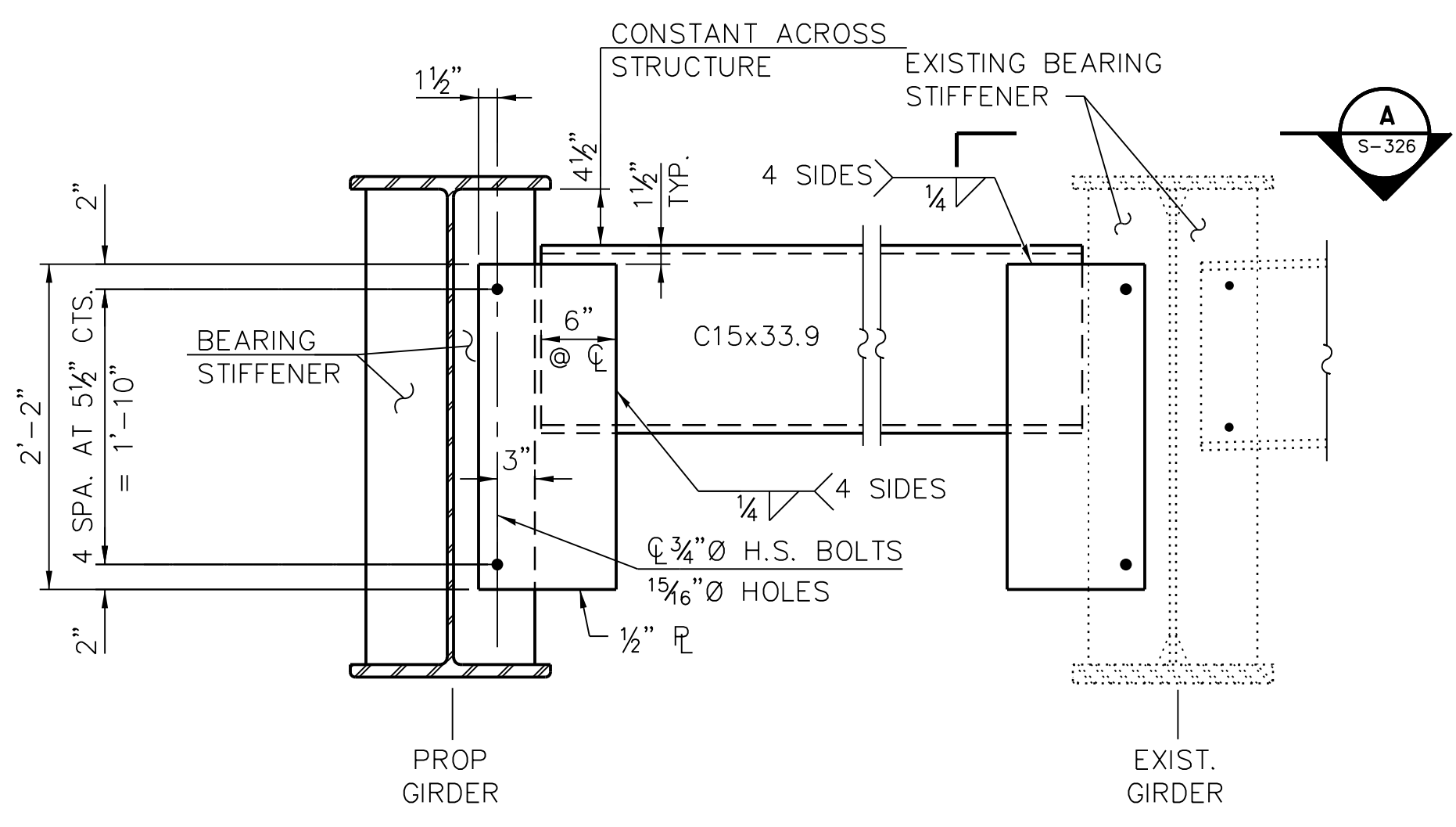
3 TOP SPLICE PLAN



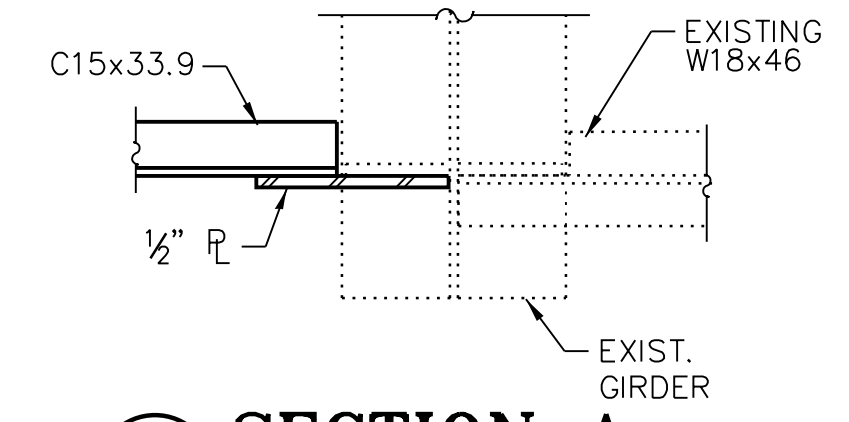
4 SPLICE ELEVATION  
(6 REQUIRED)



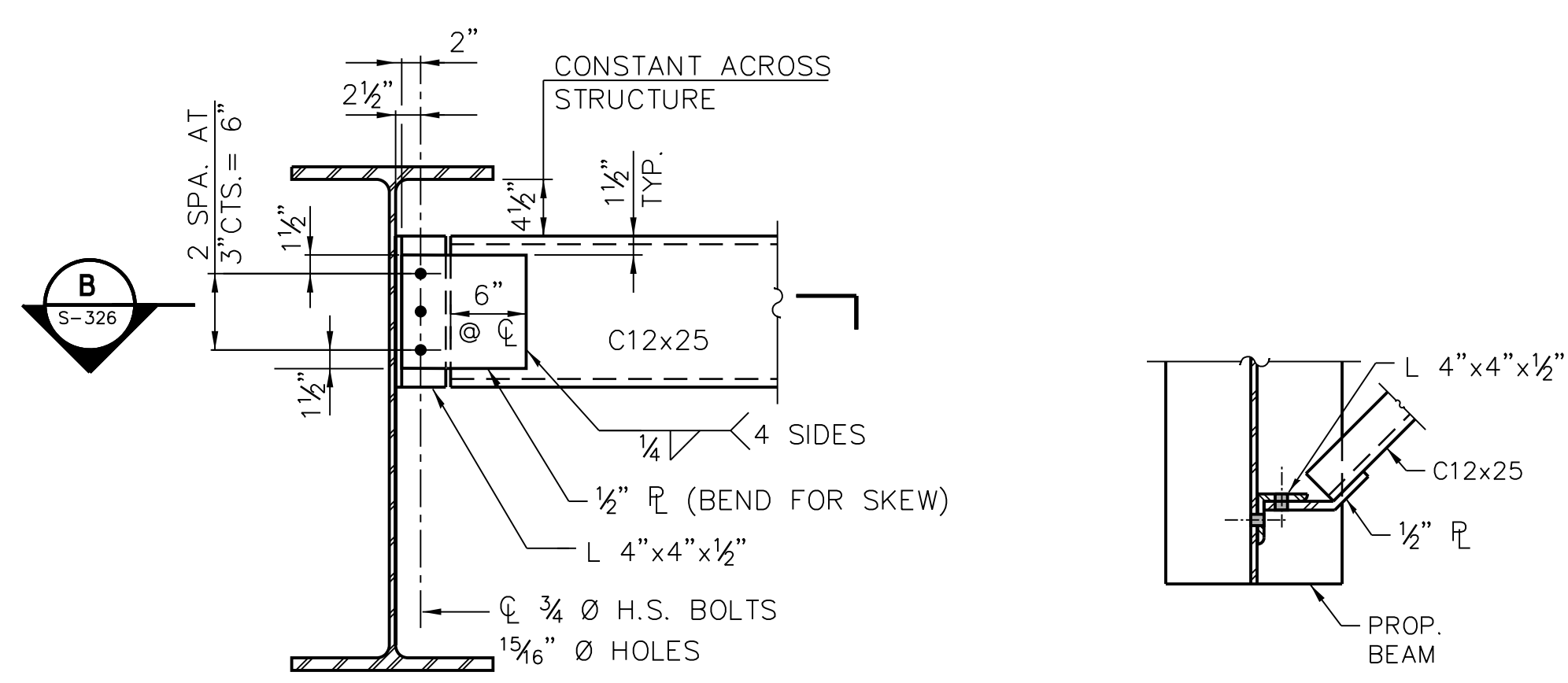
5 BOTTOM SPLICE PLAN



1 END DIAPHRAGM D3  
SEE SHEET NBB-S-327 FOR BEARING STIFFENER DETAILS



A SECTION A



B SECTION B

2 END DIAPHRAGM D4

NOTE:  
TWO HARDENED WASHERS REQUIRED FOR EACH SET OF OVERSIZED HOLES.  
ALTERNATE CHANNELS ARE PERMITTED TO FACILITATE MATERIAL ACQUISITION. THE ALTERNATIVE, IF UTILIZED, SHALL BE PROVIDED AT NO EXTRA COST TO THE AUTHORITY.  
WHERE NEW STRUCTURAL STEEL ELEMENTS ARE CONNECTED TO AN EXISTING STEEL MEMBER, ALL NEW FASTENERS SHALL BE HIGH STRENGTH BOLTS. HOLES SHALL BE SUBPUNCHED OR SUBDRILLED 1 1/16" DIA. AND REAMED IN THE FIELD TO 1 3/16" DIA. FOR 3/4" DIA. BOLTS, UNLESS OTHERWISE NOTED. HOLES SHALL BE SUBPUNCHED OR SUBDRILLED 1 3/16" DIA. AND REAMED IN THE FIELD TO 1 5/16" DIA. FOR 7/8" DIA. BOLTS, UNLESS NOTED OTHERWISE.

**NOT FOR CONSTRUCTION**



**CHICAGO TRANSIT AUTHORITY ENGINEERING**

**SENSITIVE SECURITY INFORMATION**  
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14 West 95th Street (North Terminal)  
Chicago, IL 60628

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	03/07/18	INTERMEDIATE DESIGN

LOCATION IDENTIFIER: **DR095**  
95TH ST. NORTH BUS BRIDGE  
STRUCTURAL STEEL DETAILS  
(SHEET 1 OF 4)

**NBB-S-326**

FILE NAME = 070\_DR095\_S-326.dgn  
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**cta** CHICAGO TRANSIT AUTHORITY ENGINEERING

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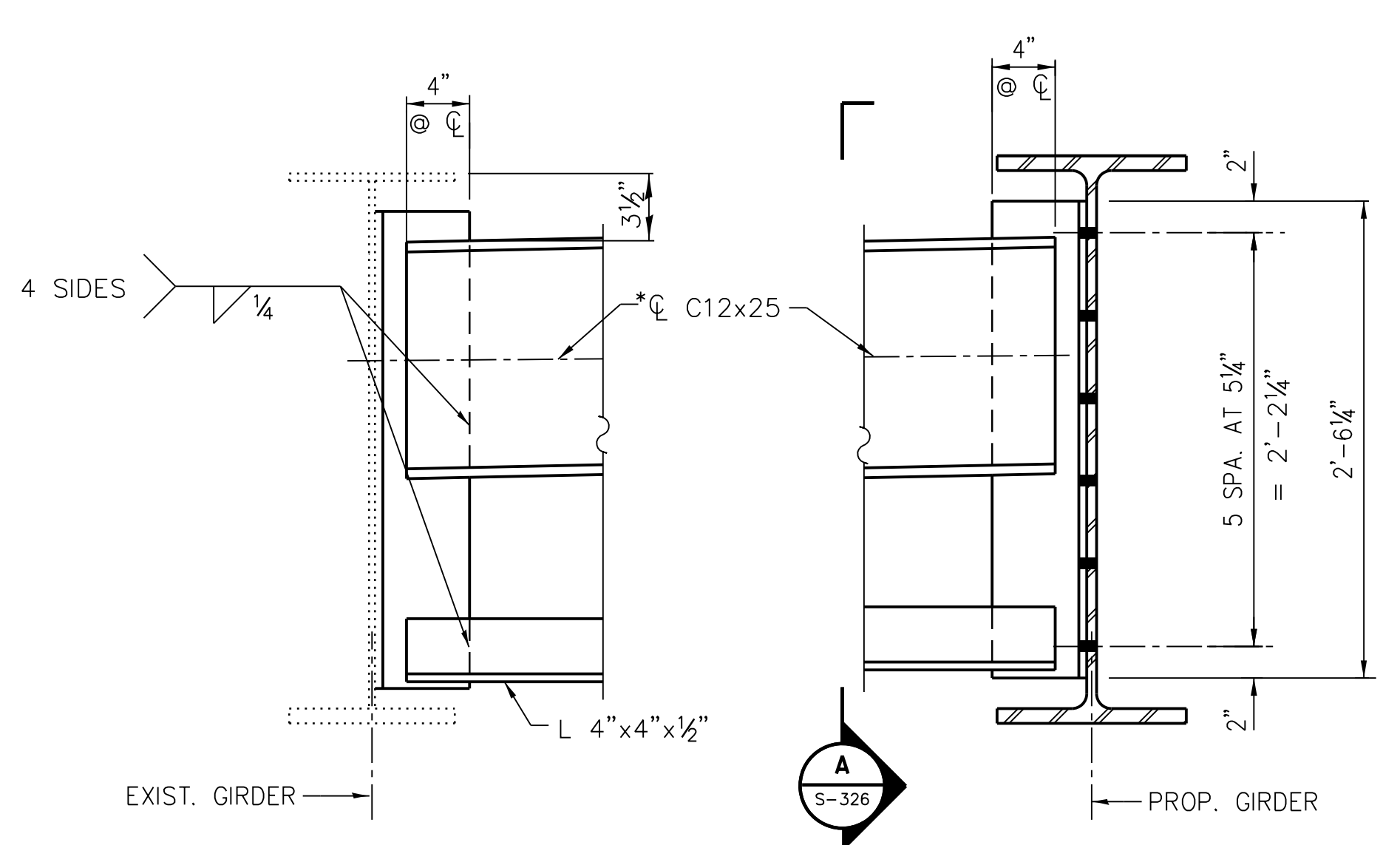
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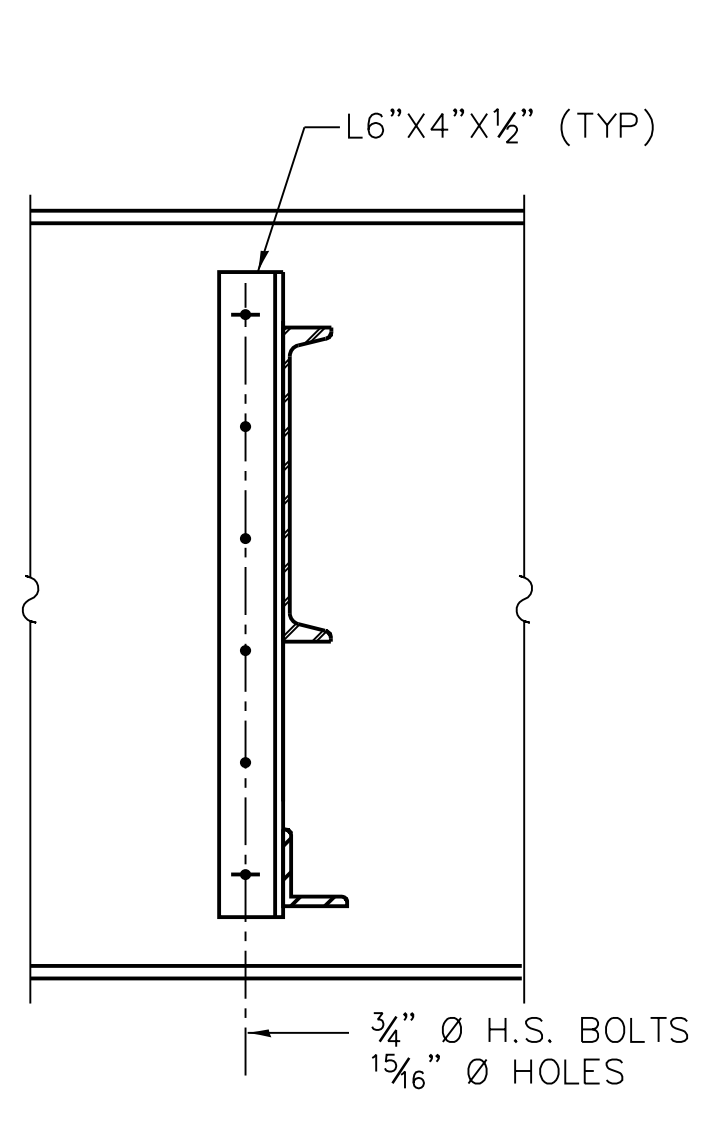
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LOCATION IDENTIFIER: **DR095**  
 95TH ST. NORTH BUS BRIDGE  
 STRUCTURAL STEEL DETAILS  
 (SHEET 2 OF 4)

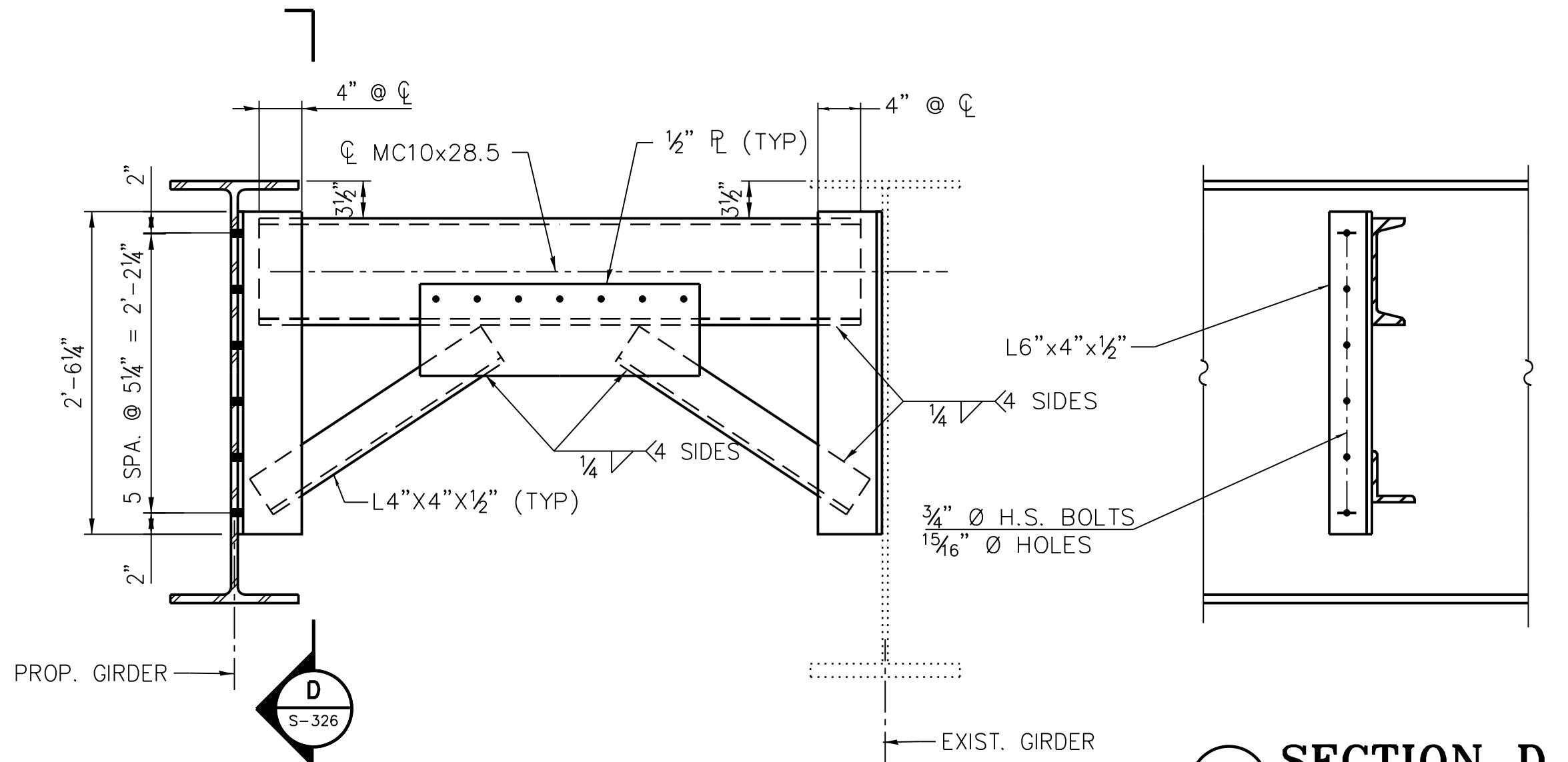
**NBB-S-326A**



**1 INTERIOR DIAPHRAGM D1**  
 \*C12x30 AS ALTERNATE

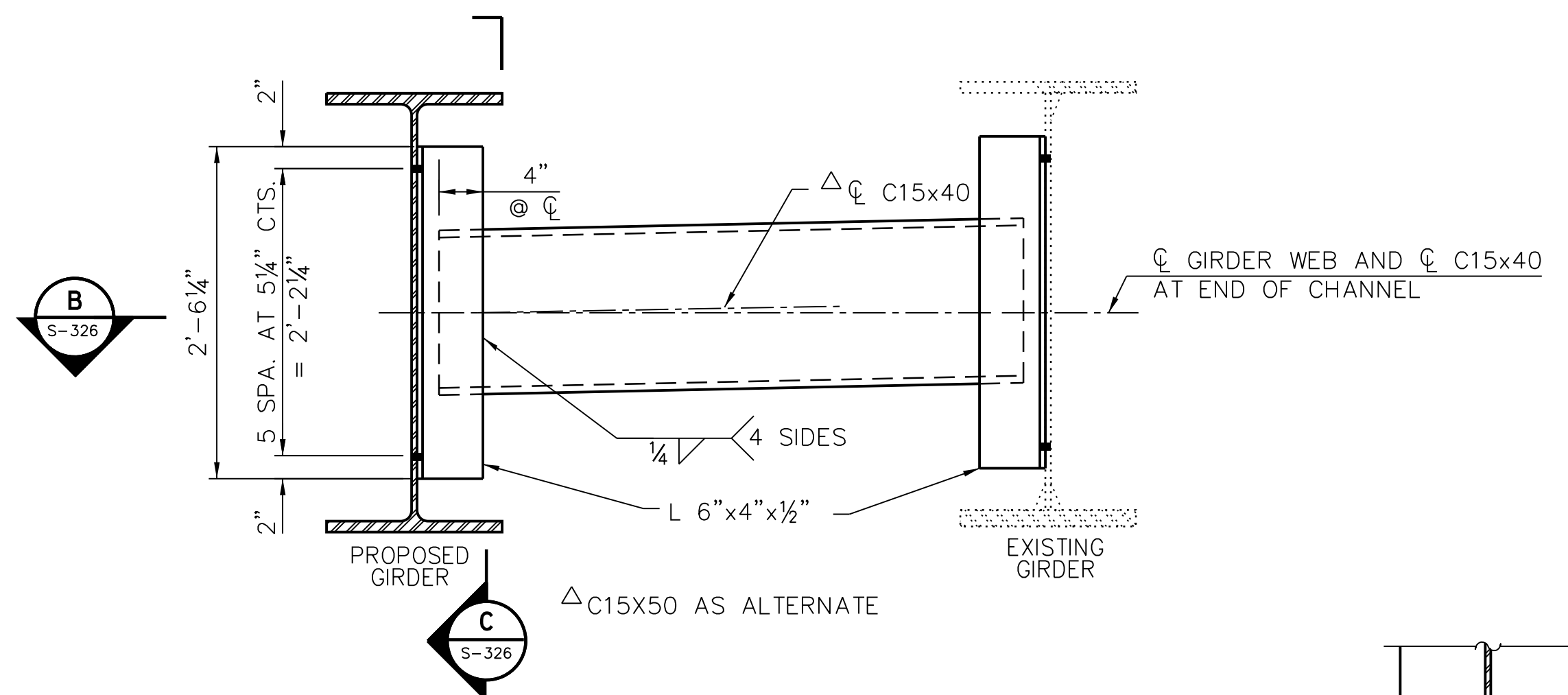


**A SECTION A**

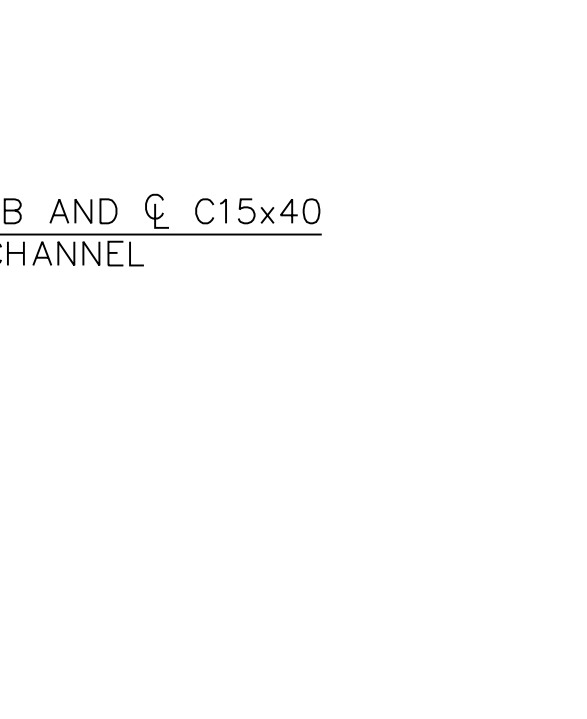


**3 INTERIOR DIAPHRAGM D5**

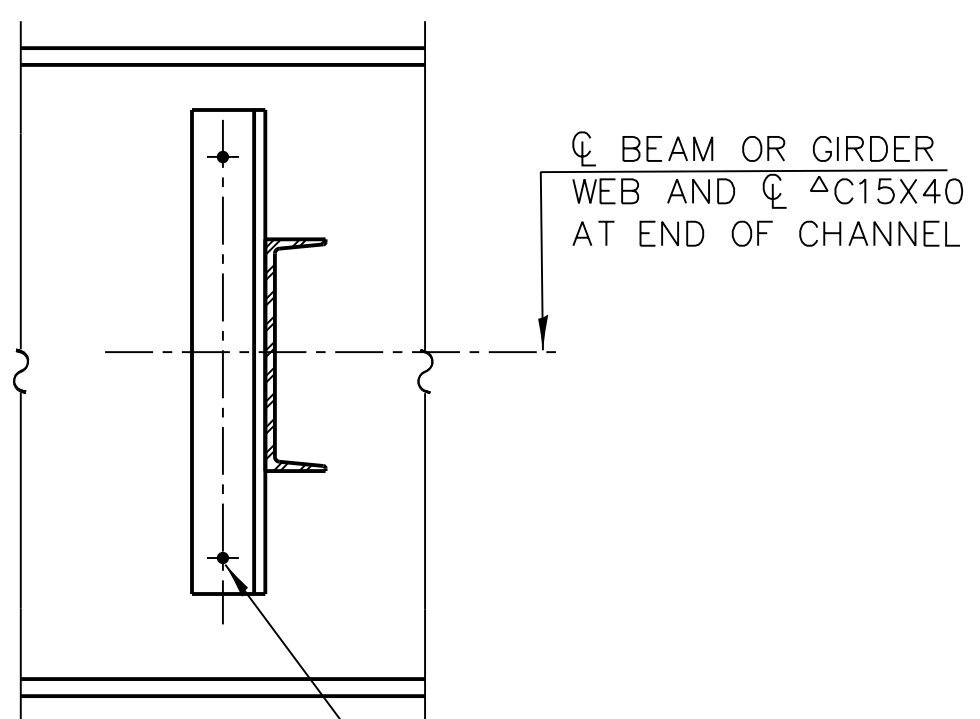
**D SECTION D**



**2 INTERIOR DIAPHRAGM D2**  
 Δ C15x50 AS ALTERNATE



**B SECTION B**



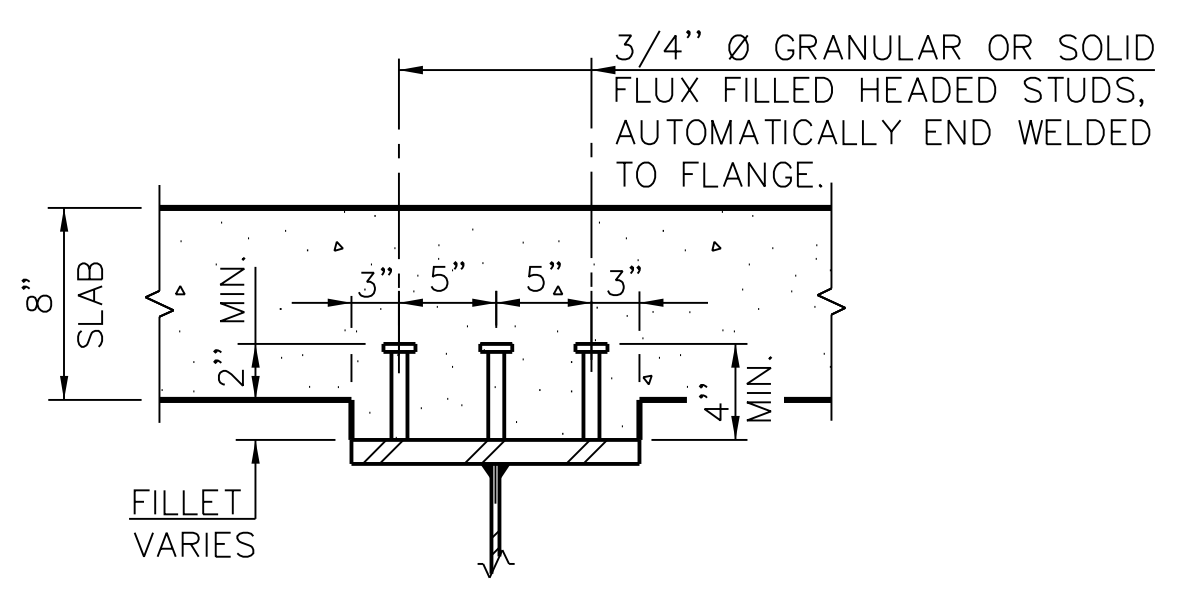
**C SECTION C**

NOTE:  
 FOR DIAPHRAGM DETAIL SUPPORTING CANOPY COLUMN, D6, SEE S-1300 SERIES DRAWINGS.

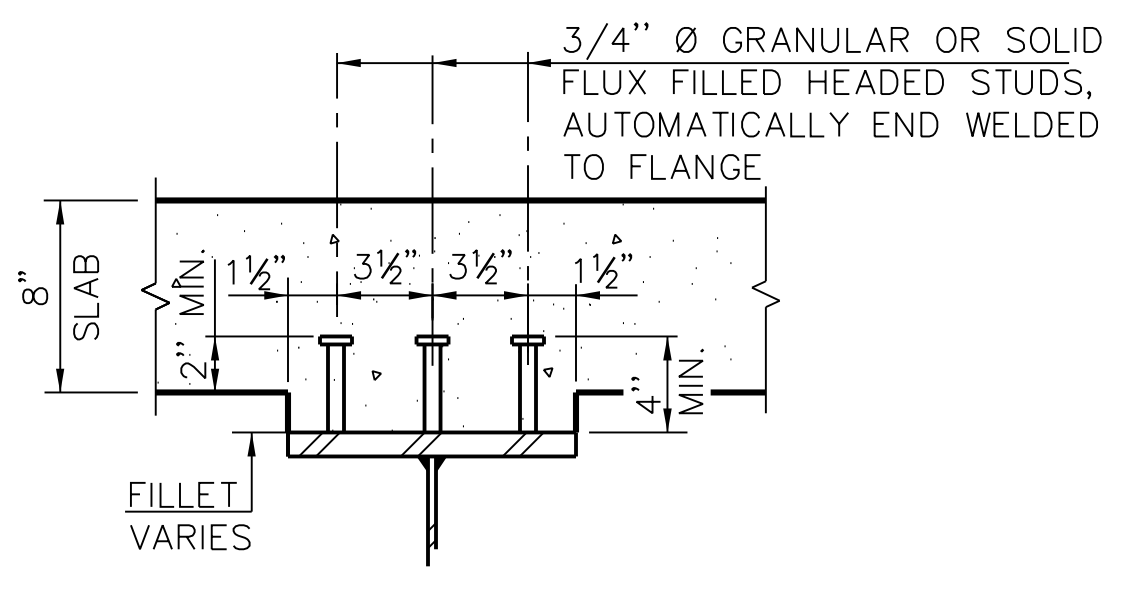
NOTE:  
 1) TWO HARDENED WASHERS REQUIRED FOR EACH SET OF OVERSIZED HOLES.  
 2) ALTERNATE CHANNELS ARE PERMITTED TO FACILITATE MATERIAL ACQUISITION. THE ALTERNATIVE, IF UTILIZED, SHALL BE PROVIDED AT NO EXTRA COST TO THE AUTHORITY.  
 3) WHERE NEW STRUCTURAL STEEL ELEMENTS ARE CONNECTED TO AN EXISTING STEEL MEMBER, ALL NEW FASTENERS SHALL BE HIGH STRENGTH BOLTS. HOLES SHALL BE SUBPUNCHED OR SUBDRILLED 1/16" DIA. AND REAMED IN THE FIELD TO 3/16" DIA. FOR 3/4" DIA. BOLTS, UNLESS OTHERWISE NOTED. HOLES SHALL BE SUBPUNCHED OR SUBDRILLED 1/16" DIA. AND REAMED IN THE FIELD TO 5/16" DIA. FOR 7/8" DIA. BOLTS, UNLESS NOTED OTHERWISE.

