





07/10/2016 1:56:18 PM

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METAL DECKING:

- METAL ROOF DECK AND ITS CONNECTIONS SHALL BE CAPABLE OF RESISTING A NET UPLIFT LOADS OF 15 PSF (ASD), IN ACCORDANCE WITH ANY ADDITIONAL APPLICABLE INSURANCE REQUIREMENTS.
- METAL ROOF DECK SHALL NOT BE USED FOR HANGING SUPPORTS FOR ANY TYPE OF BUILDING COMPONENTS INCLUDING, BUT NOT LIMITED TO: SUSPENDED CEILINGS, ELECTRICAL, LIGHT FIXTURES, SPRINKLER SYSTEMS, PLUMBING, HEATING, OR AIR CONDITIONING PIPED, DUCTS, OR ELECTRICAL CONDUITS.
- PROVIDE MINIMUM 16 GAGE EDGE ANGLES OR CLOSURE STRIPS AT PERIMETERS AND AROUND ALL OPENINGS, UNLESS NOTED OTHERWISE ON DRAWINGS.
- FASTEN STEEL DECK TO ALL STRUCTURAL SUPPORTS WITH 3/4" DIAMETER (MIN), PUDDLE WELDS, AT A MINIMUM OF 12" O.C., UNLESS NOTED OTHERWISE. CONTRACTOR MAY CHOOSE TO UTILIZE TEK-SCREWS OR OTHER DECK FASTENING ATTACHMENTS, PROVIDED THAT A COMPLETE SUBMITTAL IS PROVIDED TO THE DOR FOR REVIEW. SUCH SUBMITTAL SHALL INDICATE THAT THE DIAPHRAGM SHEAR CAPACITY ACHIEVED THROUGH THE USE OF THE SUGGESTED ALTERNATIVE DECK FASTENERS IS GREATER THAN OR EQUAL TO THAT ACHIEVED THROUGH THE USE OF PUDDLE WELDS. FASTENERS SHALL BE ARRANGED TO PROVIDE SUFFICIENT DIAPHRAGM SHEAR CAPACITY. REFER TO TYPICAL DETAIL.

PRECAST CONCRETE:

- ALL CONCRETE UNLESS NOTED OTHERWISE, SHALL BE NORMAL WEIGHT CONCRETE WITH AN APPROXIMATE DENSITY OF 145 PCF AND DEVELOP THE FOLLOWING STRENGTH AT 28 DAYS:

DESCRIPTION	f <sub>c</sub> (28-DAYS)
PRECAST CONCRETE	6,000 PSI

- REFER TO SPEC SECTION 03 40 00 AND 03 41 00 FOR PRECAST CONCRETE MIX DESIGNS AND INFORMATION NOT LISTED HEREIN.
- WELDABLE REINFORCEMENT, FOR PRECAST CONCRETE ONLY, SHALL MEET ASTM A706, LOW ALLOY GRADE 60.
- MINIMUM CLEAR CONCRETE COVER SHALL FOLLOW ACI 318-11 SECTION 7.7.3 FOR PRECAST CONCRETE, UNLESS NOTED OTHERWISE ON THE DRAWINGS.
- MECHANICAL SPLICES, COUPLER, AND TERMINATION DEVICES AS INDICATED ON DRAWINGS. SPLICE SLEEVES AND COUPLER CONNECTIONS SHALL CONFORM TO ACI 318 REQUIREMENTS AND DEVELOP AT LEAST 125 PERCENT OF THE YIELD STRENGTH OF THE BAR.
- SHOP DRAWINGS AND SCHEDULES OF ALL REINFORCING STEEL SHALL BE PREPARED BY THE CONTRACTOR AND SUBMITTED TO THE DOR FOR REVIEW PRIOR TO COMMENCEMENT OF THAT PORTION OF THE WORK. ACCESSORIES MUST BE SHOWN ON THE SHOP DRAWINGS.
- INSERTS, PLATES, STOPS, ANCHORS, ANGLES SHALL BE GALVANIZED. WELDED CONDITIONS SHALL BE REPAIRED AFTER CONNECTION HAS BEEN MADE PER ASTM A780. ALL MISC STEEL, REQUIRED FOR CONNECTIONS OR BEARING SHALL BE PROVIDED WITH THE PRECAST ELEMENTS OR AS CALLED FOR ON THE DRAWINGS. ANY DEVIATION FROM THE DETAILS SHOWN ON THE DRAWINGS SHALL BE SUBMITTED FOR APPROVAL TO THE DOR PRIOR TO FABRICATION AND ERECTION.
- PRECAST CONCRETE MEMBERS SHALL BE LIFTED BY DEVICES LOCATED WITHIN TACTILE WARNING SURFACE AREA PROVIDED AND SHOWN ON THE SHOP DRAWINGS. ALL MEMBERS SHALL BE MAINTAINED IN AN UPRIGHT POSITION UNLESS DESIGNED FOR LOADS IMPOSED BY THE CONFIGURATION. ALL LIFTING DEVICES SHALL BE GALVANIZED
- THE CONTRACTOR SHALL COORDINATE ALL PENETRATIONS OF PRECAST MEMBERS, PRIOR TO FABRICATION.
- ALL GROUT SHALL BE NON-SHRINK HIGH STRENGTH, GROUT AS INDICATED IN SPECIFICATIONS, UNLESS NOTED OTHERWISE ON THE DRAWINGS.

CONCRETE MASONRY UNITS:

- CMU SHALL MEET THE REQUIREMENT OF ASTM C90, GRADE N, TYPE 1, NORMAL WEIGHT, AND SHALL HAVE A MINIMUM COMPRESSIVE STRENGTH OF 2,500 PSI ON THE NET BEDDED AREA.  
  
MORTAR SHALL MEET THE REQUIREMENTS OF ASTM C270, TYPE S.  
  
GROUT SHALL MEET THE REQUIREMENTS OF ASTM C476.
- HORIZONTAL JOINT REINFORCEMENT AND VERTICAL REINFORCEMENT SHALL BE PROVIDED IN ALL MASONRY CONSTRUCTION. HORIZONTAL REINFORCEMENT SHALL BE LAPPED A MINIMUM OF 6 INCHES AND SPACED 16" O.C. VERTICALLY AND PROVIDED NOT MORE THAN 16" AWAY FROM TOP AND BOTTOM OF WALL UNLESS NOTED OTHERWISE. ALL HORIZONTAL LADDER TYPE REINFORCEMENT SHALL BE GALVANIZED. ALL VERTICAL AND BOND BEAM REINFORCEMENT SHALL BE EPOXY COATED. REFER TO TYPICAL DETAILS FOR ALL OTHER INFORMATION, INCLUDING BOND BEAM SPACING REQUIREMENTS.
- DOWEL ALL VERTICAL BARS TO FOUNDATIONS, BEAMS, OR SLABS, AND PROVIDE LAP 48 db OR 24 INCHES, WHICHEVER LARGER, OF 48 BAR DIAMETERS. PROVIDE STANDARD HOOKS INTO SLABS WHERE LAP CANNOT BE ACHIEVED DUE TO SLAB OR BEAM DEPTHS.
- PREFABRICATED TEES AND/OR CORNER SECTIONS SHALL BE USED AT ALL WALL INTERSECTIONS.
- ALL MASONRY WALLS SHALL BE BRACED UNTIL THEY ARE ATTACHED TO THE STRUCTURE OR OTHER PERMANENT BRACING HAS BEEN INSTALLED.
- ALL REINFORCING SHALL BE GROUTED SOLID CONTINUOUSLY, IN CELLS OR BOND BEAMS. FILLING SUCH CORES AND BOND BEAMS WITH MORTAR IS STRICTLY PROHIBITED.
- UNLESS OTHERWISE NOTED, HORIZONTAL REINFORCEMENT SHALL BE DUR-O-WALL W1.7 TRUSS OR LADDER TYPE REINFORCEMENT, OR APPROVED EQUAL, FINISH TO BE HOT DIPPED GALVANIZED.
- SEE ARCHITECTURAL DRAWINGS FOR LOCATION AND CONFIGURATION OF MASONRY WALLS. THE PRESENCE OF FOLDS AND TURNS IN THE PLAN CONFIGURATION OF THE MASONRY WALL DOES NOT CHANGE THE REQUIREMENTS OF THE TYPICAL DETAILS.
- WHERE SPLICES ARE REQUIRED, REBAR SHALL BE LAPPED A MINIMUM OF 48 TIMES THE DIAMETER OF THE BAR OR 24 INCHES, WHICHEVER LARGER.
- FIRESTOPPING SHALL BE PROVIDED AT ALL OPEN JOINTS/GAPS THROUGH CMU WALLS. FIRESTOPPING ASSEMBLY RATINGS SHALL BE THE SAME AS THE FIRE RATING OF THE CMU WALL AT ALL JOINTS/GAPS AT THE HEAD OF NON-BEARING CMU WALLS. THE FIRESTOPPING SHALL BE COMPRESSIBLE. REFER TO ARCH DWGS FOR FIRE RATINGS OF CMU WALLS.

WOOD:

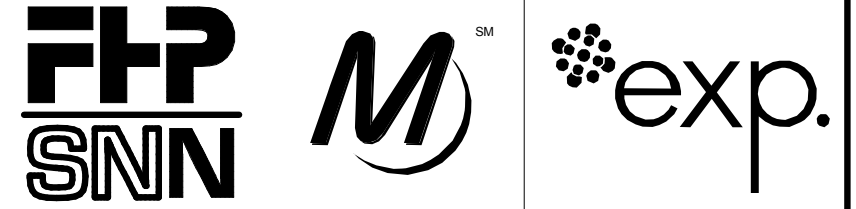
- ALL WOOD CONSTRUCTION SHALL CONFORM TO THE "NATIONAL DESIGN SPECIFICATION" BY THE NATIONAL FOREST PRODUCT ASSOCIATION.
- STRUCTURAL LUMBER SHALL HAVE THE FOLLOWING MINIMUM UNIT STRESSES:  
FB = 875 PSI  
FV = 135 PSI  
FC = 1000 PSI PARALLEL TO GRAIN  
FC = 335 PSI PERPENDICULAR TO GRAIN  
E = 1,100,000 PSI
- PLYWOOD SHEATHING SHALL MEET THE REQUIREMENTS OF PRODUCT STANDARD PS-1 FOR STRUCTURAL 1 GRADE MATERIAL.
- MOISTURE CONTENT SHALL NOT BE GREATER THAN 19%.
- ADHESIVE BETWEEN PLYWOOD AND WOOD STRUCTURAL MEMBERS SHALL BE WEATHERPROOF AND SHALL HAVE A MINIMUM SHEAR STRENGTH OF 150 PSI, ASTM D-3498.
- FOR BOLTED CONNECTIONS USE BOLTS CONFORMING TO ASTM A-307 WITH ASTM A563 NUTS. PROVIDE FLAT WASHERS.
- NAILS SHALL BE COMMON NAILS, ASTM F-1667. MINIMUM NAILED CONNECTIONS FOR WOOD FRAMING MEMBERS SHALL BE IN ACCORDANCE WITH THE NAILING SCHEDULE GIVEN IN IBC TABLE 2304.9.1.
- ALL EXTERIOR WOOD TO BE PRESSURE PRESERVATIVE TREATED.
- GALVANIZED CONNECTORS SHALL BE USED FOR ALL OUTDOOR WOOD INSTALLATIONS.

STRUCTURAL STEEL:

- ALL STRUCTURAL STEEL SHALL BE NEW STEEL CONFORMING TO ASTM STANDARD SPECIFICATIONS "STRUCTURAL STEEL" AS AMENDED TO DATE, UNLESS NOTED OTHERWISE ON THE CONTRACT DOCUMENTS.
- DESCRIPTION TYPE  
WIDE FLANGES ASTM A992, GR 50  
CHANNELS AND ANGLES ASTM A36  
ANCHOR BOLTS ASTM F1554, GR 55  
HSS TUBES ASTM A500, GR.C  
  
HIGH-STRENGTH BOLTS ASTM A325  
  
HIGH-STRENGTH BOLTS (GALVANIZED AREAS) ASTM A325, MECHANICALLY GALV BOLTS  
  
PLATES ASTM A36 UNO
- THE FOLLOWING BUILDING & CANOPY STRUCTURAL STEEL SHALL BE HOT-DIP GALVANIZED IN ACCORDANCE WITH ASTM A123. REFER TO SPECIFICATION SECTION 05 10 30.  
  
- ALL STRUCTURAL STEEL EMBEDDED IN, AND PROTRUDING OUT OF THE FOUNDATION WALL.  
  
- ALL NEW CONCOURSE LEVEL STEEL, FOR BOTH THE SOUTH AND NORTH TERMINALS, INCLUDING THE PEDESTRIAN WALKWAYS OVER THE DAN RYAN.  
  
- ALL EXTERIOR STEEL AT SOFFITS AND OVERHANGS IN NON-CONDITIONED ENVIRONMENTS AT SECOND FLOOR AND ROOF LOCATIONS.  
  
- PEDESTRIAN BRIDGE FLOOR FRAMING ONLY.  
  
- ALL BUS CANOPY STEEL
- WELDING ELECTRODES WILL BE E70XX. WELDERS, TACKERS, AND WELDING OPERATORS MUST BE AWS CERTIFIED WITHIN THE PAST SIX MONTHS.
- SHOP CONNECTIONS MAY BE BOLTED OR WELDED, UNLESS THE CONNECTION METHOD IS SPECIFICALLY INDICATED ON THE STRUCTURAL DRAWINGS.
- FIELD WELDING OF STRUCTURAL MEMBERS, NOT INDICATED ON THE STRUCTURAL DRAWINGS, IS NOT PERMITTED.
- ALL ANCHOR BOLTS AND EMBEDDED PLATES SHALL BE GALVANIZED AND HAVE TWO HEAVY HEX NUTS (ONE TO FASTEN, ONE TO LEVEL) AND ONE WASHER UNLESS INDICATED ON THE DRAWINGS. PROVIDE ANCHOR BOLTS AND EMBEDDED PLATES TO BE SET BY OTHERS.
- TOP-OF-STEEL ELEVATIONS INDICATED ON THE DRAWINGS REFER TO TOP OF TOP-BEAM FLANGE.
- THE CONTRACTOR SHALL PROVIDE ALL EMBEDDED PLATES, SLEEVES, BOX OUTS, CONDUITS, ETC, AS REQUIRED BY OTHER TRADES IN THE CONCRETE STRUCTURE.
- THE CONTRACTOR SHALL PROVIDE ALL NECESSARY TEMPORARY GUYING AND BRACING REQUIRED TO ERECT AND HOLD THE STRUCTURAL STEEL FRAME DURING CONSTRUCTION.
- MINIMUM SIZE OF FILLET WELDS SHALL BE 1/4" UNLESS NOTED OTHERWISE.
- UNLESS OTHERWISE SHOWN PROVIDE MIN. 3/8" THICK STIFFENER PLATES ON EACH SIDE OF BEAM WHEN COLUMN OCCURS ABOVE OR BELOW BEAMS.
- ALL COLUMN ENDS SHALL BE MILLED TO BEAR AND ANY MOMENT FRAME COLUMN SPLICES SHALL BE IN ACCORDANCE WITH THE TYPICAL DETAILS.
- STRUCTURAL BEAM OVER COLUMN CONNECTIONS SHALL HAVE A MINIMUM 1/2" CAP PLATE WITH 4-3/4" DIAMETER HIGH STRENGTH BOLTS, UNLESS NOTED OTHERWISE.
- CAMBER OF BEAMS AND GIRDERS SHALL BE AS DESIGNATED. C=X" INDICATES STEEL BEAM CAMBER VALUE AT MIDSPAN.
- ALL LINTELS AND ACCESSORIES SHALL BE HOT-DIP GALVANIZED. LINTELS AND OTHER STEEL SECTIONS SHALL BEAR A MINIMUM OF 8" ON THE SUPPORTING WALL.
- ALL MOMENT CONNECTIONS SHOWN AND OR NOTED ON THE DRAWINGS SHALL BE FULL PENETRATION WELDED MOMENT CONNECTIONS, UNLESS NOTED OTHERWISE.

STRUCTURAL STEEL (CONTINUED):

- UNLESS NOTED OTHERWISE, PROVIDE CAP PLATES ON ALL HSS MEMBERS
- DESIGN AND DETAIL ALL CONNECTIONS ACCORDING TO AISC FOR CONNECTION LOADS INDICATED ON PLANS.
- FOR BUS CANOPY STEEL:  
- REMOVE ANY VISIBLE WELDING AIDS AND GRIND SMOOTH.  
  
- REMOVE MILL MARKS AND HIDE PIECE MARKS PRIOR TO HOT DIP GALVANIZING.  
  
- FOR FIELD WELDING OF COLUMN SPLICES IN GALVANIZED MATERIAL USE GALVA STOP OR SIMILAR MASKING TO PREVENT ZINC FROM ADHERING TO STEEL IMMEDIATELY ADJACENT TO FIELD WELD LOCATION.  
  
- FOR OTHER PREPARATION PRIOR TO HOT DIP GALVANIZING AND REPAIR AFTERWARDS REFER TO SPECIFICATION SECTION 05 10 30.



**SENSITIVE SECURITY INFORMATION**  
WARNING: THIS RECORD CONTAINS SENSITIVE SECURITY INFORMATION THAT IS CONTROLLED UNDER 48 CFR PARTS 15 AND 1506. NO PART OF THIS RECORD MAY BE DISCLOSED TO PERSONS WITHOUT A "NEED TO KNOW" AS DEFINED IN CFR PARTS 15 AND 1506. EXCEPT WITH THE WRITTEN PERMISSION OF THE ADMINISTRATOR OF THE TRANSPORTATION SECURITY ADMINISTRATION OR THE SECRETARY OF TRANSPORTATION. UNAUTHORIZED RELEASE MAY RESULT IN CIVIL PENALTY OR OTHER ACTION. FOR U.S. GOVERNMENT AGENCIES, PUBLIC DISCLOSURE IS GOVERNED BY 5 U.S.C. 552 AND 48 CFR PARTS 15 AND 1506.

**DRAWING SCALE IS NOT GUARANTEED. CTA ASSUMES NO RISK OF LIABILITY FOR ERRORS CAUSED, DIRECTLY OR INDIRECTLY, BY SCALING OF THIS DRAWING.**

**95th Terminal Improvement**  
15 West 95th Street (South Terminal)  
Chicago, IL 60628

IN CHARGE	J. JAKALSKI
APPROVED BY	J. JAKALSKI
CHECKED BY	R.S. NAIR
DESIGNED BY	G. FORKAN
DRAWN BY	J. PEARSON
PROJECT NO.	2012-0021.02
FILE NAME	DR095_S-Terminal

MARK	DATE	DESCRIPTION
	2/9/17	ISSUED FOR CONSTRUCTION

LOCATION IDENTIFIER: **DR095**

**STRUCTURAL GENERAL NOTES - 3**

**S-003**

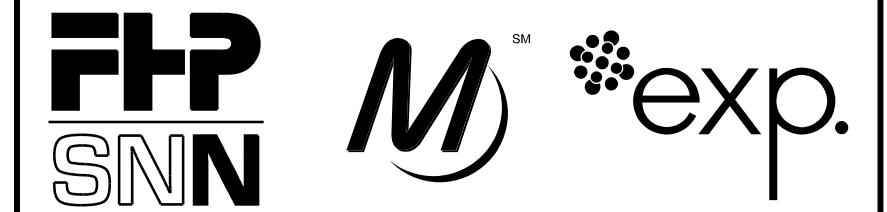
**BRIDGE GENERAL NOTES**

1. FASTENERS SHALL BE ASTM A325 TYPE 3, HOT DIP GALVANIZED BOLTS. BOLTS  $\frac{7}{8}$  INCH  $\phi$ , HOLES  $\frac{15}{16}$  INCH  $\phi$ , UNLESS OTHERWISE NOTED. SEE SPECIAL PROVISION FOR HOT DIP GALVANIZING FOR STRUCTURAL STEEL.
2. NO FIELD WELDING IS PERMITTED EXCEPT AS SPECIFIED IN THE CONTRACT DOCUMENTS.
3. REINFORCEMENT BARS SHALL BE EPOXY COATED.
4. PRIOR TO POURING THE NEW CONCRETE DECK, ALL HEAVY OR LOOSE RUST, LOOSE MILL SCALE, AND OTHER LOOSE OR POTENTIALLY DETRIMENTAL FOREIGN MATERIAL SHALL BE REMOVED FROM THE SURFACES IN CONTACT WITH CONCRETE. TIGHTLY ADHERED PAINT MAY REMAIN UNLESS OTHERWISE NOTED. REMOVAL SHALL BE ACCOMPLISHED BY METHODS THAT WILL NOT DAMAGE THE STEEL AND THE COST WILL BE INCLUDED IN THE PAY ITEM COVERING REMOVAL OF THE EXISTING CONCRETE.  
AS DIRECTED BY THE ENGINEER, EXISTING CONSTRUCTION ACCESSORIES WELDED TO THE TOP FLANGE OF BEAMS AND GIRDERS SHALL BE REMOVED. THE WELD AREAS SHALL BE GROUND FLUSH AND INSPECTED FOR CRACKS USING MAGNETIC PARTICLE TESTING (MT) OR DYE PENETRANT TESTING (PT) BY QUALIFIED PERSONNEL APPROVED BY THE ENGINEER.  
ANY CRACKS THAT CANNOT BE REMOVED BY GRINDING  $\frac{1}{4}$  INCH DEEP SHALL BE IDENTIFIED AND REPORTED TO THE ENGINEER FOR FURTHER DISPOSITION.
5. IF THE CONTRACTOR ELECTS TO USE CANTILEVER FORMING BRACKETS ON THE EXTERIOR BEAMS OR GIRDERS, THE BRACKETS SHALL BE PLACED AT THE SAME LOCATIONS AS REQUIRED FOR THE HARDWOOD BLOCKS IN ARTICLE 503.06(B) OF THE STANDARD SPECIFICATIONS. IF ADDITIONAL CANTILEVER FORMING BRACKETS ARE REQUIRED, HARDWOOD BLOCKING SHALL BE WEDGED BETWEEN THE EXTERIOR AND FIRST INTERIOR BEAM AT EACH OF THESE ADDITIONAL BRACKET LOCATIONS.
6. PLAN DIMENSIONS AND DETAILS RELATIVE TO EXISTING PLANS ARE SUBJECT TO NOMINAL CONSTRUCTION VARIATIONS. THE CONTRACTOR SHALL FIELD VERIFY EXISTING DIMENSIONS AND DETAILS AFFECTING NEW CONSTRUCTION AND MAKE NECESSARY APPROVED ADJUSTMENTS PRIOR TO CONSTRUCTION OR ORDERING OF MATERIALS. SUCH VARIATIONS SHALL NOT BE CAUSE FOR ADDITIONAL COMPENSATION FOR A CHANGE IN SCOPE OF THE WORK.
7. BEARING SEAT SURFACES SHALL BE CONSTRUCTED OR ADJUSTED TO THE DESIGNATED ELEVATIONS WITHIN A TOLERANCE OF  $\frac{1}{8}$  INCH (0.01 FT.). ADJUSTMENT SHALL BE MADE EITHER BY GRINDING THE SURFACE OR BY SHIMMING THE BEARINGS.
8. CONCRETE SEALER SHALL BE APPLIED TO AREAS OF NEW CONCRETE LOCATED UNDER DECK JOINTS. THIS SHALL INCLUDE BACKWALLS, BRIDGE SEATS AND FRONT FACES OF ABUTMENT WALLS.
9. THE EXISTING STRUCTURAL STEEL COATING MAY CONTAIN LEAD. THE CONTRACTOR SHALL TAKE APPROPRIATE PRECAUTIONS TO DEAL WITH THE PRESENCE OF LEAD ON THIS PROJECT.
10. ALL CROSS FRAMES OR DIAPHRAGMS SHALL BE INSTALLED AS STEEL IS ERECTED AND SECURED WITH ERECTION PINS AND BOLTS EXCEPT AS OTHERWISE NOTED. INDIVIDUAL CROSS FRAMES OR DIAPHRAGMS AT SUPPORTS MAY BE TEMPORARILY DISCONNECTED TO INSTALL BEARING ANCHOR RODS.
11. LOAD CARRYING COMPONENTS DESIGNATED "NTR" SHALL CONFORM TO THE IMPACT TESTING REQUIREMENT, ZONE 2.
12. TWO 1#8 IN. ADJUSTING SHIMS SHALL BE PROVIDED FOR EACH BEARING IN ADDITION TO ALL OTHER PLATES OR SHIMS AND PLACED AS SHOWN ON BEARING DETAILS.
13. EXISTING REINFORCEMENT SHALL BE CLEANED AND INCORPORATED INTO THE NEW CONSTRUCTION IN ACCORDANCE WITH IDOT STD. SPECIFICATION SECTION 501.05. COST INCLUDED WITH CONCRETE REMOVAL.

14. ALL BEARING PLATES, SIDE RETAINERS, ANCHOR BOLTS, NUTS, WASHERS AND PINTLES SHALL BE GALVANIZED ACCORDING TO ASTM A153 OR A123 AS APPLICABLE.
15. TEMPORARY EARTH RETENTION (ERS) SHALL BE DESIGNED BY A STATE OF ILLINOIS LICENSED STRUCTURAL ENGINEER, ERS SHALL BE PROVIDED AT LOCATIONS SHOWN ON THE PLANS.
16. NEW BRIDGE STRUCTURAL STEEL BEAMS AND COMPONENTS SHALL BE HOT-DIP GALVANIZED IN ACCORDANCE WITH AASHTO M111. ALL ADDITIONAL STEEL ASSOCIATED WITH THE BRIDGE STRUCTURES SHALL BE IN ACCORDANCE WITH IDOT STD. SPEC SECTION 1006.
17. CARE SHALL BE TAKEN DURING THE DECK REMOVAL PROCESS TO AVOID DAMAGING THE EXISTING STEEL BEAMS TO REMAIN IN PLACE. THE CONTRACTOR SHALL MARK THE TOP SURFACE OF THE EXISTING DECK TO IDENTIFY THE LOCATION AND LIMITS OF THE TOP FLANGES PRIOR TO THE COMMENCEMENT OF DECK REMOVAL OPERATIONS.
18. IF A BEAM IS DAMAGED DURING CONSTRUCTION. IT IS THE CONTRACTORS'S RESPONSIBILITY TO REPAIR THE DAMAGE AT HIS/HER OWN EXPENSE. THE CONTRACTOR WILL BE RESPONSIBLE FOR RETAINING A LICENSED STRUCTURAL ENGINEER IN THE STATE OF ILLINOIS TO ANALYZE THE EFFECT OF THE DAMAGE, TO MAKE REPAIR RECOMMENDATIONS AND PREPARE REPAIR DETAILS. REPAIR DETAILS SHALL BE SUBMITTED TO THE ENGINEER FOR REVIEW AND CONCURRENCE PRIOR TO THE IMPLEMENTATION OF REPAIRS.
19. ALL EPOXY COATED REINFORCING BAR LAPS SHALL BE AS NOTED IN THE TABLE BELOW UNLESS OTHERWISE NOTED.

MINIMUM BAR LAPS	
BARS	LAPS
#4	2'-11"
#5	3'-8"
#6	4'-5"
#7	5'-10"
#8	7'-8"
#9	9'-8"
#10	12'-4"

20. WHERE NEW STRUCTURAL STEEL ELEMENTS ARE CONNECTED TO AN EXISTING STEEL MEMBER, ALL NEW FASTENERS SHALL BE HIGH STRENGTH BOLTS. HOLES SHALL BE SUBPUNCHED OR SUBDRILLED  $\frac{1}{16}$ " DIA. AND REAMED IN THE FIELD TO  $\frac{13}{16}$ " DIA. FOR  $\frac{3}{4}$ " DIA. BOLTS, UNLESS OTHERWISE NOTED. HOLES SHALL BE SUBPUNCHED OR SUBDRILLED  $\frac{13}{16}$ " DIA. AND REAMED IN THE FIELD TO  $\frac{15}{16}$ " DIA. FOR  $\frac{7}{8}$ " DIA. BOLTS, UNLESS NOTED OTHERWISE.
21. EXISTING STRUCTURAL STEEL SHALL ONLY BE CLEANED AND PAINTED AS REQUIRED BY THE SPECIAL PROVISION "CLEANING AND PAINTING ADJACENT AREAS OF EXISTING STEEL STRUCTURES".
22. ANY REINFORCEMENT BARS THAT ARE DAMAGED DURING CONCRETE REMOVAL OPERATIONS SHALL BE REPAIRED OR REPLACED USING AN APPROVED BAR SPLICER OR ANCHORAGE SYSTEM. COST INCIDENTAL TO "CONCRETE REMOVAL".
23. BEAMS HAVE CONNECTION PLATES AS REQUIRED BY DESIGN. ADDITIONAL STIFENERS MAY BE ADDED AT THE CONTRACTOR'S EXPENSE AS NECESSARY TO PREVENT DISTORTION OF THE GIRDERS DURING GALVANIZING. THE CONTRACTOR SHALL COORDINATE WITH THE FABRICATOR AND THE GALVANIZER TO DETERMINE IF ADDITIONAL STIFFENERS ARE NECESSARY, AND WHERE THESE SHOULD BE PLACED. ANY PROPOSED CHANGES SHOULD BE SUBMITTED TO THE ENGINEER FOR APPROVAL PRIOR TO MAKING ANY CHANGES.
24. TEMPORARY STIFFENER ANGLES SHALL BE BOLTED TO EACH SIDE OF THE SPLICE ENDS OF EACH BEAM SEGMENT TO PREVENT DISTORTION DURING GALVANIZING. TEMPORARY STIFFENER ANGLES SHALL BOLT OR FIT AGAINST THE TOP AND BOTTOM FLANGES AND SHALL INCLUDE SPACER TUBES TO MINIMIZE DAMAGE TO GALVANIZING DURING REMOVAL.
25. WORK DESIGNATED AS CONCRETE REMOVAL AND THE ASSOCIATED CLEANING AND REPAIR OF REINFORCEMENT BARS SHALL BE INCLUDED AS PART OF SITE DEMOLITION.
26. THESE NOTES APPLY TO SHEETS 95TH-S-101 THROUGH 95TH-S-150A, SBB-S-201 THROUGH SBB-S-235, NBB-S-301 THROUGH NBB-S-337, EXE-S-401 THROUGH EXE-S-410, EXW-S-501 THOUGH EXW-S-508, AND S-1400 THROUGH S-1408.



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**95th Terminal Improvement**  
 14 West 95th Street (North Terminal)  
 15 West 95th Street (South Terminal)  
 Chicago, IL 60628

IN CHARGE	S. KADERBEK
APPROVED BY	D.ZROKA
CHECKED BY	P.MAID
DESIGNED BY	L.SOMMER
DRAWN BY	T.SLEDGE
PROJECT NO.	2012-0021
FILE NAME	File Name

MARK	DATE	DESCRIPTION
	10/06/16	ISSUED FOR CONSTRUCTION

LOCATION IDENTIFIER: **DR095**

BRIDGE  
 GENERAL NOTES  
 (SHEET 1 OF 2)

**GEN-S-003A**

vscorp

FILE NAME = 070\_DR095\_S-003A.dgn  
 PLOT DATE = 10/7/2016

vscarpa

**BRIDGE GENERAL NOTES**

1. The Contractor shall employ a State of Illinois Licensed Structural Engineer to review loads and design any required earth retention or other ground stabilization required to protect adjacent work.
2. All elevations are based upon the C.C.D. datum.
3. Temporary casings shall be used to maintain and prevent caving of adjacent ground and to support adjacent loadings. Temporary casings shall be used for all drilled shafts and extend to at least 10 feet below grade. The Contractor's Structural Engineer shall evaluate and determine whether the temporary casing is required to be deeper than 10 feet based on adjacent loads.
4. Rock socketed caissons shall use permanent casings or be drilled using the wet method to ensure that the shaft sides are maintained and to control water infiltration.
5. The interstitial space between the permanent casing and the temporary casing shall be filled with lean concrete with a minimum compressive strength of 3,000 psi. Grouting shall occur concurrent with the removal of the temporary casing.
6. The bearing elevation for the caissons has been estimated based on the soil borings provided with these Contract Documents. The actual elevation of bearing shall be determined by testing in the field. The Contractor should anticipate some variance in the actual elevation of bearing strata.
7. Protect the ends of all exposed reinforcing bars for follow on work with approved end protectors.
8. All construction work shall be arranged to avoid unscheduled interference with CTA operations.
9. These notes apply to sheets 95TH-S-101 through 95TH-S-150A, SBB-S-201 through SBB-S-235, NBB-S-301 through NBB-S-337, EXE-S-401 through EXE-S-410, EXW-S-501 through EXW-S-508, and S-1400 through S-1408.



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 14 West 95th Street (North Terminal)  
 15 West 95th Street (South Terminal)  
 Chicago, IL 60628

IN CHARGE	S. KADERBEK
APPROVED BY	D.ZROKA
CHECKED BY	P.MAID
DESIGNED BY	L.SOMMER
DRAWN BY	T.SLEDGE
PROJECT NO.	2012-0021
FILE NAME	File Name

	10/06/16	ISSUED FOR CONSTRUCTION

MARK	DATE	DESCRIPTION
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LOCATION IDENTIFIER: **DR095**

BRIDGE  
 GENERAL NOTES  
 (SHEET 2 OF 2)

**GEN-S-003B**

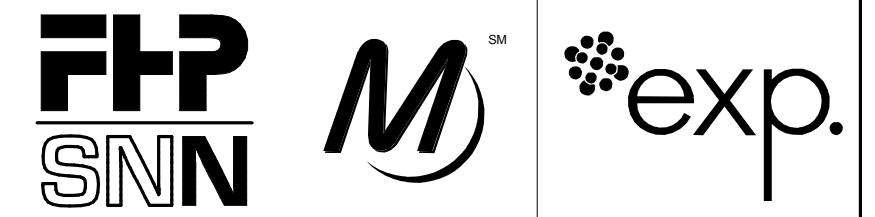
FILE NAME = 070\_DR095\_S-003B.dgn  
 PLOT DATE = 10/7/2016

ABBREVIATIONS:

ADDL	ADDITIONAL	GA	GAGE OR GAUGE	R OR RAD	RADIUS
ALT	ALTERNATE	GALV	HOT-DIP GALVANIZED	REF	REFERENCE
ACI	AMERICAN CONCRETE INSTITUTE	GEN	GENERAL	REINF	REINFORCE, REINFORCING, REINFORCEMENT
AISC	AMERICAN INSTITUTE OF STEEL CONSTRUCTION	GR	GRADE	REM	REMAINDER
AISI	AMERICAN IRON AND STEEL INSTITUTE	GB	GRADE BEAM	REQD	REQUIRED
ASTM	AMERICAN SOCIETY FOR TESTING AND MATERIALS	HMA	HOT-MIX ASPHALT	RETG	RETAINING
AWS	AMERICAN WELDING SOCIETY	HVAC	HEATING, VENTILATING, AND AIR CONDITIONING	RET	RETURN
AB	ANCHOR BOLT	HGT	HEIGHT	RE	RIGHT END
APPROX	APPROXIMATELY	HP	HIGH POINT	RD	ROOF DRAIN
ARCH	ARCHITECT OR ARCHITECTURAL	HS	HIGH STRENGTH	SCH	SCHEDULE
BAL	BALANCE	H OR HORIZ	HORIZONTAL	SCHED	SCHEDULE
BK	BACK	IN	INCH	SECT	SECTION
BTW	BETWEEN	INFO	INFORMATION	SC	SHEAR CONNECTOR
BM	BEAM	INCL	INCLUDING OR INCLUSIVE	SHT	SHEET
BRG	BEARING	ID	INSIDE DIAMETER	SLV	SHORT LEG VERTICAL
BLK	BLOCK	IF	INSIDE FACE	SIM	SIMILAR
B OR BOT	BOTTOM	INSUL	INSULATION	SOG	SLAB ON GRADE
BEW	BOTTOM EACH WAY	INT	INTERIOR	SOE	SUPPORT OF EXCAVATION
BRKT	BRACKET	INV	INVERT	S	SOUTH
BLDG	BUILDING	JT	JOINT	SPA	SPACING OR SPACES
CIP	CAST-IN-PLACE	KIP(S)	1 KIP = 1,000 POUNDS	SPEC(S)	SPECIFICATIONS
CTR	CENTERED OR CENTERED	KSI	KIPS PER SQUARE INCH	SPL	SPLICE LENGTH
C	CENTERLINE	LBS	POUNDS	SQ	SQUARE
CG	CENTER OF GRAVITY	LE	LEFT END	SS	STAINLESS STEEL
CJ	CONSTRUCTION OR CONTROL JOINT	LW	LIGHT WEIGHT	STD	STANDARD
	CENTER LINE	LWC	LIGHT WEIGHT CONCRETE	STD SPEC	IDOT STANDARD SPECIFICATION 2012 AND RECURING SPECIAL PROVISIONS
CL	CLEAR	LOC	LOCATION	STL	STEEL
CLSM	CONTROLLED LOW-STRENGTH MATERIAL	LG	LONG	SF	STEP FOOTING
COL	COLUMN	LL	LOWER LEVEL	STIFF	STIFFENER
COMP	COMPRESSION	LLV	LONG LEG VERTICAL	STRUCT	STRUCTURAL
CONC	CONCRETE	LP	LOW POINT	SP	SUMP PIT
CMU	CONCRETE MASONRY UNIT	MANUF	MANUFACTURER	SYM	SYMMETRICAL
CONN	CONNECTION	MFR	MANUFACTURER	TEMP	TEMPERATURE OR TEMPORARY
CONST	CONSTRUCTION	MATL	MATERIAL	TENS	TENSION
CONT	CONTINUOUS	MAX	MAXIMUM	TFE	TETRAFLUORETHYLENE
DEP	DEPRESSION	MECH	MECHANICAL	THK	THICK OR THICKNESS
DET	DETAIL	MEP	MECHANICAL, ELECTRICAL, & PLUMBING	THRD	THREADED
Ld	DEVELOPMENT LENGTH	MEZZ	MEZZANINE	TB	TIE BEAM
D OR Ø	DIAMETER	MID	MIDDLE	X	TIMES OR BY
DIA	DIAMETER	MID-PT	MID POINT	T	TOP
DIM	DIMENSION	MIN	MINIMUM	T & B	TOP AND BOTTOM
DIR	DIRECTION	MISC	MISCELLANEOUS	TOC	TOP OF CONCRETE
DOR	DESIGNER OF RECORD	NFPA	NATIONAL FOREST PRODUCTS ASSOCIATION	TOS	TOP OF STEEL
DWL(S)	DOWEL OR DOWELS	NF	NEAR FACE	TOW	TOP OF WALL
DN	DOWN	NOM	NOMINAL	TBG	TRANSFER GRADE BEAM
DWG	DRAWING	NW	NORMAL WEIGHT	TYP	TYPICAL
EA	EACH	NWC	NORMAL WEIGHT CONCRETE	UNO	UNLESS NOTED OTHERWISE
EE	EACH END	N	NORTH	UL	UPPER LAYER
EF	EACH FACE	NIC	NOT IN CONTRACT	V OR VERT	VERTICAL
ES	EACH SIDE	NTS	NOT TO SCALE	VEF	VERTICAL EACH FACE
EW	EACH WAY	No. OR #	NUMBER	VIF	VERTICAL INSIDE FACE
ELEC	ELECTRICAL	OC	ON CENTER	VIF	VERIFY IN FIELD
EL	ELEVATOR	OPNG	OPENING	VOF	VERTICAL OUTSIDE FACE
EL OR ELEV	ELEVATION	OPP	OPPOSITE	W	WEST
EMBED	EMBEDMENT	OH	OPPOSITE HAND	WCO	WALL CLEAN OUT
EQ	EQUAL	OD	OUTSIDE DIAMETER	WPG	WATERPROOFING
EQUIP	EQUIPMENT	OF	OUTSIDE FACE	WWF	WELDED WIRE FABRIC
ETC	ETCETERA	PEN	PENETRATION	WWPA	WESTERN WOOD PRODUCTS ASSOC
EXIST	EXISTING	PC	PILE CAP	WWR	WELDED WIRE REINFORCEMENT
EXP BOLT	EXPANSION BOLT	PCC	PORTLAND CEMENT CONCRETE	W	WITH
EXP JT	EXPANSION JOINT	PL	PLATE	W/O	WITHOUT
EXT	EXTERIOR	PT	POINT	WP	WORK POINT
FCM	FRACTURE CRITICAL MEMBER	PT	POST-TENSIONED		
FF	FAR FACE	PTB	POST-TENSIONED BAR		
FL	FLOOR	PSF	POUNDS PER SQUARE FOOT		
FT	FEET OR FOOT	PSI	POUNDS PER SQUARE INCH		
FIN	FINISH	PVC	POLYVINYL CHLORIDE		
FIN FL	FINISHED FLOOR	PC OR P/C	PRECAST CONCRETE		
FTG	FOOTING				
FNDN	FOUNDATION				

SYMBOLS:

	GRID LINE MARK No.
	DRAWING REVISION NUMBER
	MOMENT CONNECTION
	ELEVATION
	STEPPED SLAB OR CONTOUR
	SPAN DIRECTION
	NORTH ARROW
	GRAPHIC SCALE
	PLAN/SECTION/DETAIL
	SECTION/DETAIL REFERENCE
	ELEVATION (LOOKING - NORTH/WEST/EAST/SOUTH)
	CUT SECTION/DETAIL (CROSSING REFER TO)
	REMOVAL
	EXISTING
	PROPOSED



**SENSITIVE SECURITY INFORMATION**  
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**95th Terminal Improvement**  
 15 West 95th Street (South Terminal)  
 Chicago, IL 60628

IN CHARGE	J. JAKALSKI
APPROVED BY	J. JAKALSKI
CHECKED BY	R.S. NAIR
DESIGNED BY	G. FORKAN
DRAWN BY	M. DONAHUE
PROJECT NO.	2012-0021.02
FILE NAME	DR095_S-Terminal

MARK	DATE	DESCRIPTION
	2/9/17	ISSUED FOR CONSTRUCTION

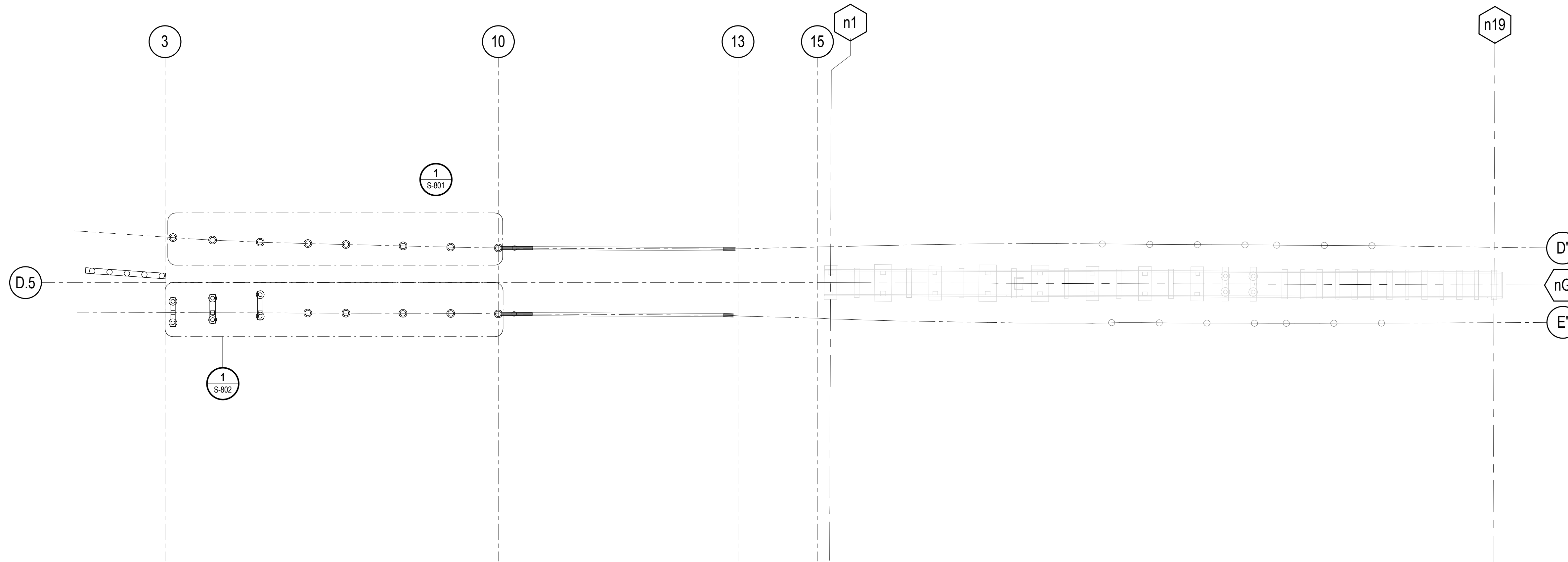
LOCATION IDENTIFIER: **DR095**

**STRUCTURAL ABBREVIATIONS AND SYMBOLS**

**S-004**

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NOTE: EXISTING STRUCTURE TO REMAIN.

**1 REFERENCE KEY PLAN - FOUNDATION**  
S-005 SCALE: 1" = 40'-0"



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**95th Terminal Improvement**  
15 West 95th Street (South Terminal)  
Chicago, IL 60628

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DRAWN BY	M. DONAHUE
PROJECT NO.	2012-0021.02
FILE NAME	DR095_S-StationFoundation

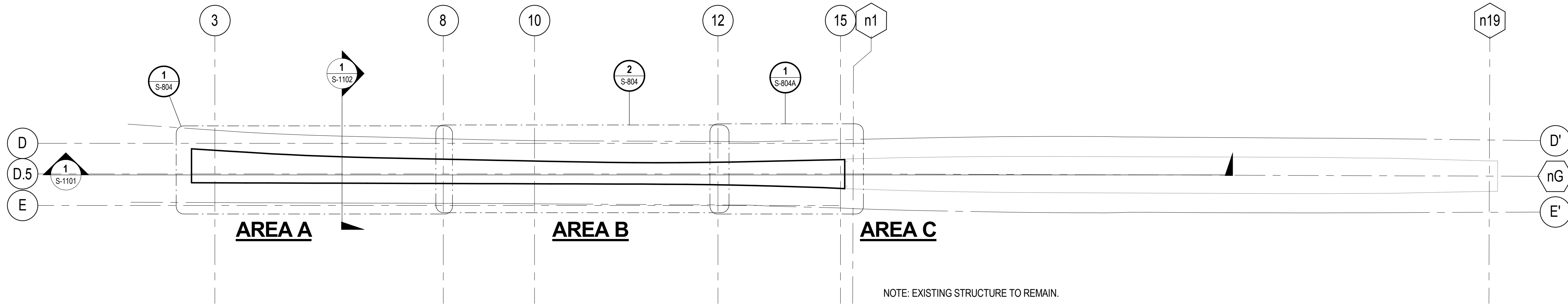
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MARK	DATE	DESCRIPTION

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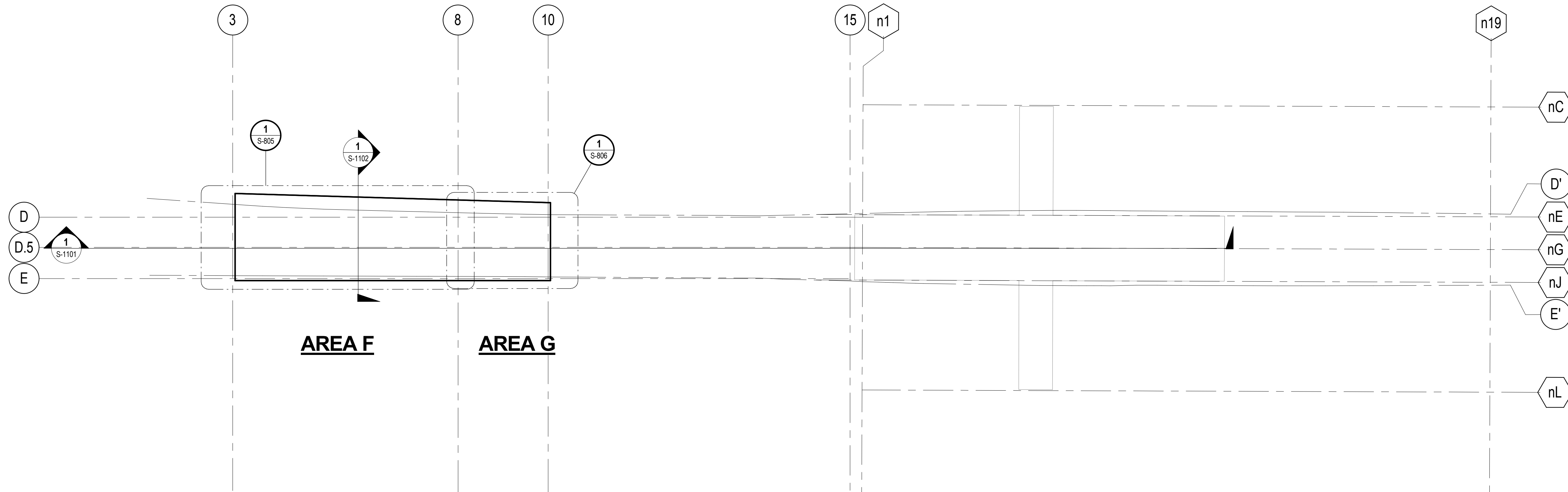
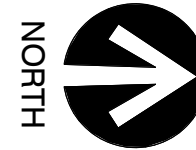
**REFERENCE KEY PLAN -  
FOUNDATIONS**

**S-005**

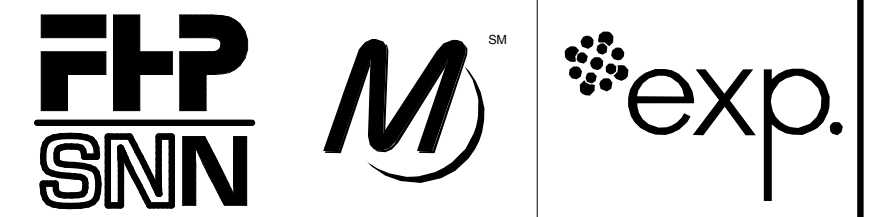
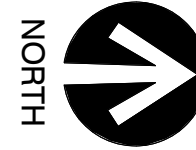
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**1** PLATFORM LEVEL - OVERALL PLAN SOUTH  
S-006 SCALE: 1" = 40'-0"



**2** CONCOURSE LEVEL - OVERALL PLAN  
S-006 SCALE: 1" = 40'-0"



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Chicago, IL 60628

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APPROVED BY	J. JAKALSKI
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DESIGNED BY	G. FORKAN
DRAWN BY	M. DONAHUE
PROJECT NO.	2012-0021.02
FILE NAME	DR095_S-Terminal

MARK	DATE	DESCRIPTION
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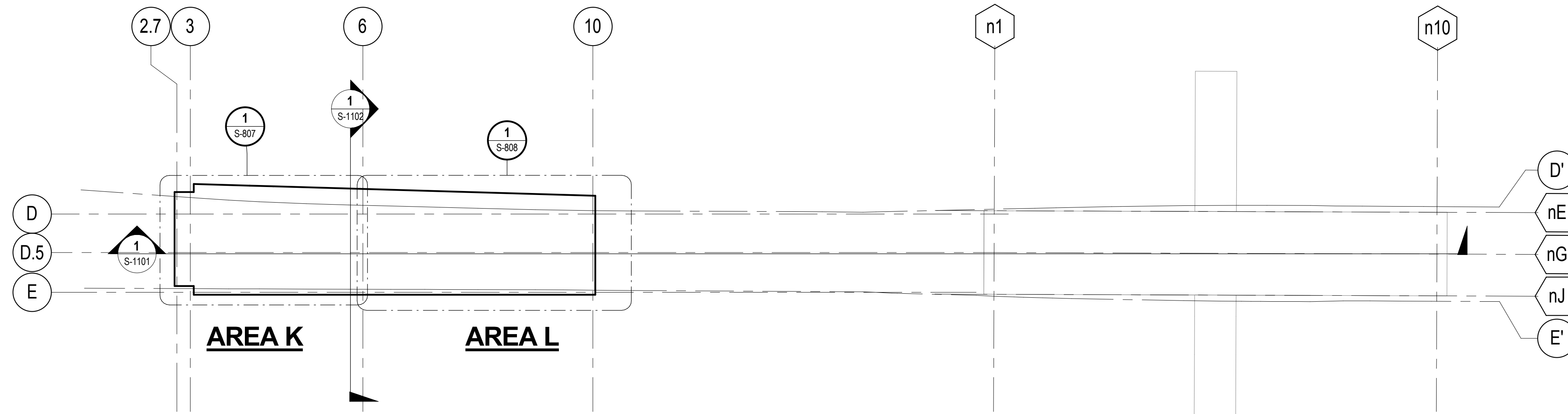
REFERENCE KEY PLANS -  
PLATFORM & CONCOURSE LEVEL

S-006

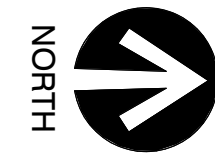


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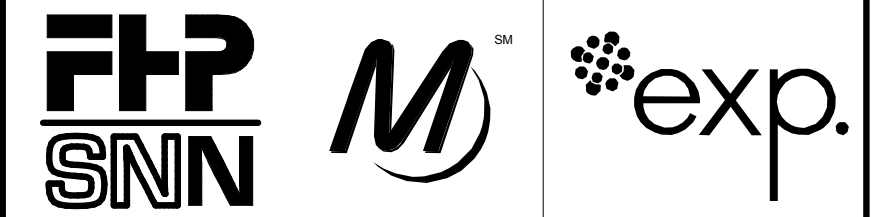
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**1** REFERENCE KEY PLAN - LEVEL 2 & EXISTING ROOF  
 S-007 SCALE: 1" = 40'-0"



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**cta** CHICAGO TRANSIT AUTHORITY ENGINEERING

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 Chicago, IL 60628

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PROJECT NO.	2012-0021.02
FILE NAME	DR095_S-Terminal

MARK	DATE	DESCRIPTION
	2/9/17	ISSUED FOR CONSTRUCTION

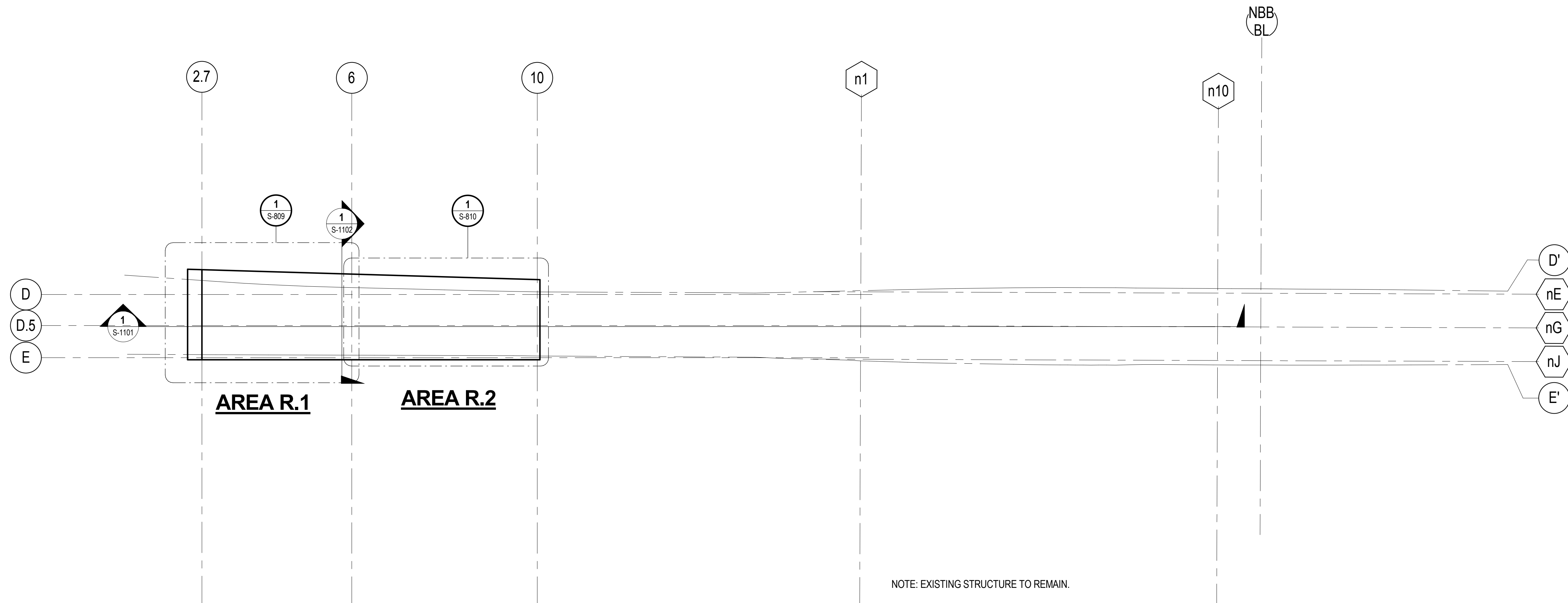
LOCATION IDENTIFIER: **DR095**

**REFERENCE KEY PLAN - LEVEL 2 & EXISTING ROOF**

**S-007**

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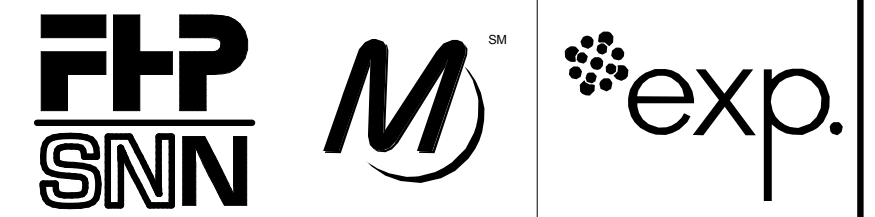
**1** REFERENCE KEY PLAN ROOF  
S-008

SCALE: 1" = 40'-0"

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PROJECT NO.	2012-0021.02
FILE NAME	DR095_S-Terminal

MARK	DATE	DESCRIPTION
	2/9/17	ISSUED FOR CONSTRUCTION

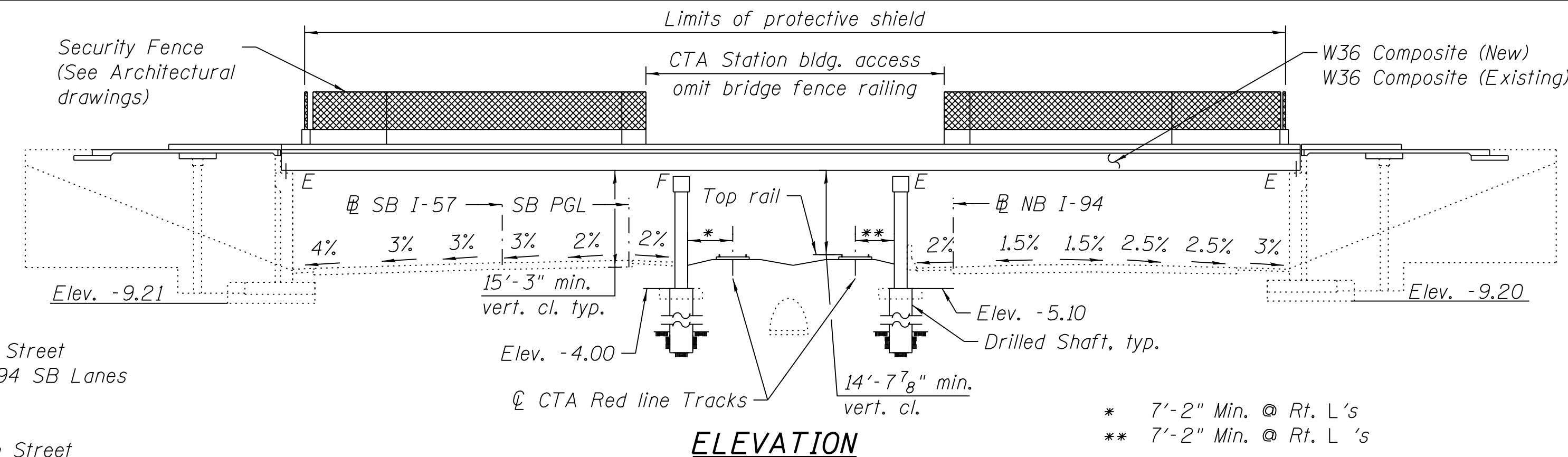
LOCATION IDENTIFIER: DR095

**REFERENCE KEY PLAN - ROOF AND CANOPY**

**S-008**

FILE NAME = 070\_DR095\_S-101.dgn  
 PLOT DATE = 11/29/2016

vscarpa



Bench Mark: City of Chicago benchmark No. 551. Bench monument located about 50 feet south of the south line of W. 95th St., about 11 feet east of the west line of S. Perry Ave. Elevation 16.092.

Existing Structure: Structure No. 016-0437 was built in 1959, as F.A.I. Route 4-Project BI-04-B(46) In 1998, the superstructure was replaced with a 7 1/2" concrete deck on W36 beams. The superstructure consists of three continuous spans measuring 193'-0" from back to back of abutments, with an out-to-out width of 119'-1". The substructure consists of vaulted abutments and solid wall piers supported on spread footings.

Traffic shall be maintained at all times utilizing stage construction.

No salvage.

**HIGHWAY CLASSIFICATION**

FAP Rte. 0029 - U.S. Rte. 12/20 (95th St.)  
 Functional Class: Other principal arterial  
 ADT: 22,100 (2013); 23,433 (2040)  
 ADTT: 5.5%  
 DHV: 2110  
 Design Speed: 40 m.p.h.  
 Posted Speed: 30 m.p.h.  
 Two-Way Traffic  
 Directional Distribution: 69:31

**LOADING HS20-44 (New Const.)**

Allow 25#/sq. ft. for future wearing surface.

**DESIGN SPECIFICATIONS**

2002 AASHTO Standard Specifications for Highway Bridges 17th edition.

**DESIGN STRESSES**

FIELD UNITS	EXISTING
f'c = 5,000 psi	f'c = 3,500 psi
fy = 60,000 psi (Reinforcement)	fy = 60,000 psi (Reinf.)
fy = 50,000 psi (M270 Grade 50)	fy = 50,000 psi (Structural)
fy = 36,000 psi (M270 Grade 36)	fy = 36,000 psi

**SEISMIC DATA**

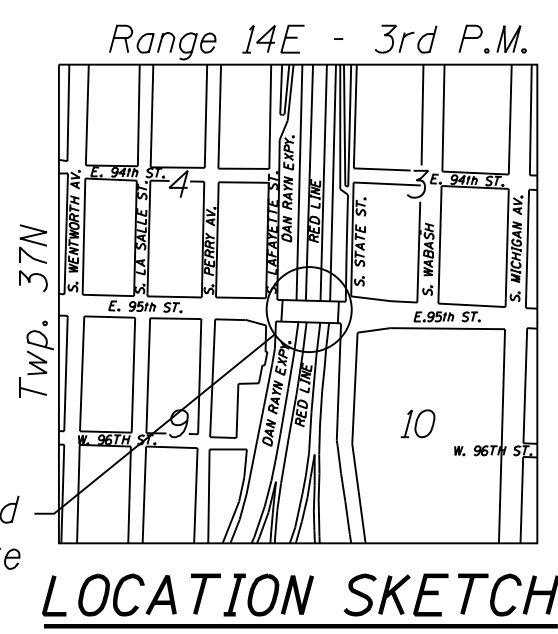
Seismic Performance Category (SPC) = A  
 Horizontal Bedrock Acceleration Coefficient (A) = 0.04g  
 Site Coefficient = 1.0

Notes:  
 Existing profile grades to remain unchanged.

No freefall deck drains will be permitted in the span over the tracks or within 10 ft. of cross arms of a railroad pole line.

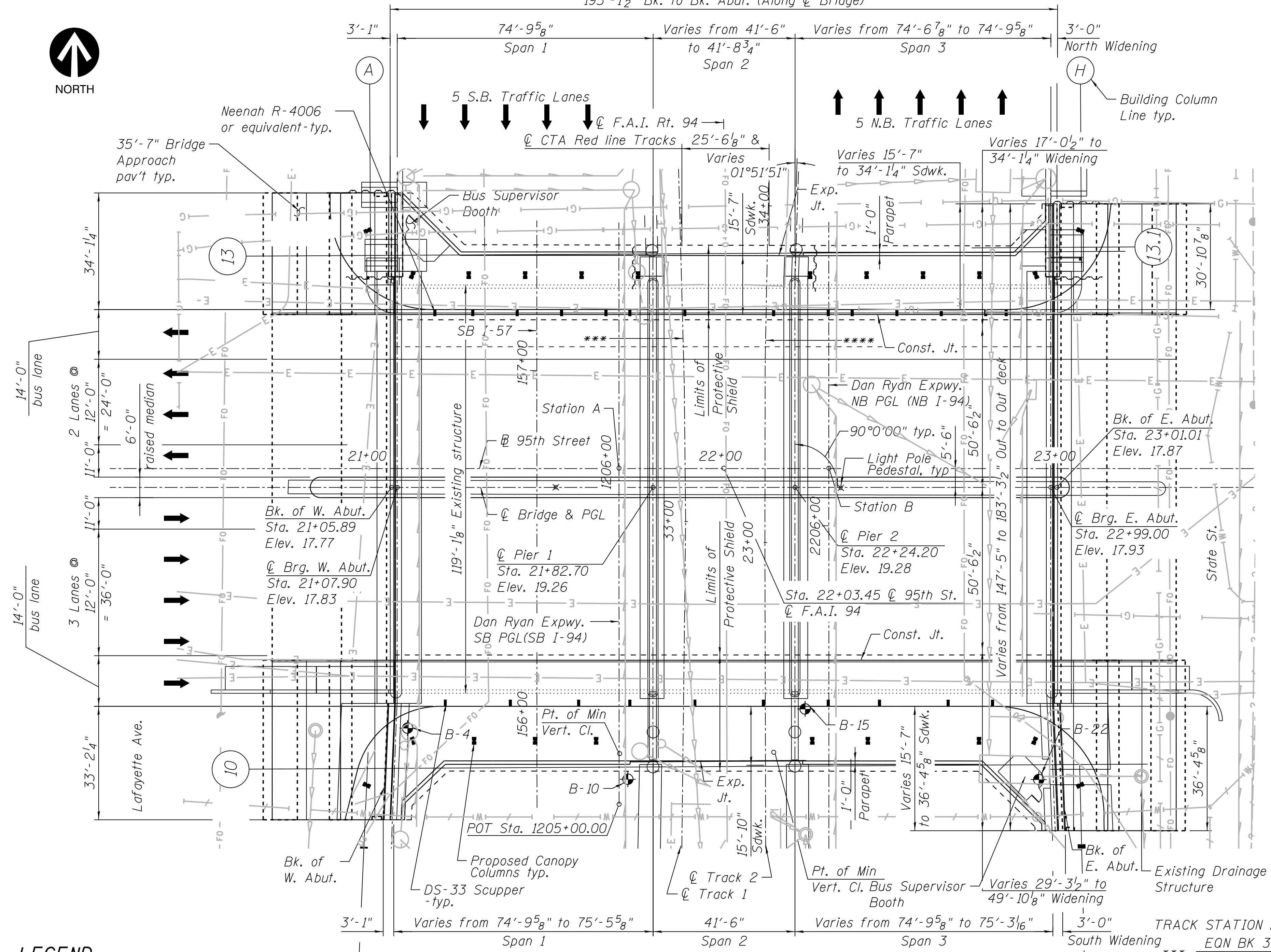
Milhouse Engineering and Construction, Inc.  
 Stan-lee C. Kaderbek  
 No. 81-4620  
 Expires 11-30-2016  
 I certify to the best of my knowledge, information and belief, this bridge design is structurally adequate for the design loading shown on these plans. The design is an economical one for the style of structure and complies with the requirements of the design specifications shown.

Collins Engineers, Incorporated  
 Ewa K. Mroczek  
 No. 81-6067  
 Expires 11-30-2016  
 Reviewed and approved for structural adequacy.