FILE NAME = 070\_DR095\_S-326A.dgn  
 PLOT DATE = 5/24/2018

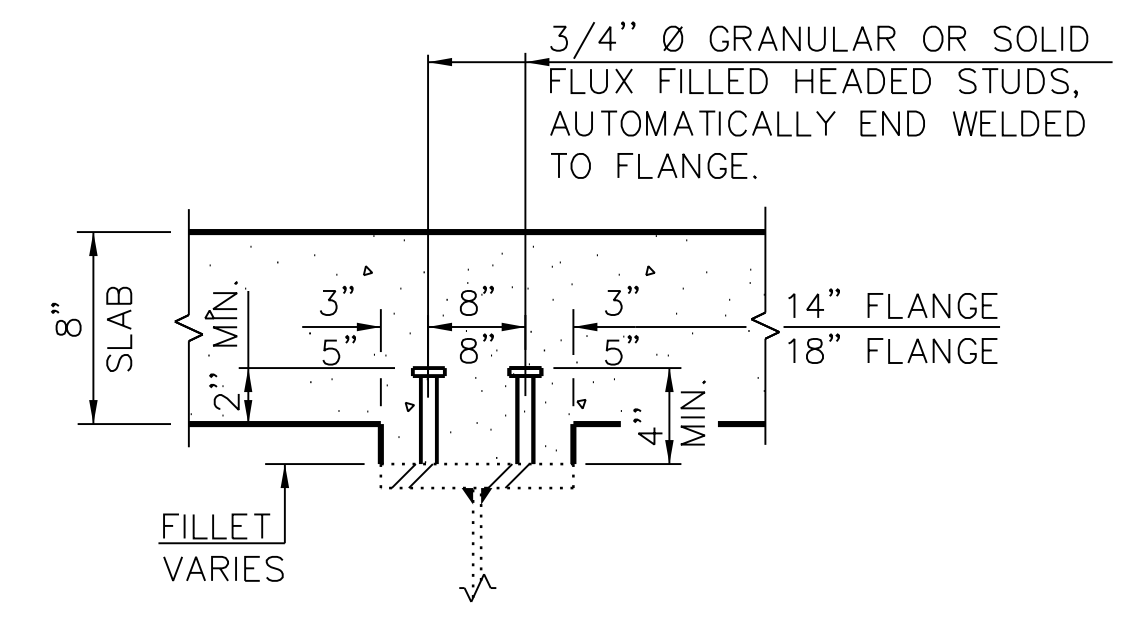
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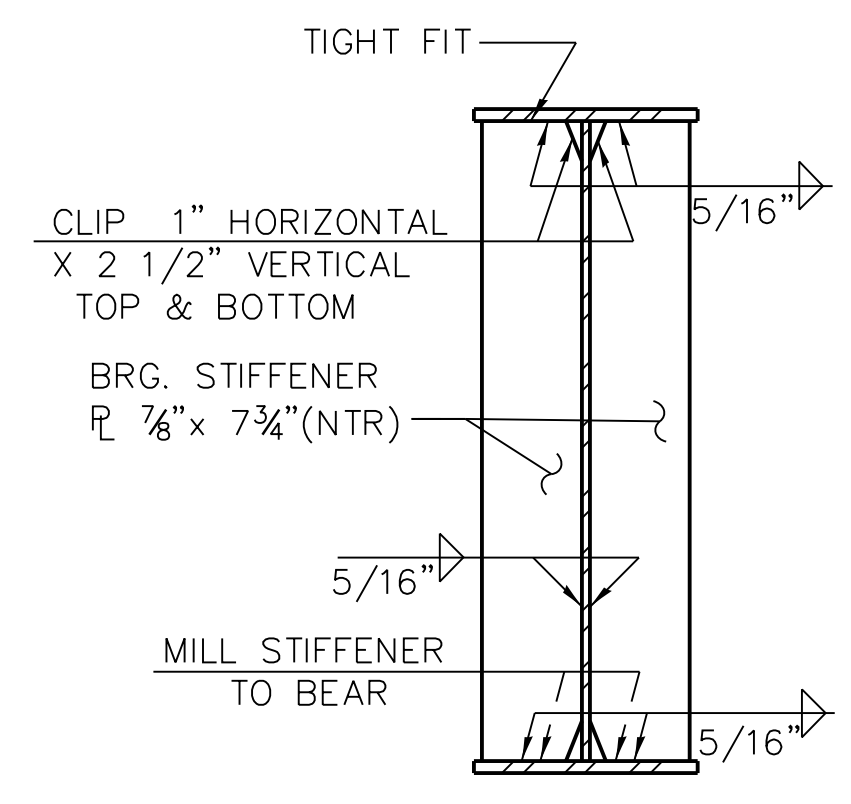
**A SECTION**  
S-323



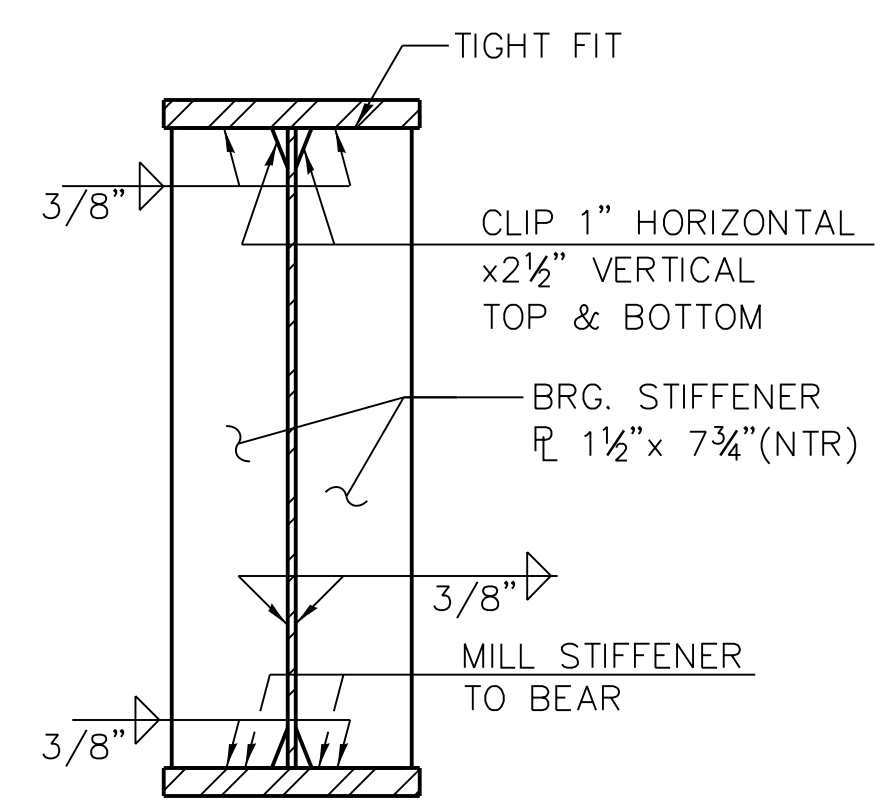
**B SECTION**  
S-323



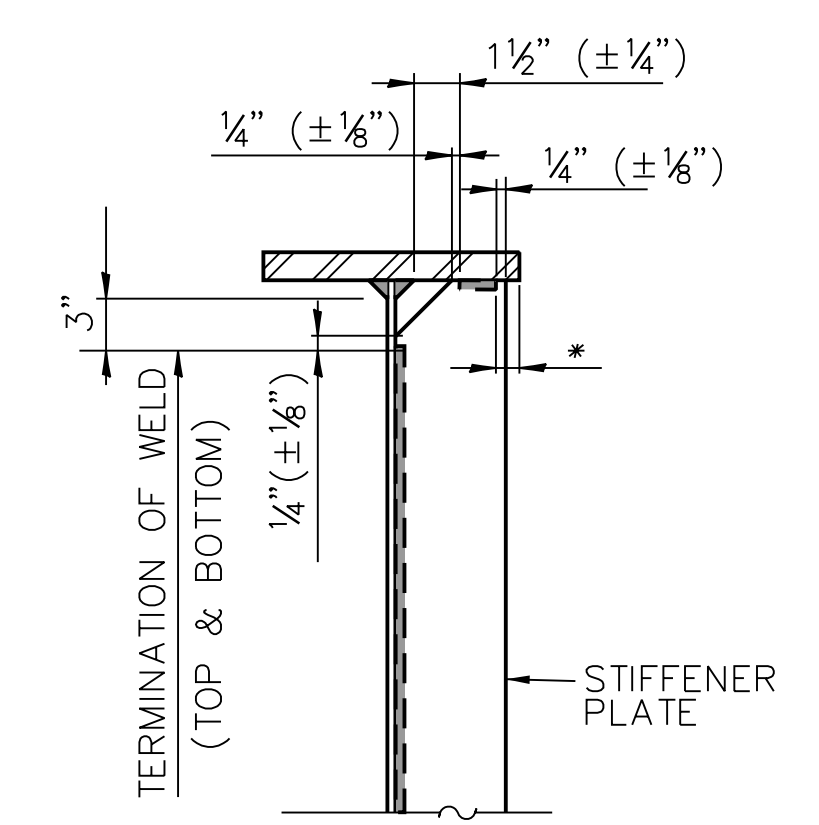
**C SECTION**  
S-324



**2 SECTION AT ABUT.**

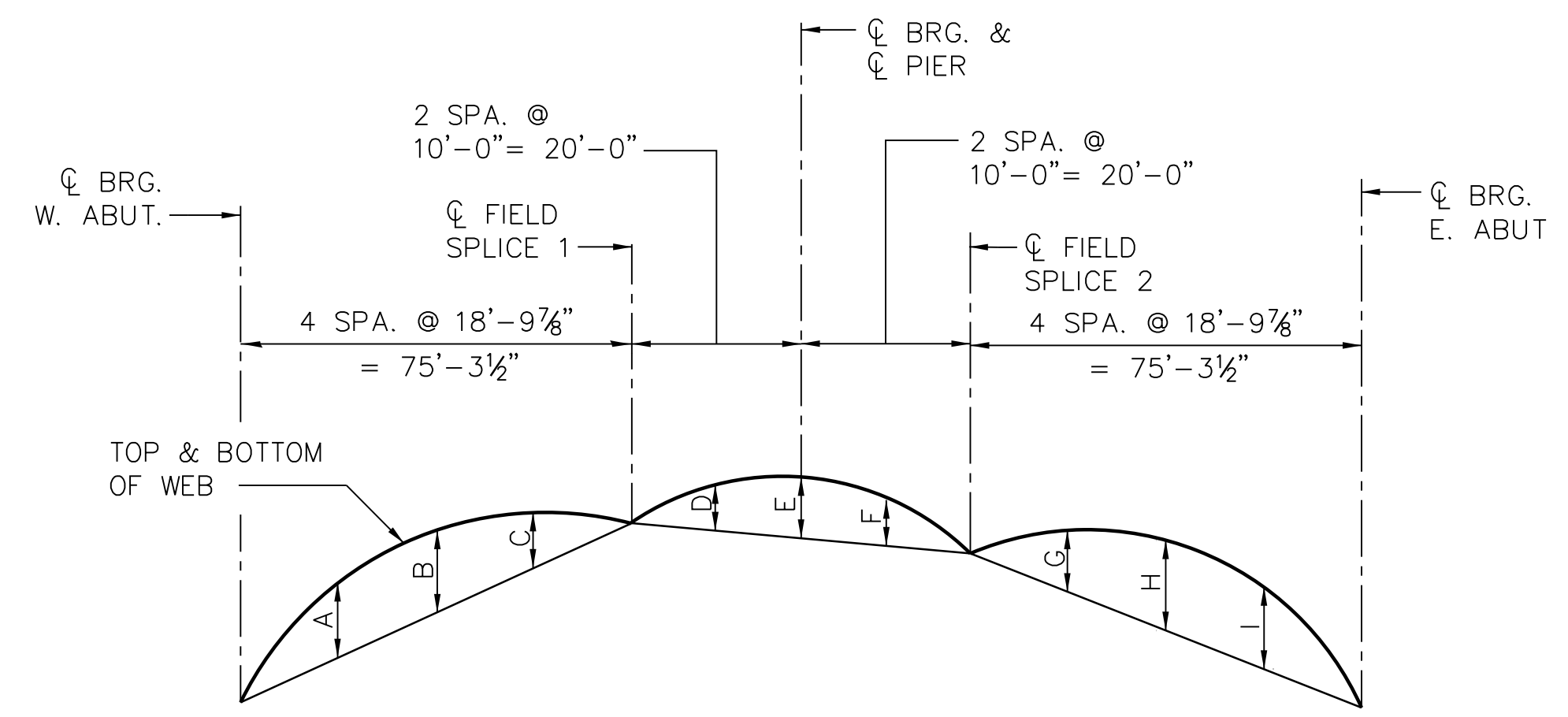


**3 SECTION AT PIER**



**WELD TERMINATION DETAIL**

\* IF STIFFENER PLATE EXTENDS BEYOND FLANGE EDGE, THE WELD SHALL BE TERMINATED 1/4" (± 1/8") FROM THE EDGE.



**4 CAMBER DIAGRAM**  
(GIRDERS 1 THRU 3)

**CAMBER VALUES**

GIRDER NO.	A	B	C	D	E	F	G	H	I
1	3/4"	3/2"	3"	7/8"	1"	7/8"	2 1/2"	2 1/2"	2"
2,3	3/8"	4 1/2"	3 1/2"	3/4"	3/4"	3/4"	3"	3 3/8"	2 3/4"

**TOP OF WEB ELEVATIONS**  
(FOR FABRICATION ONLY)

GIRDER NO.	CL BRG. W. ABUT.	FIELD SPLICE 1	CL BRG. PIER	FIELD SPLICE 2	CL BRG. E. ABUT.
1	16.871	17.564	17.662	17.541	16.943
2	16.904	17.597	17.695	17.573	16.976
3	16.844	17.537	17.635	17.513	16.916

**TOP OF BEAM ELEVATIONS**  
(FOR FABRICATION ONLY)

BEAM NO.	CL BRG. W. ABUT.	END	BEAM NO.	START	CL BRG. E. ABUT.
4	16.870	16.993	6	17.032	16.947
5	16.792	17.191	7	17.179	16.875

**NOT FOR CONSTRUCTION**



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**95th Terminal Improvement**  
14 West 95th Street (North Terminal)  
Chicago, IL 60628

IN CHARGE	S. KADERBEK
APPROVED BY	D.ZROKA
CHECKED BY	P.MAID
DESIGNED BY	L.SOMMER
DRAWN BY	T.SLEDGE
PROJECT NO.	2012-0021
FILE NAME	File Name

MARK	DATE	DESCRIPTION
	05/23/18	FINAL DESIGN
	03/07/18	INTERMEDIATE DESIGN

LOCATION IDENTIFIER: **DR095**  
95TH ST. NORTH BUS BRIDGE  
STRUCTURAL STEEL DETAILS  
(SHEET 3 OF 4)

**NBB-S-327**

FILE NAME = 070\_DR095\_S-327.dgn  
PLOT DATE = 5/24/2018

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**CHICAGO TRANSIT AUTHORITY ENGINEERING**

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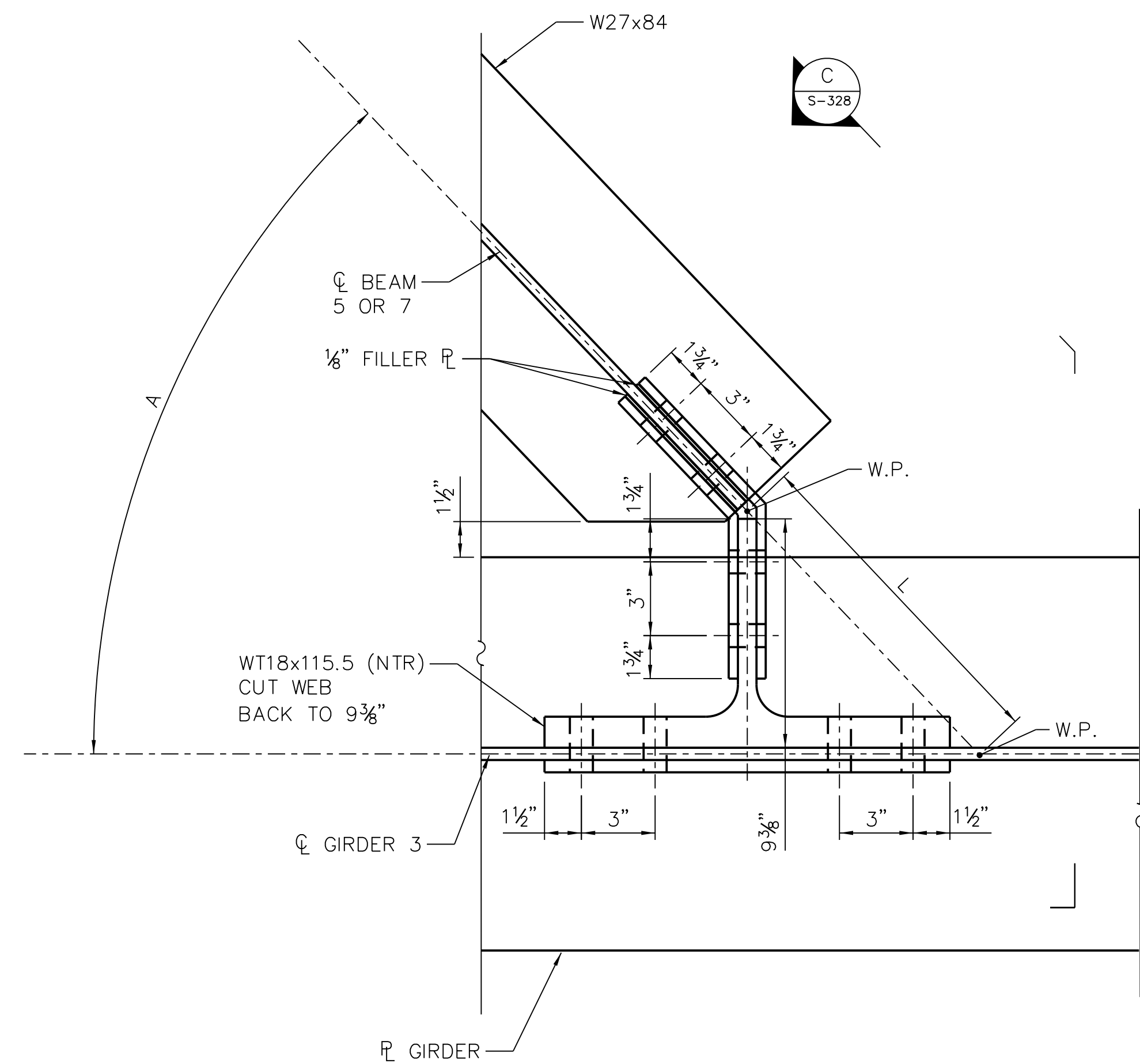
IN CHARGE	S. KADERBEK
APPROVED BY	D.ZROKA
CHECKED BY	P.MAID
DESIGNED BY	L.SOMMER
DRAWN BY	T.SLEDGE
PROJECT NO.	2012-0021
FILE NAME	File Name

MARK	DATE	DESCRIPTION
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	03/07/18	INTERMEDIATE DESIGN

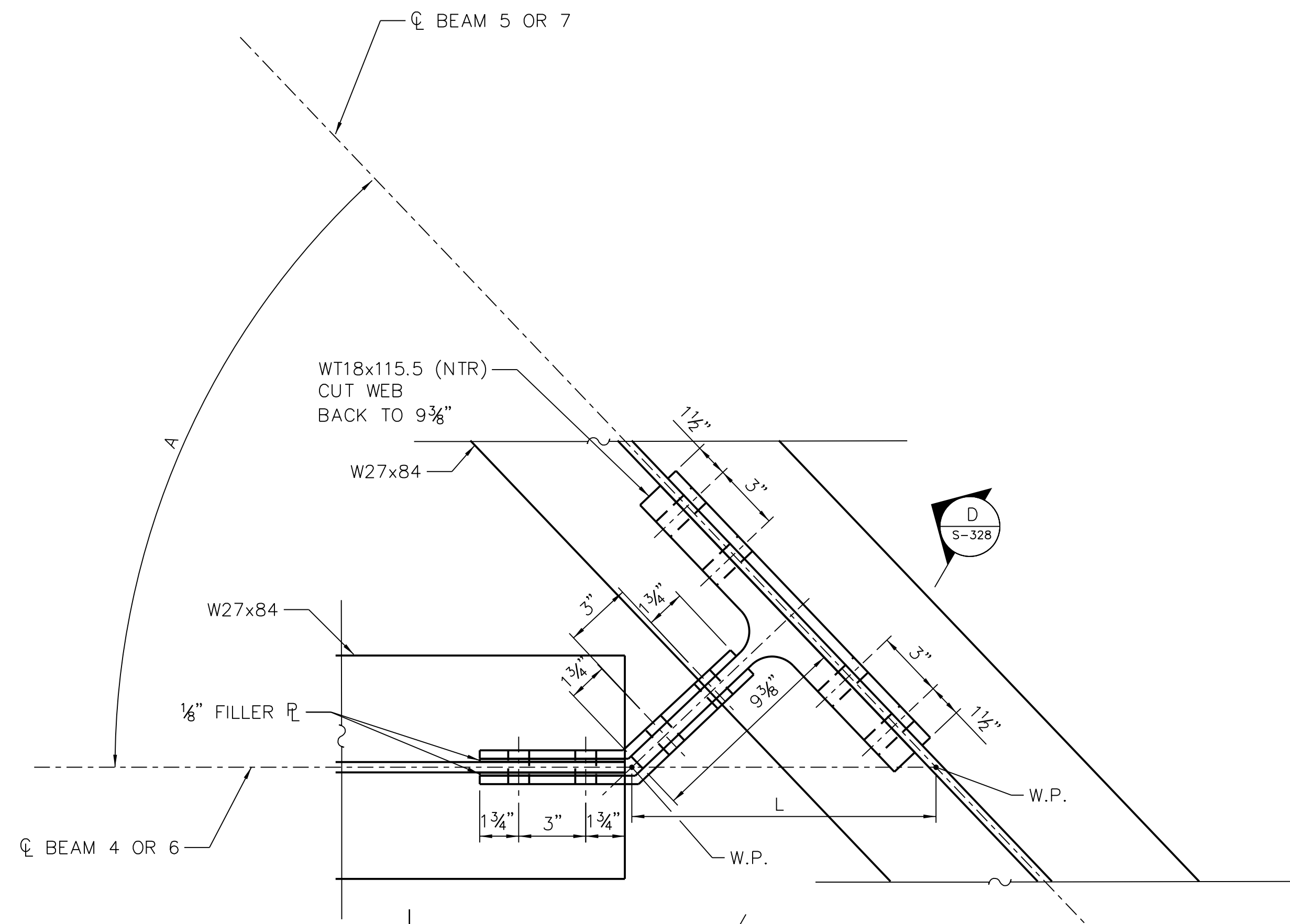
LOCATION IDENTIFIER: **DR095**

95TH ST. NORTH BUS BRIDGE  
STRUCTURAL STEEL DETAILS  
(SHEET 4 OF 4)

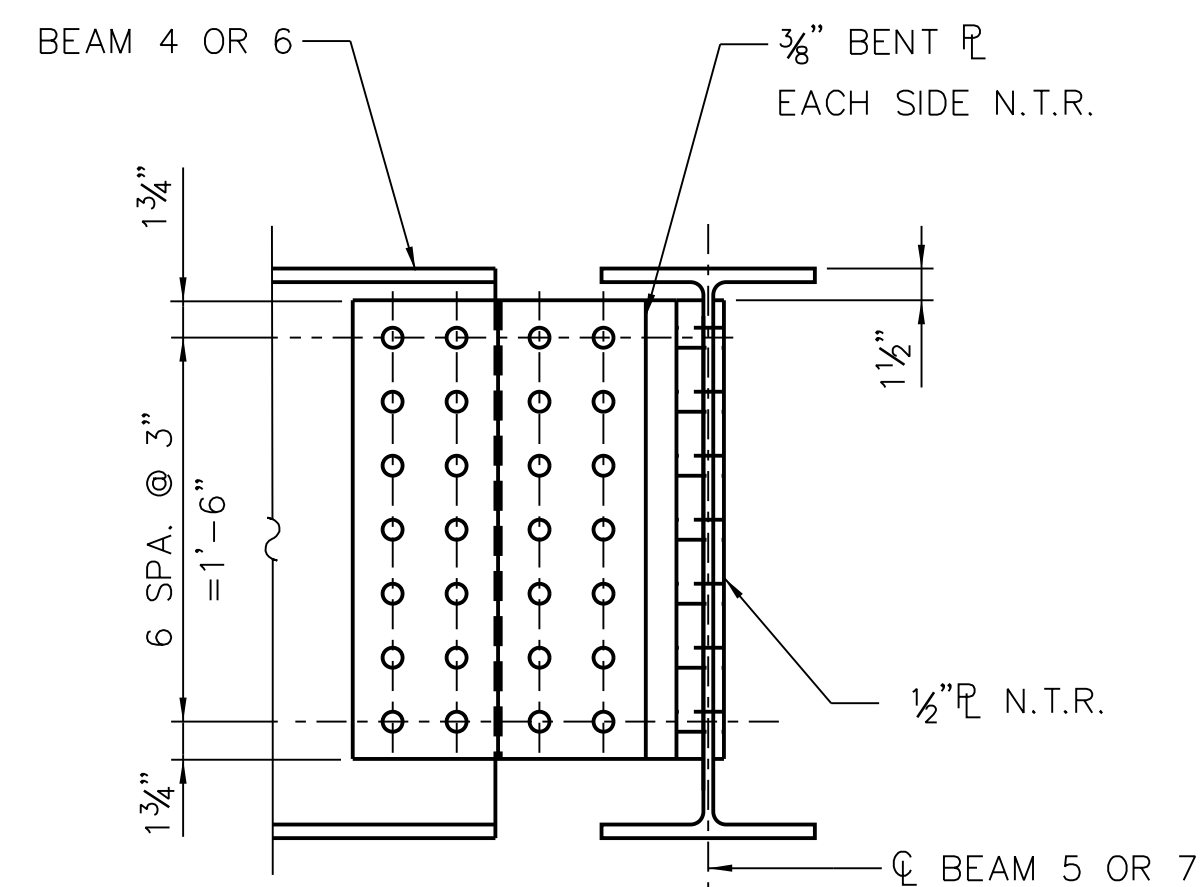
**NBB-S-328**



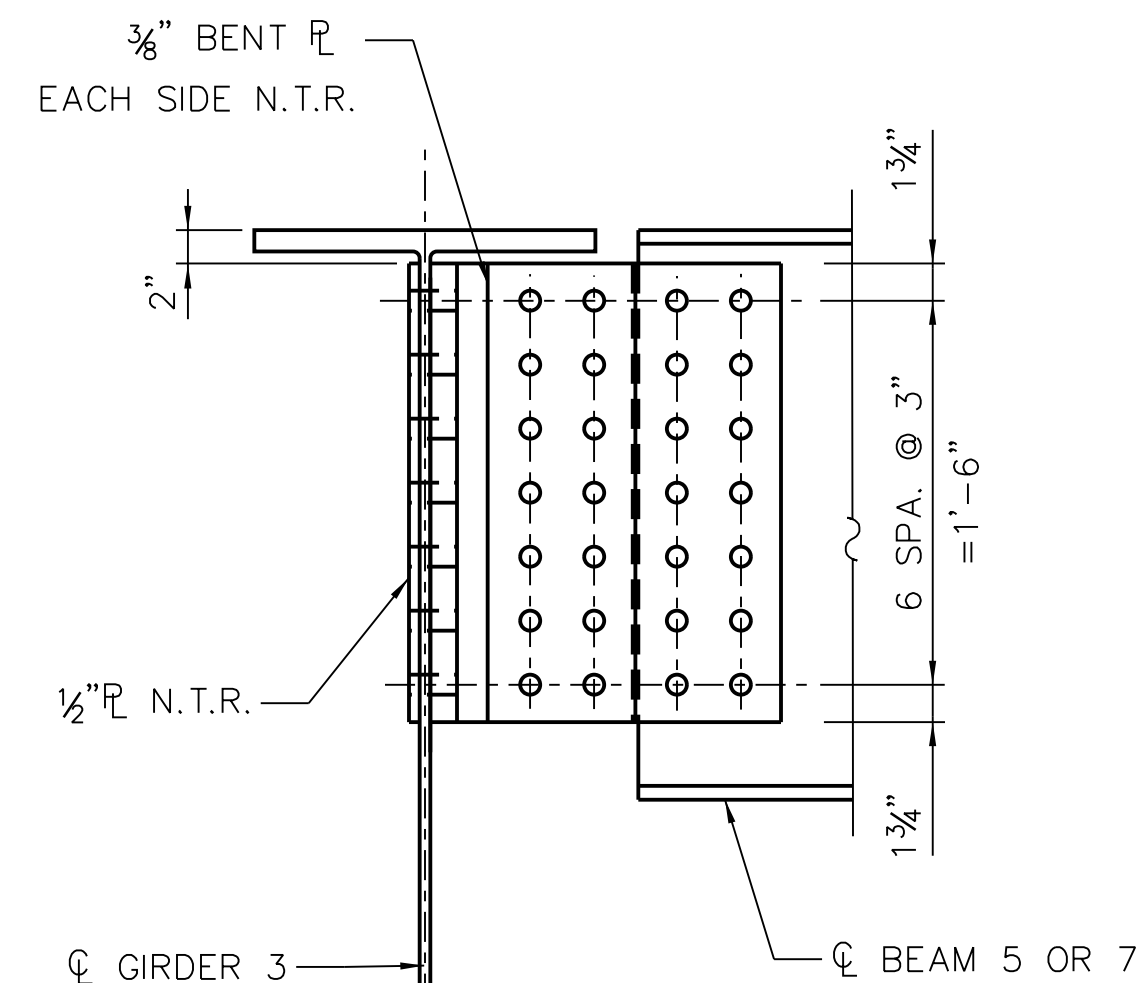
**A** DETAIL  
S-328



**B** DETAIL  
S-328



**D** DETAIL  
S-328



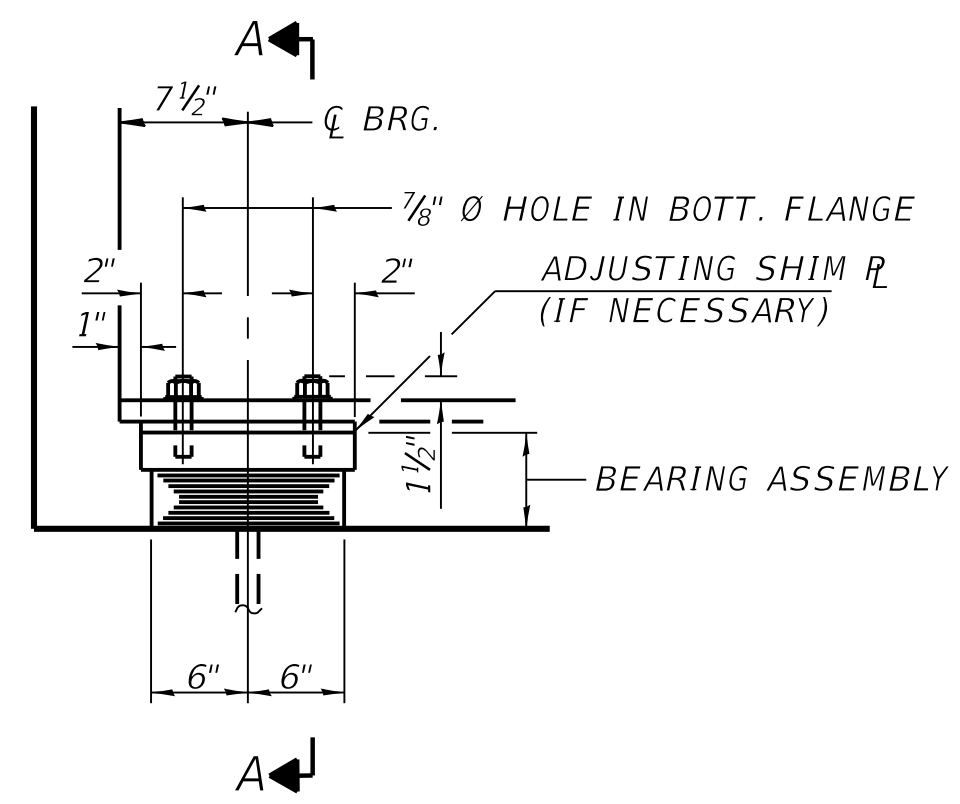
**C** DETAIL  
S-328

GIRDER	BEAM	BEAM	ANGLE A (DEGREES)	L
3	5	-	44°59'48"	1'-1 5/8"
-	5	4	44°59'48"	1'-1 5/8"
3	7	-	45°00'12"	1'-1 5/8"
-	7	6	45°00'12"	1'-1 5/8"

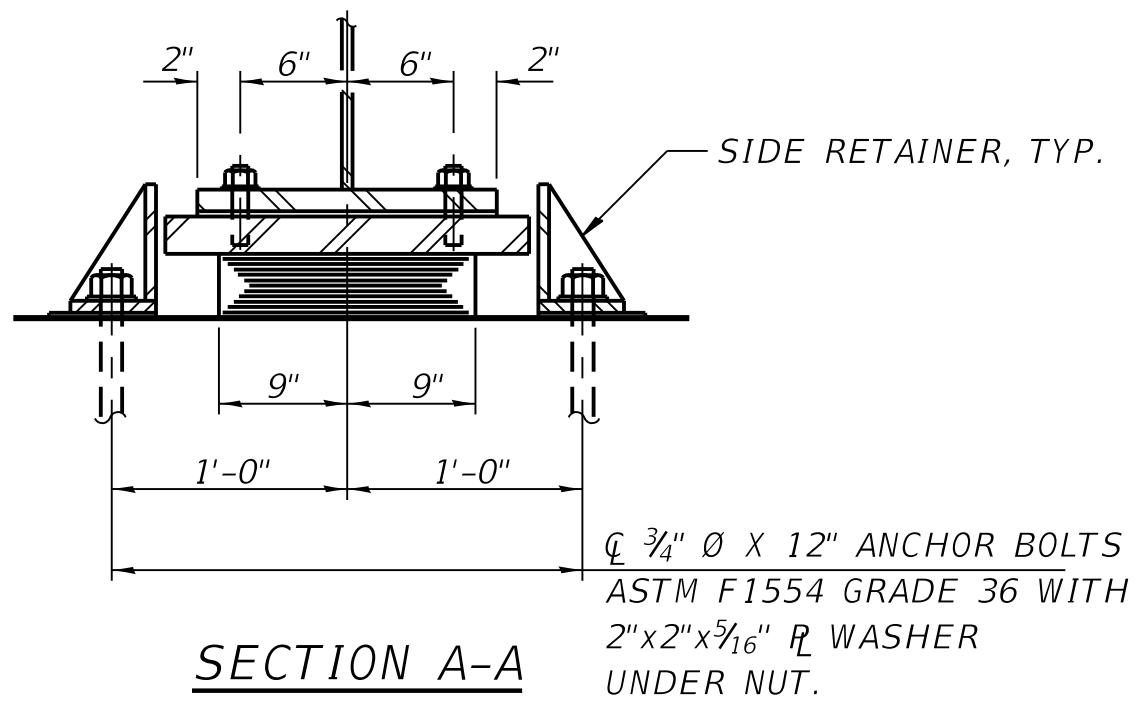
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PLOT DATE = 5/24/2018

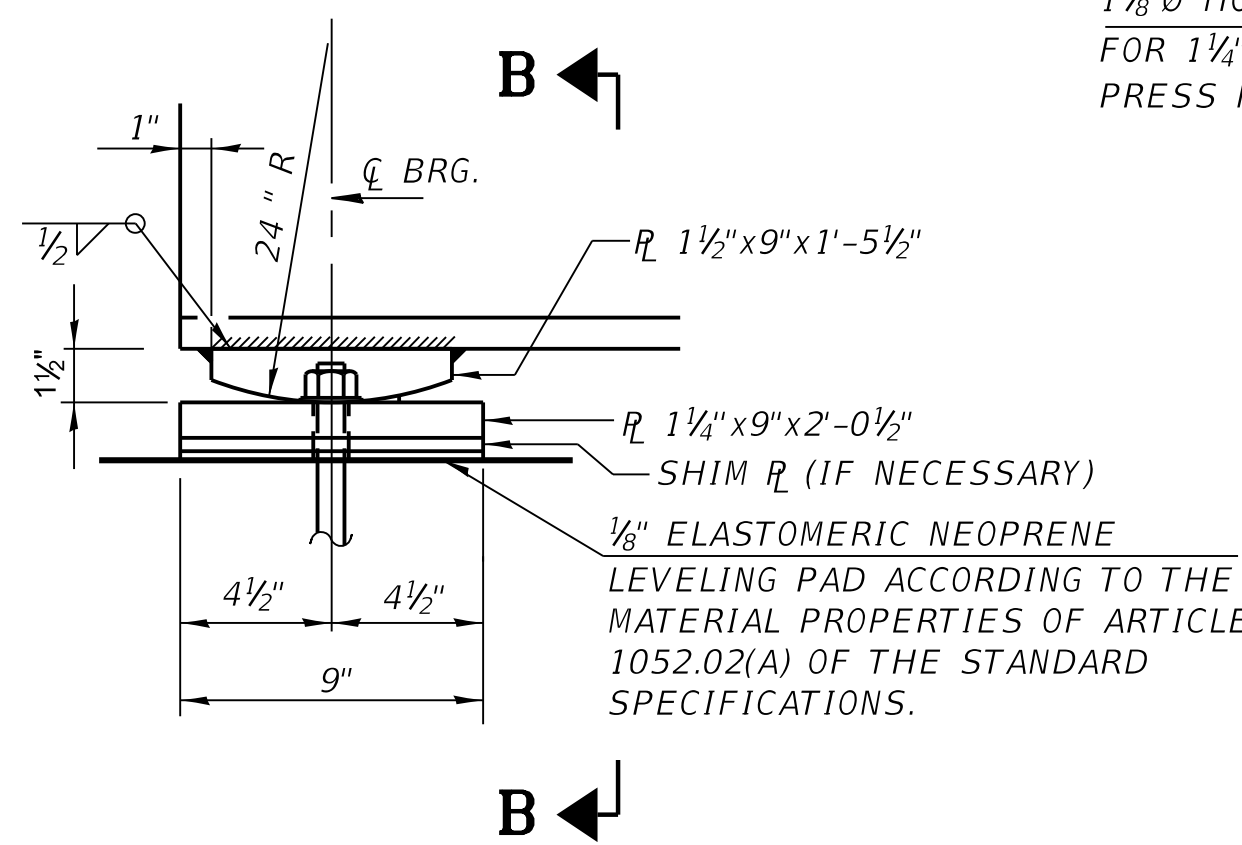
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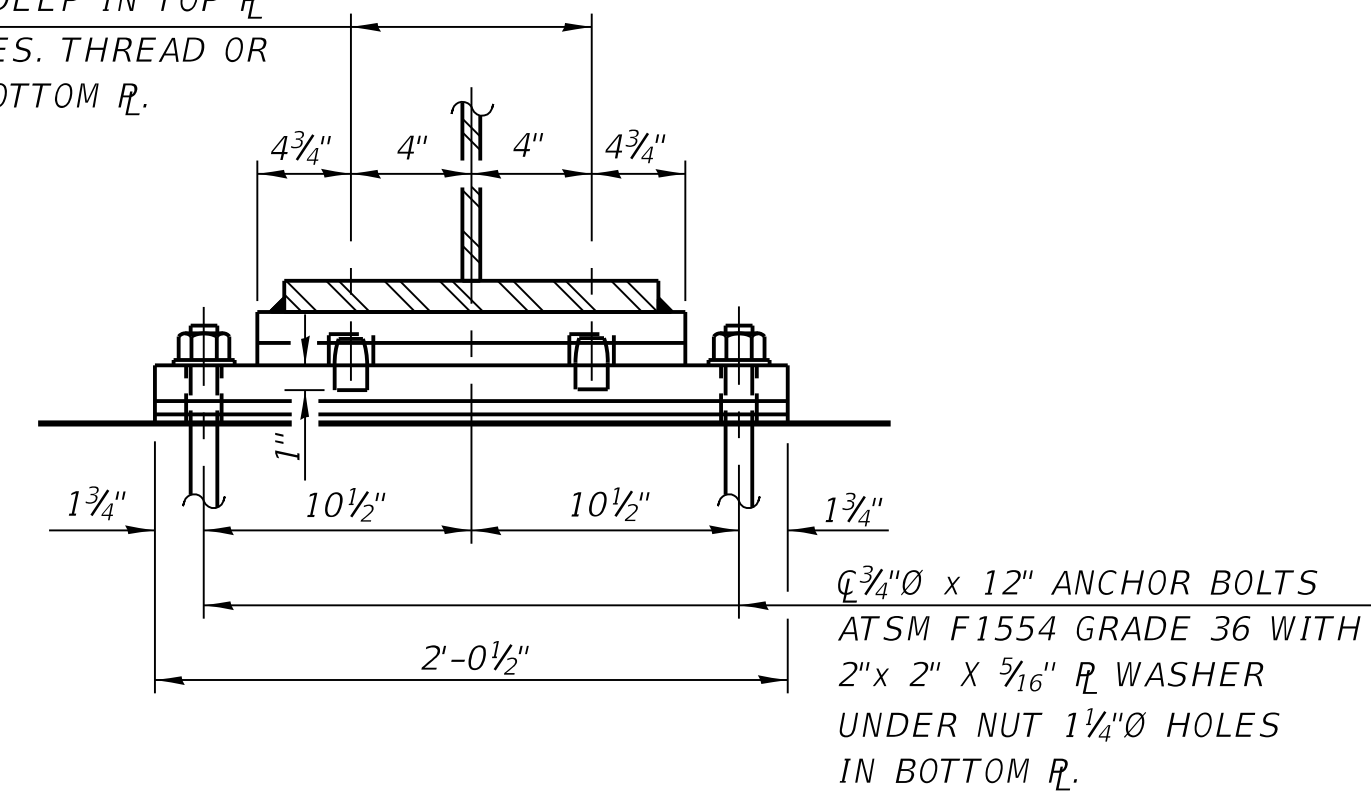
ELEVATION AT WEST ABUTMENT



SECTION A-A



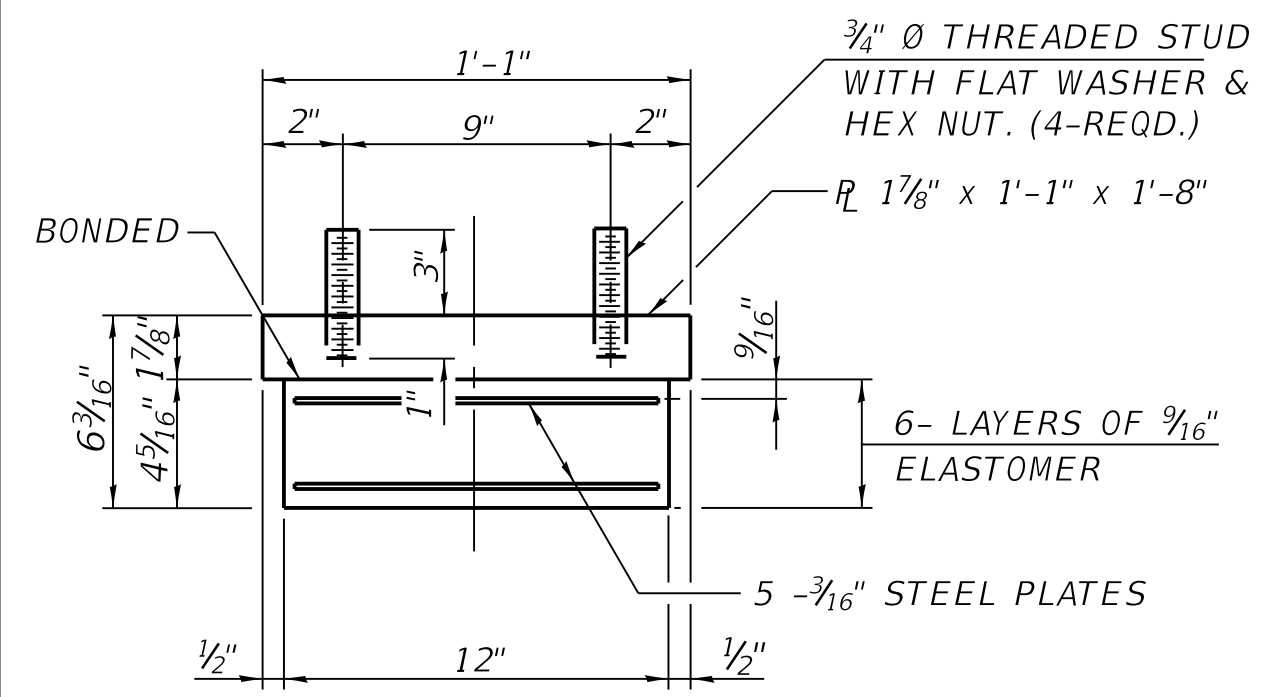
ELEVATION AT EAST ABUTMENT



SECTION B-B

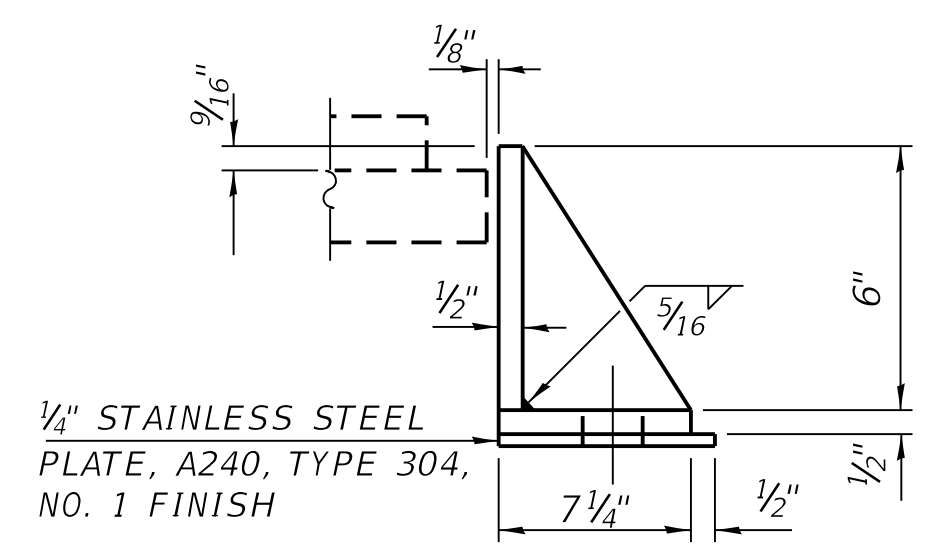
**TYPE I ELASTOMERIC EXPANSION BEARING**  
AT WEST ABUTMENT-PROPOSED GIRDER 2 & GIRDER 3

**FIXED BEARING AT EAST ABUTMENT**  
PROPOSED GIRDERS 1 THRU 3



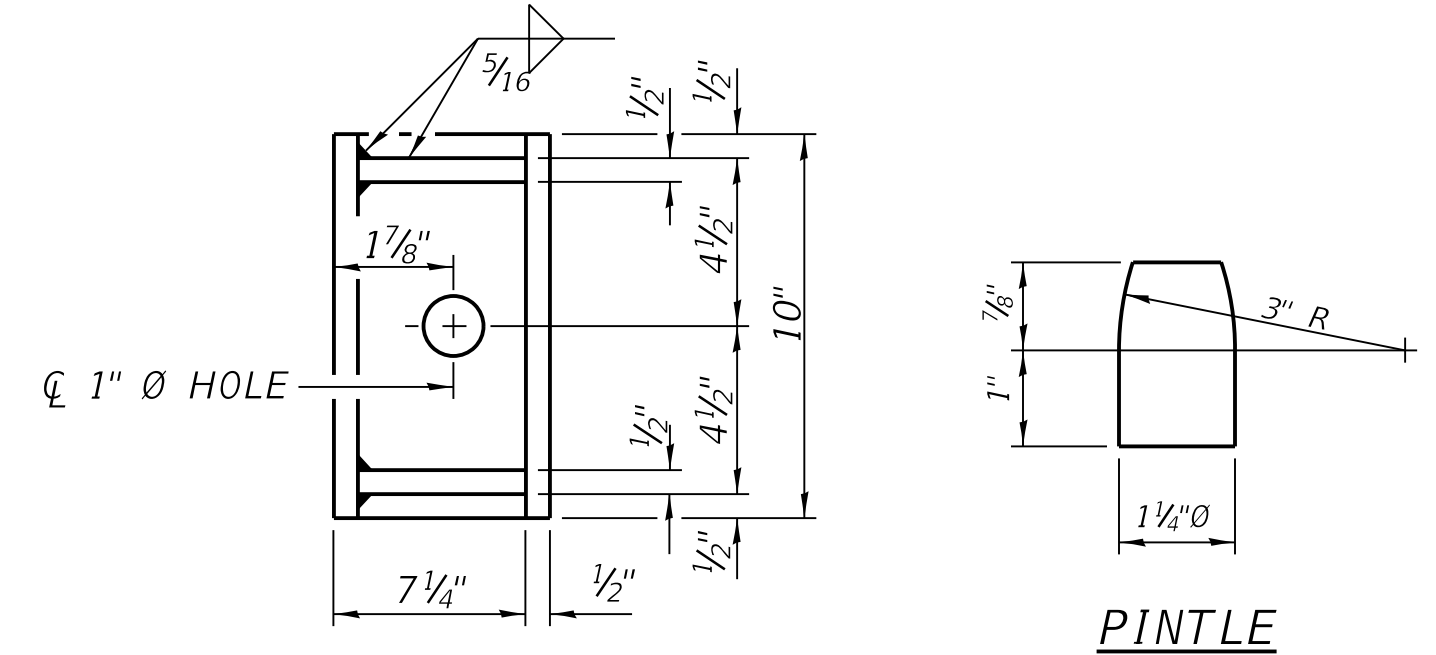
BEARING ASSEMBLY

NOTE:  
SHIM PLATES SHALL NOT BE PLACED UNDER BEARING ASSEMBLY.



SIDE RETAINER

EQUIVALENT ROLLED ANGLE WITH STIFFENERS WILL BE ALLOWED IN LIEU OF WELDED PLATES.



PINTLE

NOTES:

- ANCHOR BOLTS SHALL BE ASTM F1554 ALL-THREAD (OR AN ENGINEER-APPROVED ALTERNATE MATERIAL) OF THE GRADE(S) AND DIAMETER(S) SPECIFIED. THE CORRESPONDING SPECIFIED GRADE OF AASHTO M314 ANCHOR BOLTS MAY BE USED IN LIEU OF ASTM F1554.
- ANCHOR BOLTS AT FIXED BEARINGS MAY BE EITHER CAST IN PLACE OR INSTALLED IN HOLES DRILLED AFTER THE SUPPORTED MEMBER IS IN PLACE.
- ANCHOR BOLTS FOR TYPE II BEARINGS SHALL BE PLACED IN HOLES DRILLED IN THE CONCRETE THROUGH HOLES IN THE BOTTOM BEARING PLATE AFTER MEMBERS ARE IN PLACE. SIDE RETAINERS SHALL BE PLACED AFTER BOLTS ARE INSTALLED.
- DRILLED AND SET ANCHOR BOLTS SHALL BE INSTALLED ACCORDING TO ARTICLE 521.06 OF THE IDOT STANDARD SPECIFICATIONS.
- THE 1/8" PTFE SHEET SHALL BE BONDED DIRECTLY TO THE TOP STEEL PLATE WITH A TWO-COMPONENT, MEDIUM VISCOSITY EPOXY RESIN, CONFORMING TO THE REQUIREMENTS OF THE FEDERAL SPECIFICATION MMM-A-134, TYPE I. THE BOND AGENT SHALL BE APPLIED ON THE FULL AREA OF THE CONTACT SURFACES.
- BONDING OF 1/8" PTFE SHEET DURING VULCANIZING PROCESS WILL BE PERMITTED PROVIDED THE PROCESS AND METHOD OF ADJUSTING ASSEMBLY HEIGHT IS APPROVED BY THE ENGINEER.
- TWO 1/8" ADJUSTING SHIMS SHALL BE PROVIDED FOR EACH BEARING IN ADDITION TO ALL OTHER PLATES OR SHIMS AND PLACED AS SHOWN ON BEARINGS DETAILS.
- PRIOR TO ORDERING ANY MATERIAL, THE CONTRACTOR SHALL VERIFY IN THE FIELD ALL THE BEARING HEIGHT AND SHIM THICKNESS DIMENSIONS.
- ALL BEARING PLATES, SIDE RETAINERS, ANCHOR BOLTS, NUTS, WASHERS AND PINTLES SHALL BE GALVANIZED ACCORDING TO AASHTO M111 OR M232 AS APPLICABLE.
- ANCHOR BOLTS AND SIDE RETAINERS AT ALL SUPPORTS SHALL BE INSTALLED AS EACH MEMBER IS ERECTED UNLESS AN EQUIVALENT TEMPORARY MEANS OF LATERAL RESTRAINT IS USED.

**NOT FOR CONSTRUCTION**



**CHICAGO TRANSIT AUTHORITY ENGINEERING**

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Chicago, IL 60628

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APPROVED BY	D.ZROKA
CHECKED BY	P.MAID
DESIGNED BY	L.SOMMER
DRAWN BY	T.SLEDGE
PROJECT NO.	2012-0021
FILE NAME	File Name

MARK	DATE	DESCRIPTION
	05/23/18	FINAL DESIGN
	03/07/18	INTERMEDIATE DESIGN

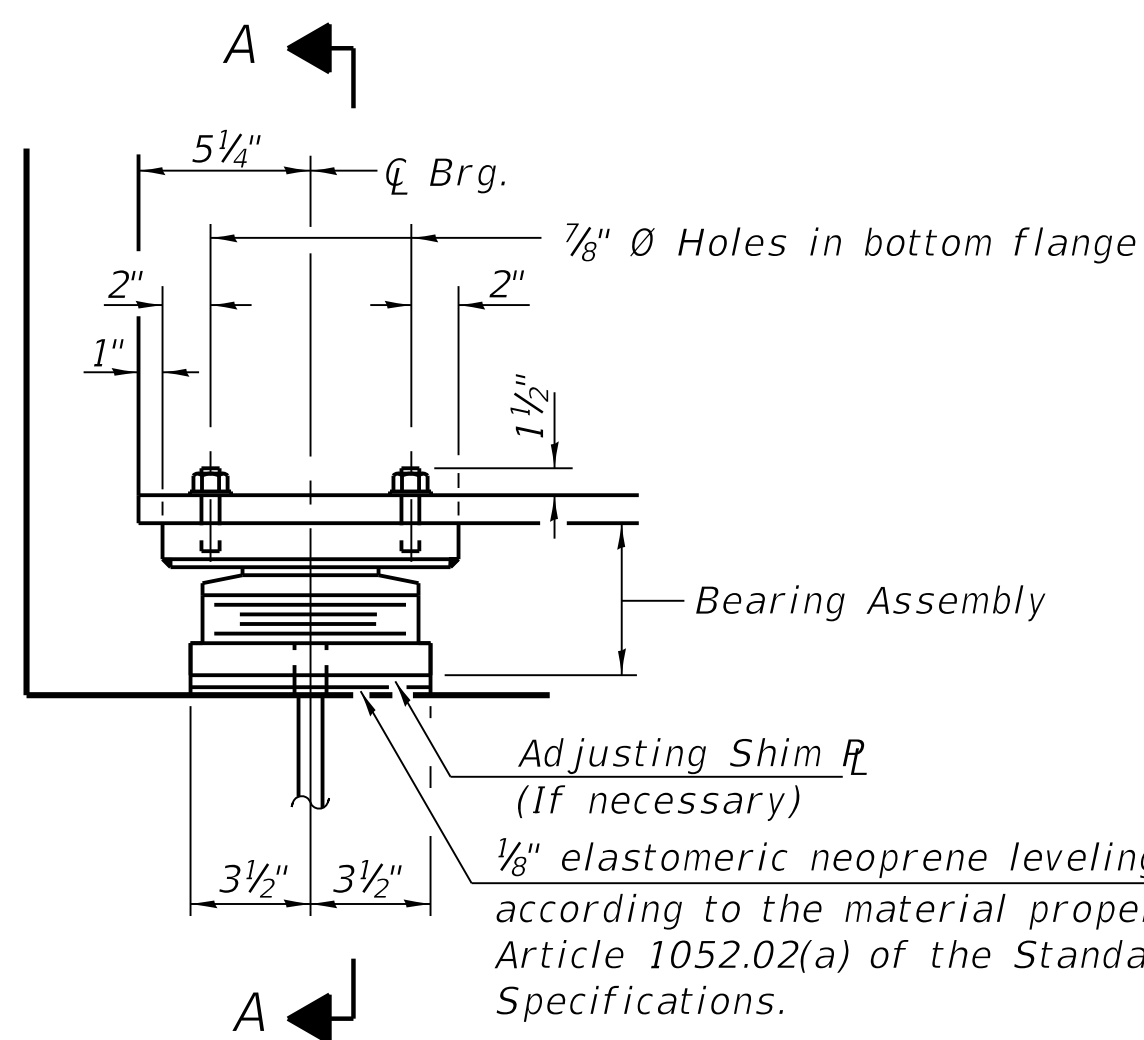
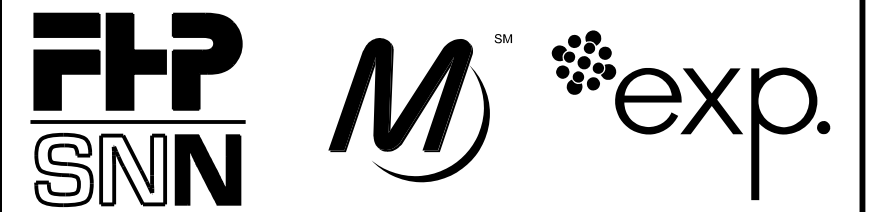
LOCATION IDENTIFIER: **DR095**  
**95TH ST. NORTH BUS BRIDGE BEARING DETAILS (SHEET 1 OF 3)**

**NBB-S-329**

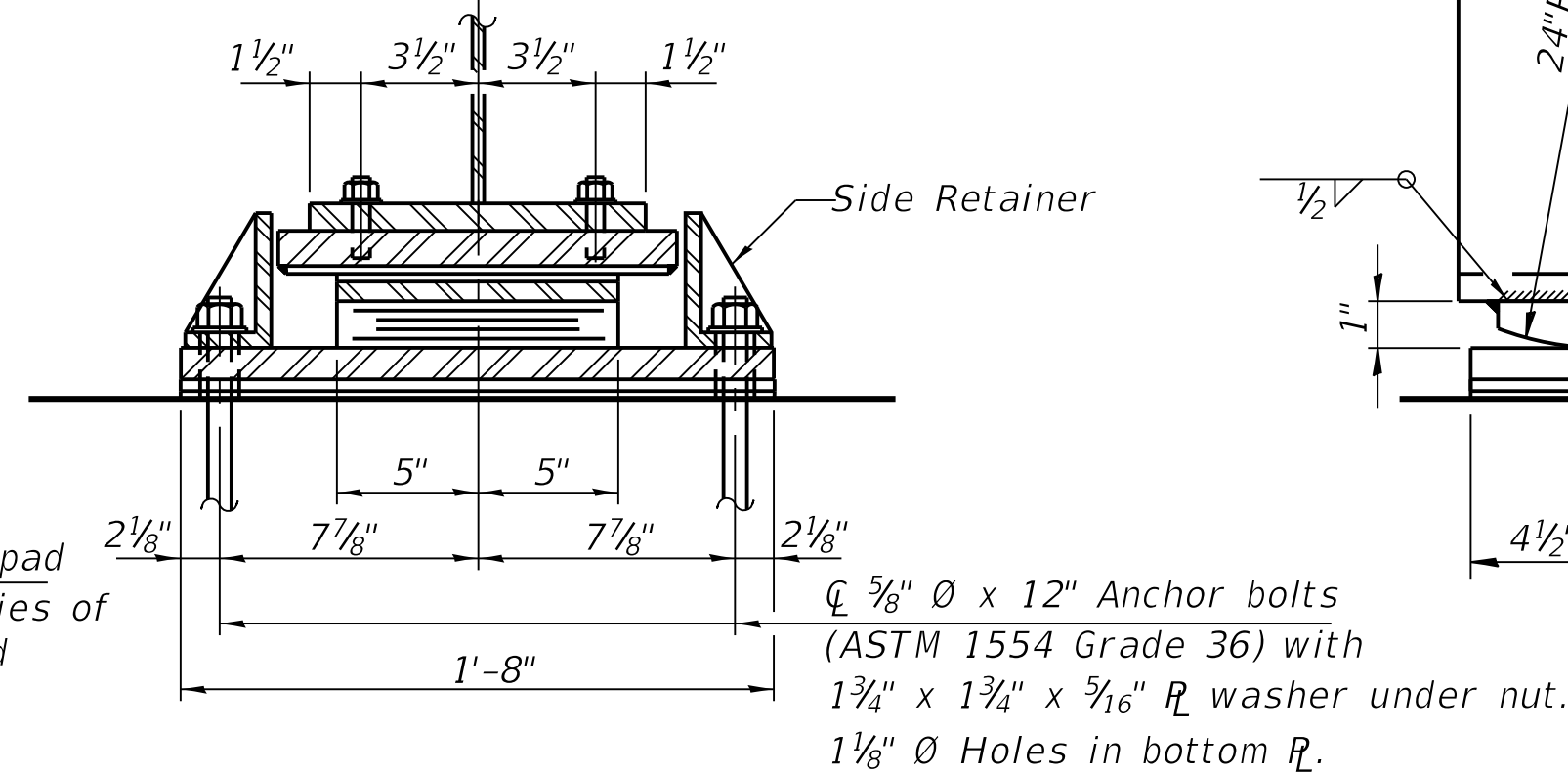
FILE NAME = 070\_DR095\_S-329.dgn  
PLOT DATE = 5/24/2018



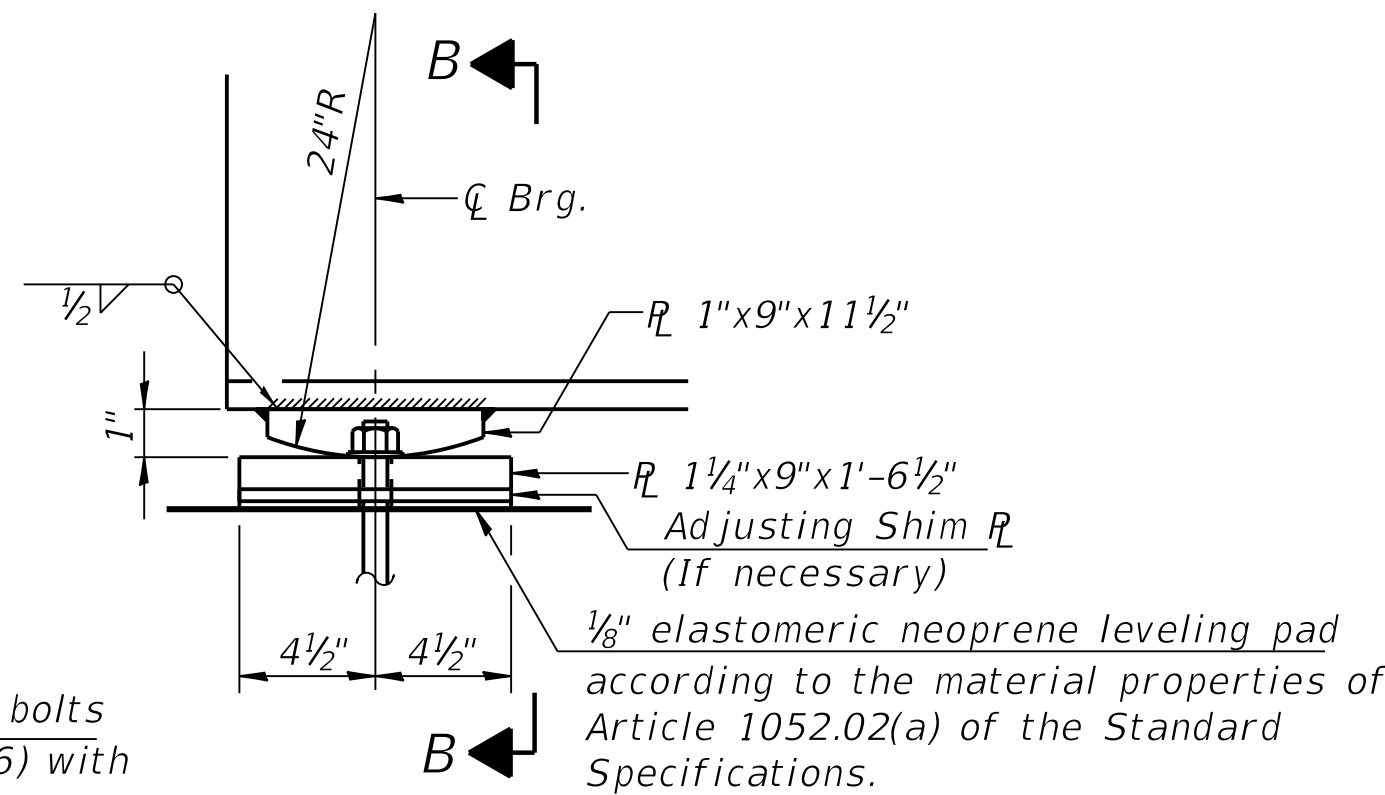
**NOT FOR CONSTRUCTION**



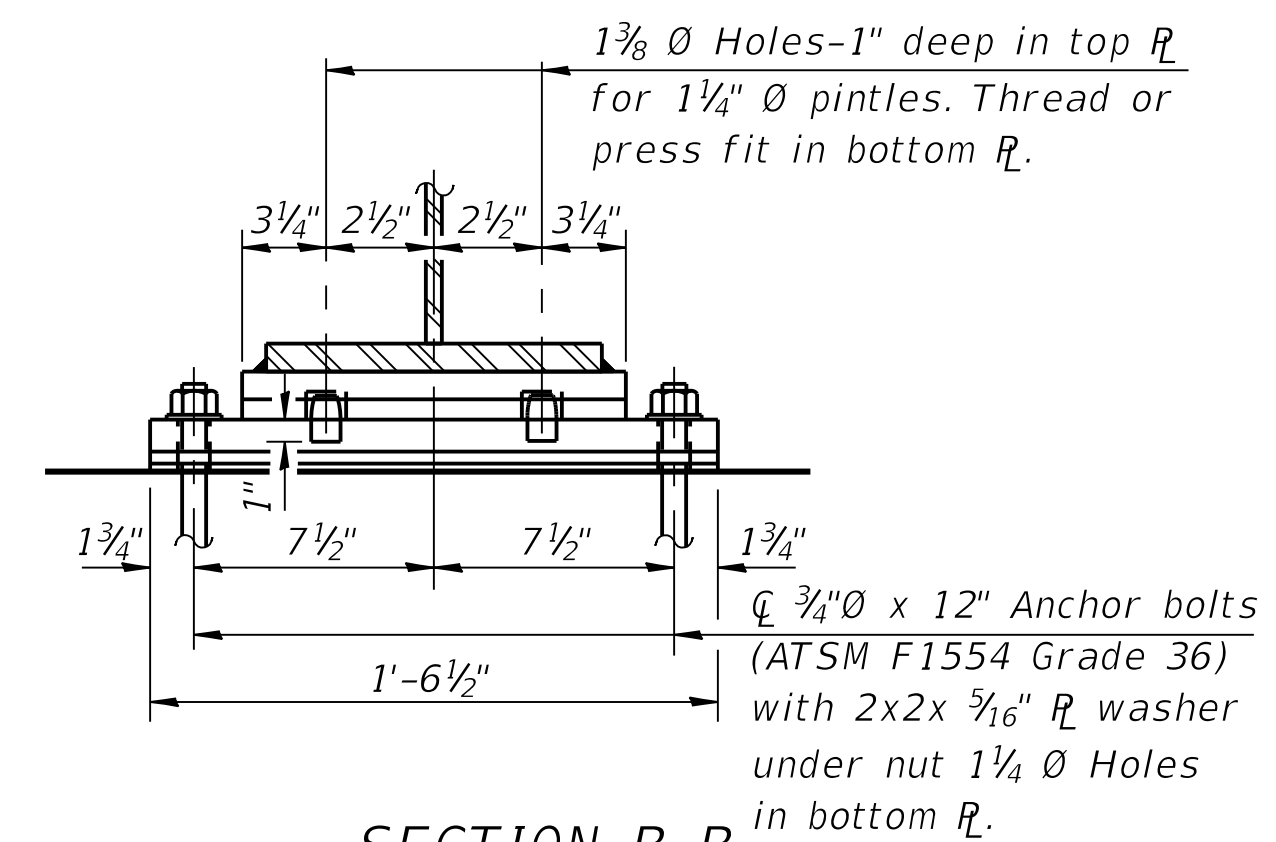
**ELEVATION AT WEST ABUTMENT**



**SECTION A-A**



**ELEVATION AT EAST ABUTMENT**



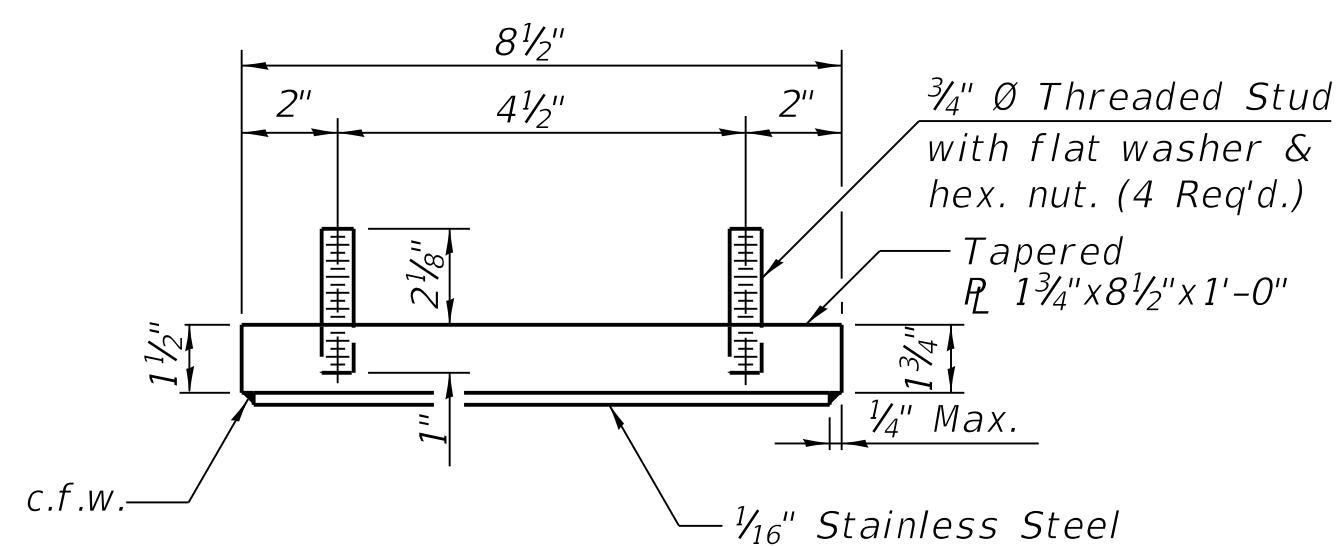
**SECTION B-B**

**TYPE II ELASTOMERIC EXP. BRG.**

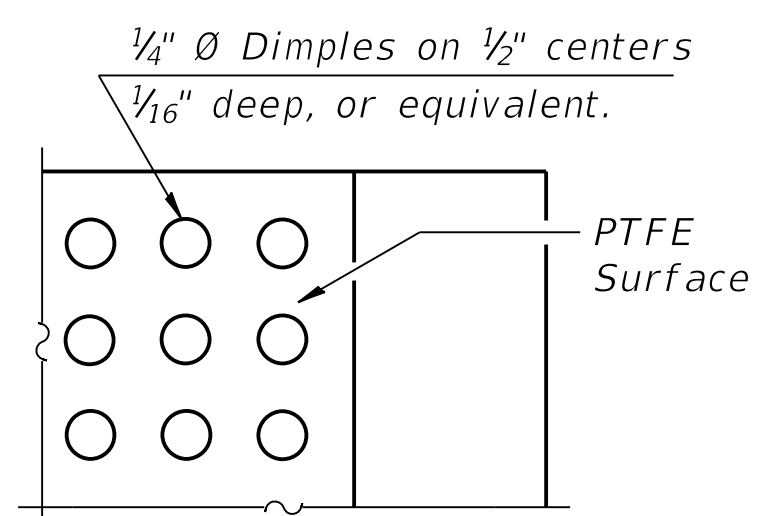
At West Abutment-Proposed Beam 4 & Beam 5

**FIXED BEARING**

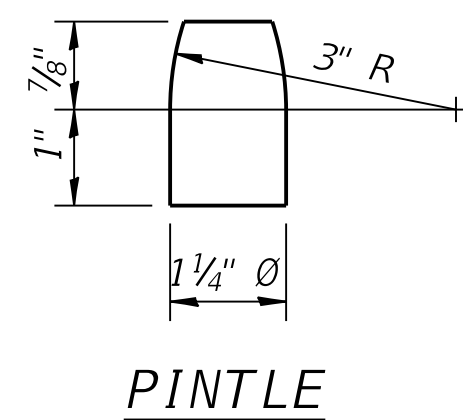
At East Abutment-Proposed Beam 6 & Beam 7



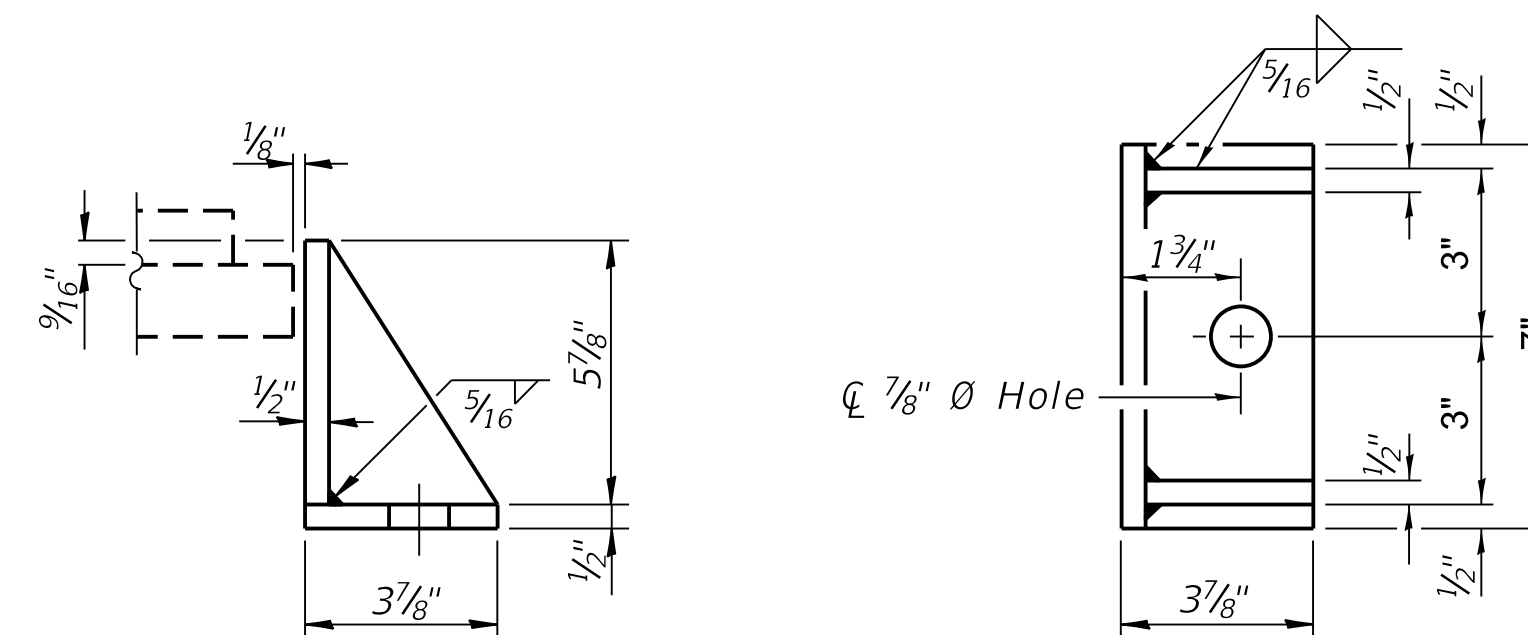
**TOP BEARING ASSEMBLY**



**PLAN-PTFE SURFACE**

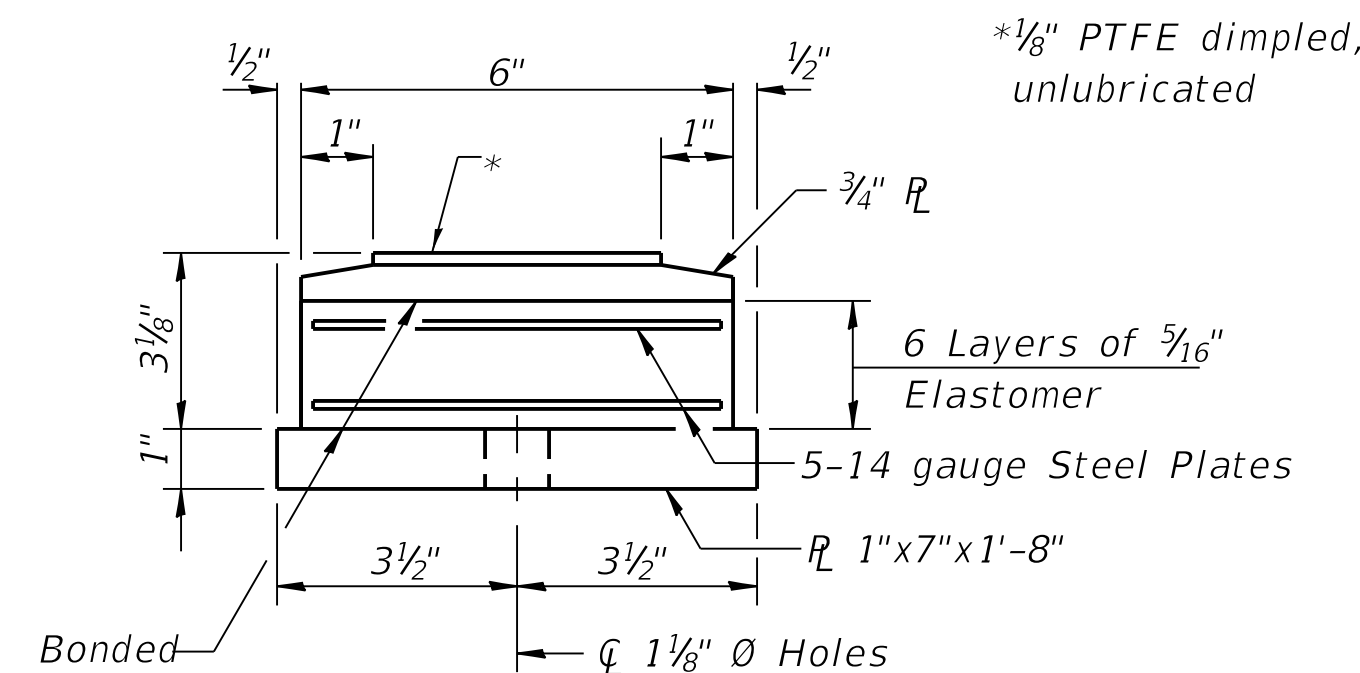


**PINTLE**

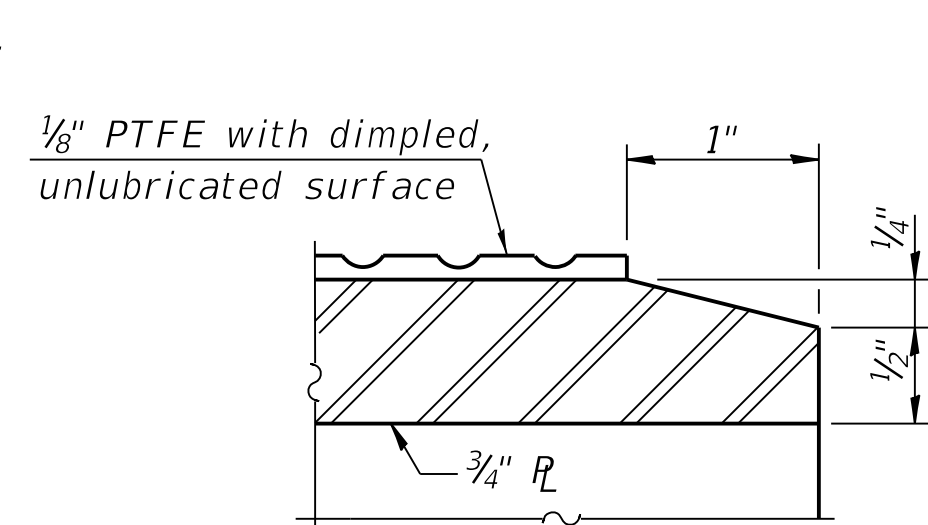


**SIDE RETAINER**

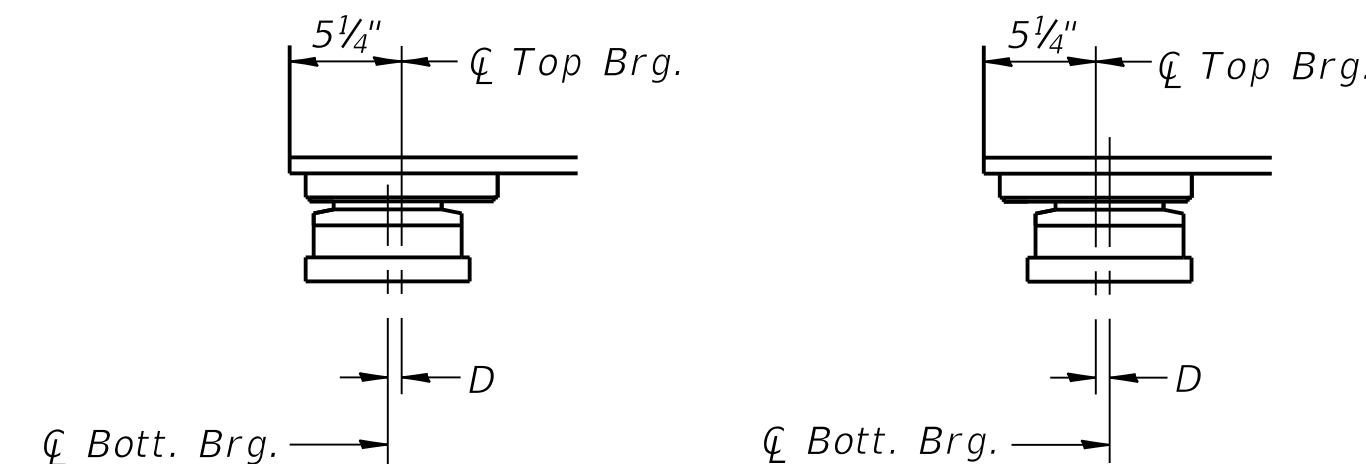
Equivalent rolled angle with stiffeners will be allowed in lieu of welded plates.