Station A  
 = Sta. 21+72.58 @ ⊙ 95th Street  
 = Sta. 1205+98.37 @ ⊙ I-94 SB Lanes

Station B  
 = Sta. 22+33.92 @ ⊙ 95th Street  
 = Sta. 2206+16.82 @ ⊙ I-94 NB Lanes



**LEGEND**

- ⊙ Indicates Boring Location
- Indicates Bus Canopy Column

**PLAN**



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**95th Terminal Improvement**  
 15 West 95th Street (South Terminal)  
 Chicago, IL 60628

IN CHARGE	S. KADERBEK
APPROVED BY	D.ZROKA
CHECKED BY	P.MAID
DESIGNED BY	L.SOMMER
DRAWN BY	T.SLEDGE
PROJECT NO.	2012-0021
FILE NAME	File Name

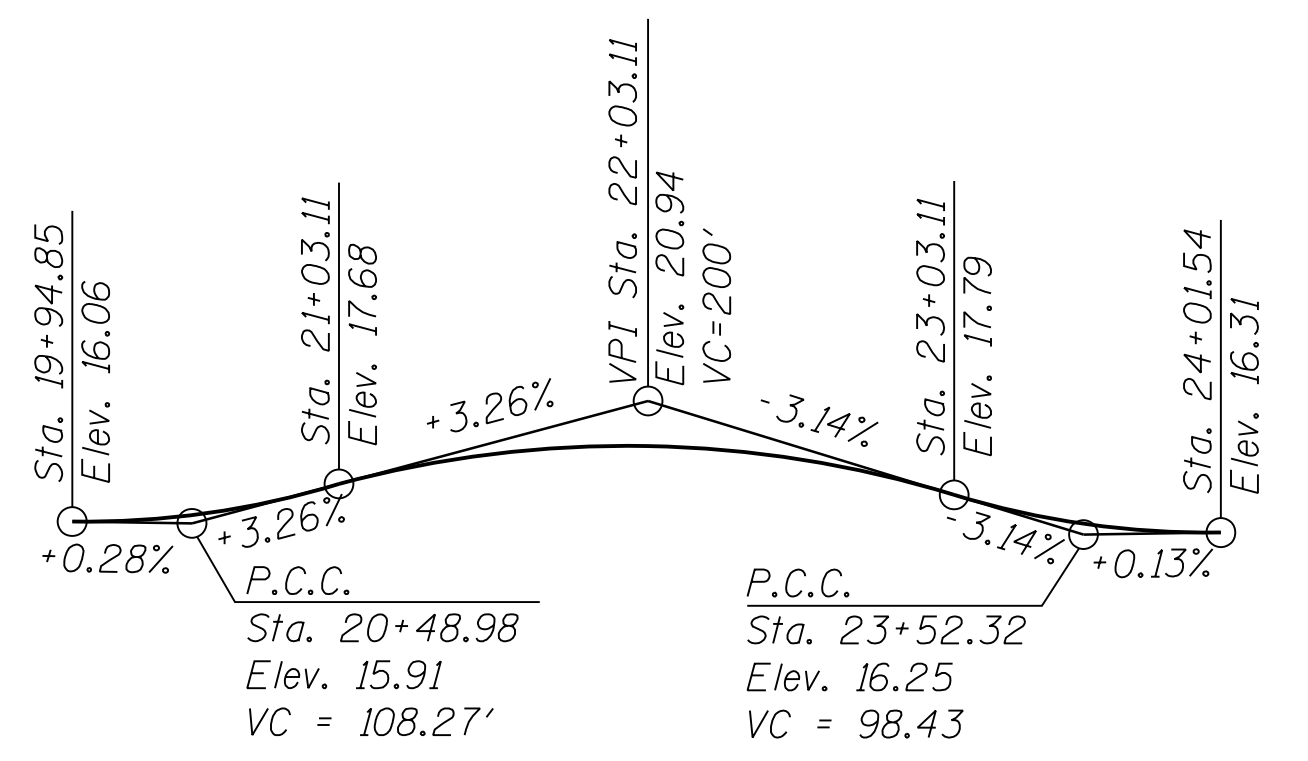
MARK	DATE	DESCRIPTION
	2/9/17	ISSUED FOR CONSTRUCTION

LOCATION IDENTIFIER: DR095

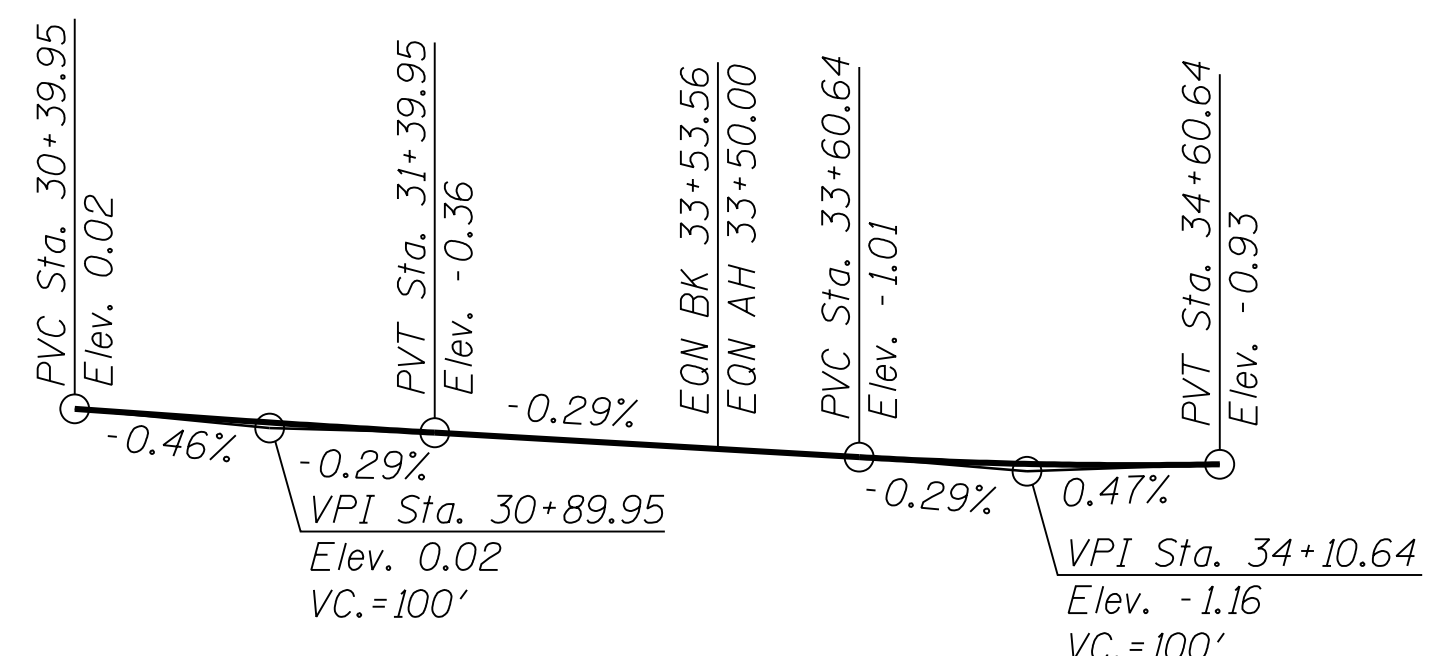
95TH ST. BRIDGE  
 GENERAL PLAN AND ELEVATION  
 SN 016-0437

**95TH-S-101**

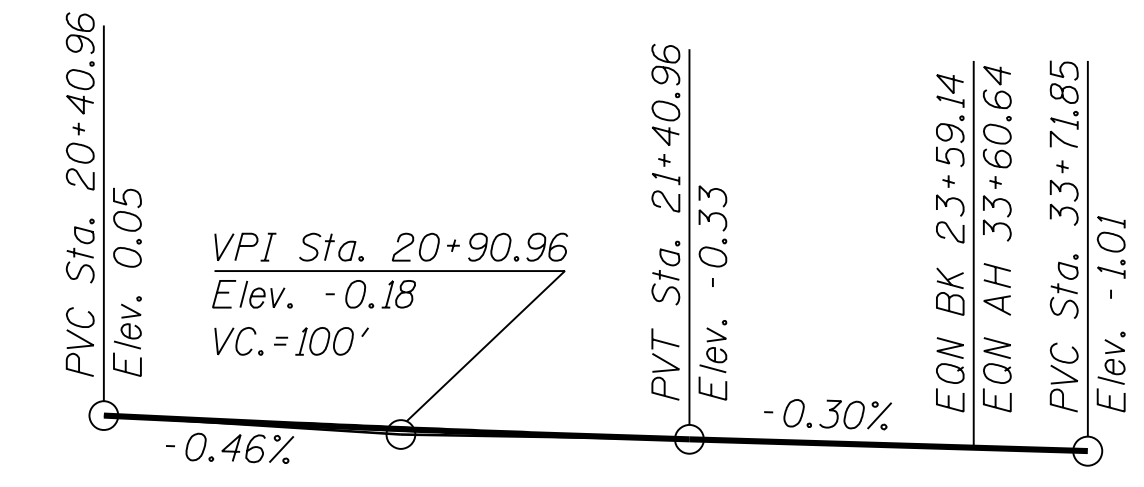
vscarpa



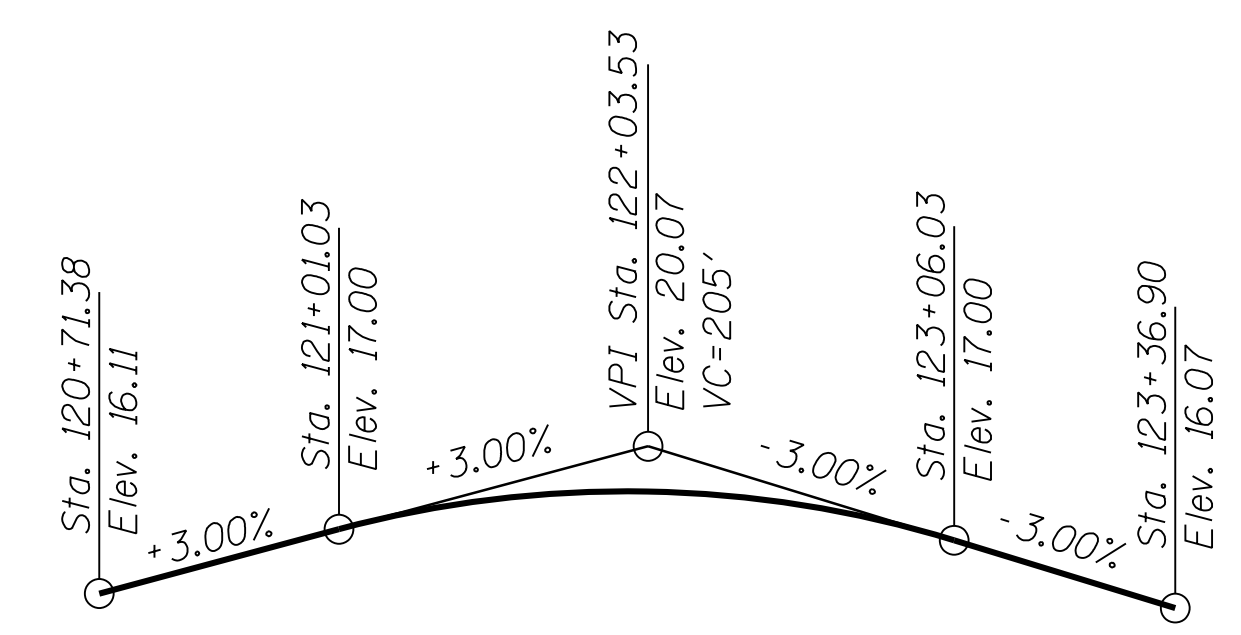
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(Along 95th St., See Note 1)



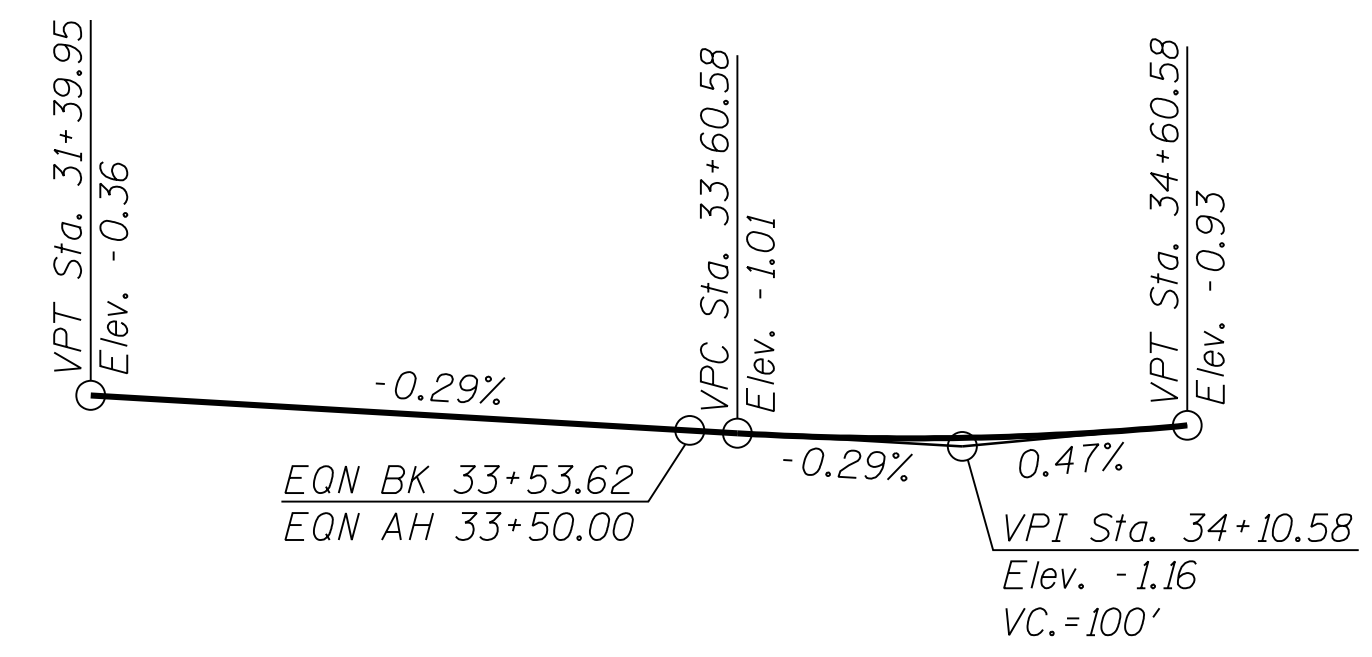
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(Along Track 1)



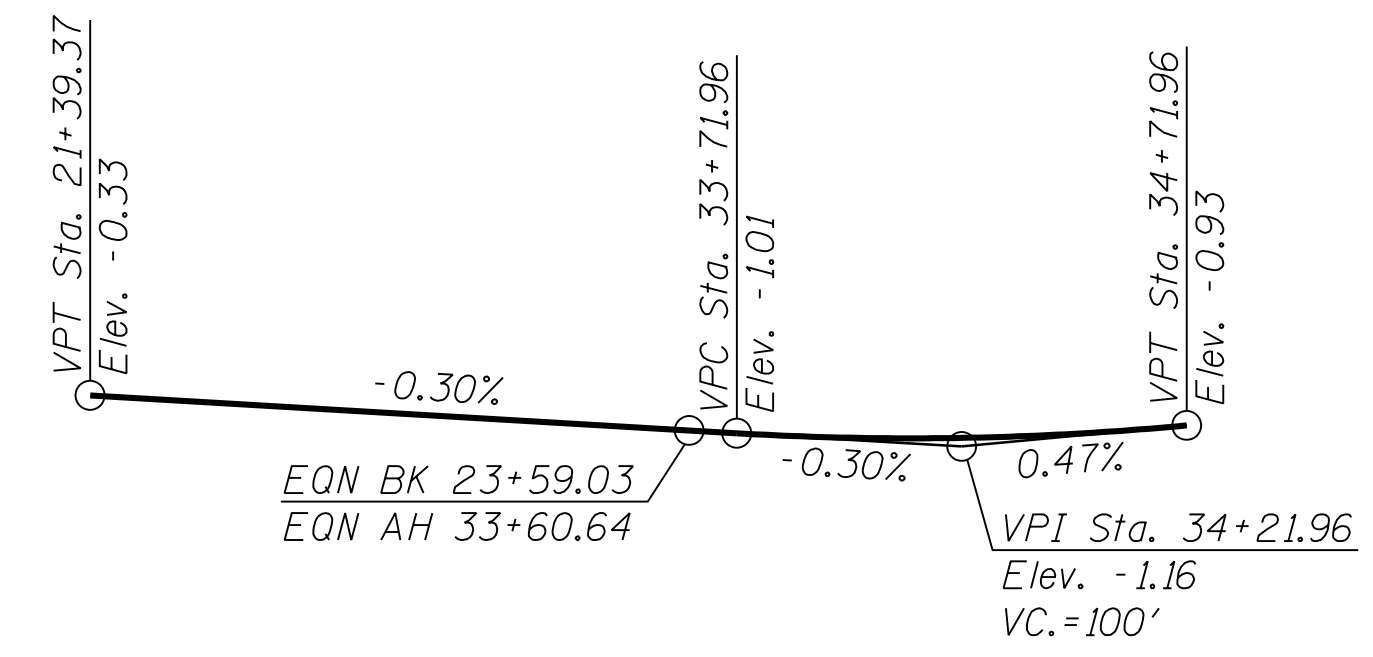
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(Along Track 2)



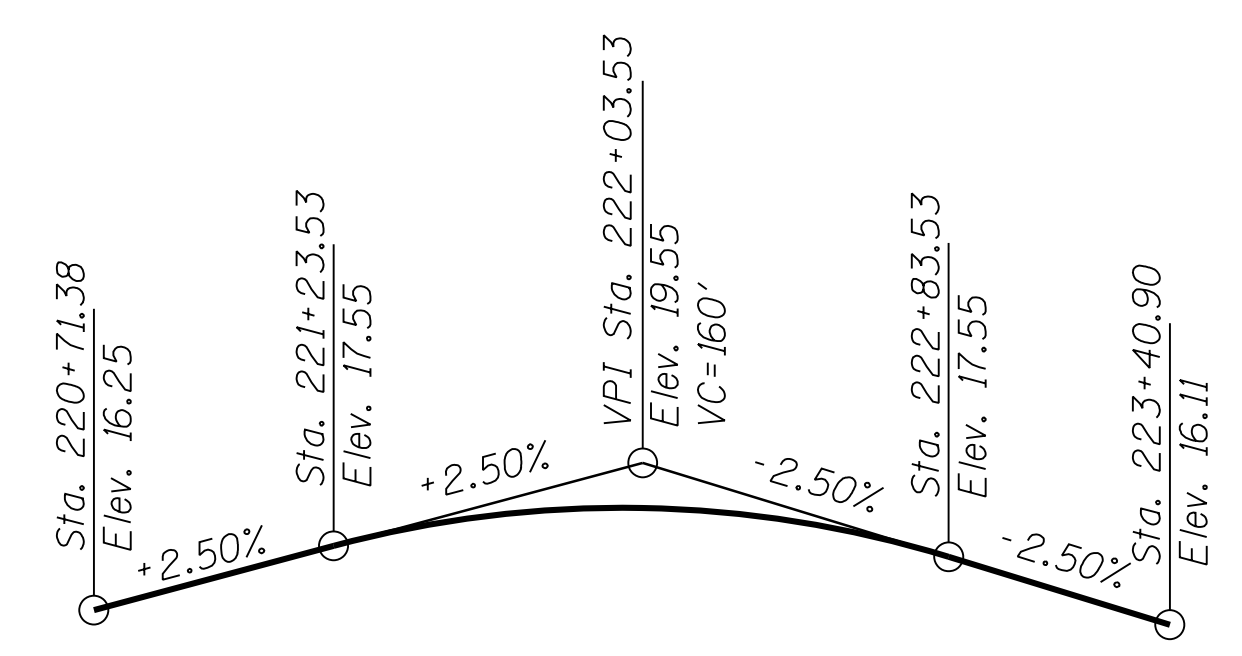
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(Along South Const. Jt., See Note 1)



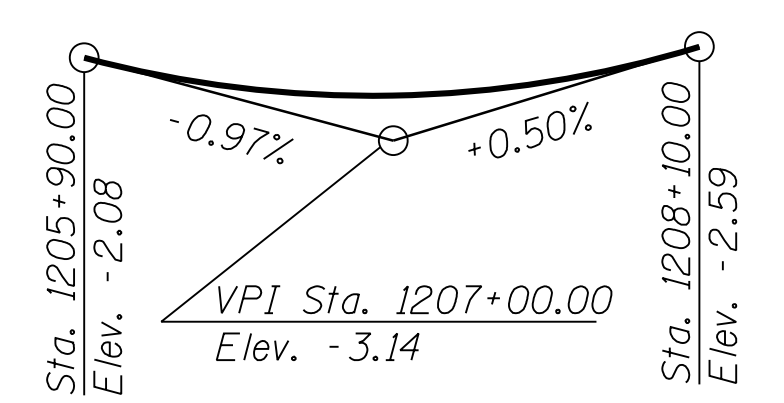
**PROPOSED PROFILE GRADE**  
(Along Track 1)



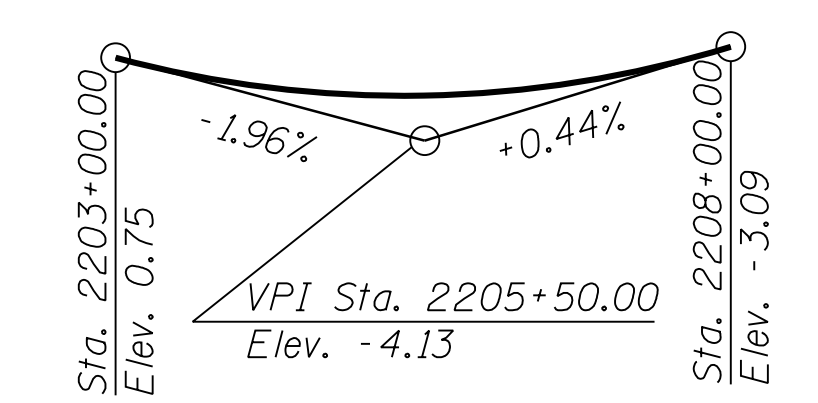
**PROPOSED PROFILE GRADE**  
(Along Track 2)



**EXISTING PROFILE GRADE**  
(Along North Const. Jt., See Note 1)



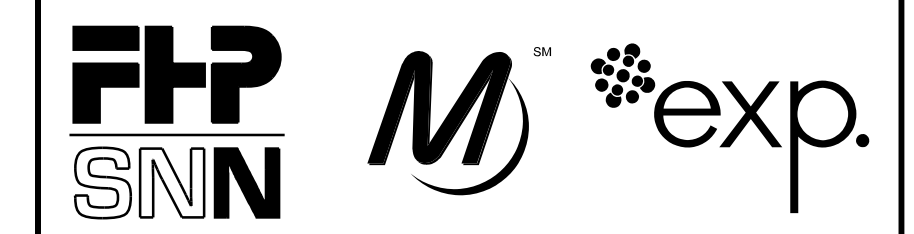
**EXISTING PROFILE GRADE**  
(Along SB I-94 Dan Ryan Expressway See Note 1)



**EXISTING PROFILE GRADE**  
(Along NB I-94 Dan Ryan Expressway See Note 1)

Note:  
1. Existing profiles are representative of a best fit from field survey by Sanchez & Associates Land Surveying on 05-21-2013 and 09-29-2014. Existing profile grade is to remain unchanged.

FILE NAME = 070\_DR095\_S-102.dgn  
PLOT DATE = 8/31/2016



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**95th Terminal Improvement**  
15 West 95th Street (South Terminal)  
Chicago, IL 60628

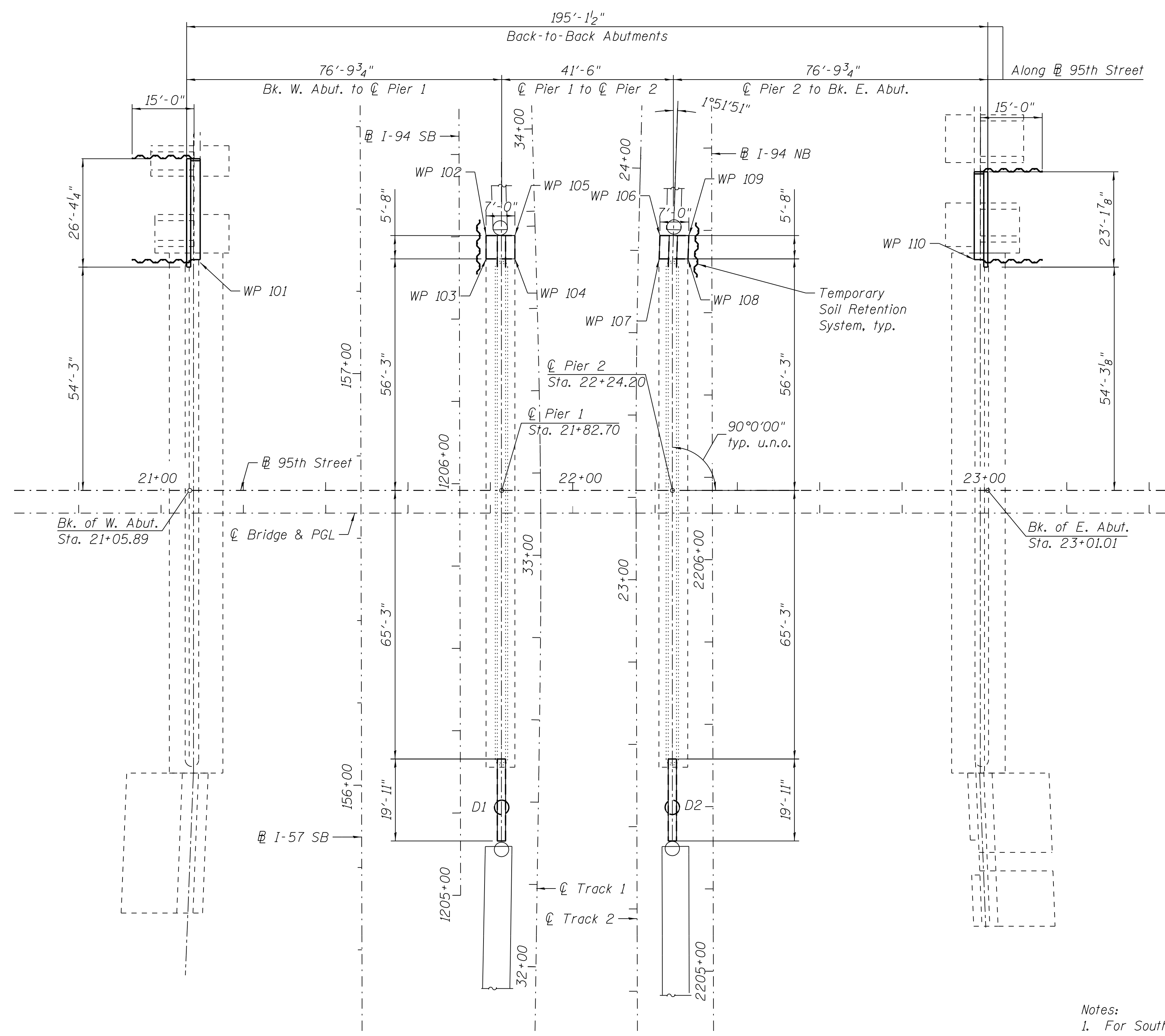
IN CHARGE	S. KADERBEK
APPROVED BY	D.ZROKA
CHECKED BY	P.MAID
DESIGNED BY	L.SOMMER
DRAWN BY	T.SLEDGE
PROJECT NO.	2012-0021
FILE NAME	File Name

MARK	DATE	DESCRIPTION
	2/9/17	ISSUED FOR CONSTRUCTION

LOCATION IDENTIFIER: **DR095**  
95TH ST. BRIDGE  
GENERAL DATA  
SN 016-0437

**95TH-S-102**

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**FOUNDATION PLAN**



**WORK POINTS**

WP No.	Station	Offset	Baseline
101	1206+54.59	-62.926	SB I-94
102	1206+60.28	6.510	SB I-94
103	1206+54.62	6.547	SB I-94
104	1206+54.59	13.547	SB I-94
105	1206+60.26	13.509	SB I-94
106	2206+78.77	-12.592	NB I-94
107	2206+73.10	-12.826	NB I-94
108	2206+73.08	-5.826	NB I-94
109	2206+78.74	-5.589	NB I-94
110	2206+72.77	63.741	NB I-94

**DRILLED SHAFTS**

Drilled Shaft	Station	Offset	Baseline
D1	1205+21.39	10.002	SB I-94
D2	2205+39.99	-9.863	NB I-94

- Notes:
1. For South Widening Abutment Details, See Advance Package 2 Plans.
  2. For North Widening Abutment Details, See Sheets 95TH-S-135 and 95TH-S-140.
  3. For Pier Widening Details, See sheets 95TH-S-145, 95TH-S-146, 95TH-S-148, 95TH-S-149 & 95TH-S-150.
  4. Work Points are based on Alignments Established on Sheets V-100 thru V-104.



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**95th Terminal Improvement**  
 14 West 95th Street (North Terminal)  
 15 West 95th Street (South Terminal)  
 Chicago, IL 60628

<b>IN CHARGE</b>	S. KADERBEK
<b>APPROVED BY</b>	D.ZROKA
<b>CHECKED BY</b>	P.MAID
<b>DESIGNED BY</b>	L.SOMMER
<b>DRAWN BY</b>	T.SLEDGE
<b>PROJECT NO.</b>	2012-0021
<b>FILE NAME</b>	File Name

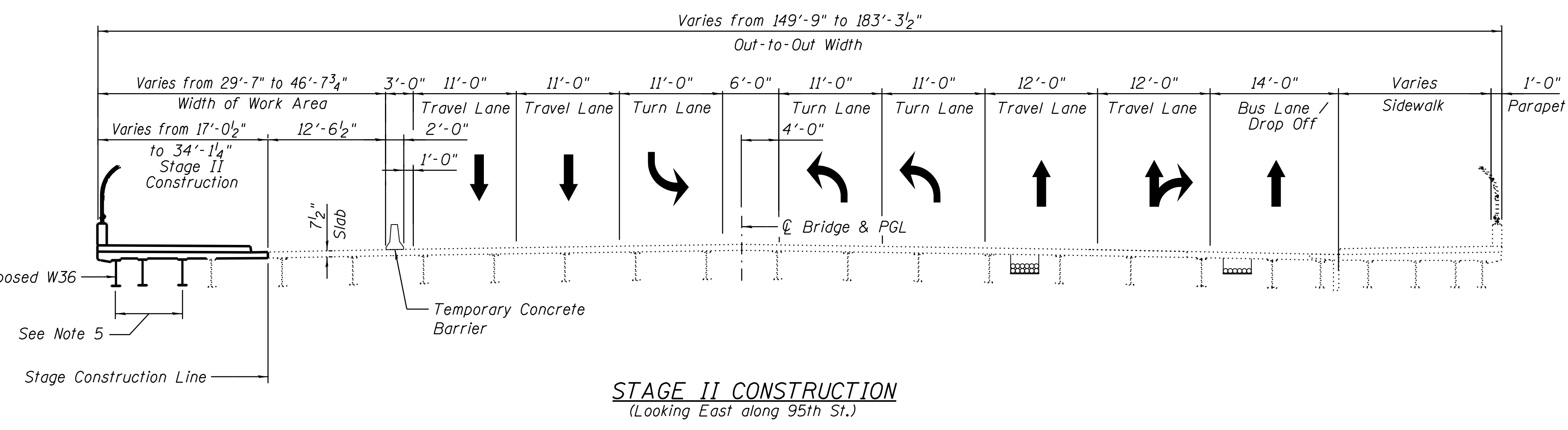
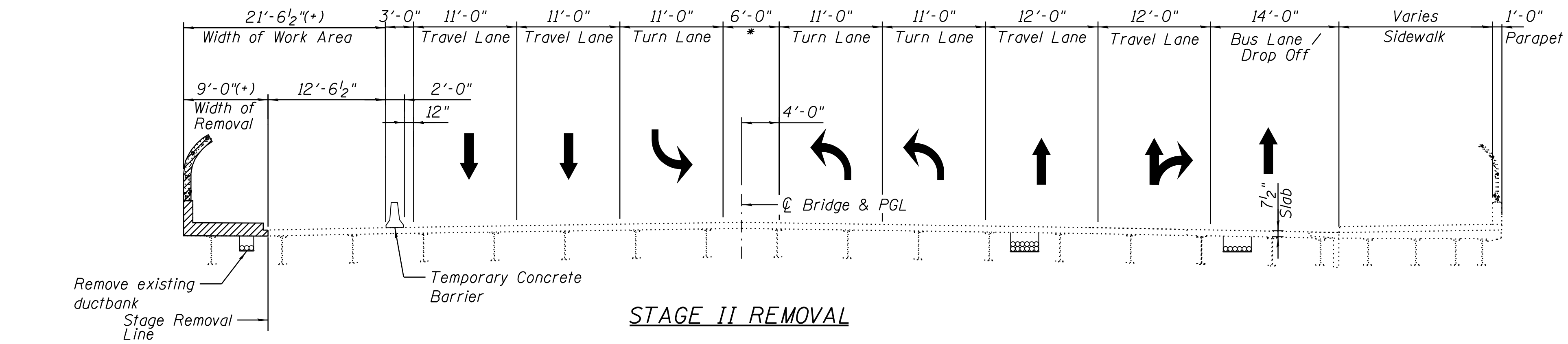
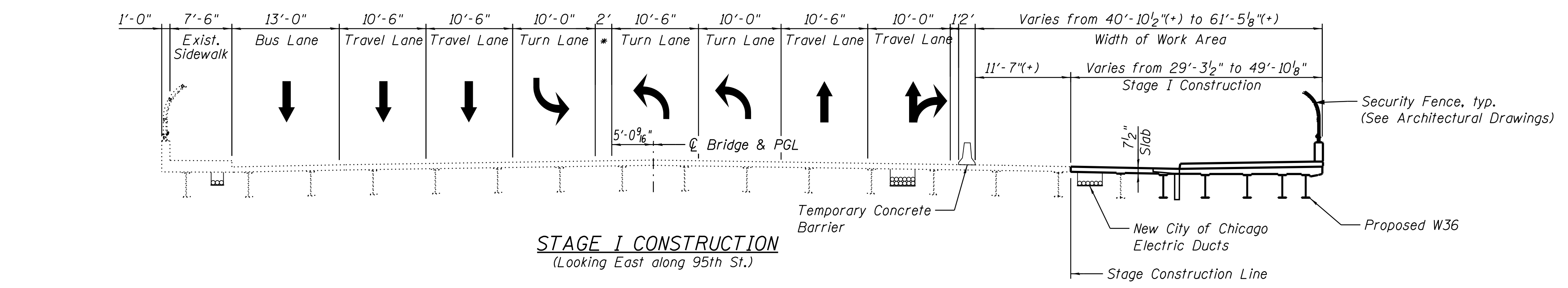
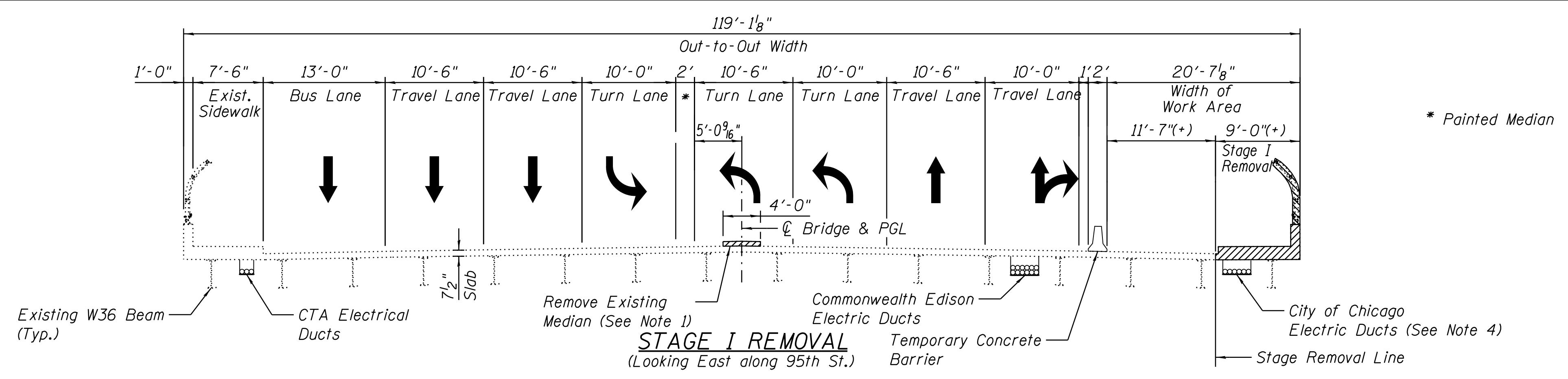
MARK	DATE	DESCRIPTION
	10/06/16	ISSUED FOR CONSTRUCTION

**LOCATION IDENTIFIER: DR095**  
 95TH ST. BRIDGE  
 FOOTING LAYOUT  
 SN 016-0437

**95TH-S-103**

FILE NAME = 070\_DR095\_S-103.dgn  
 PLOT DATE = 10/7/2016

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**LEGEND**

..... Existing

▨ Removal

— Proposed

- Notes:
1. Removal of the existing median along 95th Street shall be completed utilizing temporary lane closures prior to commencement of the 95th St. Stage I Maintenance of Traffic Plan. For additional information, See Maintenance of Traffic Plans. SEE IDOT STANDARD 701602, DISTRICT 1 DETAIL TC-17.
  2. Existing Bridge Deck and widened portion of the deck shall be shot blasted prior to thin polymer overlay installation. See specifications.
  3. Proposed concrete median, shot blasting, thin polymer overlay and expansion joint shall be completed utilizing temporary lane closures. SEE IDOT STANDARD 701701. For additional information, See Maintenance of Traffic Plans
  4. City of Chicago ductbank to be relocated via aerial lines prior to deck removal, see Traffic Signal and Utility Plans.
  5. Bottom of slab level under sidewalk.
  6. Refer to Electrical Drawings for Lighting Details and Locations.
  7. See sheet 95TH-S-105 for final bridge cross section.



**CHICAGO TRANSIT AUTHORITY ENGINEERING**

**SENSITIVE SECURITY INFORMATION**

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**95th Terminal Improvement**  
15 West 95th Street (South Terminal)  
Chicago, IL 60628

<b>IN CHARGE</b>	S. KADERBEK
<b>APPROVED BY</b>	D.ZROKA
<b>CHECKED BY</b>	P.MAID
<b>DESIGNED BY</b>	L.SOMMER
<b>DRAWN BY</b>	T.SLEDGE
<b>PROJECT NO.</b>	2012-0021
<b>FILE NAME</b>	File Name

MARK	DATE	DESCRIPTION
	2/9/17	ISSUED FOR CONSTRUCTION

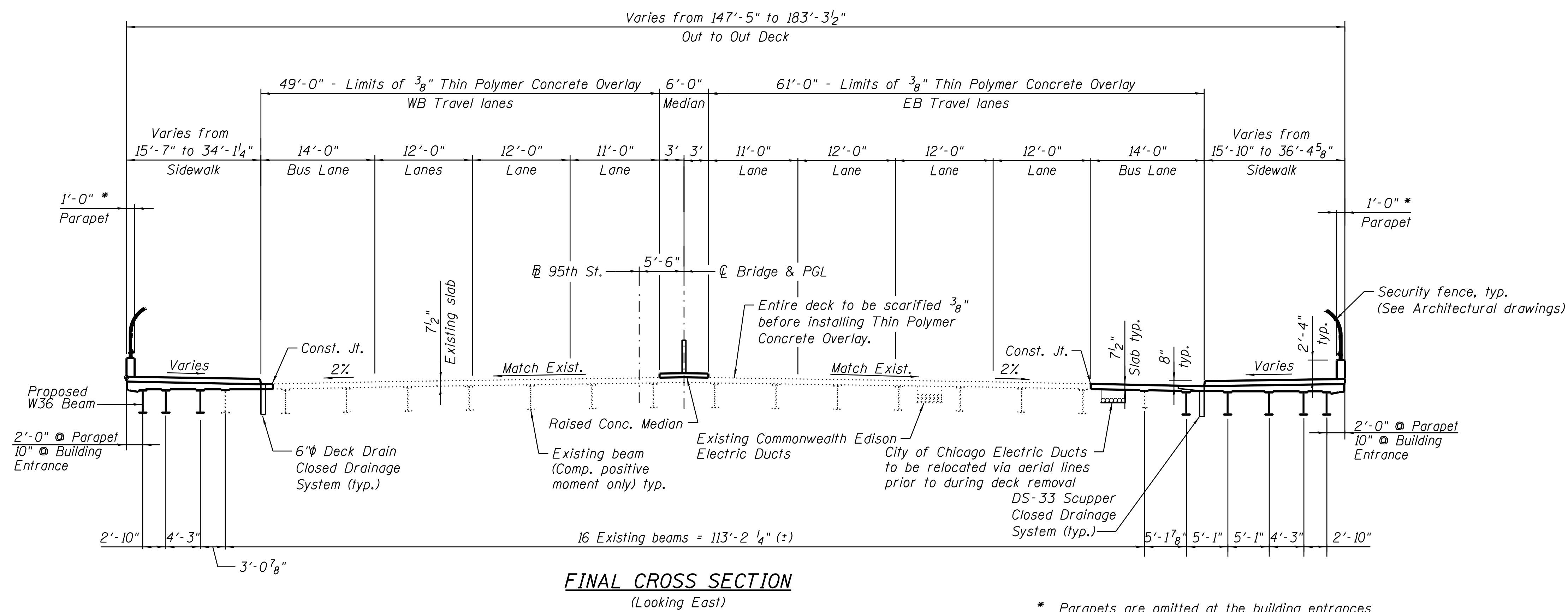
**LOCATION IDENTIFIER: DR095**

**95TH ST. BRIDGE**  
**STAGE CONSTRUCTION DETAILS**  
(SHEET 1 OF 3)  
SN 016-0437

**95TH-S-104**

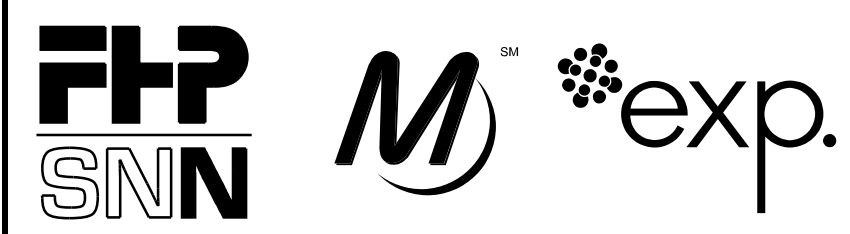
FILE NAME = 070\_DR095\_S-104.dgn  
PLOT DATE = 8/31/2016

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\* Parapets are omitted at the building entrances

Notes:  
1. The 3/8" Thin Polymer Overlay to be included on the new and existing bridge decks, shall be in accordance with specification 32 13 33.



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**95th Terminal Improvement**  
15 West 95th Street (South Terminal)  
Chicago, IL 60628

IN CHARGE	S. KADERBEK
APPROVED BY	D.ZROKA
CHECKED BY	P.MAID
DESIGNED BY	L.SOMMER
DRAWN BY	T.SLEDGE
PROJECT NO.	2012-0021
FILE NAME	File Name

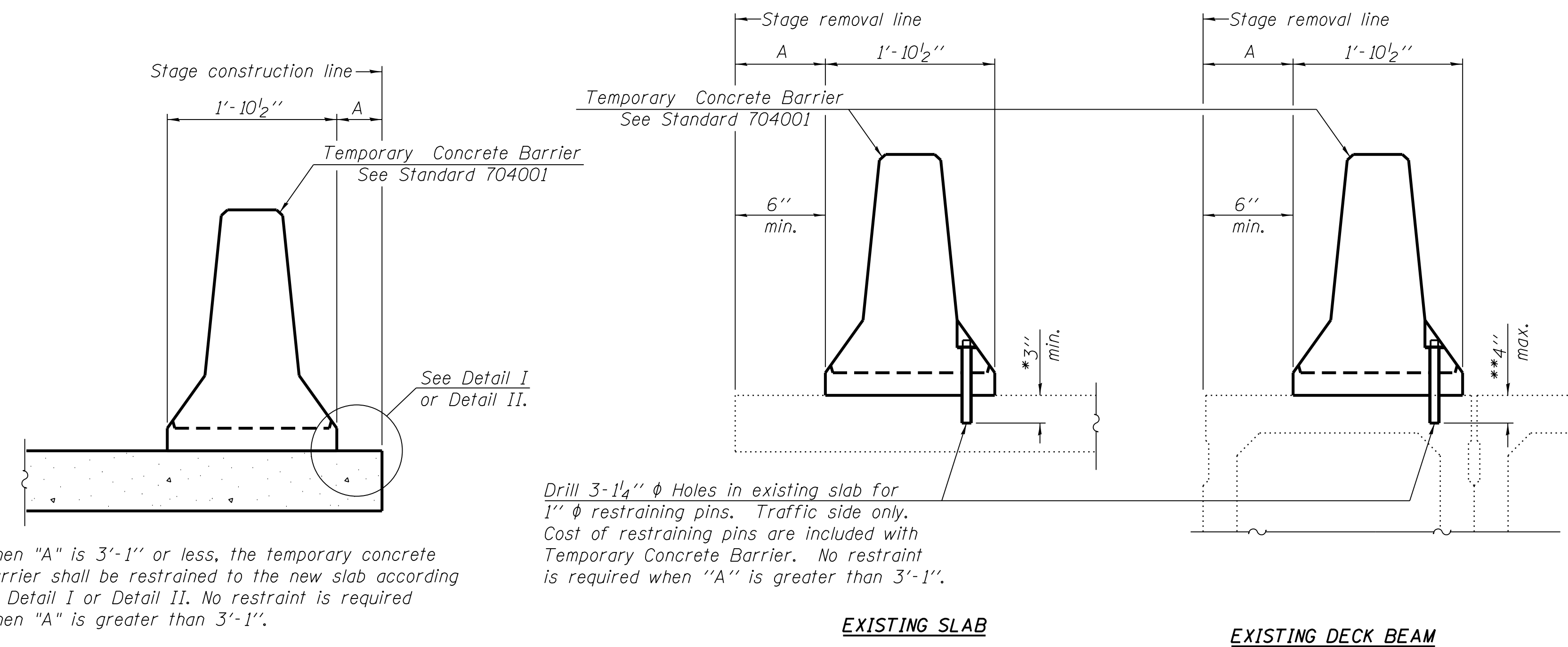
MARK	DATE	DESCRIPTION
	2/9/17	ISSUED FOR CONSTRUCTION

LOCATION IDENTIFIER: **DR095**  
95TH ST. BRIDGE  
STAGE CONSTRUCTION DETAILS  
(SHEET 3 OF 3)  
SN 016-0437

**95TH-S-105**

FILE NAME = 070\_DR095\_S-105.dgn  
PLOT DATE = 8/31/2016

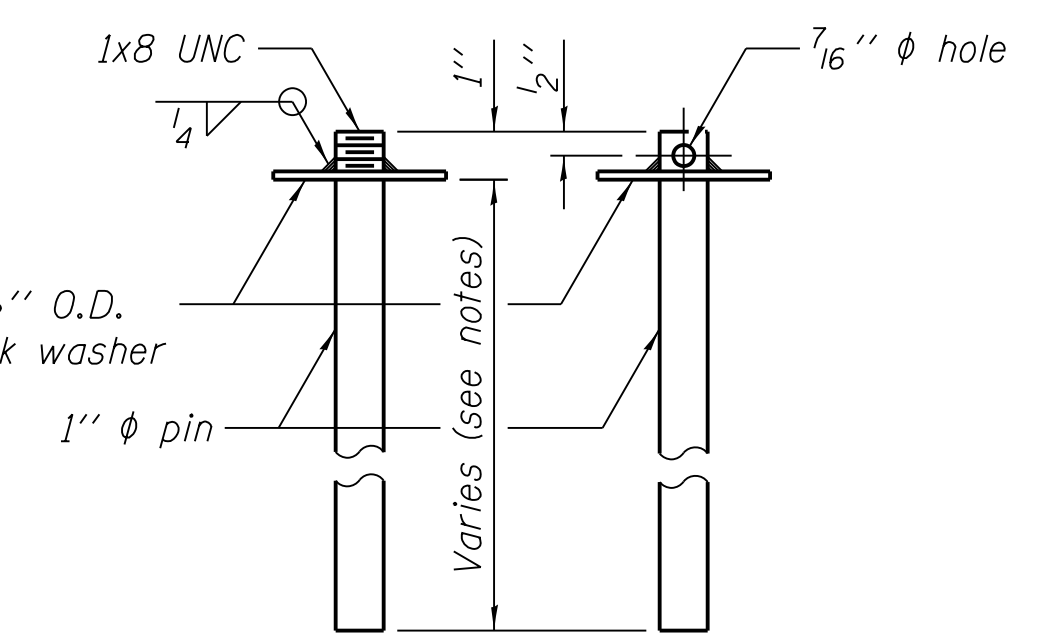
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When "A" is 3'-1" or less, the temporary concrete barrier shall be restrained to the new slab according to Detail I or Detail II. No restraint is required when "A" is greater than 3'-1".

Drill 3-1/4"  $\phi$  Holes in existing slab for 1"  $\phi$  restraining pins. Traffic side only. Cost of restraining pins are included with Temporary Concrete Barrier. No restraint is required when "A" is greater than 3'-1".

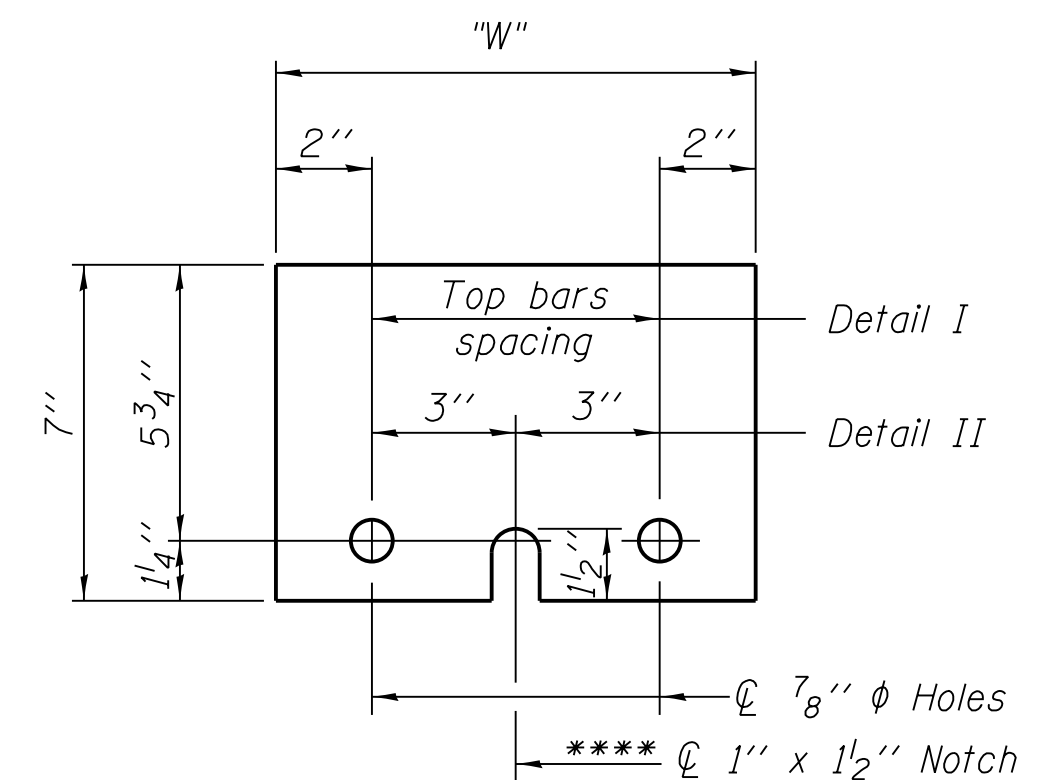
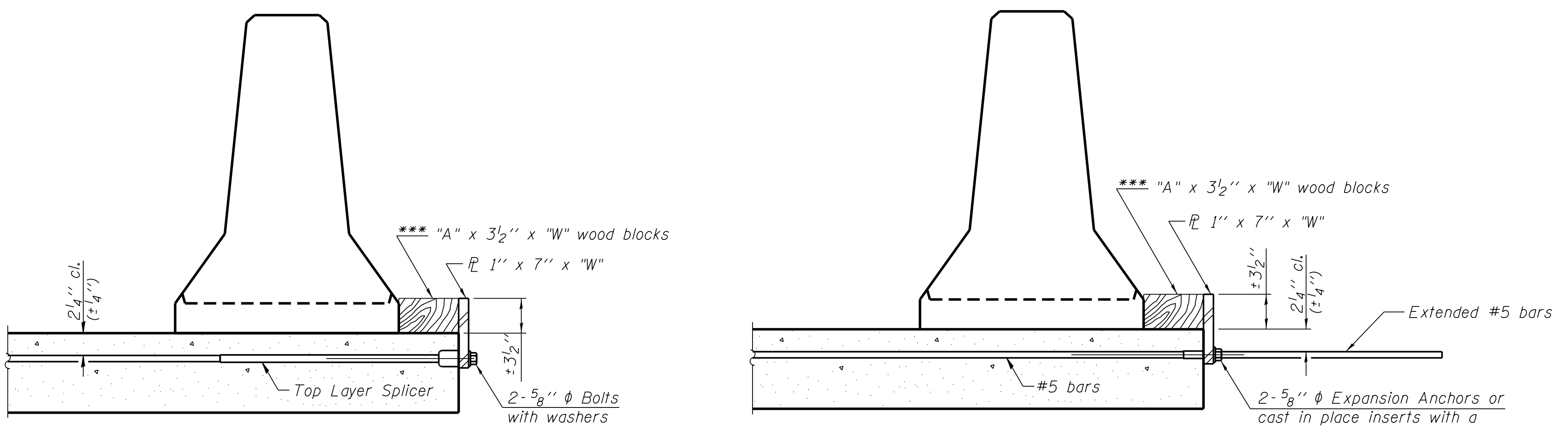
\* Dimension shown is minimum required embedment into concrete. If hot-mix asphalt wearing surface is present, minimum embedment shall be in addition to wearing surface depth.  
 \*\* If existing deck beam is to remain in place after stage construction, embedment shall only be into wearing surface and not into existing deck beam concrete.



US Std. 1 1/16" I.D. x 2 1/2" O.D. x approx. 8 gauge thick washer

NEW SLAB

SECTIONS THRU SLAB OR DECK BEAM



STEEL RETAINER PLATE 1" x 7" x "W"

\*\*\*\* Required only with Detail II

NOTES

Detail I - With Bar Splicer or Couplers:  
 Connect one (1) 1" x 7" x "W" steel plate to the top layer of couplers with 2-5/8"  $\phi$  bolts screwed to coupler at approximate center of each barrier panel.  
 Detail II - With Extended Reinforcement Bars:  
 Connect one (1) 1" x 7" x "W" steel plate to the concrete slab or concrete wearing surface with 2-5/8"  $\phi$  Expansion Anchors or cast in place inserts spaced between the top layer of reinforcement at approximate center of each barrier panel.  
 Cost of retainer assembly is included with Temporary Concrete Barrier. The 1" x 7" x "W" plate shall not be removed until stage II construction forms and all reinforcement bars are in place and the concrete is ready to be placed.

\*\*\*\* Wood blocks may be omitted when required to provide minimum stage traffic lane width. When the wood blocks are omitted, the concrete barrier shall be in direct contact with the steel retainer plate.



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95th Terminal Improvement  
 15 West 95th Street (South Terminal)  
 Chicago, IL 60628

IN CHARGE	S. KADERBEK
APPROVED BY	D.ZROKA
CHECKED BY	P.MAID
DESIGNED BY	L.SOMMER
DRAWN BY	T.SLEDGE
PROJECT NO.	2012-0021
FILE NAME	File Name

MARK	DATE	DESCRIPTION
	2/9/17	ISSUED FOR CONSTRUCTION

LOCATION IDENTIFIER: DR095  
 95TH ST. BRIDGE  
 TEMPORARY CONCRETE BARRIER  
 SN 016-0437

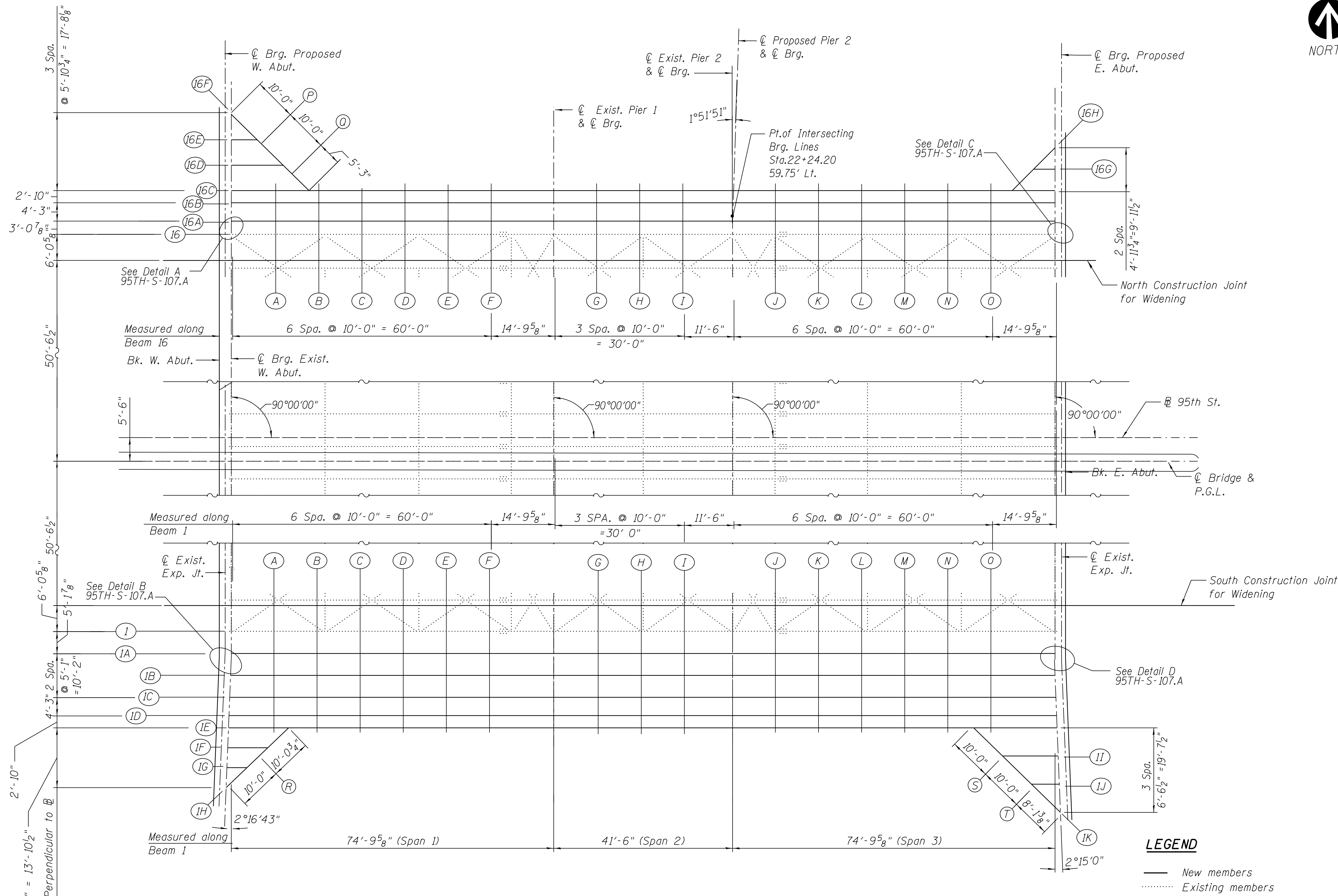
95TH-S-106

FILE NAME = 070\_DR095\_S-106.dgn  
 PLOT DATE = 8/31/2016



FILE NAME = 070\_DR095\_S-107.dgn  
 PLOT DATE = 8/31/2016

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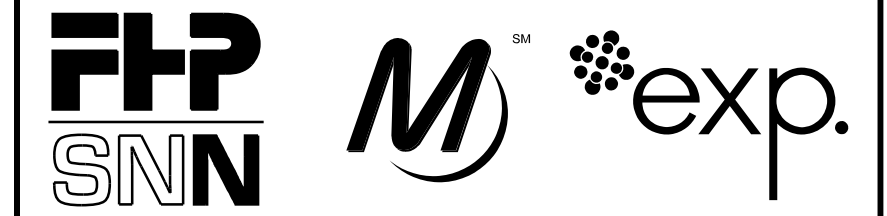
**PLAN**

**LEGEND**

- New members
- ..... Existing members

Notes:

1. All offsets from  $\hat{C}$  Bridge & P.G.L.
2. Elevations set with respect to Profile Grade at Const. Jt. See 95TH-S-102



**cta** CHICAGO TRANSIT AUTHORITY ENGINEERING

**SENSITIVE SECURITY INFORMATION**

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**95th Terminal Improvement**  
 15 West 95th Street (South Terminal)  
 Chicago, IL 60628

IN CHARGE	S. KADERBEK
APPROVED BY	D.ZROKA
CHECKED BY	P.MAID
DESIGNED BY	L.SOMMER
DRAWN BY	T.SLEDGE
PROJECT NO.	2012-0021
FILE NAME	File Name

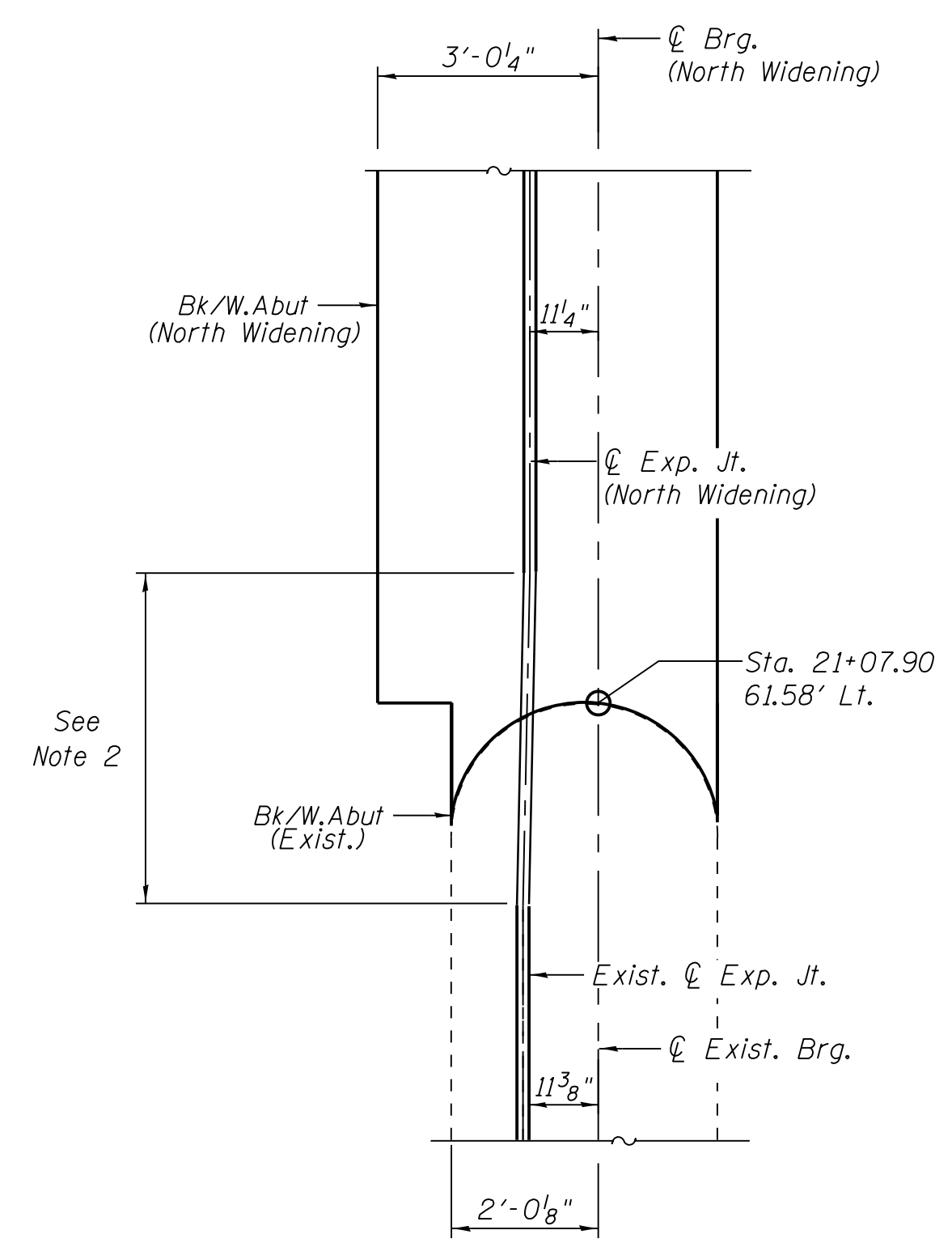
MARK	DATE	DESCRIPTION
	2/9/17	ISSUED FOR CONSTRUCTION

LOCATION IDENTIFIER: **DR095**

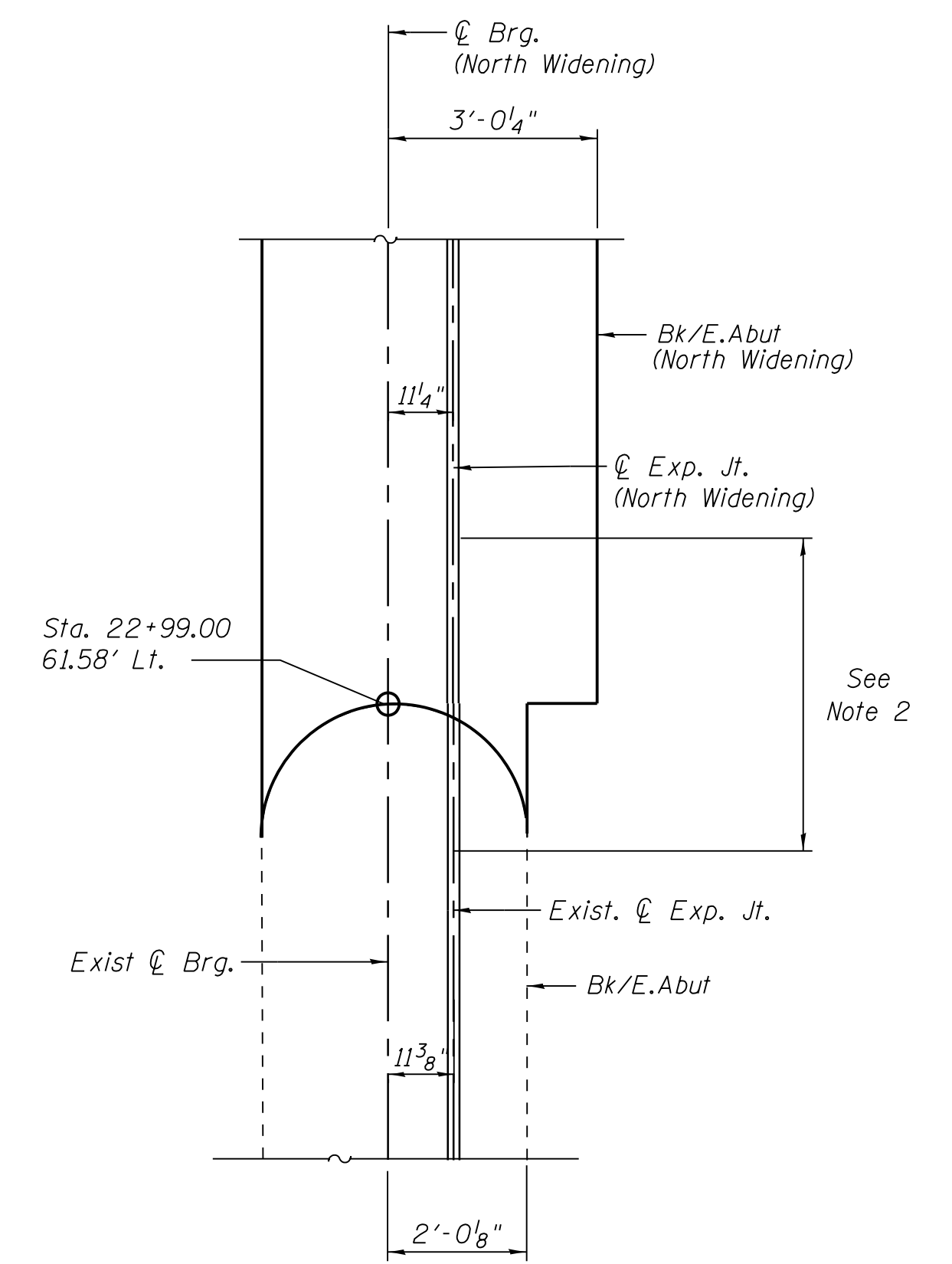
95TH ST. BRIDGE  
 TOP OF SLAB ELEVATIONS  
 (SHEET 1 OF 6)  
 SN 016-0437

**95TH-S-107**

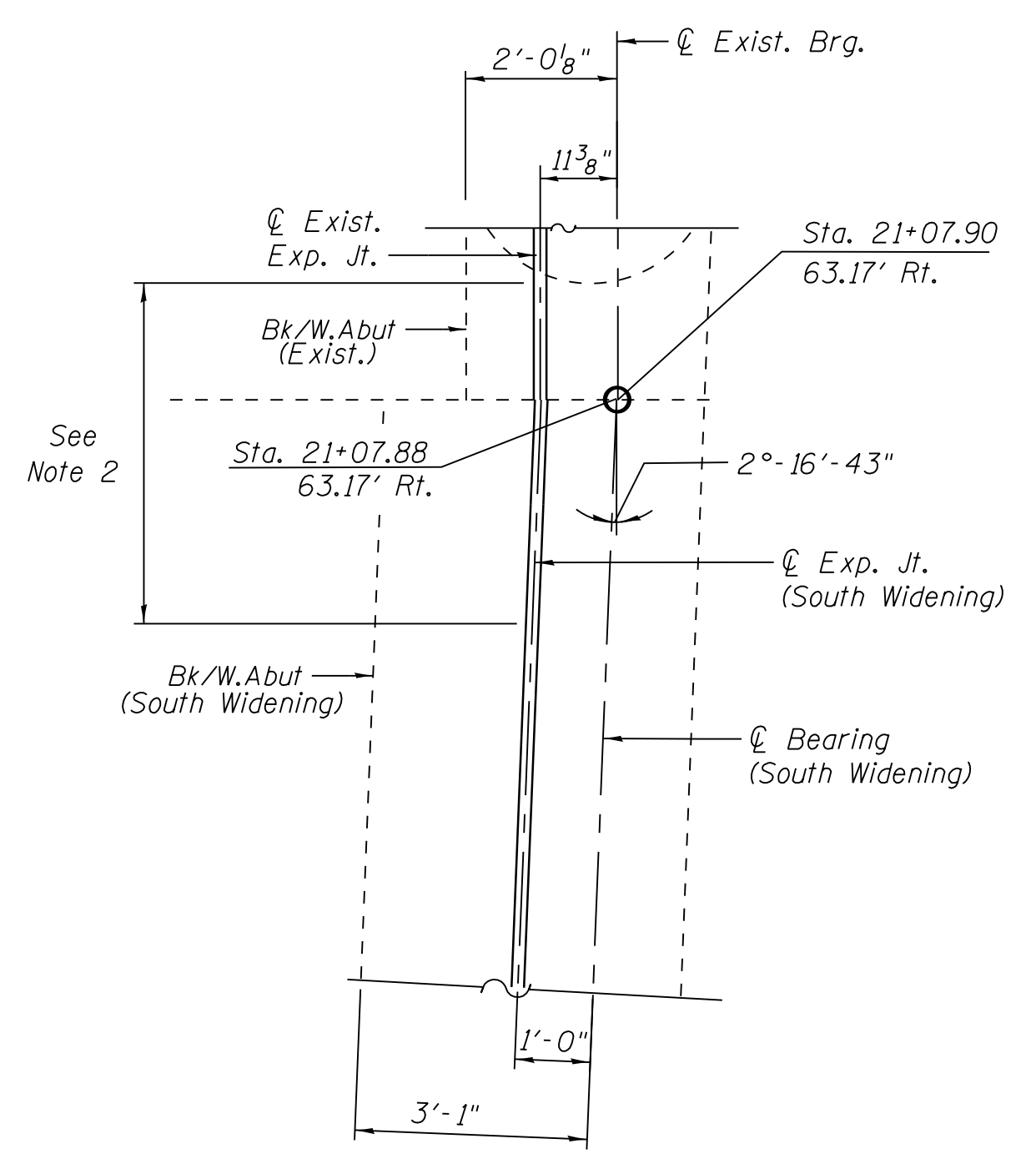
vscarpa



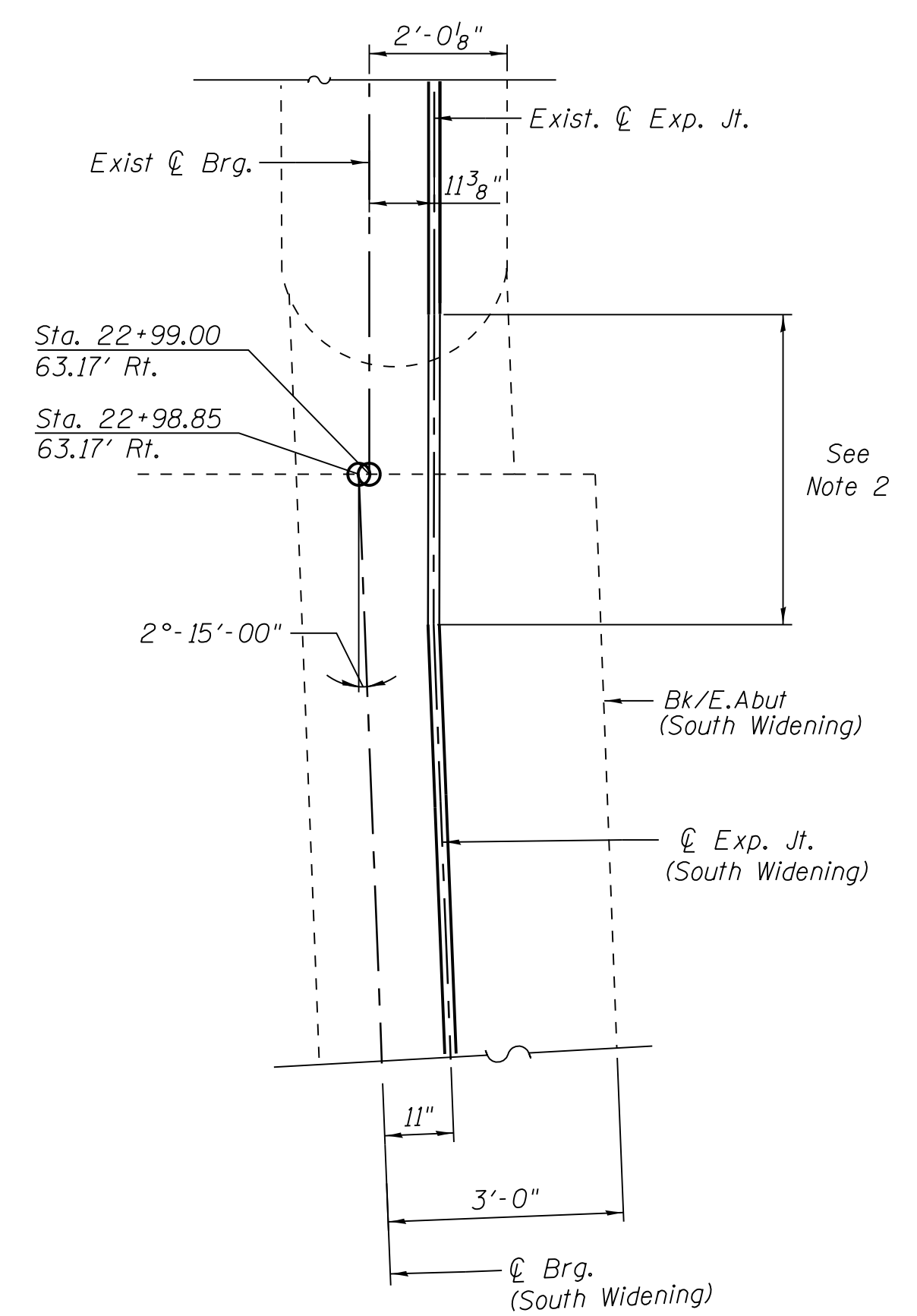
**DETAIL A**



**DETAIL C**



**DETAIL B**



**DETAIL D**

- Notes:
- 1) All offsets with respect to  $\varnothing$  Bridge & P.G.L.
  - 2) Contractor to transition expansion joint at widened abutments so that joint is continuous over bridge.