**BOTTOM BEARING ASSEMBLY**



**SECTION THRU PTFE**



**BELOW 50°F.**

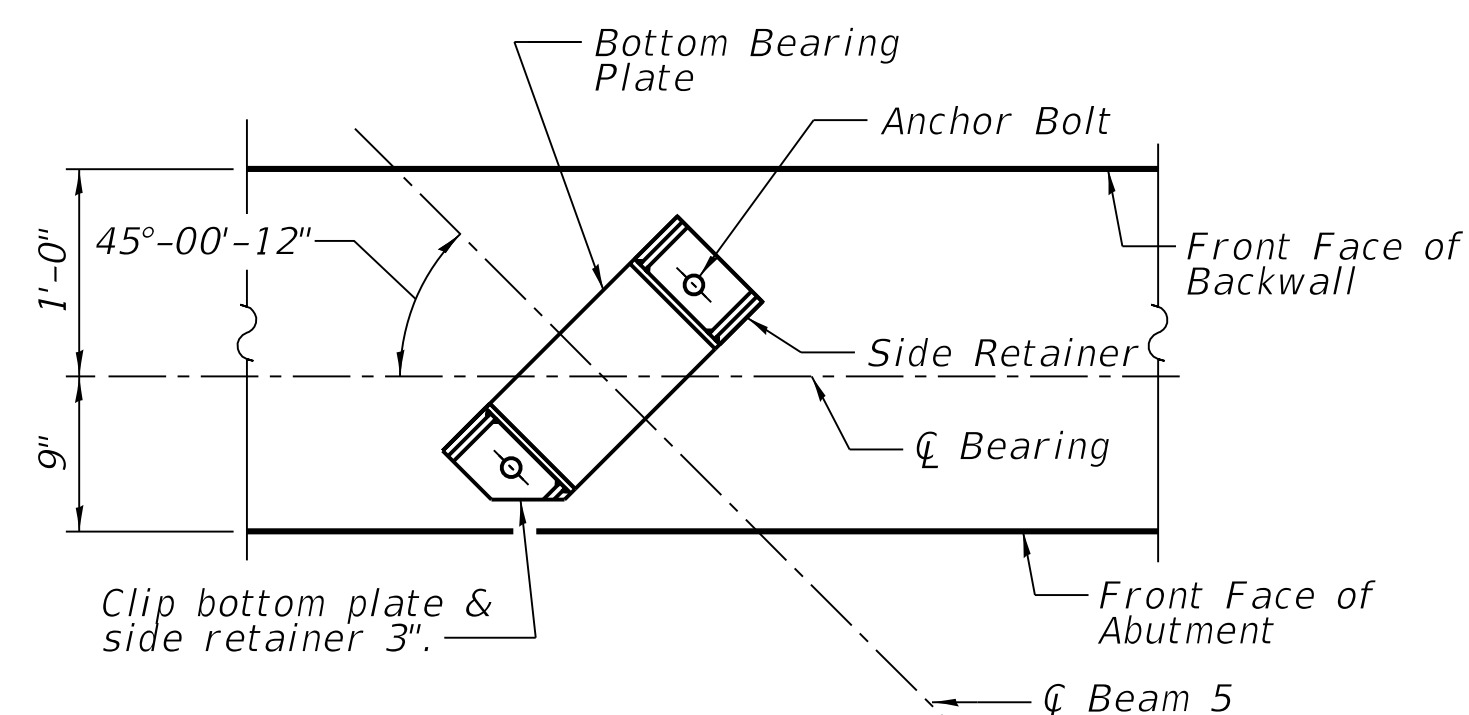
D=1/8" per each 100' of expansion for every 15° temp. change from the normal temp. of 50°F.

**ABOVE 50°F.**

**EXPANSION BEARING ORIENTATION**

The above diagrams are for informational purposes only to show the amount of expected offset "D" for the current temperature in the field.

Note:  
For notes, see NBB-S-329



**BOTTOM PLATE CLIP DETAIL**

(Beam 5 shown, Beam 7 similar)

**CHICAGO TRANSIT AUTHORITY ENGINEERING**

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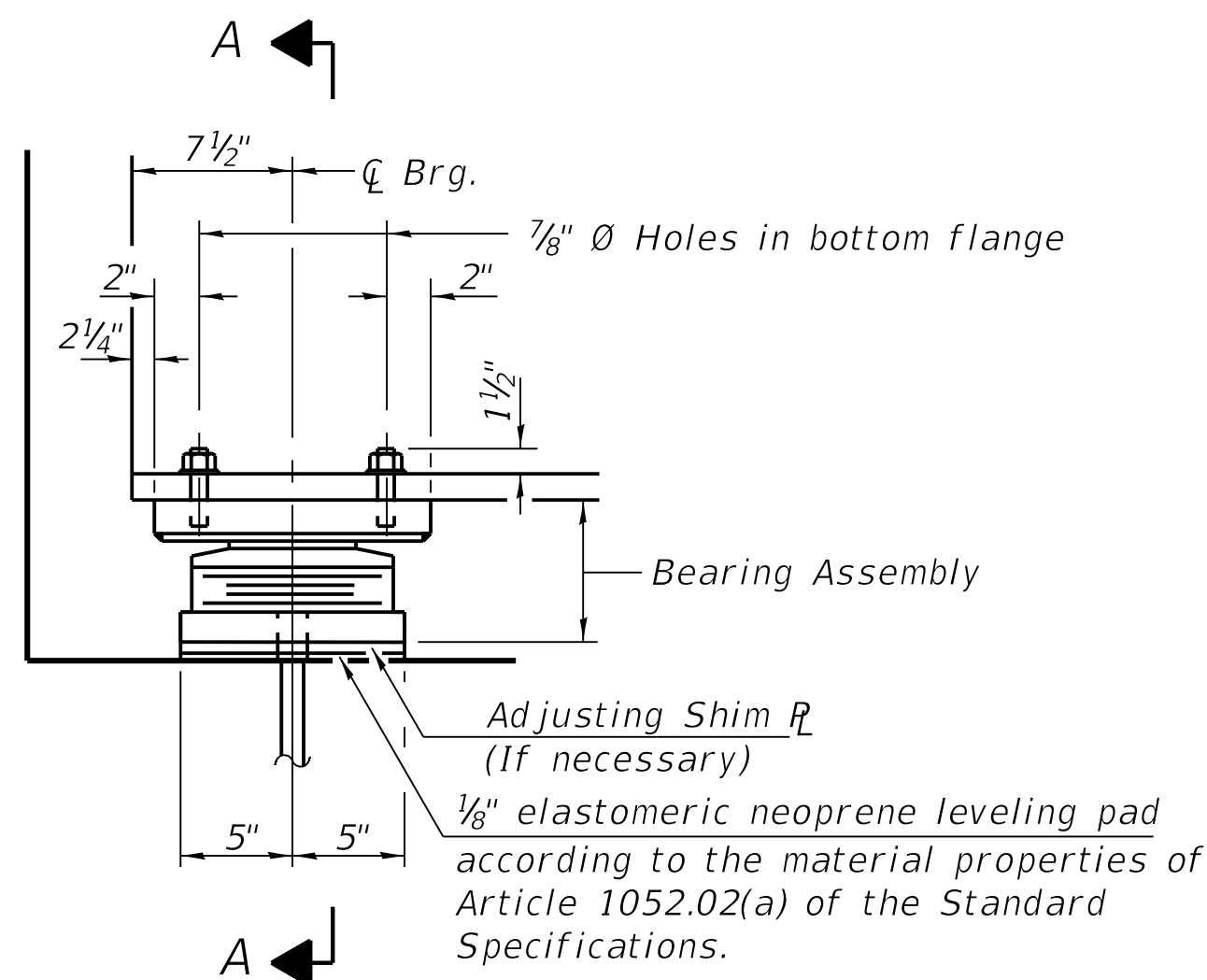
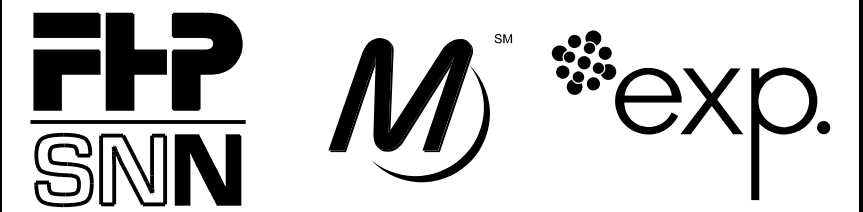
MARK	DATE	DESCRIPTION
	05/23/18	FINAL DESIGN
	03/07/18	INTERMEDIATE DESIGN

LOCATION IDENTIFIER: **DR095**

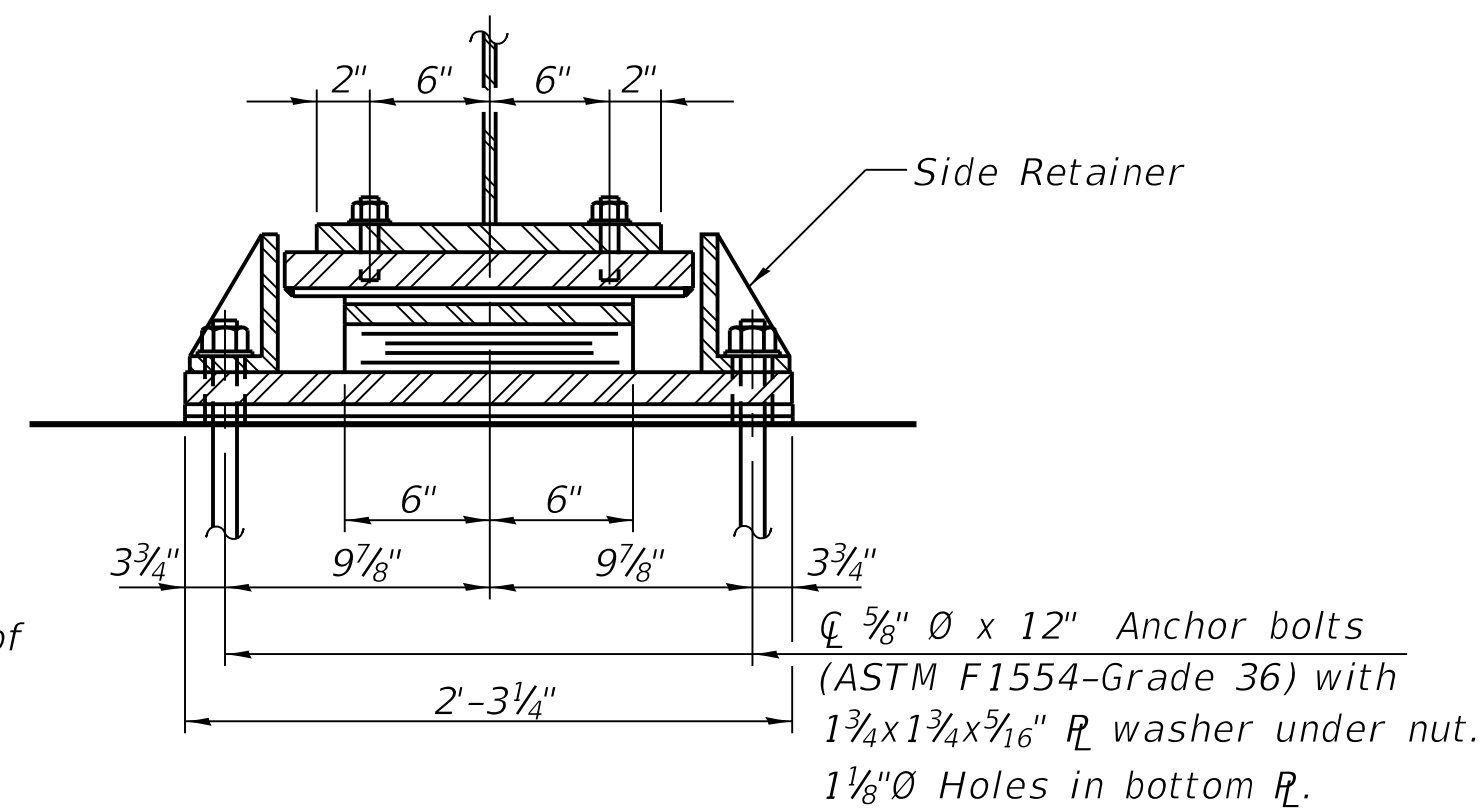
95TH ST. NORTH BUS BRIDGE  
BEARING DETAILS  
(SHEET 2 OF 3)

**NBB-S-329A**

**NOT FOR CONSTRUCTION**

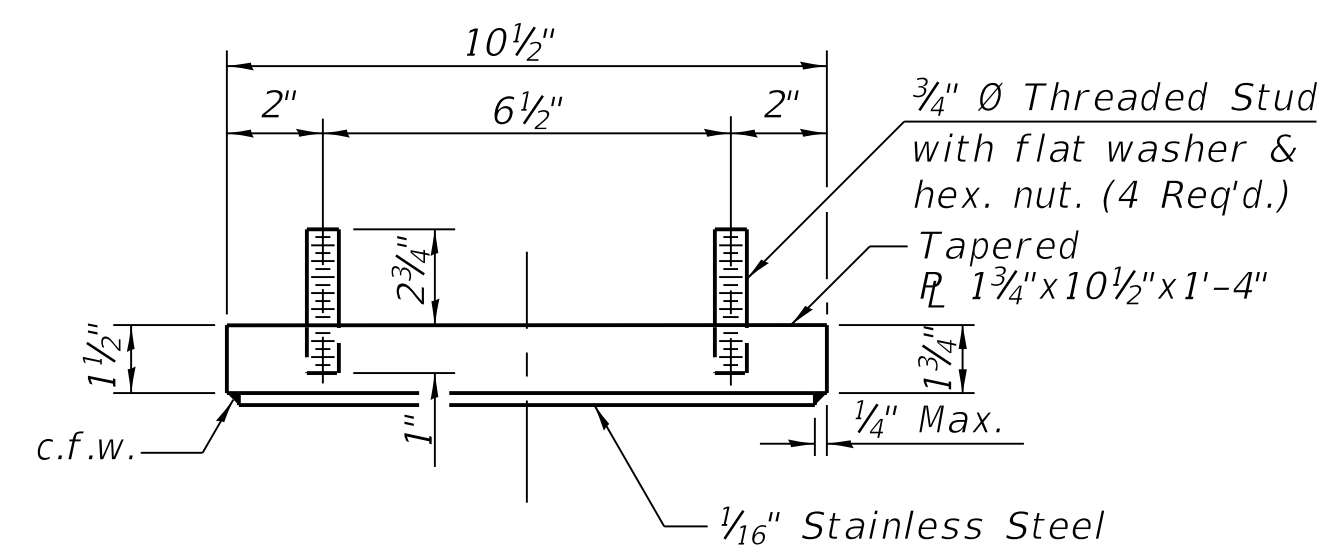


**ELEVATION AT WEST ABUTMENT**  
 Proposed Girder 1

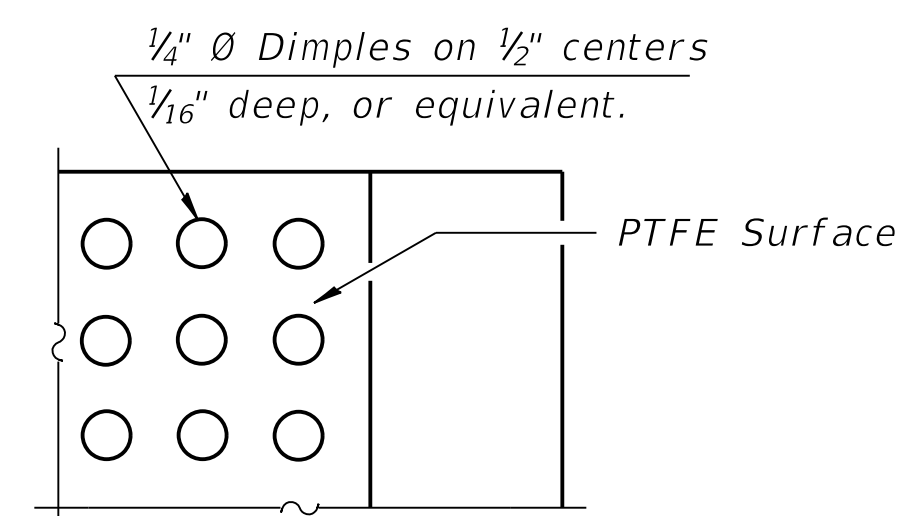


**SECTION A-A**

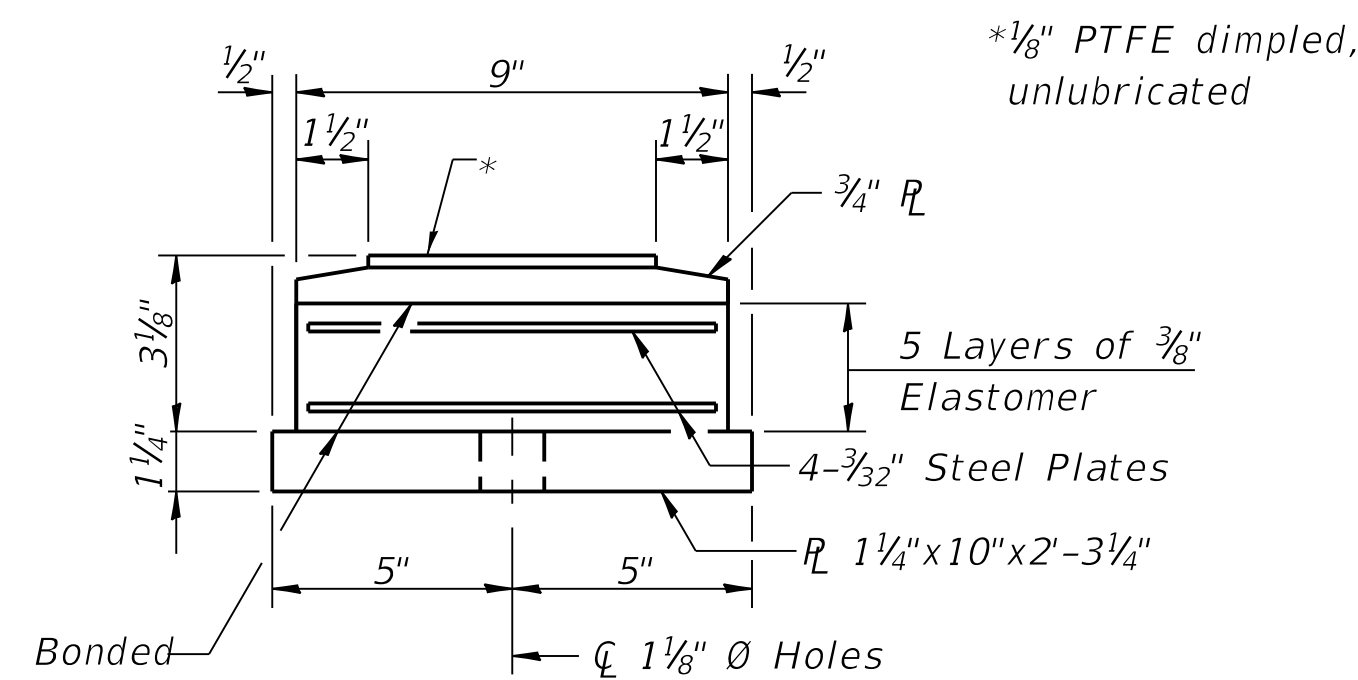
**TYPE II ELASTOMERIC EXP. BRG.**



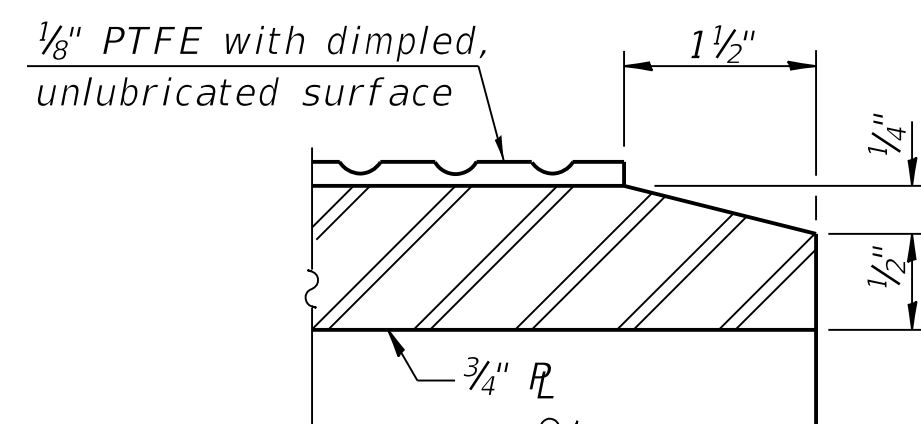
**TOP BEARING ASSEMBLY**



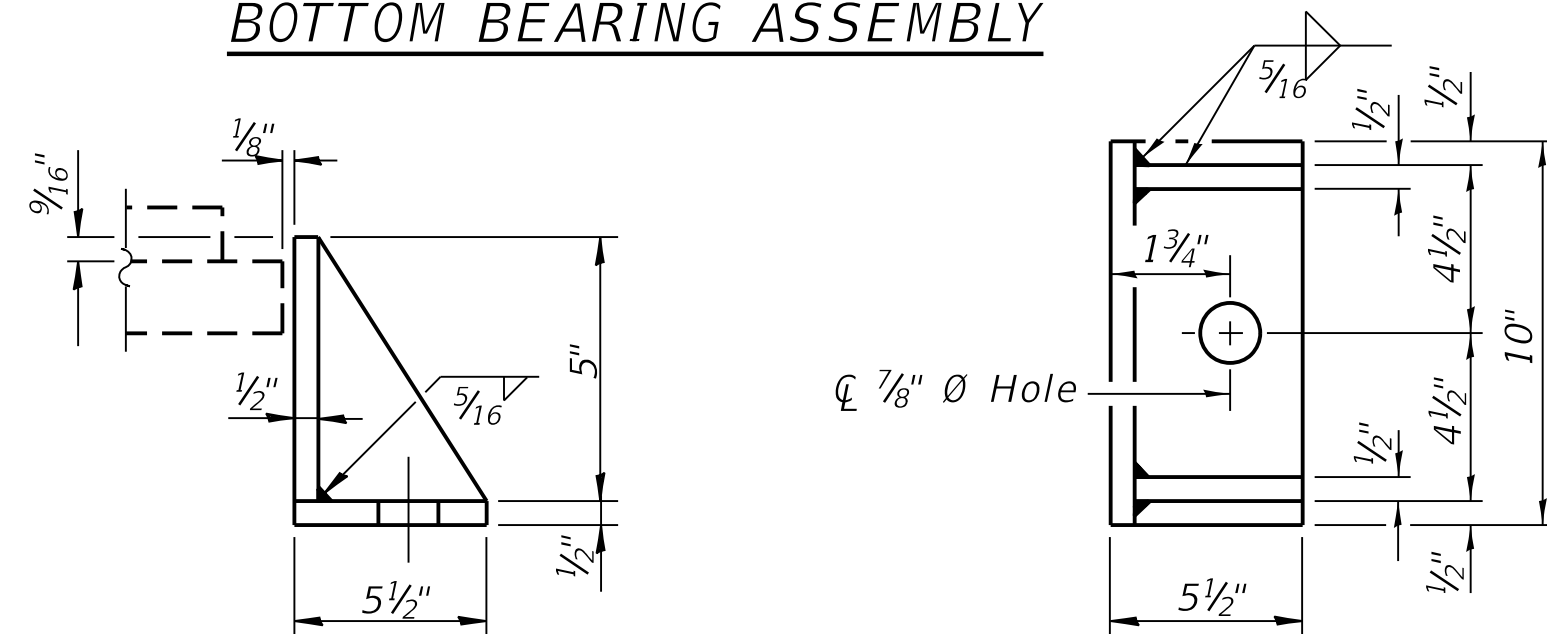
**PLAN-PTFE SURFACE**



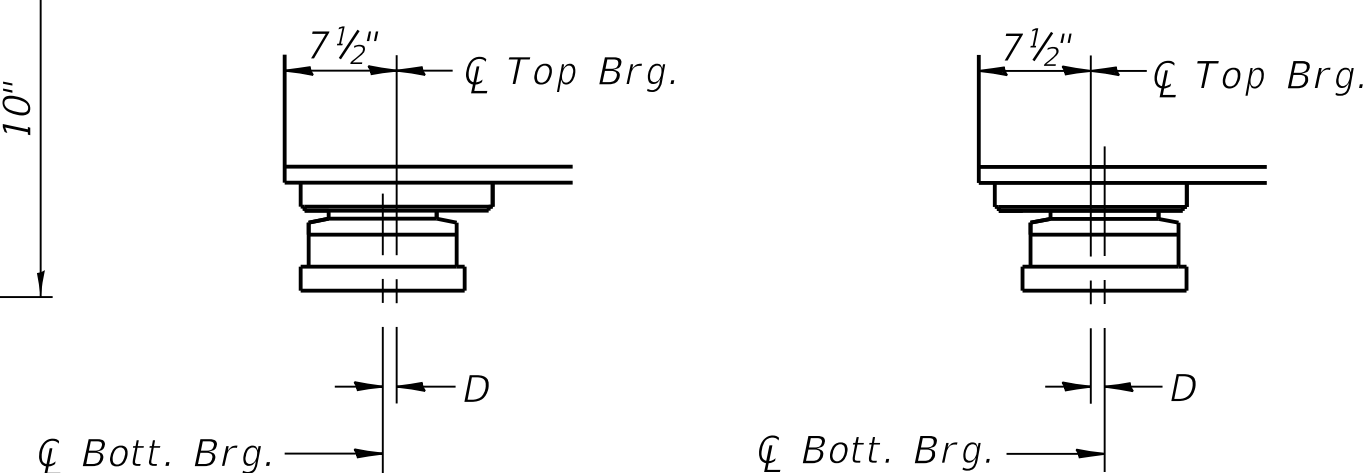
**BOTTOM BEARING ASSEMBLY**



**SECTION THRU PTFE**



**SIDE RETAINER**  
 Equivalent rolled angle with stiffeners  
 will be allowed in lieu of welded plates.



**EXPANSION BEARING ORIENTATION**  
 BELOW 50°F. ABOVE 50°F.  
 D=1/8" per each 100' of expansion for every 15° temp.  
 change from the normal temp. of 50°F.

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 amount of expected offset "D" for the current temperature in the field.

**CHICAGO TRANSIT AUTHORITY ENGINEERING**

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 SECURITY ADMINISTRATION OR THE SECRETARY OF TRANSPORTATION. UNAUTHORIZED  
 RELEASE MAY RESULT IN CIVIL PENALTY OR OTHER ACTION. FOR U.S. GOVERNMENT  
 AGENCIES, PUBLIC DISCLOSURE IS GOVERNED BY U.S.C. 552 AND 49 CFR PARTS 15  
 AND 1520.

DRAWING SCALE IS NOT GUARANTEED.  
 CTA ASSUMES NO RISK OF LIABILITY FOR  
 ERRORS CAUSED, DIRECTLY OR INDIRECTLY,  
 BY SCALING OF THIS DRAWING.

**95th Terminal Improvement**  
 14 West 95th Street (North Terminal)  
 Chicago, IL 60628

IN CHARGE	S. KADERBEK
APPROVED BY	D.ZROKA
CHECKED BY	P.MAID
DESIGNED BY	L.SOMMER
DRAWN BY	T.SLEDGE
PROJECT NO.	2012-0021
FILE NAME	File Name

MARK	DATE	DESCRIPTION
	05/23/18	FINAL DESIGN
	03/07/18	INTERMEDIATE DESIGN

LOCATION IDENTIFIER: **DR095**  
 95TH ST. NORTH BUS BRIDGE  
 BEARING DETAILS  
 (SHEET 3 OF 3)

**NBB-S-329B**

Note:  
 For notes, see NBB-S-329

vscorppa

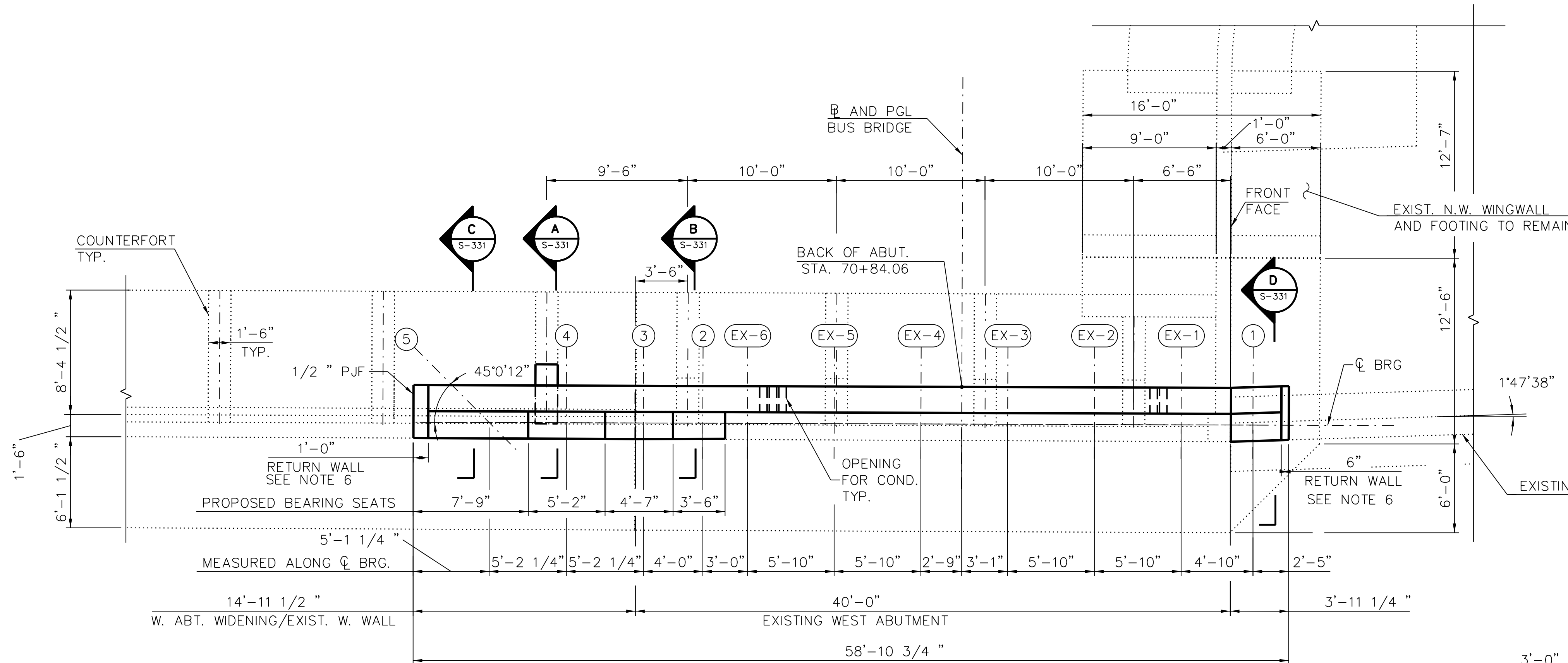
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 PLOT DATE = 5/24/2018

**NOT FOR CONSTRUCTION**

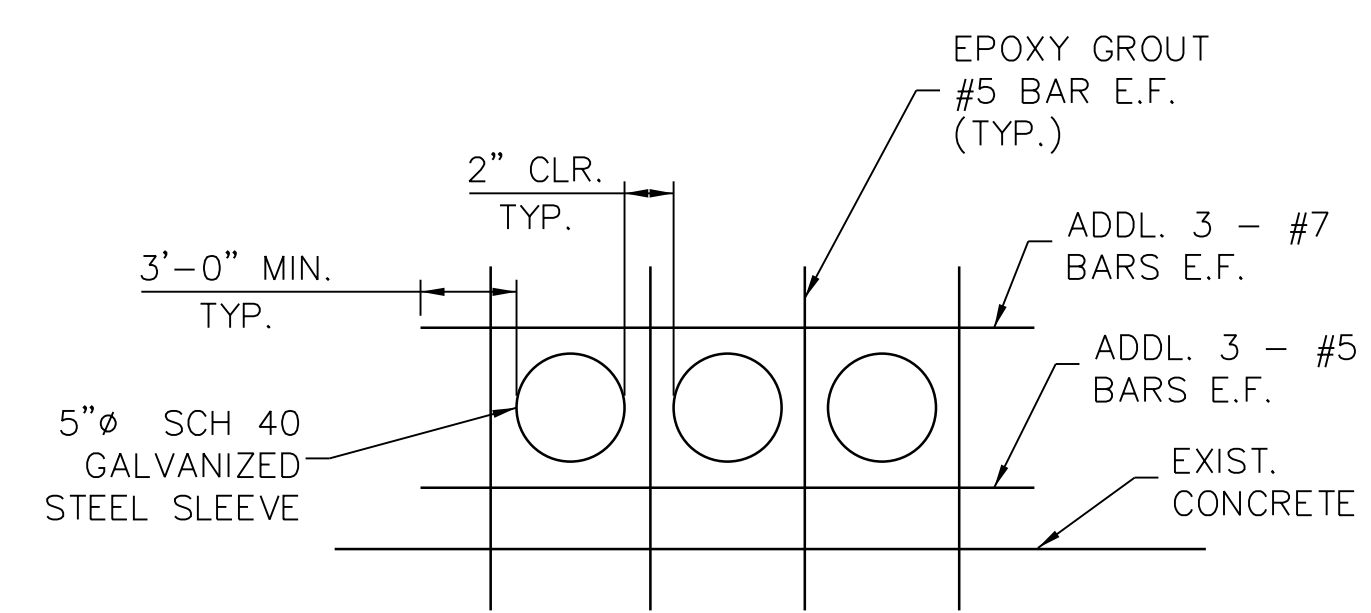


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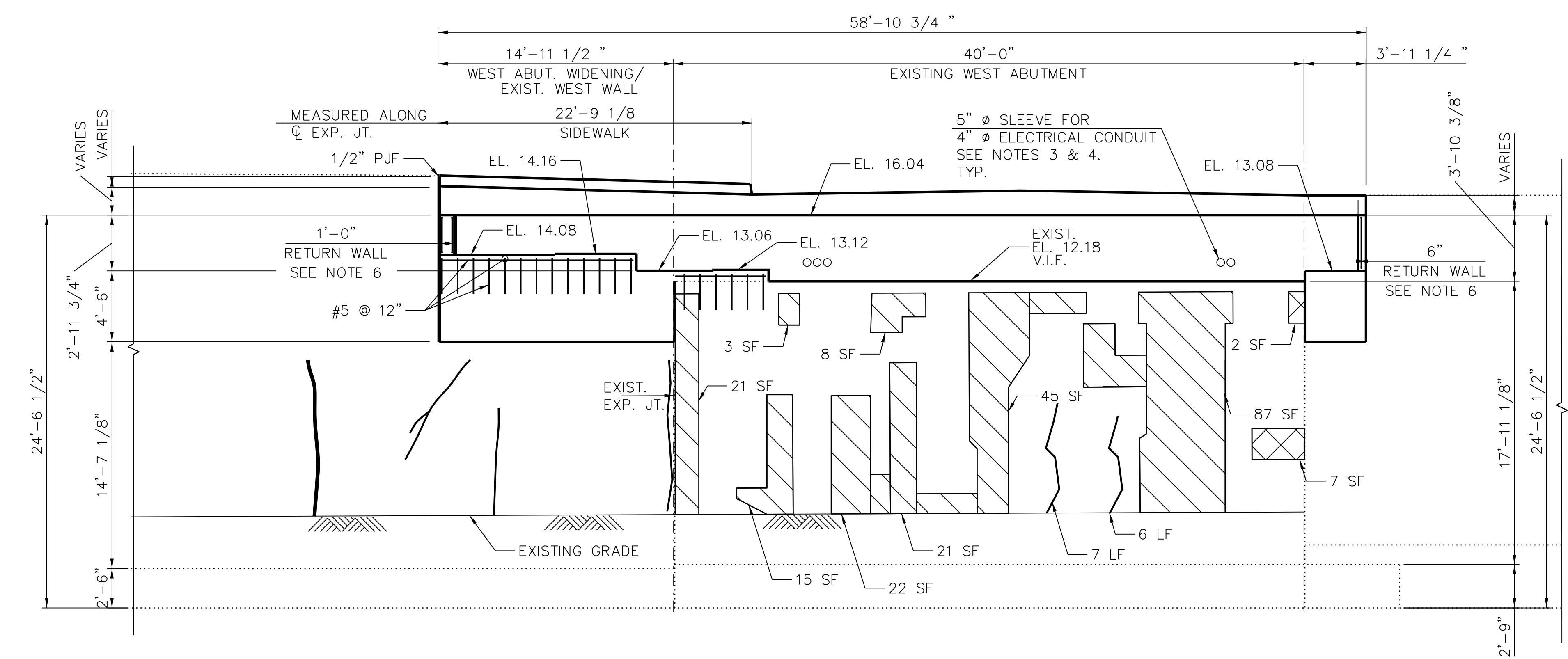
1. FOR REPAIRS IN EXISTING WEST WALL, SEE SHEETS EXW-S-504 THRU EXW-S-506.
2. FOR STRUCTURAL REPAIR OF CONCRETE DETAILS, SEE SHEET 95TH-S-150A.
3. SEE SHEET E-004 FOR PENETRATION LOCATIONS.
4. NON-SHRINK GROUT BETWEEN PIPE AND SLEEVE AS REQUIRED.
5. SEE NBB-S-331 FOR REINFORCEMENT DETAILS.
6. PROVIDE #4 @ 12" O.C. E.W. IN RETURN WALLS.



**1 WEST ABUTMENT PLAN**



**3 PENETRATION DETAIL**  
SCALE: N.T.S.



**2 WEST ABUTMENT ELEVATION**  
(LOOKING WEST)

**LEGEND**

- STRUCTURAL REPAIR OF CONCRETE (DEPTH GREATER THAN 5 INCHES)
- STRUCTURAL REPAIR OF CONCRETE (DEPTH EQUAL TO OR LESS THAN 5 INCHES)
- LOW PRESSURE EPOXY INJECTION (WIDTH >0.06")
- SF - SQUARE FOOT
- LF - LINEAR FOOT

**cta CHICAGO TRANSIT AUTHORITY ENGINEERING**

**SENSITIVE SECURITY INFORMATION**  
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**95th Terminal Improvement**  
14 West 95th Street (North Terminal)  
Chicago, IL 60628

<b>IN CHARGE</b>	S. KADERBEK
<b>APPROVED BY</b>	D.ZROKA
<b>CHECKED BY</b>	P.MAID
<b>DESIGNED BY</b>	L.SOMMER
<b>DRAWN BY</b>	T.SLEDGE
<b>PROJECT NO.</b>	2012-0021
<b>FILE NAME</b>	File Name

MARK	DATE	DESCRIPTION
	05/23/18	FINAL DESIGN
	03/07/18	INTERMEDIATE DESIGN

**LOCATION IDENTIFIER: DR095**  
**95TH STREET NORTH BUS BRIDGE WEST ABUTMENT PLAN AND ELEVATION**

**NBB-S-330**

**NOT FOR CONSTRUCTION**



**CHICAGO TRANSIT AUTHORITY ENGINEERING**

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**95th Terminal Improvement**  
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Chicago, IL 60628

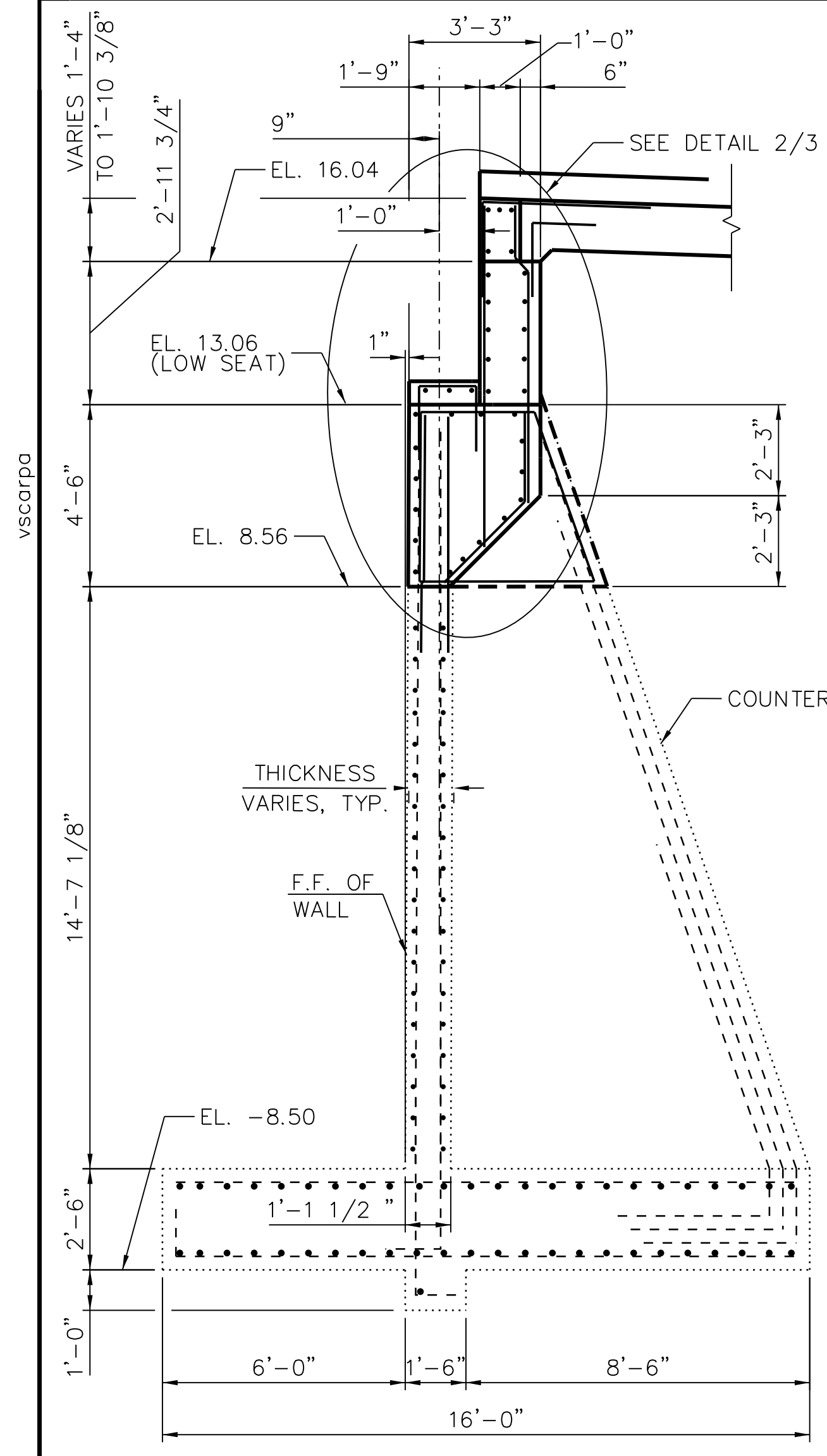
<b>IN CHARGE</b>	S. KADERBEK
<b>APPROVED BY</b>	D.ZROKA
<b>CHECKED BY</b>	P.MAID
<b>DESIGNED BY</b>	L.SOMMER
<b>DRAWN BY</b>	T.SLEDGE
<b>PROJECT NO.</b>	2012-0021
<b>FILE NAME</b>	File Name

MARK	DATE	DESCRIPTION
	05/23/18	FINAL DESIGN
	03/07/18	INTERMEDIATE DESIGN

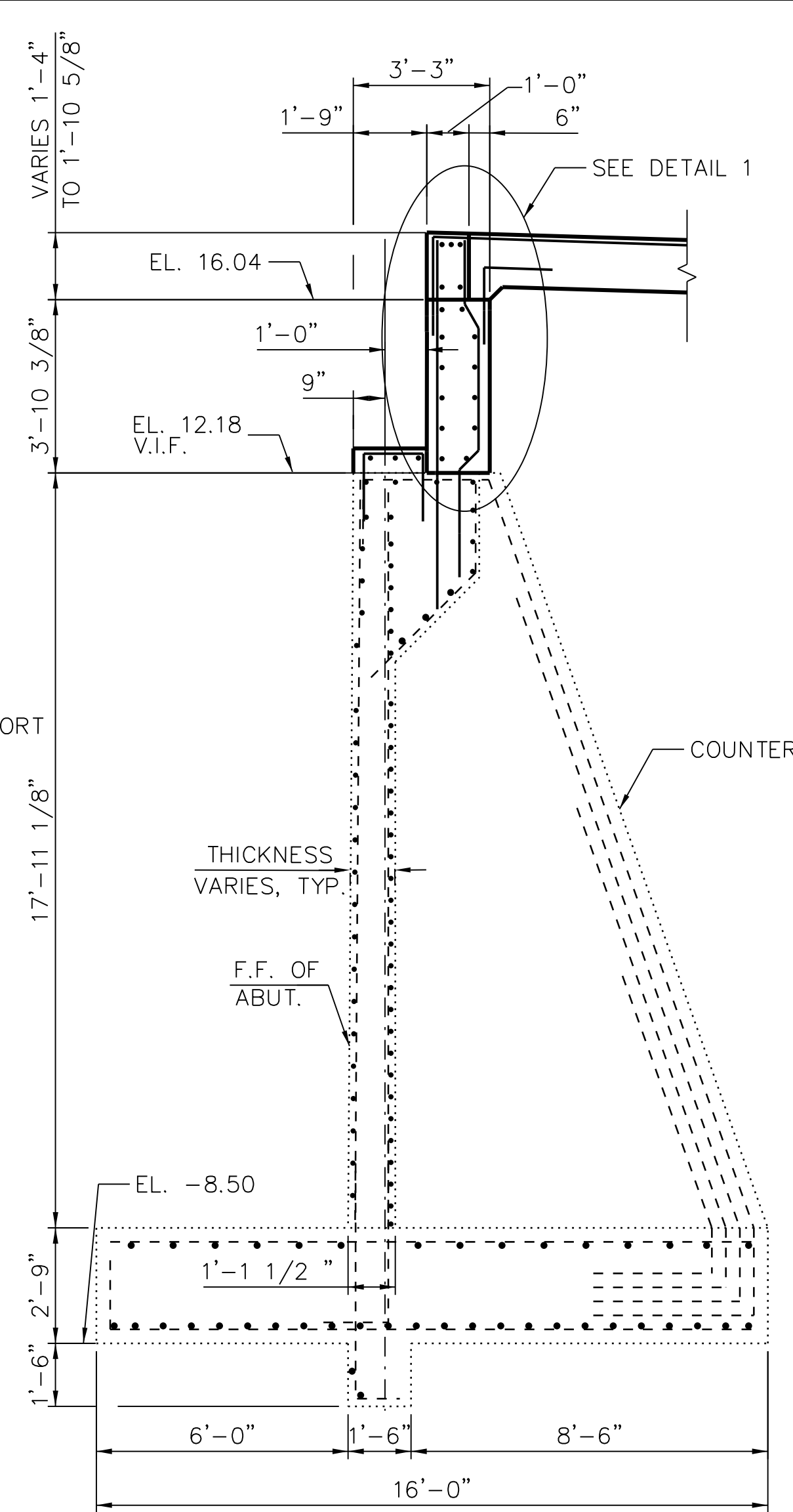
LOCATION IDENTIFIER: **DR095**

**95TH ST. NORTH BUS BRIDGE WEST ABUTMENT SECTIONS AND DETAILS**

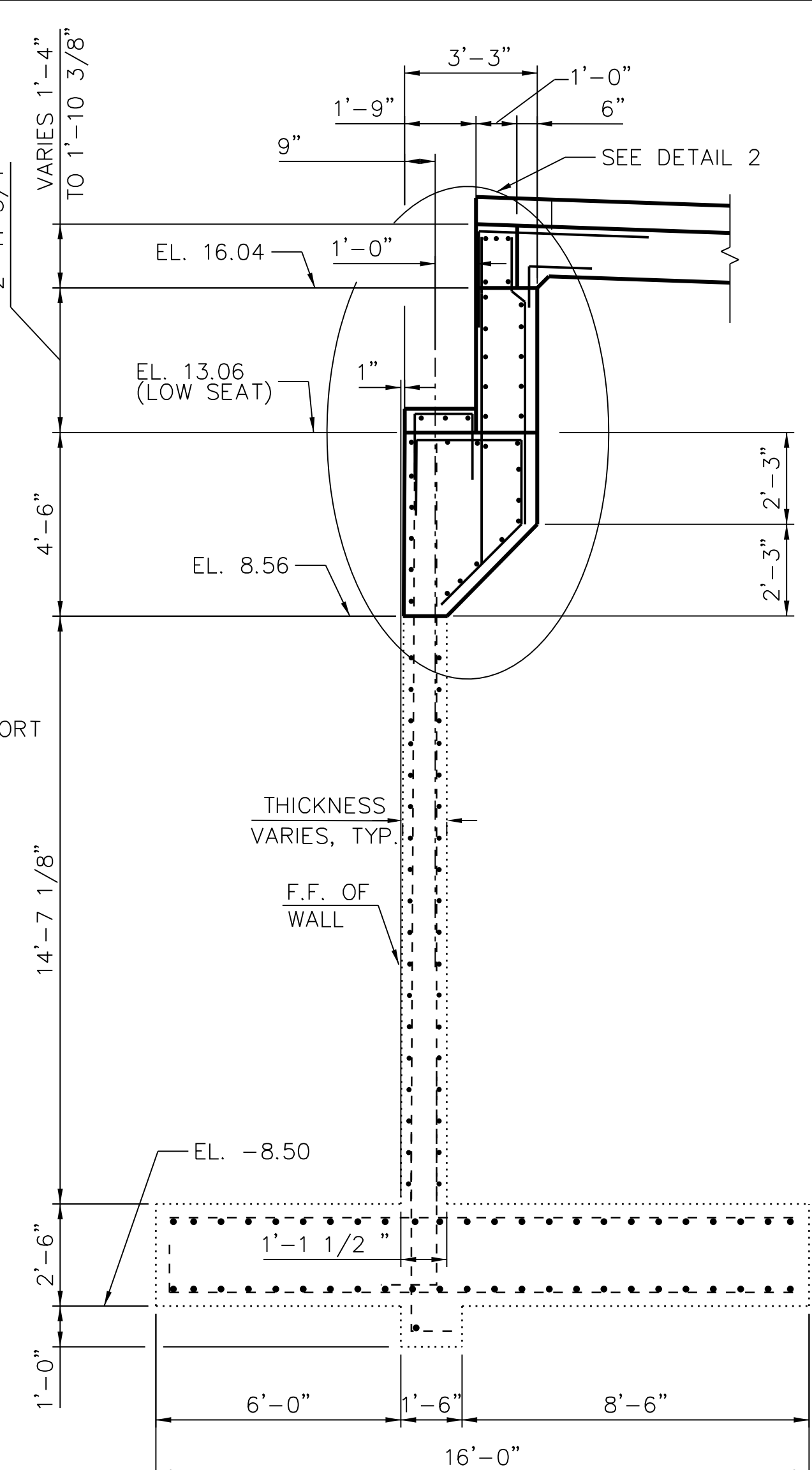
**NBB-S-331**



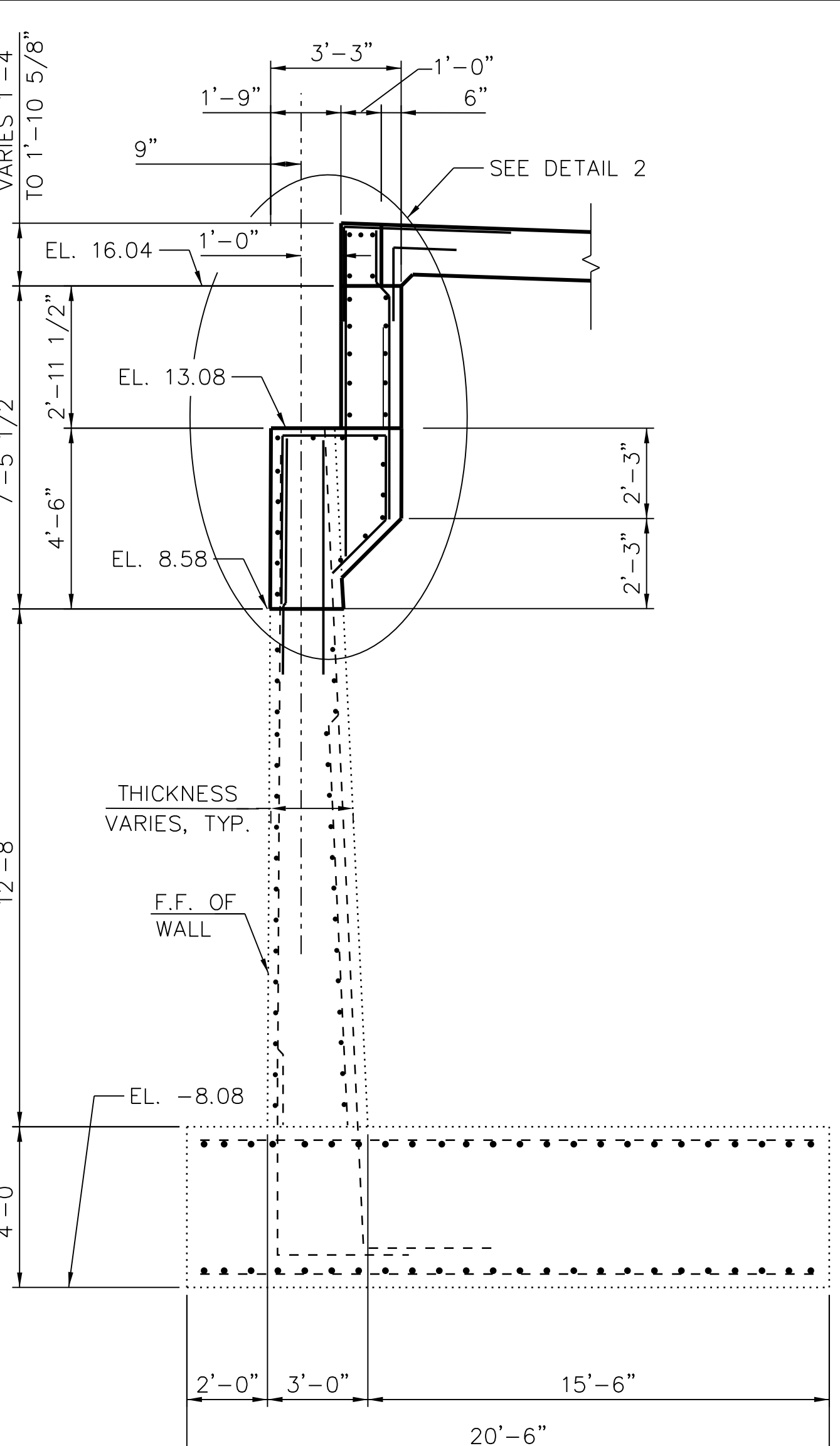
**A SECTION A**  
S-330 (LOOKING SOUTH)



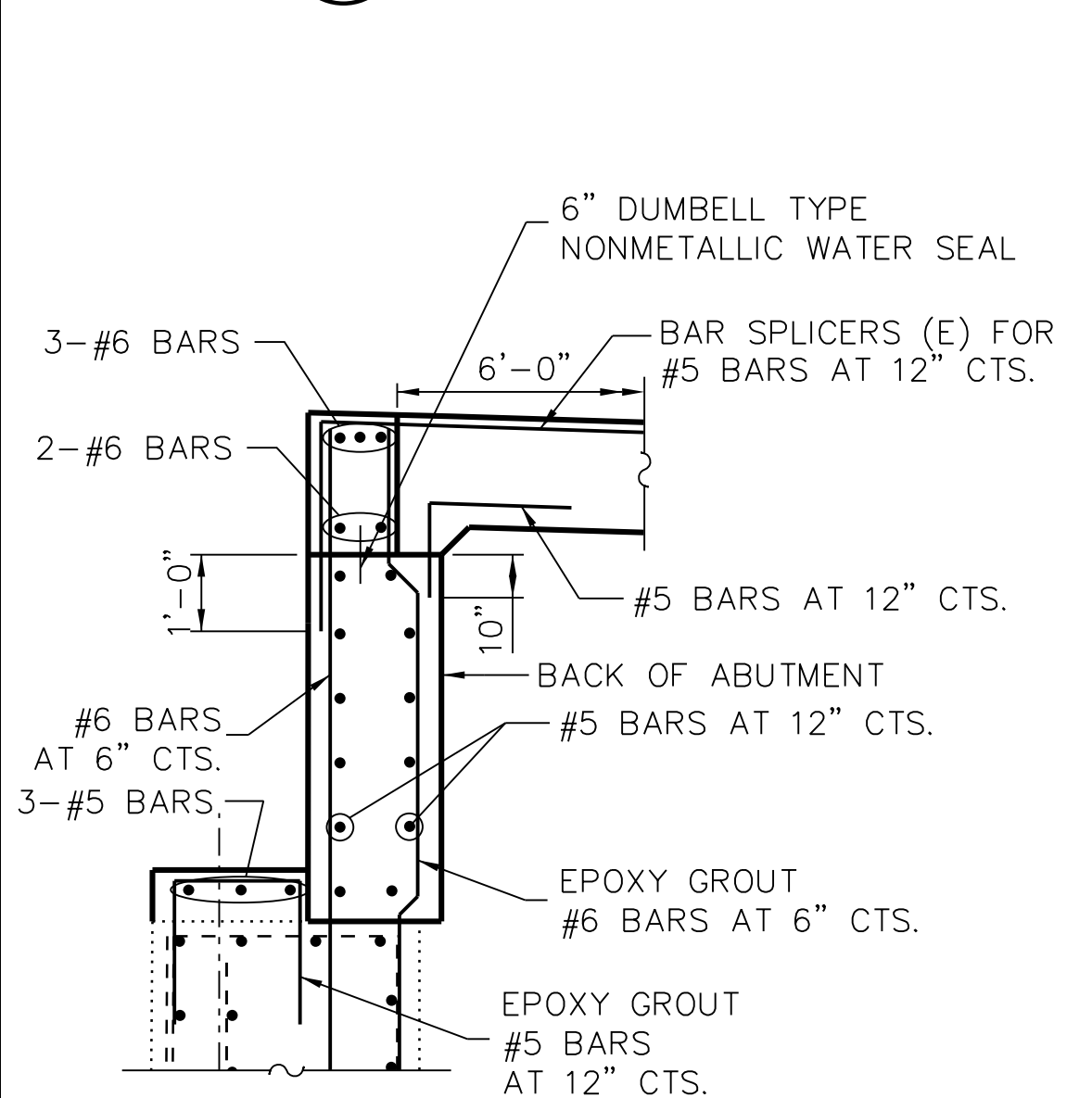
**B SECTION B**  
S-330 (LOOKING SOUTH)



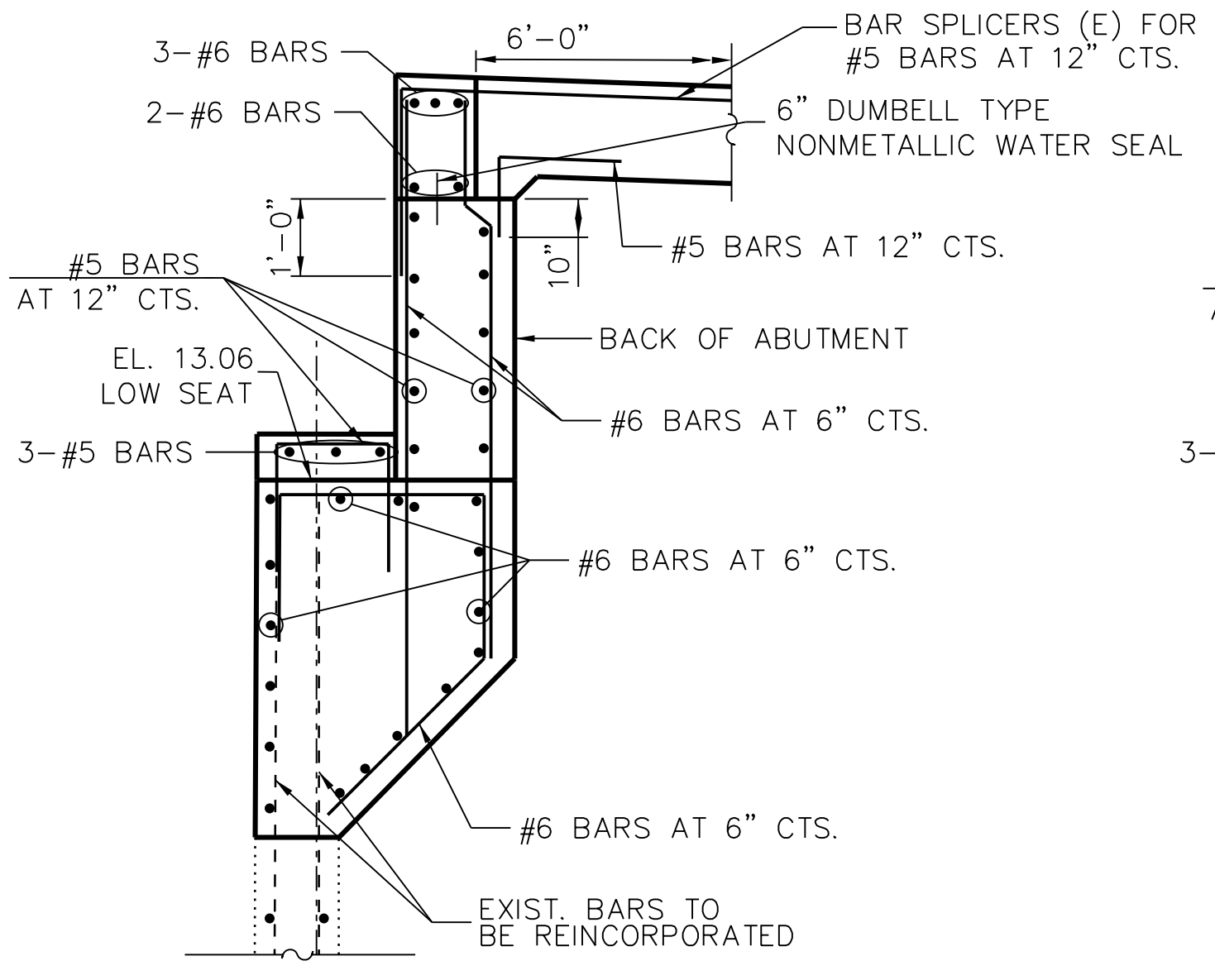
**C SECTION C**  
S-330 (LOOKING SOUTH)



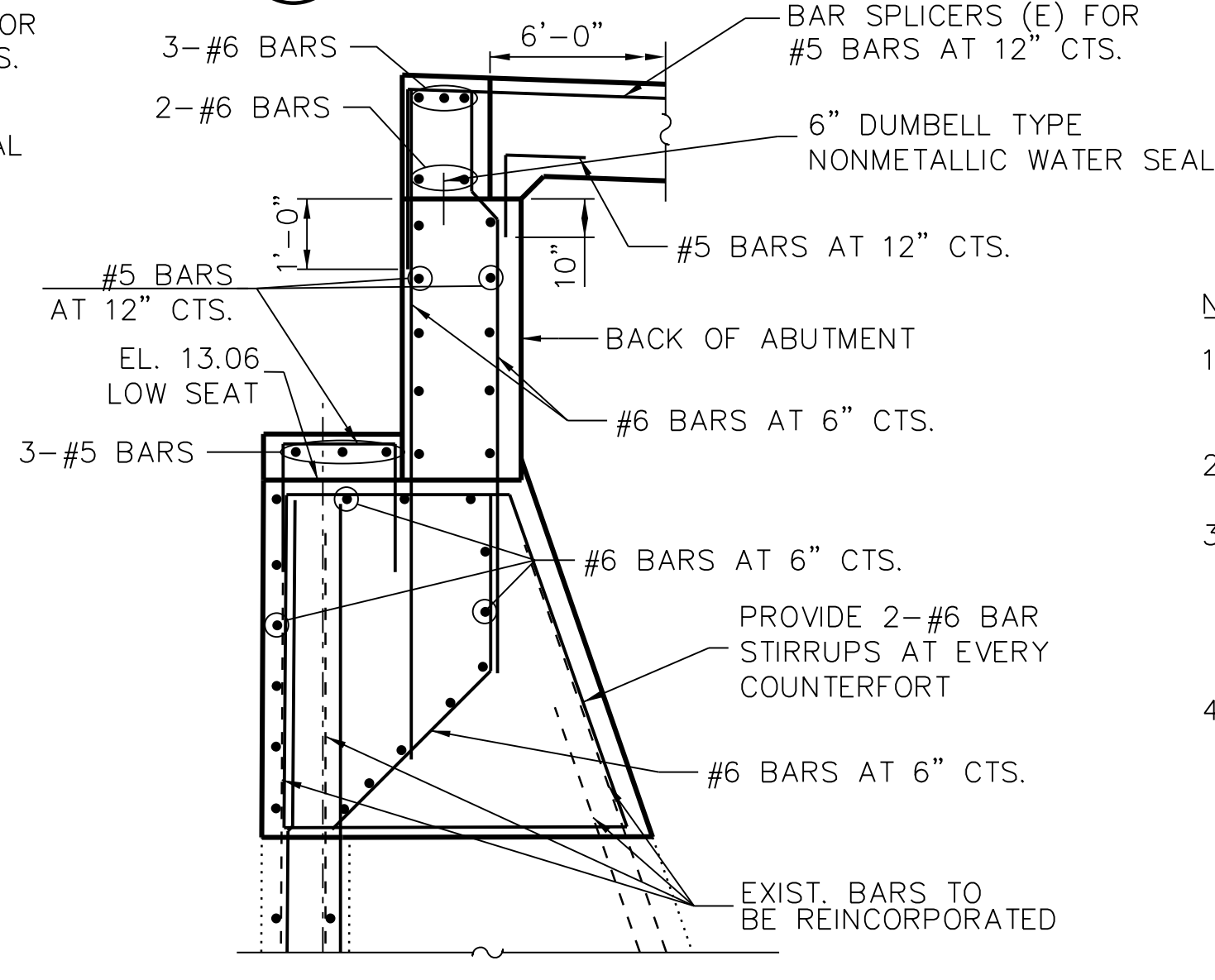
**D SECTION D**  
S-330 (LOOKING SOUTH)



**1 DETAIL 1**  
S-331



**2 DETAIL 2**  
S-331

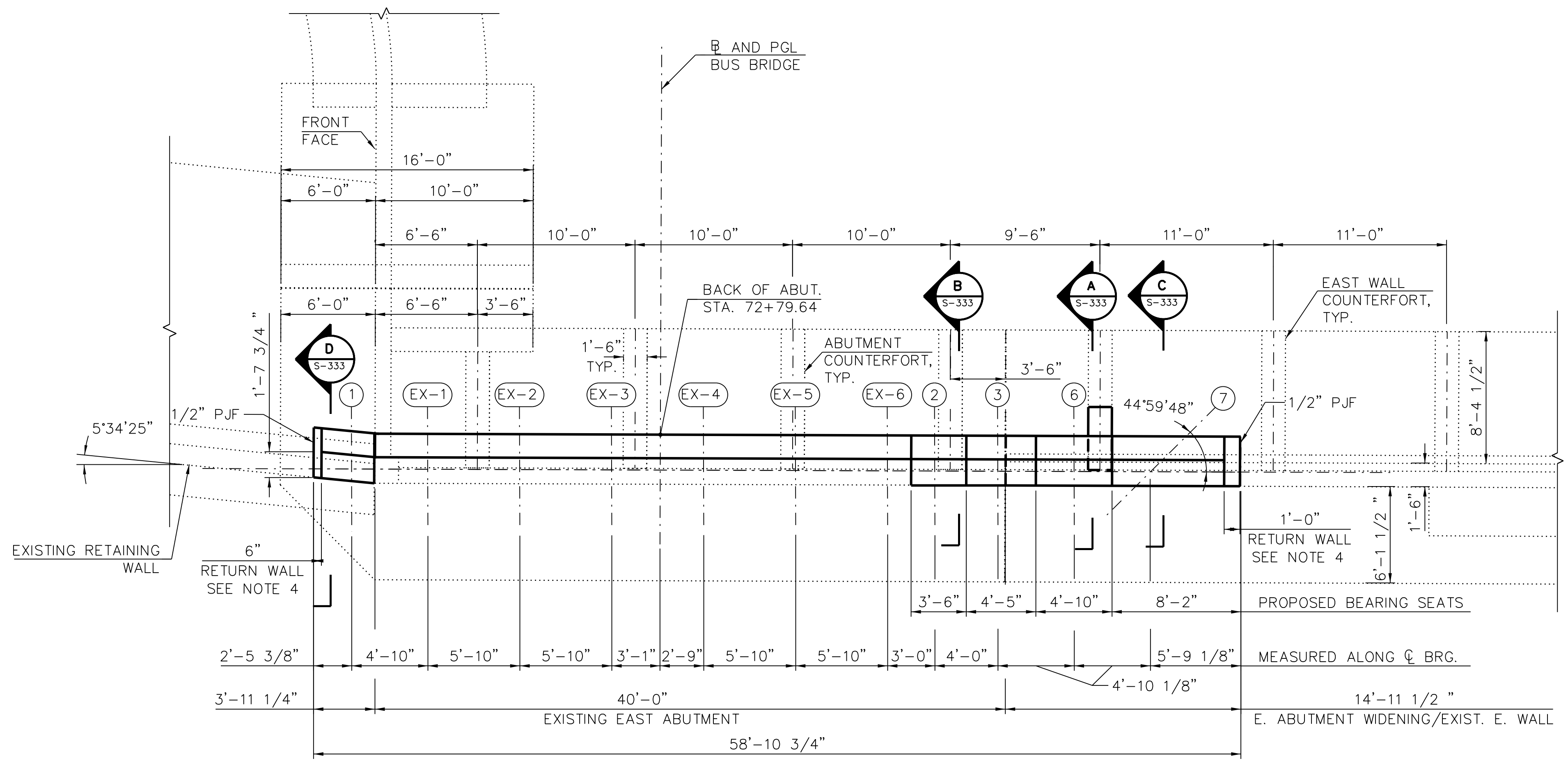


**3 DETAIL 3**  
S-331

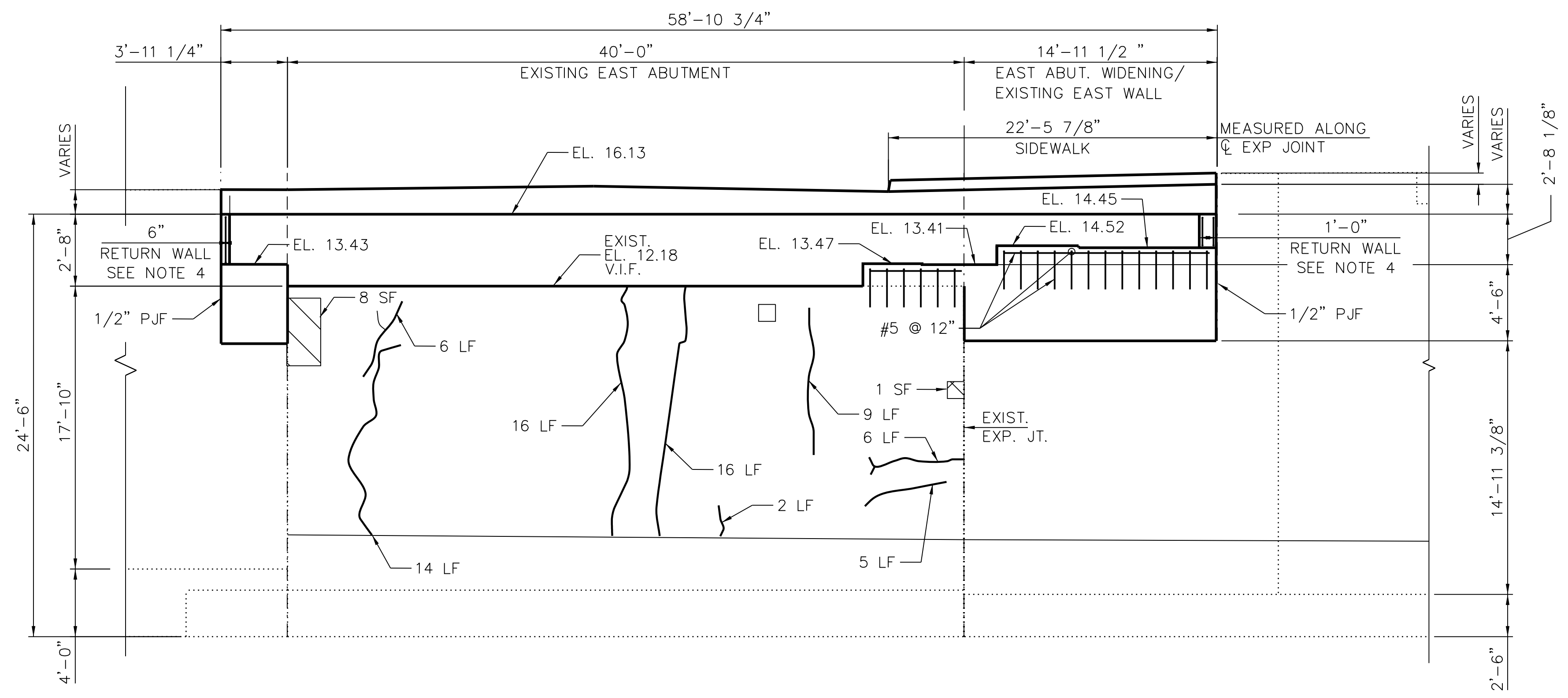
- NOTE:**
- FOR CELLULAR CONCRETE, SEE SHEETS NBB-S-334 AND NBB-S-335.
  - FOR DETAILS OF EXP. JT. SEE NBB-S-321.
  - EXISTING REINFORCEMENT SHALL BE CLEANED AND INCORPORATED INTO THE NEW CONSTRUCTION IN ACCORDANCE WITH IDOT STD. SPECIFICATION SECTION 501.05.
  - DRILLING AND GROUTING OF REINFORCEMENT INTO EXISTING CONCRETE SHALL BE IN ACCORDANCE WITH IDOT STD SPEC ARTICLE 584. BARS SHALL BE EMBEDDED A MINIMUM OF 9". BARS SHALL MAINTAIN A MINIMUM CLEARANCE OF 4" FROM THE FACE OF CONCRETE.
    - A. 7/8" Ø HOLE FOR #5 BARS
    - B. 1" Ø HOLE #6 BARS

FILE NAME = 070\_DR095\_S-331.dgn  
PLOT DATE = 5/24/2018

vscorp



1 EAST ABUTMENT AND WINGWALL PLAN



2 ELEVATION OF EAST ABUTMENT AND EAST WALL (LOOKING EAST)

NOTES:

- FOR REPAIRS IN EXISTING EAST WALL, SEE SHEETS EXE-S-404 THRU EXE-S-406.
- FOR STRUCTURAL REPAIR OF CONCRETE DETAILS, SEE SHEET 95TH-S-150A.
- SEE NBB-S-333 FOR REINFORCEMENT DETAILS.
- PROVIDE #4 @ 12" O.C. E.W. IN RETURN WALLS.