FILE NAME = 070\_DR095\_S-107A.dgn  
PLOT DATE = 8/31/2016



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**95th Terminal Improvement**  
15 West 95th Street (South Terminal)  
Chicago, IL 60628

IN CHARGE	S. KADERBEK
APPROVED BY	D.ZROKA
CHECKED BY	P.MAID
DESIGNED BY	L.SOMMER
DRAWN BY	T.SLEDGE
PROJECT NO.	2012-0021
FILE NAME	File Name

MARK	DATE	DESCRIPTION
	2/9/17	ISSUED FOR CONSTRUCTION

LOCATION IDENTIFIER: **DR095**

95TH ST. BRIDGE  
TOP OF SLAB ELEVATIONS  
(SHEET 2 OF 6)  
SN 016-0437

**95TH-S-107A**

**SOUTH CONSTRUCTION JOINT**

Location	Station	Offset	Theoretical Grade Elevations	Theoretical Grade Elevations Adjusted For Dead Load Deflection
BK W. ABUT	21+05.89	50.54	17.14	17.14
CL EXP JT	21+06.95	50.54	17.17	17.17
CL BRG. W. ABUT	21+07.90	50.54	17.20	17.20
A	21+17.90	50.54	17.46	17.50
B	21+27.90	50.54	17.70	17.78
C	21+37.90	50.54	17.90	18.00
D	21+47.90	50.54	18.08	18.18
E	21+57.90	50.54	18.23	18.31
F	21+67.90	50.54	18.35	18.40
CL BRG. PIER 1	21+82.70	50.54	18.47	18.47
G	21+92.70	50.54	18.52	18.50
H	22+02.70	50.54	18.53	18.51
I	22+12.70	50.54	18.52	18.50
CL BRG. PIER 2	22+24.20	50.54	18.47	18.47
J	22+34.20	50.54	18.40	18.43
K	22+44.20	50.54	18.29	18.36
L	22+54.20	50.54	18.16	18.25
M	22+64.20	50.54	17.99	18.09
N	22+74.20	50.54	17.80	17.89
O	22+84.20	50.54	17.58	17.64
CL BRG. E. ABUT	22+99.00	50.54	17.20	17.20
CL EXP JT	22+99.95	50.54	17.17	17.17
BK E. ABUT	23+01.01	50.54	17.14	17.14

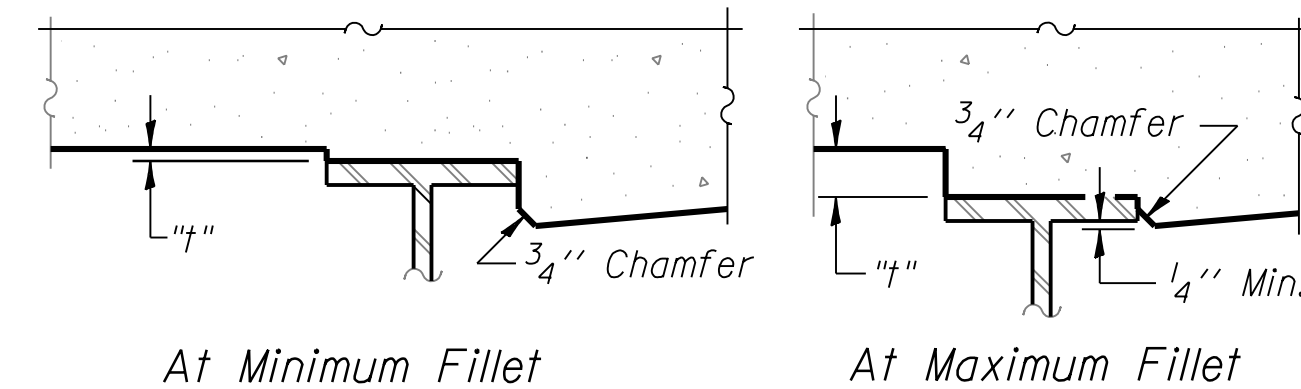
**BEAM 1**

Location	Station	Offset	Theoretical Grade Elevations	Theoretical Grade Elevations Adjusted For Dead Load Deflection
BK W. ABUT	21+05.89	56.59	17.02	17.02
CL EXP JT	21+06.95	56.59	17.05	17.05
CL BRG. W. ABUT	21+07.90	56.59	17.08	17.08
A	21+17.90	56.59	17.34	17.38
B	21+27.90	56.59	17.58	17.66
C	21+37.90	56.59	17.78	17.88
D	21+47.90	56.59	17.96	18.06
E	21+57.90	56.59	18.11	18.19
F	21+67.90	56.59	18.23	18.28
CL BRG. PIER 1	21+82.70	56.59	18.35	18.35
G	21+92.70	56.59	18.40	18.38
H	22+02.70	56.59	18.41	18.39
I	22+12.70	56.59	18.40	18.38
CL BRG. PIER 2	22+24.20	56.59	18.35	18.35
J	22+34.20	56.59	18.27	18.30
K	22+44.20	56.59	18.17	18.24
L	22+54.20	56.59	18.04	18.13
M	22+64.20	56.59	17.87	17.97
N	22+74.20	56.59	17.68	17.77
O	22+84.20	56.59	17.46	17.52
CL BRG. E. ABUT	22+99.00	56.59	17.08	17.08
CL EXP JT	22+99.95	56.59	17.05	17.05
BK E. ABUT	23+01.01	56.59	17.02	17.02

**BEAM 1A**

Location	Station	Offset	Theoretical Grade Elevations	Theoretical Grade Elevations Adjusted For Dead Load Deflection
BK W. ABUT	21+05.89	61.75	16.92	16.92
CL EXP JT	21+06.95	61.75	16.95	16.95
CL BRG. W. ABUT	21+07.90	61.75	16.97	16.97
A	21+17.90	61.75	17.24	17.28
B	21+27.90	61.75	17.47	17.54
C	21+37.90	61.75	17.68	17.77
D	21+47.90	61.75	17.86	17.95
E	21+57.90	61.75	18.01	18.08
F	21+67.90	61.75	18.12	18.16
CL BRG. PIER 1	21+82.70	61.75	18.25	18.25
G	21+92.70	61.75	18.29	18.28
H	22+02.70	61.75	18.31	18.29
I	22+12.70	61.75	18.30	18.28
CL BRG. PIER 2	22+24.20	61.75	18.25	18.25
J	22+34.20	61.75	18.17	18.20
K	22+44.20	61.75	18.07	18.12
L	22+54.20	61.75	17.93	18.00
M	22+64.20	61.75	17.77	17.85
N	22+74.20	61.75	17.58	17.65
O	22+84.20	61.75	17.36	17.41
CL BRG. E. ABUT	22+99.00	61.75	16.98	16.98
CL EXP JT	22+99.95	61.75	16.95	16.95
BK E. ABUT	23+01.01	61.75	16.92	16.92

Note: All Offsets with respect to  $\phi$  Bridge & P.G.L.



To determine "t": After all structural steel has been erected, elevations of the top flanges of the beams shall be taken at intervals shown on S-107. These elevations subtracted from the "Theoretical Grade Elevations Adjusted for Dead Load Deflection" shown on sheets S-108 through S-110.A minus slab thickness, equals the fillet heights "t" above top flange of beams.

**FILLET HEIGHTS**



**SENSITIVE SECURITY INFORMATION**

WARNING: THIS RECORD CONTAINS SENSITIVE SECURITY INFORMATION THAT IS CONTROLLED UNDER 49 CFR PARTS 15 AND 1520. NO PART OF THIS RECORD MAY BE DISCLOSED TO PERSONS WITHOUT A "NEED TO KNOW" AS DEFINED IN 49 CFR PARTS 15 AND 1520, EXCEPT WITH THE WRITTEN PERMISSION OF THE ADMINISTRATOR OF THE TRANSPORTATION SECURITY ADMINISTRATION OR THE SECRETARY OF TRANSPORTATION. UNAUTHORIZED RELEASE MAY RESULT IN CIVIL PENALTY OR OTHER ACTION. FOR U.S. GOVERNMENT AGENCIES, PUBLIC DISCLOSURE IS GOVERNED BY 5 U.S.C. 552 AND 49 CFR PARTS 15 AND 1520.

DRAWING SCALE IS NOT GUARANTEED. CTA ASSUMES NO RISK OF LIABILITY FOR ERRORS CAUSED, DIRECTLY OR INDIRECTLY, BY SCALING OF THIS DRAWING.

**95th Terminal Improvement**  
15 West 95th Street (South Terminal)  
Chicago, IL 60628

IN CHARGE	S. KADERBEK
APPROVED BY	D.ZROKA
CHECKED BY	P.MAID
DESIGNED BY	L.SOMMER
DRAWN BY	T.SLEDGE
PROJECT NO.	2012-0021
FILE NAME	File Name

2/9/17	ISSUED FOR CONSTRUCTION	
MARK	DATE	DESCRIPTION

LOCATION IDENTIFIER: **DR095**

95TH ST. BRIDGE  
TOP OF SLAB ELEVATIONS  
(SHEET 3 OF 6)  
SN 016-0437

**95TH-S-108**

vscarp

BEAM IB

Table with 5 columns: Location, Station, Offset, Theoretical Grade Elevations, Theoretical Grade Elevations Adjusted For Dead Load Deflection. Rows include BK W. ABUT, CL EXP JT, CL BRG. W. ABUT, A through O, CL BRG. PIER 1, CL BRG. PIER 2, CL BRG. E. ABUT, CL EXP JT, BK E. ABUT.

BEAM IC

Table with 5 columns: Location, Station, Offset, Theoretical Grade Elevations, Theoretical Grade Elevations Adjusted For Dead Load Deflection. Rows include BK W. ABUT, CL EXP JT, CL BRG. W. ABUT, A through O, CL BRG. PIER 1, CL BRG. PIER 2, CL BRG. E. ABUT, CL EXP JT, BK E. ABUT.

BEAM ID

Table with 5 columns: Location, Station, Offset, Theoretical Grade Elevations, Theoretical Grade Elevations Adjusted For Dead Load Deflection. Rows include BK W. ABUT, CL EXP JT, CL BRG. W. ABUT, A through O, CL BRG. PIER 1, CL BRG. PIER 2, CL BRG. E. ABUT, CL EXP JT, BK E. ABUT.

BEAM IE

Table with 5 columns: Location, Station, Offset, Theoretical Grade Elevations, Theoretical Grade Elevations Adjusted For Dead Load Deflection. Rows include BK W. ABUT, CL EXP JT, CL BRG. W. ABUT, A through O, CL BRG. PIER 1, CL BRG. PIER 2, CL BRG. E. ABUT, CL EXP JT, BK E. ABUT.

BEAM IF

Table with 5 columns: Location, Station, Offset, Theoretical Grade Elevations, Theoretical Grade Elevations Adjusted For Dead Load Deflection. Rows include BK W. ABUT, CL EXP JT, CL BRG. W. ABUT, END.

BEAM IG

Table with 5 columns: Location, Station, Offset, Theoretical Grade Elevations, Theoretical Grade Elevations Adjusted For Dead Load Deflection. Rows include BK W. ABUT, CL EXP JT, CL BRG. W. ABUT, END.

BEAM IH

Table with 5 columns: Location, Station, Offset, Theoretical Grade Elevations, Theoretical Grade Elevations Adjusted For Dead Load Deflection. Rows include BK W. ABUT, CL EXP JT, CL BRG. W. ABUT, R, END.

BEAM II

Table with 5 columns: Location, Station, Offset, Theoretical Grade Elevations, Theoretical Grade Elevations Adjusted For Dead Load Deflection. Rows include START, CL BRG. E. ABUT, CL EXP JT, BK E. ABUT.

BEAM IJ

Table with 5 columns: Location, Station, Offset, Theoretical Grade Elevations, Theoretical Grade Elevations Adjusted For Dead Load Deflection. Rows include START, CL BRG. E. ABUT, CL EXP JT, BK E. ABUT.

BEAM IK

Table with 5 columns: Location, Station, Offset, Theoretical Grade Elevations, Theoretical Grade Elevations Adjusted For Dead Load Deflection. Rows include START, S, T, CL BRG. E. ABUT, CL EXP JT, BK E. ABUT.

Note: All Offsets with respect to C Bridge & P.G.L.



SENSITIVE SECURITY INFORMATION
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95th Terminal Improvement
15 West 95th Street (South Terminal)
Chicago, IL 60628

Table with 2 columns: Field Name, Value. Rows include IN CHARGE (S. KADERBEK), APPROVED BY (D.ZROKA), CHECKED BY (P.MAID), DESIGNED BY (L.SOMMER), DRAWN BY (T.SLEDGE), PROJECT NO. (2012-0021), FILE NAME (File Name).

Table with 3 columns: MARK, DATE, DESCRIPTION. Row includes 2/9/17 ISSUED FOR CONSTRUCTION.

LOCATION IDENTIFIER: DR095
95TH ST. BRIDGE
TOP OF SLAB ELEVATIONS
(SHEET 4 OF 6)
SN 016-0437

95TH-S-109

NORTH CONSTRUCTION JOINT

Table with 5 columns: Location, Station, Offset, Theoretical Grade Elevations, Theoretical Grade Elevations Adjusted For Dead Load Deflection. Rows include BK W. ABUT, CL EXP JT, CL BRG. W. ABUT, A through O, and CL BRG. E. ABUT.

BEAM 16

Table with 5 columns: Location, Station, Offset, Theoretical Grade Elevations, Theoretical Grade Elevations Adjusted For Dead Load Deflection. Rows include BK W. ABUT, CL EXP JT, CL BRG. W. ABUT, A through O, and CL BRG. E. ABUT.

BEAM 16A

Table with 5 columns: Location, Station, Offset, Theoretical Grade Elevations, Theoretical Grade Elevations Adjusted For Dead Load Deflection. Rows include BK W. ABUT, CL EXP JT, CL BRG. W. ABUT, A through O, and CL BRG. E. ABUT.

BEAM 16B

Table with 5 columns: Location, Station, Offset, Theoretical Grade Elevations, Theoretical Grade Elevations Adjusted For Dead Load Deflection. Rows include BK W. ABUT, CL EXP JT, CL BRG. W. ABUT, A through O, and CL BRG. E. ABUT.

BEAM 16C

Table with 5 columns: Location, Station, Offset, Theoretical Grade Elevations, Theoretical Grade Elevations Adjusted For Dead Load Deflection. Rows include BK W. ABUT, CL EXP JT, CL BRG. W. ABUT, A through O, and CL BRG. E. ABUT.

BEAM 16D

Table with 5 columns: Location, Station, Offset, Theoretical Grade Elevations, Theoretical Grade Elevations Adjusted For Dead Load Deflection. Rows include BK W. ABUT, CL EXP JT, CL BRG. W. ABUT, and END.

BEAM 16E

Table with 5 columns: Location, Station, Offset, Theoretical Grade Elevations, Theoretical Grade Elevations Adjusted For Dead Load Deflection. Rows include BK W. ABUT, CL EXP JT, CL BRG. W. ABUT, and END.

BEAM 16F

Table with 5 columns: Location, Station, Offset, Theoretical Grade Elevations, Theoretical Grade Elevations Adjusted For Dead Load Deflection. Rows include BK W. ABUT, CL EXP JT, CL BRG. W. ABUT, P, Q, and END.

Note: All Offsets with respect to @ Bridge & P.G.L.



SENSITIVE SECURITY INFORMATION

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95th Terminal Improvement
15 West 95th Street (South Terminal)
Chicago, IL 60628

Table with 2 columns: Role and Name. Includes IN CHARGE (S. KADERBEK), APPROVED BY (D.ZROKA), CHECKED BY (P.MAID), DESIGNED BY (L.SOMMER), DRAWN BY (T.SLEDGE), PROJECT NO. (2012-0021), and FILE NAME.

Table with 3 columns: MARK, DATE, DESCRIPTION. Includes 2/9/17 ISSUED FOR CONSTRUCTION.

LOCATION IDENTIFIER: DR095

95TH ST. BRIDGE
TOP OF SLAB ELEVATIONS
(SHEET 5 OF 6)
SN 016-0437

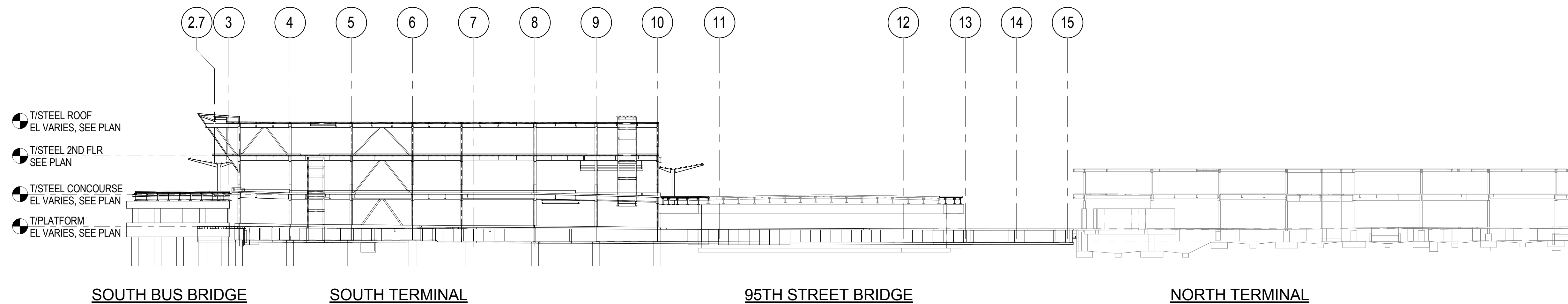
95TH-S-110

vscarpa

FILE NAME = 070\_DR095\_S-110.dgn
PLOT DATE = 8/31/2016

07/10/2016 2:35:24 PM

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**1** BUILDING LONGITUDINAL SECTION  
S-1101 SCALE: 1" = 30'-0"

NOTE:  
THIS DRAWING IS ISSUED FOR GENERAL INFORMATION ONLY. THE PURPOSE OF THIS DRAWING IS TO INDICATE THE STRUCTURAL FRAMING FOR THE PROJECT AND SHALL NOT BE USED FOR CONSTRUCTION. SEE THE REFERENCED ENLARGED PLANS FOR AREAS WHERE LIMITS OF WORK ARE SHOWN.



**SENSITIVE SECURITY INFORMATION**  
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**95th Terminal Improvement**  
15 West 95th Street (South Terminal)  
Chicago, IL 60628

IN CHARGE	J. JAKALSKI
APPROVED BY	J. JAKALSKI
CHECKED BY	R.S. NAIR
DESIGNED BY	G. FORKAN
DRAWN BY	M. DONAHUE
PROJECT NO.	2012-0021.02
FILE NAME	DR095_S-Terminal

MARK	DATE	DESCRIPTION
	2/9/17	ISSUED FOR CONSTRUCTION

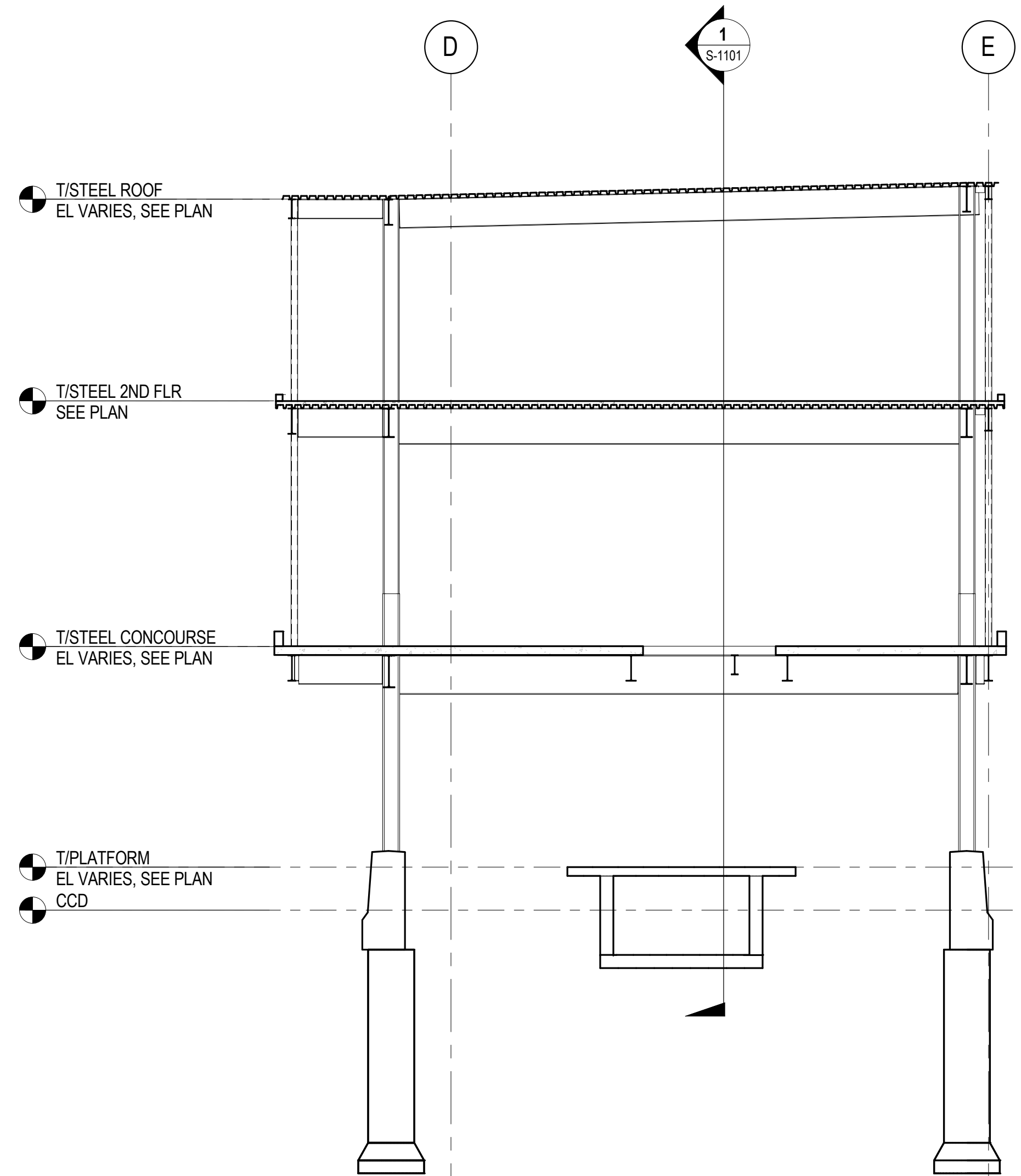
LOCATION IDENTIFIER: DR095

**BUILDING TYPICAL SECTIONS  
LONGITUDINAL**

**S-1101**

07/10/2016 2:36:32 PM

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**1** SOUTH TERMINAL - TRANSVERSE SECTION  
 S-1102 SCALE: 1/8" = 1'-0"

NOTE:  
 THIS DRAWING IS ISSUED FOR GENERAL INFORMATION ONLY. THE PURPOSE OF THIS DRAWING IS TO INDICATE THE STRUCTURAL FRAMING FOR THE PROJECT AND SHALL NOT BE USED FOR CONSTRUCTION. SEE THE REFERENCED ENLARGED PLANS FOR AREAS WHERE LIMITS OF WORK ARE SHOWN.



**SENSITIVE SECURITY INFORMATION**  
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**95th Terminal Improvement**  
 15 West 95th Street (South Terminal)  
 Chicago, IL 60628

IN CHARGE	J. JAKALSKI
APPROVED BY	J. JAKALSKI
CHECKED BY	R.S. NAIR
DESIGNED BY	G. FORKAN
DRAWN BY	M. DONAHUE
PROJECT NO.	2012-0021.02
FILE NAME	DR095_S-Terminal

MARK	DATE	DESCRIPTION
	2/9/17	ISSUED FOR CONSTRUCTION

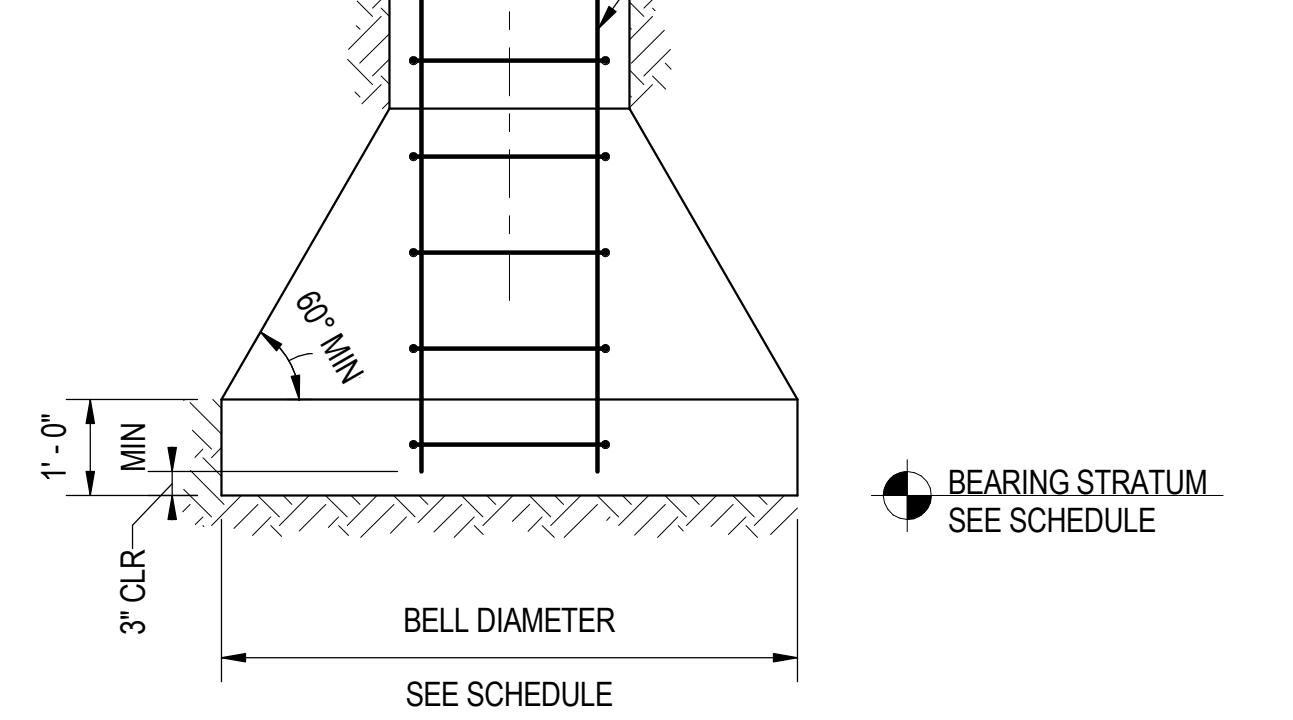
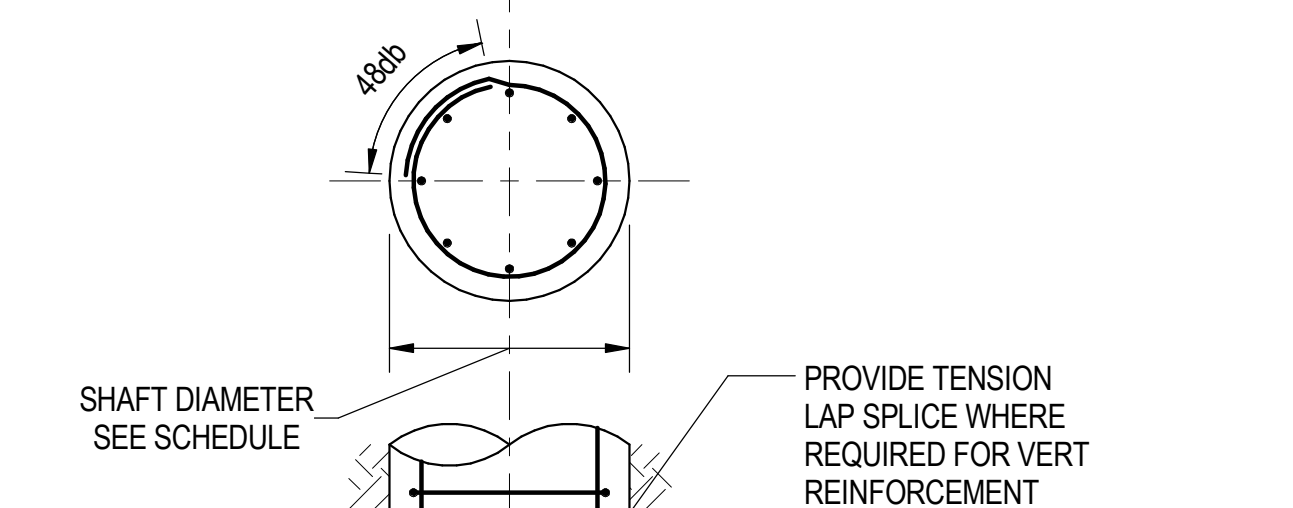
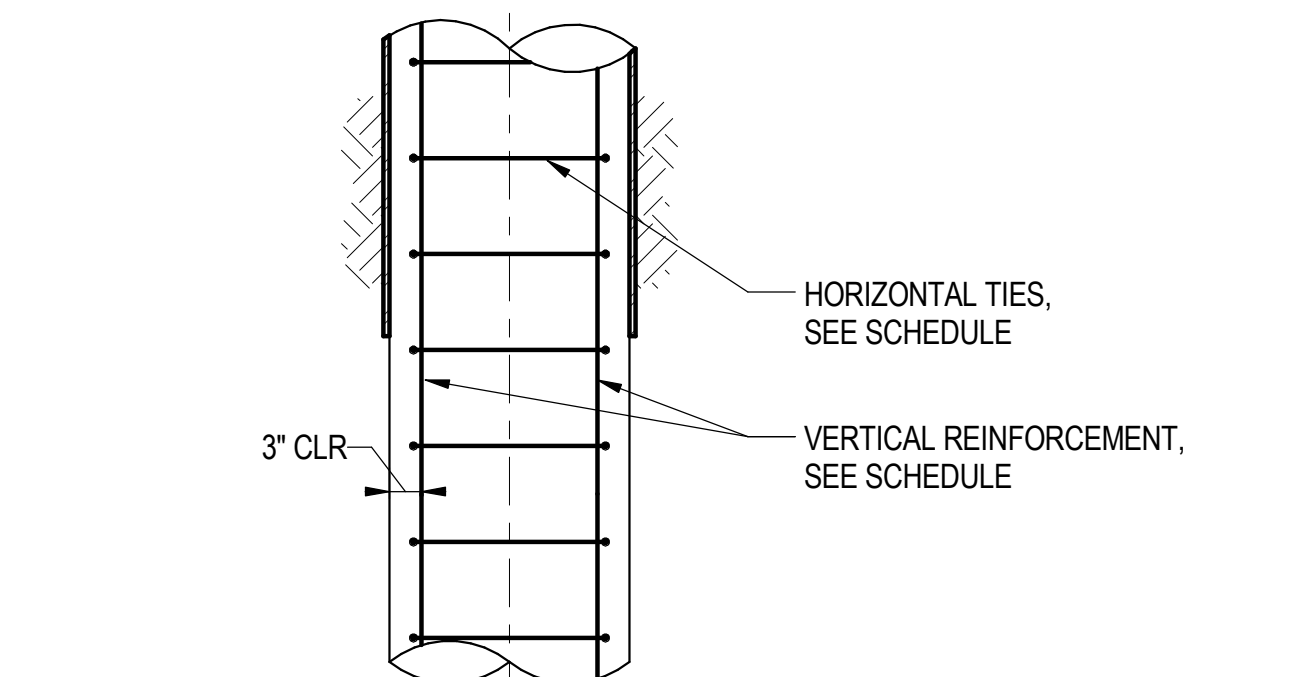
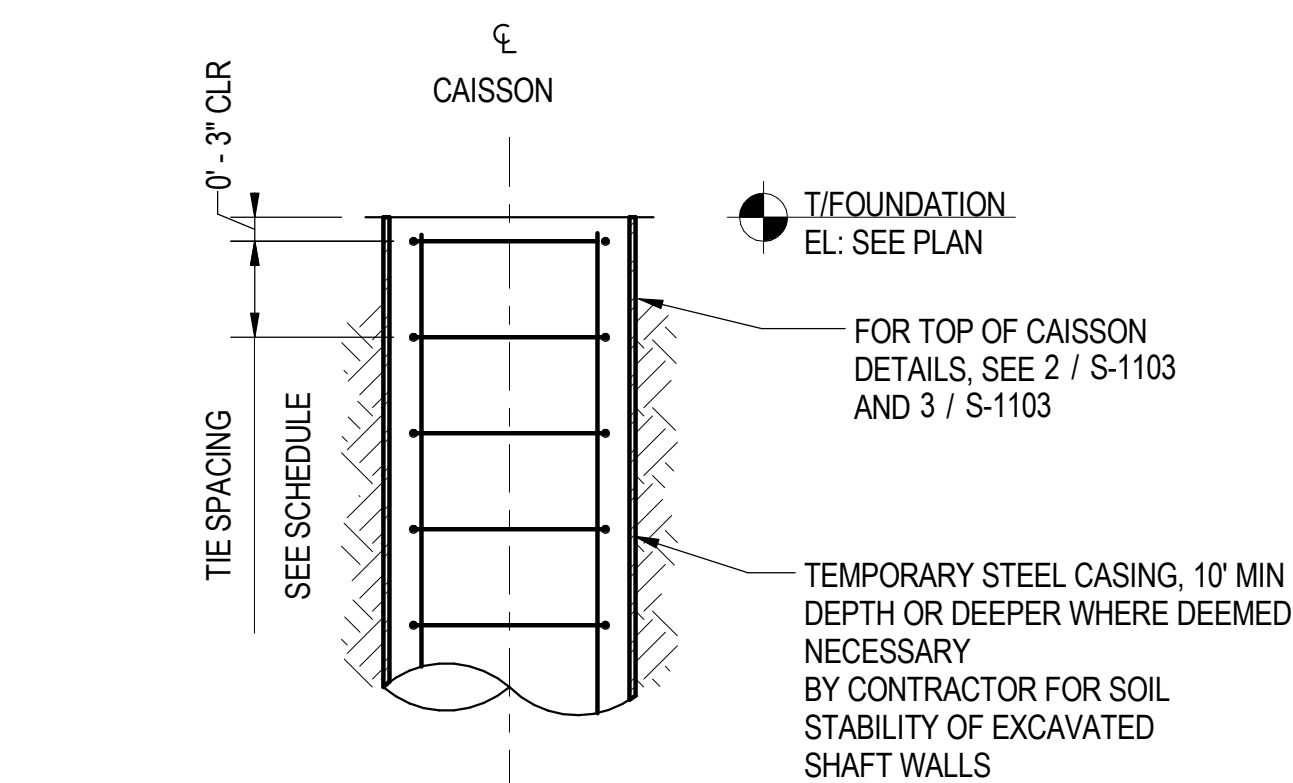
LOCATION IDENTIFIER: **DR095**

**BUILDING TYPICAL SECTIONS  
 TRANSVERSE**

**S-1102**

06/10/2016 4:07:43 PM

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CAISSON SCHEDULE						
MARK	SIZE		REINFORCEMENT		BEARING STRATUM (CCD)	REMARKS
	SHAFT DIAMETER	BELL DIAMETER	VERTICAL BARS	TIES		
F1	3' - 6"	5' - 0"	12-#10	#3@12"	-20' - 0"	BEARING CAPACITY OF 27 KSF
F2	2' - 6"	5' - 0"	10-#8	#3@12"	-29' - 0"	BEARING CAPACITY OF 27 KSF

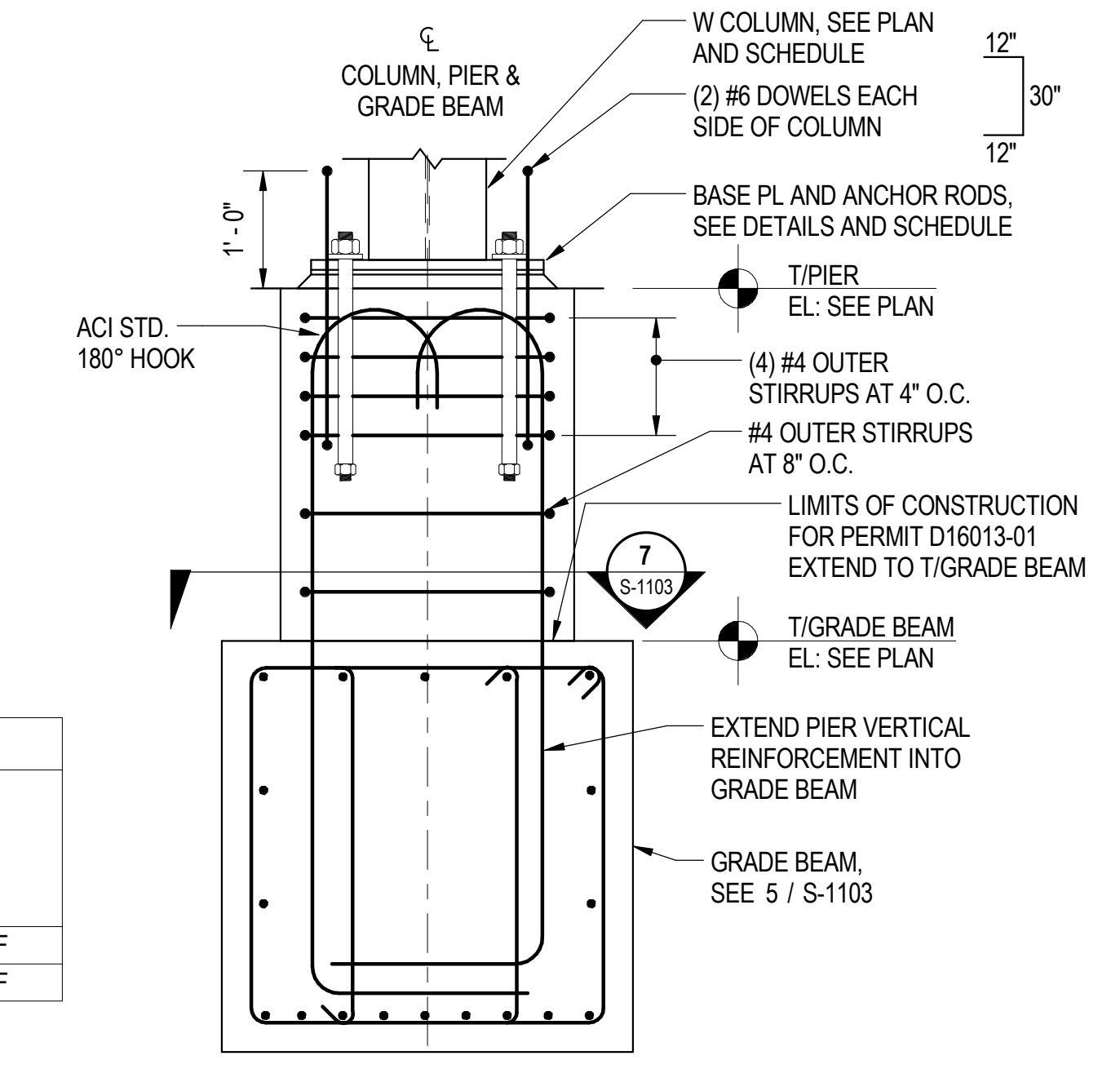
- NOTES:
- SEE GENERAL PROJECT NOTES FOR Fc.
  - BEARING CAPACITY = NET ALLOWABLE BEARING PRESSURE.
  - BEARING CAPACITIES PER WANG ENGINEERING INC. GEOTECHNICAL DESIGN MEMORANDUM DATED 03/03/2016.

2 TYPICAL TOP OF CAISSON AT COLUMN  
S-1103 SCALE: 3/4" = 1'-0"

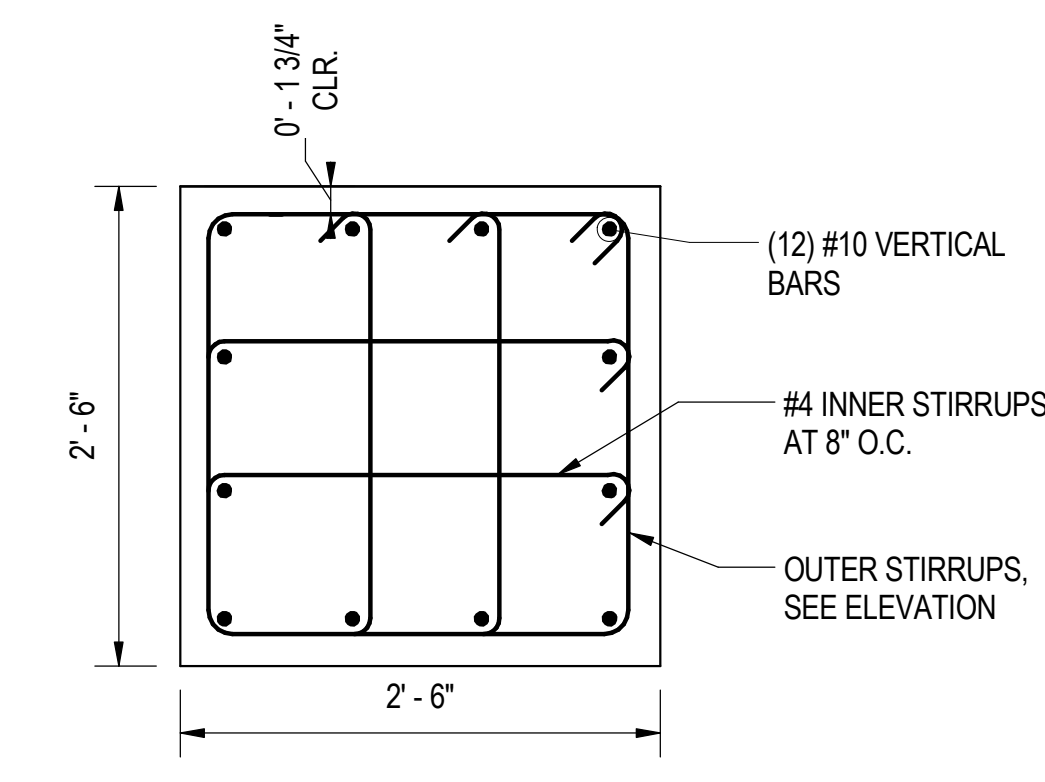
3 TYPICAL TOP OF CAISSON AT GRADE BEAM  
S-1103 SCALE: 3/4" = 1'-0"

4 TYPICAL GRADE BEAM ELEVATION  
S-1103 SCALE: 1/2" = 1'-0"

5 TYPICAL GRADE BEAM REINFORCEMENT  
S-1103 SCALE: 3/4" = 1'-0"



6 TYPICAL COLUMN PIER ELEVATION  
S-1103 SCALE: 3/4" = 1'-0"



7 TYPICAL PIER REINFORCEMENT  
S-1103 SCALE: 1" = 1'-0"



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**95th Terminal Improvement**  
15 West 95th Street (South Terminal)  
Chicago, IL 60628

IN CHARGE	J. JAKALSKI
APPROVED BY	J. JAKALSKI
CHECKED BY	R.S. NAIR
DESIGNED BY	G. FORKAN
DRAWN BY	D. LALLY
PROJECT NO.	2012-0021.02
FILE NAME	DR095_S-StationFoundation

MARK	DATE	DESCRIPTION
	10/6/16	ISSUED FOR CONSTRUCTION

LOCATION IDENTIFIER: DR095

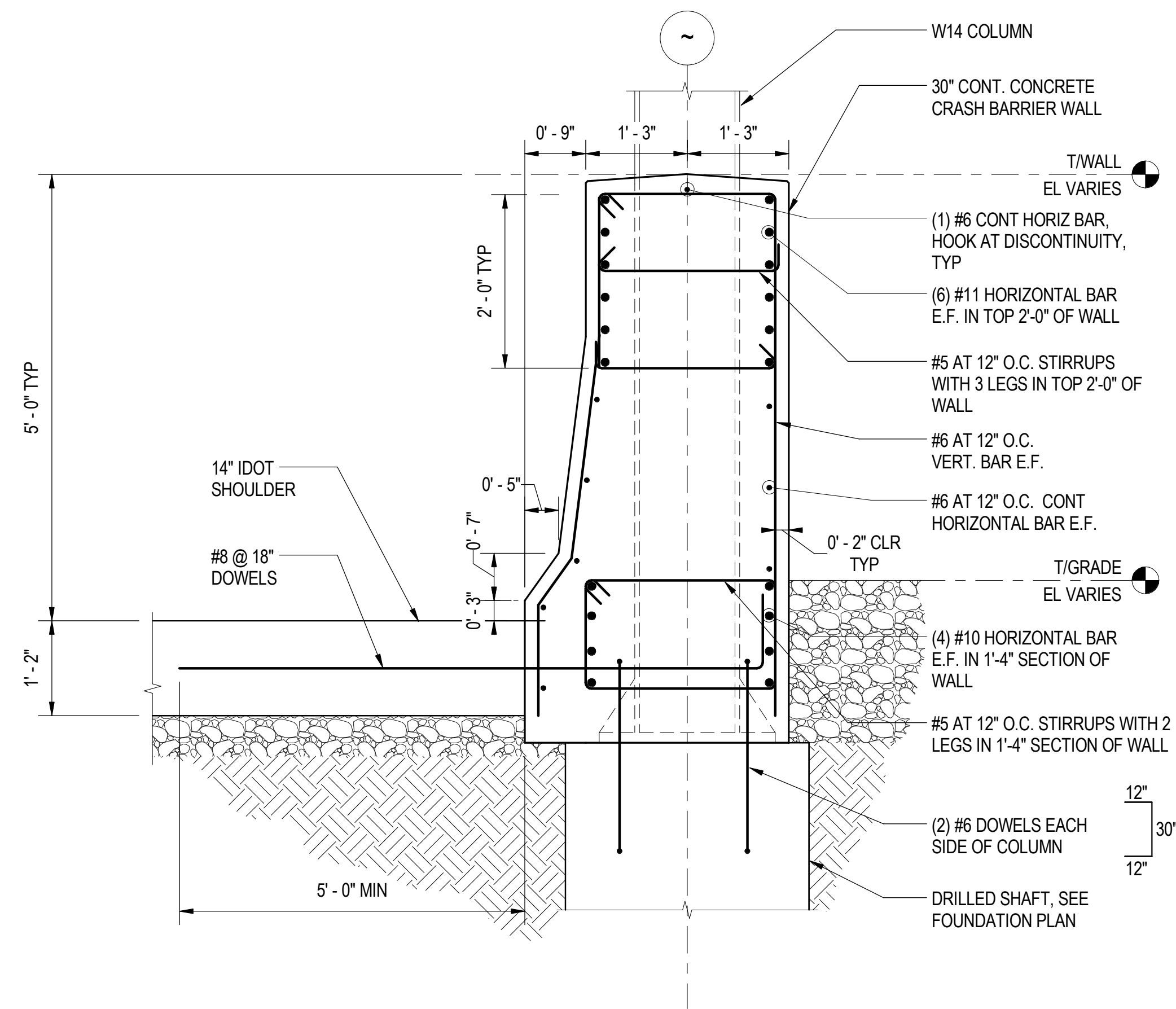
FOUNDATION DETAILS

S-1103

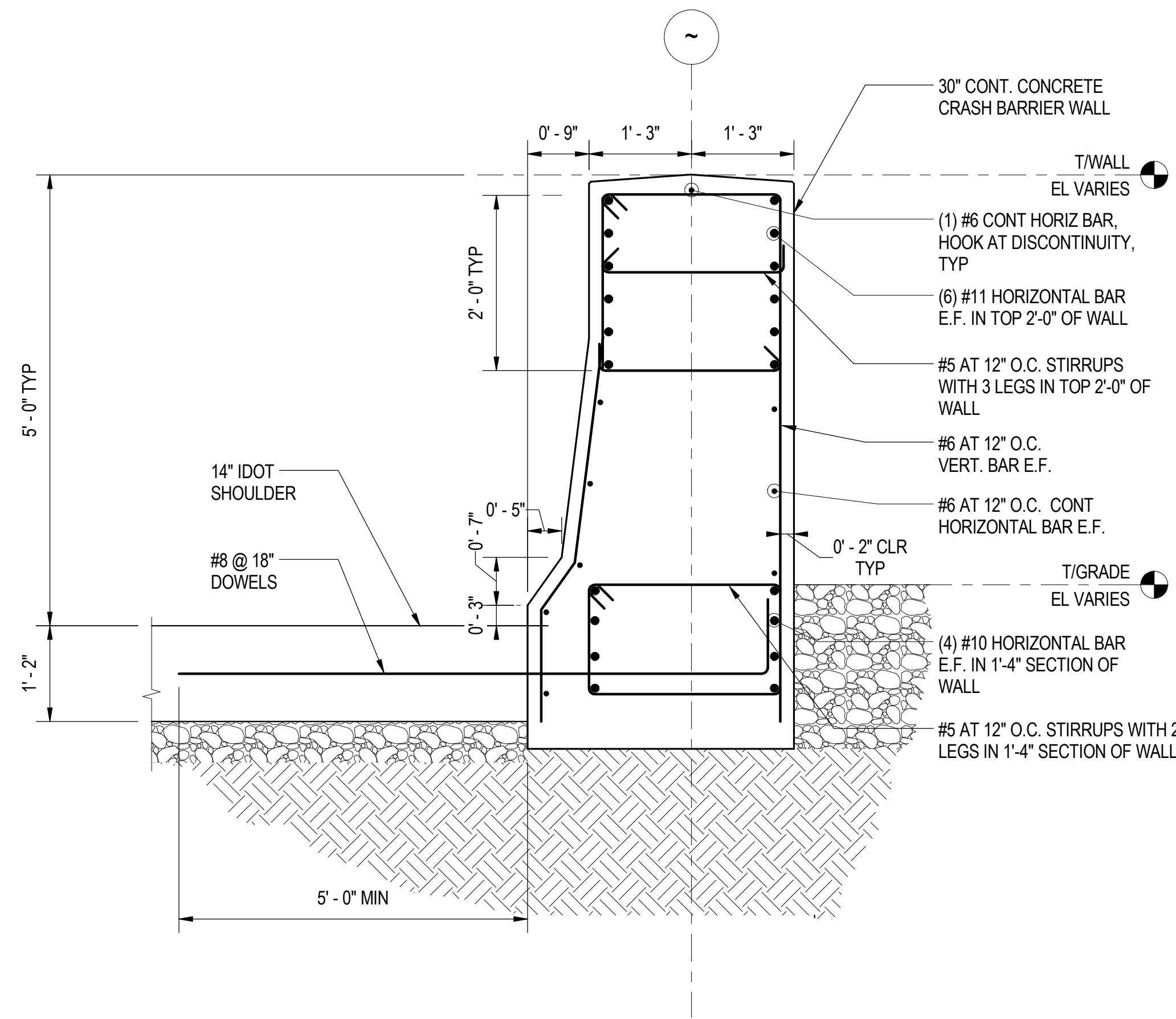


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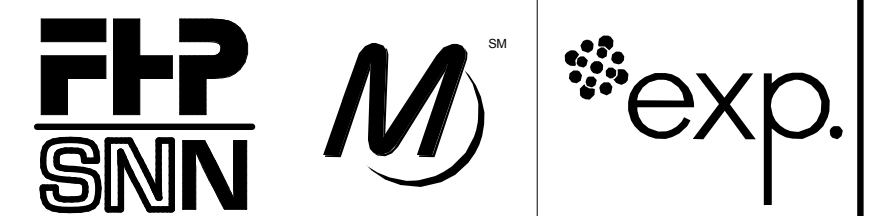
**1** CRASH BARRIER WALL AT FOUNDATION  
S-1104 SCALE: 3/4" = 1'-0"



**2** CRASH BARRIER BETWEEN FOUNDATIONS  
S-1104 SCALE: 3/4" = 1'-0"

**SHEET NOTES:**

- FOR GENERAL FOUNDATION NOTES, SEE S-000 SERIES.
- FOR TYPICAL DETAILS, SEE S-1100 SERIES.
- ALL REBAR SHALL BE EPOXY COATED.
- SEE TYPICAL DETAILS FOR MINIMUM BAR LAP DIMENSIONS, UNLESS WHERE INDICATED ON THE DRAWINGS. WHEN LAPPING DIFFERENT DIAMETER BARS, LARGER BAR LAP SHALL APPLY.



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**95th Terminal Improvement**  
15 West 95th Street (South Terminal)  
Chicago, IL 60628

IN CHARGE	J. JAKALSKI
APPROVED BY	J. JAKALSKI
CHECKED BY	R.S. NAIR
DESIGNED BY	G. FORKAN
DRAWN BY	M. DONAHUE
PROJECT NO.	2012-0021.02
FILE NAME	DR095_S-StationFoundation

MARK	DATE	DESCRIPTION
	2/9/17	ISSUED FOR CONSTRUCTION

LOCATION IDENTIFIER: **DR095**

**CRASH BARRIER WALL DETAILS**

**S-1104**

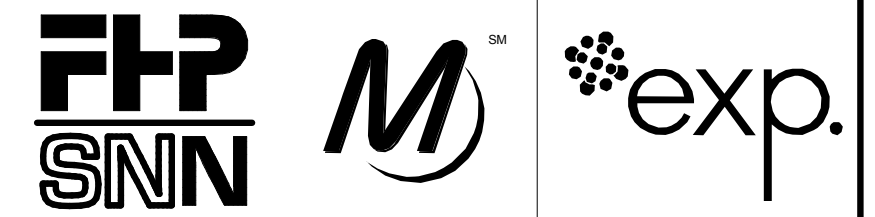
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COLUMN ELEVATION	D'-3.1 & E'-3.1	D'-4 & E'-4	D'-5 & E'-5	D'-6 & E'-6	D'-6.8 & E'-6.8	D'-8 & E'-8	D'-9 & E'-9	D'-10 & E'-10	C'-5 & C'-6	E-5 & E-6
ROOF T.O.S. EL VARIES SEE PLAN										
SECOND FLOOR T.O.S. SEE PLAN	W14x176	W14x176	W14x176	W14x176	W14x176	W14x176	W14x176	W14x176	W14x257	HSS6x6x3/8
CONCOURSE T.O.S. EL VARIES SEE PLAN	W14x193	W14x193	W14x193	W14x193	W14x193	W14x193	W14x193	W14x193	W14x257	INDICATES SPLICE WITH COLUMN BELOW, 4'-0" ABOVE T.O.C. ELEVATION, TYP U.N.O.
FOUNDATION CAP T.O.C. EL VARIES SEE PLAN										
SPLICE DETAIL	SP-1	SP-1	SP-2	SP-2	SP-2	SP-2	SP-2	SP-2	-	-
BASE PL MARK	BP-1	BP-1	BP-1	BP-2	BP-1	BP-2	BP-2	BP-3	-	-

**SHEET NOTES:**

- FOR GENERAL NOTES, SEE DWGS S-001 TO S-004.
- FOR TYPICAL DETAILS REFER TO S-1112 TO S-1120.
- ALL STEEL SHALL BE CONTINUOUS U.N.O. REFER TO TYPICAL DETAILS FOR GRAVITY COLUMN SPLICING REQUIREMENTS. ALL SPLICE LOCATIONS NOT SHOWN SHALL BE REVIEWED AND APPROVED BY THE DOR PRIOR TO PROCEEDING WITH FABRICATION.
- ALL STEEL AT CONCOURSE LEVEL AND BELOW SHALL BE GALVANIZED.
- REFER TO S-832 FOR SOUTH TERMINAL BRACED FRAME ELEVATIONS.
- REFER TO S-833 AND S-834 FOR SOUTH TERMINAL MOMENT FRAME ELEVATIONS.



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**95th Terminal Improvement**  
 15 West 95th Street (South Terminal)  
 Chicago, IL 60628

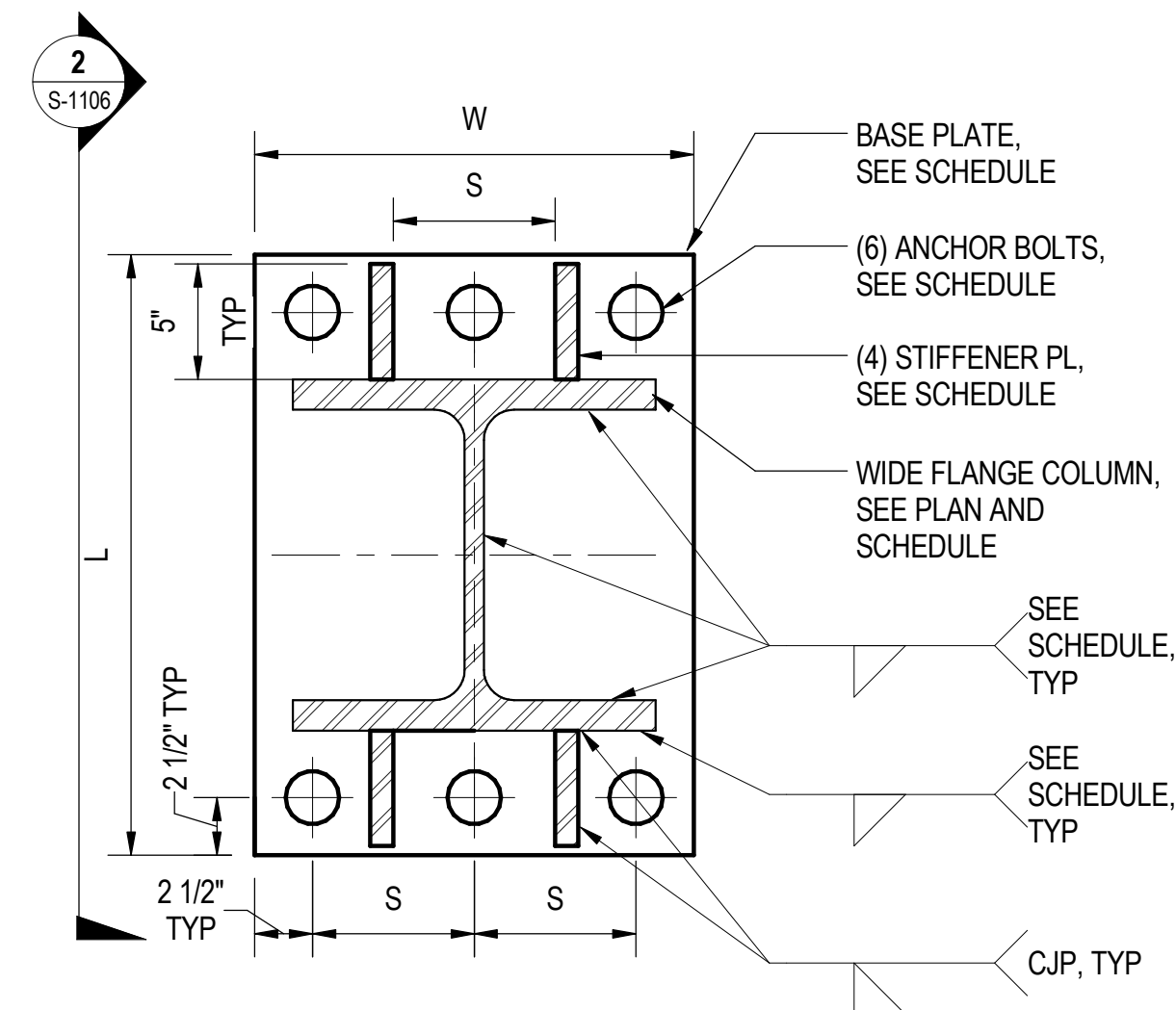
IN CHARGE	J. JAKALSKI
APPROVED BY	J. JAKALSKI
CHECKED BY	R.S. NAIR
DESIGNED BY	G. FORKAN
DRAWN BY	J. PEARSON
PROJECT NO.	2012-0021.02
FILE NAME	DR095_S-Terminal

MARK	DATE	DESCRIPTION
	2/9/17	ISSUED FOR CONSTRUCTION

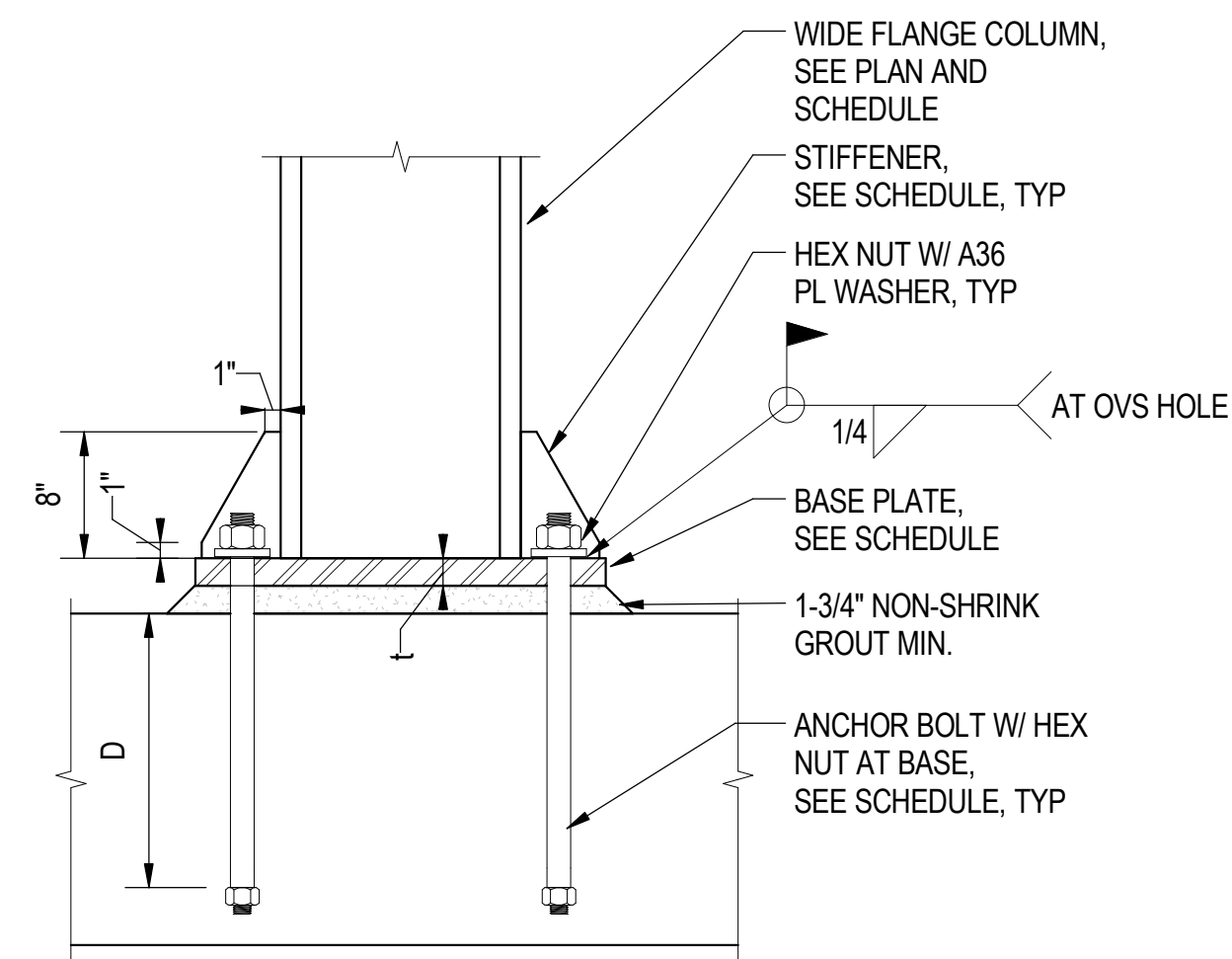
LOCATION IDENTIFIER: **DR095**

**STEEL COLUMN SCHEDULES**

**S-1105**



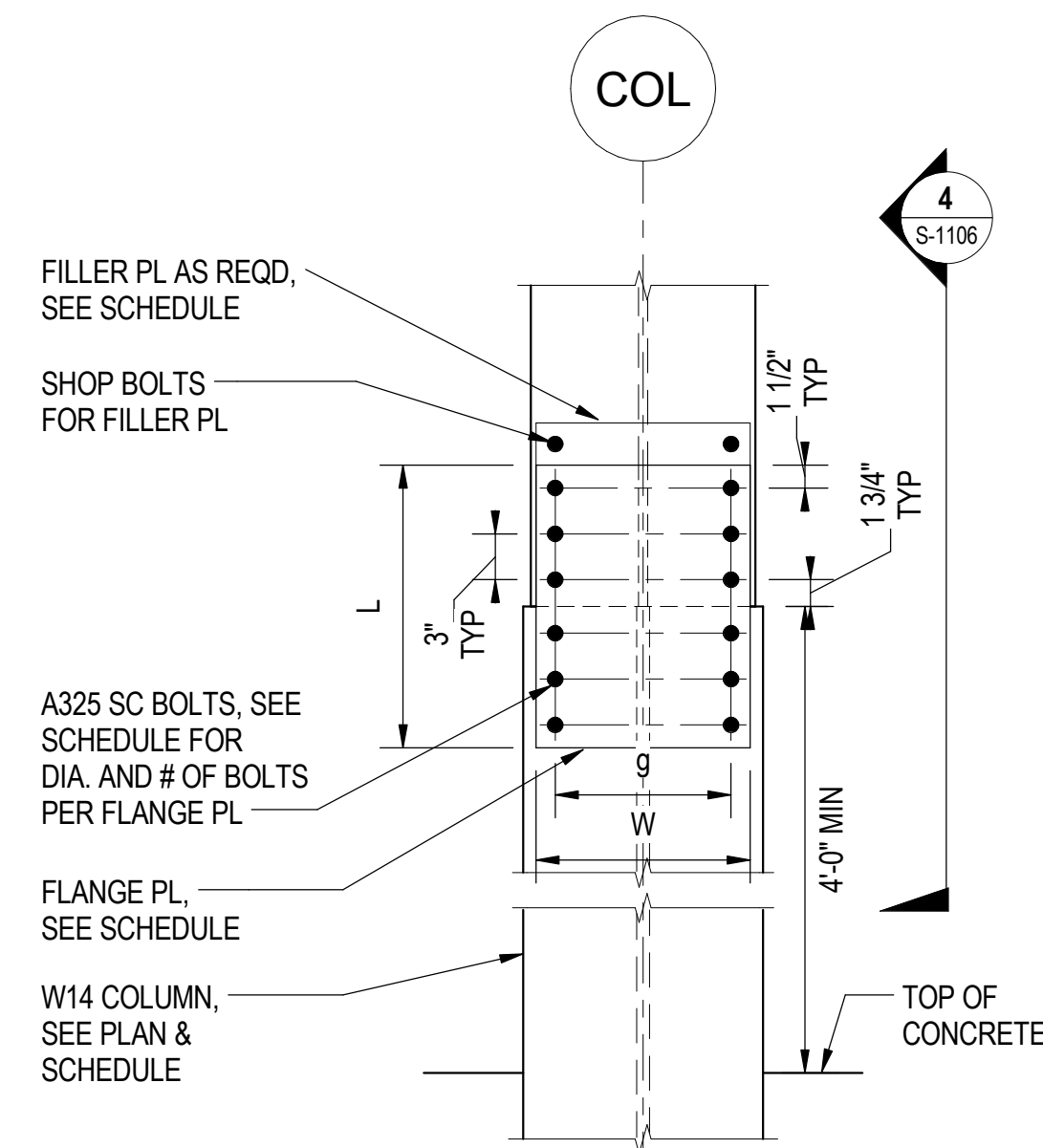
**1** COLUMN BASE PLATE PLAN  
S-1106 SCALE: 1 1/2" = 1'-0"