**NOT FOR CONSTRUCTION**



**CHICAGO TRANSIT AUTHORITY ENGINEERING**

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**95th Terminal Improvement**  
 14 West 95th Street (North Terminal)  
 Chicago, IL 60628

IN CHARGE	S. KADERBEK
APPROVED BY	D.ZROKA
CHECKED BY	P.MAID
DESIGNED BY	L.SOMMER
DRAWN BY	T.SLEDGE
PROJECT NO.	2012-0021
FILE NAME	File Name

MARK	DATE	DESCRIPTION
	05/23/18	FINAL DESIGN
	03/07/18	INTERMEDIATE DESIGN

LOCATION IDENTIFIER: **DR095**  
 95TH ST. NORTH BUS BRIDGE  
 EAST ABUTMENT PLAN  
 AND ELEVATION

**NBB-S-332**

LEGEND:

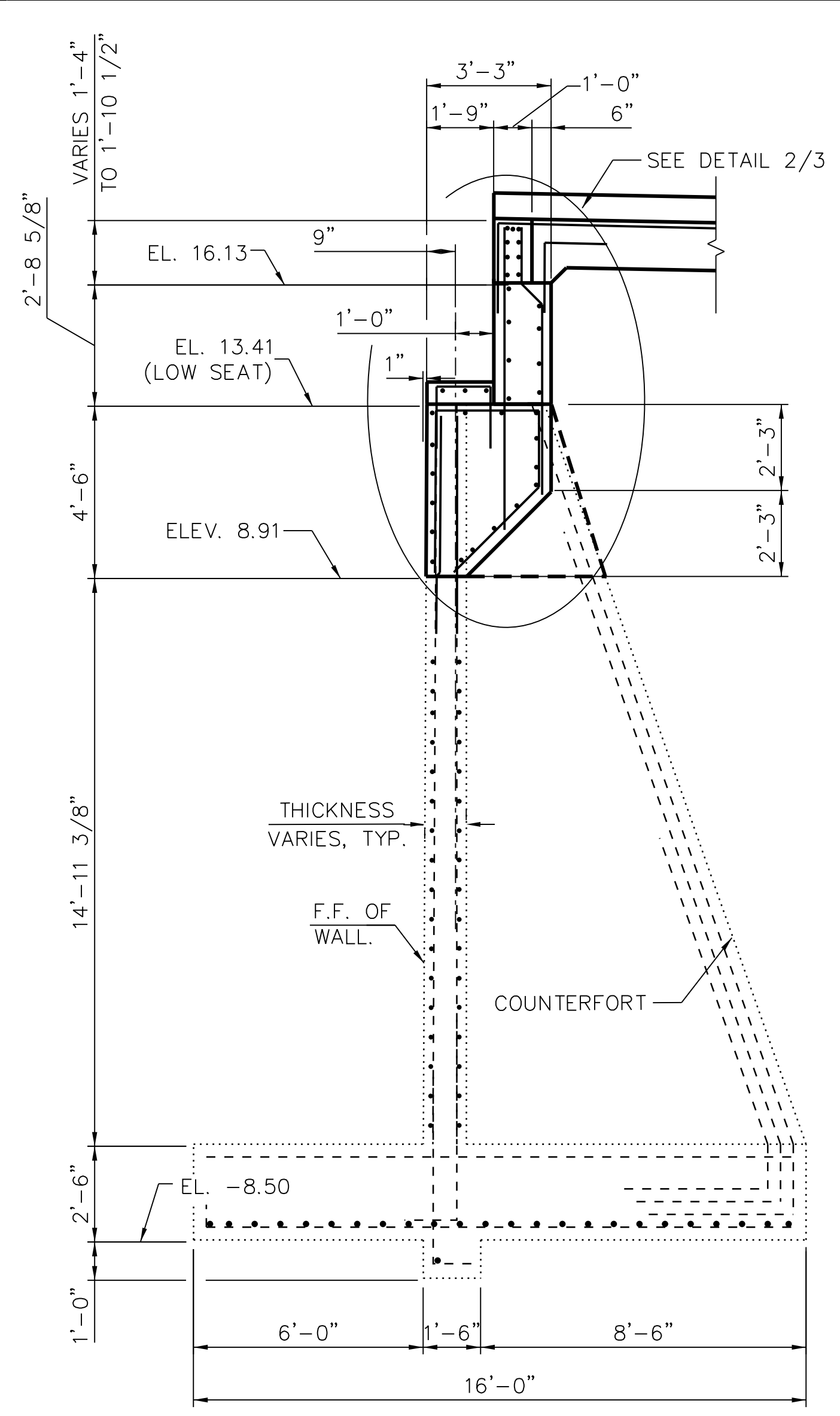
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- STRUCTURAL REPAIR OF CONCRETE (DEPTH EQUAL TO OR LESS THAN 5 INCHES)
- LOW PRESSURE EPOXY INJECTION (WIDTH >0.06")
- SF - SQUARE FOOT
- LF - LINEAR FOOT

FILE NAME = 070\_DR095\_S-332.dgn  
 PLOT DATE = 5/24/2018

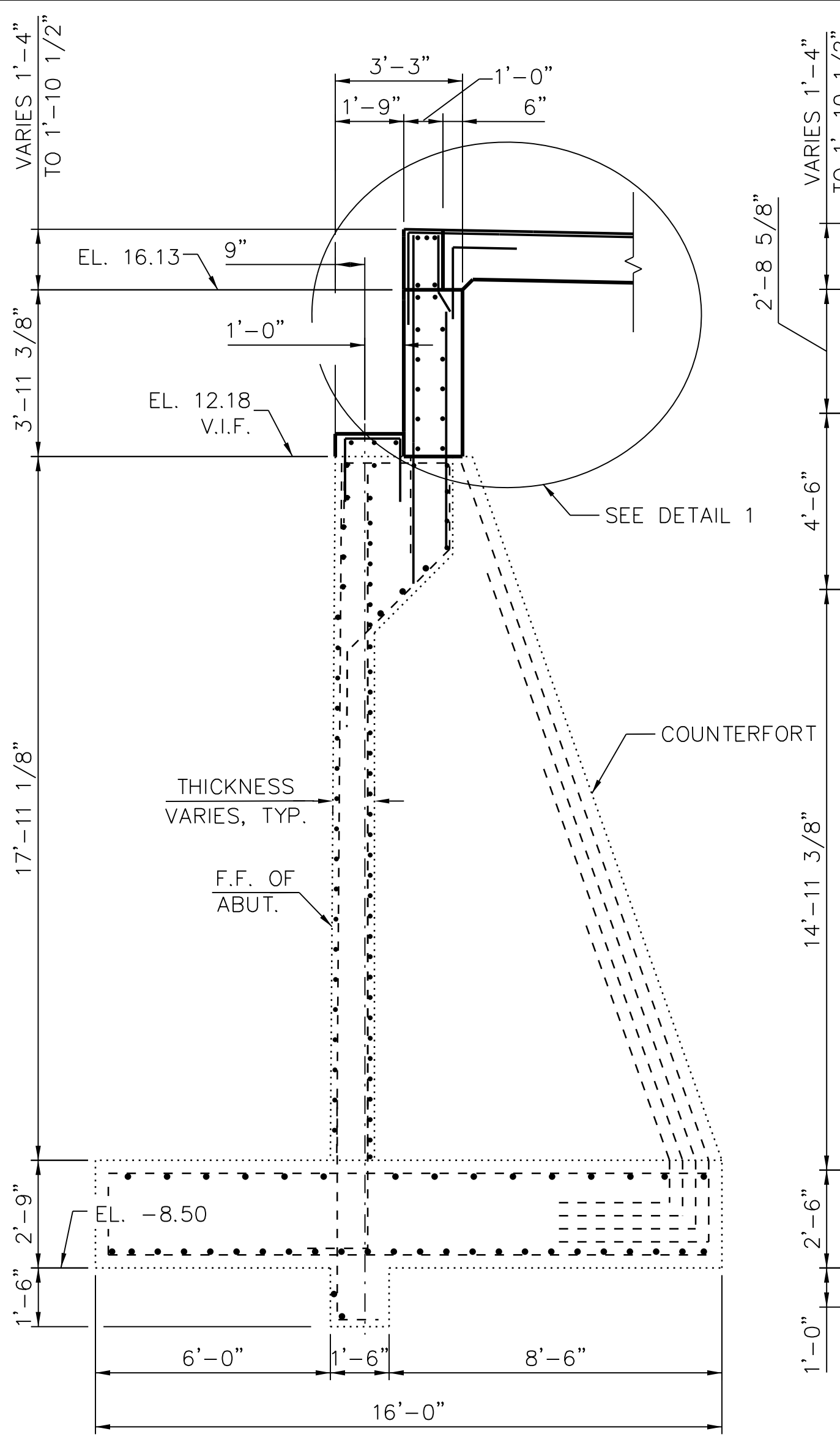


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 PLOT DATE = 5/24/2018

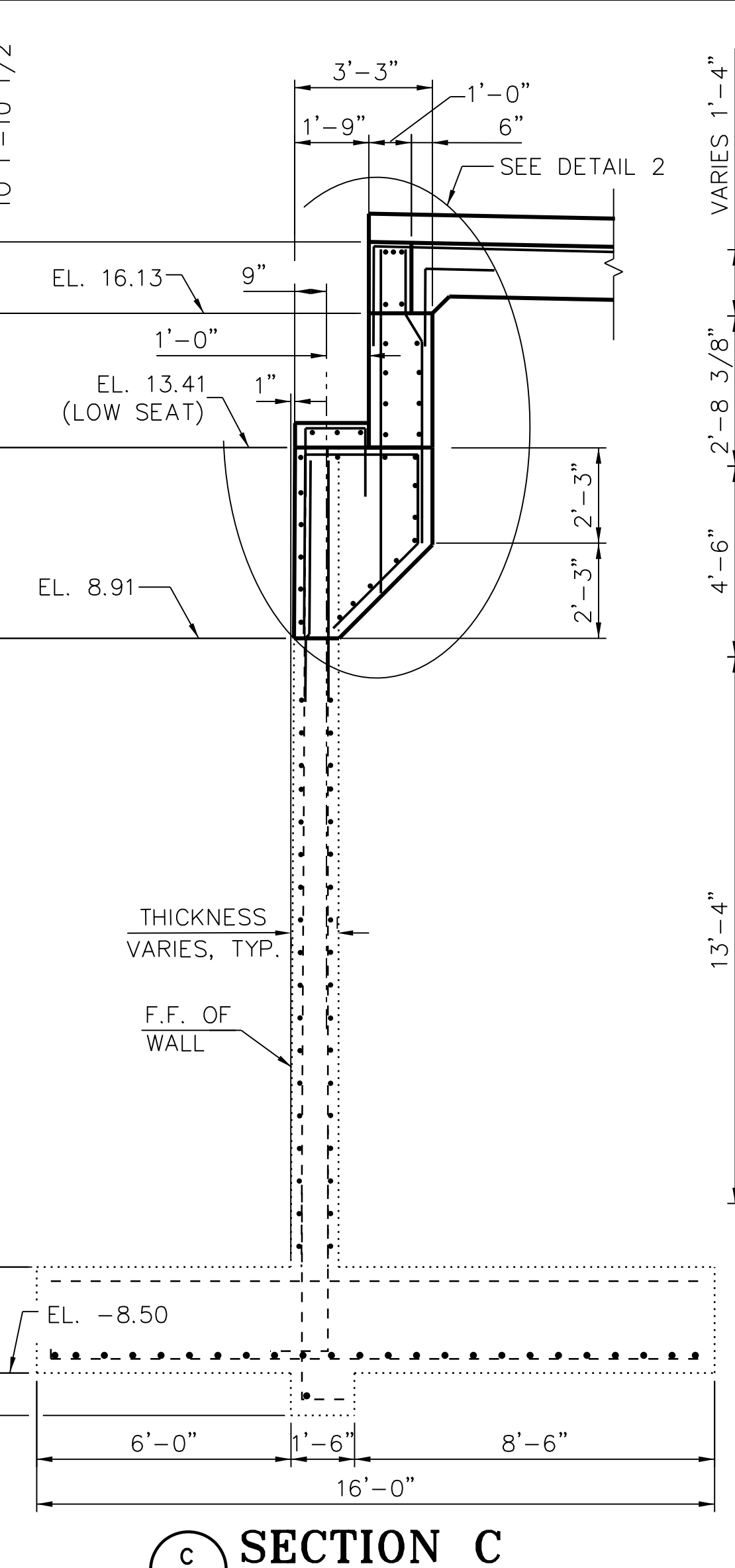
vscorppa



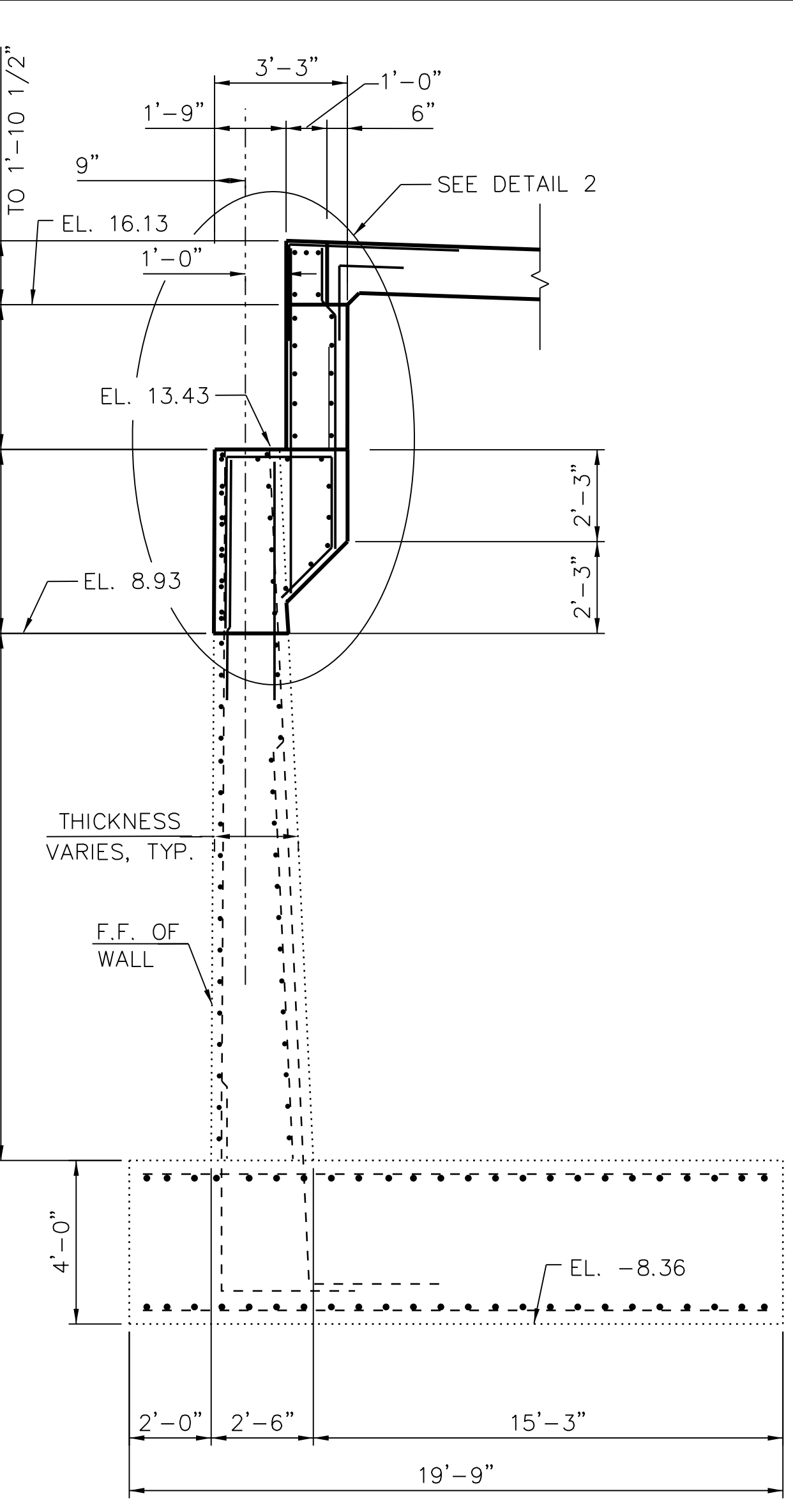
**A SECTION A**  
 (LOOKING NORTH)



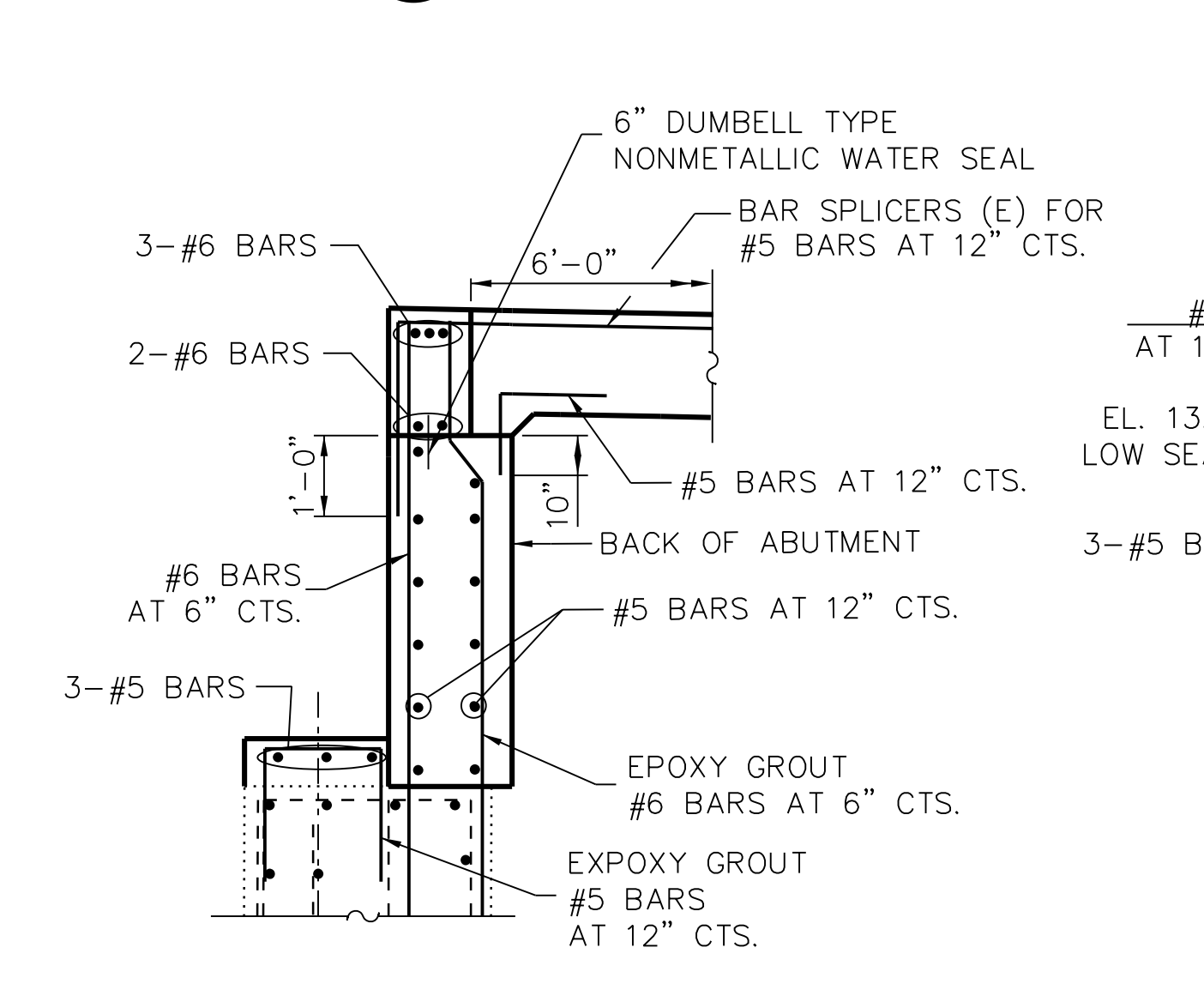
**B SECTION B**  
 (LOOKING NORTH)



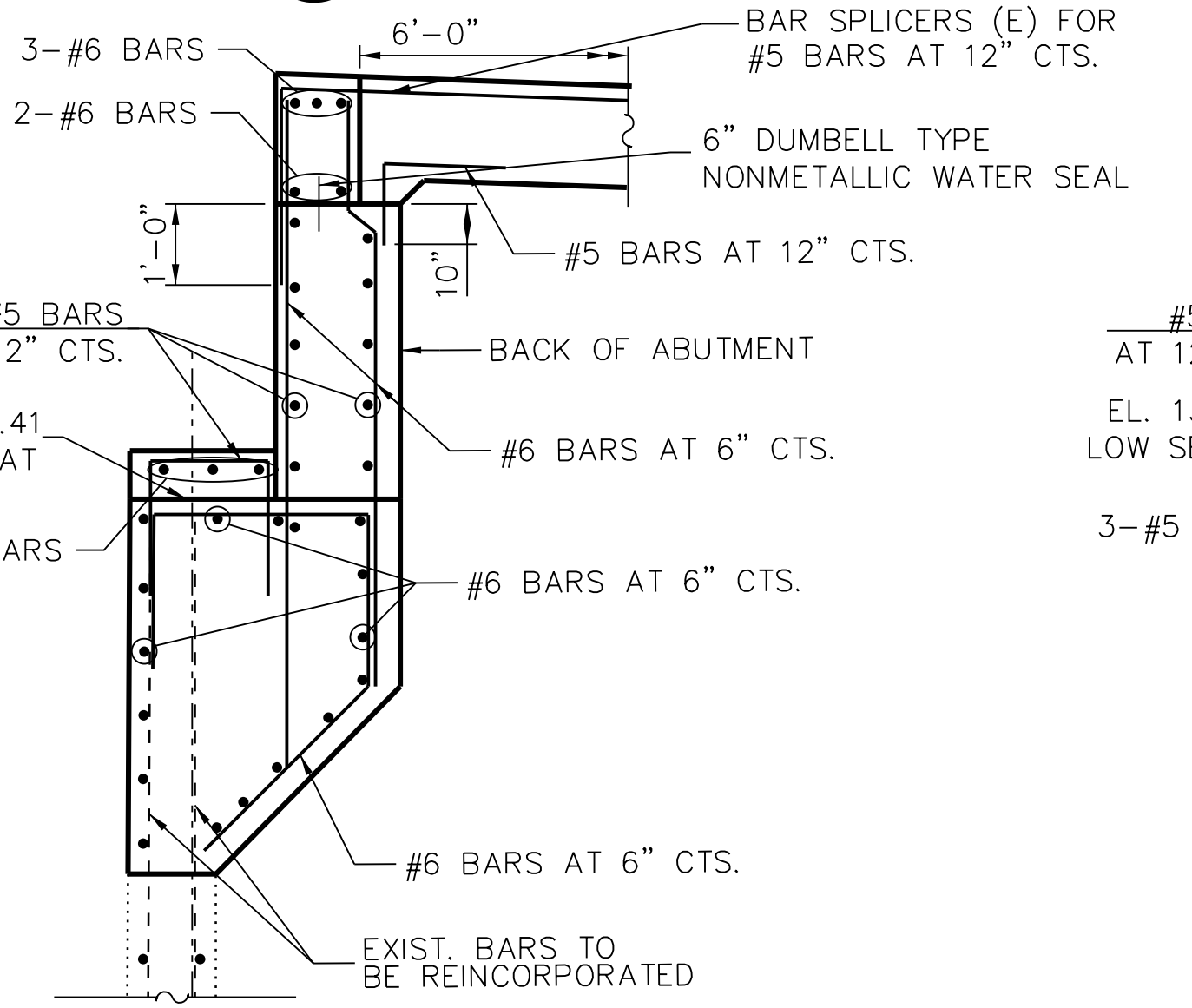
**C SECTION C**  
 (LOOKING NORTH)



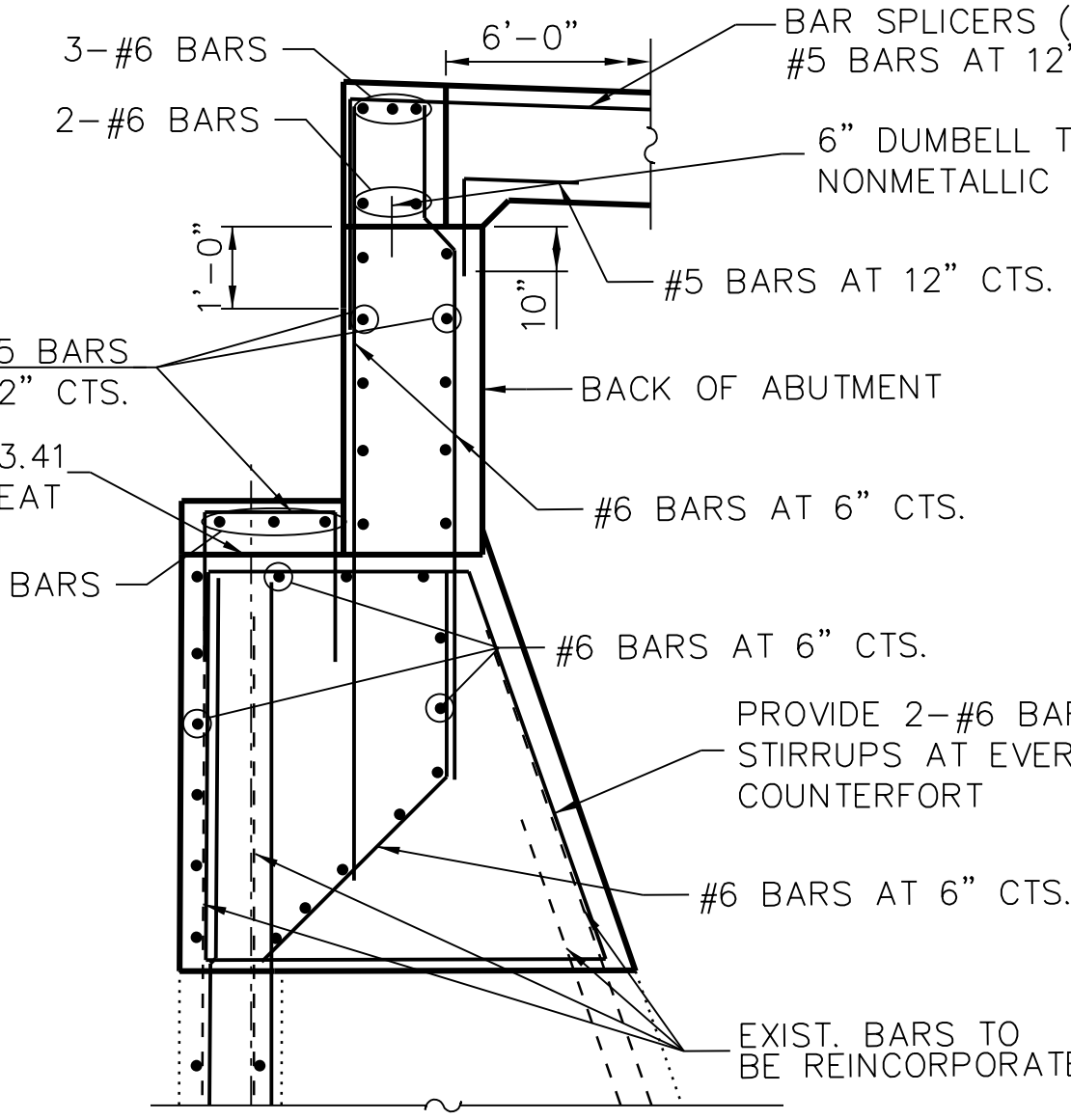
**D SECTION D**  
 (LOOKING NORTH)



**1 DETAIL 1**



**2 DETAIL 2**



**3 DETAIL 3**

- NOTE:**
- FOR CELLULAR CONCRETE, SEE SHEETS NBB-S-334 AND NBB-S-335.
  - FOR DETAILS OF EXP. JT. SEE NBB-S-321.
  - EXISTING REINFORCEMENT SHALL BE CLEANED AND INCORPORATED INTO THE NEW CONSTRUCTION IN ACCORDANCE WITH IDOT STD. SPECIFICATION SECTION 501.05.
  - DRILLING AND GROUTING OF REINFORCEMENT INTO EXISTING CONCRETE SHALL BE IN ACCORDANCE WITH IDOT STD SPEC ARTICLE 584. BARS SHALL BE EMBEDDED A MINIMUM OF 9". BARS SHALL MAINTAIN A MINIMUM CLEARANCE OF 4" FROM THE FACE OF CONCRETE.
    - A. 7/8" Ø HOLE FOR #5 BARS
    - B. 1" Ø HOLE #6 BARS

**NOT FOR CONSTRUCTION**



**CHICAGO TRANSIT AUTHORITY ENGINEERING**

**SENSITIVE SECURITY INFORMATION**  
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**95th Terminal Improvement**  
 14 West 95th Street (North Terminal)  
 Chicago, IL 60628

<b>IN CHARGE</b>	S. KADERBEK
<b>APPROVED BY</b>	D.ZROKA
<b>CHECKED BY</b>	P.MAID
<b>DESIGNED BY</b>	L.SOMMER
<b>DRAWN BY</b>	T.SLEDGE
<b>PROJECT NO.</b>	2012-0021
<b>FILE NAME</b>	File Name

MARK	DATE	DESCRIPTION
	05/23/18	FINAL DESIGN
	03/07/18	INTERMEDIATE DESIGN

**LOCATION IDENTIFIER: DR095**  
 95TH ST. NORTH BUS BRIDGE  
 EAST ABUTMENT SECTIONS  
 AND DETAILS

**NBB-S-333**

**NOT FOR CONSTRUCTION**



**CHICAGO TRANSIT AUTHORITY ENGINEERING**

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**95th Terminal Improvement**  
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 Chicago, IL 60628

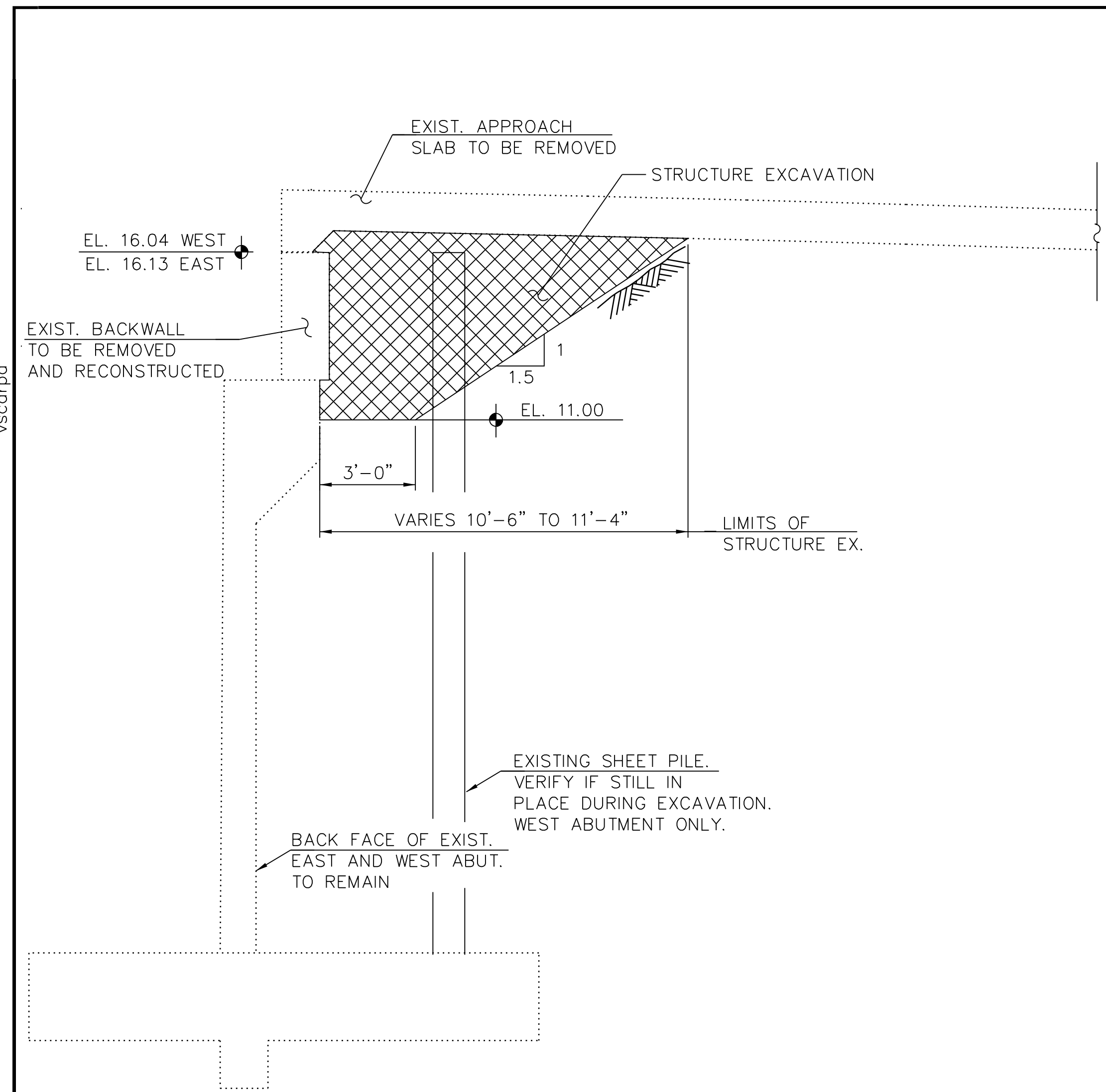
IN CHARGE	S. KADERBEK
APPROVED BY	D.ZROKA
CHECKED BY	P.MAID
DESIGNED BY	L.SOMMER
DRAWN BY	T.SLEDGE
PROJECT NO.	2012-0021
FILE NAME	File Name


MARK	DATE	DESCRIPTION
	05/23/18	FINAL DESIGN
	03/07/18	INTERMEDIATE DESIGN

LOCATION IDENTIFIER: **DR095**

**95TH ST. NORTH BUS BRIDGE  
 EAST & WEST ABUTMENT  
 EXCAVATION & CELLULAR CONCRETE**

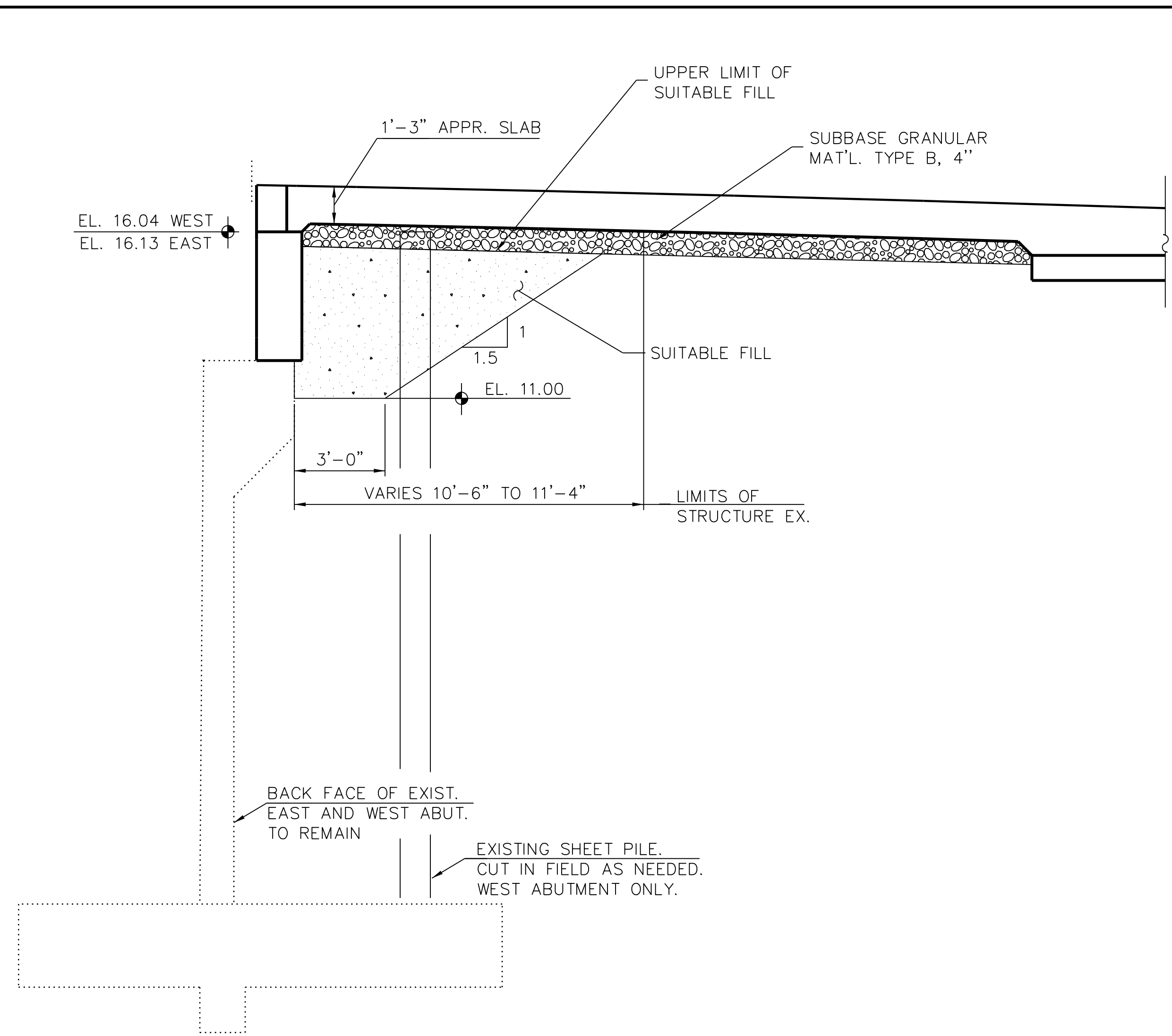
**NBB-S-334**



**B SECTION B - EXCAVATION**  
 (WEST ABUTMENT - LOOKING SOUTH)  
 (EAST ABUTMENT - LOOKING NORTH)

FILE NAME = 070\_DR095\_S-334.dgn  
 PLOT DATE = 5/24/2018

- SUGGESTED CONSTRUCTION SEQUENCE:
1. REMOVE EXISTING APPROACH SLAB AS SHOWN ON SHEET NBB-S-303.
  2. EXCAVATE BEHIND EXISTING RETAINING WALL TO THE LIMITS SHOWN ON THE PLANS.
  3. REMOVE BACKWALL AS SHOWN ON SHEETS NBB-S-303 AND NBB-S-304.
  4. RECONSTRUCT BACKWALL AS SHOWN ON SHEETS NBB-S-330 AND NBB-S-332.
  5. BACKFILL WITH APPROVED SUITABLE FILL.
  6. BACKFILL WITH SUBBASE GRANULAR MAT'L. TYPE B, 4".
  7. CONSTRUCT APPROACH SLAB, CLOSURE POUR AND SIDEWALK.

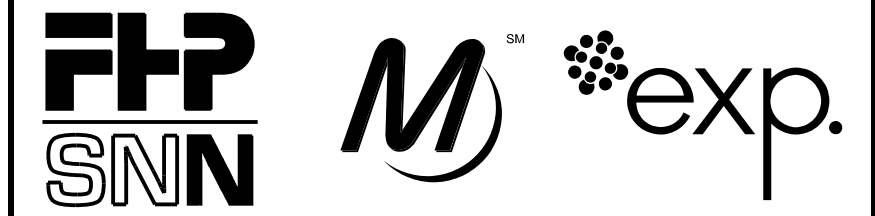


**B SECTION B - SUITABLE FILL**  
 (WEST ABUTMENT - LOOKING SOUTH)  
 (EAST ABUTMENT - LOOKING NORTH)

LEGEND:

	STRUCTURE EXCAVATION
	SUITABLE FILL

**NOT FOR CONSTRUCTION**



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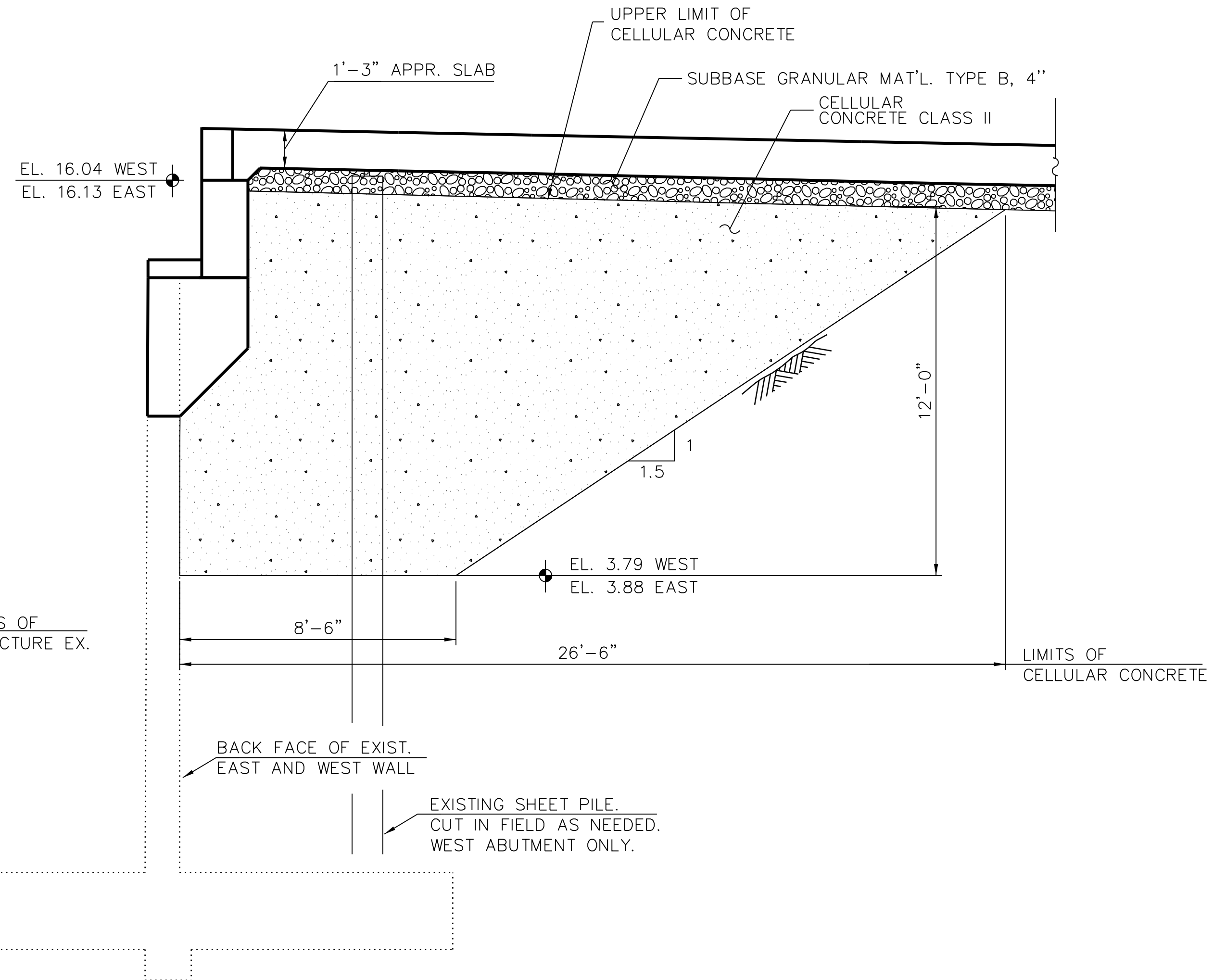
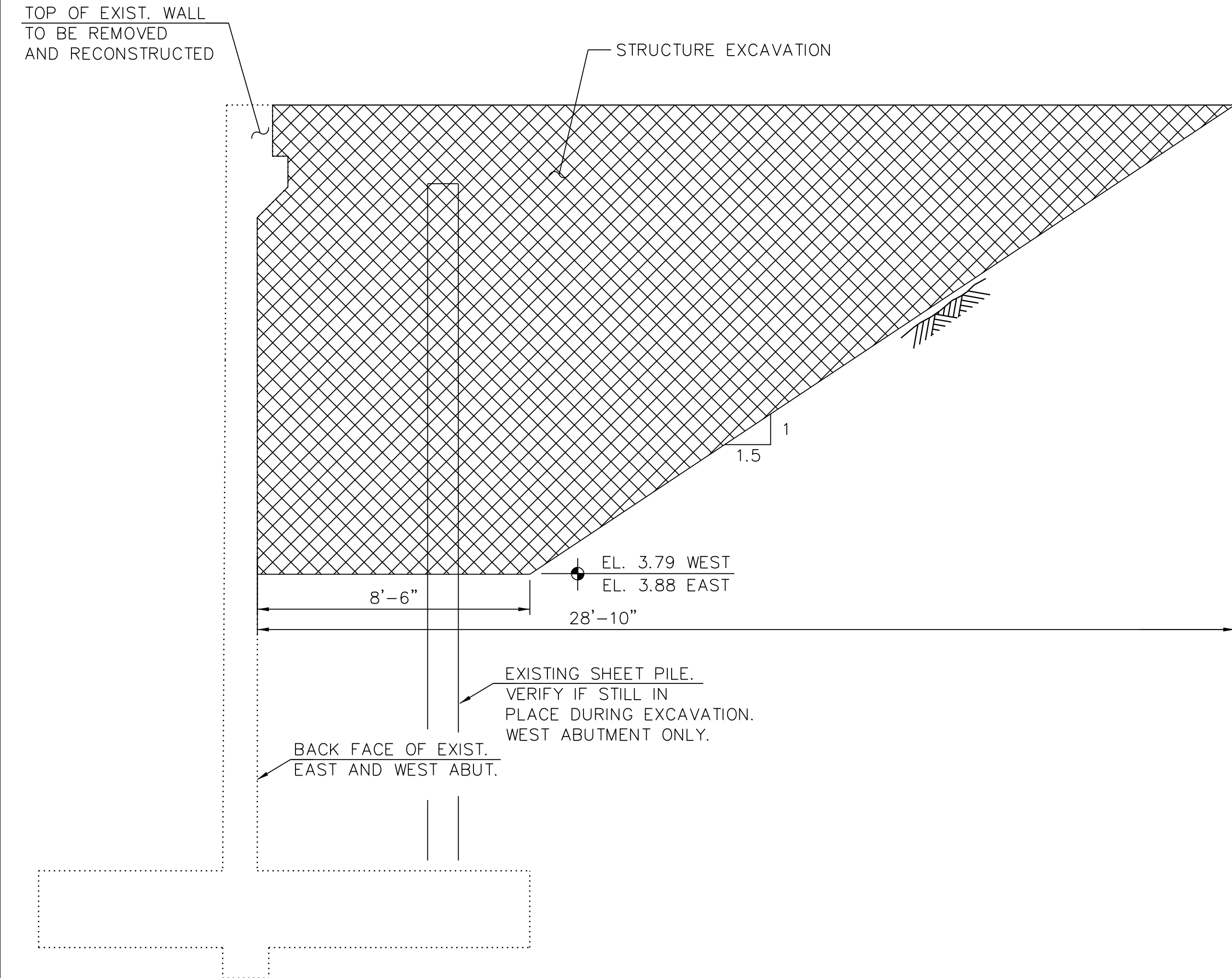
IN CHARGE	S. KADERBEK
APPROVED BY	D.ZROKA
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DESIGNED BY	L.SOMMER
DRAWN BY	T.SLEDGE
PROJECT NO.	2012-0021
FILE NAME	File Name

05/23/18	FINAL DESIGN
03/07/18	INTERMEDIATE DESIGN

MARK	DATE	DESCRIPTION
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LOCATION IDENTIFIER: **DR095**  
 95TH ST. NORTH BUS BRIDGE  
 EAST & WEST EXISTING WALL  
 EXCAVATION & CELLULAR CONCRETE

**NBB-S-335**



**SECTION A, C, & D - EXCAVATION**

(WEST ABUTMENT - LOOKING SOUTH)  
 (EAST ABUTMENT - LOOKING NORTH)

**SUGGESTED CONSTRUCTION SEQUENCE:**

1. REMOVE EXISTING PAVEMENT. (SEE CIVIL PLANS).
2. EXCAVATE BEHIND EXISTING RETAINING WALL TO THE LIMITS SHOWN ON THE PLANS.
3. REMOVE PORTION OF RETAINING WALL SHOWN ON SHEETS NBB-S-303 AND NBB-S-304.
4. RECONSTRUCT BACKWALL AS SHOWN ON SHEETS NBB-S-330 AND NBB-S-332.
5. BACKFILL WITH APPROVED CELLULAR CONCRETE IN LIFTS NOT EXCEEDING FOUR (4) FEET.
6. BACKFILL WITH SUBBASE GRANULAR MAT'L. TYPE B, 4".
7. CONSTRUCT APPROACH SLAB, CLOSURE POUR AND SIDEWALK.

**SECTION A, C, & D - CELLULAR CONCRETE**

(WEST ABUTMENT - LOOKING SOUTH)  
 (EAST ABUTMENT - LOOKING NORTH)

**NOTES:**

1. CELLULAR CONCRETE SHALL BE PLACED IN LAYERS NOT EXCEEDING 4 FEET IN THICKNESS.

**LEGEND:**

- STRUCTURE EXCAVATION
- CELLULAR CONCRETE

vscorppa

FILE NAME = 070\_DR095\_S-335.dgn  
 PLOT DATE = 5/24/2018

**NOT FOR CONSTRUCTION**



**CHICAGO TRANSIT AUTHORITY ENGINEERING**

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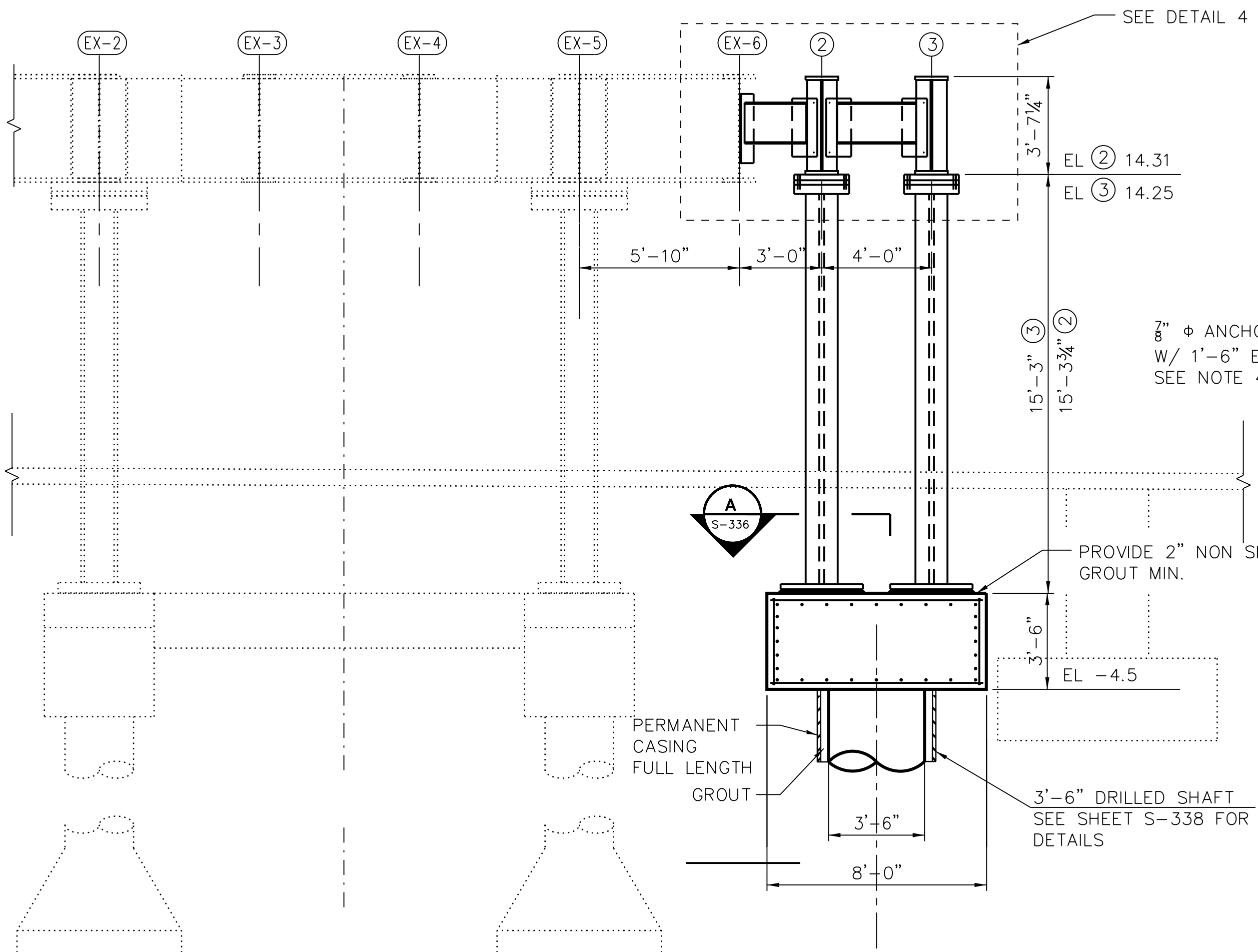
<b>IN CHARGE</b>	S. KADERBEK
<b>APPROVED BY</b>	D.ZROKA
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<b>FILE NAME</b>	File Name

<b>MARK</b>	<b>DATE</b>	<b>DESCRIPTION</b>
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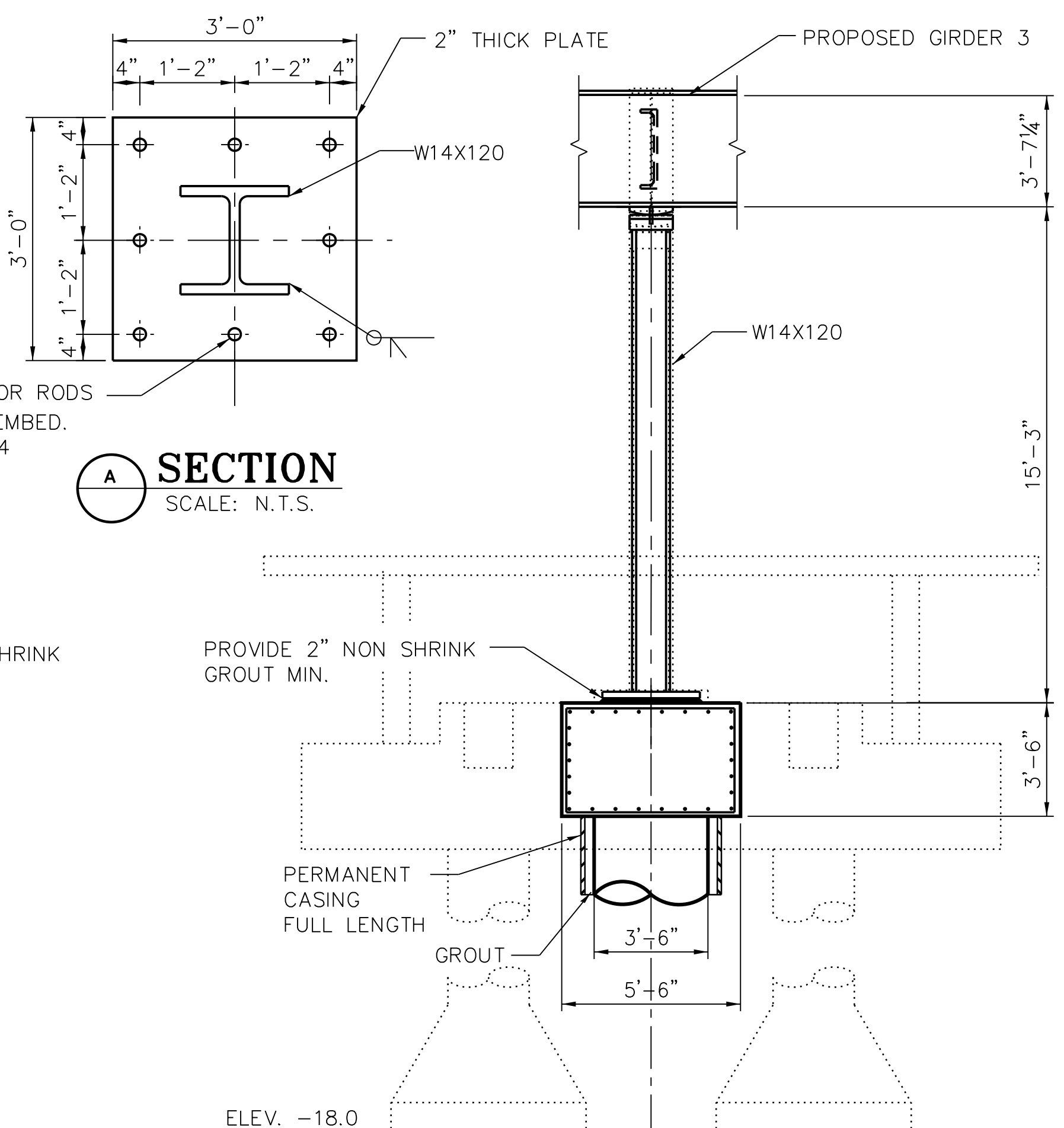
LOCATION IDENTIFIER: **DR095**

**95TH ST. NORTH BUS BRIDGE  
PIER PLAN AND ELEVATION  
(SHEET 1 OF 2)**

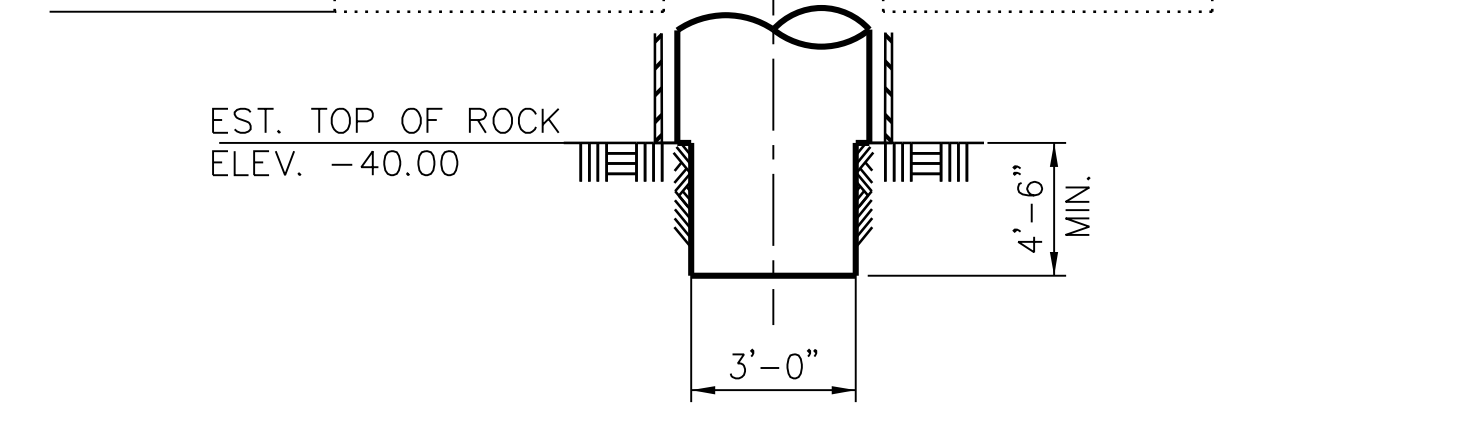
**NBB-S-336**



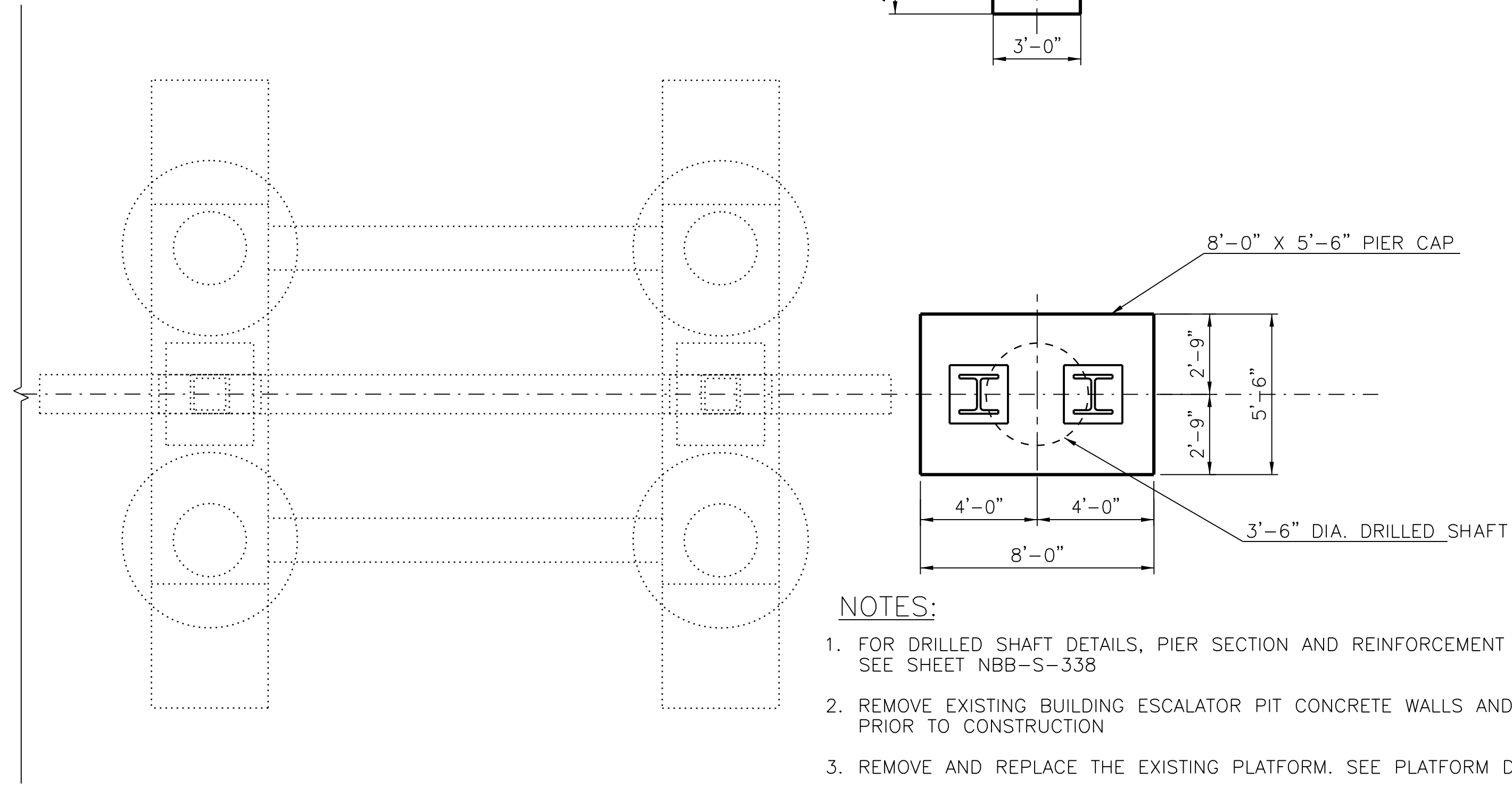
**1 ELEVATION**  
(LOOKING EAST @ SOUTH END)



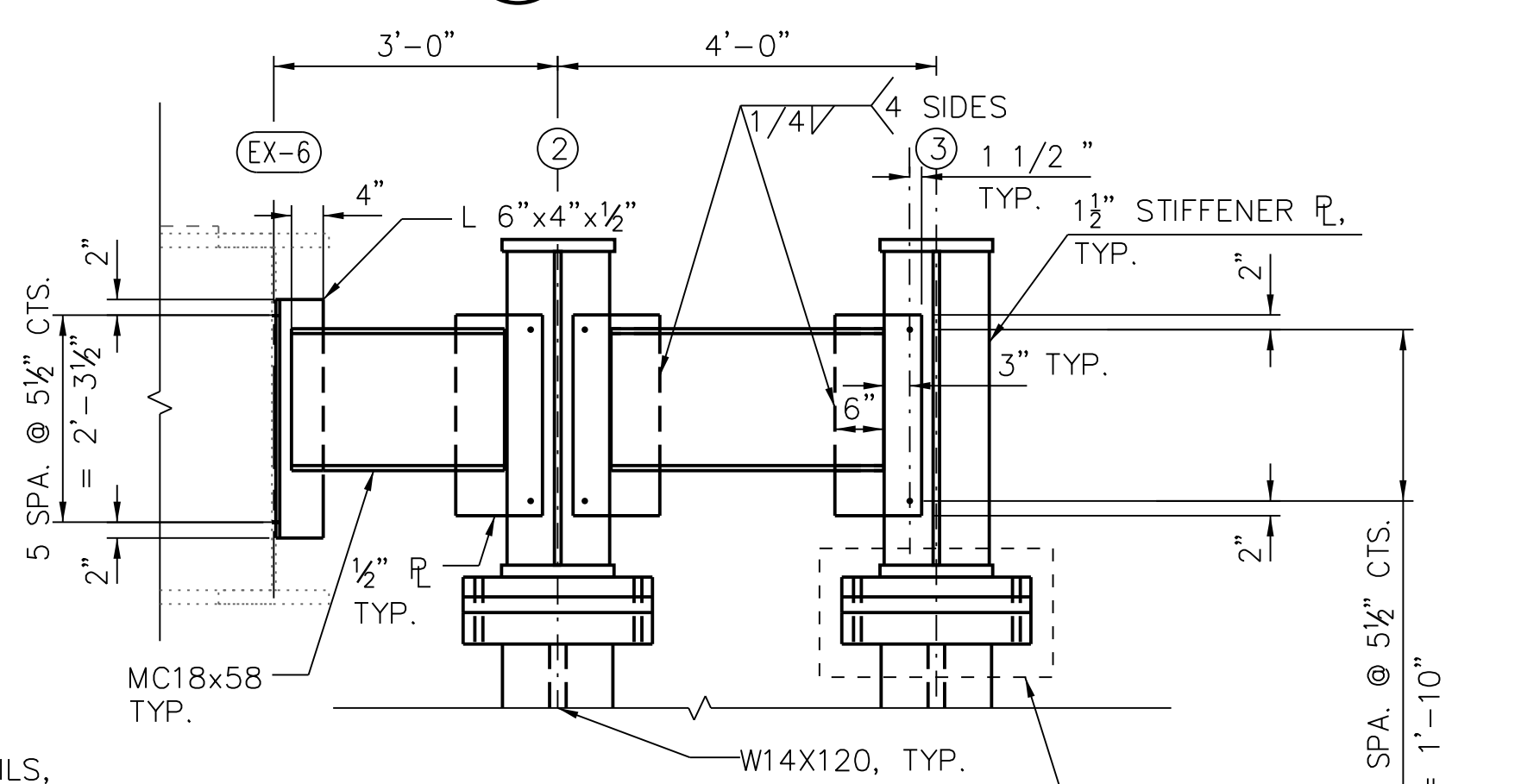
**A SECTION**  
SCALE: N.T.S.



**2 END VIEW**  
(LOOKING NORTH)



**3 PLAN**



**4 DETAIL**  
(LOOKING EAST)

- NOTES:**
- FOR DRILLED SHAFT DETAILS, PIER SECTION AND REINFORCEMENT DETAILS, SEE SHEET NBB-S-338
  - REMOVE EXISTING BUILDING ESCALATOR PIT CONCRETE WALLS AND SLAB PRIOR TO CONSTRUCTION
  - REMOVE AND REPLACE THE EXISTING PLATFORM. SEE PLATFORM DRAWINGS.
  - PROVIDE HEX NUT AND PLATE WASHER AT TOP AND BOTTOM OF ANCHOR RODS.

FACTORED RESISTANCE FOR DRILLED SHAFTS D1				
SHAFT DIA (FT.)	MIN. ROCK SOCKET LENGTH (FT.)	ROCK SOCKET DIA (FT.)	FACTORED RESISTANCE (K)	ALLOWABLE BEARING PRESSURE (KSF)
3'-6"	4'-6" (MIN.)	3'-0"	2600	370

FILE NAME = 070\_DR095\_S-336.dgn PLOT DATE = 5/24/2018 vscarppa

**NOT FOR CONSTRUCTION**



**CHICAGO TRANSIT AUTHORITY ENGINEERING**

**SENSITIVE SECURITY INFORMATION**

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**95th Terminal Improvement**  
14 West 95th Street (North Terminal)  
Chicago, IL 60628

IN CHARGE	S. KADERBEK
APPROVED BY	D.ZROKA
CHECKED BY	P.MAID
DESIGNED BY	L.SOMMER
DRAWN BY	T.SLEDGE
PROJECT NO.	2012-0021
FILE NAME	File Name

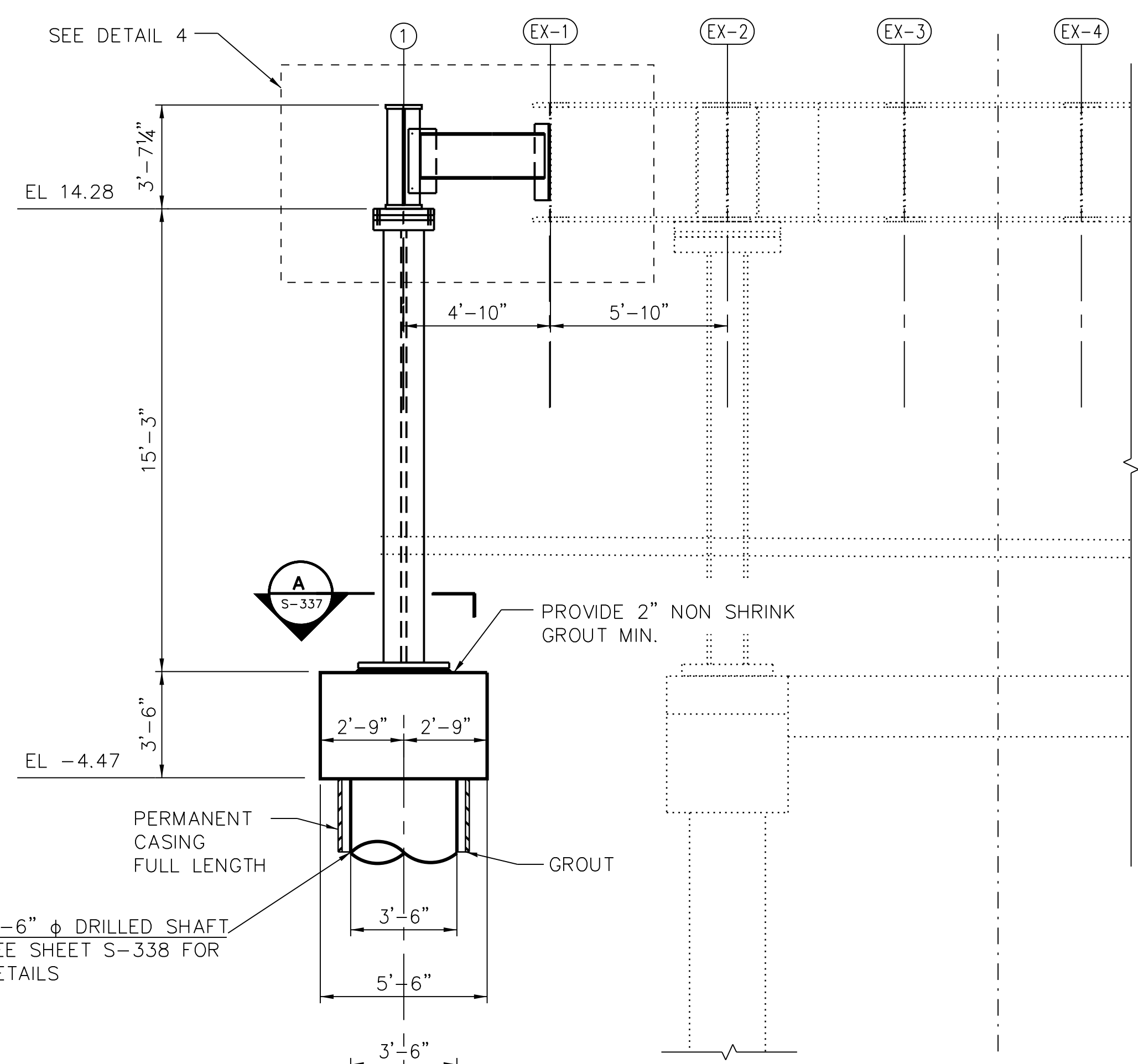
MARK	DATE	DESCRIPTION
	05/23/18	FINAL DESIGN
	03/07/18	INTERMEDIATE DESIGN

LOCATION IDENTIFIER: **DR095**

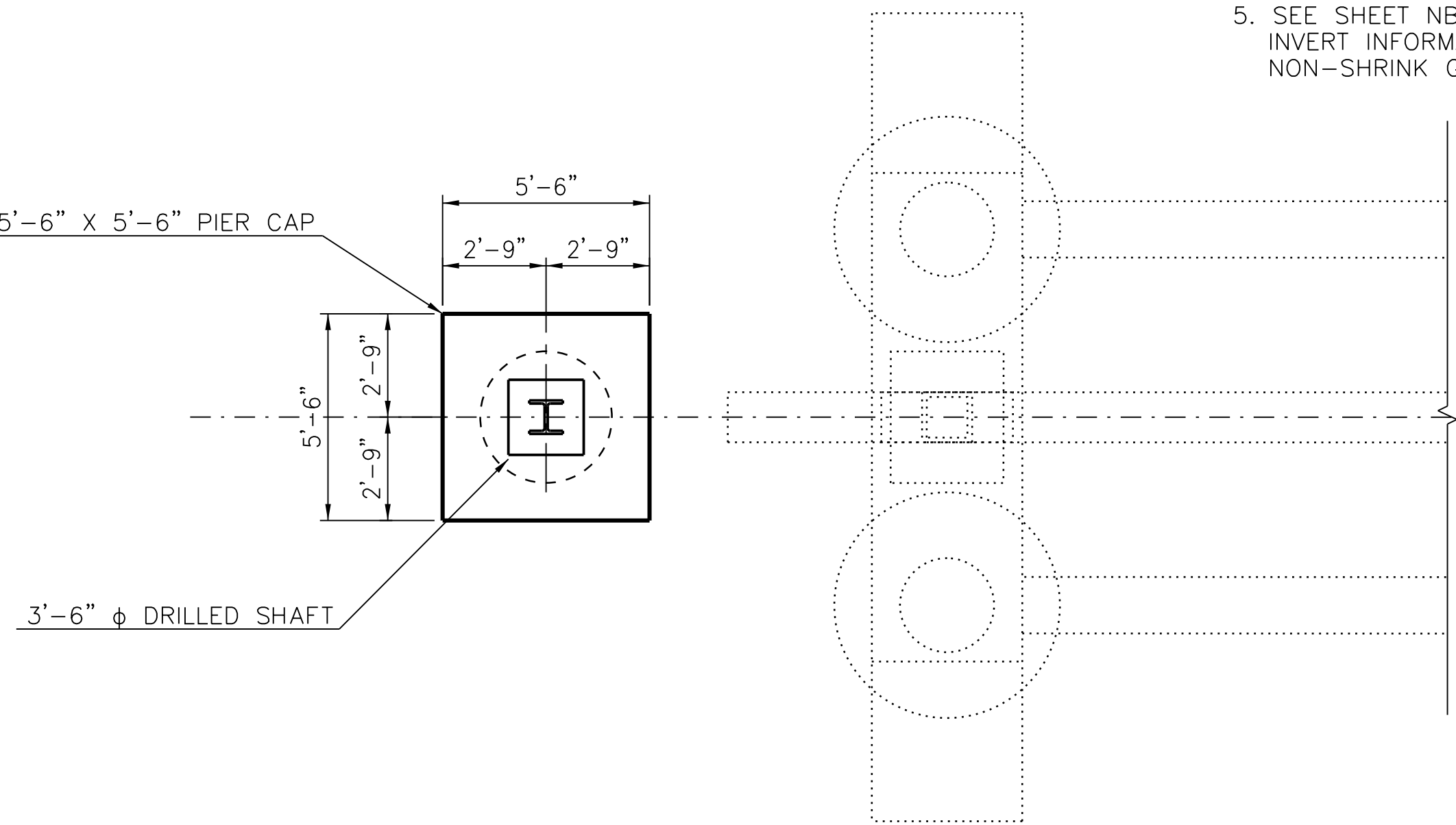
95TH ST. NORTH BUS BRIDGE  
PIER PLAN AND ELEVATION  
(SHEET 2 OF 2)

**NBB-S-337**

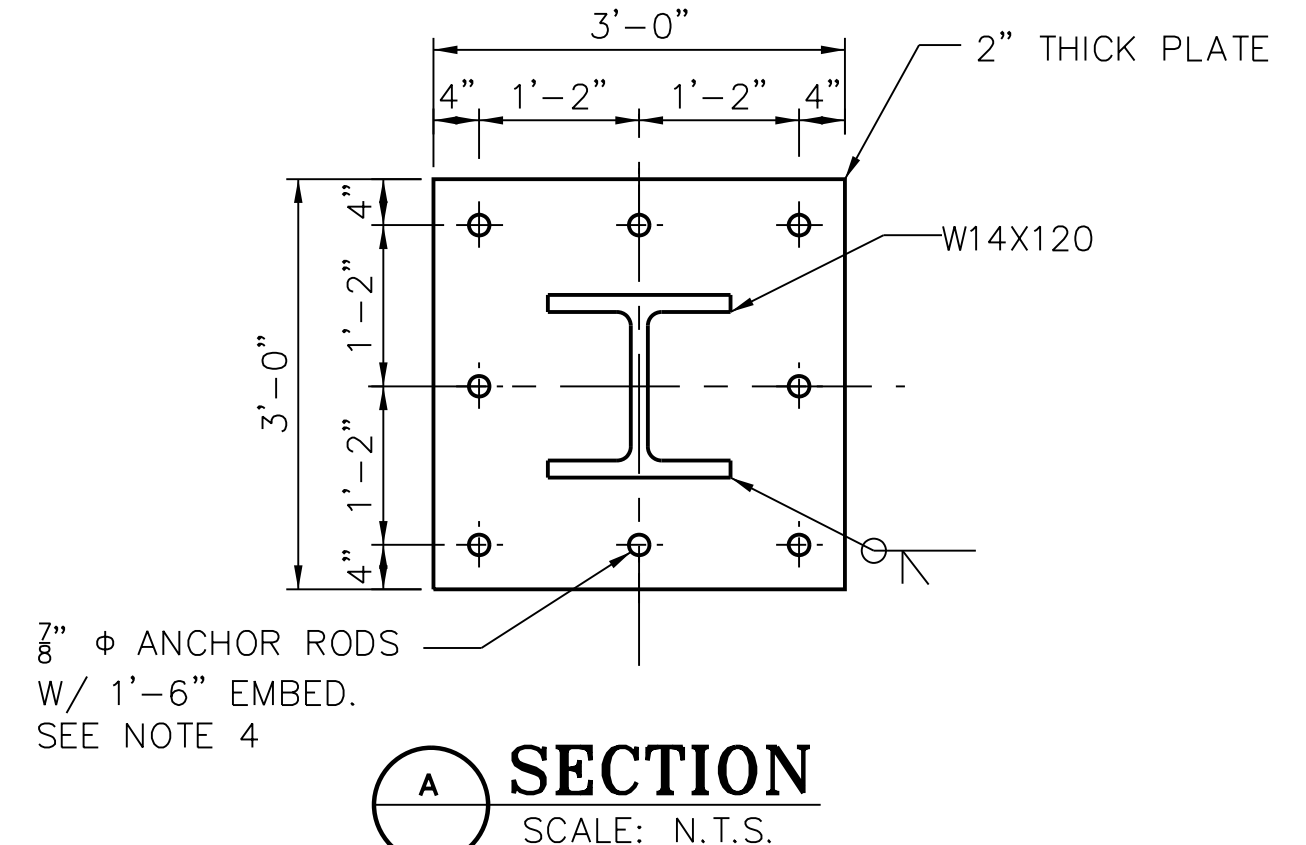
FILE NAME = 070\_DR095\_S-337.dgn  
PLOT DATE = 5/24/2018