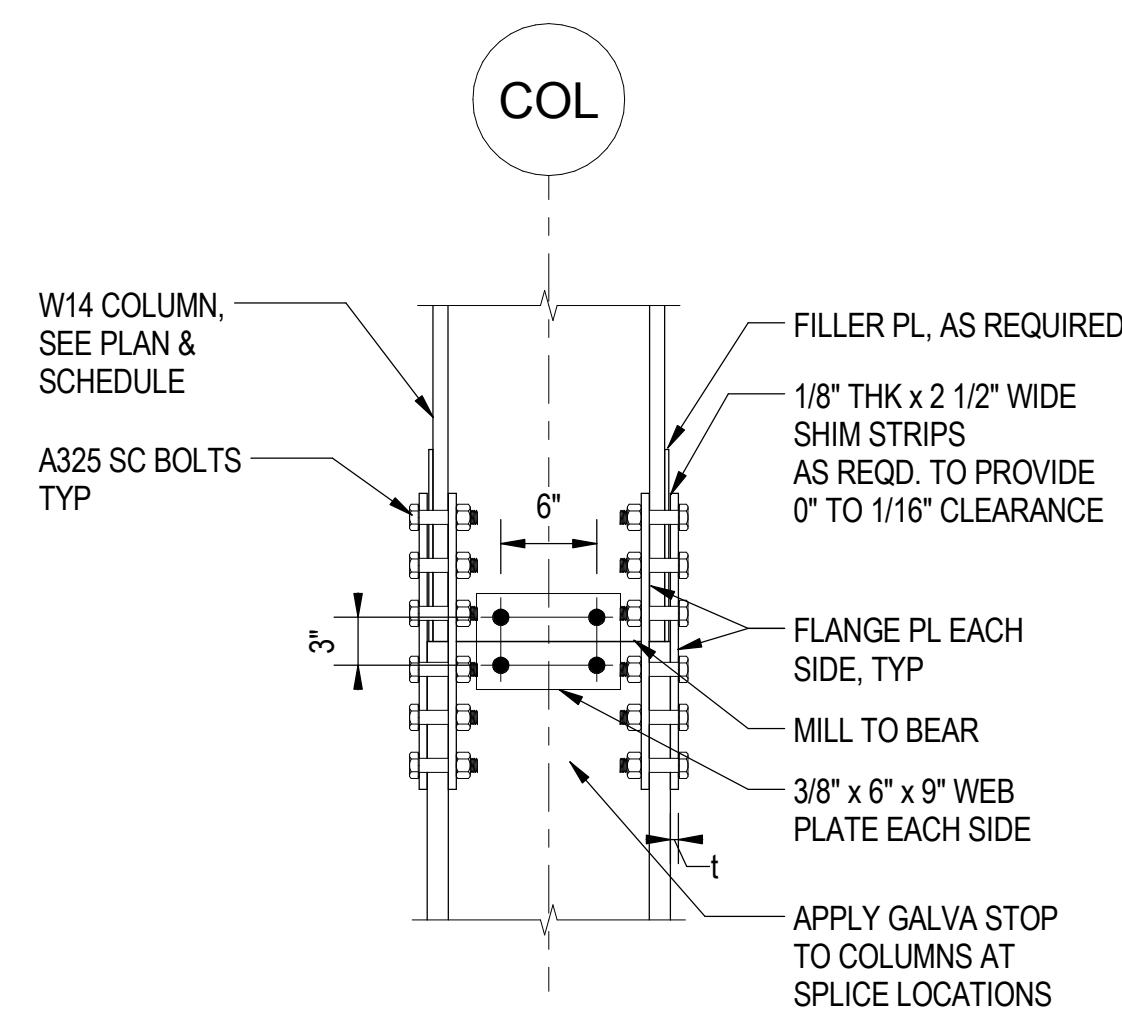
**2** COLUMN BASE PLATE SECTION  
S-1106 SCALE: 1" = 1'-0"

BASE PLATE SCHEDULE							
MARK	BASE PLATE			ANCHOR BOLTS		STIFFENER THICKNESS	COL. TO BASE PL FILLET WELD SIZE
	L	W	t	DIA.	D		
BP-1	26"	19"	1.75"	1.25"	18"	1"	5/16"
BP-2	26"	19"	1.75"	1.25"	18"	1.25"	3/8"
BP-3	26"	19"	1.75"	1.5"	18"	1.5"	1/2"

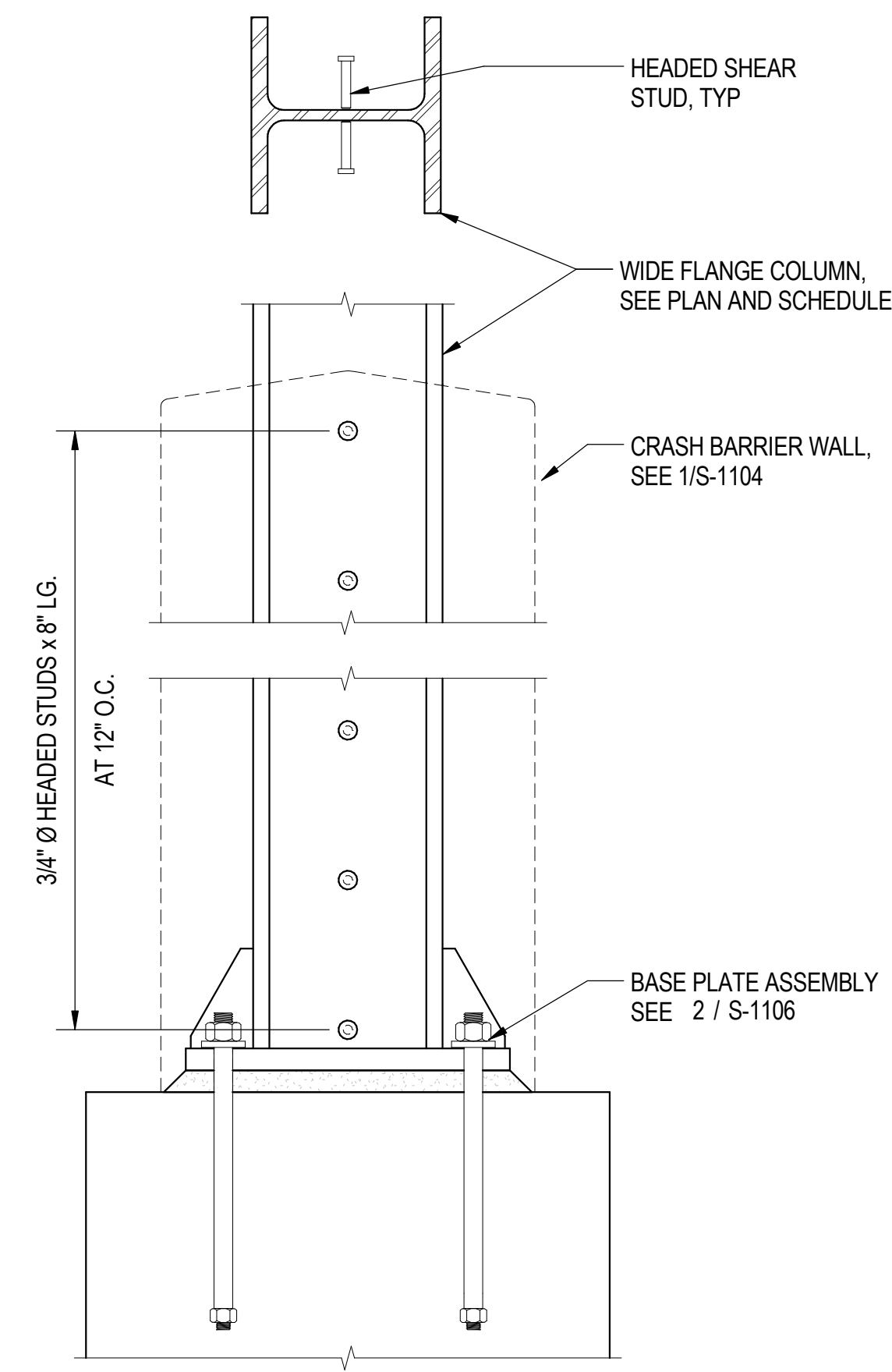
COLUMN SPLICE SCHEDULE						
MARK	FLANGE PLATE					ALL BOLTS DIA.
	L	W	t	g	# OF BOLTS / PL	
SP-1	30"	14"	3/4"	11.5"	20	1"
SP-2	42"	14"	3/4"	11.5"	28	1"



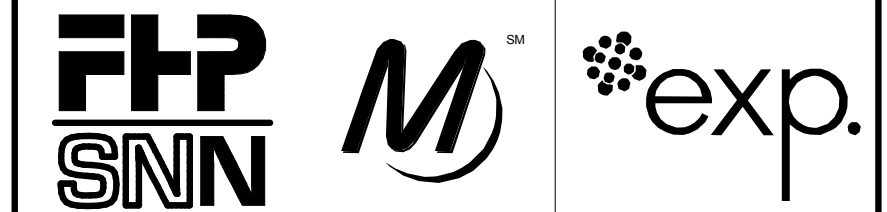
**3** COLUMN SPLICE DETAIL  
S-1106 SCALE: 1" = 1'-0"



**4** COLUMN SPLICE SECTION  
S-1106 SCALE: 1" = 1'-0"



**5** HEADED STUD AT COLUMN DETAIL  
S-1106 SCALE: 1" = 1'-0"



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**95th Terminal Improvement**  
15 West 95th Street (South Terminal)  
Chicago, IL 60628

IN CHARGE	J. JAKALSKI
APPROVED BY	J. JAKALSKI
CHECKED BY	R.S. NAIR
DESIGNED BY	G. FORKAN
DRAWN BY	M. DONAHUE
PROJECT NO.	2012-0021.02
FILE NAME	DR095_S-Terminal

MARK	DATE	DESCRIPTION
	2/9/17	ISSUED FOR CONSTRUCTION

LOCATION IDENTIFIER: **DR095**

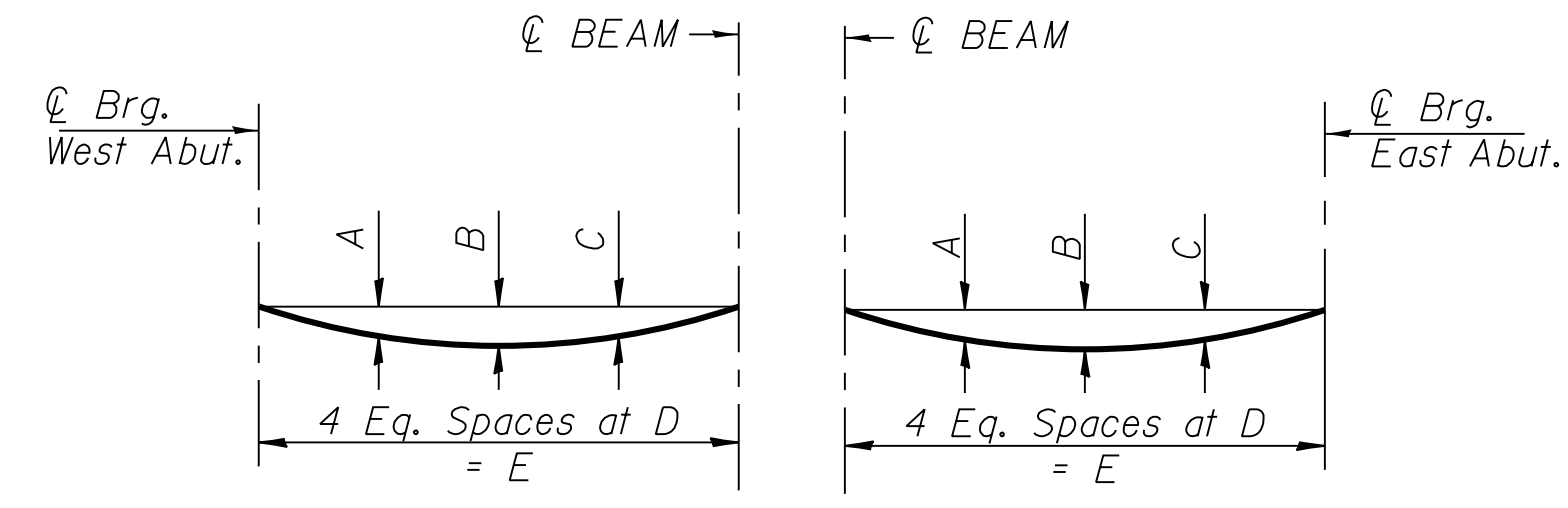
**COLUMN AND BASE PLATE DETAILS**

**S-1106**

vscarp

**BEAM 16G**

Location	Station	Offset	Theoretical Grade Elevations	Theoretical Grade Elevations Adjusted For Dead Load Deflection
START	22+94.03	-71.73	17.24	17.25
CL BRG. E. ABUT	22+99.00	-71.73	17.09	17.09
CL EXP JT	22+99.94	-71.73	17.06	17.06
BK E. ABUT	23+02.02	-71.73	17.00	17.00



**DEAD LOAD DEFLECTIONS**

Beam	A	B	C	D	E
1K	1/8"	1/4"	1/4"	7'-0 3/8"(-)	28'-1 3/8"
16F	1/8"	1/8"	1/8"	6'-3 3/4"(+)	25'-3 1/16"

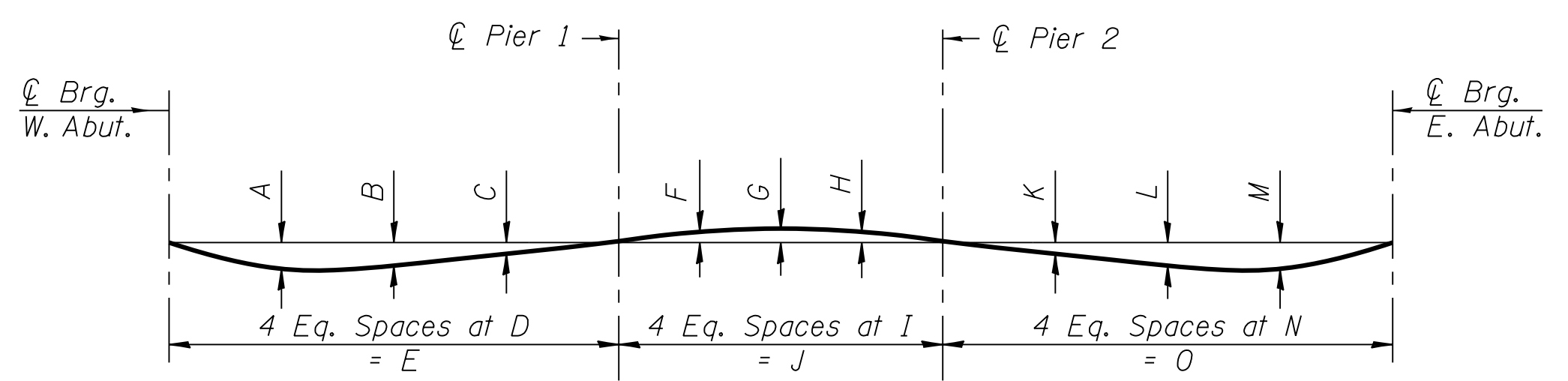
Note:  
Beams 1F, 1G, 1H, 1I, 1J, 16D, 16E, 16G & 16H have no measurable dead load deflection.

**BEAM 16H**

Location	Station	Offset	Theoretical Grade Elevations	Theoretical Grade Elevations Adjusted For Dead Load Deflection
START	22+89.06	-66.75	17.37	17.40
CL BRG. E. ABUT	22+99.00	-76.71	17.09	17.09
CL EXP JT	22+99.94	-77.65	17.06	17.06
BK E. ABUT	23+02.02	-79.73	17.00	17.00

**DEAD LOAD DEFLECTIONS**

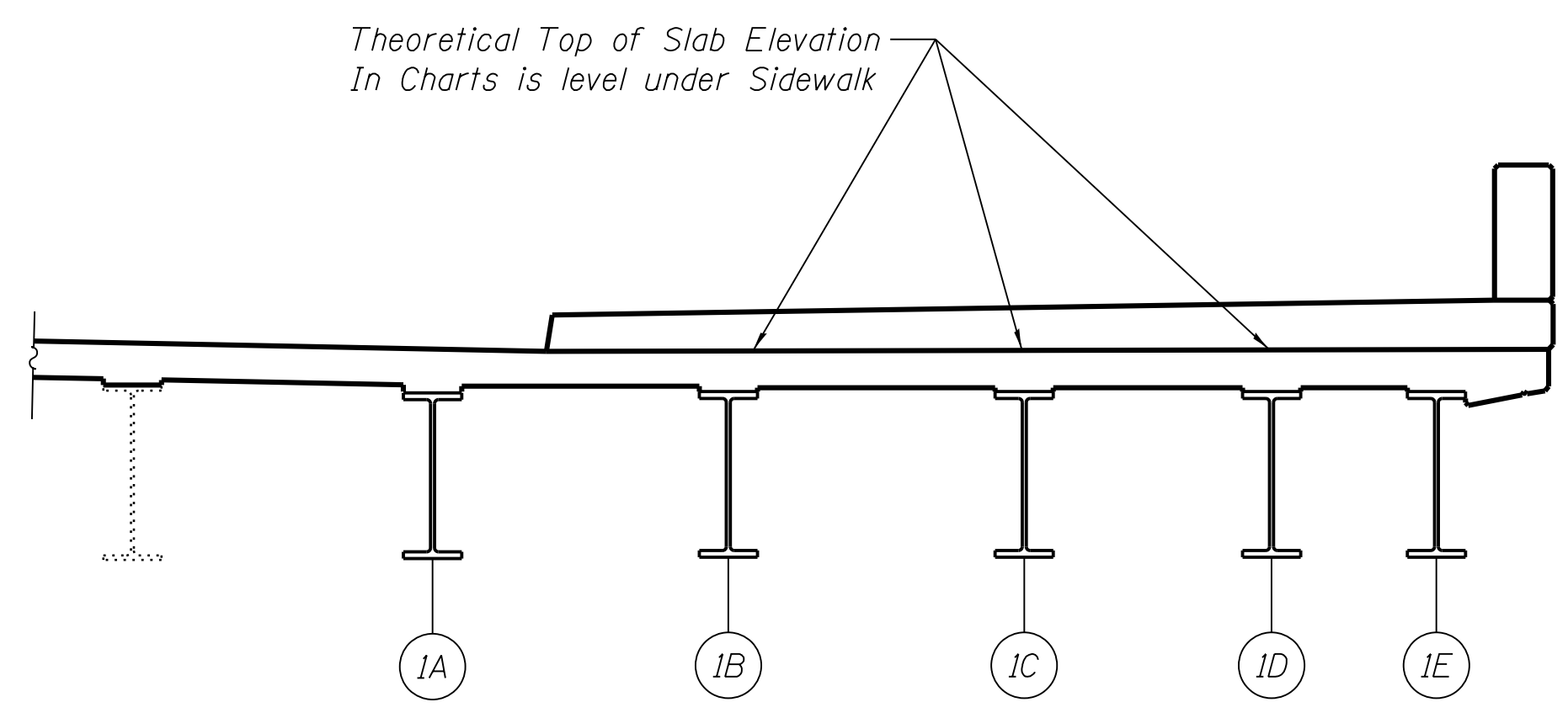
Beam	Span 1					Span 2					Span 3				
	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O
1A	7/8"	1 1/8"	5/8"	18'-10 3/8"(+)	75'-5 5/8"	1/8"	1/4"	1/8"	10'-4 1/2"	41'-6"	5/8"	1"	3/4"	18'-9 3/4"(+)	75'-3 1/16"
1B	7/8"	1 1/8"	5/8"	18'-10"(+)	75'-4 1/16"	1/8"	1/4"	1/8"	10'-4 1/2"	41'-6"	5/8"	1"	3/4"	18'-9 1/2"(-)	75'-1 15/16"
1C	1 1/2"	2"	1 1/4"	18'-9 1/2"	75'-2"	3/8"	1/2"	3/8"	10'-4 1/2"	41'-6"	1 1/4"	2"	1 1/2"	18'-9"(-)	74'-11 15/16"
1D	1 1/2"	2"	1 1/4"	18'-8 7/8"(+)	74'-11 9/16"	3/8"	1/2"	3/8"	10'-4 1/2"	41'-6"	1 1/4"	2"	1 1/2"	18'-8 3/8"(+)	74'-9 9/16"
1E	3/4"	7/8"	1/2"	18'-8 3/8"(+)	74'-9 5/8"	1/8"	1/4"	1/8"	10'-4 1/2"	41'-6"	5/8"	1"	3/4"	18'-8 3/8"(+)	74'-9 5/8"
16A	1 1/2"	2"	1 1/4"	18'-8 3/8"(+)	74'-9 5/8"	3/8"	1/2"	3/8"	10'-4 1/2"	41'-6"	1 1/4"	2"	1 1/2"	18'-8 3/8"(+)	74'-9 5/8"
16B	1 1/2"	2"	1 1/4"	18'-8 3/8"(+)	74'-9 5/8"	3/8"	1/2"	3/8"	10'-4 7/8"(+)	41'-7 5/8"	1 1/4"	2"	1 1/2"	18'-8"(+)	74'-8"
16C	3/4"	1"	5/8"	18'-8 3/8"(+)	74'-9 5/8"	1/8"	1/4"	1/8"	10'-5 1/8"(+)	41'-8 3/4"	1/2"	7/8"	5/8"	18'-7 3/4"(-)	74'-6 7/8"



**DEAD LOAD DEFLECTION DIAGRAM**

(Includes weight of concrete, excluding beams).

Note:  
The above deflections are not to be used in the field if the engineer is working from the grade elevations adjusted for dead load deflections as shown on sheets 95TH-S-108 thru 95TH-S-110A.



**THEORETICAL TOP OF SLAB ELEVATION FOR BEAMS UNDER SIDEWALK**

(Applies to beams 1B thru 1J & 16 thru 16H)

Note:  
All Offsets with respect to  $\bar{C}$  Bridge & P.G.L.

FILE NAME = 070\_DR095\_S-110A.dgn  
PLOT DATE = 8/31/2016



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**95th Terminal Improvement**  
15 West 95th Street (South Terminal)  
Chicago, IL 60628

IN CHARGE	S. KADERBEK
APPROVED BY	D.ZROKA
CHECKED BY	P.MAID
DESIGNED BY	L.SOMMER
DRAWN BY	T.SLEDGE
PROJECT NO.	2012-0021
FILE NAME	File Name

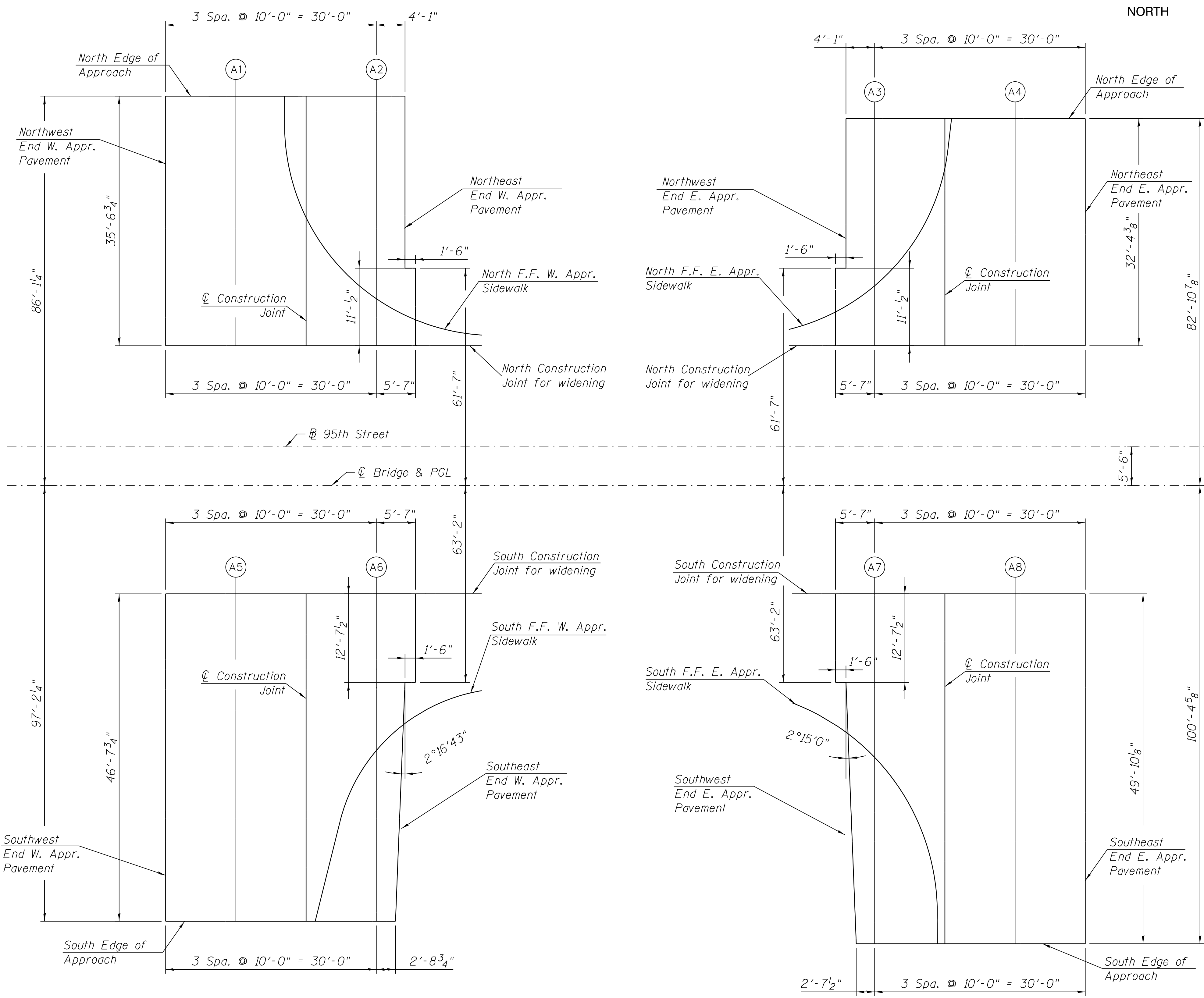
MARK	DATE	DESCRIPTION
	2/9/17	ISSUED FOR CONSTRUCTION

LOCATION IDENTIFIER: **DR095**  
95TH ST. BRIDGE  
TOP OF SLAB ELEVATIONS  
(SHEET 6 OF 6)  
SN 016-0437

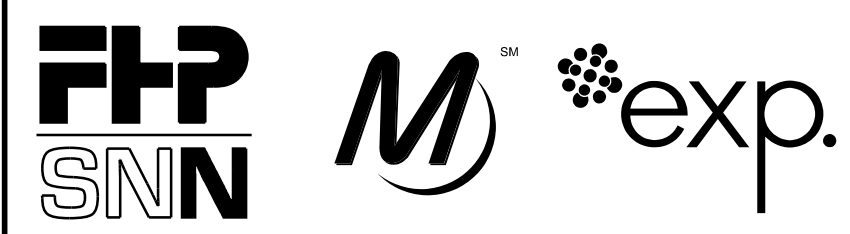
**95TH-S-110A**

vscarpa

FILE NAME = 070\_DR095\_S-111.dgn  
PLOT DATE = 8/31/2016



PLAN



**cta** CHICAGO TRANSIT AUTHORITY ENGINEERING

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**95th Terminal Improvement**  
 15 West 95th Street (South Terminal)  
 Chicago, IL 60628

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PROJECT NO.	2012-0021
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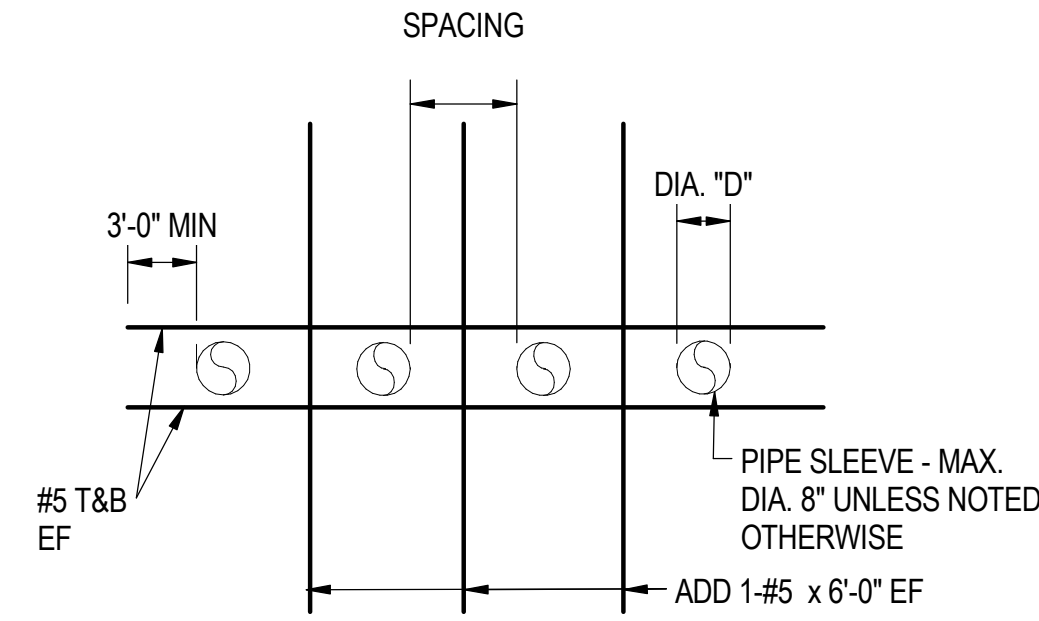
LOCATION IDENTIFIER: **DR095**

95TH ST. BRIDGE  
 TOP OF APPROACH SLAB ELEVATIONS  
 (SHEET 1 OF 2)  
 SN 016-0437

**95TH-S-111**

07/10/2016 3:07:47 PM

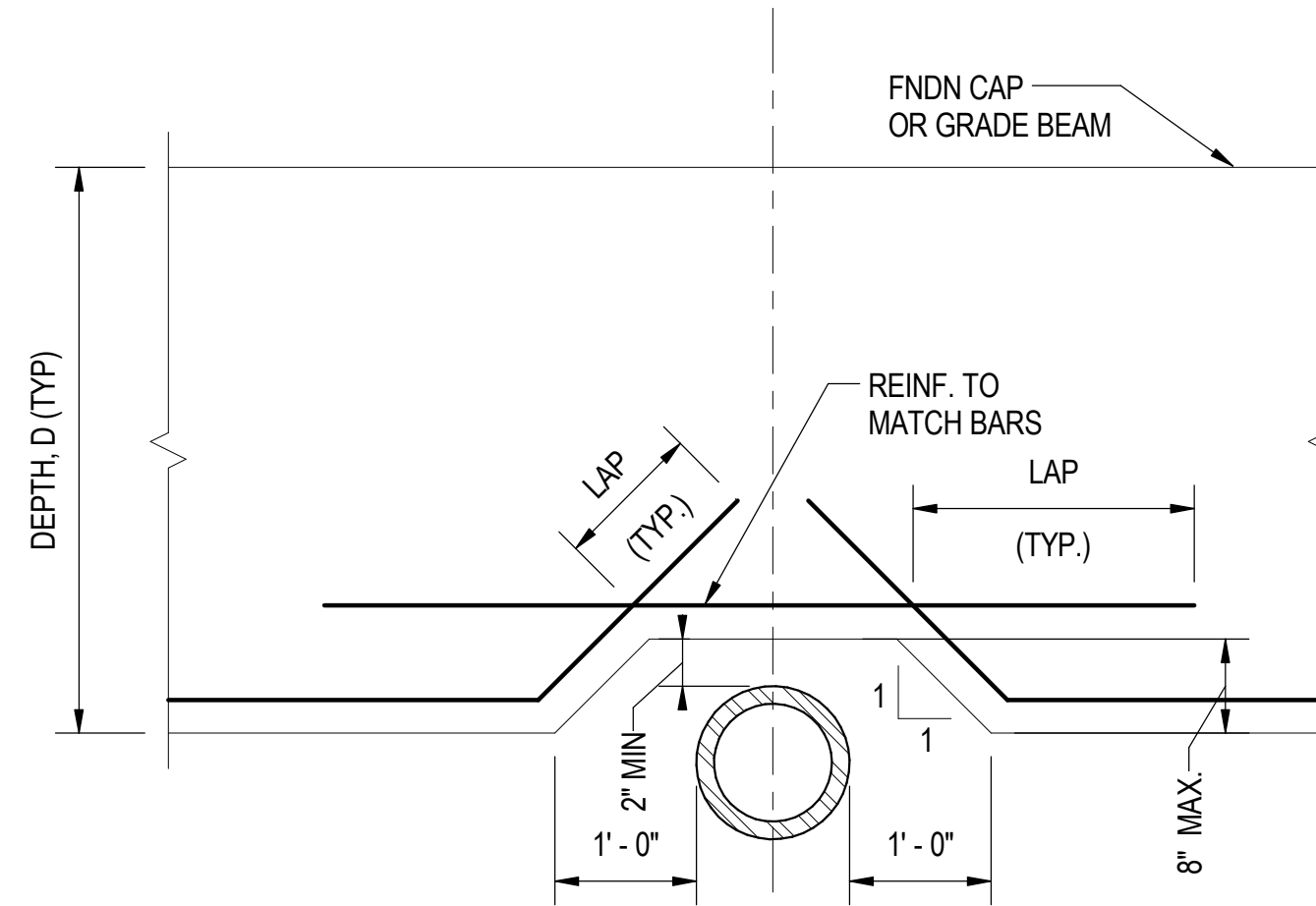
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1. DETAIL APPLIES TO LOCATION WHERE  $S > 4"$  OR  $S < 18"$ .
2. MIN. PIPE SPACING SHALL BE  $2 \times "D"$  WITH SPACING NO LESS THAN  $4"$  CLEAR.
3. FOR SPACING WHERE  $S < 4"$ , SLEEVES SHALL BE CONSIDERED AS A GROUP AND THE TYPICAL OPENING IN CONCRETE WALL OR SLAB DETAIL SHALL APPLY.
4. FOR SLEEVE OPENINGS LESS THAN TYPICAL WALL OR SLAB REINFORCING, NO ADDITIONAL REINFORCEMENT IS REQUIRED.

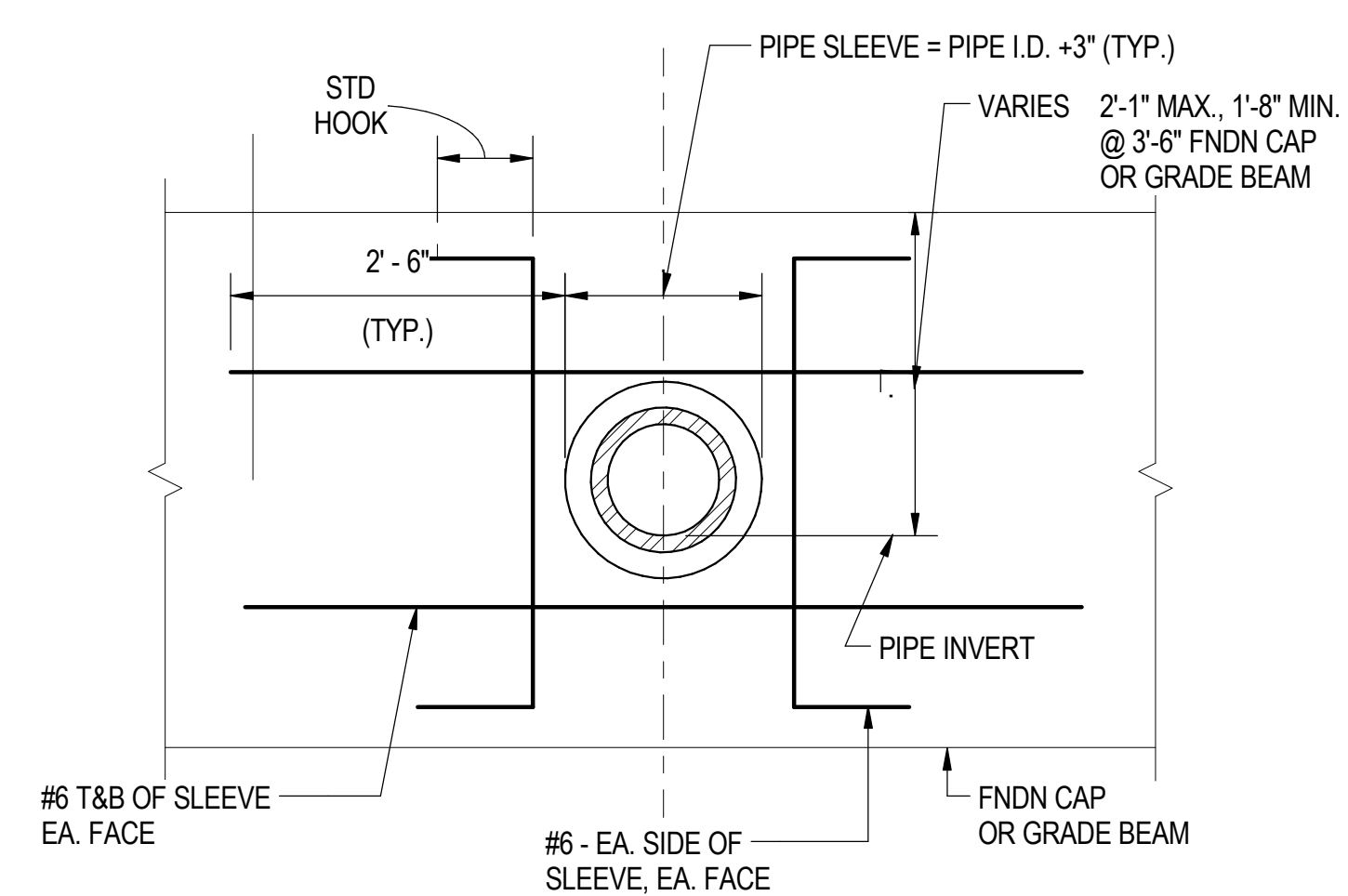
**1** REINFORCING AROUND SLEEVES

S-1112 SCALE:  $1" = 1'-0"$



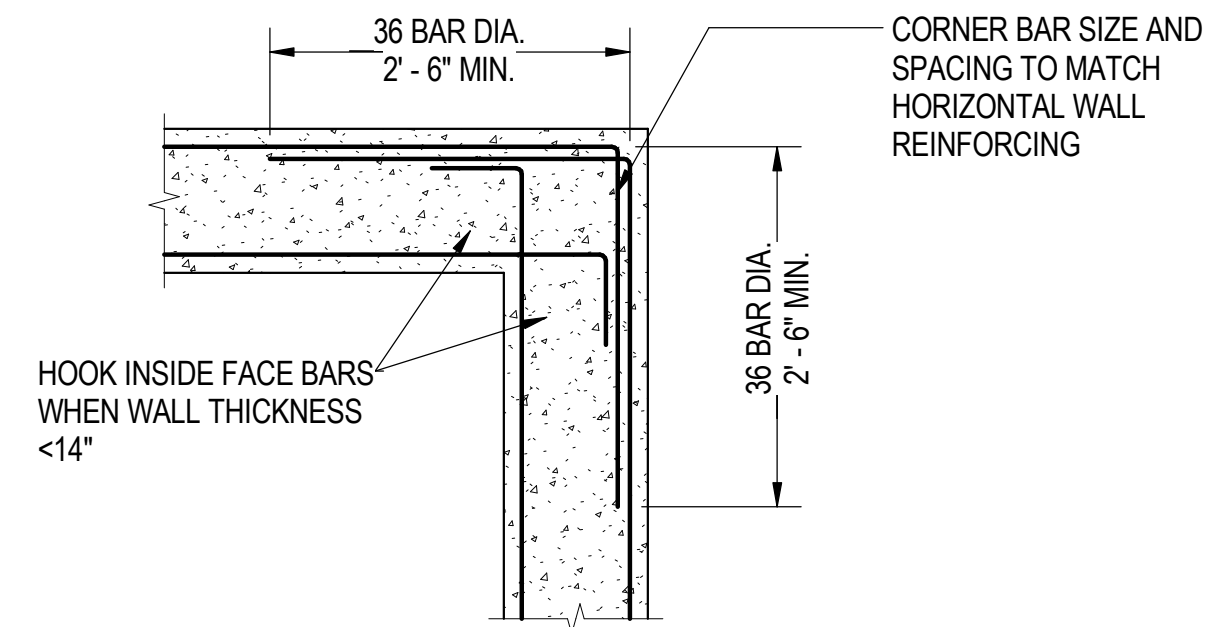
**2** BOTTOM BLOCKOUT DETAIL

S-1112 SCALE:  $3/4" = 1'-0"$



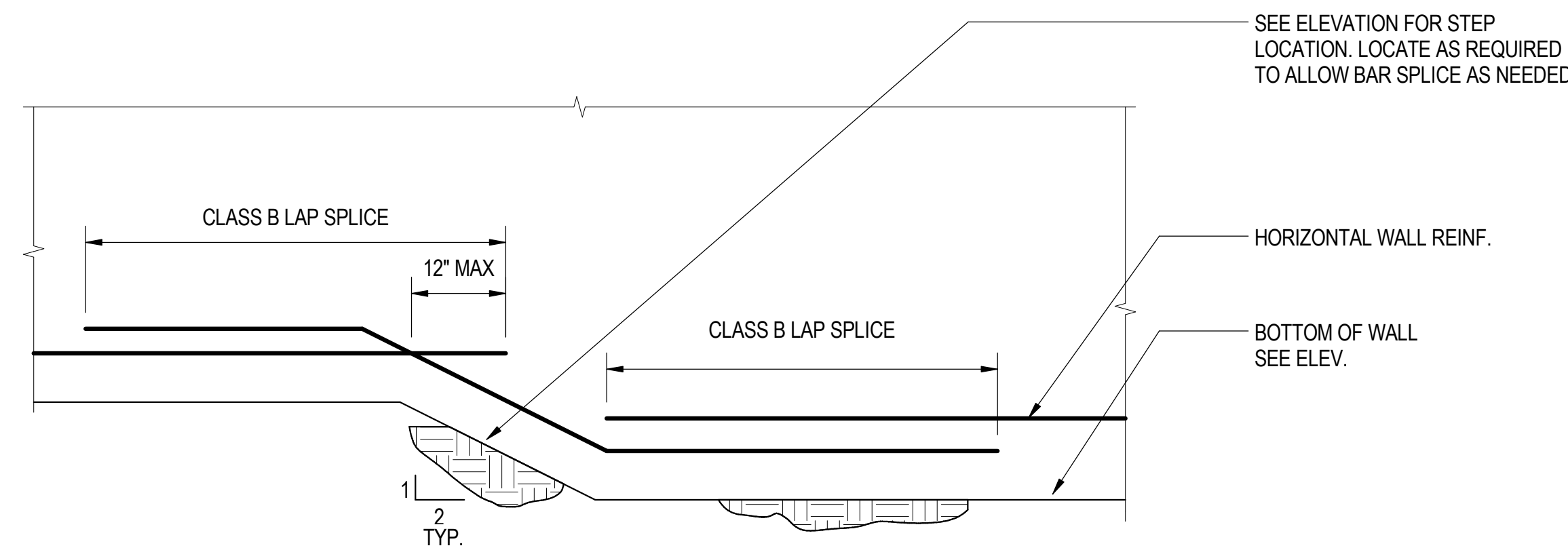
**3** MID BLOCKOUT DETAIL

S-1112 SCALE:  $3/4" = 1'-0"$



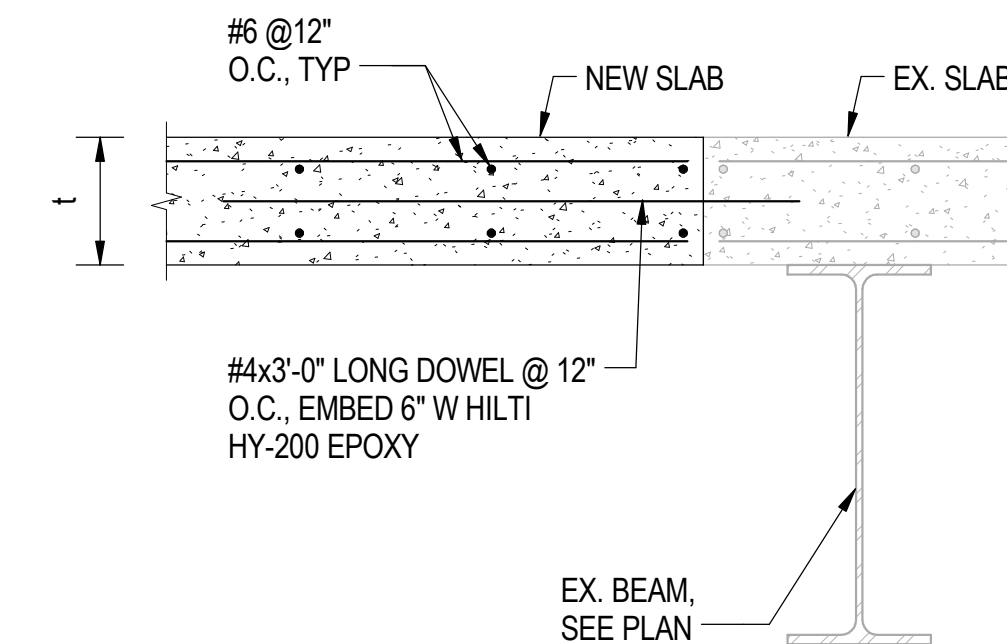
**4** TYPICAL CORNER DETAIL FOR HORIZONTAL REINFORCING

S-1112 SCALE:  $3/4" = 1'-0"$



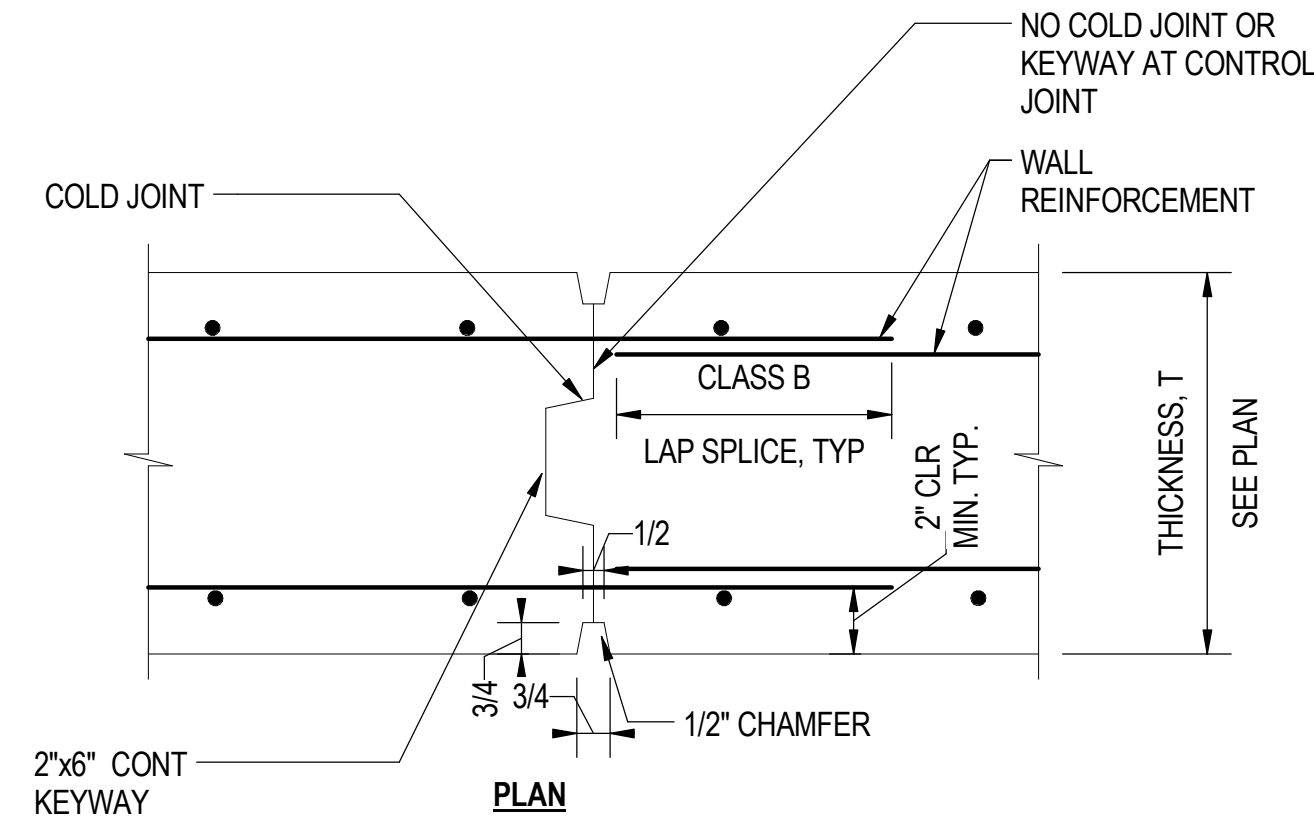
**5** TYP. STEP FTG. DETAIL

S-1112 SCALE:  $3/4" = 1'-0"$



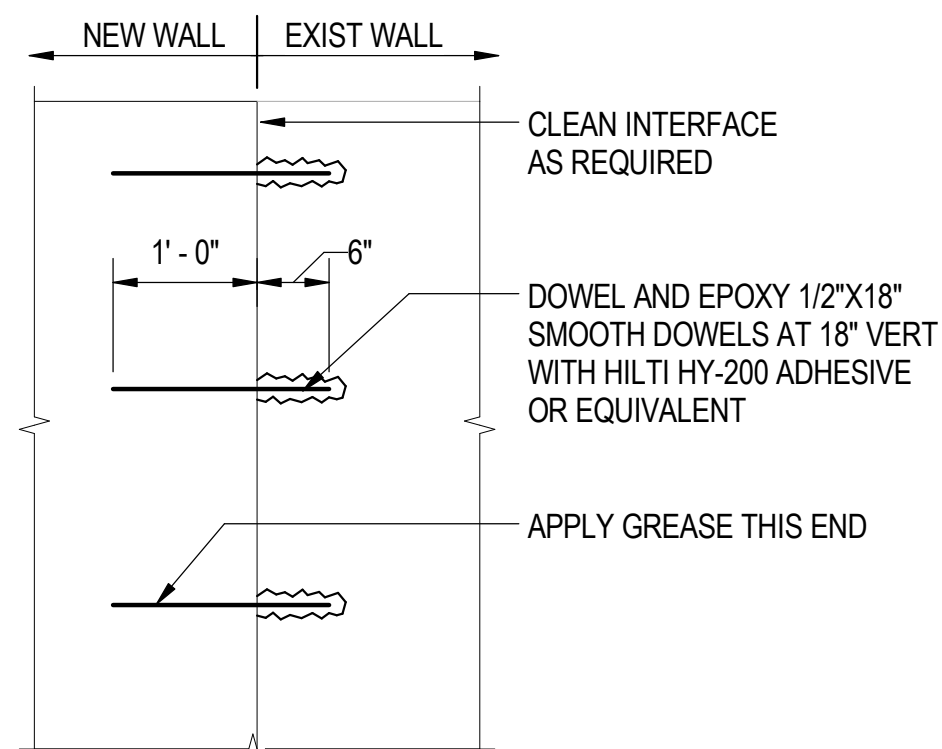
**6** NEW TO EXISTING SLAB JOINT DETAIL

S-1112 SCALE:  $1" = 1'-0"$



**7** CONST. JT. DETAIL FOR BARRIER WALLS

S-1112 SCALE:  $1 1/2" = 1'-0"$

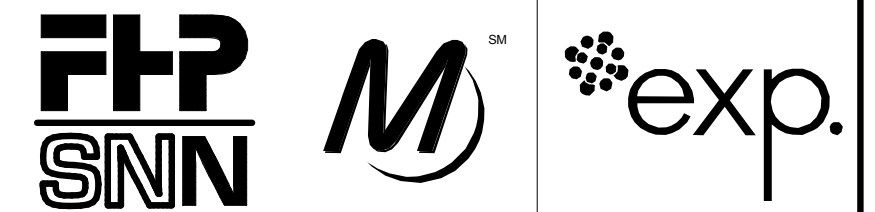


**8** DOWELS FOR NEW TO EXISTING WALL

S-1112 SCALE:  $3/4" = 1'-0"$

**NOTES:**

1. PRIOR TO PROCEEDING WITH ANY CONCRETE WALL OR SLAB POURS, THE CONTRACTOR SHALL SUBMIT COMPLETE COORDINATION DRAWINGS INDICATING ALL PENETRATIONS THROUGH CONCRETE FOR REVIEW AND APPROVAL. REFER TO MEP AND ARCH DWGS FOR ALL INFORMATION NOT SHOWN.



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**95th Terminal Improvement**

15 West 95th Street (South Terminal)  
Chicago, IL 60628

IN CHARGE	J. JAKALSKI
APPROVED BY	J. JAKALSKI
CHECKED BY	R.S. NAIR
DESIGNED BY	G. FORKAN
DRAWN BY	C. BOVE
PROJECT NO.	2012-0021.02
FILE NAME	DR095_S-Terminal

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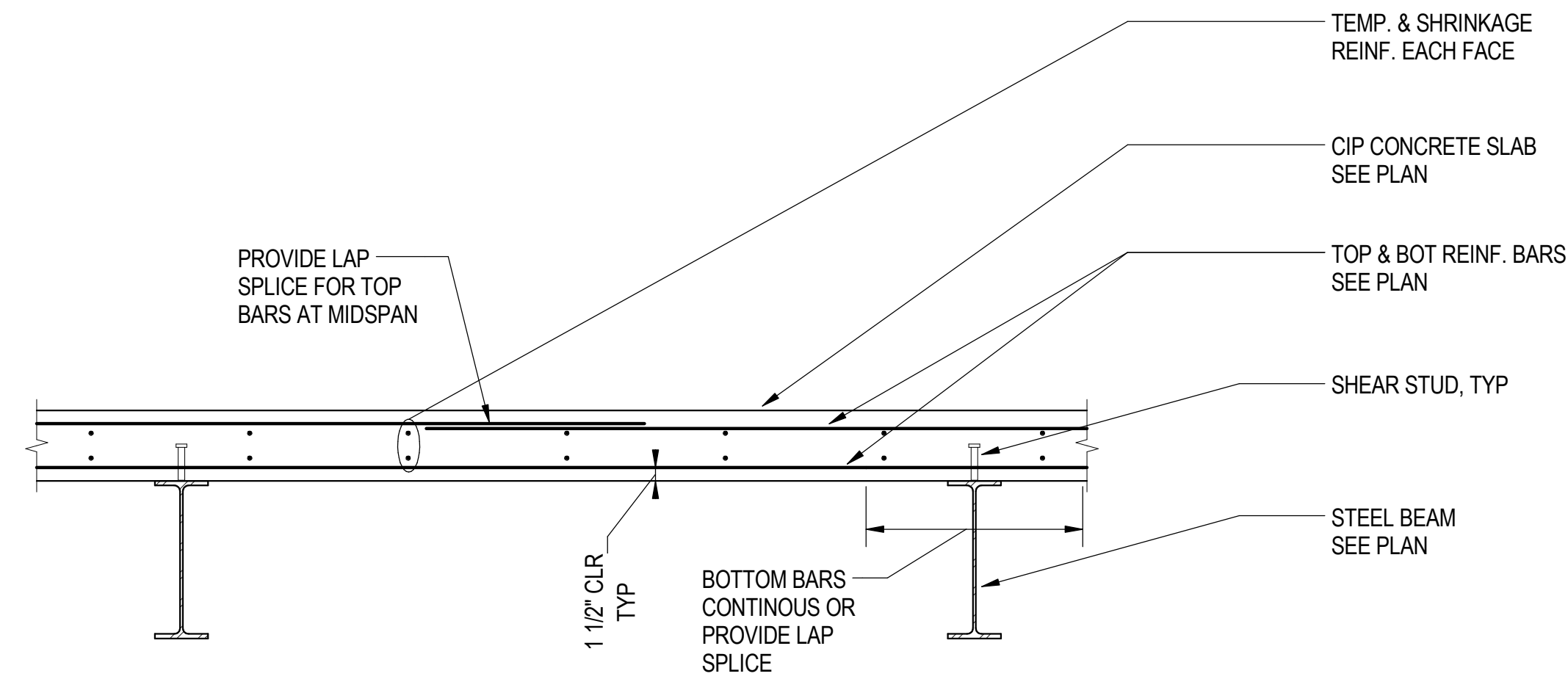
LOCATION IDENTIFIER: **DR095**

**TYPICAL DETAILS-CONCRETE-1**

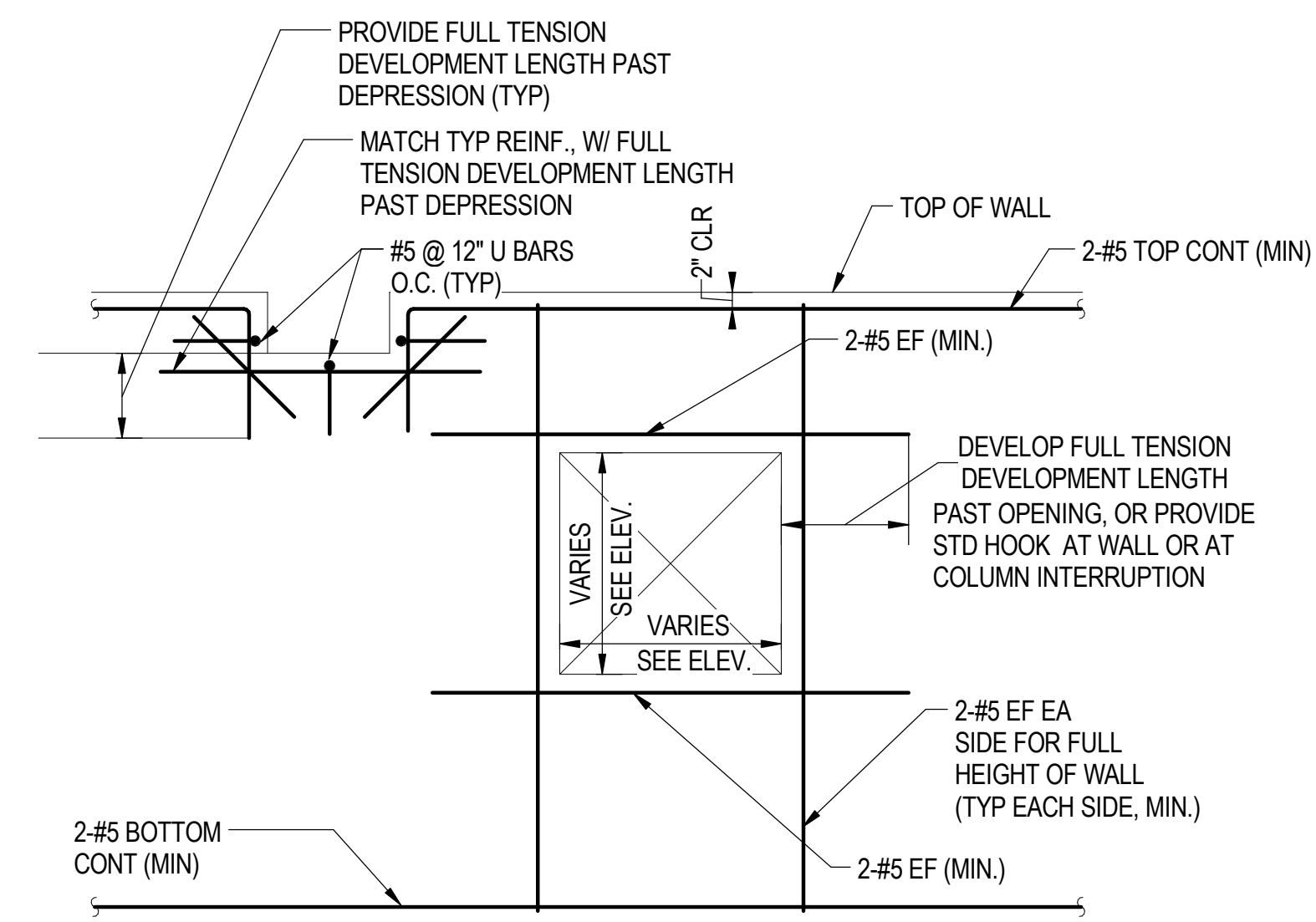
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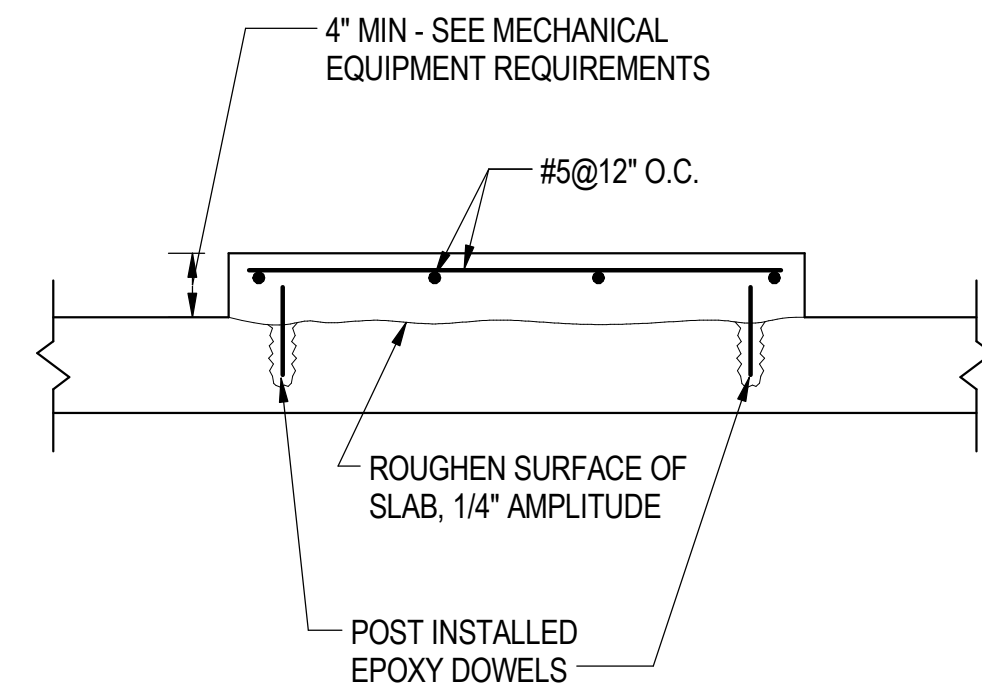
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**1** CIP CONCRETE SLAB DETAIL  
S-1113 SCALE: 3/4" = 1'-0"

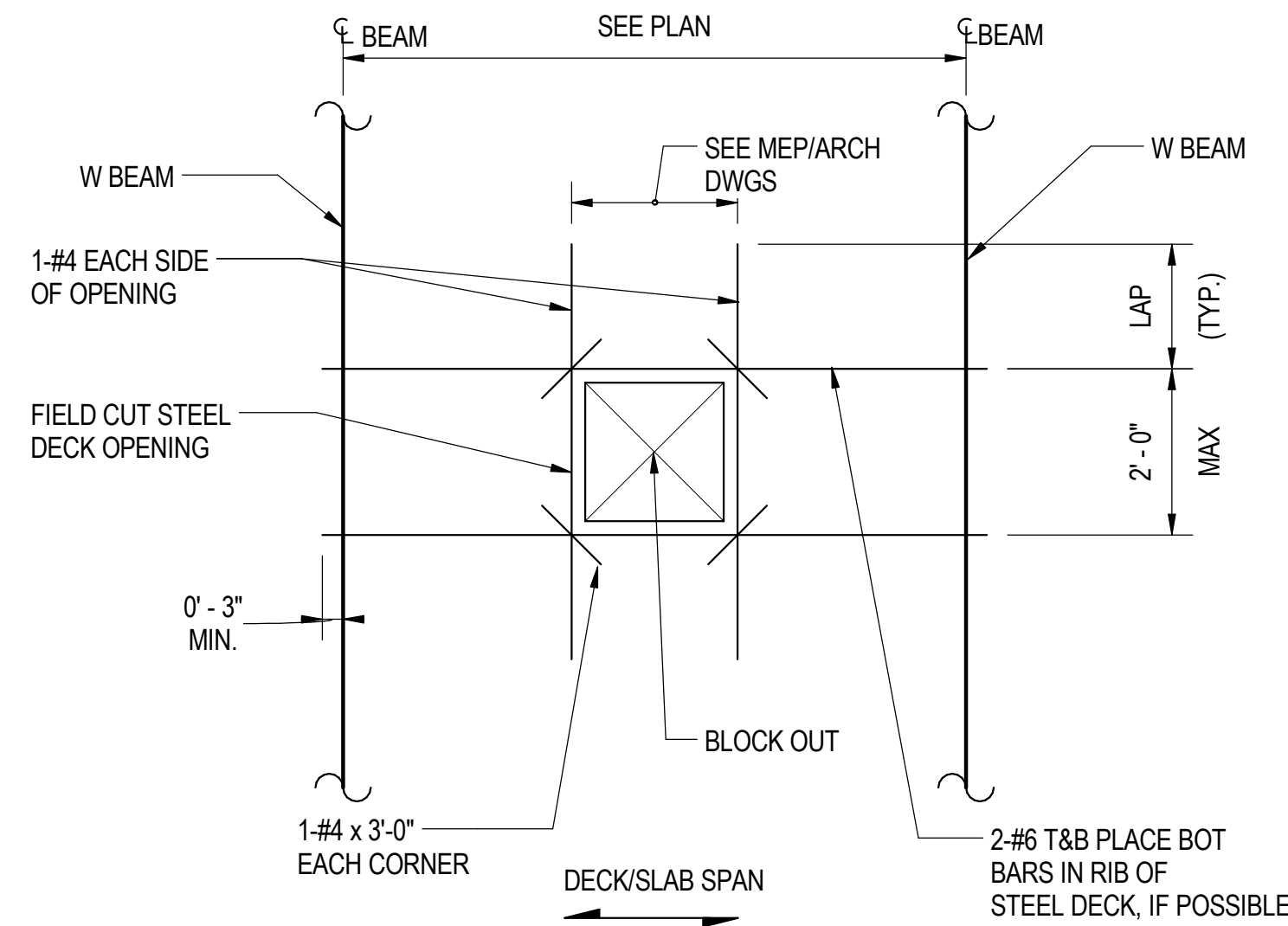


**3** OPENING IN REINFORCED CONCRETE WALLS  
S-1113 SCALE: 1" = 1'-0"



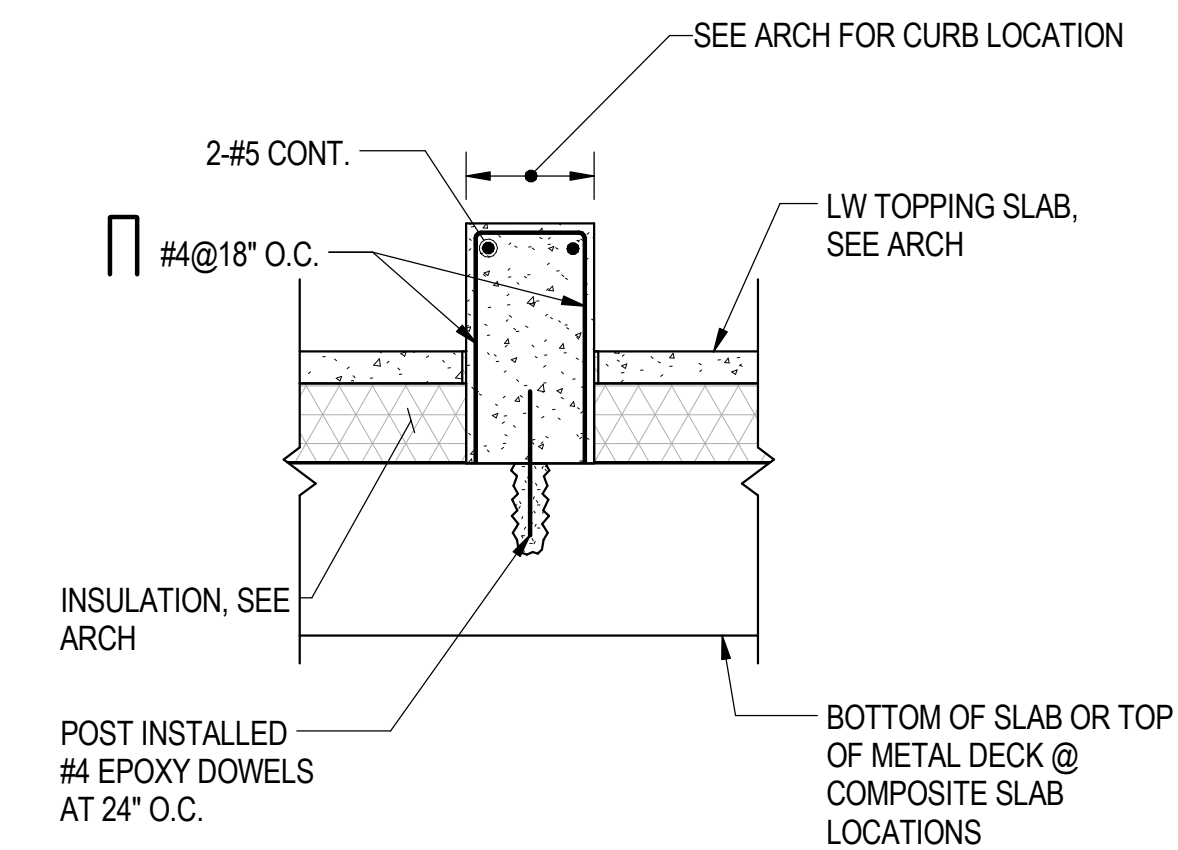
- NOTES:**
- CONTRACTOR TO SUBMIT ALL ANCHOR INFORMATION BASED ON MANUFACTURER REQUIREMENTS, DO NOT DAMAGE EXISTING REINFORCEMENT.
  - COORDINATE SIZE AND LOCATION WITH ARCHITECTURAL AND MECHANICAL DRAWINGS.

**4** CONCRETE PAD FOR MEP EQUIPMENT  
S-1113 SCALE: 1" = 1'-0"

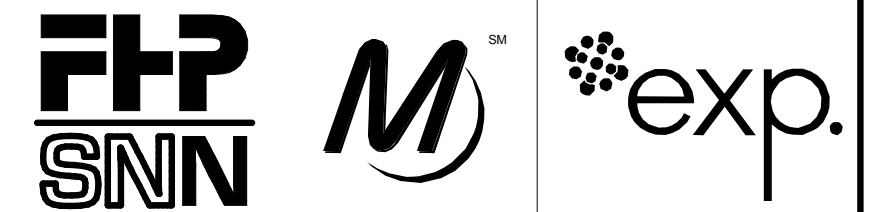


- NOTE:**
- DO NOT CUT DECKING UNTIL CONCRETE HAS ATTAINED 80% OF DESIGN STRENGTH.
  - OPENING MAY BE DEFINED BY A GROUP OF SLEEVES.
  - FOR OPENINGS LARGER THAN 2'-0", REFER TO PLANS FOR STRUCTURAL STEEL FRAMING AT OPENING LOCATIONS.
  - PRIOR TO PROCEEDING WITH ANY CONCRETE SLAB POURS, THE CONTRACTOR SHALL SUBMIT COMPLETE COORDINATION DRAWINGS INDICATING ALL PENETRATIONS THROUGH CONCRETE FOR REVIEW AND APPROVAL. REFER TO MEP AND ARCH DWGS FOR ALL INFORMATION NOT SHOWN.

**2** REINF AT CIP OR COMPOSITE SLAB OPENING  
S-1113 SCALE: 1/2" = 1'-0"



**5** INTERIOR CONCRETE CURB DETAIL  
S-1113 SCALE: 1" = 1'-0"



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**95th Terminal Improvement**  
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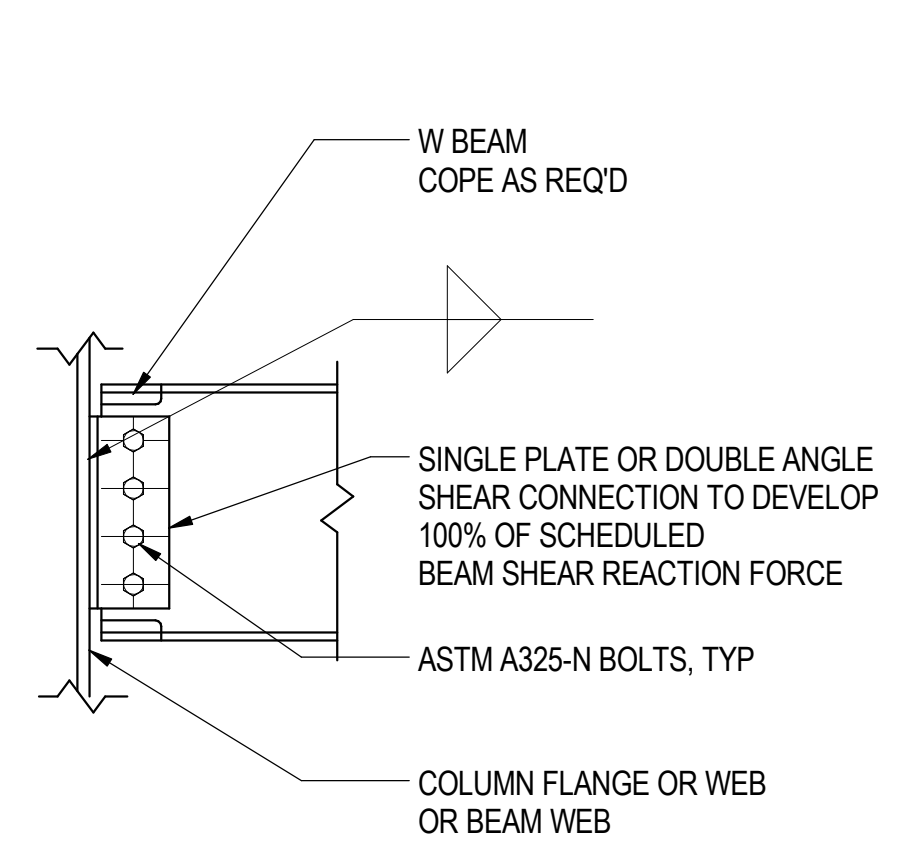
IN CHARGE	J. JAKALSKI
APPROVED BY	J. JAKALSKI
CHECKED BY	R.S. NAIR
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	2/9/17	ISSUED FOR CONSTRUCTION

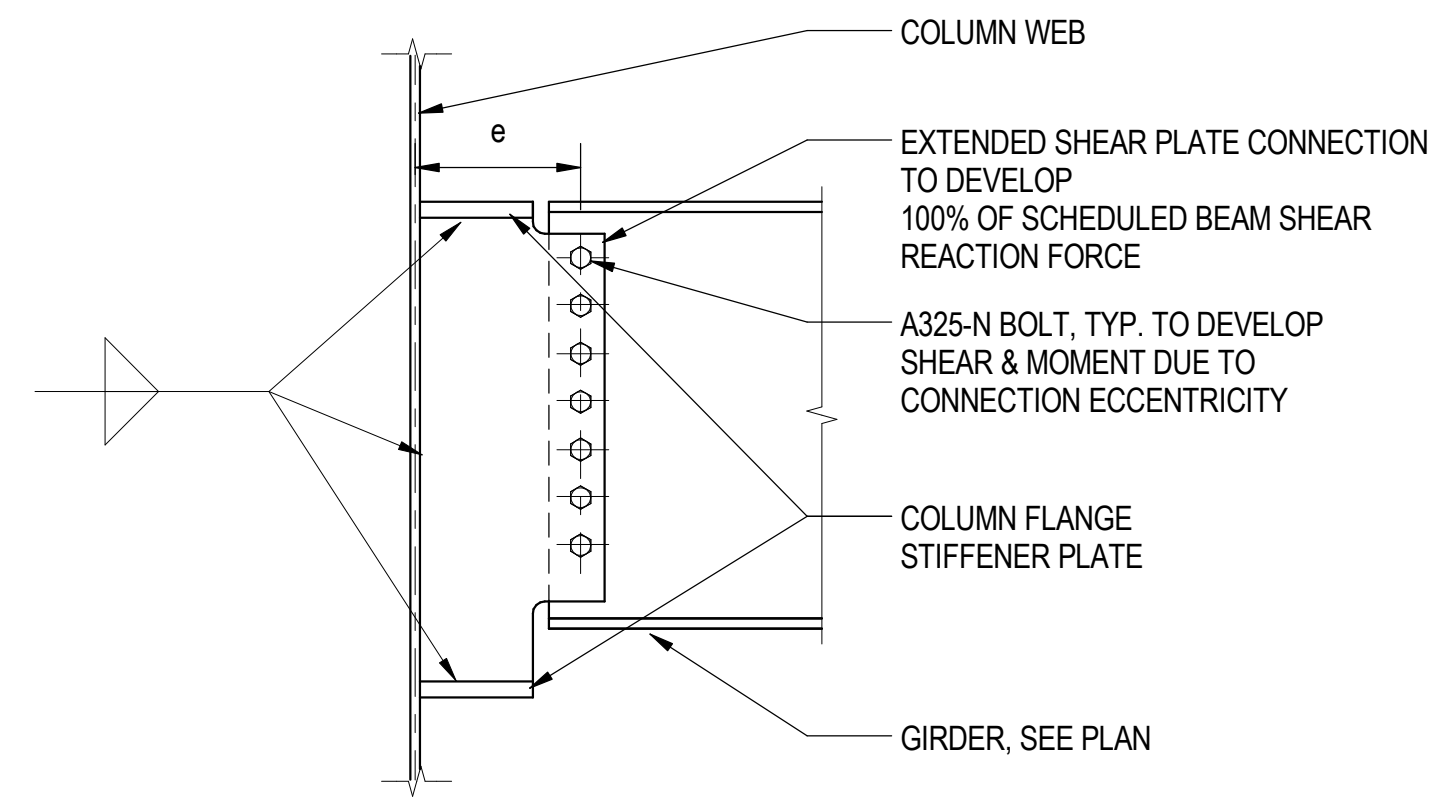
LOCATION IDENTIFIER: **DR095**

**TYPICAL DETAILS-CONCRETE-2**

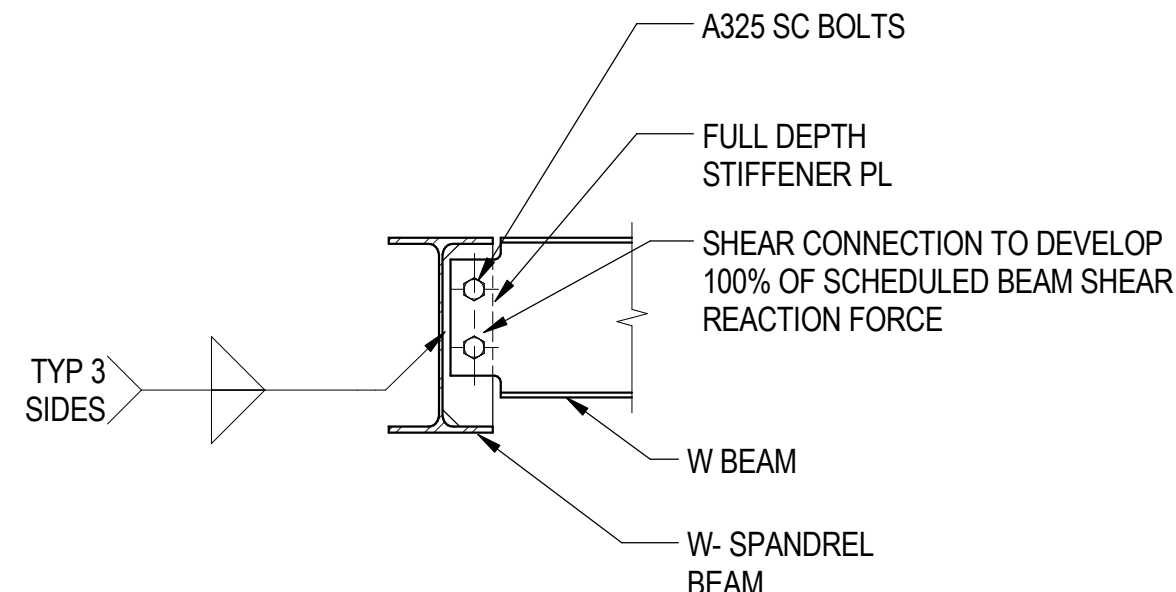
**S-1113**



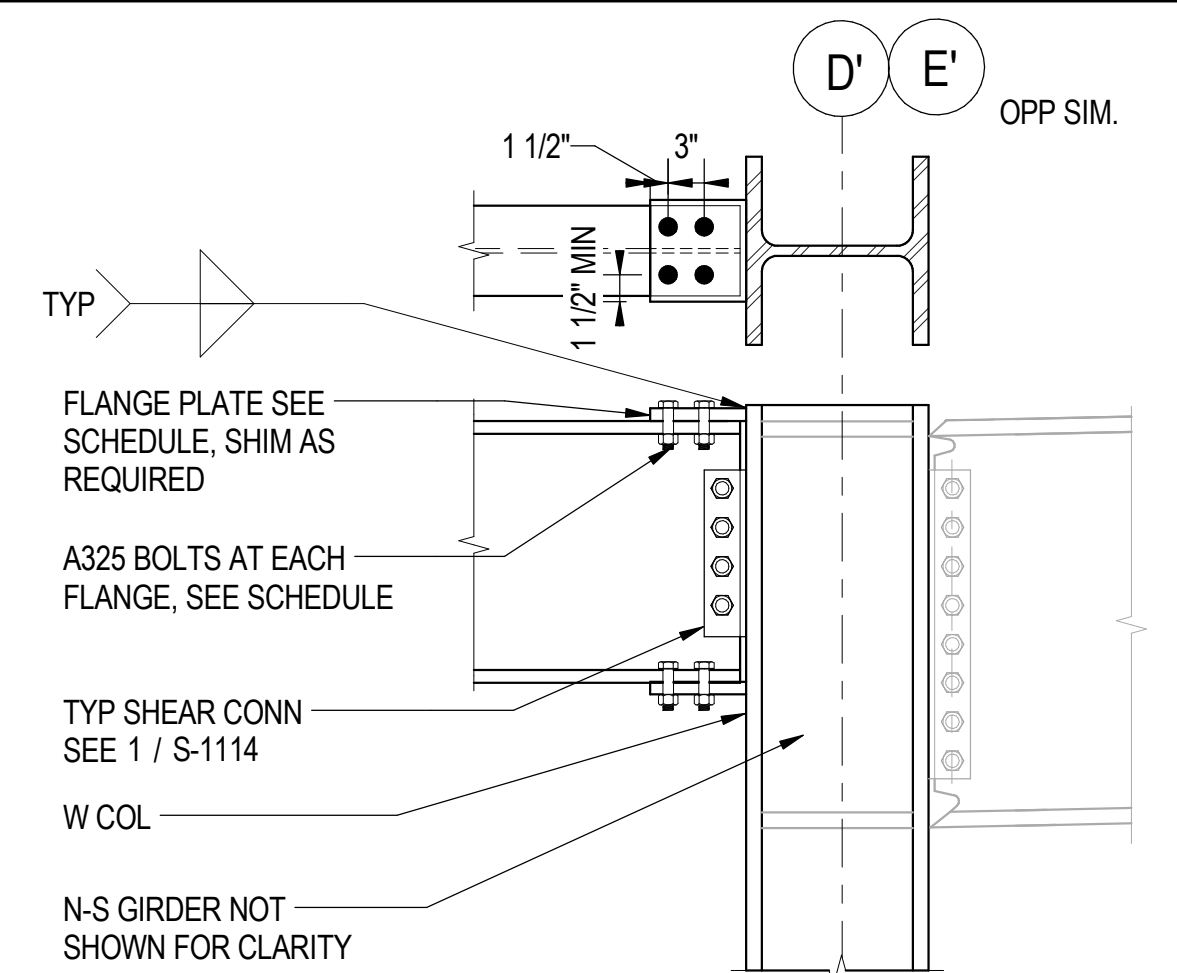
**1 TYP BEAM SHEAR CONNECTION**  
S-1114 SCALE: 1" = 1'-0"



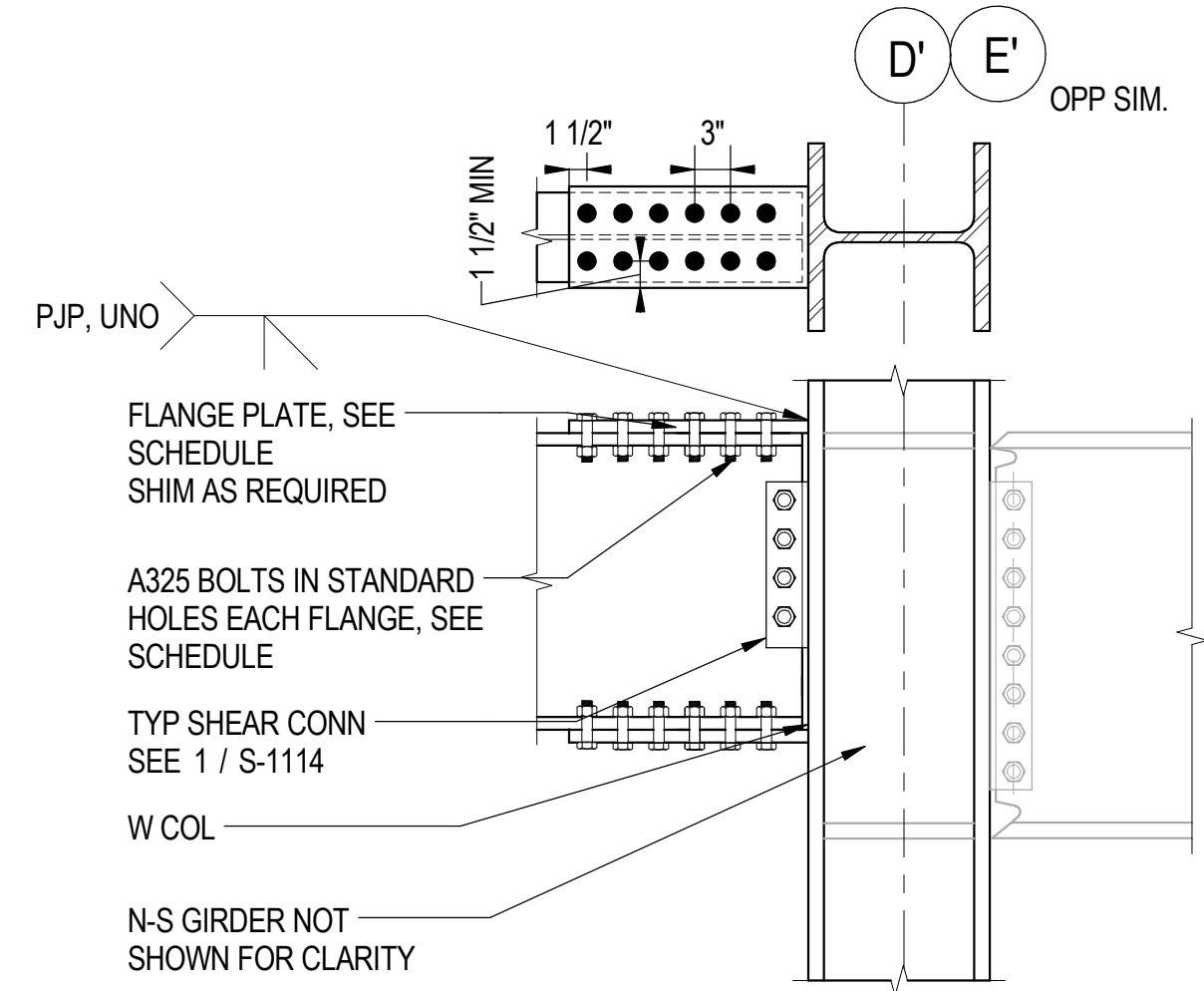
**2 GIRDER TO COLUMN AT STIFFENER CONNECTION**  
S-1114 SCALE: 1" = 1'-0"



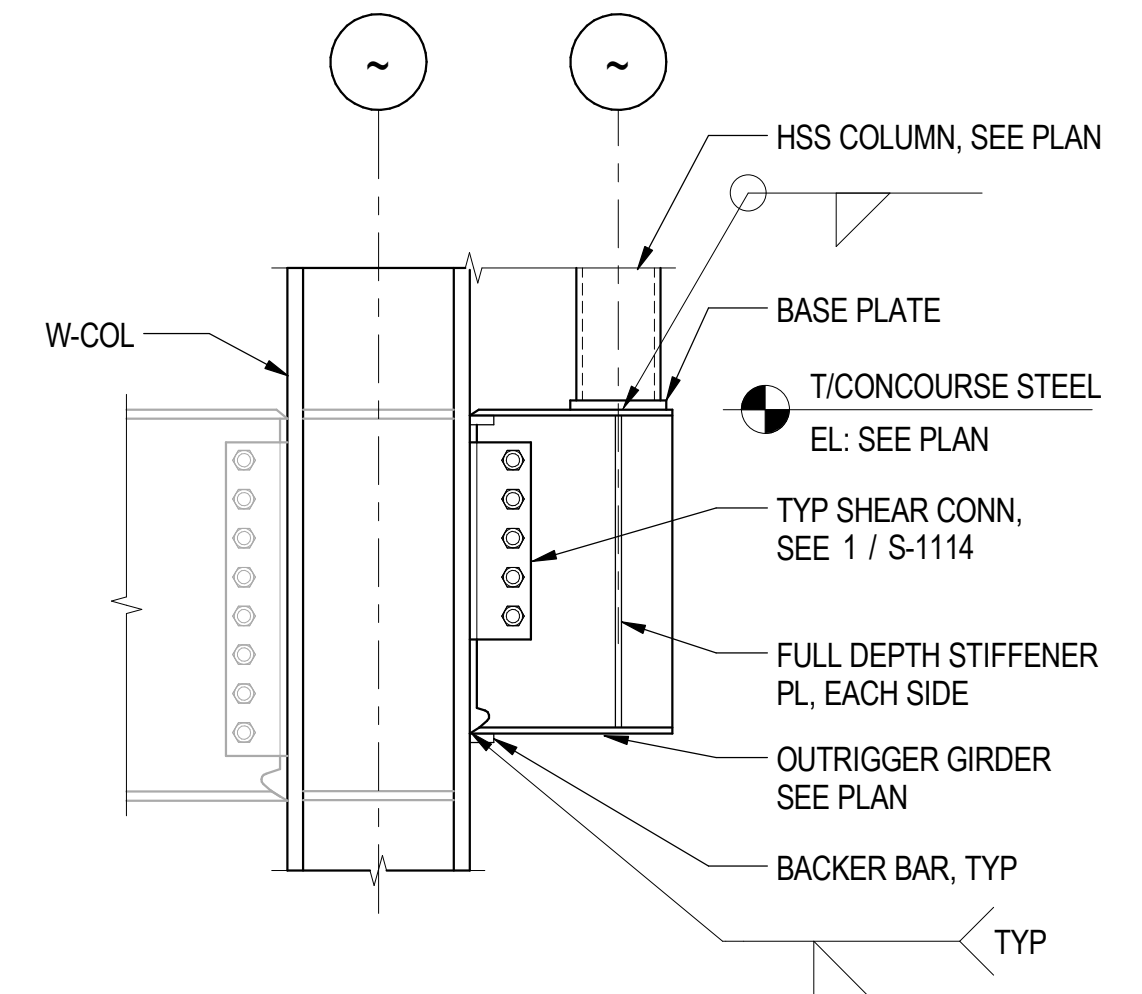
**3 BEAM TO FULL DEPTH STIFFENER**  
S-1114 SCALE: 1" = 1'-0"



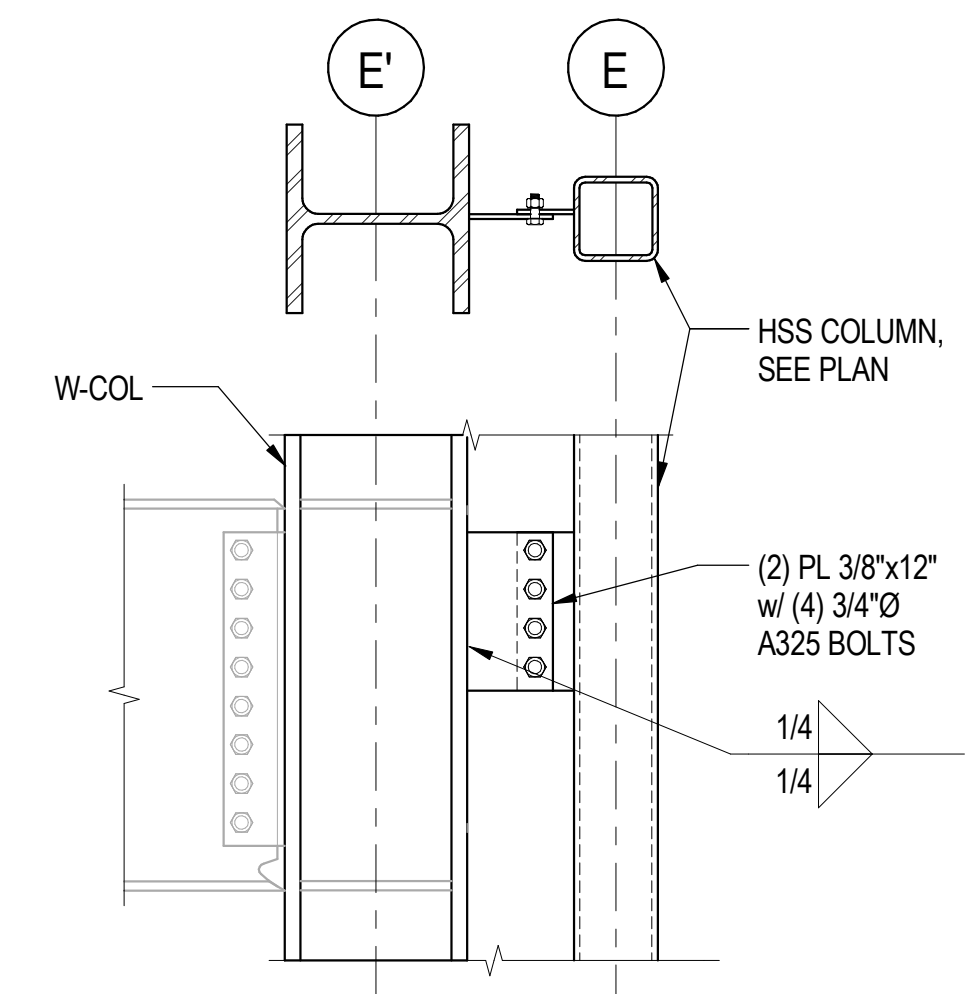
**4 ROOF OUTRIGGER CONNECTION**  
S-1114 SCALE: 3/4" = 1'-0"



**5 TYP OUTRIGGER CONNECTION**  
S-1114 SCALE: 3/4" = 1'-0"

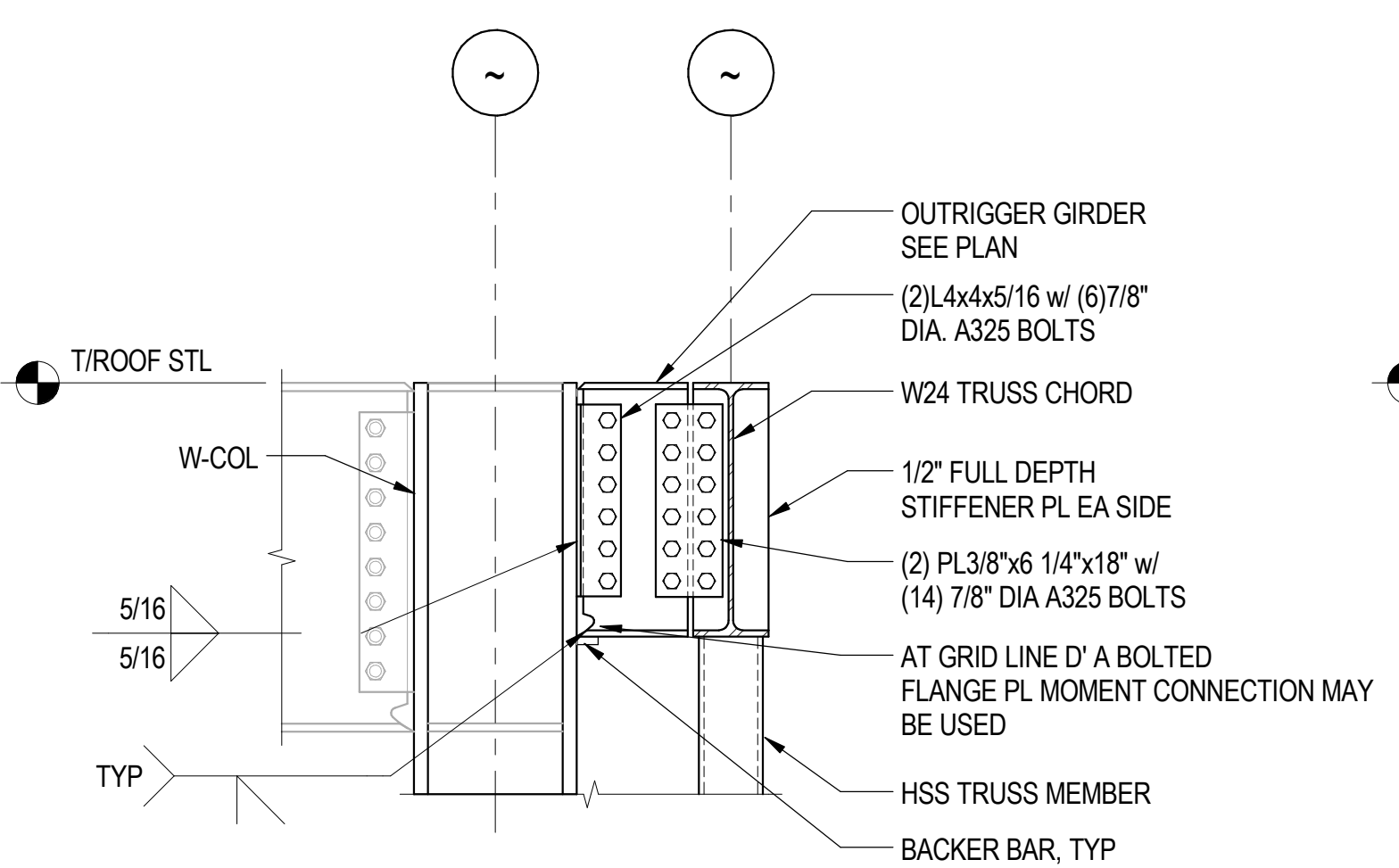


**6 CONCOURSE OUTRIGGER BEAM AT HSS COLUMN**  
S-1114 SCALE: 3/4" = 1'-0"

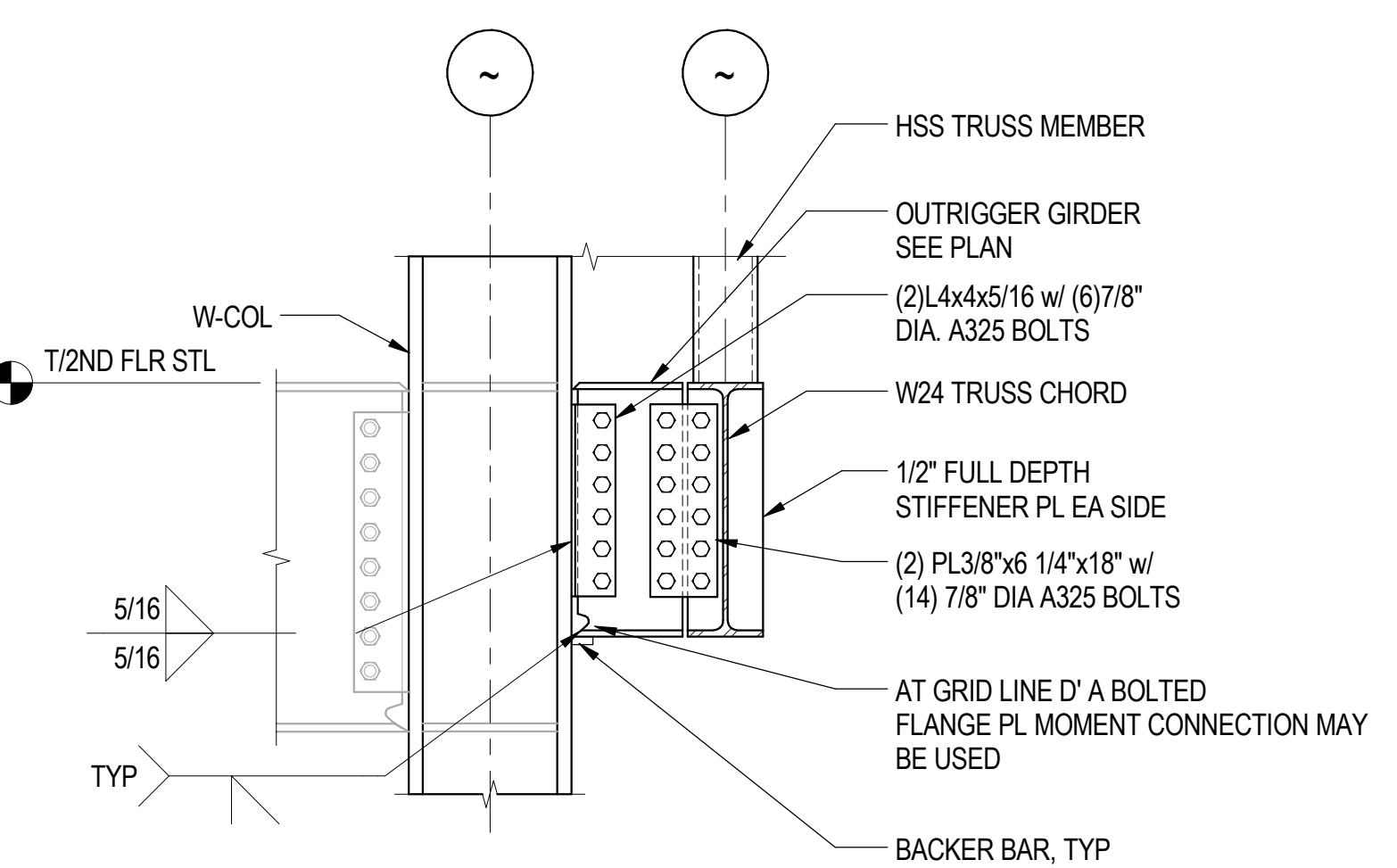


**7 CONNECTION AT HSS COL**  
S-1114 SCALE: 3/4" = 1'-0"

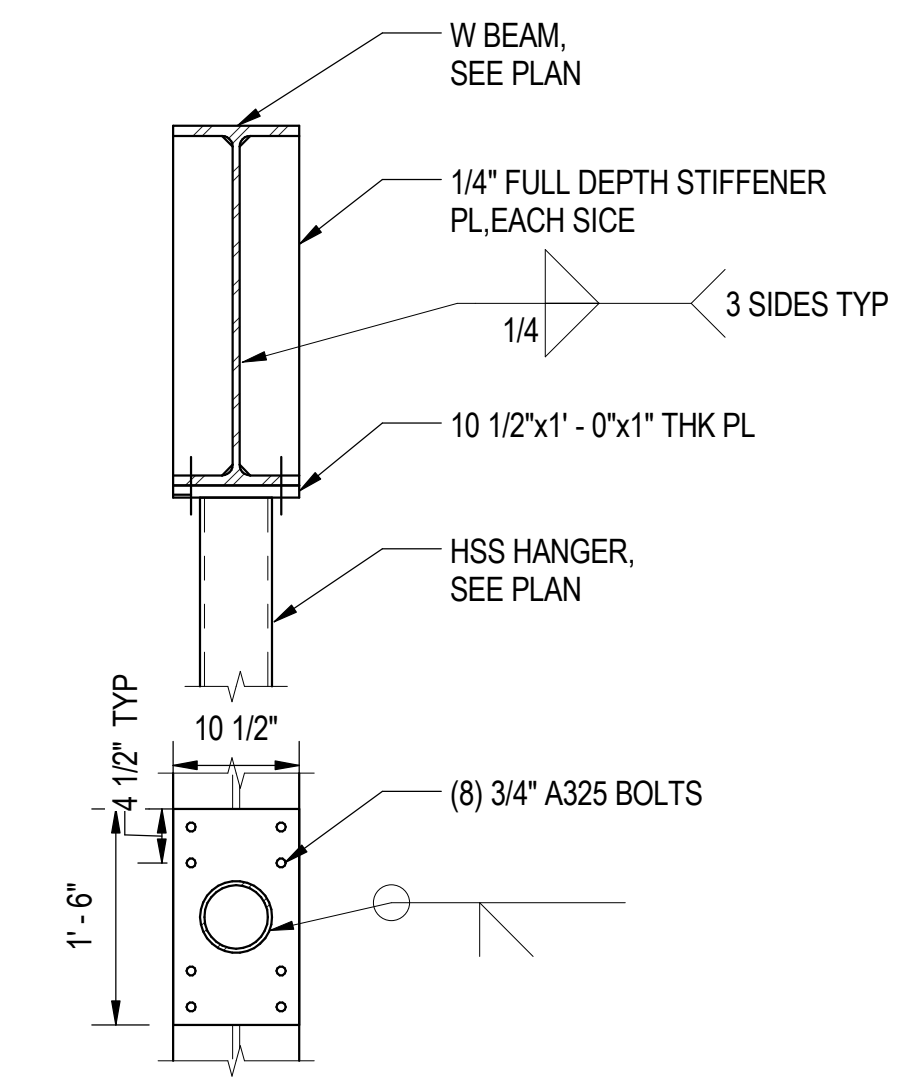
OUTRIGGER CONNECTION SCHEDULE					
MARK	BEAM	PLATE SIZE	BOLTS	WELD SIZE	DETAIL
1	W18	PL 3/4x7X6	(4) 7/8" A325	5/16"	4 / S-1114
2	W24	PL 1x7X9	(6) 7/8" A325	1/2"	5 / S-1114
3	W27	PL 1 1/4x9X18	(12) 1" A325	CJP	5 / S-1114



**8 ROOF OUTRIGGER BEAM AT TRUSS**  
S-1114 SCALE: 3/4" = 1'-0"



**9 OUTRIGGER BEAM AT TRUSS**  
S-1114 SCALE: 3/4" = 1'-0"



**10 HANGER CONNECTION**  
S-1114 SCALE: 3/4" = 1'-0"

SHEAR CONNECTION SCHEDULE	
BEAM	REACTION (kips)
W12	25
W18	50
W21	50
W24	70
W27	110
W30	125
W33	150
W36	200
HSS20	45

NOTES:  
1. SHEAR CONNECTIONS SHALL BE DESIGNED BY A LICENSED SE IN THE STATE OF IL TO DEVELOP 100% OF THE INDICATED FORCES  
2. INDICATED FORCES ARE FACTORED LOADS  
3. DESIGN FOR THE FORCES IN THIS SCHEDULE UNLESS NOTED OTHERWISE ON PLAN.



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**95th Terminal Improvement**  
15 West 95th Street (South Terminal)  
Chicago, IL 60628

IN CHARGE	J. JAKALSKI
APPROVED BY	J. JAKALSKI
CHECKED BY	R.S. NAIR
DESIGNED BY	G. FORKAN
DRAWN BY	C. BOVE
PROJECT NO.	2012-0021.02
FILE NAME	DR095_S-Terminal

MARK	DATE	DESCRIPTION
	2/9/17	ISSUED FOR CONSTRUCTION

LOCATION IDENTIFIER: **DR095**

**TYPICAL DETAILS - STEEL - 1**

**S-1114**

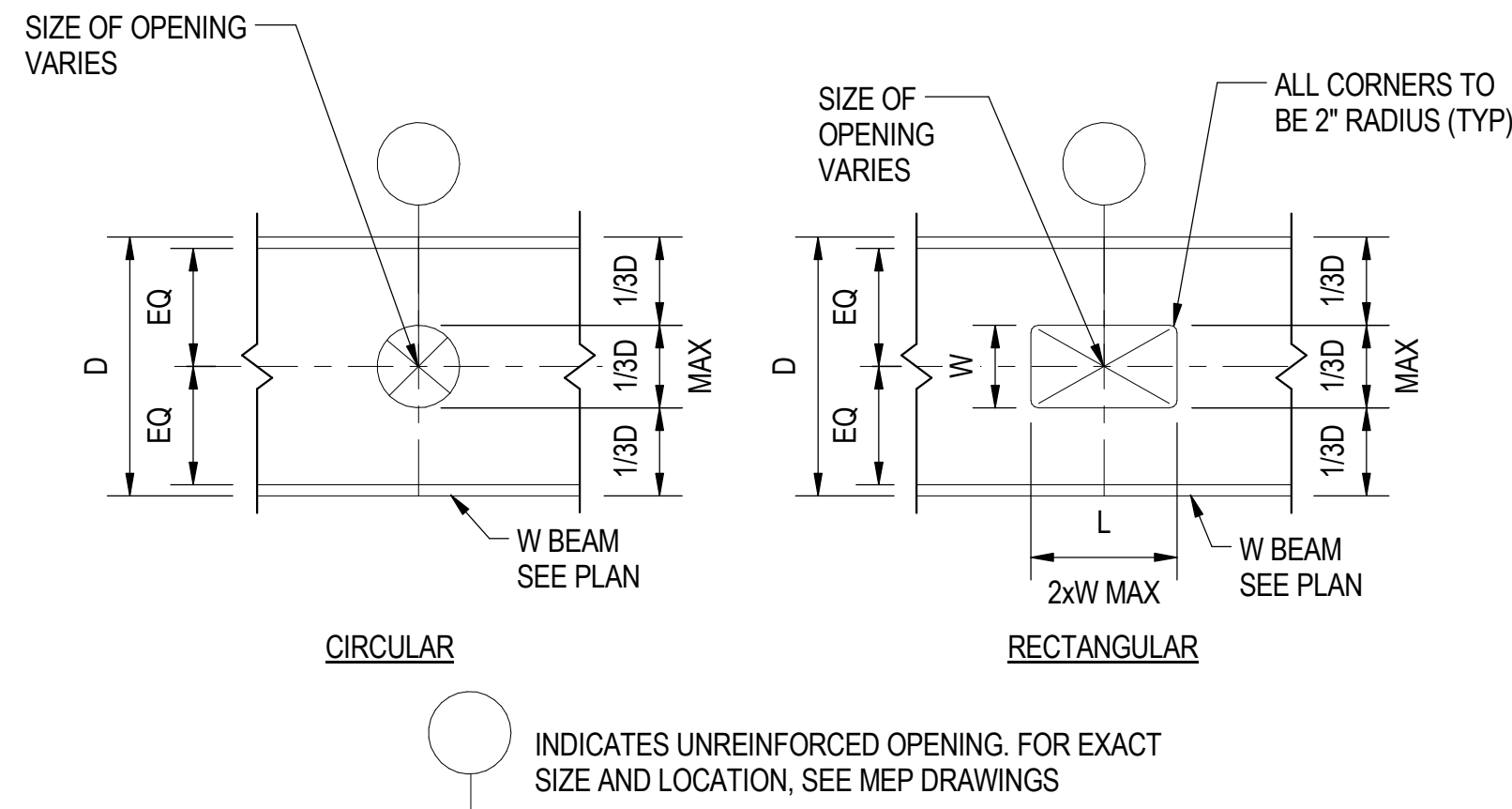
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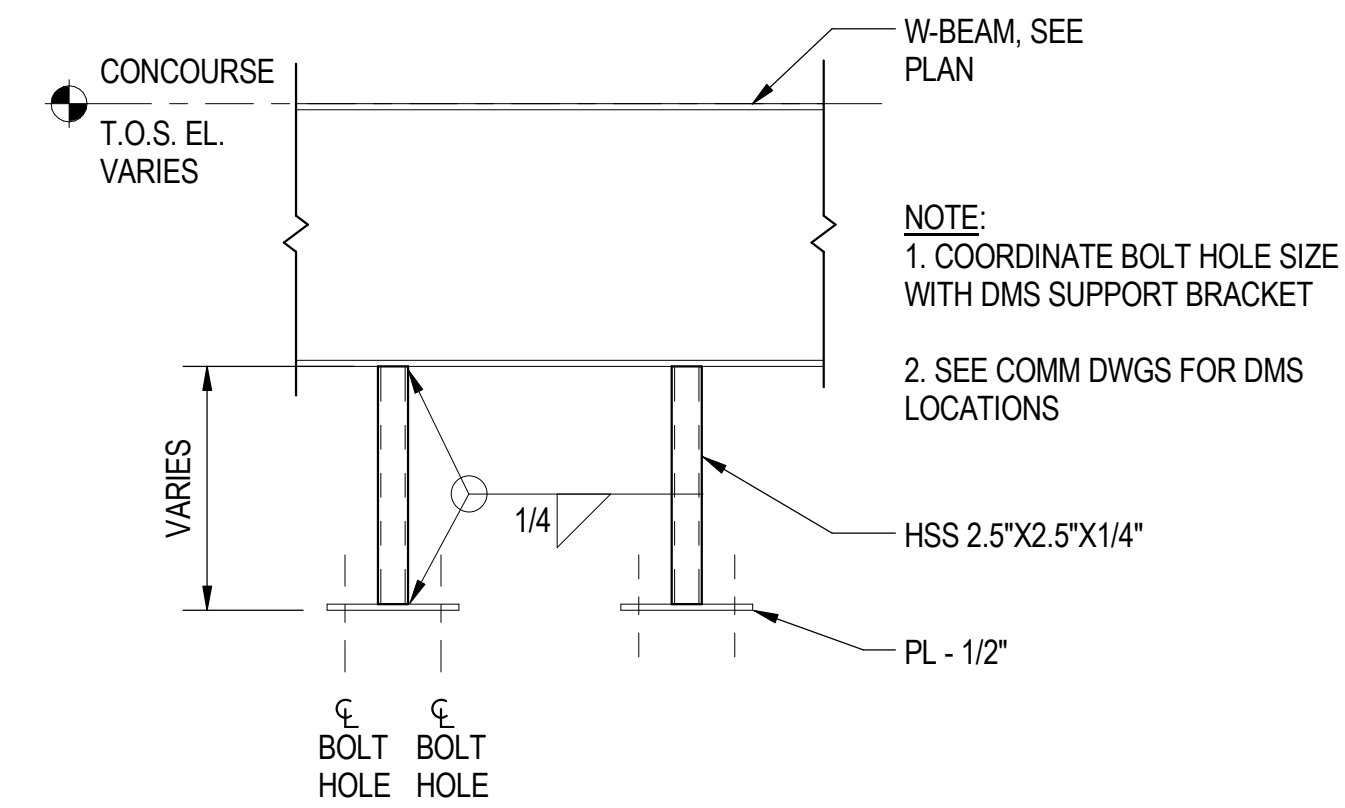
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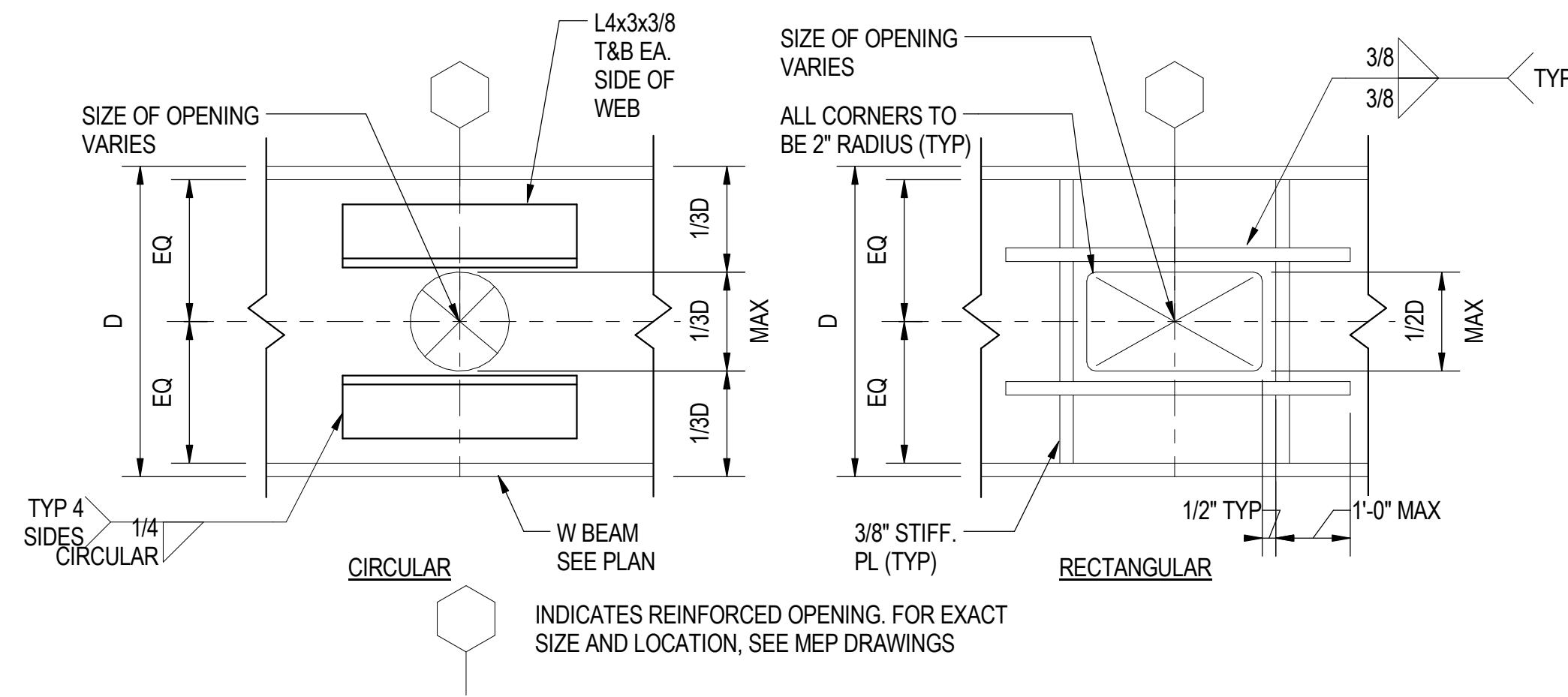


**1 UNREINFORCED BEAM OPENING**  
S-1115 SCALE: 1" = 1'-0"

**2 ROOF OPENING IN STEEL DECK**  
S-1115 SCALE: 1/4" = 1'-0"

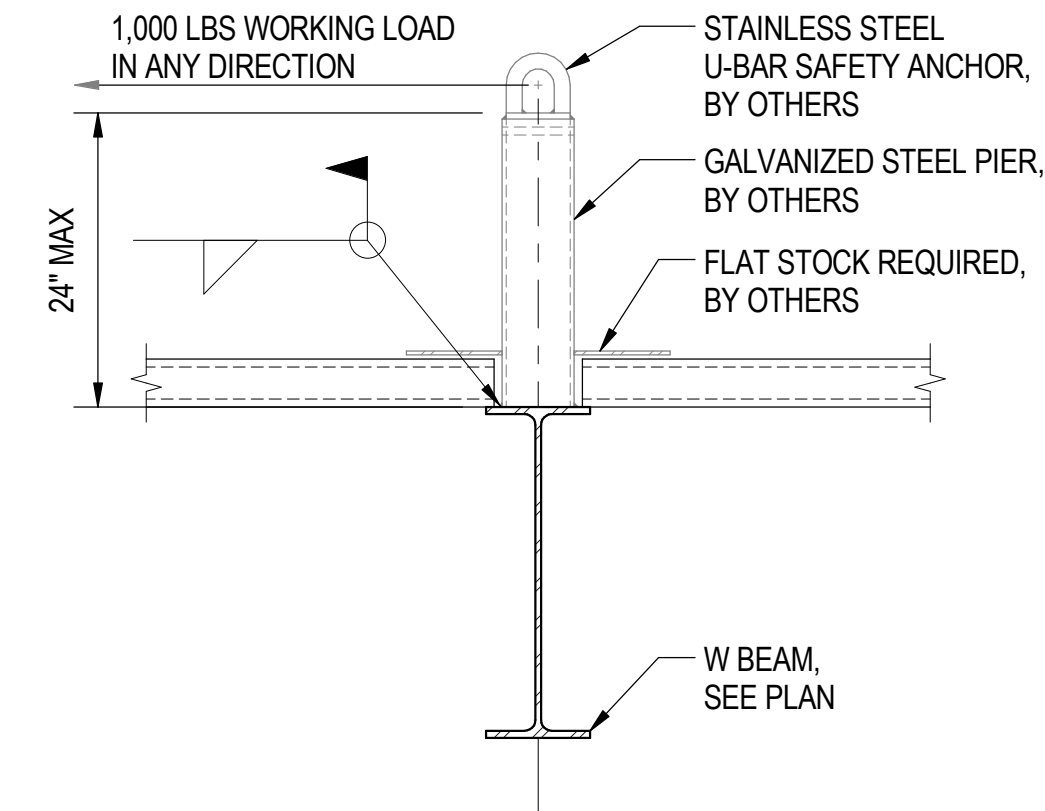


**3 DMS SUPPORTS AT PLATFORM**  
S-1115 SCALE: 3/4" = 1'-0"

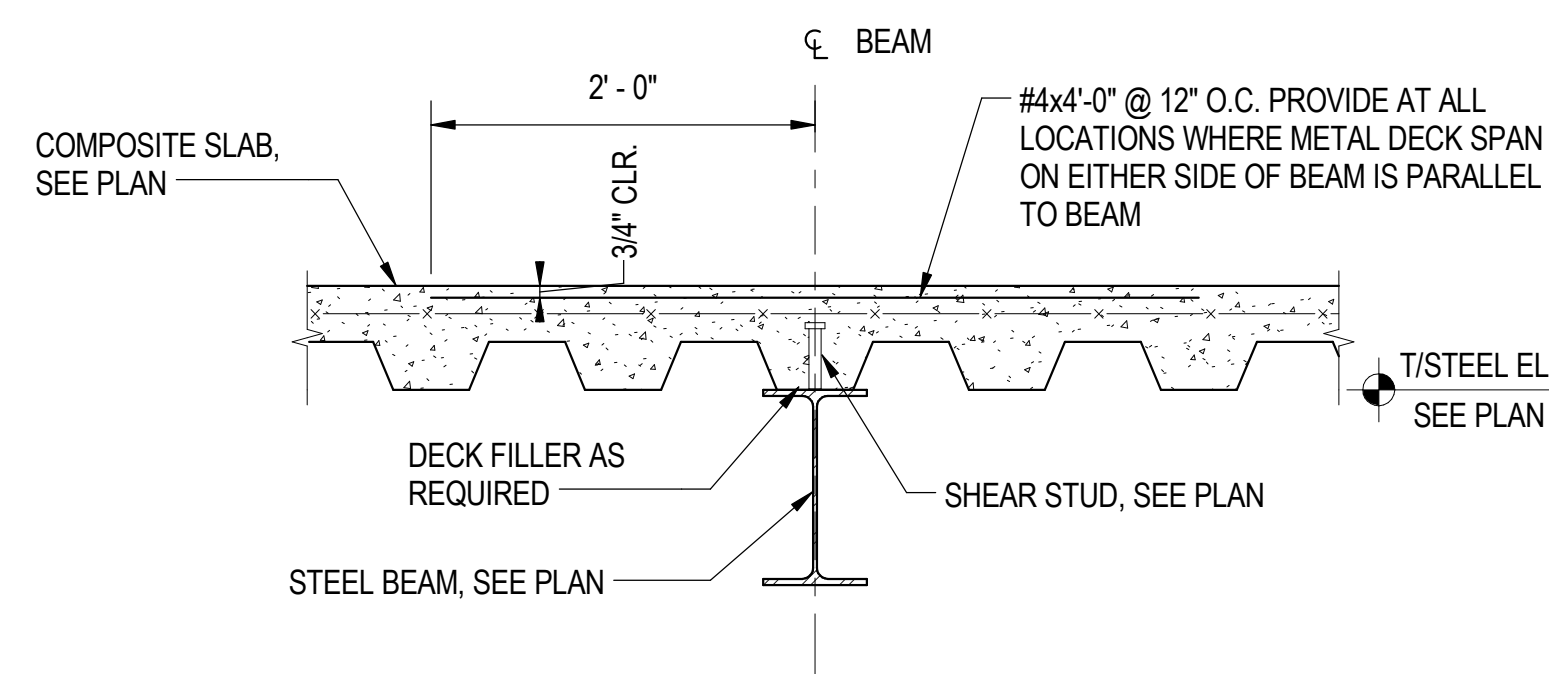


**4 REINFORCED BEAM OPENING**  
S-1115 SCALE: 1 1/2" = 1'-0"

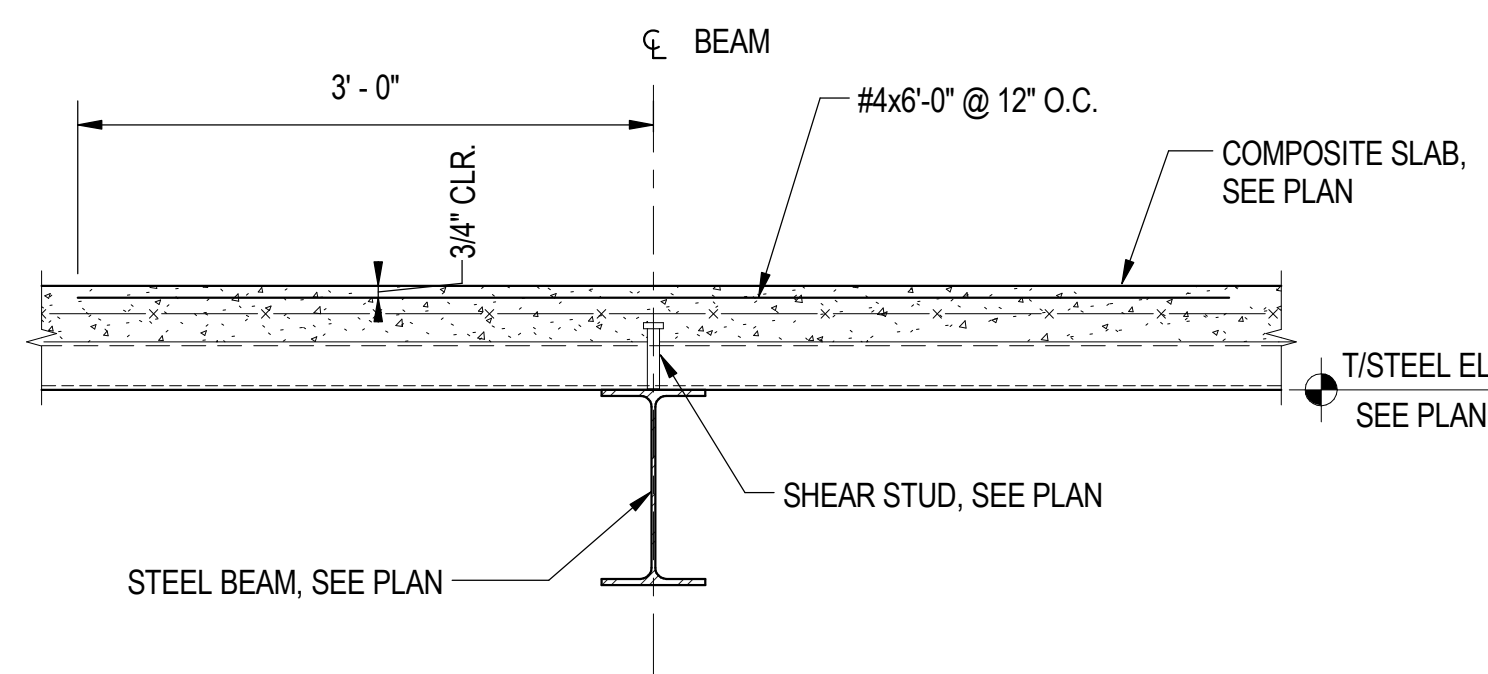
**5 SLAB OPENING FOR MECH**  
S-1115 SCALE: 1/4" = 1'-0"



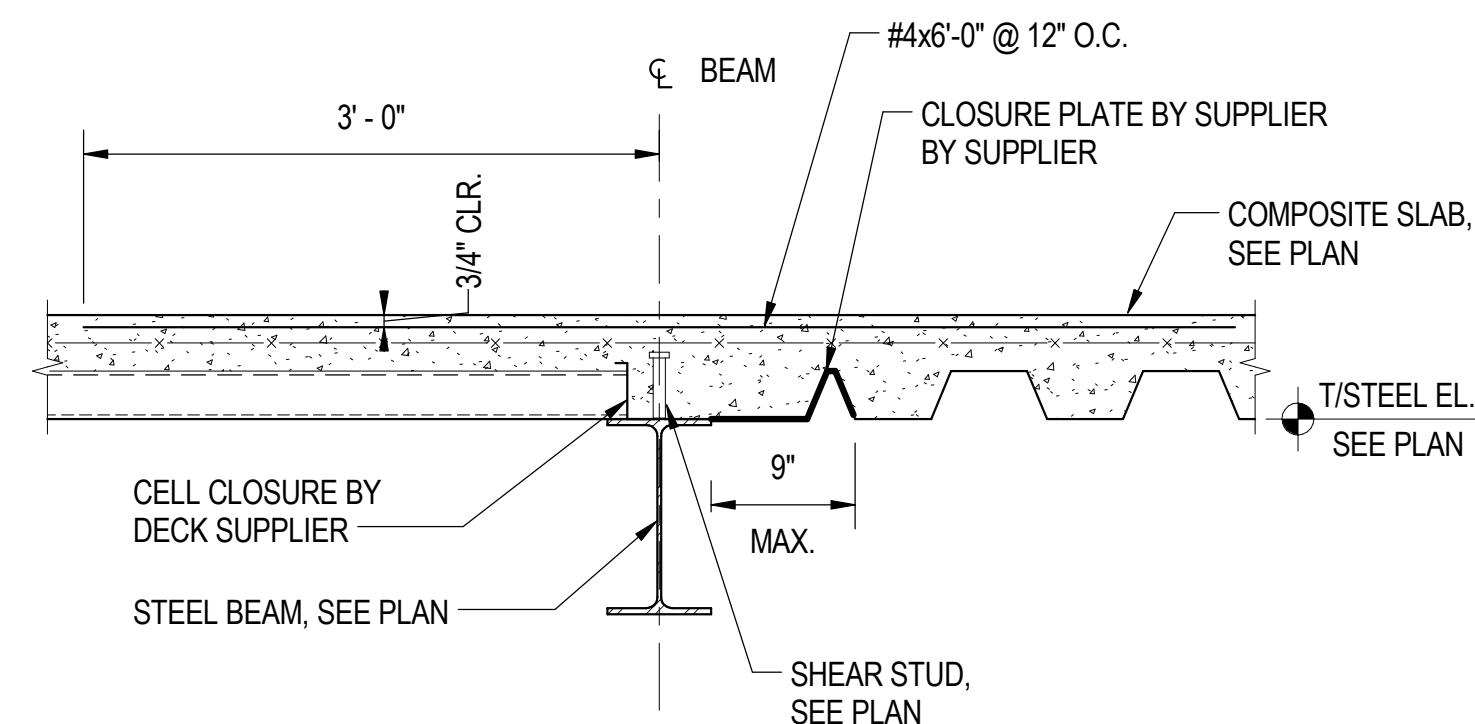
**6 DAVIT DETAIL**  
S-1115 SCALE: 1" = 1'-0"



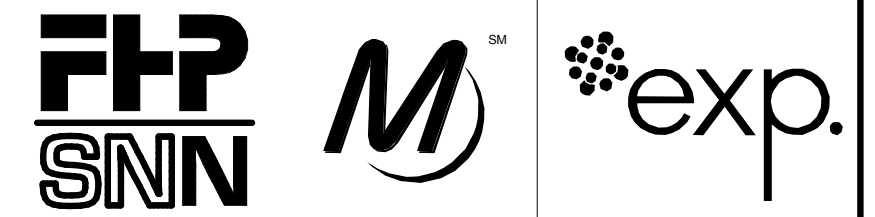
**7 TYP. COMPOSITE SLAB PARALLEL TO BEAM**  
S-1115 SCALE: 1" = 1'-0"



**8 TYP. COMPOSITE SLAB PERP. TO BEAM**  
S-1115 SCALE: 1" = 1'-0"



**9 TYP COMPOSITE SLAB DIRECTION CHANGE AT BEAM**  
S-1115 SCALE: 1" = 1'-0"



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**95th Terminal Improvement**  
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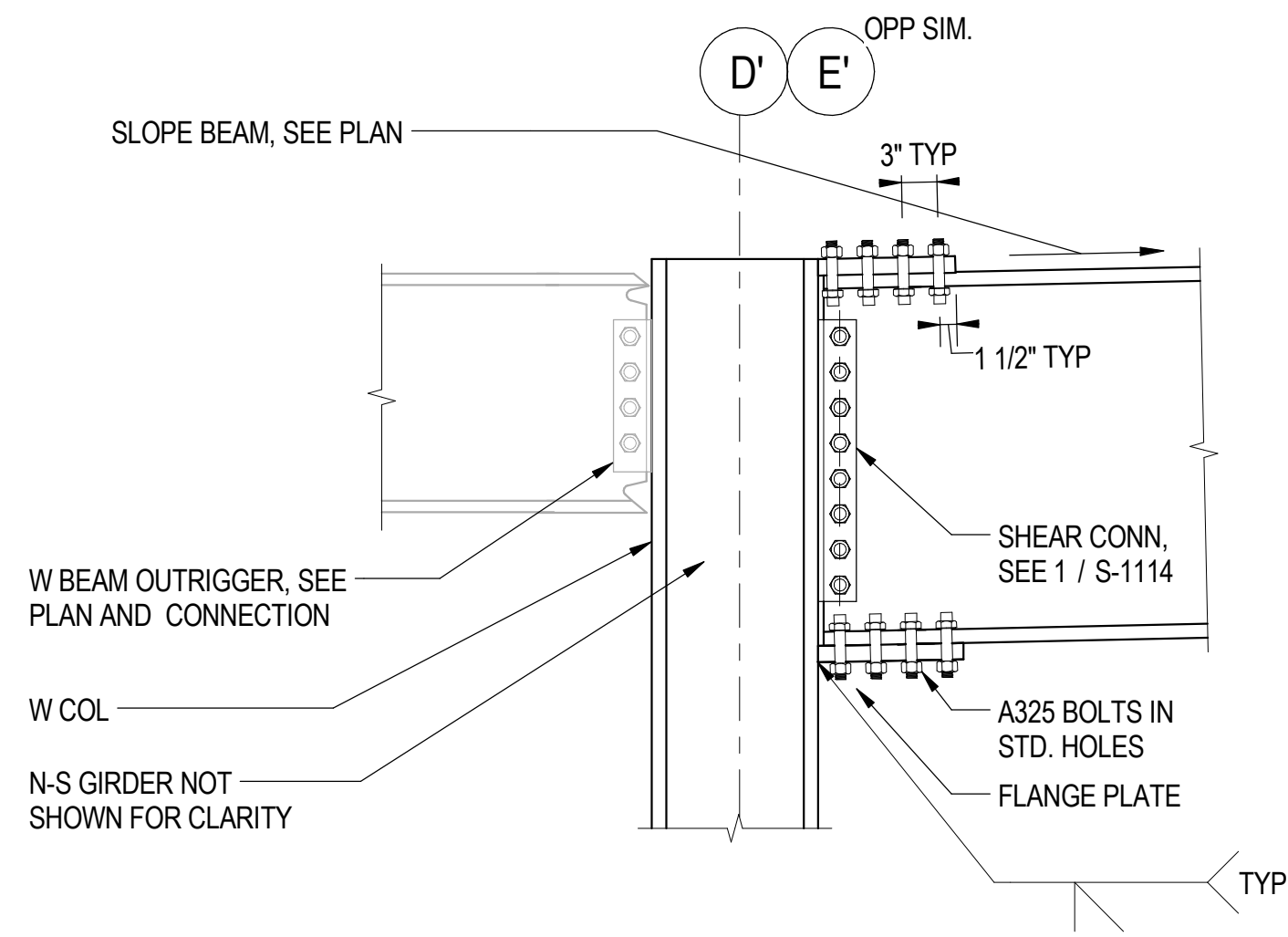
IN CHARGE	J. JAKALSKI
APPROVED BY	J. JAKALSKI
CHECKED BY	R.S. NAIR
DESIGNED BY	G. FORKAN
DRAWN BY	C. BOVE
PROJECT NO.	2012-0021.02
FILE NAME	DR095_S-Terminal