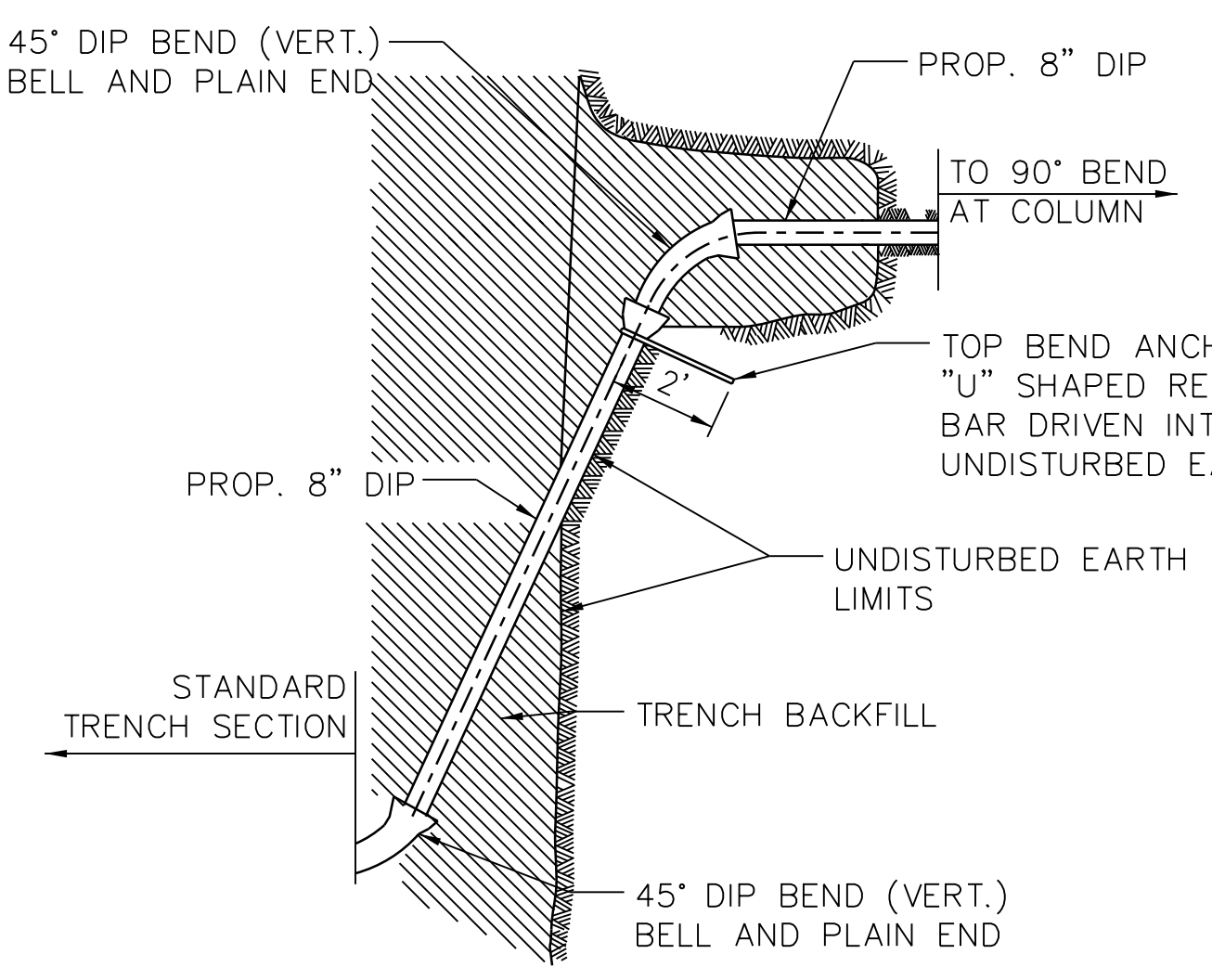
**1 ELEVATION**  
(LOOKING EAST @ NORTH END)



**3 PLAN**

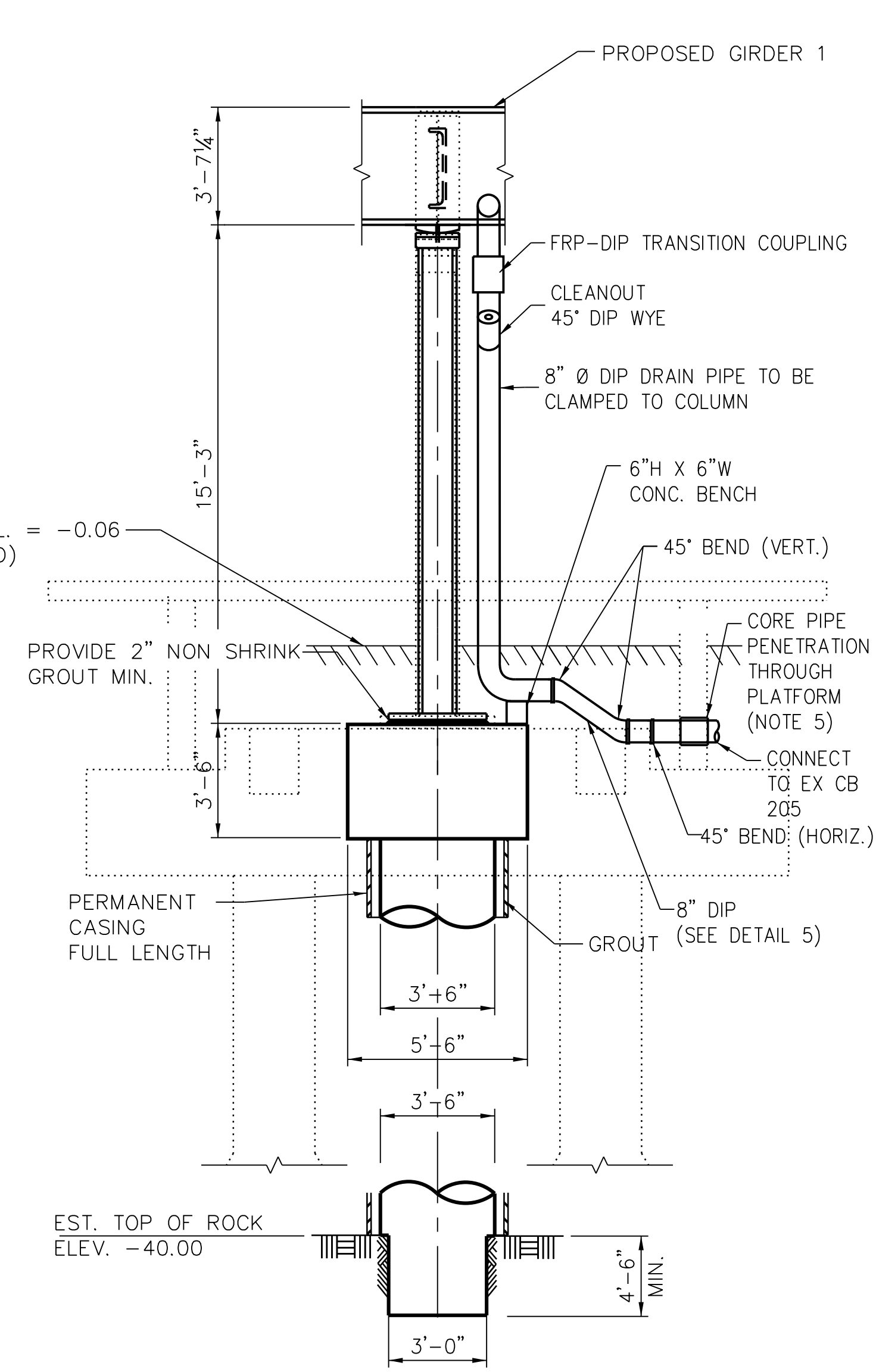


**A SECTION**  
SCALE: N.T.S.

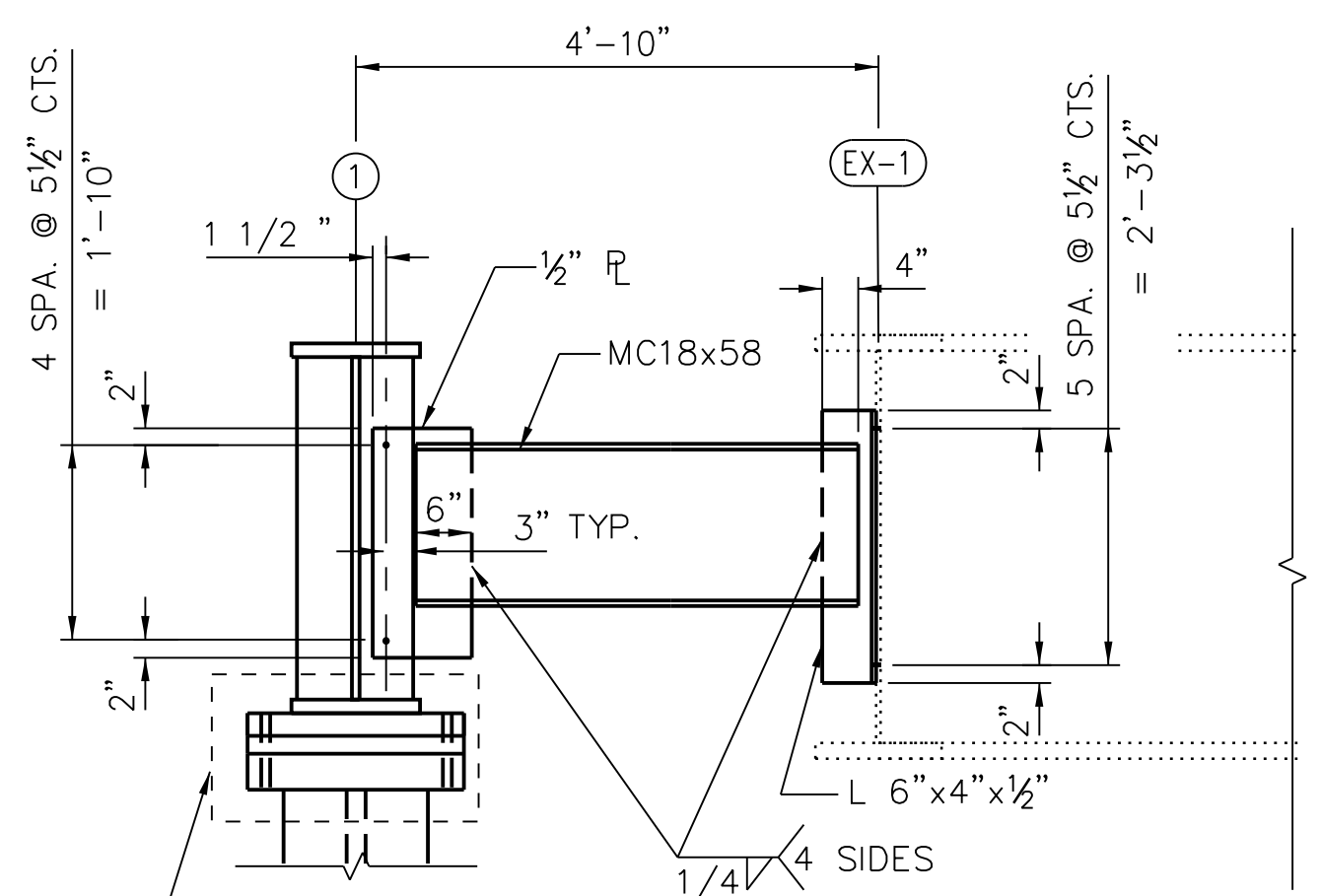


**5 PIPE SUPPORT DETAIL**

- NOTES:**
- FOR DRILLED SHAFT DETAILS, PIER SECTION AND REINFORCEMENT DETAILS, SEE SHEET NBB-S-338
  - REMOVE EXISTING BUILDING ESCALATOR PIT CONCRETE WALLS AND SLAB PRIOR TO CONSTRUCTION
  - REMOVE AND REPLACE THE EXISTING PLATFORM. SEE PLATFORM DRAWINGS.
  - PROVIDE HEX NUT AND PLATE WASHER AT TOP AND BOTTOM OF ANCHOR RODS.
  - SEE SHEET NBB-S-305 FOR ADDITIONAL DRAINAGE ROUTING AND PIPE INVERT INFORMATION. SEAL 12" PIPE PENETRATION WITH 1" MIN. NON-SHRINK GROUT.



**2 END VIEW**  
(LOOKING SOUTH)

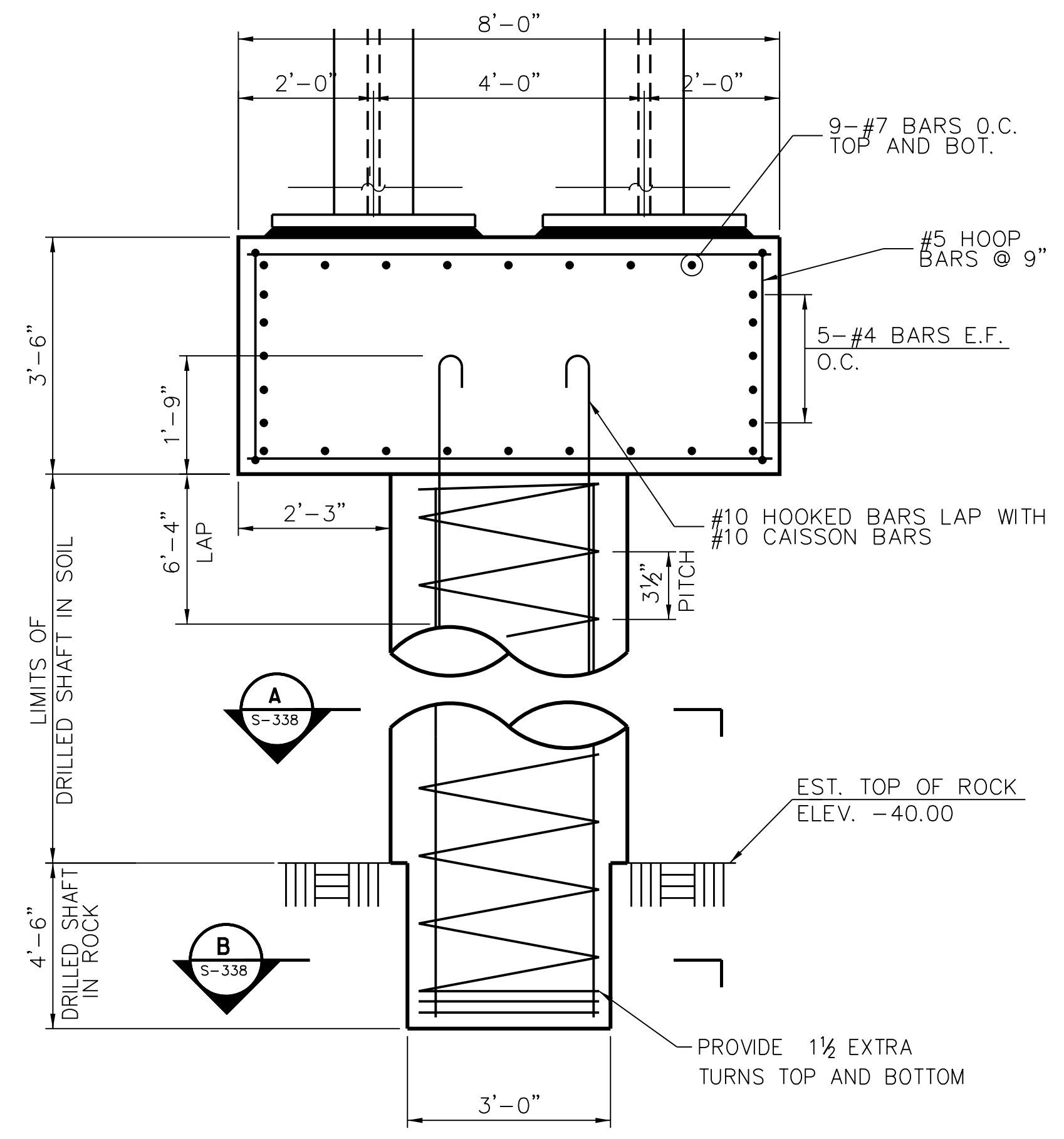


**4 DETAIL**  
(LOOKING EAST)

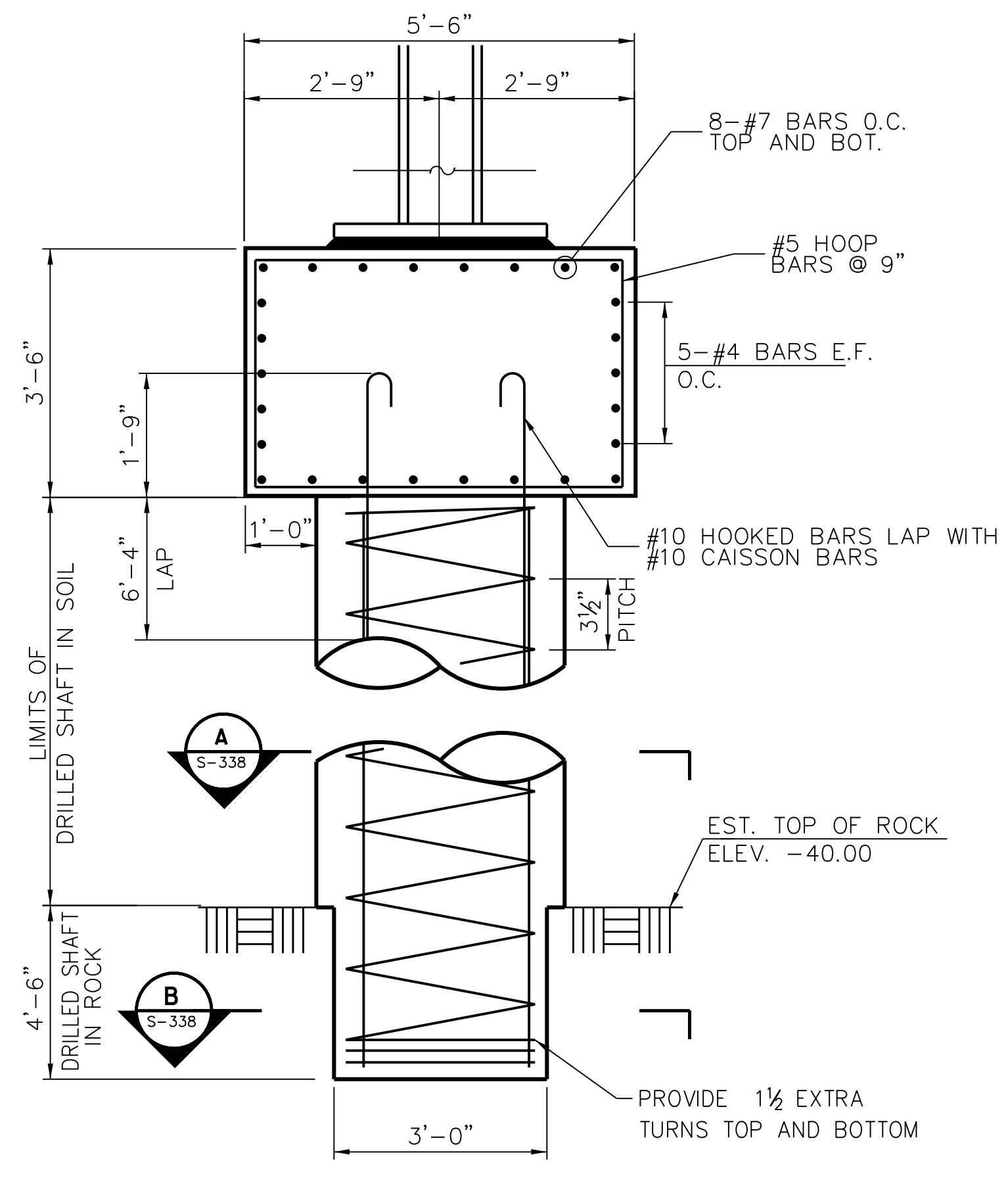
SHAFT DIA (FT.)	MIN. ROCK SOCKET LENGTH (FT.)	ROCK SOCKET DIA (FT)	FACTORED RESISTANCE (K)	ALLOWABLE BEARING PRESSURE (KSF)
3'-6"	4'-6" (MIN.)	3'-0"	2600	370



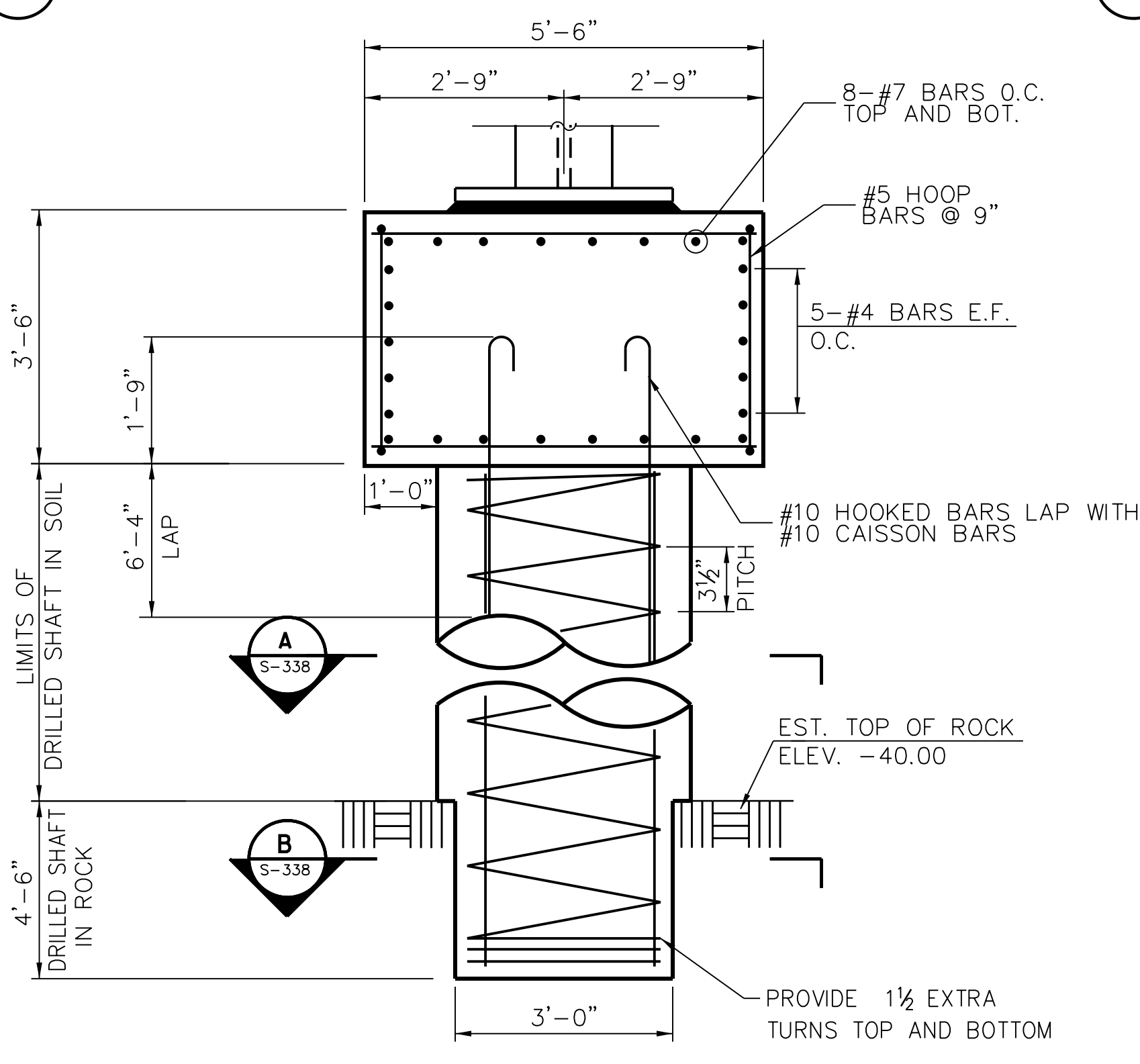
vscorppa



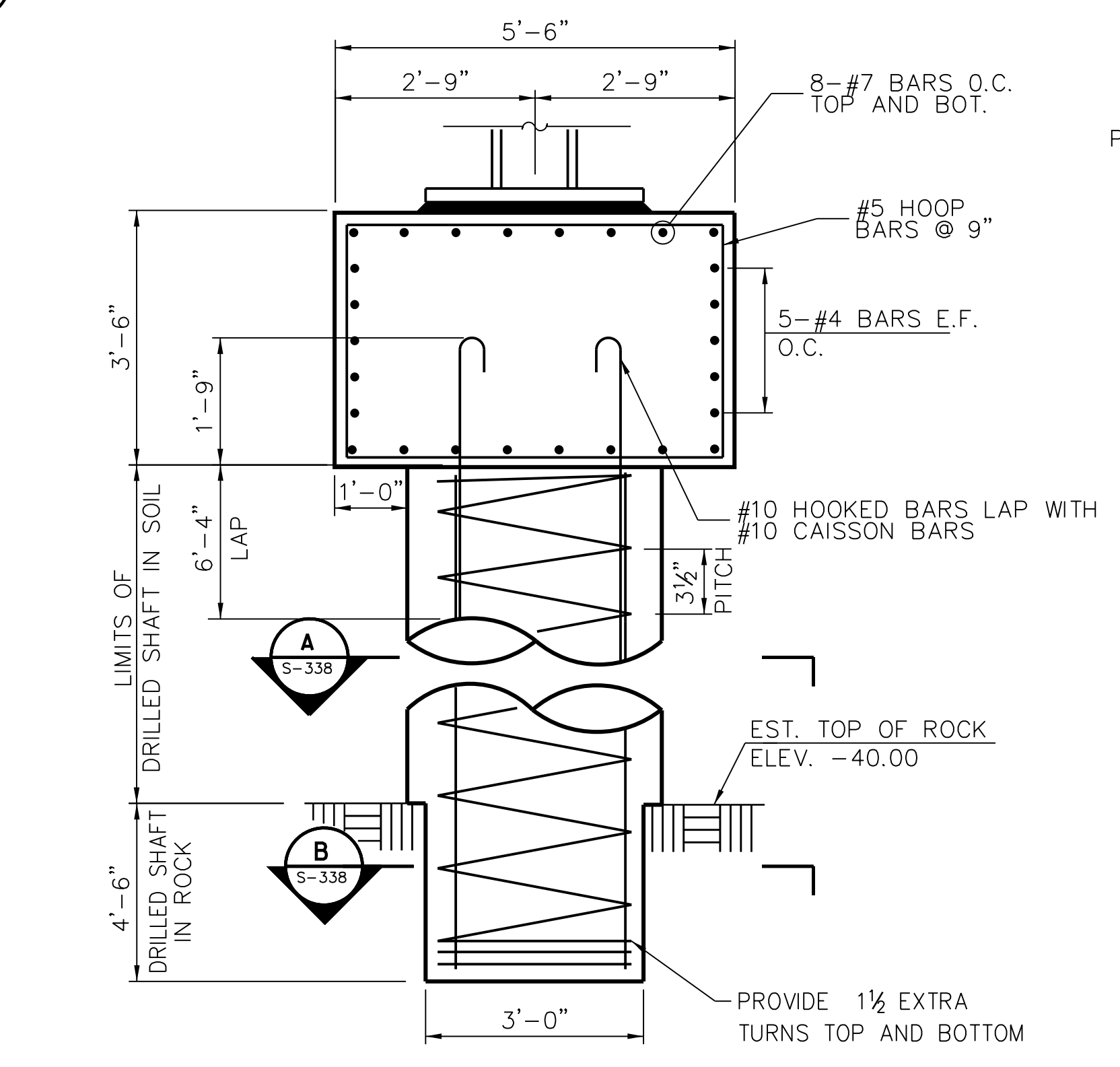
1 PARTIAL ELEVATION AT SOUTH END



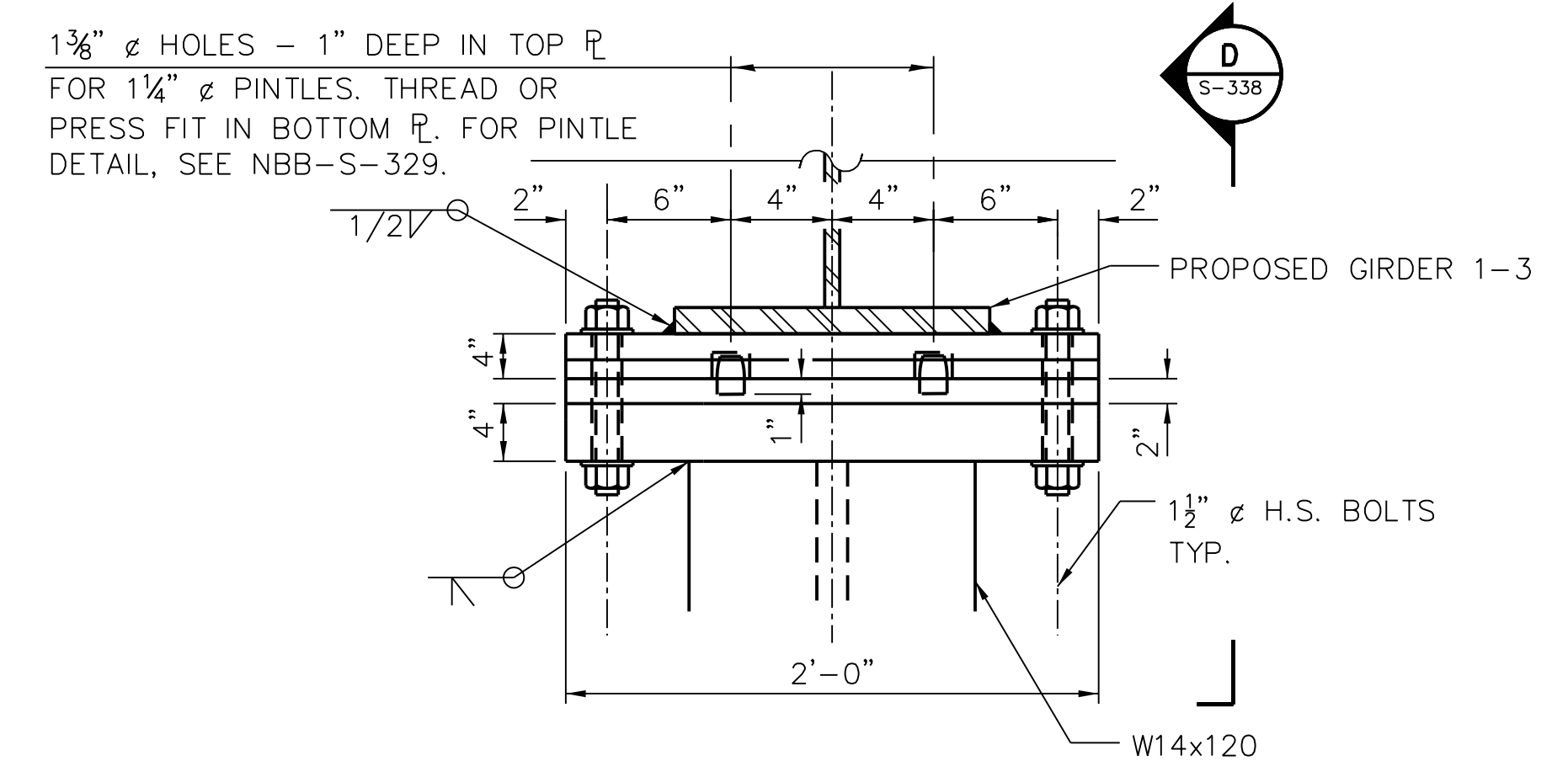
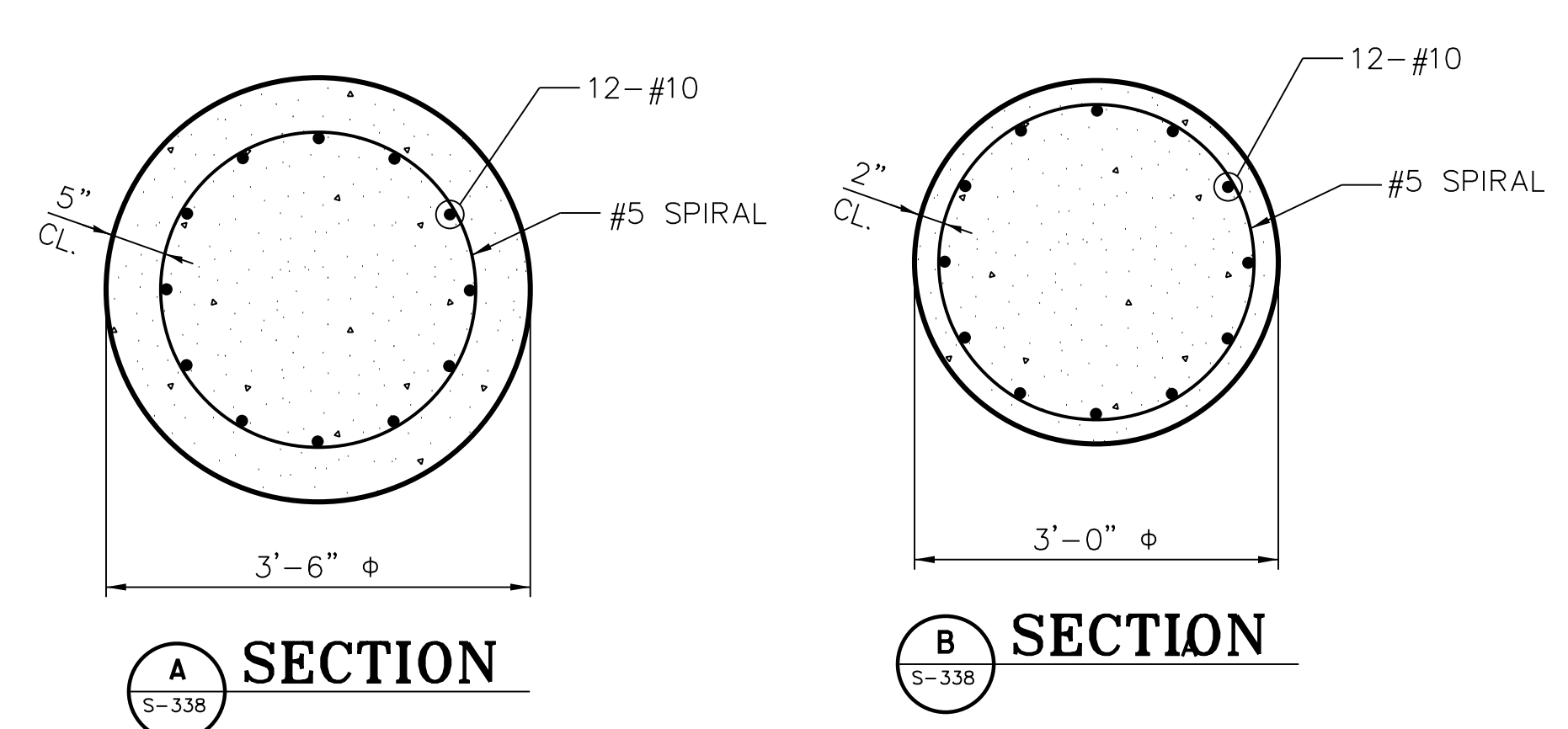
2 PARTIAL END VIEW AT SOUTH END



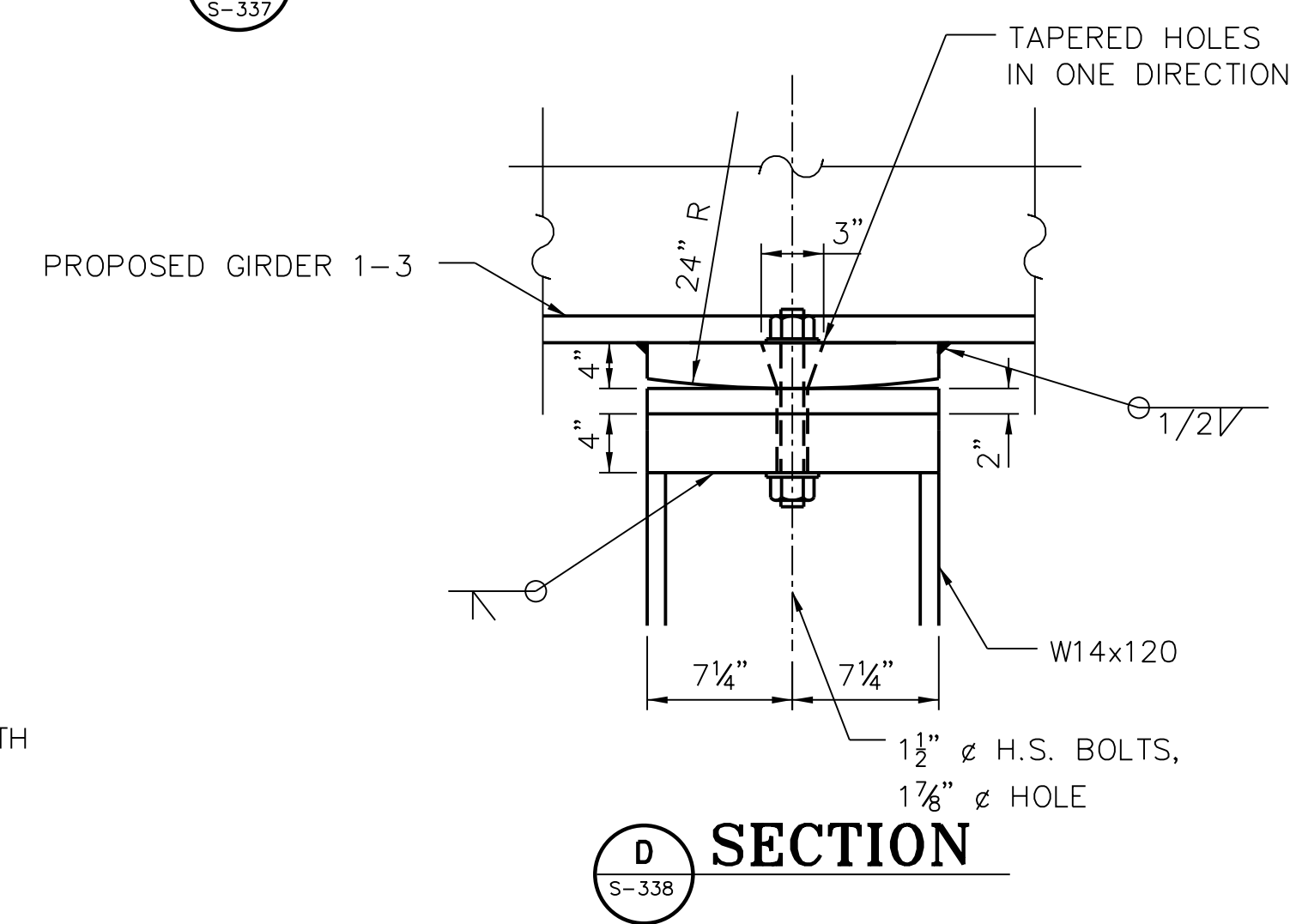
3 PARTIAL ELEVATION AT NORTH END



4 PARTIAL END VIEW AT NORTH END



C TOP PLATE CONNECTION DETAIL



D SECTION

NOTES:

1. BARS EQUALLY SPACED, UNLESS OTHERWISE NOTED.
2. SPACE REINFORCEMENT TO MISS ANCHOR RODS.
3. ALL EDGES SHALL HAVE STANDARD 3/4" CHAMFER.
4. THE REINFORCEMENT DETAILING IS BASED ON THE FOOTING ELEVATION AND ESTIMATED TOP OF ROCK ELEVATION SHOWN AND MAY CHANGE DEPENDING ON THE ACTUAL TOP OF ROCK ENCOUNTERED AT EACH SHAFT. IT IS THE CONTRACTOR'S RESPONSIBILITY TO MAKE NECESSARY, APPROVED ADJUSTMENTS.
5. THE ROCK ELEVATIONS SHOWN ARE BASED UPON SOIL BORINGS AND ARE APPROXIMATE. THE LENGTH OF DRILLED SHAFT FOUNDATION SHALL BE ADJUSTED IN THE FIELD AS REQUIRED AND AS DIRECTED BY THE ENGINEER. THE ENGINEER WILL BE THE SOLE JUDGE AS TO WHEN THE TOP OF ROCK ELEVATION HAS BEEN ENCOUNTERED IN THE FIELD.

NOT FOR CONSTRUCTION



CHICAGO TRANSIT AUTHORITY ENGINEERING

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95th Terminal Improvement  
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Chicago, IL 60628

IN CHARGE	S. KADERBEK
APPROVED BY	D.ZROKA
CHECKED BY	P.MAID
DESIGNED BY	L.SOMMER
DRAWN BY	T.SLEDGE
PROJECT NO.	2012-0021
FILE NAME	File Name

MARK	DATE	DESCRIPTION
	05/23/18	FINAL DESIGN
	03/07/18	INTERMEDIATE DESIGN

LOCATION IDENTIFIER: DR095

95TH ST. NORTH BUS BRIDGE  
PIER SECTIONS AND DETAILS

NBB-S-338

FILE NAME = 070\_DR095\_S-338.dgn  
PLOT DATE = 5/24/2018