MARK	DATE	DESCRIPTION
	2/9/17	ISSUED FOR CONSTRUCTION

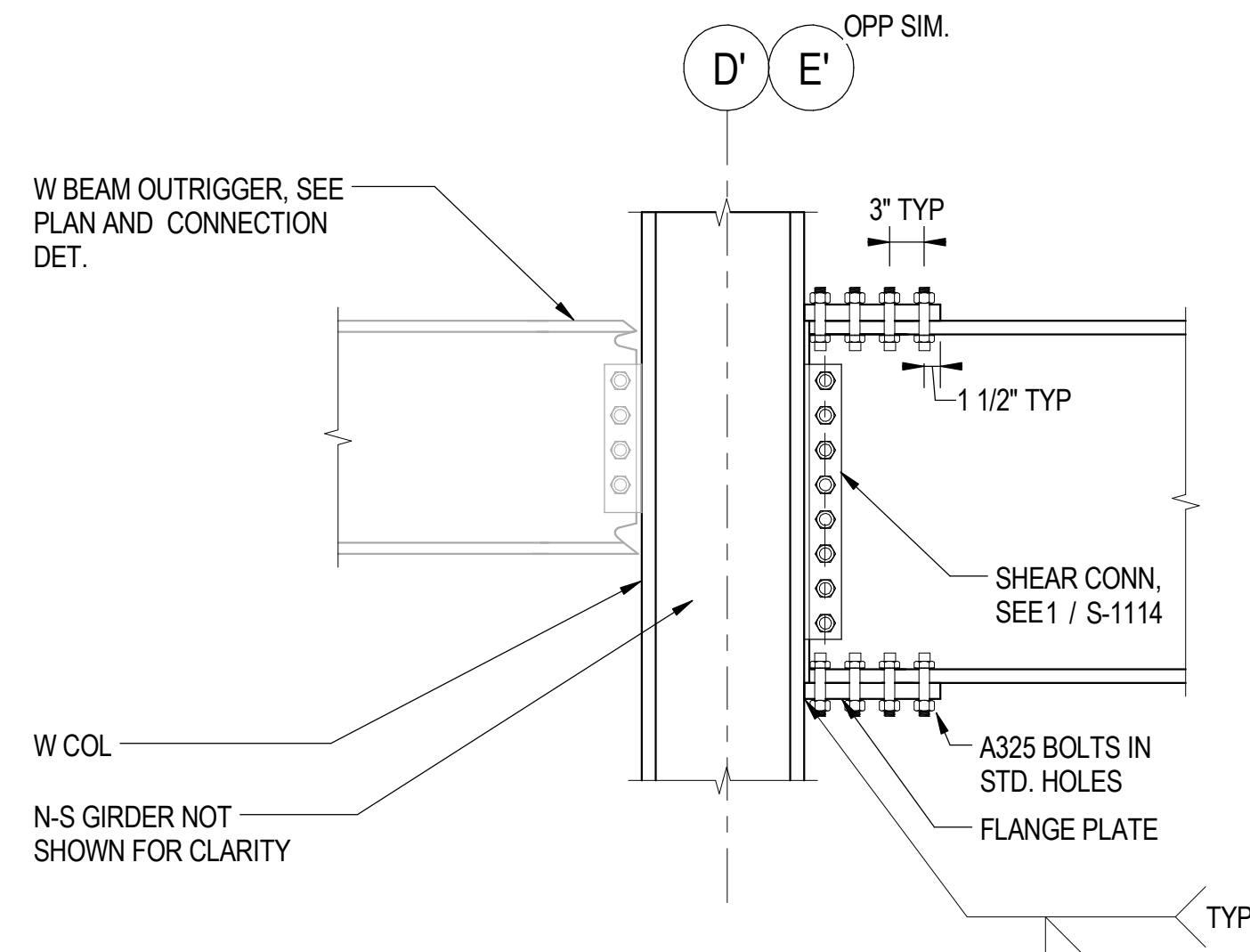
LOCATION IDENTIFIER: **DR095**

**TYPICAL DETAILS - STEEL - 2**

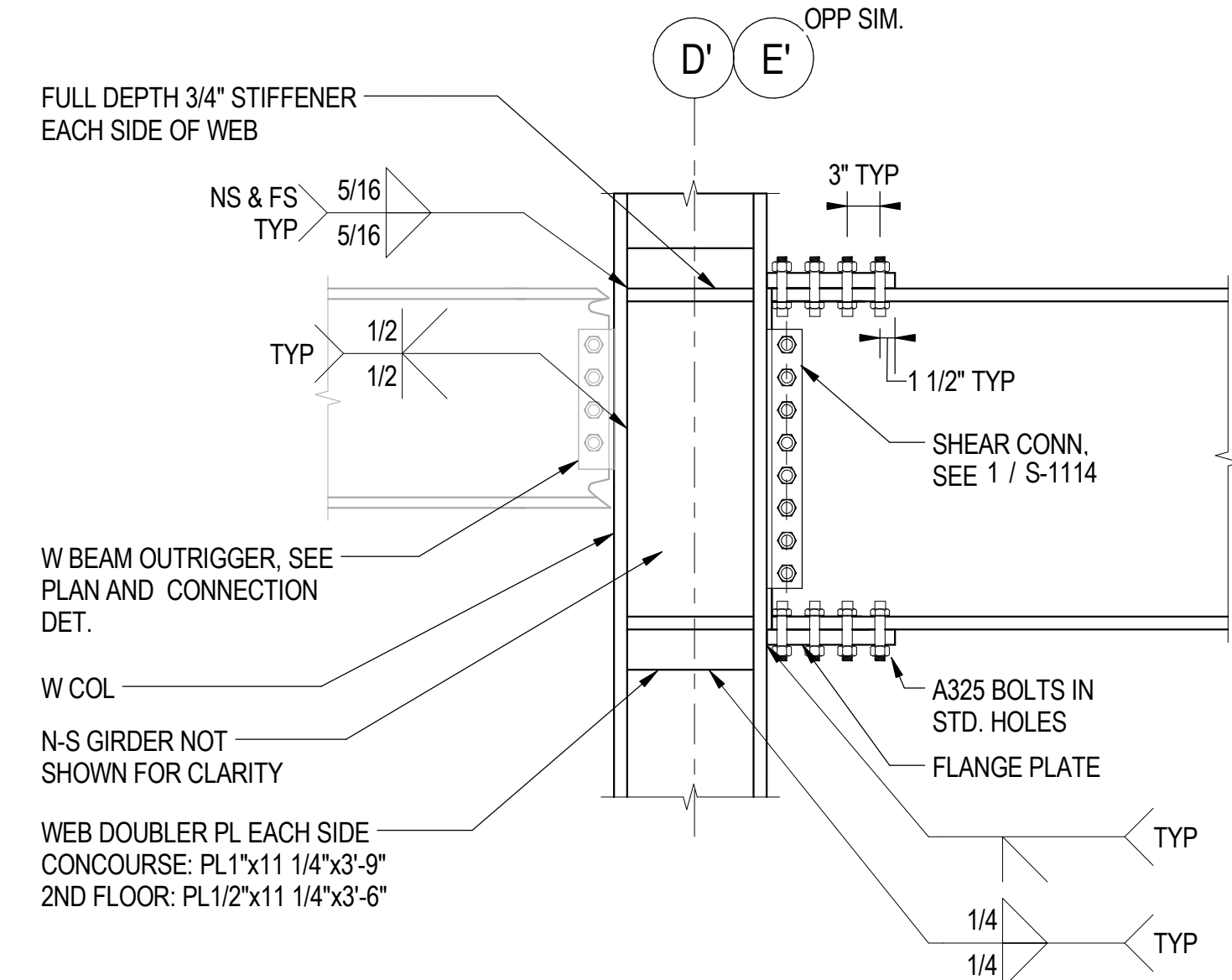
**S-1115**



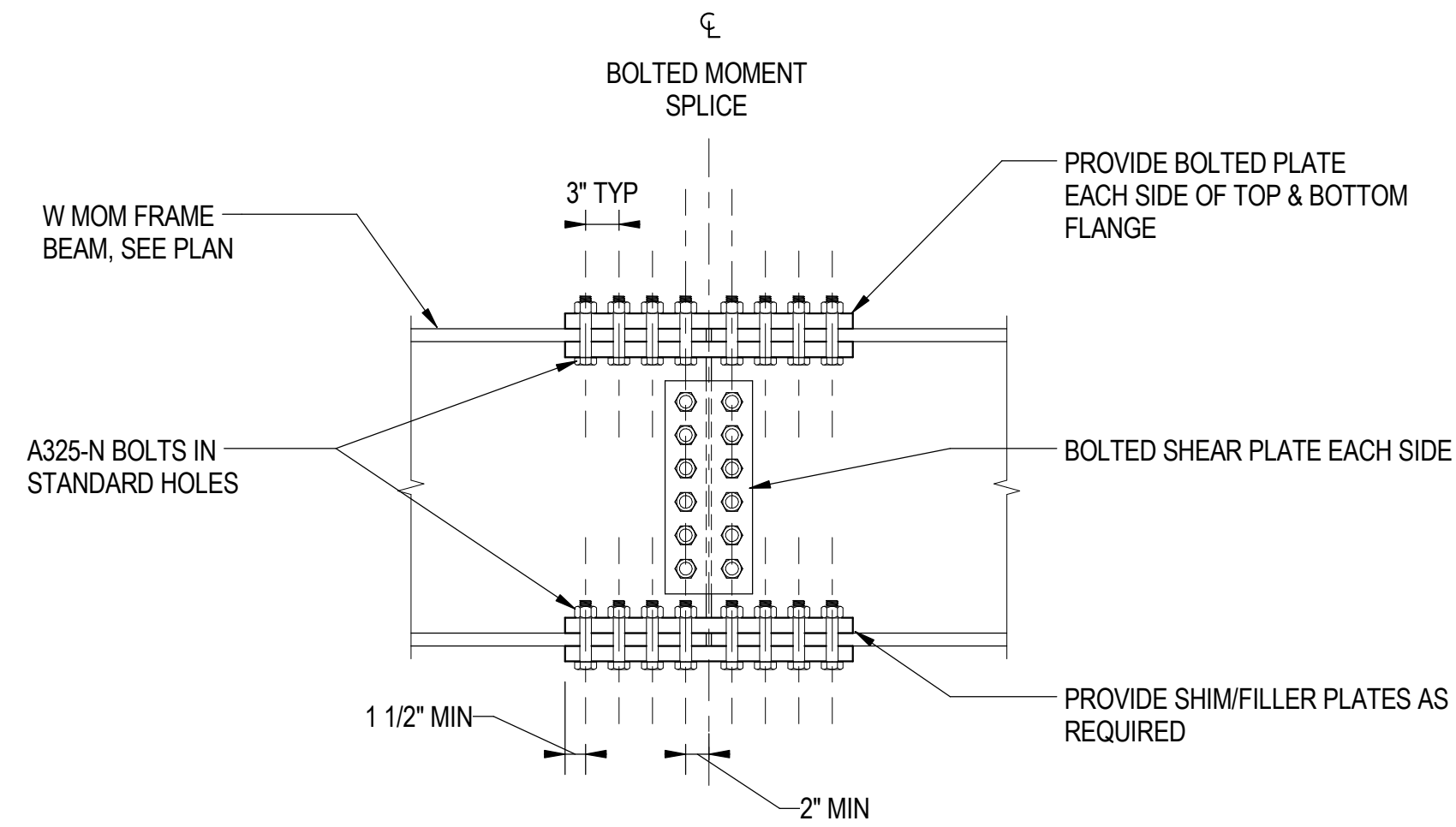
**1** MOMENT FRAME CONNECTION AT ROOF  
S-1116 SCALE: 3/4" = 1'-0"



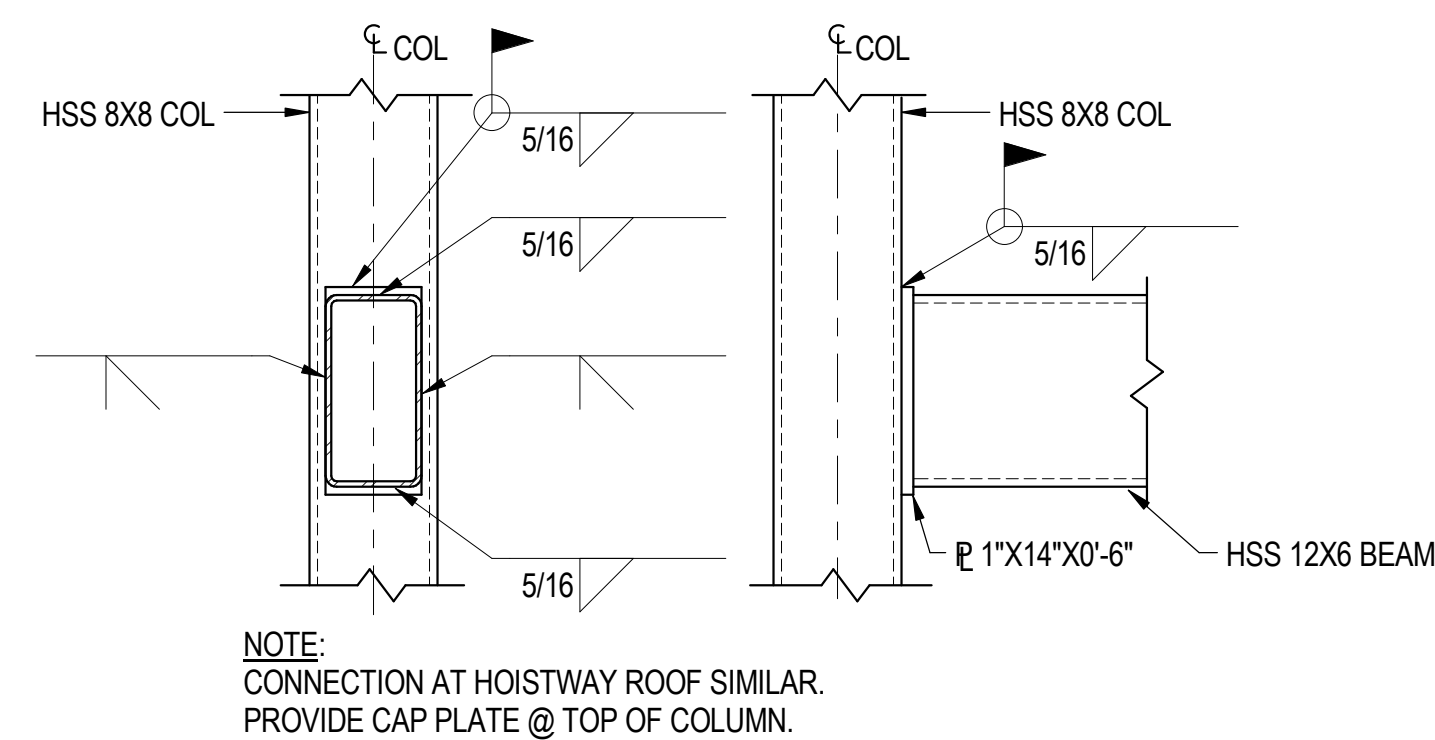
**2** MOMENT FRAME CONNECTION  
S-1116 SCALE: 3/4" = 1'-0"



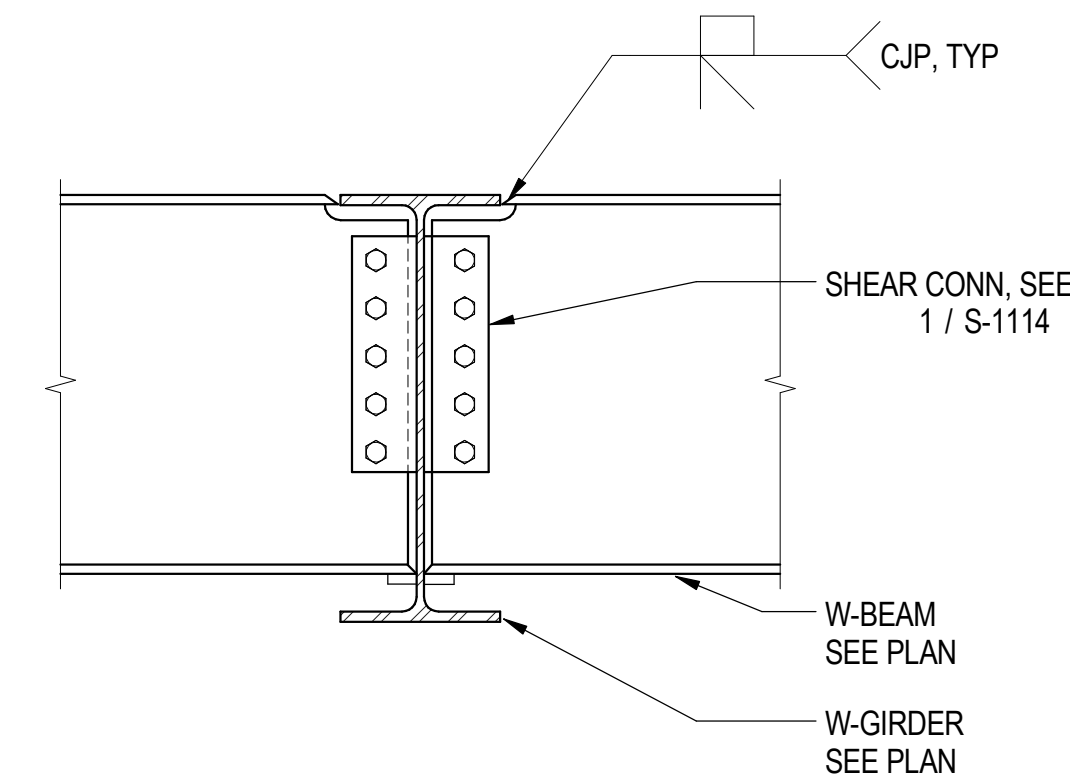
**3** STIFFENED MOMENT FRAME CONNECTION  
S-1116 SCALE: 3/4" = 1'-0"



**4** BOLTED MOMENT SPLICE DETAIL  
S-1116 SCALE: 3/4" = 1'-0"



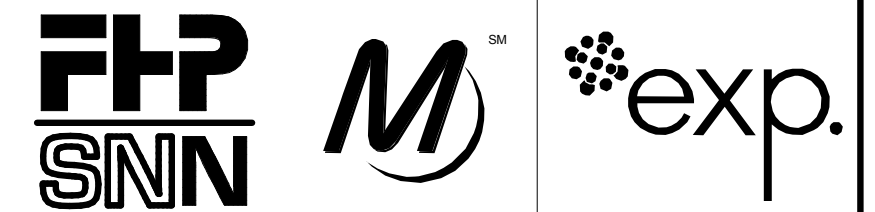
**5** HSS BEAM TO COLUMN MOMENT CONNECTION  
S-1116 SCALE: 1" = 1'-0"



**7** BEAM TO GIRDER MOMENT CONNECTION  
S-1116 SCALE: 1" = 1'-0"

MOMENT FRAME CONNECTION FORCE SCHEDULE		
MARK	MOMENT (K-FT)	DETAIL
1	200	1 / S-1116
2	600	2 / S-1116
3	850	3 / S-1116
4	1600	3 / S-1116
5	1300	2 / S-1116

NOTE:  
1. MOMENT CONNECTIONS SHALL BE DESIGNED BY A LICENSED SE IN THE STATE OF IL TO DEVELOP 100% OF THE INDICATED FORCES  
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15 West 95th Street (South Terminal)  
Chicago, IL 60628

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DRAWN BY	C. BOVE
PROJECT NO.	2012-0021.02
FILE NAME	DR095_S-Terminal

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LOCATION IDENTIFIER: DR095

TYPICAL MOMENT CONNECTION DETAILS - 1

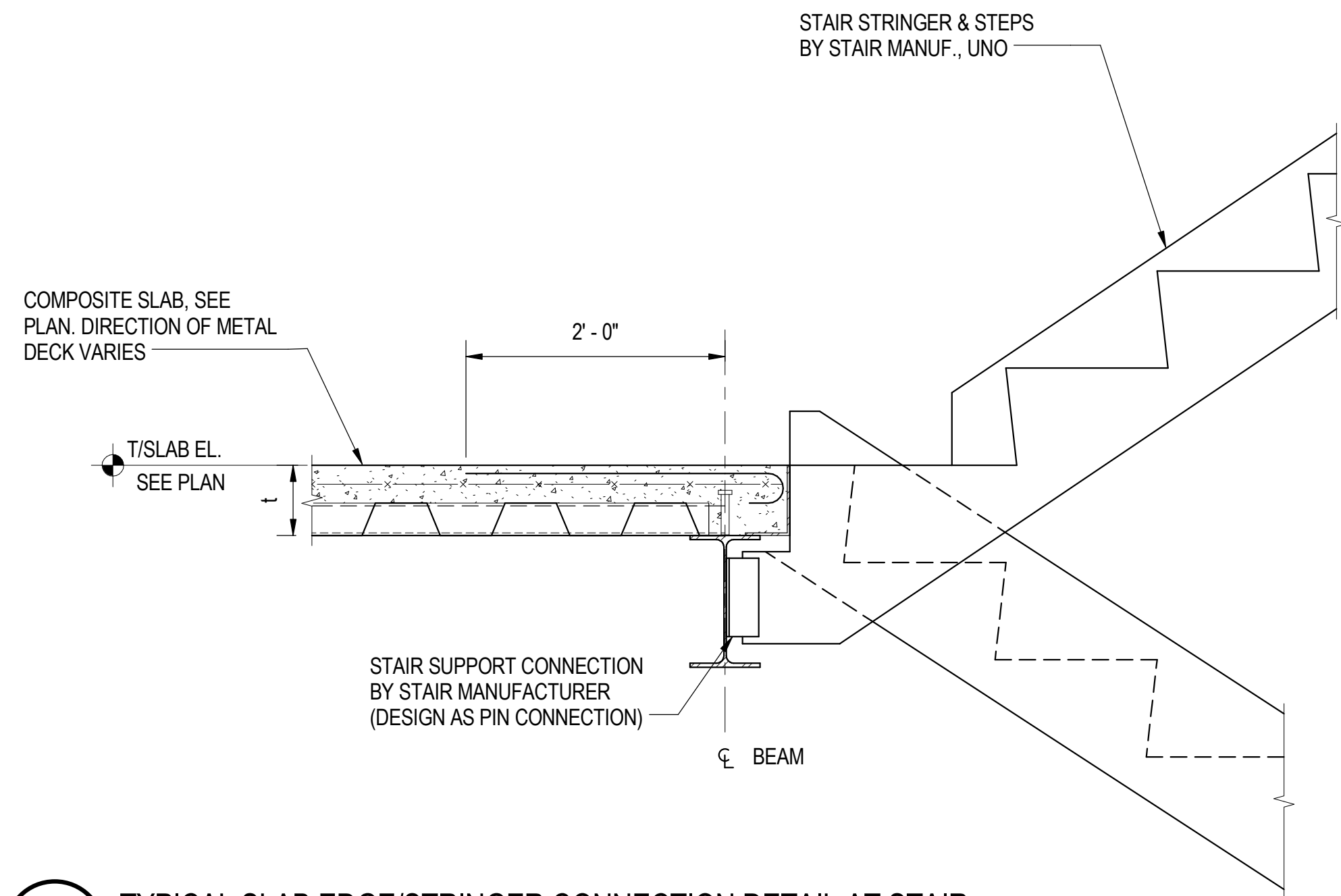
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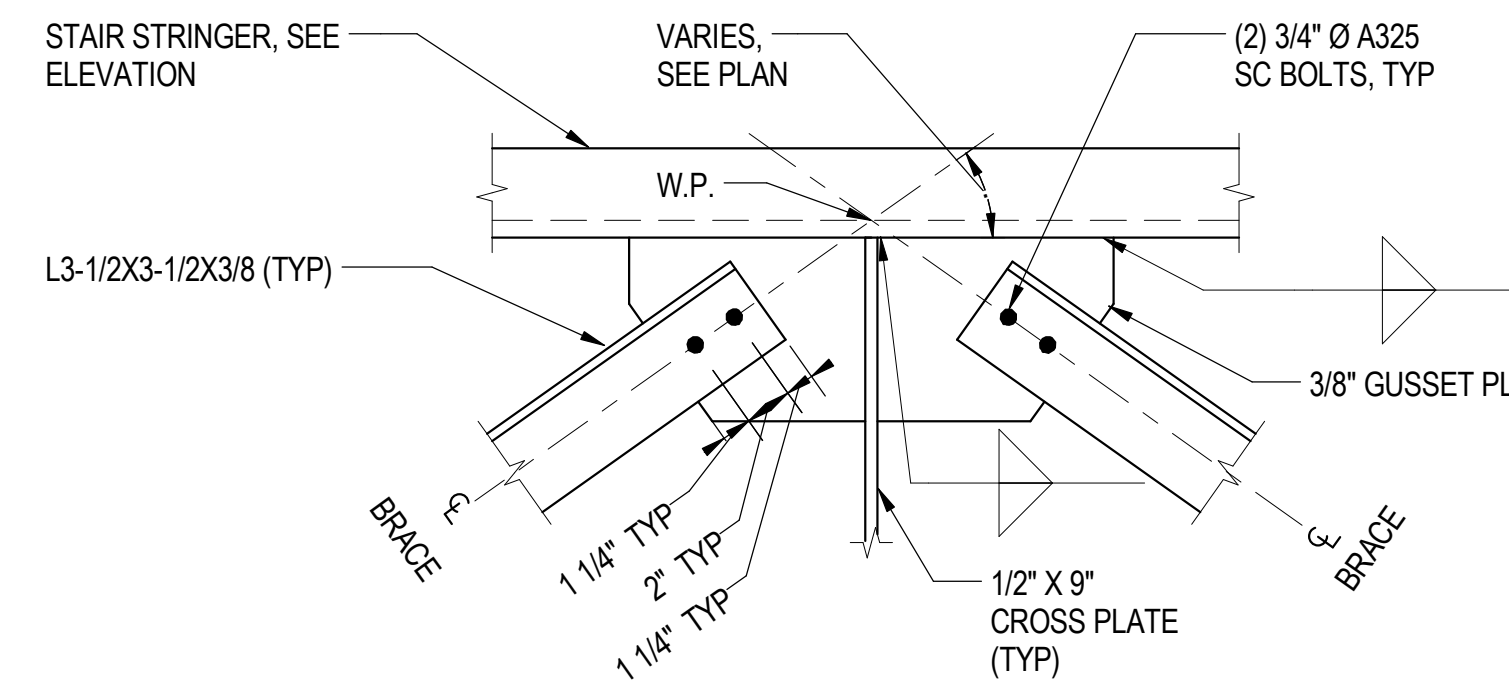
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30/11/2016 6:51:21 PM

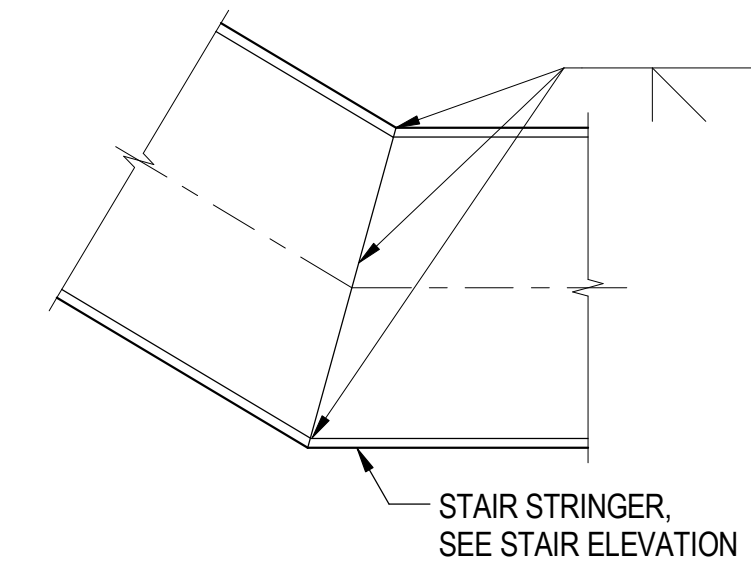
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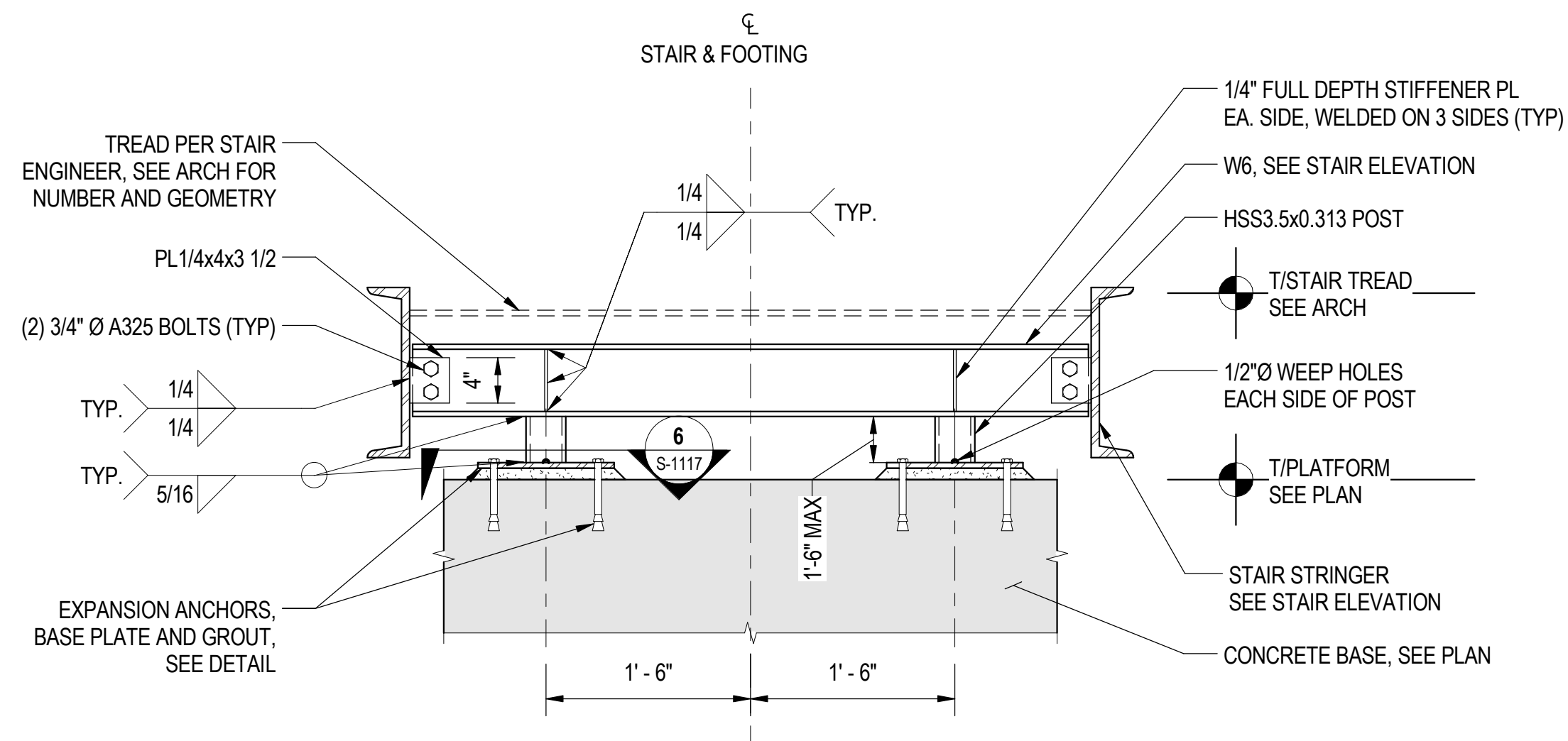
**1** TYPICAL SLAB EDGE/STRINGER CONNECTION DETAIL AT STAIR  
S-1117 SCALE: 1" = 1'-0"



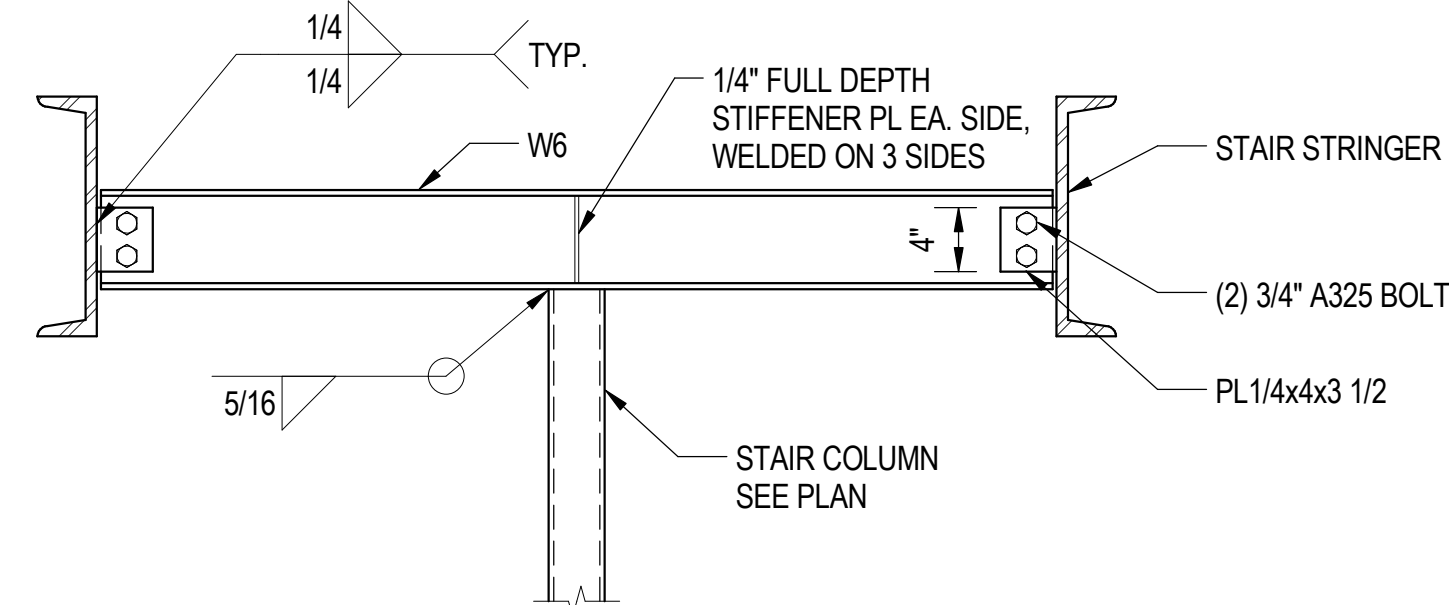
**2** TYPICAL STAIR BRACING CONNECTION  
S-1117 SCALE: 1 1/2" = 1'-0"



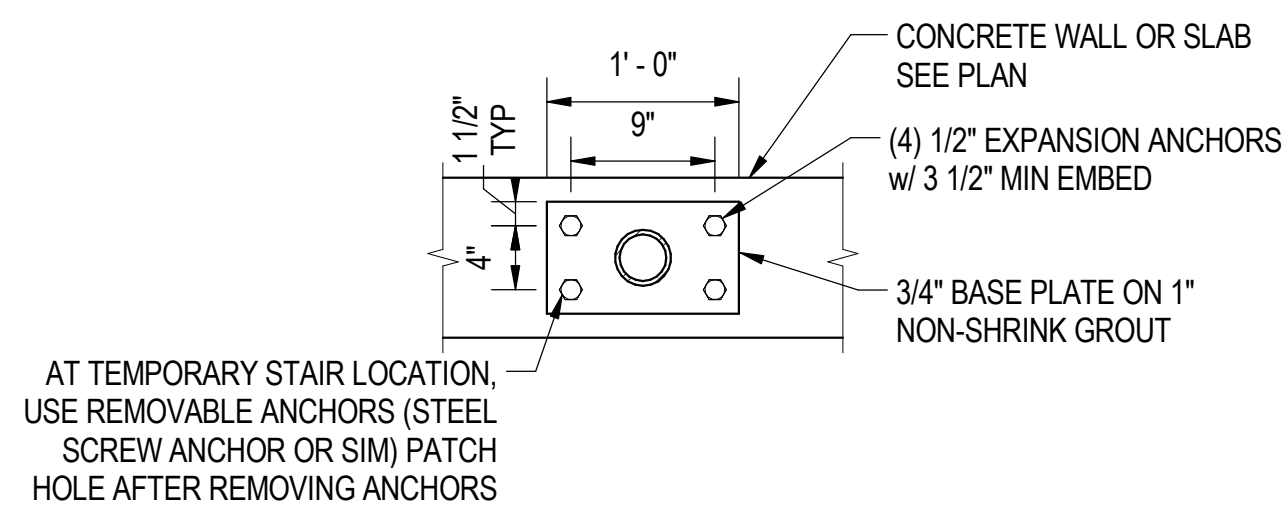
**3** TYPICAL STAIR STRINGER DOG LEG  
S-1117 SCALE: 1" = 1'-0"



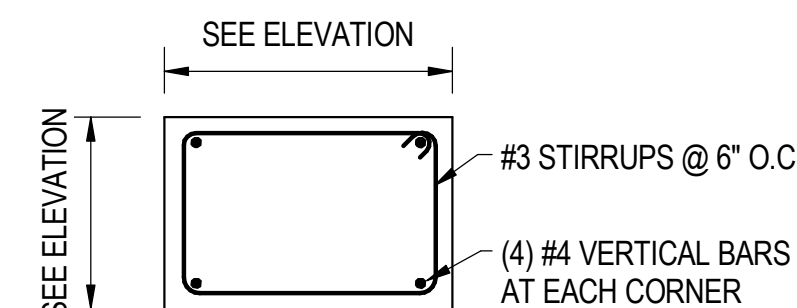
**4** STAIR BASE SUPPORT  
S-1117 SCALE: 1" = 1'-0"



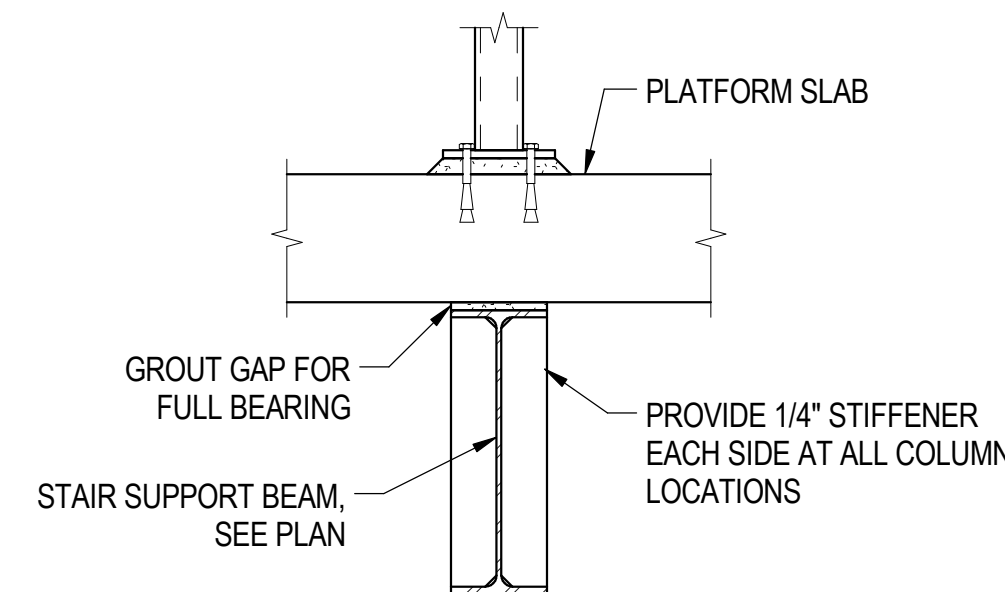
**5** SECTION AT COLUMN  
S-1117 SCALE: 1" = 1'-0"



**6** STAIR BASE PLATE DETAIL  
S-1117 SCALE: 1" = 1'-0"



**7** PEDESTAL DETAIL  
S-1117 SCALE: 1" = 1'-0"



**8** STAIR SUPPORT BEAM DETAIL  
S-1117 SCALE: 1" = 1'-0"



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15 West 95th Street (South Terminal)  
Chicago, IL 60628

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APPROVED BY	J. JAKALSKI
CHECKED BY	R.S. NAIR
DESIGNED BY	G. FORKAN
DRAWN BY	J. PEARSON
PROJECT NO.	2012-0021.02
FILE NAME	DR095_S-Terminal

MARK	DATE	DESCRIPTION
	2/9/17	ISSUED FOR CONSTRUCTION

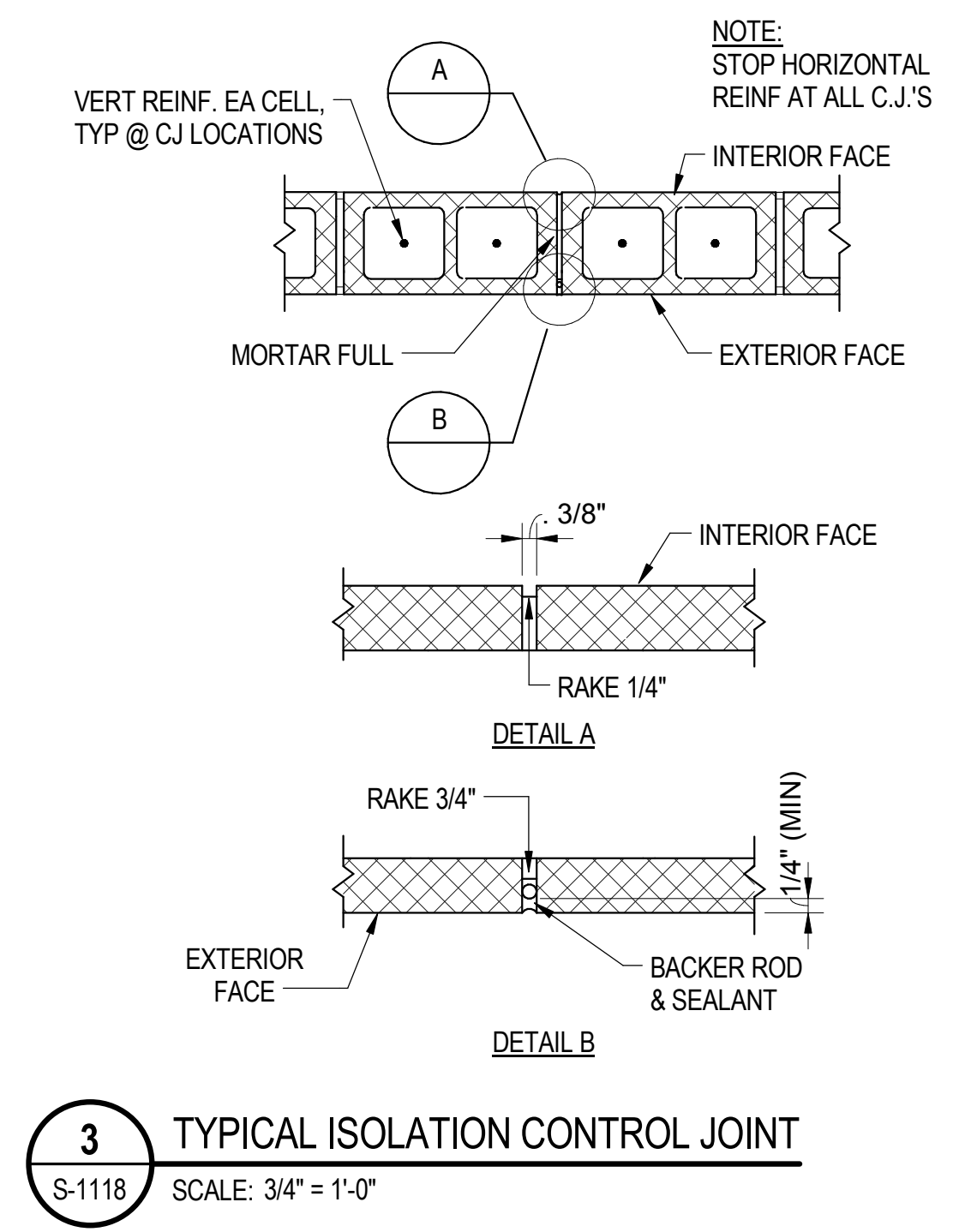
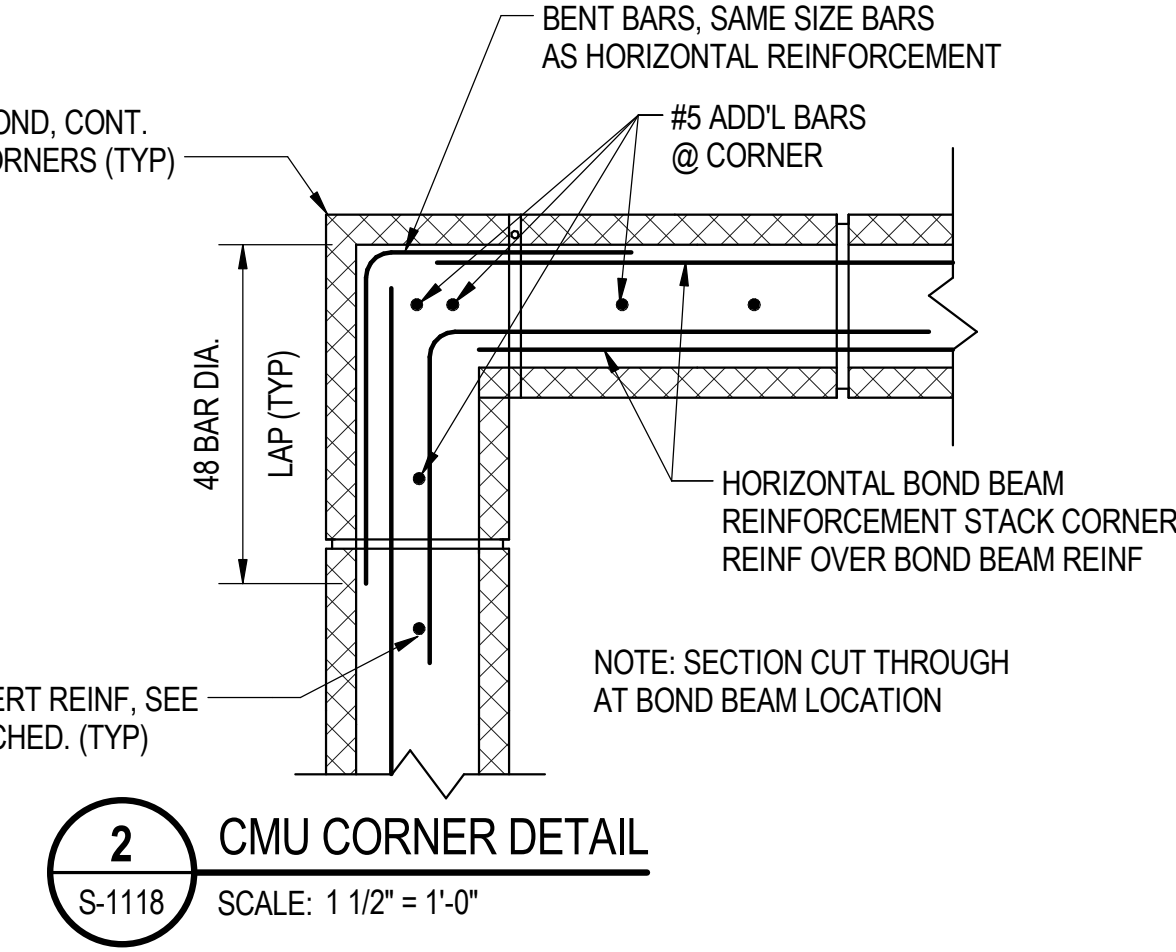
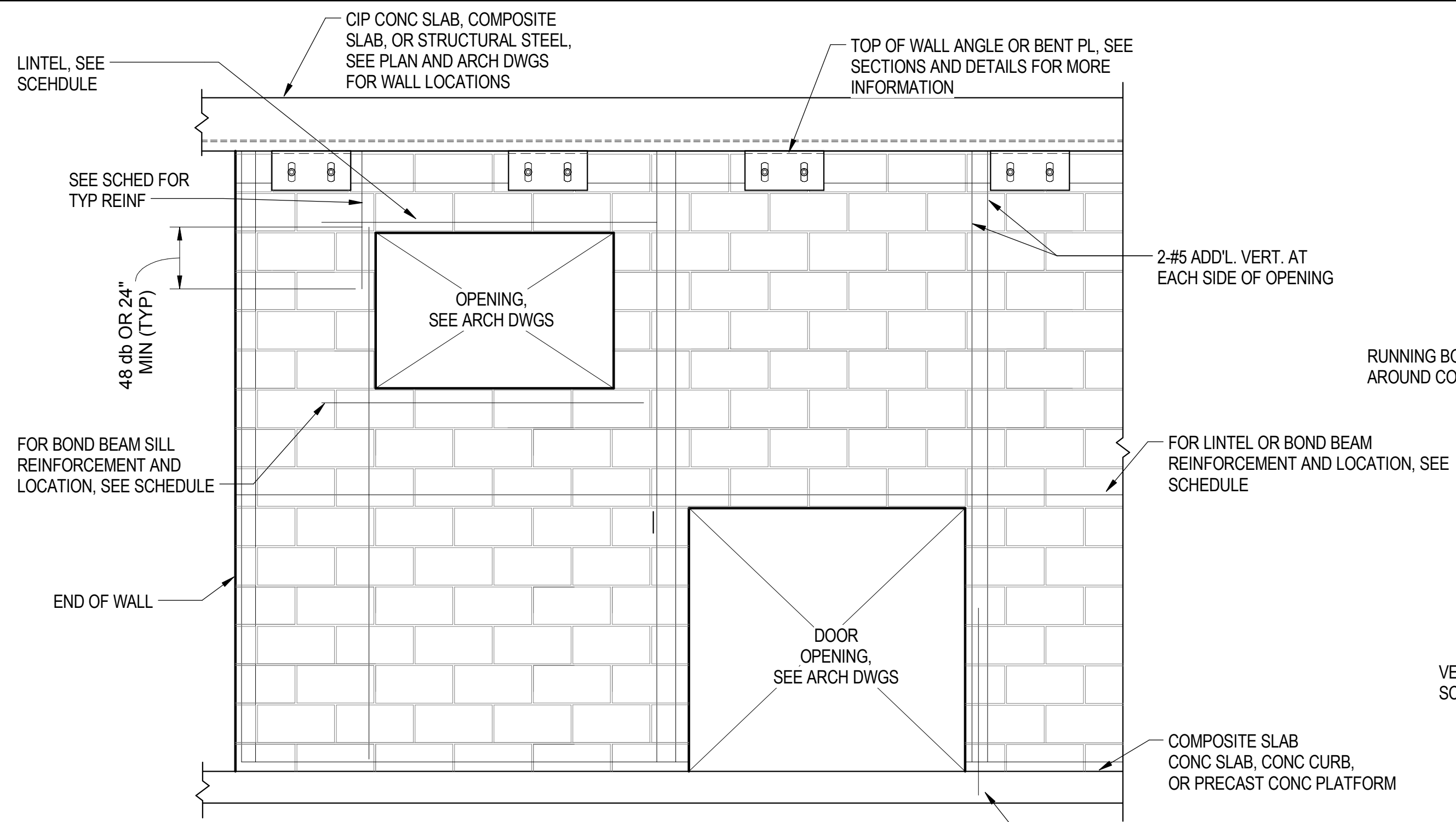
LOCATION IDENTIFIER: DR095

TYPICAL MISC STEEL DETAILS

S-1117

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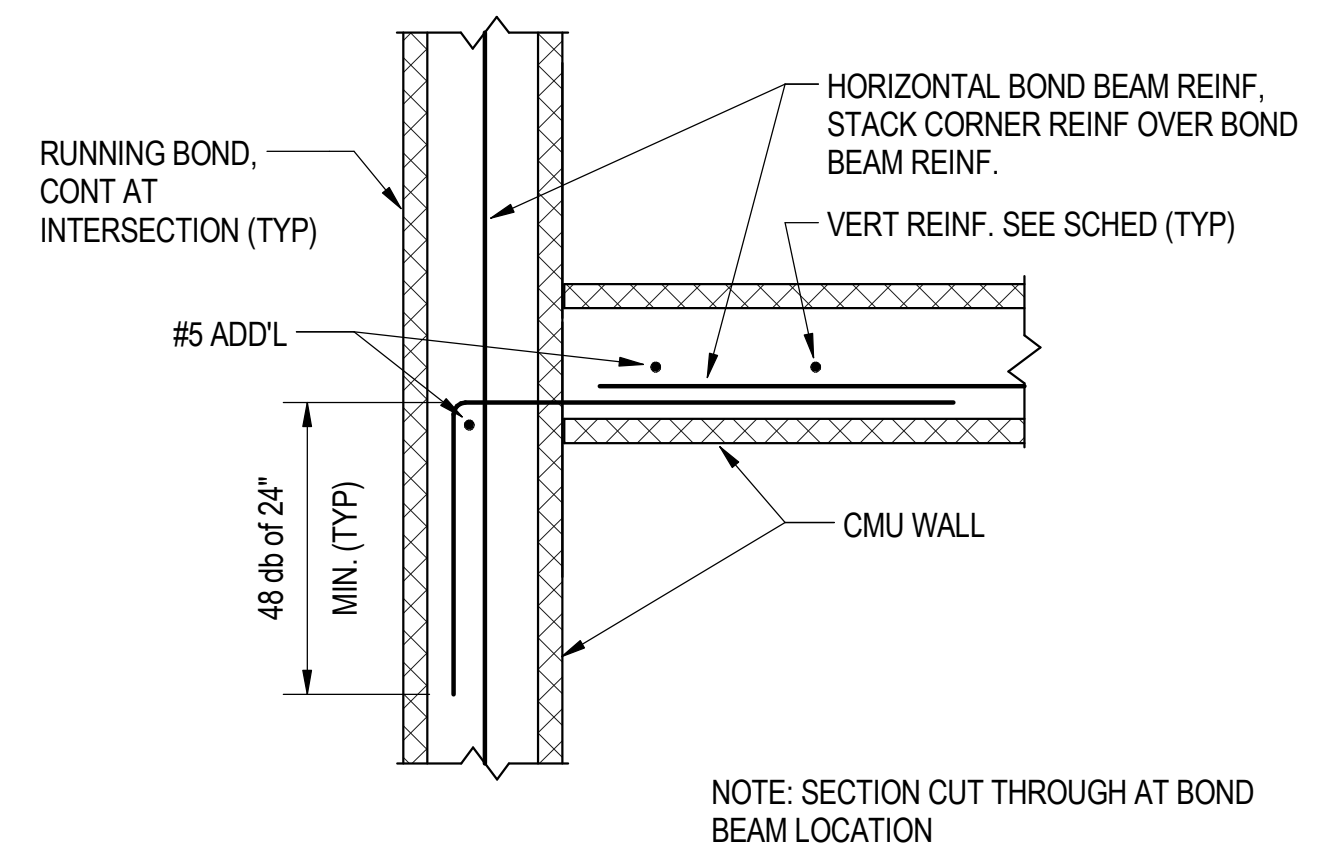
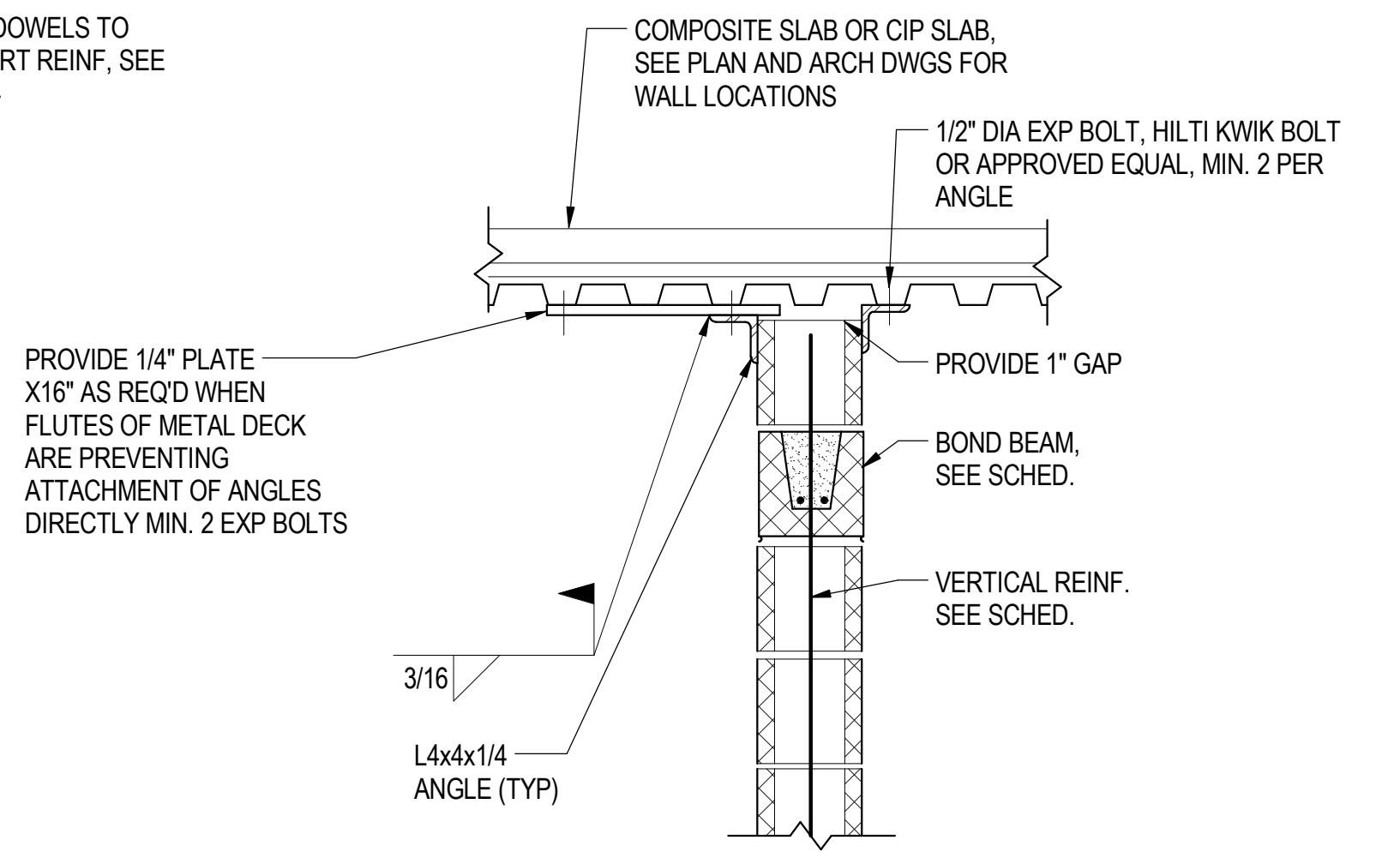
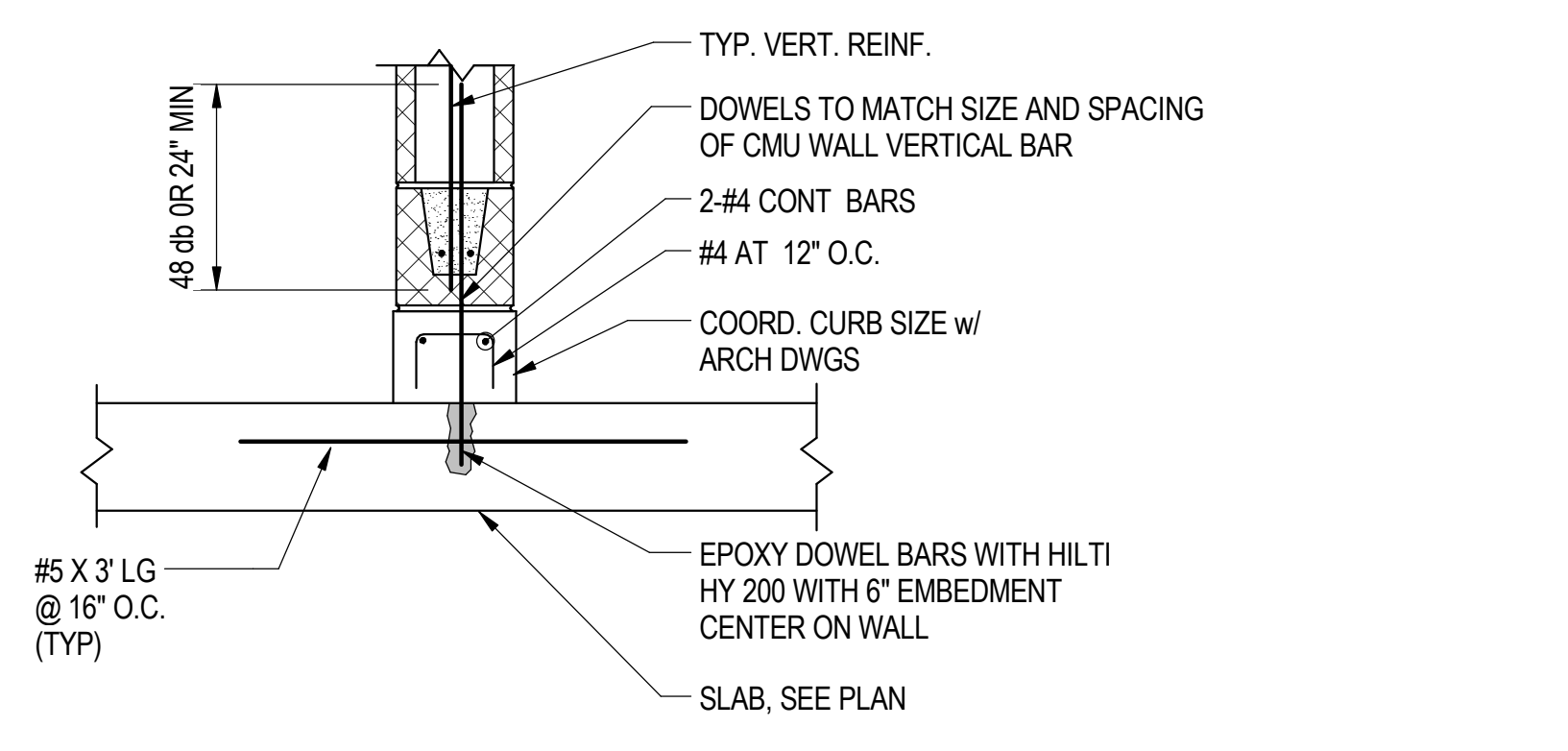
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1 TYPICAL MASONRY WALL ELEVATION  
S-1118 SCALE: 3/4" = 1'-0"

2 CMU CORNER DETAIL  
S-1118 SCALE: 1 1/2" = 1'-0"

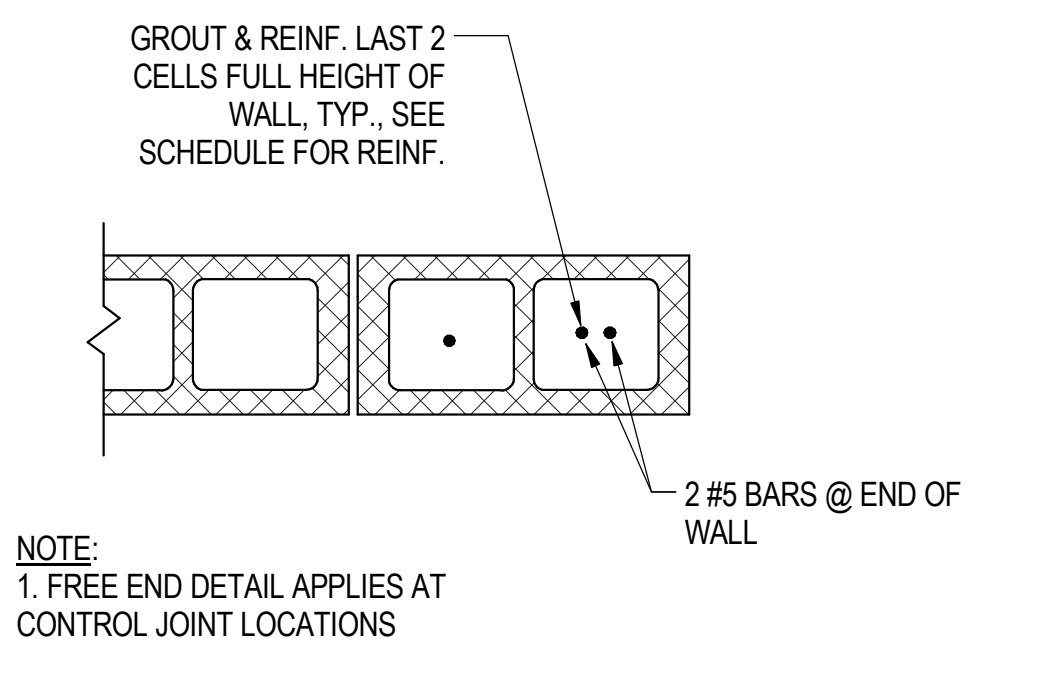
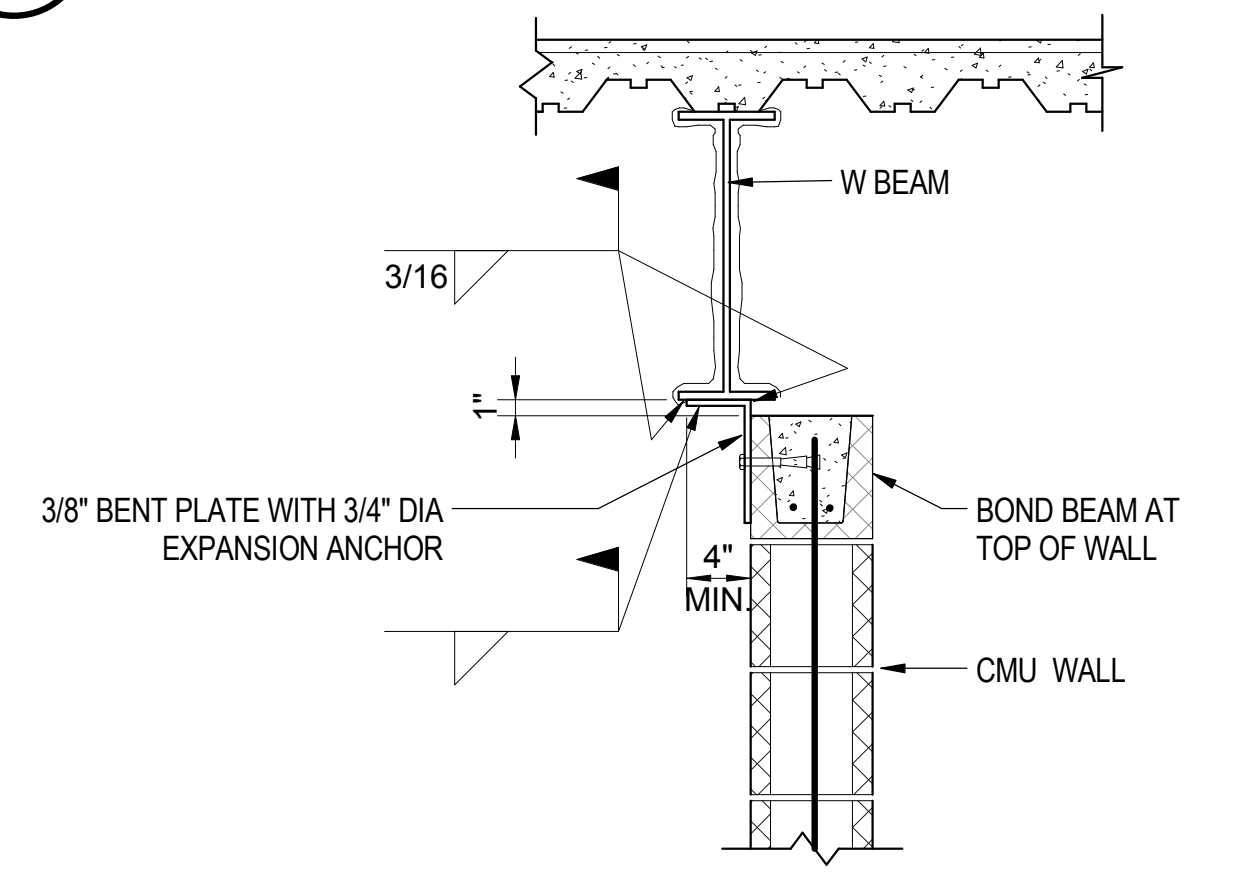
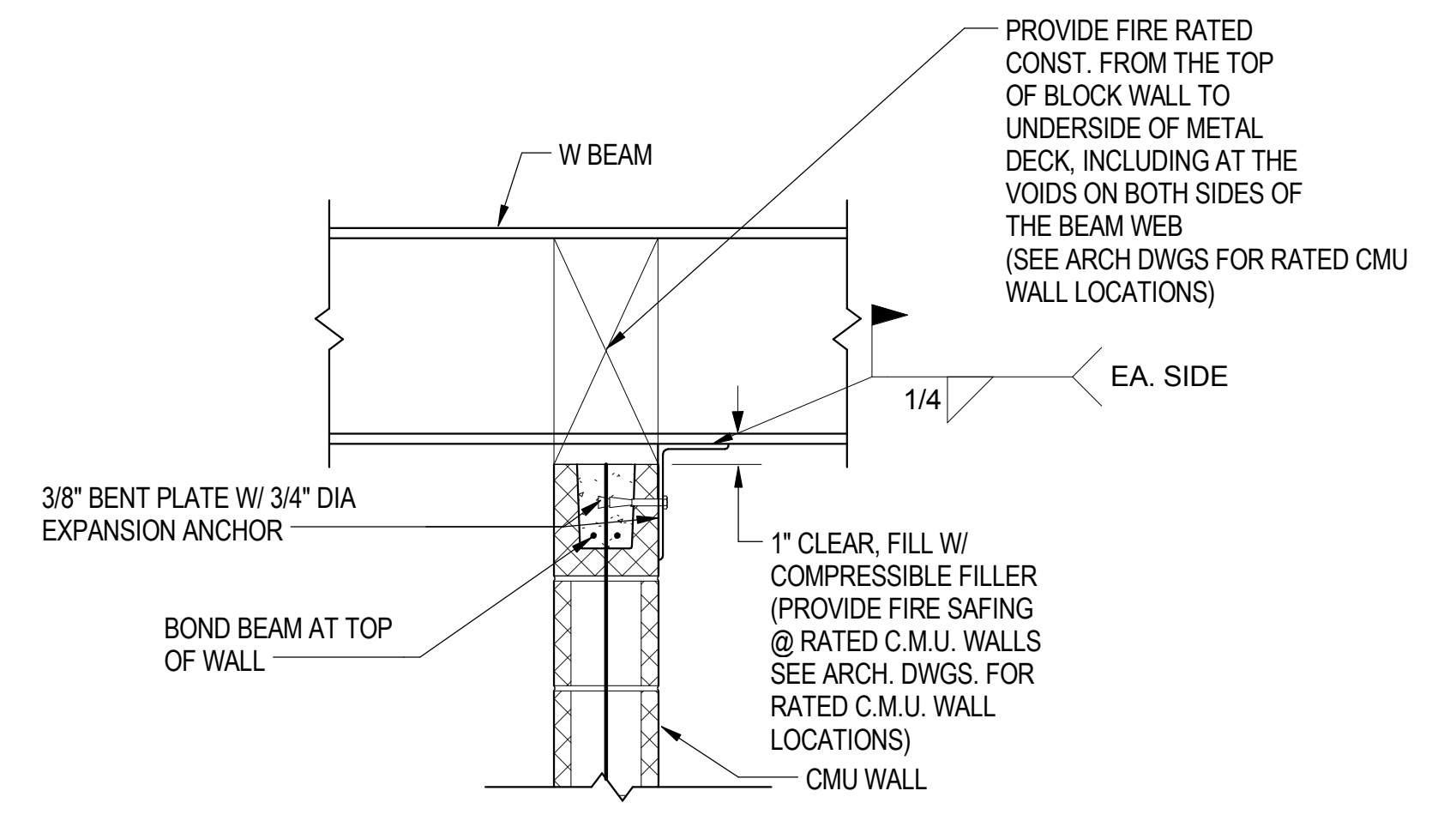
3 TYPICAL ISOLATION CONTROL JOINT  
S-1118 SCALE: 3/4" = 1'-0"



4 TYPICAL CMU WALL ANCHORAGE DETAIL  
S-1118 SCALE: 1" = 1'-0"

5 CMU BRACE TO SLAB ABOVE  
S-1118 SCALE: 1" = 1'-0"

6 TYPICAL CMU INTERSECTION DETAIL  
S-1118 SCALE: 1" = 1'-0"



7 CMU BRACING FOR CMU WALLS PERPENDICULAR TO BEAM  
S-1118 SCALE: 1" = 1'-0"

8 CMU BRACING FOR PARALLEL TO BEAM  
S-1118 SCALE: 1" = 1'-0"

9 FREE END DETAIL  
S-1118 SCALE: 1" = 1'-0"



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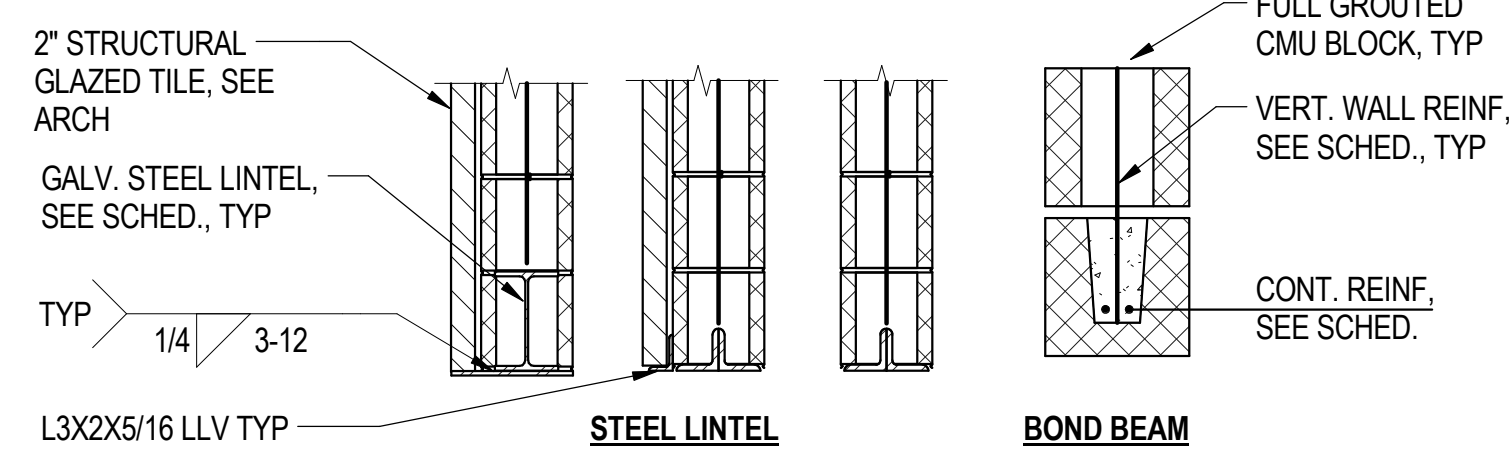
IN CHARGE	J. JAKALSKI
APPROVED BY	J. JAKALSKI
CHECKED BY	R.S. NAIR
DESIGNED BY	G. FORKAN
DRAWN BY	C. BOVE
PROJECT NO.	2012-0021.02
FILE NAME	DR095_S-Terminal

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LOCATION IDENTIFIER: **DR095**

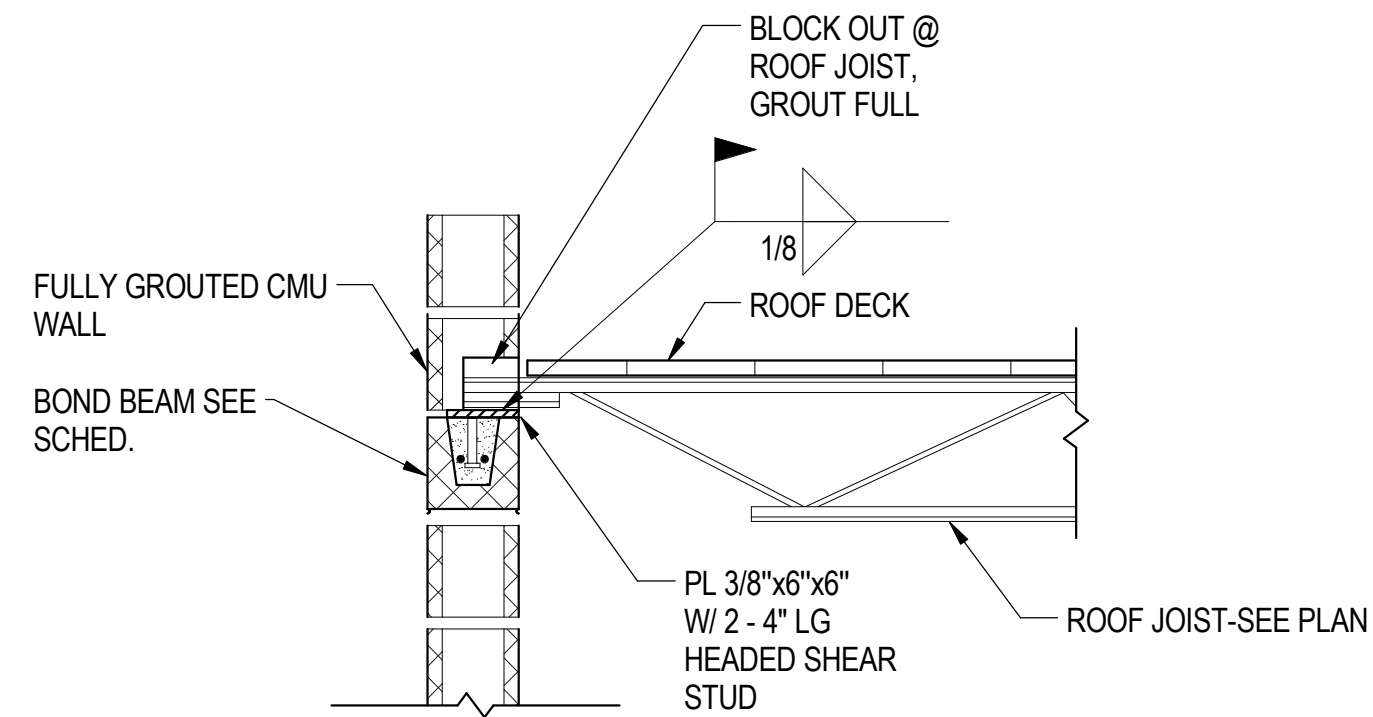
**TYPICAL DETAILS - CMU - 1**

**S-1118**

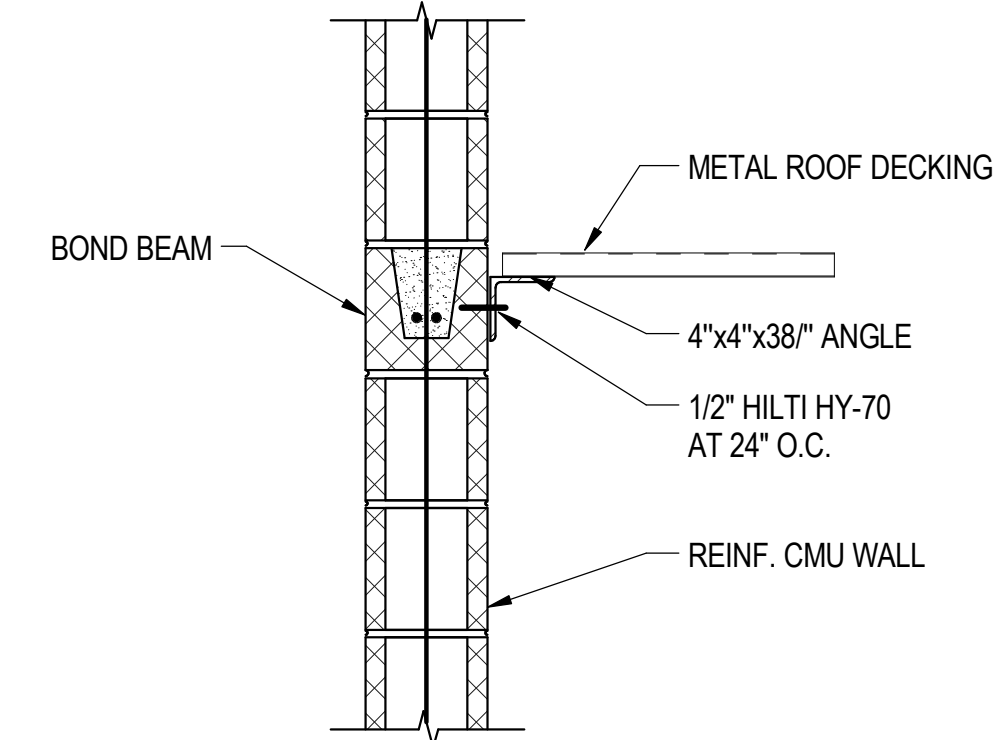


NOTE: LINTELS SHALL BEAR A MIN. OF 4" INTO CMU BLOCK AT EACH SIDE OF OPENING.

**1** CMU BOND BEAM AND LINTEL TYPICAL DETAIL  
S-1119 SCALE: 3/4" = 1'-0"



**2** BAR JOIST DETAIL  
S-1119 SCALE: 3/4" = 1'-0"



**3** TYP ANGLE CONNECTION DETAIL  
S-1119 SCALE: 1" = 1'-0"

TYPICAL EXTERIOR MASONRY WALL SCHEDULE		
THICKNESS	REINFORCEMENT	8" CMU
HEIGHT	VERTICAL	#5 @ 32"
UP TO 20'-0"	HORIZONTAL	9 GAUGE JOINT REINF @ 16" O.C., LADDER TYPE

TYPICAL INTERIOR MASONRY WALL SCHEDULE			
THICKNESS	REINFORCEMENT	6" CMU	8" CMU
HEIGHT	VERTICAL	#4 @ 48"	#4 @ 48"
UP TO 20'-0"	HORIZONTAL	9 GAUGE JOINT REINF @ 16" O.C., LADDER TYPE	9 GAUGE JOINT REINF @ 16" O.C., LADDER TYPE

6" CMU WALL LINTEL SCHEDULE	
MAXIMUM MASONRY OPENING	LINTEL
UP TO 4'-0"	TYP BOND BEAM OR 2- L3 1/2 x2 1/2 x5/16
4'-0" TO 8'-0"	2 - L5x3x5/16 LLV
8'-0" TO 12'-0"	W8x21 w/ 1/4" PL

8" CMU WALL LINTEL SCHEDULE	
MAXIMUM MASONRY OPENING	LINTEL
UP TO 4'-0"	TYP BOND BEAM OR 2- L3 1/2x3 1/2 x5/16
4'-0" TO 8'-0"	2 - L5x3 1/2x5/16 LLV
8'-0" TO 12'-0"	W8x21 w/ 1/4" PL

TYPICAL BOND BEAM SCHEDULE	
8"	6"
SINGLE BOND BEAM 2-#5 W/ 1 1/2" BOTTOM COVER	SINGLE BOND BEAM 1-#6 W/ 1 1/2" BOTTOM COVER

NOTE:  
1. BOND BEAMS SHALL BE LOCATED AT TOP AND BOTTOM OF WALLS, AT TOP AND BOTTOM OF OPENINGS AND AT 8' O.C.



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LOCATION IDENTIFIER: DR095

TYPICAL DETAILS - CMU - 2

S-1119

07/10/2016 3:14:16 PM

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vscarp

NORTH EDGE OF APPROACH

Location	Station	Offset	Theoretical Grade Elevations
Northwest End W. Appr. Pav't.	20+71.38	+80.60	16.74
A1	20+81.38	+80.60	16.90
North F.F. W. Appr. Sidewalk	20+88.31	+80.60	17.67
Construction Joint	20+91.38	+80.60	17.67
A2	21+01.38	+80.60	17.69
Northeast End W. Appr. Pav't.	21+05.47	+80.60	17.70
Northwest End E. Appr. Pav't.	23+01.60	+77.40	17.62
A3	23+05.68	+77.40	17.61
Construction Joint	23+15.68	+77.40	17.58
North F.F. E. Appr. Sidewalk	23+16.64	+77.40	17.58
A4	23+25.68	+77.40	16.85
Northeast End E. Appr. Pav't.	23+35.68	+77.40	16.68

NORTH CONSTRUCTION JOINT

Location	Station	Offset	Theoretical Grade Elevations
Northwest End W. Appr. Pav't.	20+71.38	+45.04	16.25
A1	20+81.38	+45.04	16.50
Construction Joint	20+91.38	+45.04	16.75
A2	21+01.38	+45.04	17.00
Northeast End W. Appr. Pav't.	21+06.97	+45.04	17.14
Northwest End E. Appr. Pav't.	23+00.10	+45.04	17.14
A3	23+05.68	+45.04	17.00
Construction Joint	23+15.68	+45.04	16.75
A4	23+25.68	+45.04	16.50
Northeast End E. Appr. Pav't.	23+35.68	+45.04	16.25

SOUTH CONSTRUCTION JOINT

Location	Station	Offset	Theoretical Grade Elevations
Southwest End W. Appr. Pav't.	20+71.38	-56.04	16.12
A5	20+81.38	-56.04	16.42
Construction Joint	20+91.38	-56.04	16.71
A6	21+01.38	-56.04	17.00
Southeast End W. Appr. Pav't.	21+06.97	-56.04	17.17
Southwest End E. Appr. Pav't.	23+00.10	-56.04	17.17
A7	23+05.68	-56.04	17.00
Construction Joint	23+15.68	-56.04	16.69
A8	23+25.68	-56.04	16.38
Southeast End E. Appr. Pav't.	23+35.68	-56.04	16.08

SOUTH EDGE OF APPROACH

Location	Station	Offset	Theoretical Grade Elevations
Southwest End W. Appr. Pav't.	20+71.38	-102.69	16.53
A5	20+81.38	-102.69	16.67
Construction Joint	20+91.38	-102.69	16.82
South F.F. W. Appr. Sidewalk	20+92.68	-102.69	17.51
A6	21+01.38	-102.69	17.59
Southeast End W. Appr. Pav't.	21+04.11	-102.69	17.61
Southwest End E. Appr. Pav't.	23+03.06	-105.89	17.94
A7	23+05.68	-105.89	17.90
South F.F. E. Appr. Sidewalk	23+14.90	-105.89	17.76
Construction Joint	23+15.68	-105.89	17.07
A8	23+25.68	-105.89	16.82
Southeast End E. Appr. Pav't.	23+35.68	-105.89	16.57

NOTE:

1. All offsets are to centerline.

FILE NAME = 070\_DR095\_S-112.dgn  
PLOT DATE = 8/31/2016



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**95th Terminal Improvement**  
 15 West 95th Street (South Terminal)  
 Chicago, IL 60628

IN CHARGE	S. KADERBEK
APPROVED BY	D.ZROKA
CHECKED BY	P.MAID
DESIGNED BY	L.SOMMER
DRAWN BY	T.SLEDGE
PROJECT NO.	2012-0021
FILE NAME	File Name

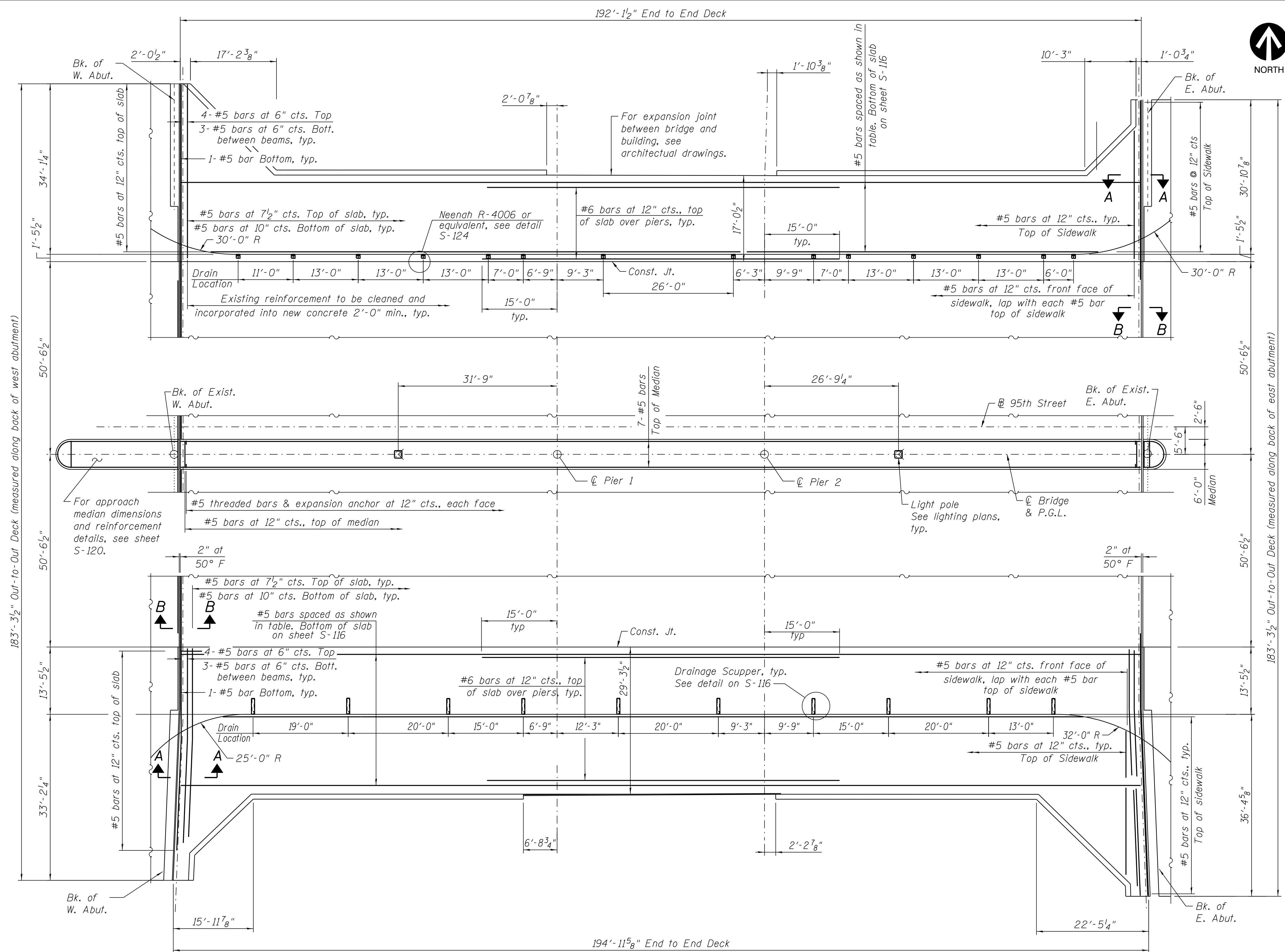
MARK	DATE	DESCRIPTION
	2/9/17	ISSUED FOR CONSTRUCTION

LOCATION IDENTIFIER: **DR095**

95TH ST. BRIDGE  
 TOP OF APPROACH SLAB ELEVATIONS  
 (SHEET 2 OF 2)  
 SN 016-0437

**95TH-S-112**

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**PLAN**

Notes:  
 Top and longitudinal bars should not be lapped at the same locations in the deck,  
 nor shall the top and bottom transverse bars be lapped at the same location.

For additional notes, typical cross section, Sections A-A and B-B, see sheet 95TH-S-115.

FILE NAME = 070\_DR095\_S-114.dgn  
 PLOT DATE = 8/31/2016



**CHICAGO TRANSIT AUTHORITY ENGINEERING**

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 Chicago, IL 60628

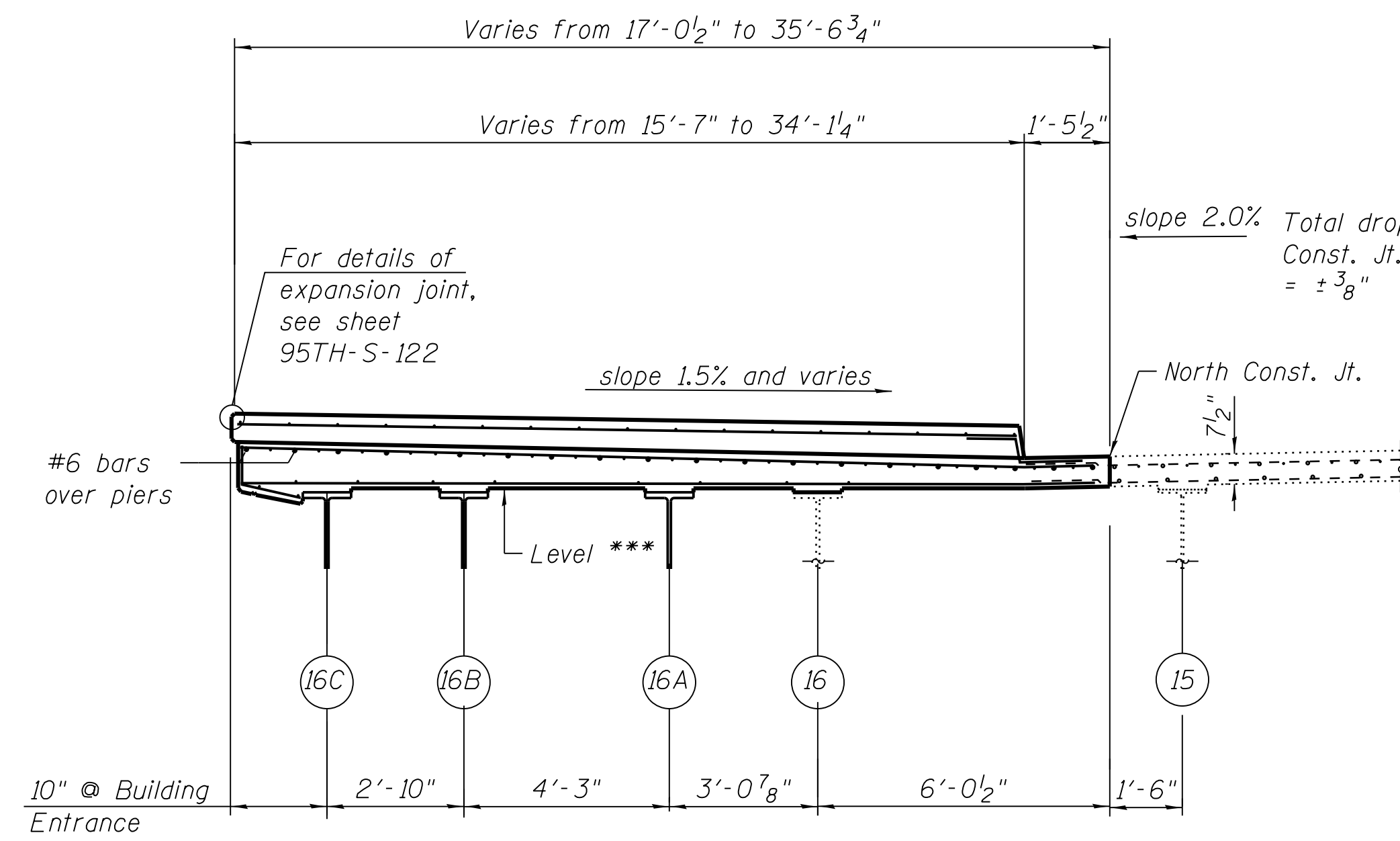
IN CHARGE	S. KADERBEK
APPROVED BY	D.ZROKA
CHECKED BY	P.MAID
DESIGNED BY	L.SOMMER
DRAWN BY	T.SLEDGE
PROJECT NO.	2012-0021
FILE NAME	File Name

MARK	DATE	DESCRIPTION
	2/9/17	ISSUED FOR CONSTRUCTION

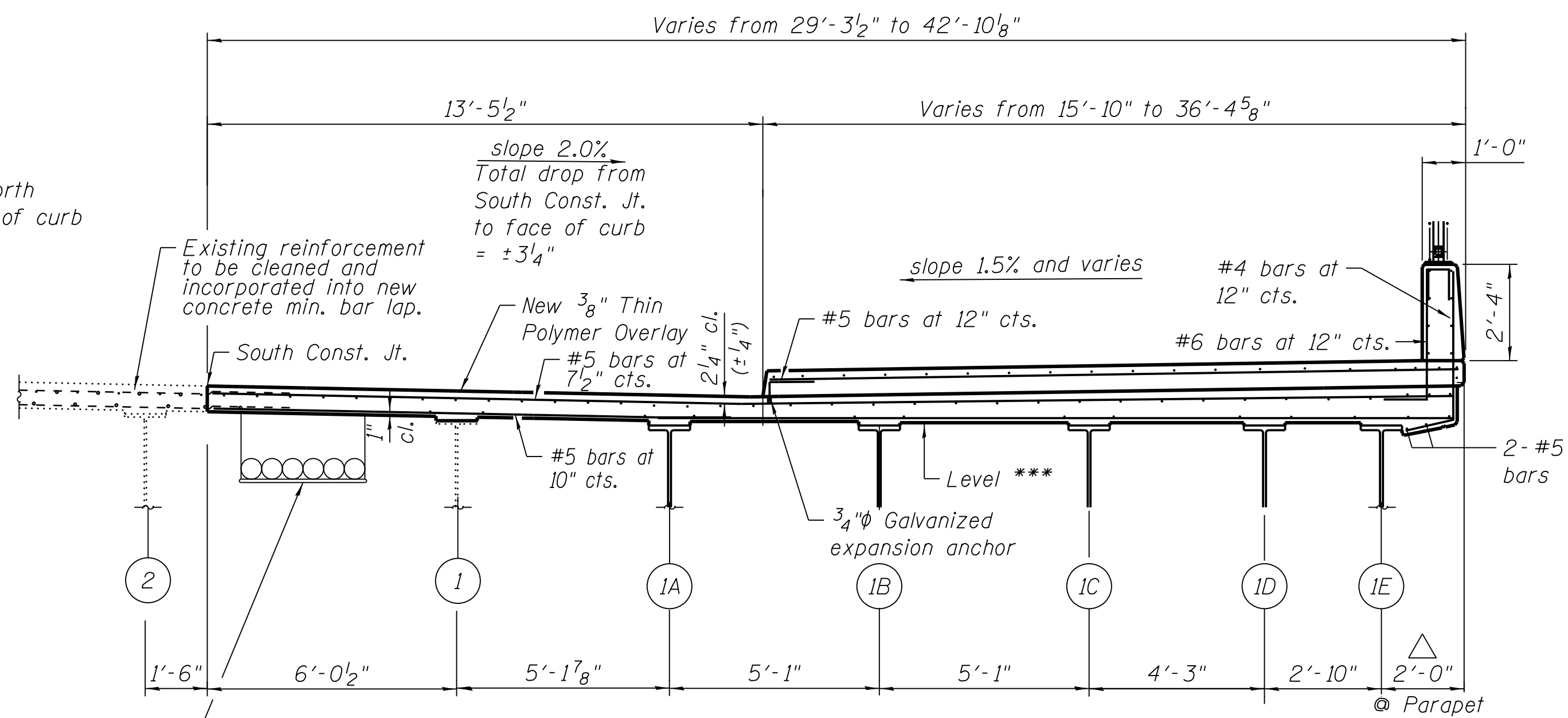
LOCATION IDENTIFIER: **DR095**  
 95TH ST. BRIDGE  
 SUPERSTRUCTURE  
 SN 016-0437

**95TH-S-114**

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NEAR PIER



NEAR MIDSPAN

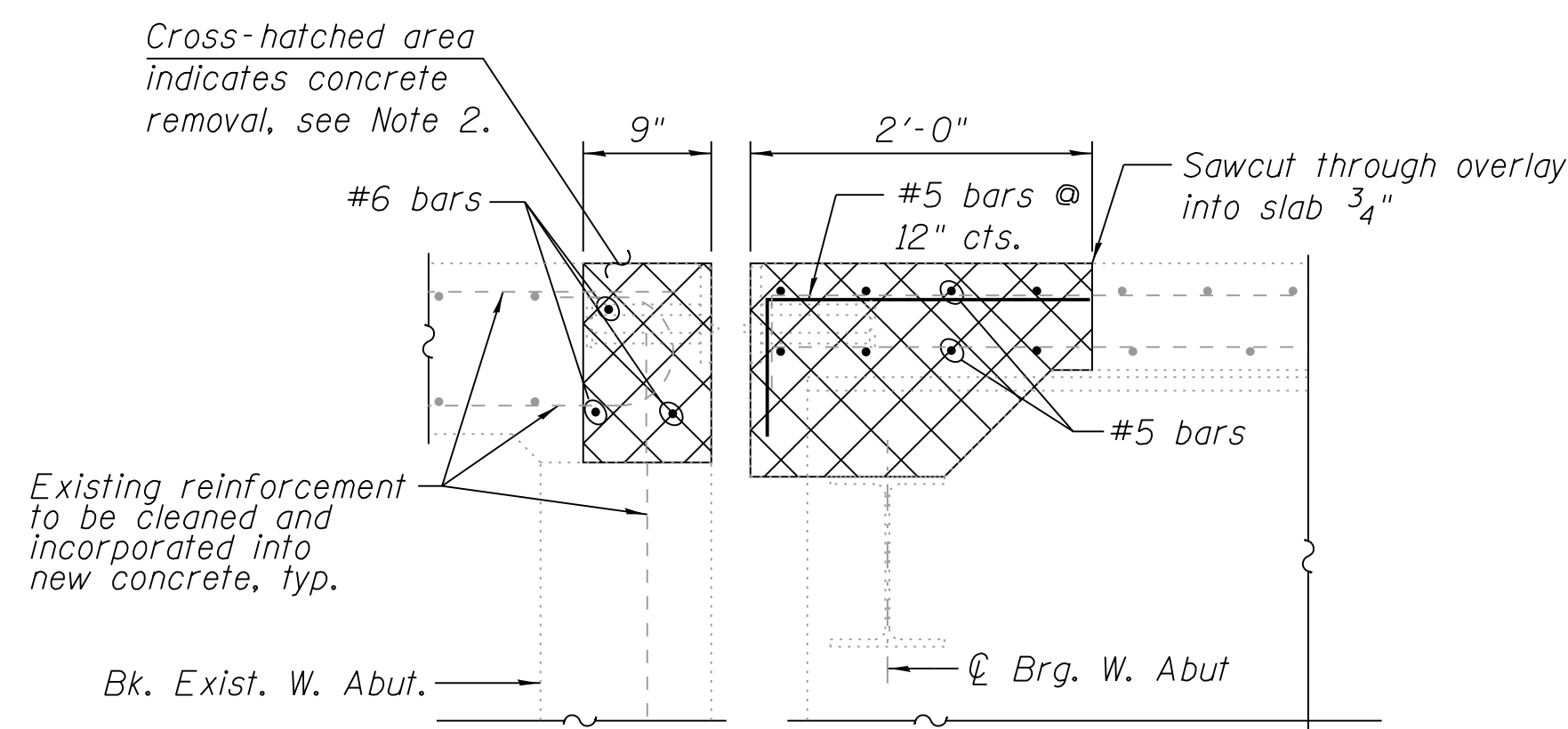
\*\*\* Bottom slab level between beams under sidewalk.  
 △ 2'-6" @ beam 1K

City of Chicago Electric Ducts, see Signal Plans.

DECK CROSS SECTION

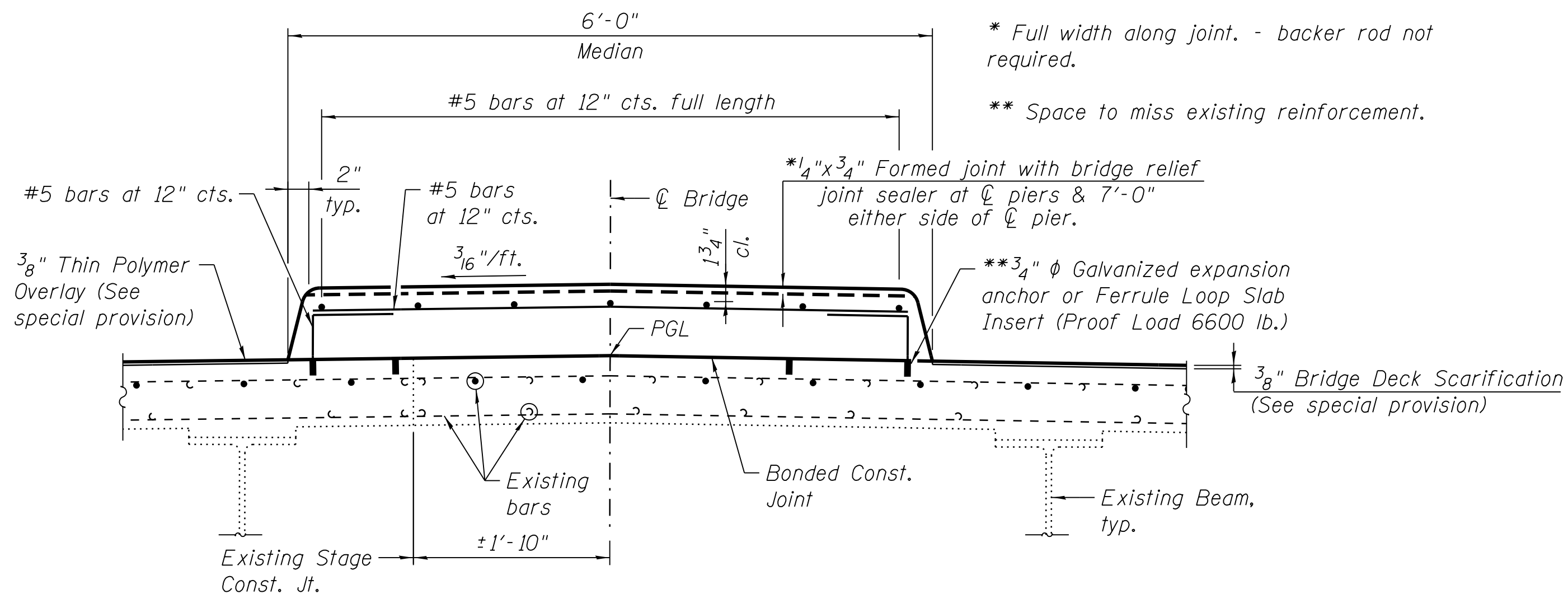
Note: See Framing Plan S-125 For Beam Locations at corners of Bridge

MINIMUM BAR LAPS	
BARS	LAPS
#4	2'-7"
#5	3'-3"
#6	3'-10"

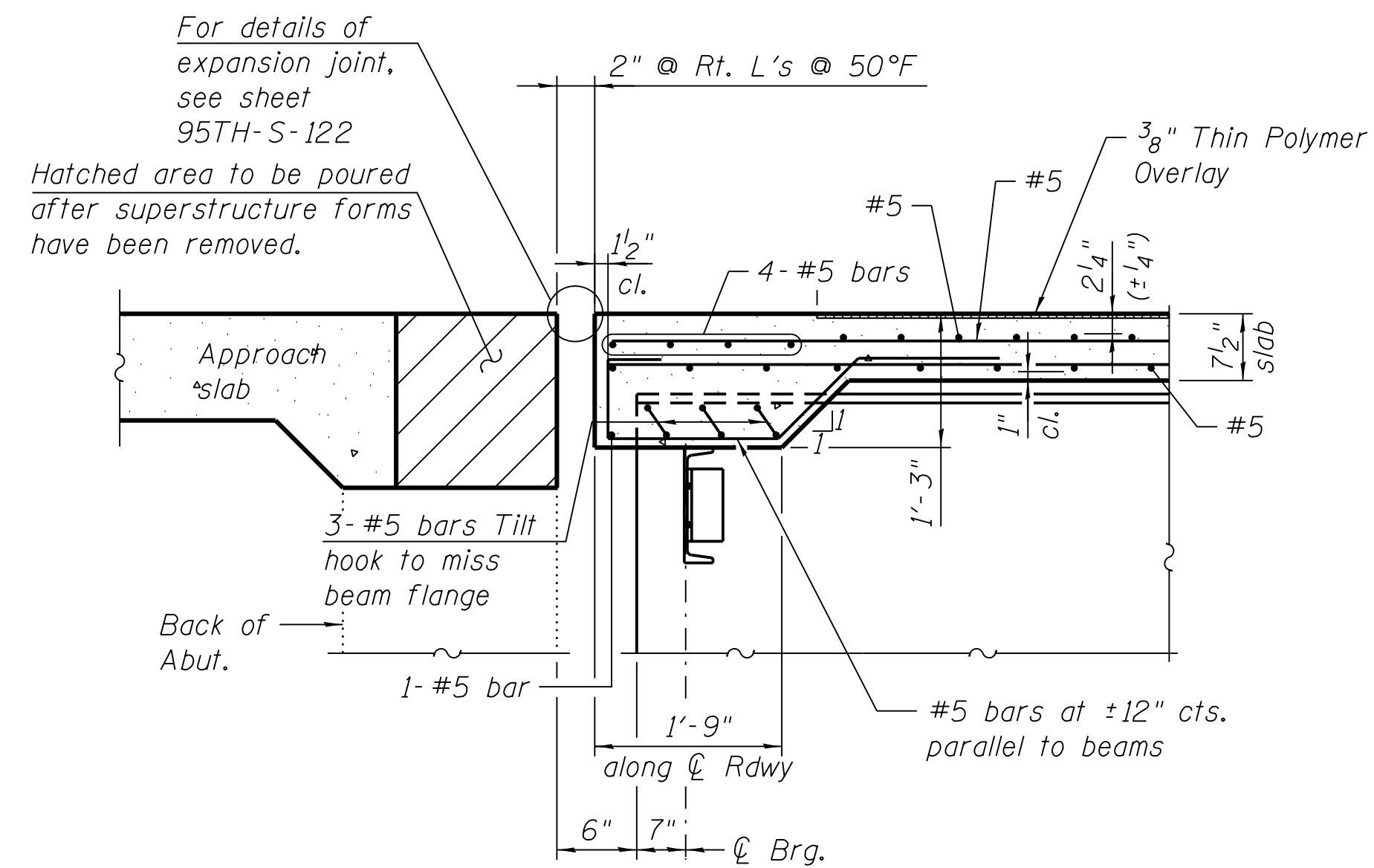


SECTION B-B AT EXISTING ABUTMENT

(Preformed Joint Seal not shown for clarity. W. Abut. shown, E. Abut. similar)



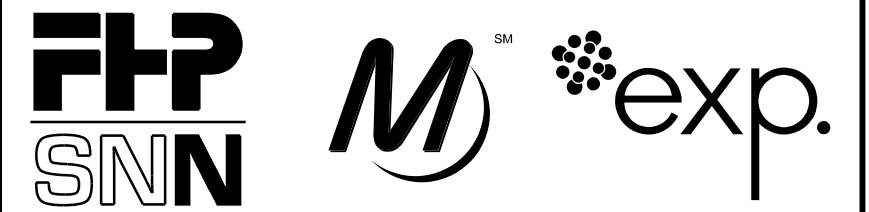
SUPERIMPOSED MEDIAN - INSTALL AFTER STAGE II CONSTRUCTION



SECTION A-A (at widened abutment)

Notes:

- For bridge fence railing details refer to Architectural drawings, see sheets A-842 and A-844.
- Existing expansion joint to be removed and replaced. For expansion joint replacement details within existing bridge deck, see sheet 95TH-S-122.



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95th Terminal Improvement  
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DESIGNED BY	L.SOMMER
DRAWN BY	T.SLEDGE
PROJECT NO.	2012-0021
FILE NAME	File Name

2/9/17	ISSUED FOR CONSTRUCTION	
MARK	DATE	DESCRIPTION

LOCATION IDENTIFIER: DR095

95TH ST. BRIDGE  
 SUPERSTRUCTURE DETAILS  
 (SHEET 1 OF 3)  
 SN 016-0437

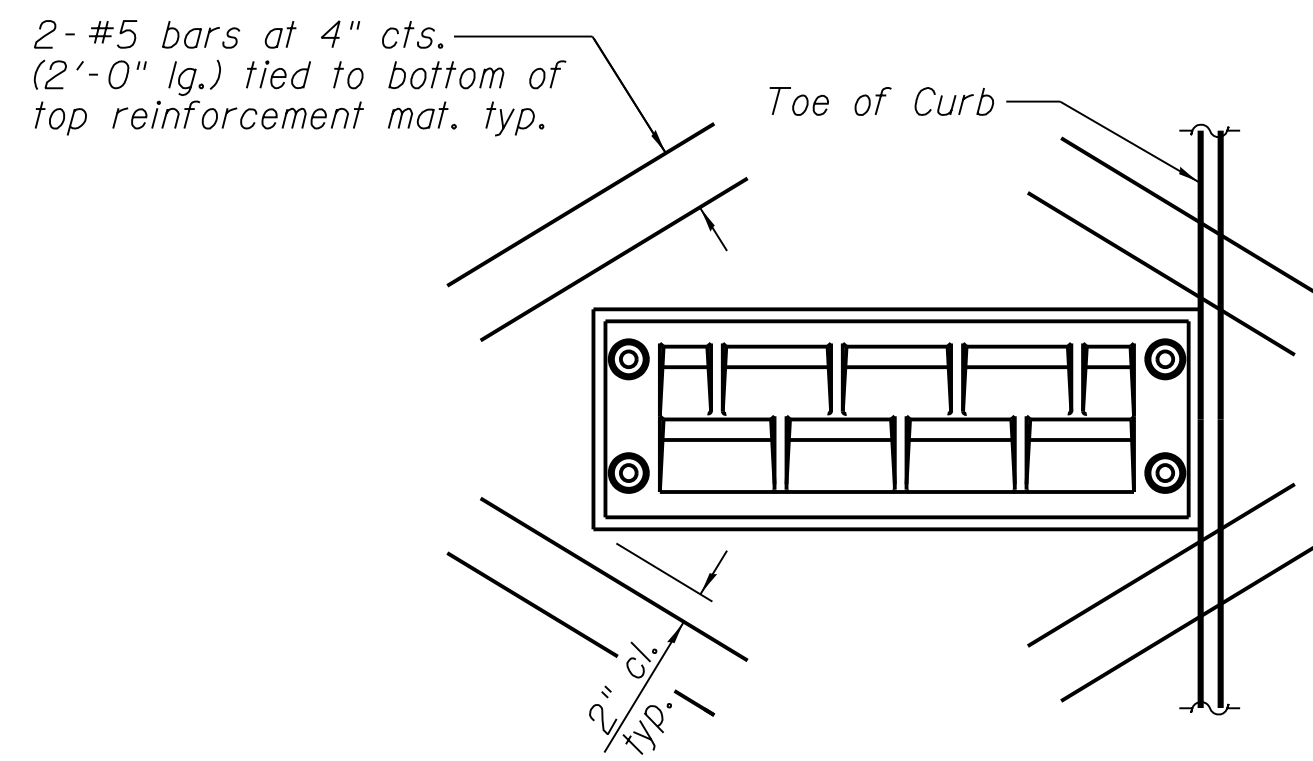
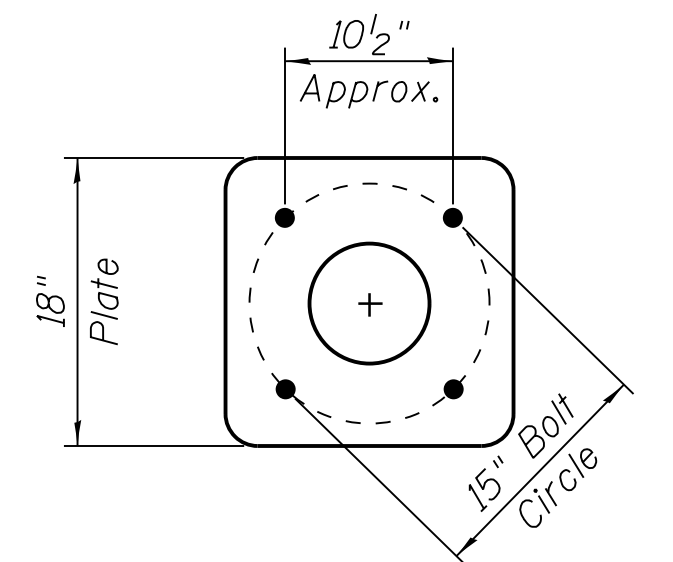
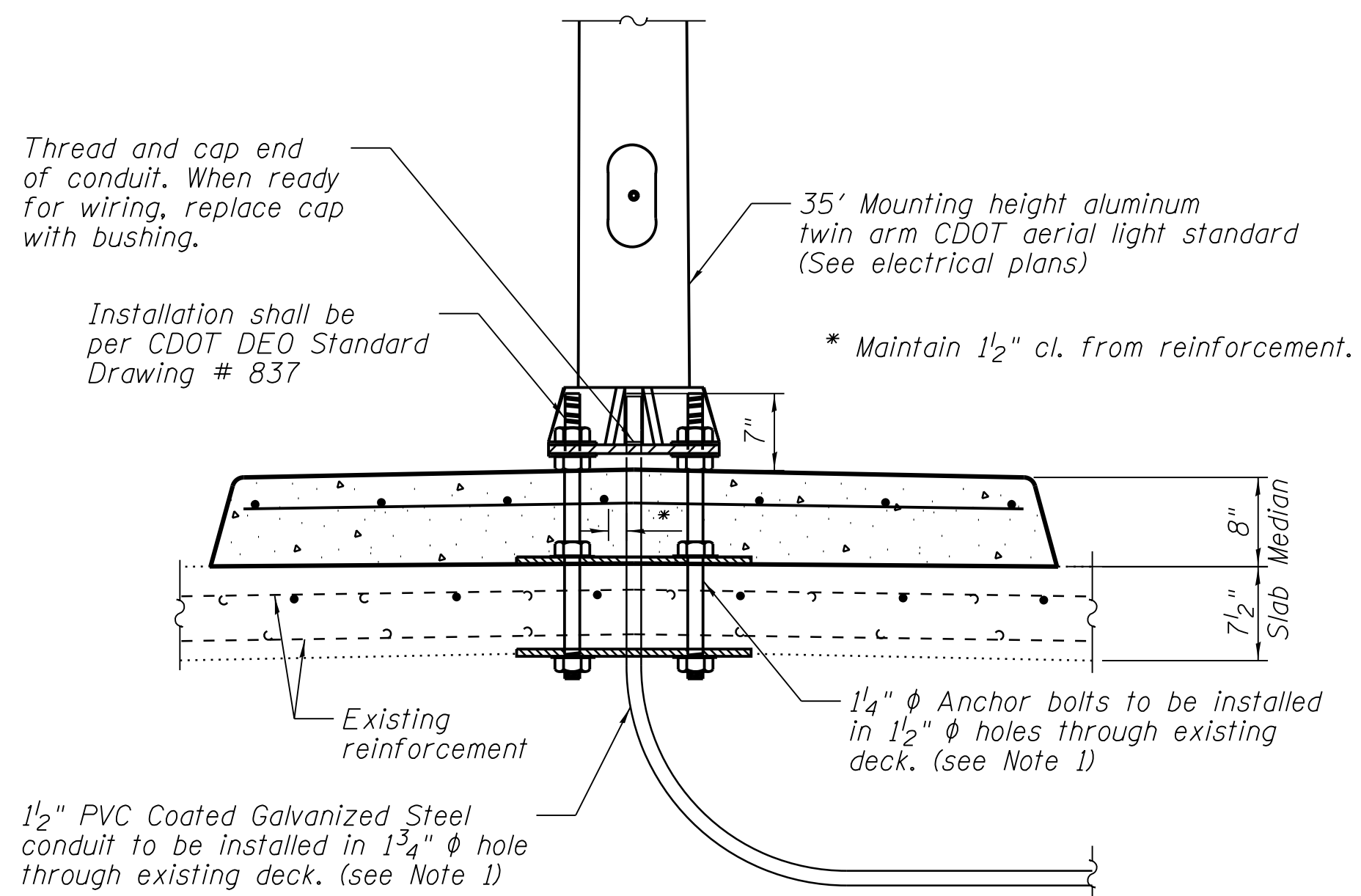
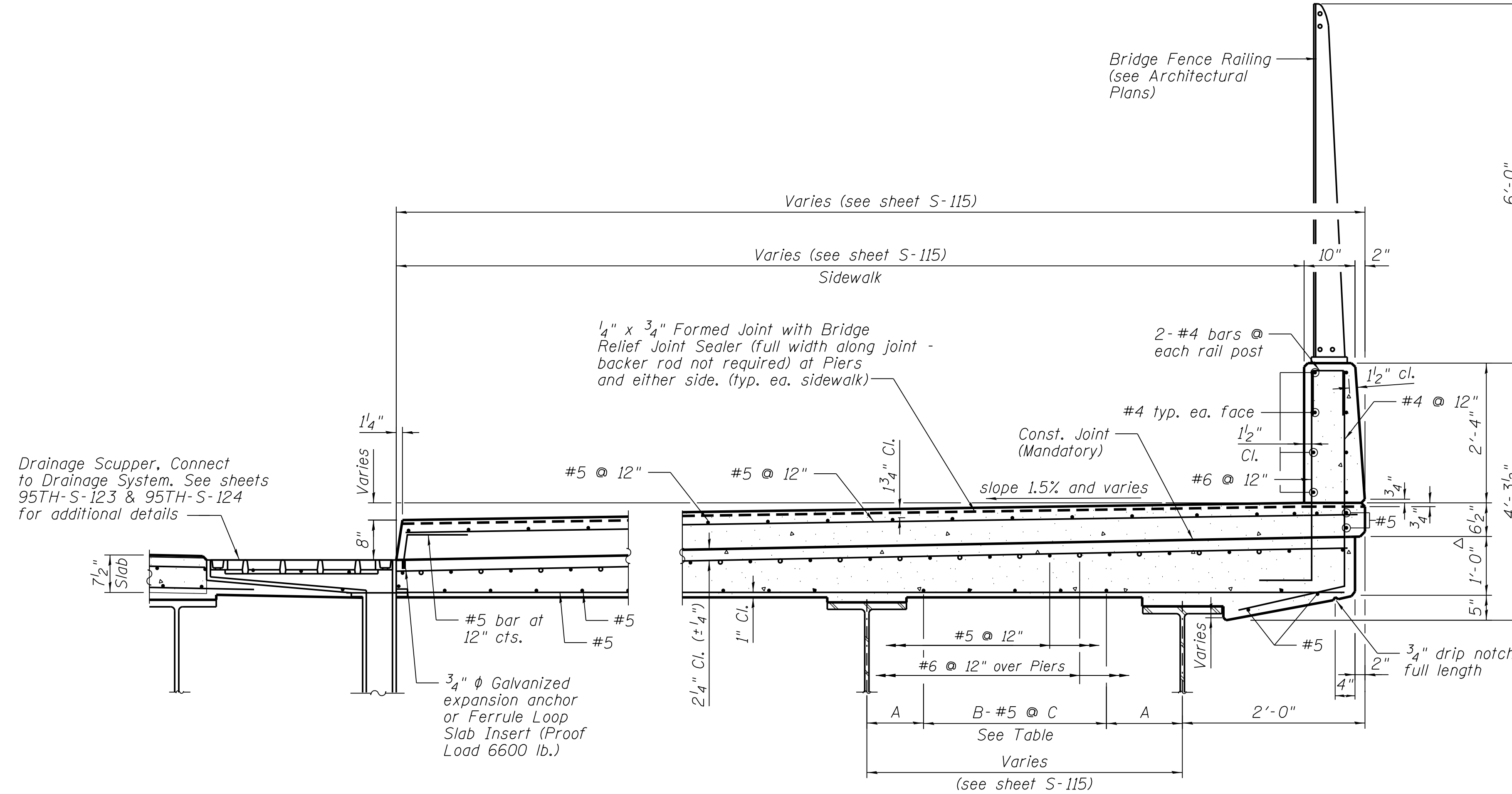
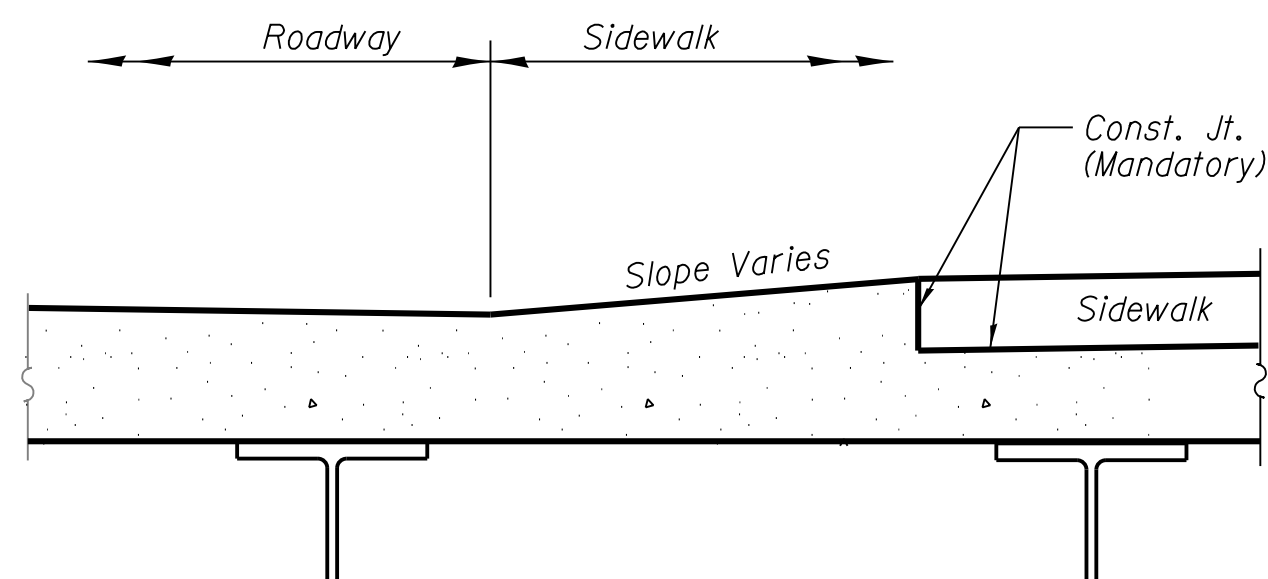
95TH-S-115

FILE NAME = 070\_DR095\_S-115.dgn  
 PLOT DATE = 8/31/2016



**BOTTOM LONGITUDINAL REINFORCEMENT TABLE**

Beam Location	A	B	C
1H 1E	9 3/4"	5	9"
1K 1E	9 1/4"	7	10"
1E 1D	10"	3	7"
1D 1C	10 1/2"	4	10"
1C 1B	10 1/2"	5	10"
1B 1A	10 1/2"	5	10"
1A 1	11"(-)	5	10"
1-2	10 1/2"(-)	7	10"
15-16	10 1/2"(-)	7	10"
16-16A	10"(-)	3	8 1/2"
16A-16B	10 1/2"	4	10"
16B-16C	10"	3	7"
16C-16F	10 3/8"	6	10"
16C-16H	9 7/8"	5	10"



- Notes:
1. Drilled holes through existing deck shall be located to miss existing reinforcement.
  2. For additional light pole details, see electrical plans.
  3. Drains shall be located clear of all diaphragms.



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 15 West 95th Street (South Terminal)  
 Chicago, IL 60628

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PROJECT NO.	2012-0021
FILE NAME	File Name

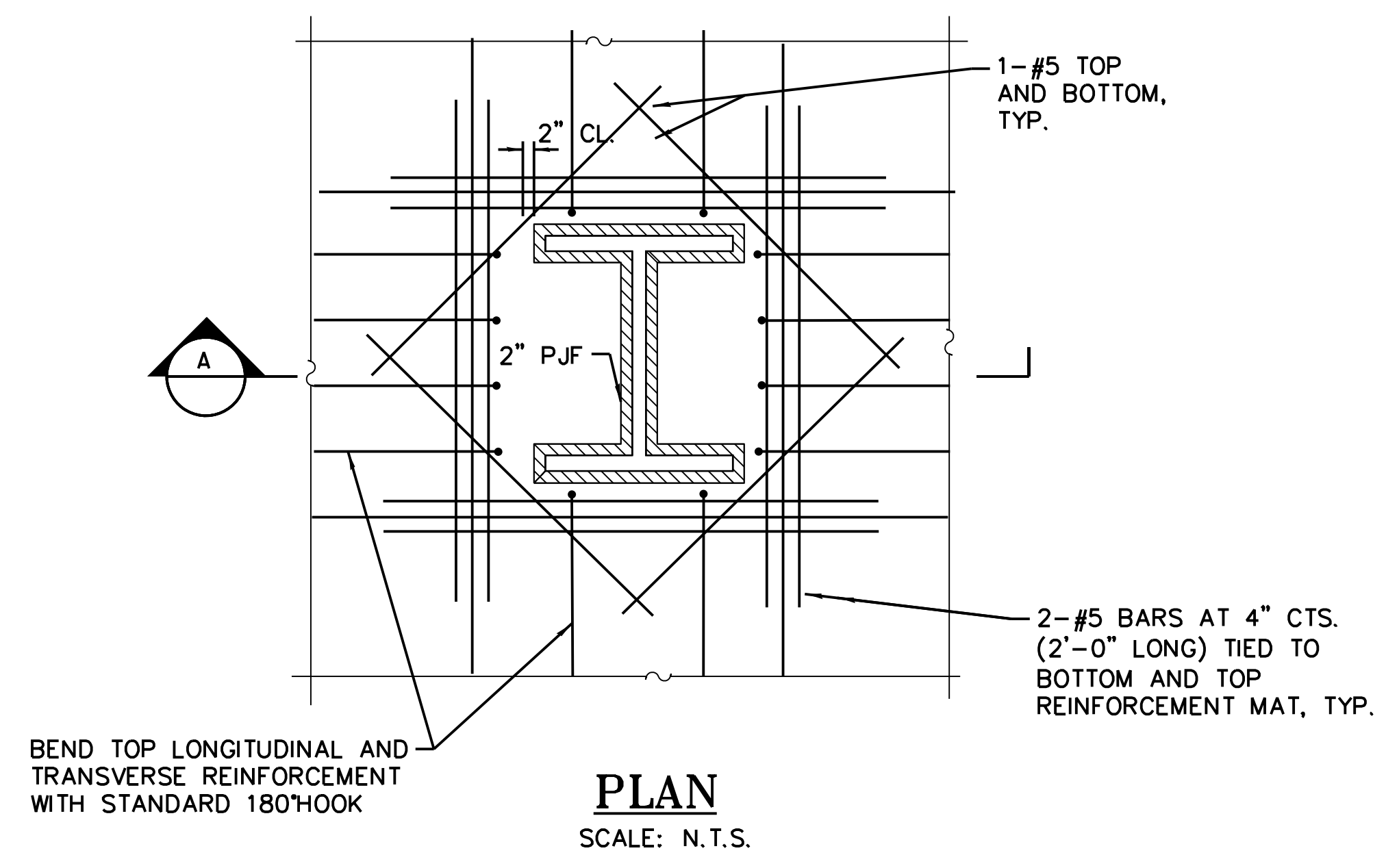
2/9/17	ISSUED FOR CONSTRUCTION	
MARK	DATE	DESCRIPTION

LOCATION IDENTIFIER: **DR095**

**95TH ST. BRIDGE**  
 SUPERSTRUCTURE DETAILS  
 (SHEET 2 OF 3)  
 SN 016-0437

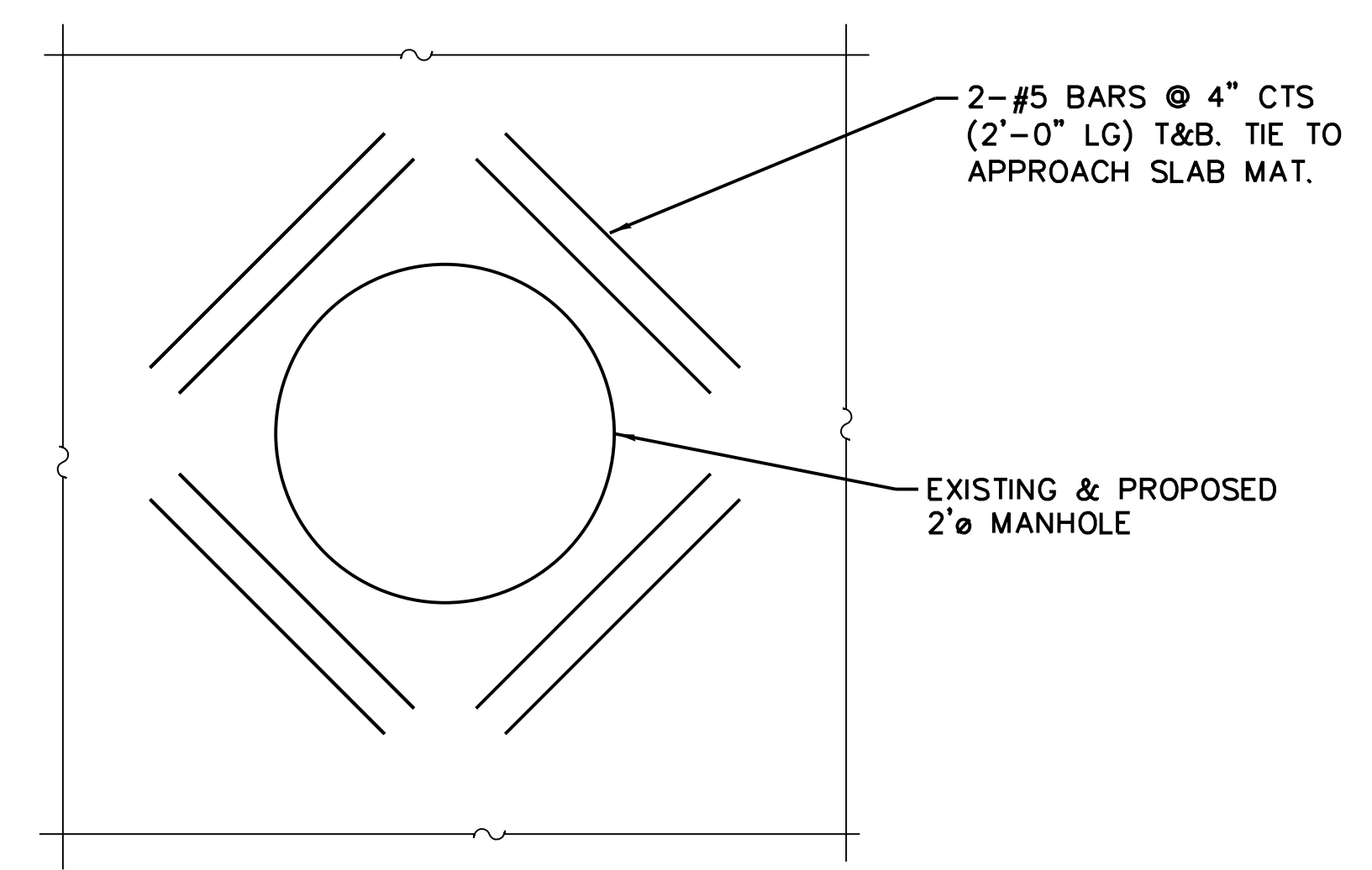
**95TH-S-116**

vscarpqa

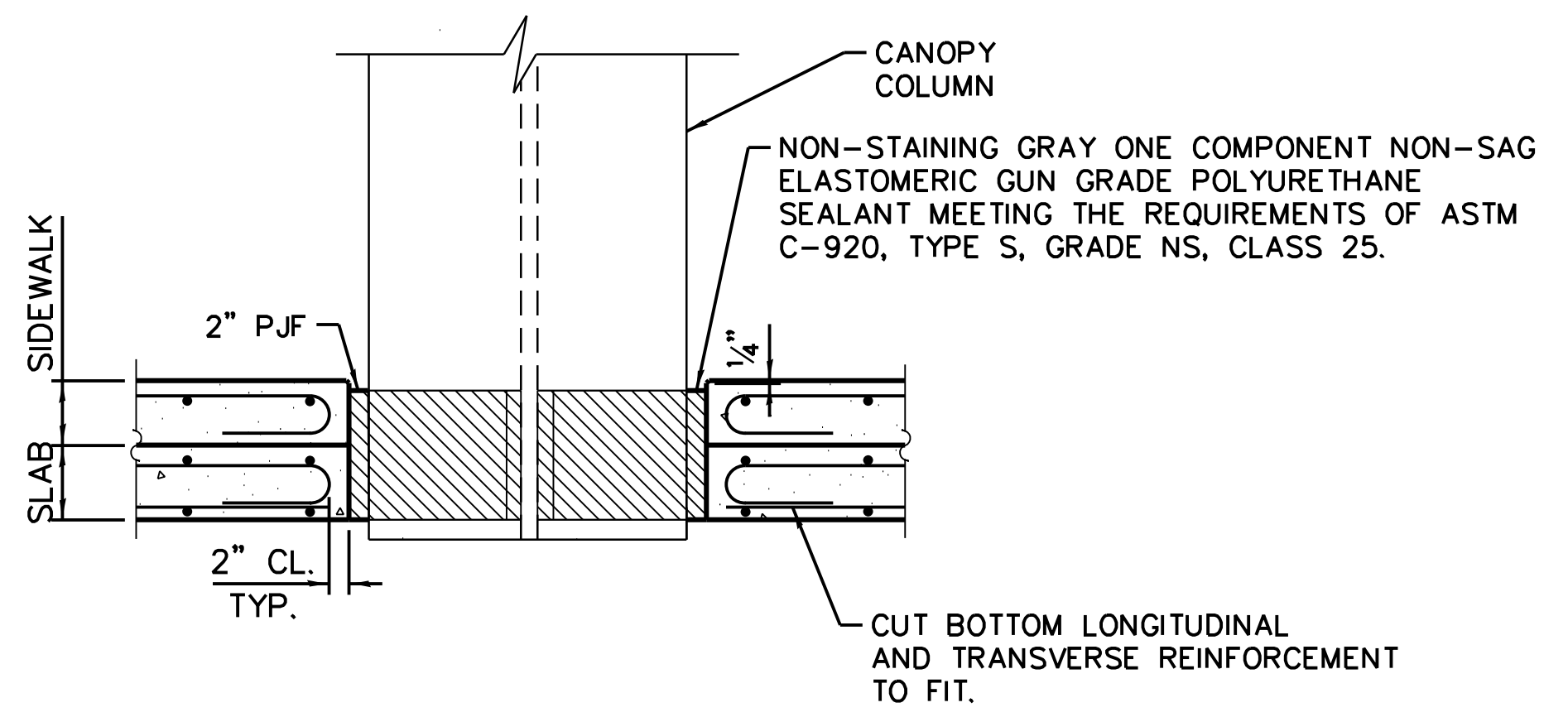


BEND TOP LONGITUDINAL AND TRANSVERSE REINFORCEMENT WITH STANDARD 180°HOOK

**PLAN**  
SCALE: N.T.S.



**OPENING IN APPROACH SLAB**



**SECTION A-A**

STEEL COLUMN CONNECTION NOT SHOWN FOR CLARITY.



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Chicago, IL 60628

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APPROVED BY	D.ZROKA
CHECKED BY	P.MAID
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DRAWN BY	T.SLEDGE
PROJECT NO.	2012-0021
FILE NAME	File Name

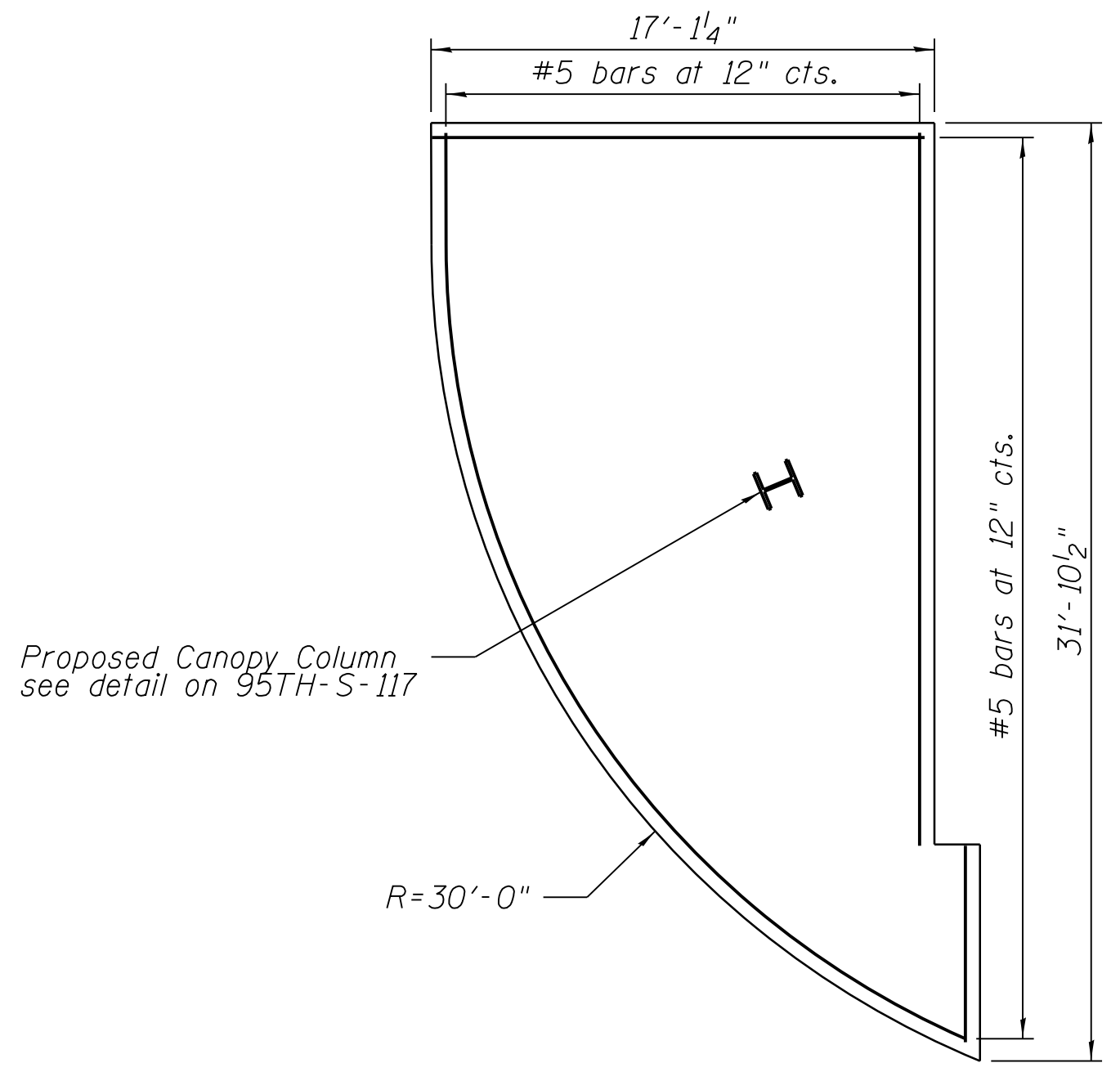
MARK	DATE	DESCRIPTION
	2/9/17	ISSUED FOR CONSTRUCTION

LOCATION IDENTIFIER: **DR095**  
95TH ST. BRIDGE  
SUPERSTRUCTURE DETAILS  
(SHEET 3 OF 3)  
SN 016-0437

**95TH-S-117**

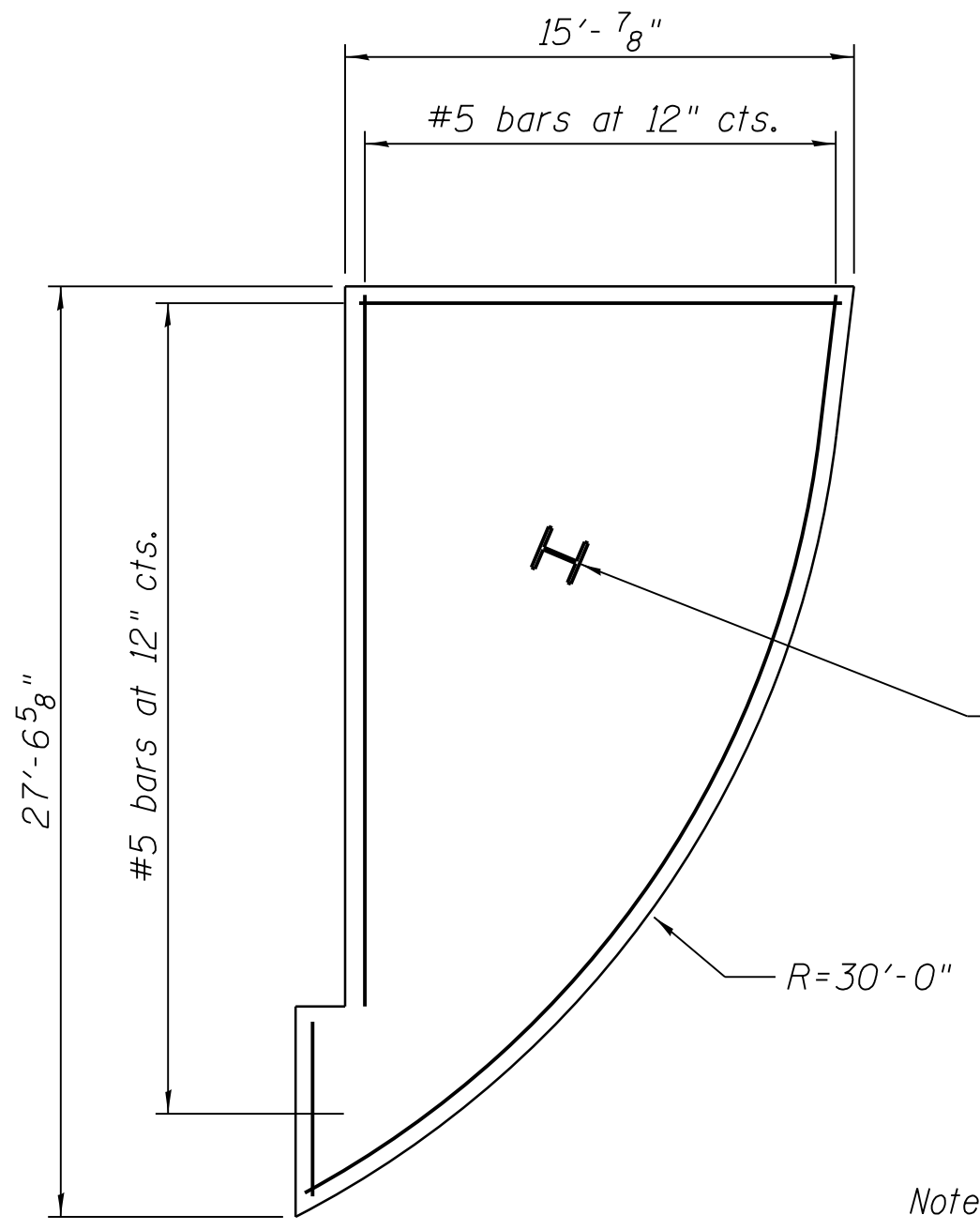
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PLOT DATE = 8/31/2016

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### SIDEWALK PLAN

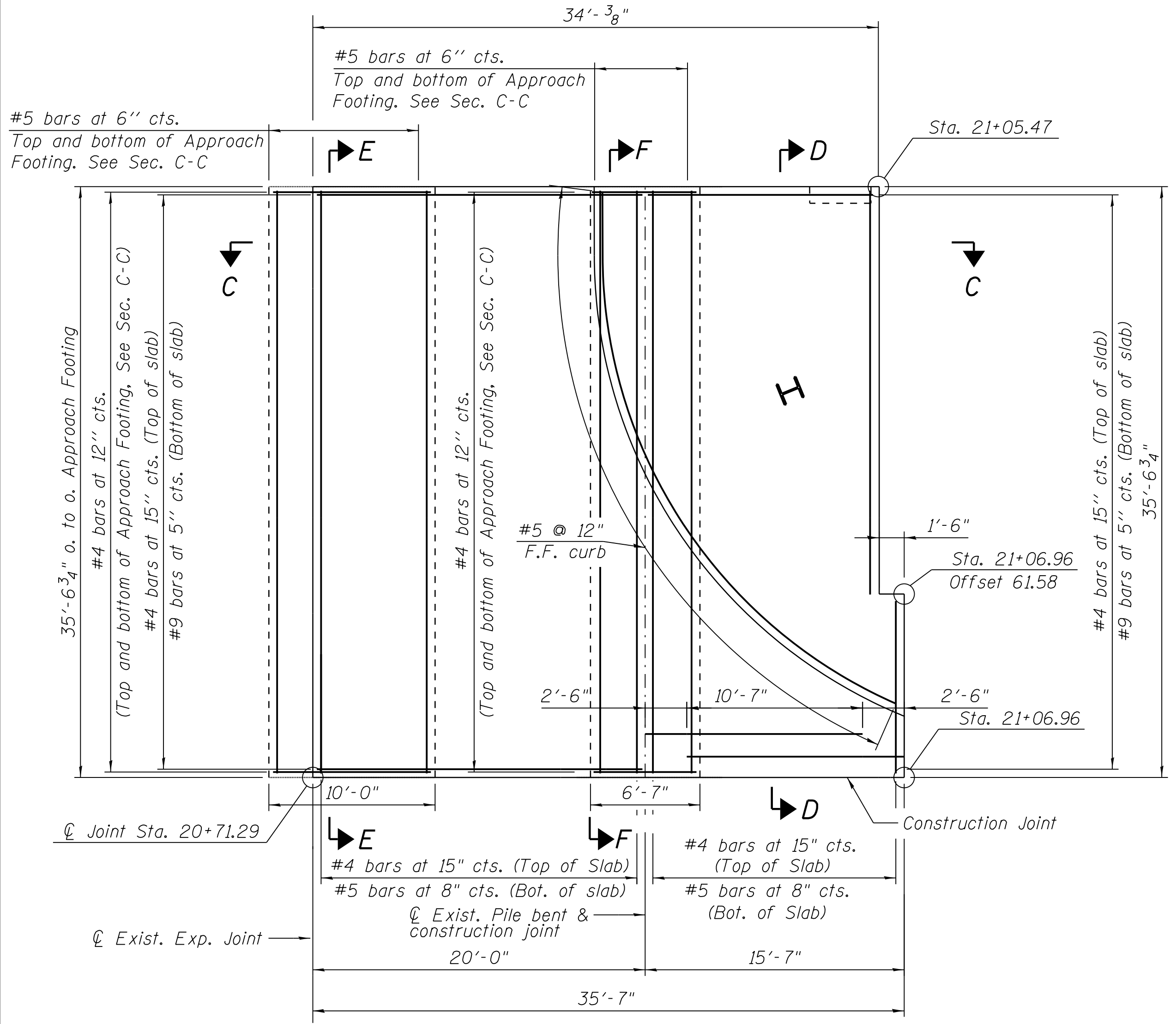
(North Widening - West Approach Sidewalk)



### SIDEWALK PLAN

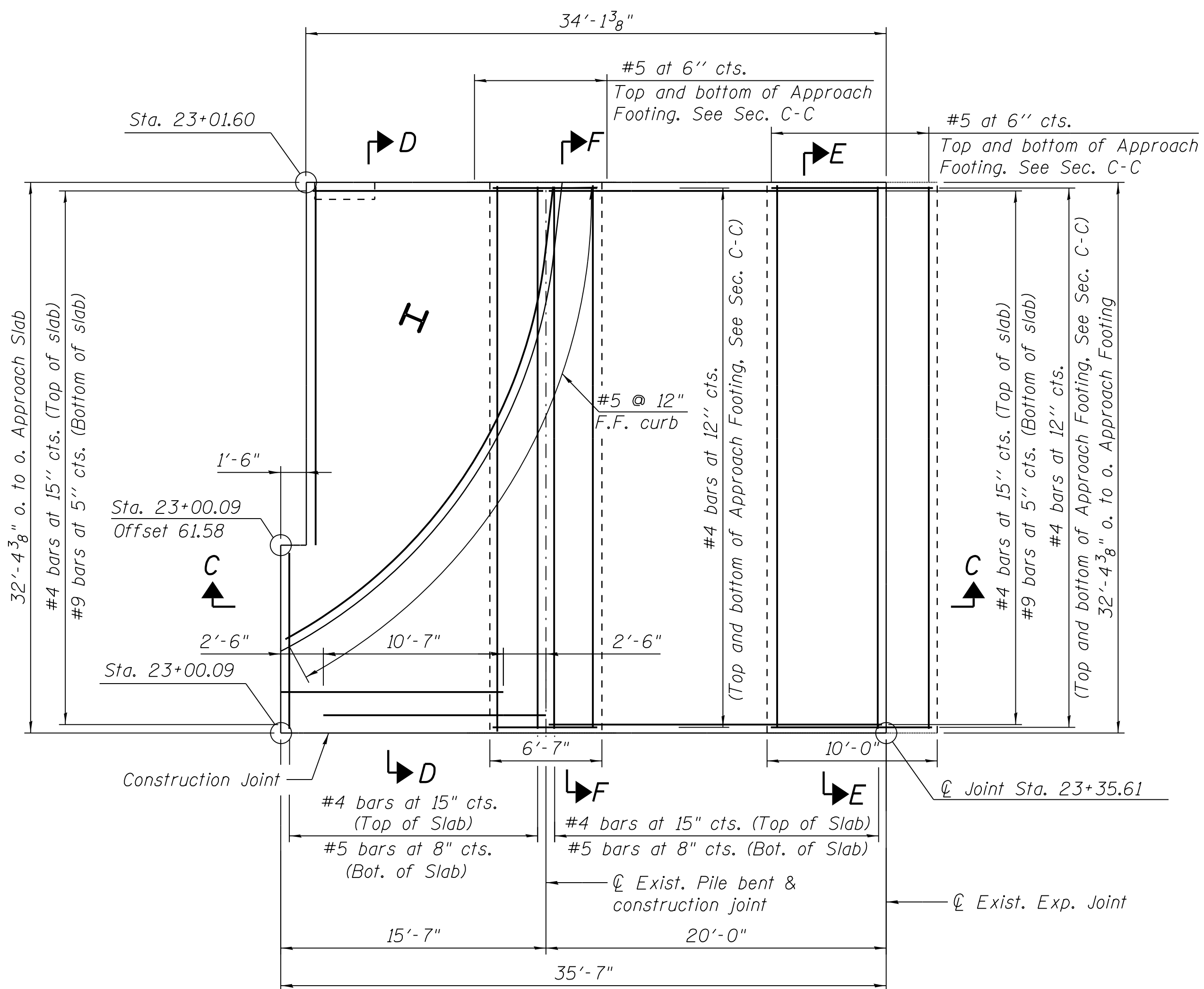
(North Widening - East Approach Sidewalk)

- Notes:
1. See sheet S-120 for Sections C-C, D-D and E-E and F-F.
  2. See sheet S-111 and S-112 for top of Approach slab elevations.



### APPROACH SLAB PLAN

(North Widening - West Approach)



### APPROACH SLAB PLAN

(North Widening - East Approach)

Note: All offsets are measured off  $\varnothing$  Bridge and PGL.

FILE NAME = 070\_DR095\_S-118.dgn  
PLOT DATE = 11/17/2016



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 Chicago, IL 60628

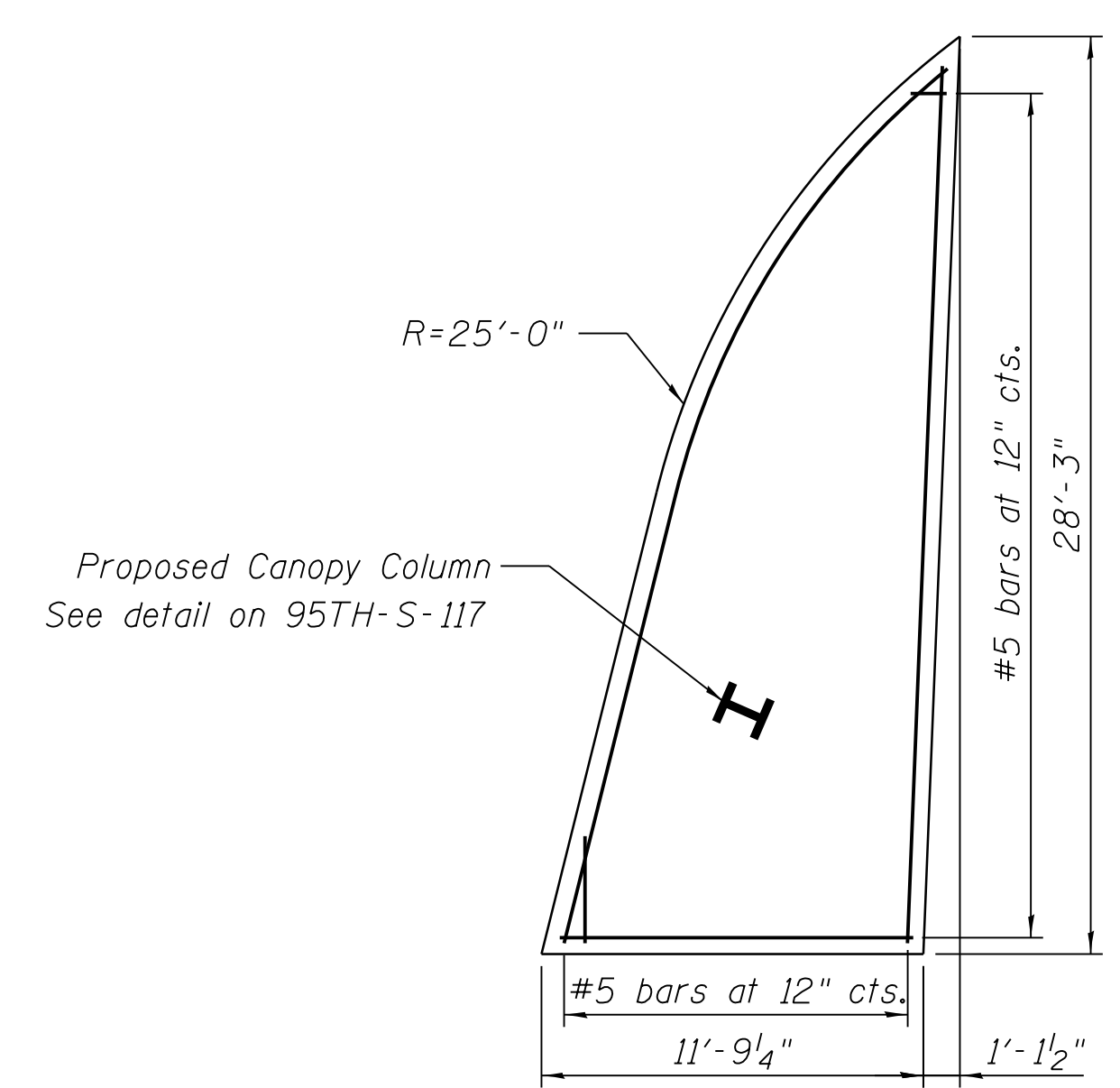
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APPROVED BY	D.ZROKA
CHECKED BY	P.MAID
DESIGNED BY	L.SOMMER
DRAWN BY	T.SLEDGE
PROJECT NO.	2012-0021
FILE NAME	File Name

MARK	DATE	DESCRIPTION
	2/9/17	ISSUED FOR CONSTRUCTION

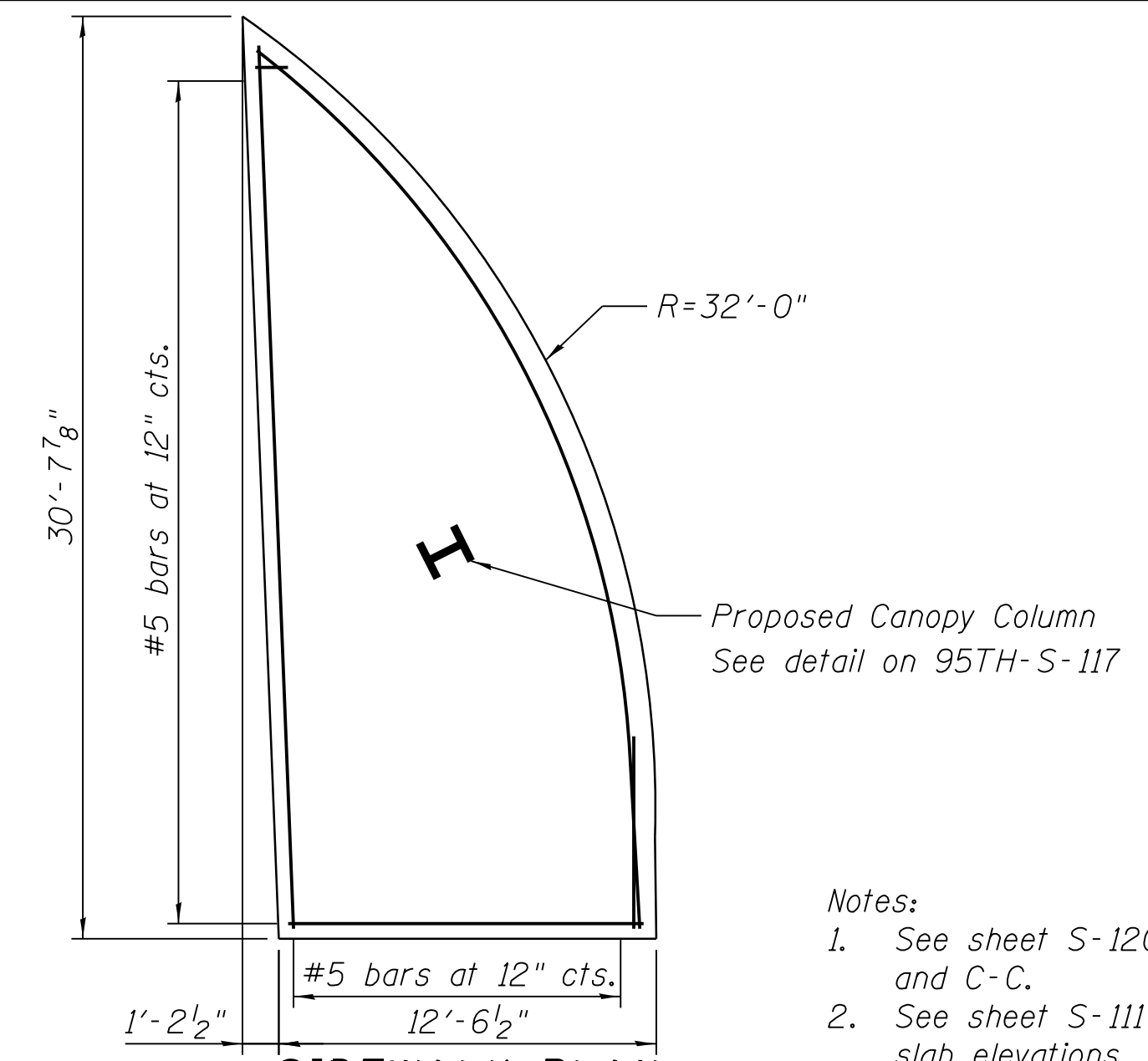
LOCATION IDENTIFIER: **DR095**  
 95TH ST. BRIDGE  
 BRIDGE APPROACH SLAB DETAILS  
 (SHEET 1 OF 3)  
 SN 016-0437

**95TH-S-118**

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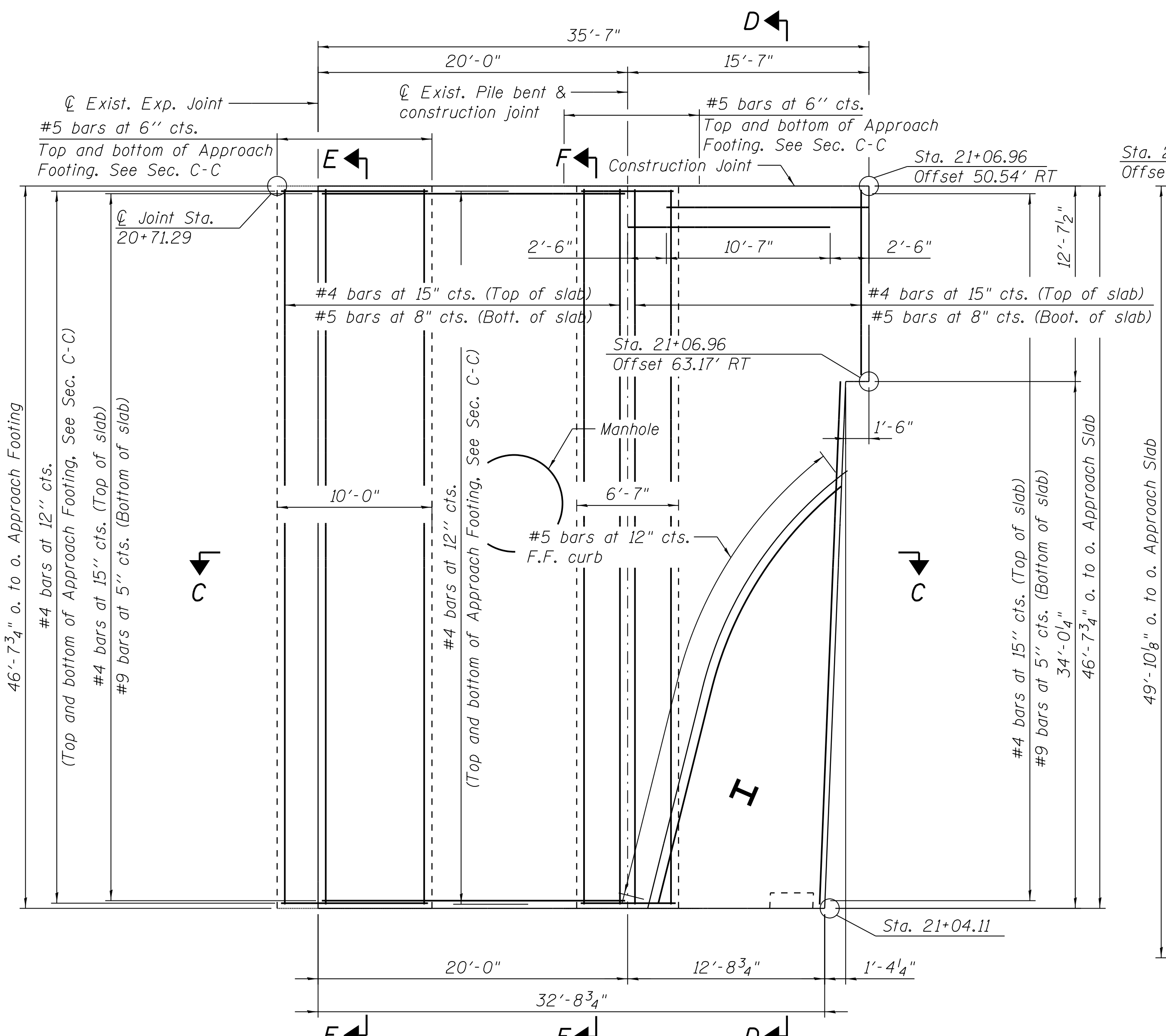


**SIDEWALK PLAN**  
(South Widening - West Approach Sidewalk)

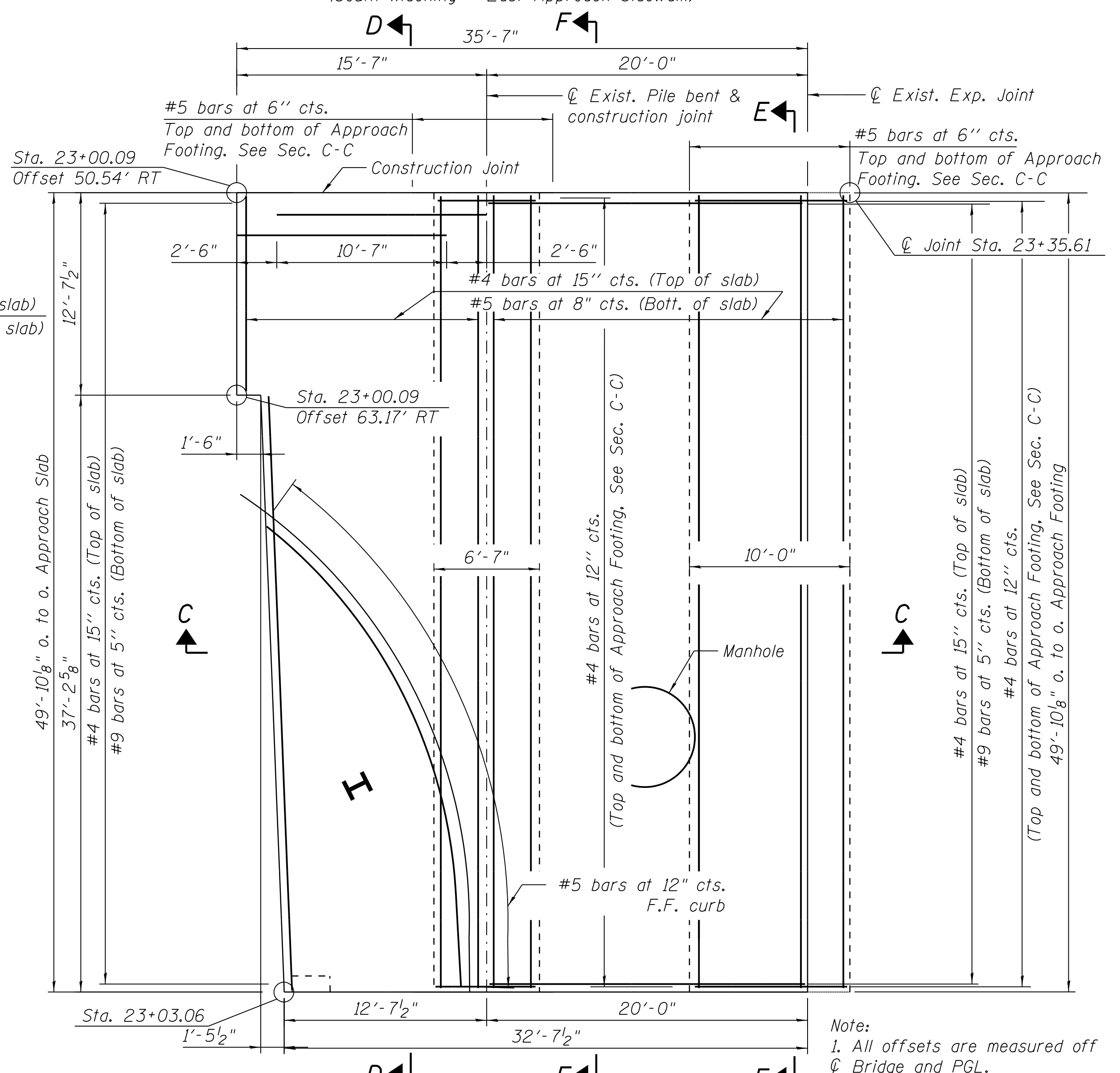


**SIDEWALK PLAN**  
(South Widening - East Approach Sidewalk)

- Notes:
- See sheet S-120 for Sections D-D, E-E, F-F and C-C.
  - See sheet S-111 and S-112 for top of Approach slab elevations.



**APPROACH SLAB PLAN**  
(South Widening - West Approach)



**APPROACH SLAB PLAN**  
(South Widening - East Approach)

- Note:
- All offsets are measured off  $\text{\textcircled{C}}$  Bridge and PGL.
  - Manhole detail on 95th-S-117, coordinate with traffic signal drawings.

FILE NAME = 070\_DR095\_S-119.dgn  
PLOT DATE = 11/17/2016



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Chicago, IL 60628

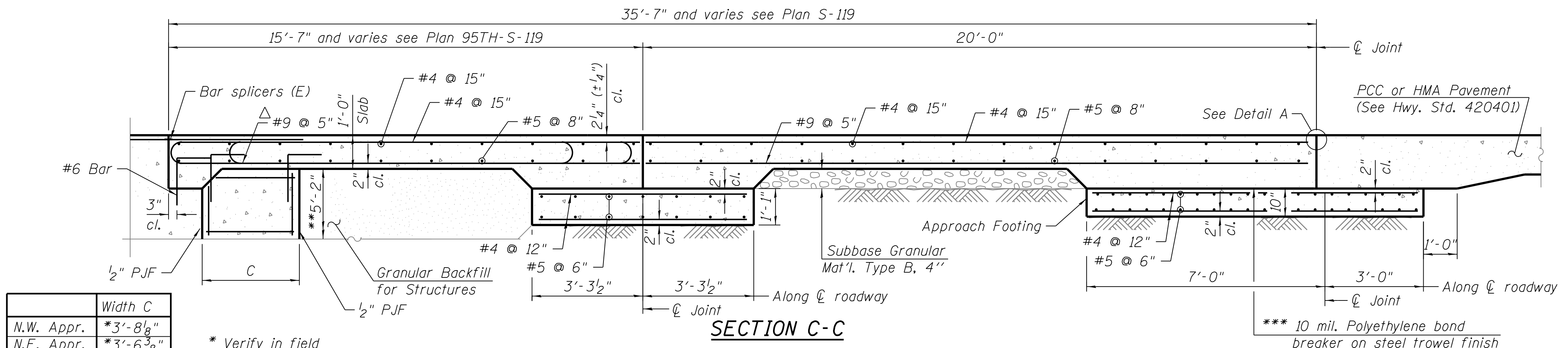
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<b>APPROVED BY</b>	D.ZROKA
<b>CHECKED BY</b>	P.MAID
<b>DESIGNED BY</b>	L.SOMMER
<b>DRAWN BY</b>	T.SLEDGE
<b>PROJECT NO.</b>	2012-0021
<b>FILE NAME</b>	File Name

MARK	DATE	DESCRIPTION
	2/9/17	ISSUED FOR CONSTRUCTION

**LOCATION IDENTIFIER: DR095**  
**95TH ST. BRIDGE**  
**BRIDGE APPROACH SLAB DETAILS**  
(SHEET 2 OF 3)  
SN 016-0437

**95TH-S-119**

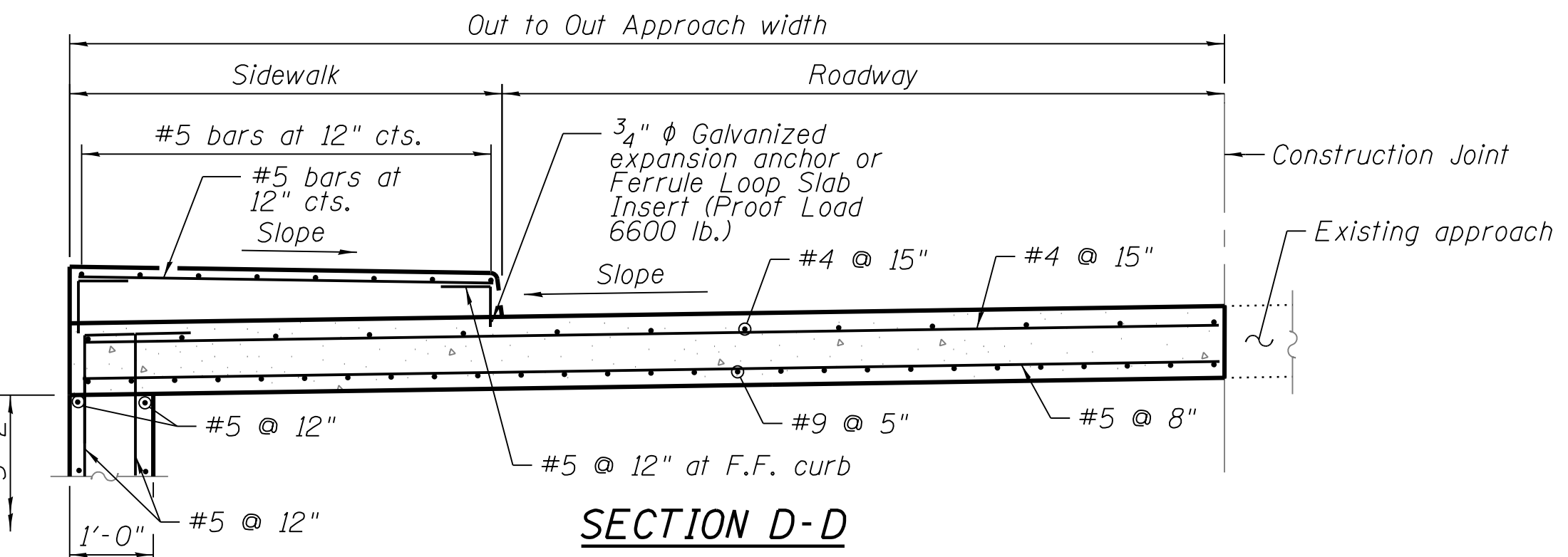
Rbrzezon



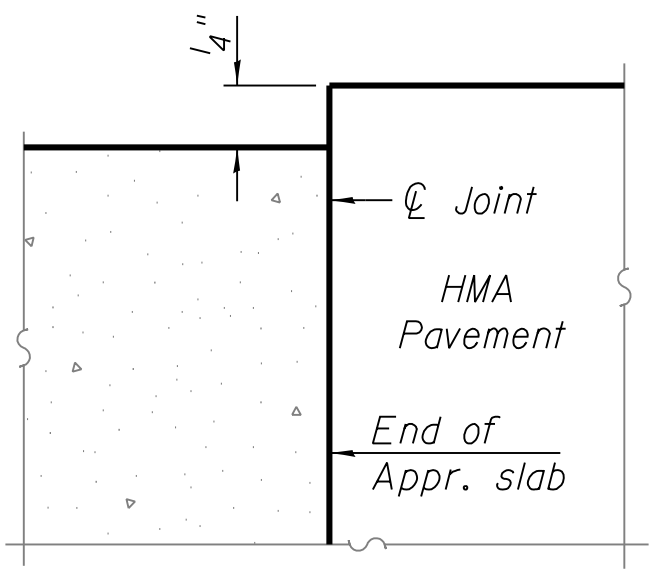
	Width C
N.W. Appr.	*3'-8 1/8"
N.E. Appr.	*3'-6 3/8"
S.W. Appr.	*2'-9 3/8"
S.E. Appr.	*2'-4 1/2"

\* Verify in field  
 \*\* Depth is 1' below the the bottom of adjacent grade beam, Verify in field.

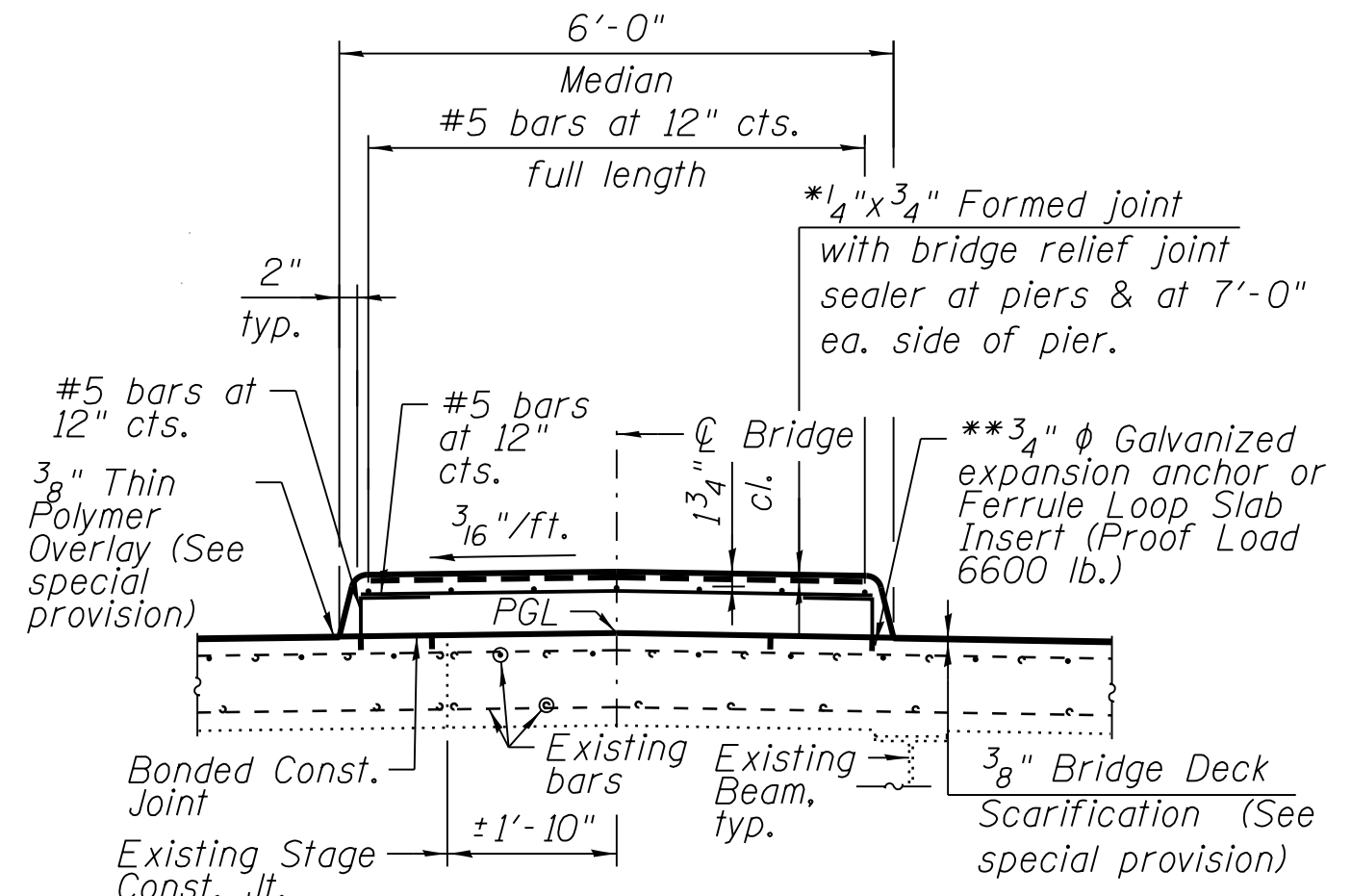
△ Tilt #9 bars as required to maintain clearance



**SECTION D-D**

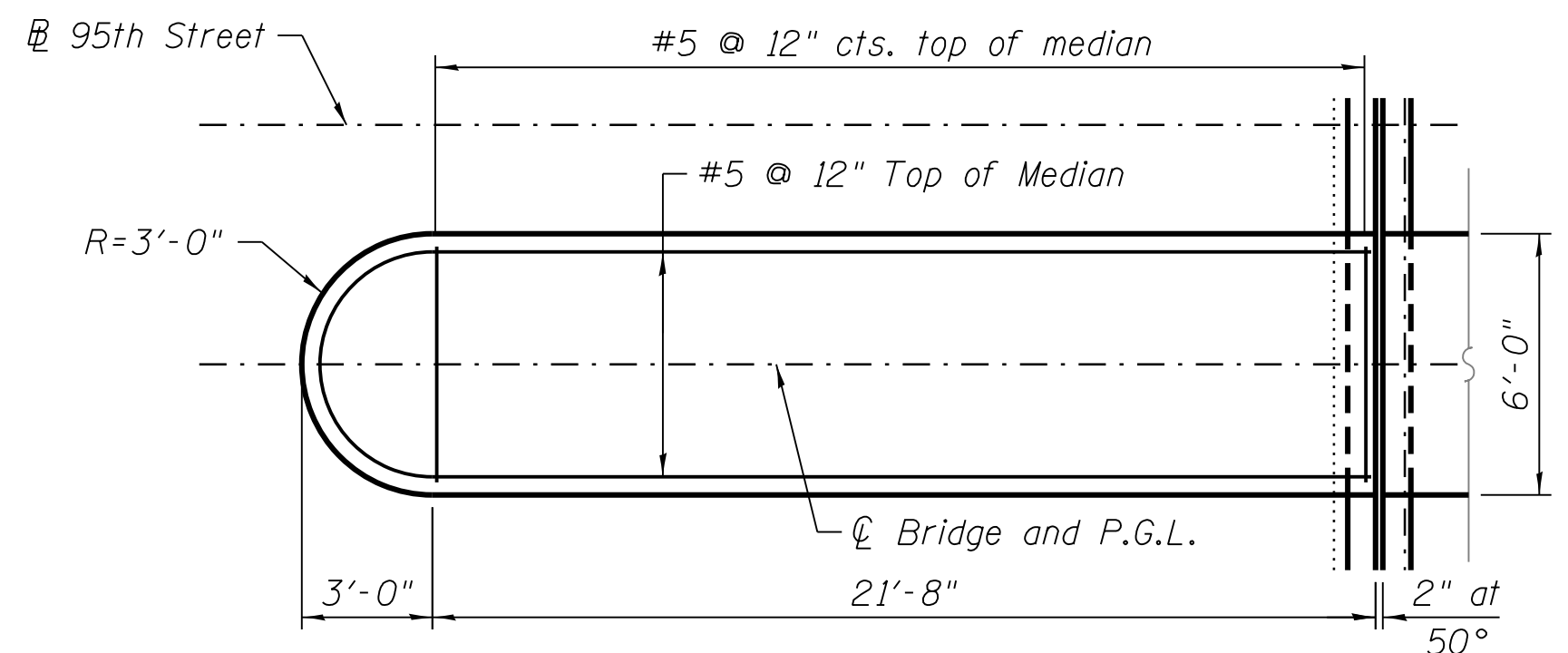


**DETAIL A**

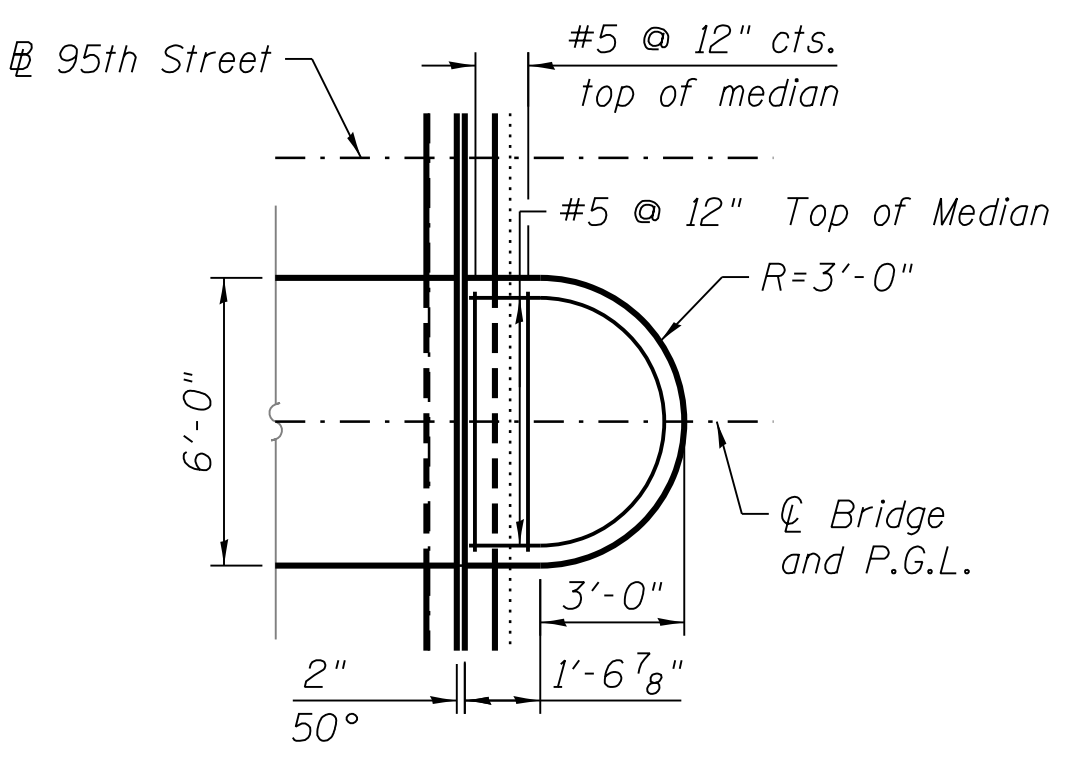


**SUPERIMPOSED MEDIAN -  
 INSTALL AFTER STAGE II CONSTRUCTION**

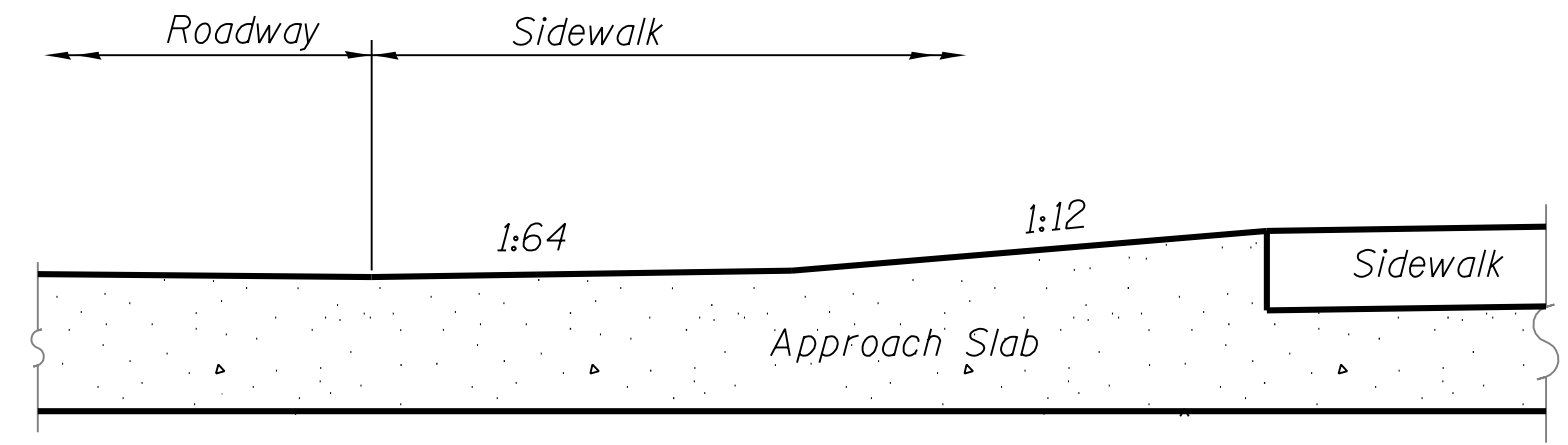
\* Full width along joint. - backer rod not required.  
 \*\* Space to miss existing reinforcement.



**PLAN - WEST MEDIAN**

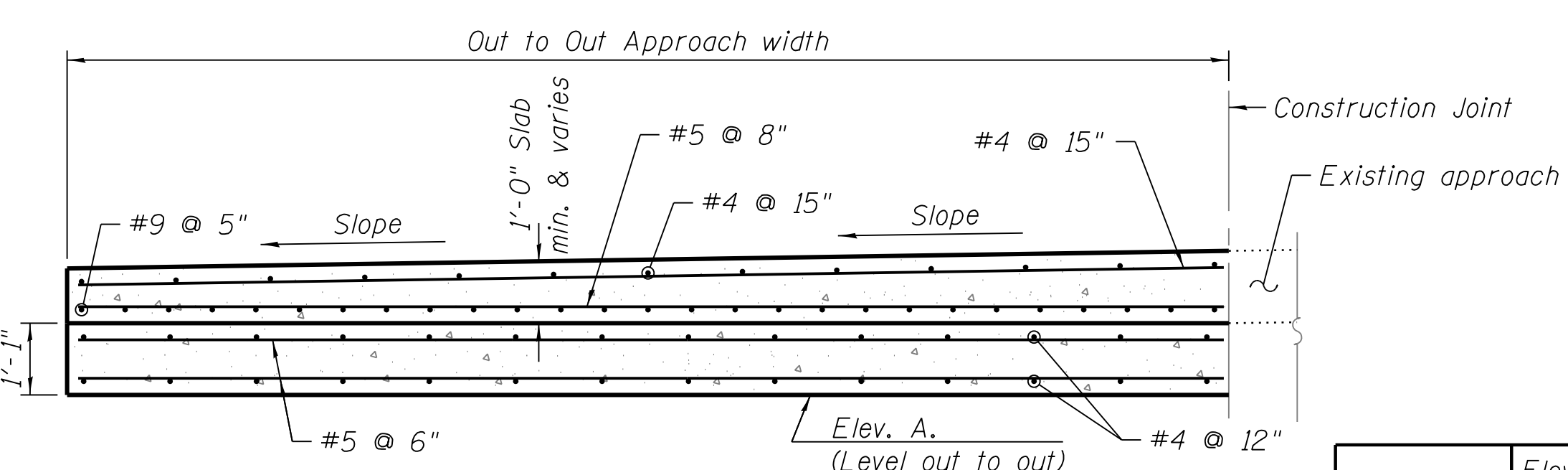


**PLAN - EAST MEDIAN**



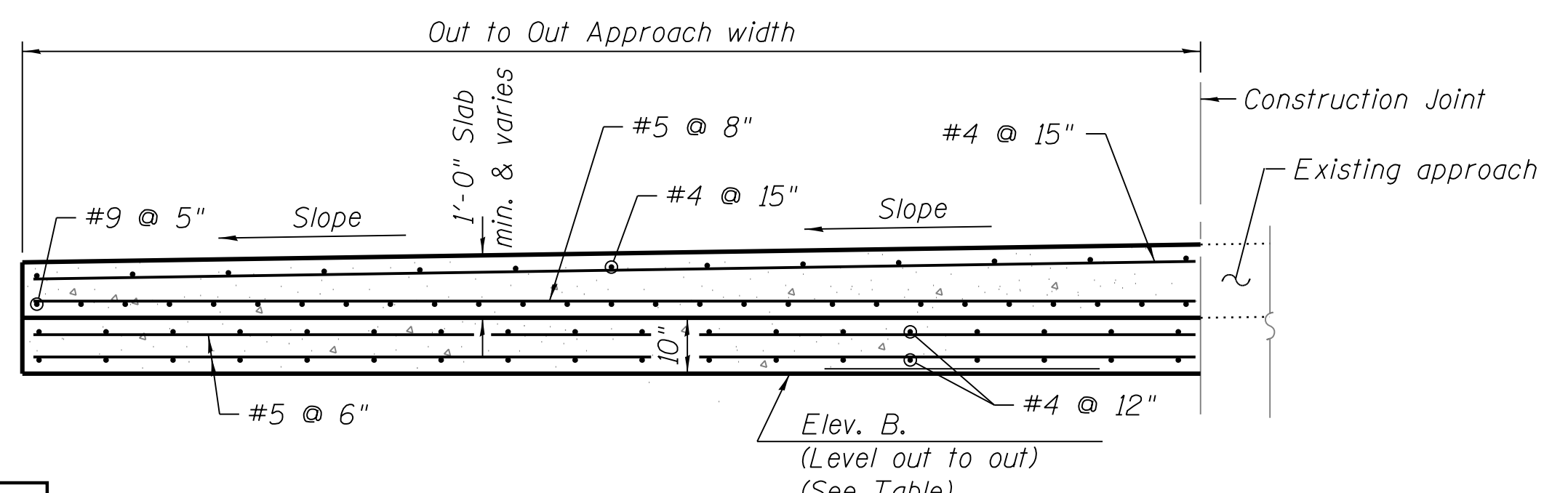
**SECTION THRU ADA RAMP**

Note:  
 See ADA Street Ramp details in Civil Drawings



**SECTION F-F**

(Approach section at Appr. Footing)



**SECTION E-E**

(Approach section at Appr. Footing)

Notes:  
 1. The approach footing maximum applied service bearing pressure (Qmax) = 2.0 ksf.  
 2. See Plan for dimensions not shown.

	Elev. A	Elev. B
N.W. Appr.	14.48	14.33
S.W. Appr.	14.54	14.20
N.E. Appr.	14.58	14.33
S.E. Appr.	14.52	14.16



**CHICAGO TRANSIT  
 AUTHORITY  
 ENGINEERING**

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**95th Terminal Improvement**  
 15 West 95th Street (South Terminal)  
 Chicago, IL 60628

<b>IN CHARGE</b>	S. KADERBEK
<b>APPROVED BY</b>	D.ZROKA
<b>CHECKED BY</b>	P.MAID
<b>DESIGNED BY</b>	L.SOMMER
<b>DRAWN BY</b>	T.SLEDGE
<b>PROJECT NO.</b>	2012-0021
<b>FILE NAME</b>	File Name

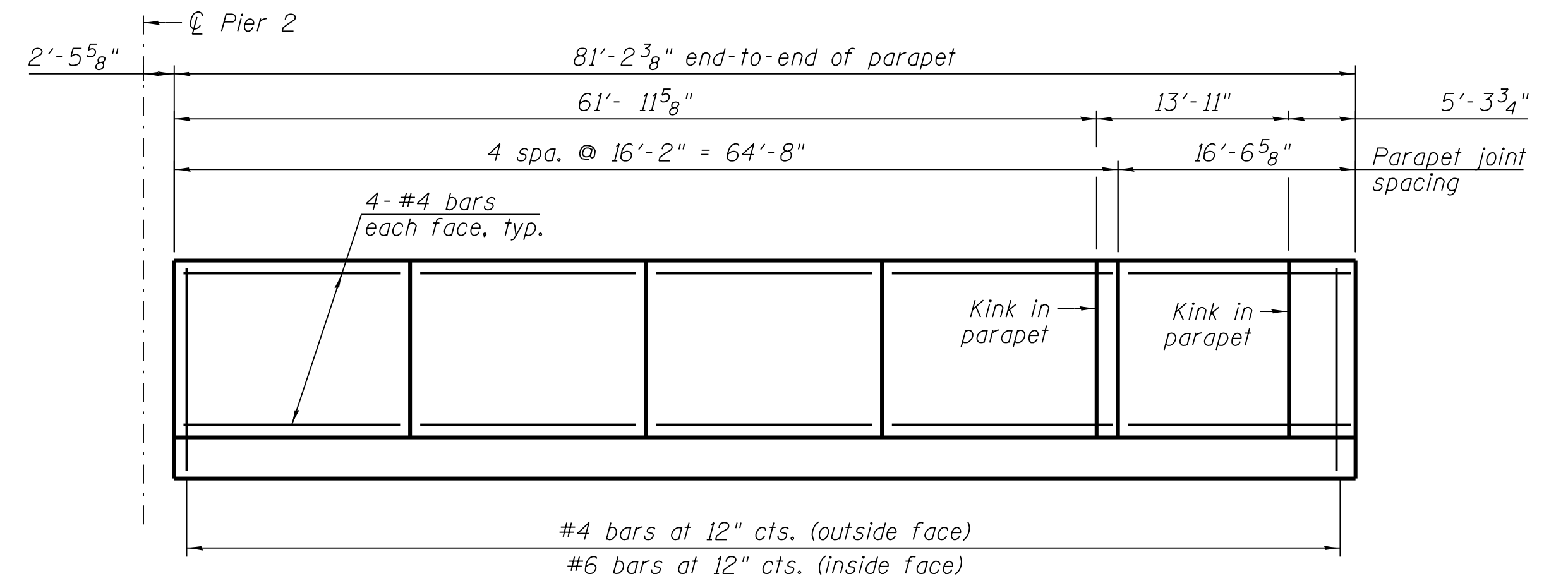
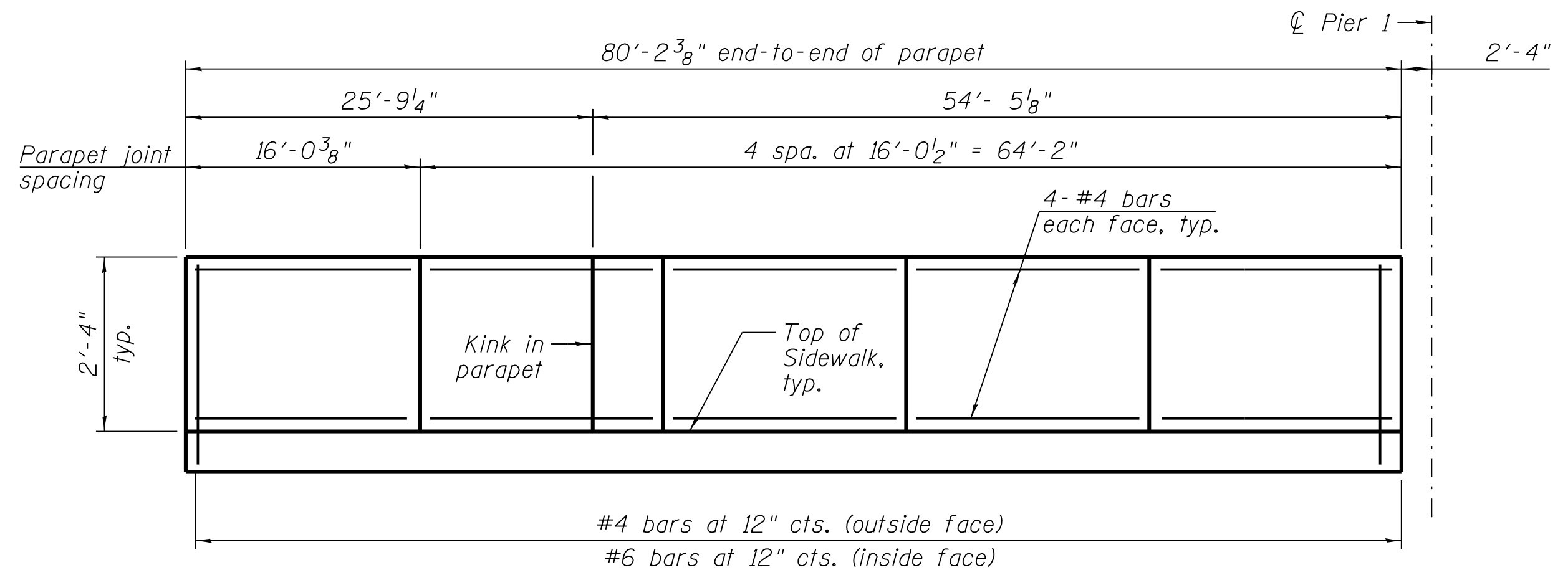
MARK	DATE	DESCRIPTION
	2/9/17	ISSUED FOR CONSTRUCTION

**LOCATION IDENTIFIER: DR095**  
 95TH ST. BRIDGE  
 BRIDGE APPROACH SLAB DETAILS  
 (SHEET 3 OF 3)  
 SN 016-0437

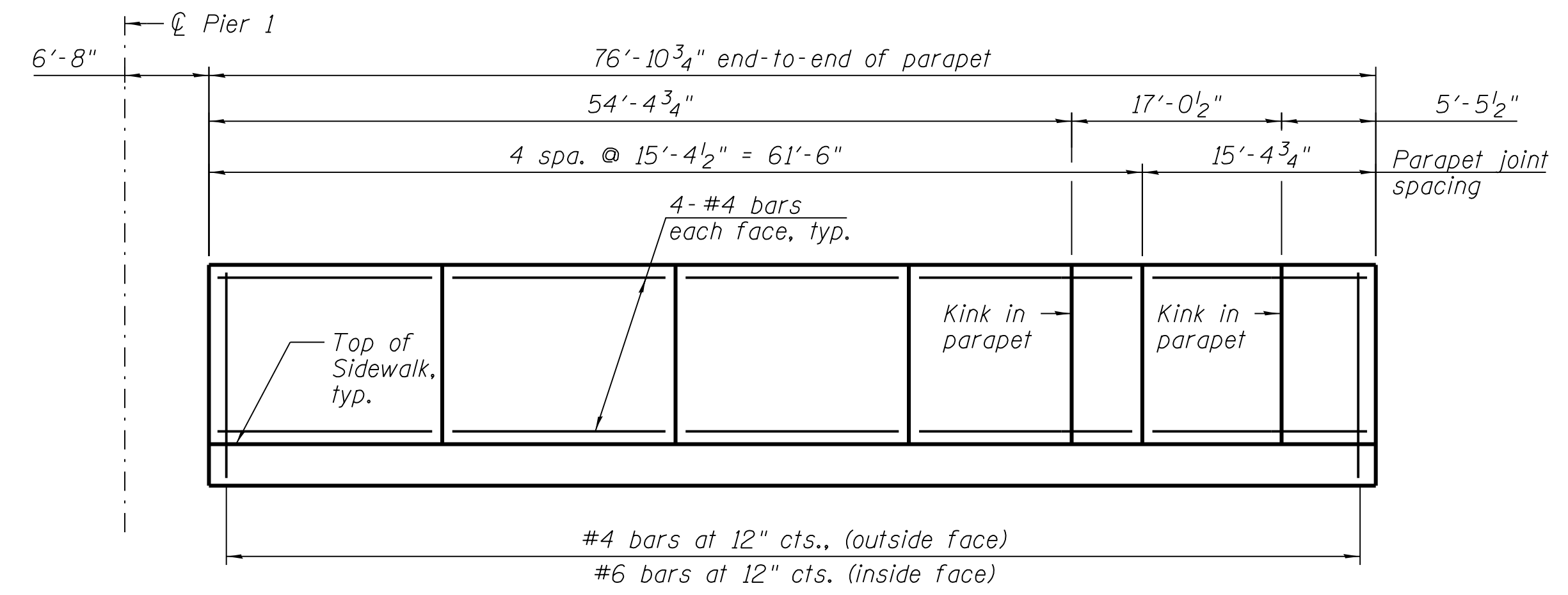
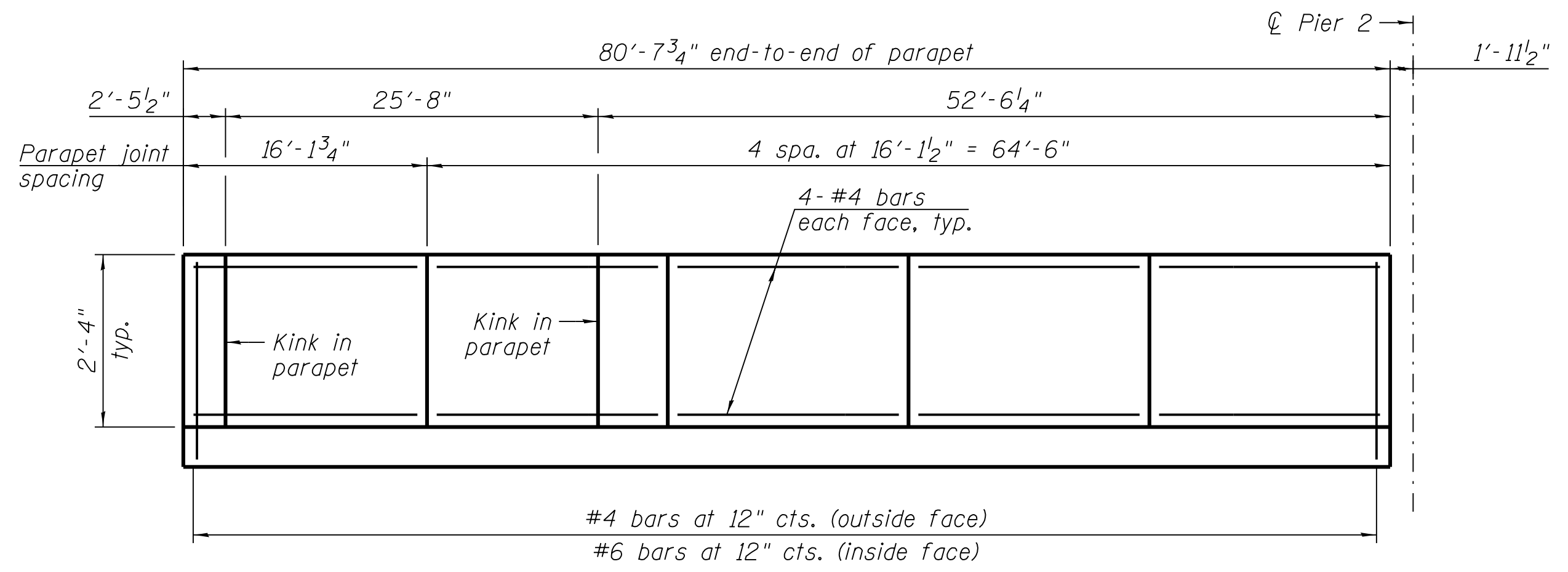
**95TH-S-120**

FILE NAME = 070\_DR095\_S-120.dgn  
 PLOT DATE = 11/28/2016

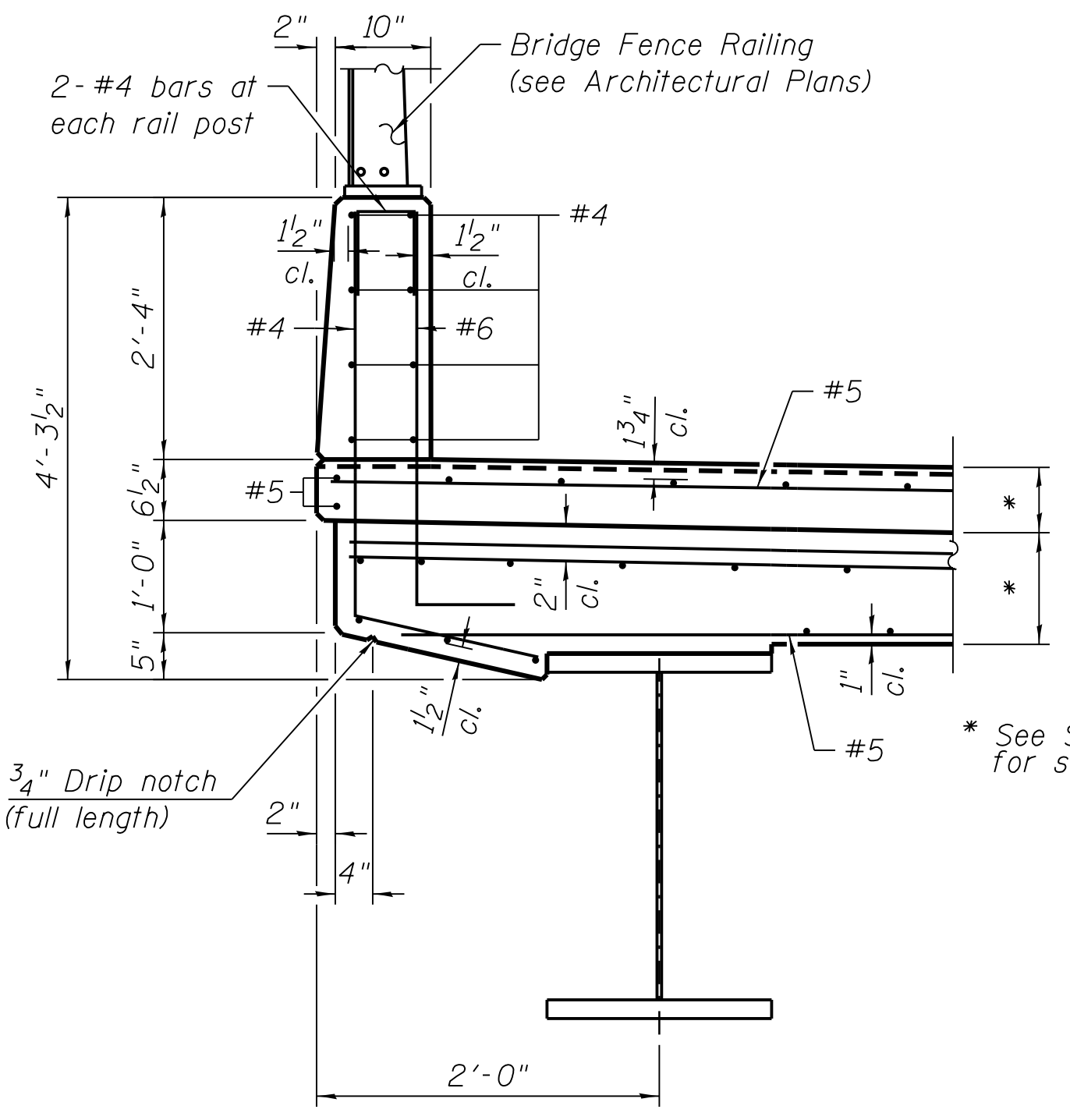
vscarpa



**INSIDE ELEVATION OF NORTH PARAPET**  
(Dimensions shown measured along inside face of parapet looking North)



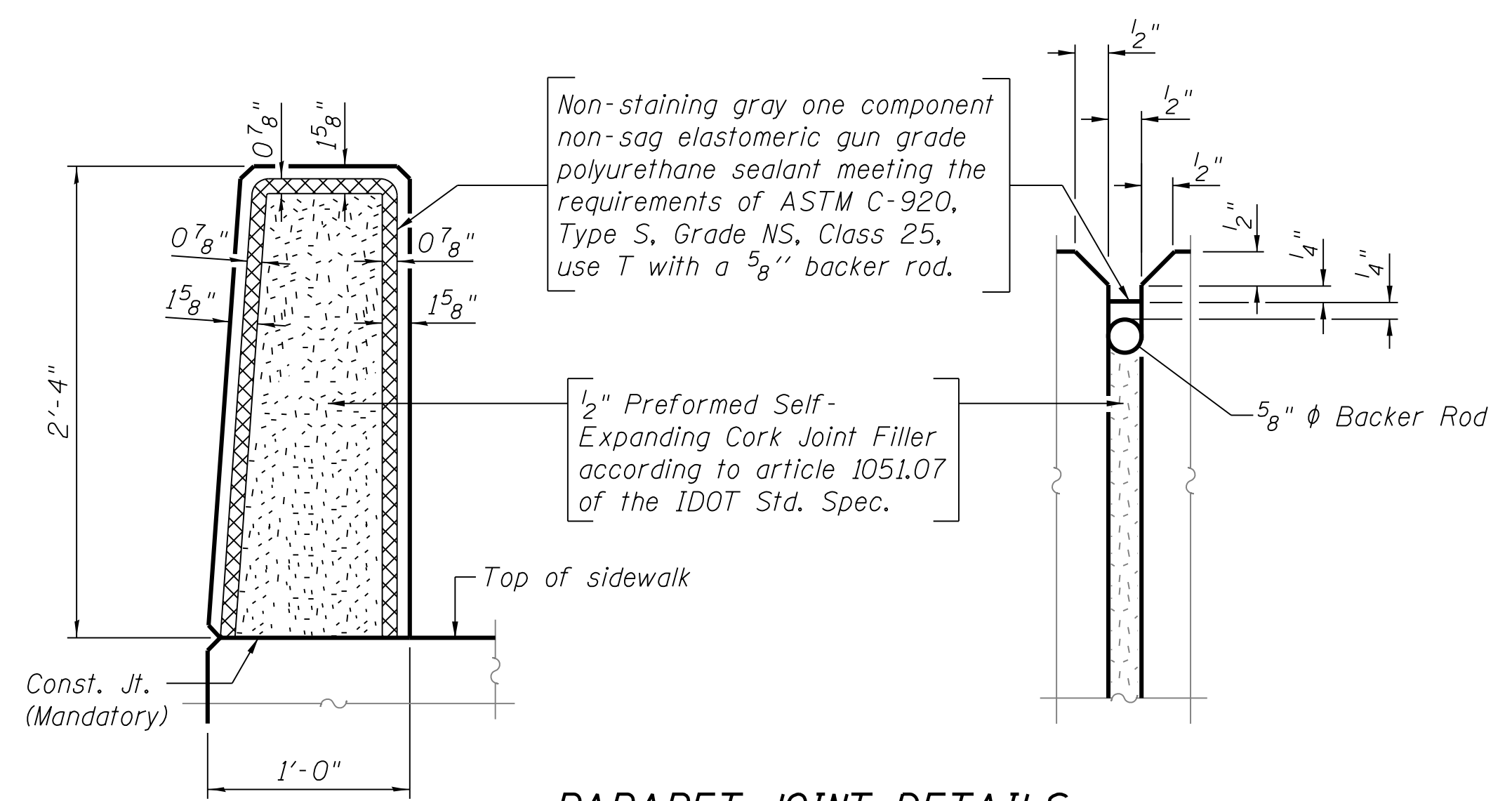
**INSIDE ELEVATION OF SOUTH PARAPET**  
(Dimensions shown measured along inside face of parapet looking South)



MINIMUM BAR LAPS	
BARS	LAPS
#4	2'-7"
#5	3'-3"
#6	3'-10"
#7	5'-2"
#8	6'-9"
#9	8'-7"
#10	10'-10"

\* See Sheets 95TH-S-115 & 95TH-S-116 for slab thickness and sidewalk dimensions.

**MINIMUM BAR LAP**  
(Parapet)  
#4 bar = 2'-0"



**PARAPET JOINT DETAILS**

Note:  
1. For sidewalk reinforcement, see sheets 95TH-S-115 and 95TH-S-116.

FILE NAME = 070\_DR095\_S-121.dgn  
PLOT DATE = 8/31/2016



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**95th Terminal Improvement**  
15 West 95th Street (South Terminal)  
Chicago, IL 60628

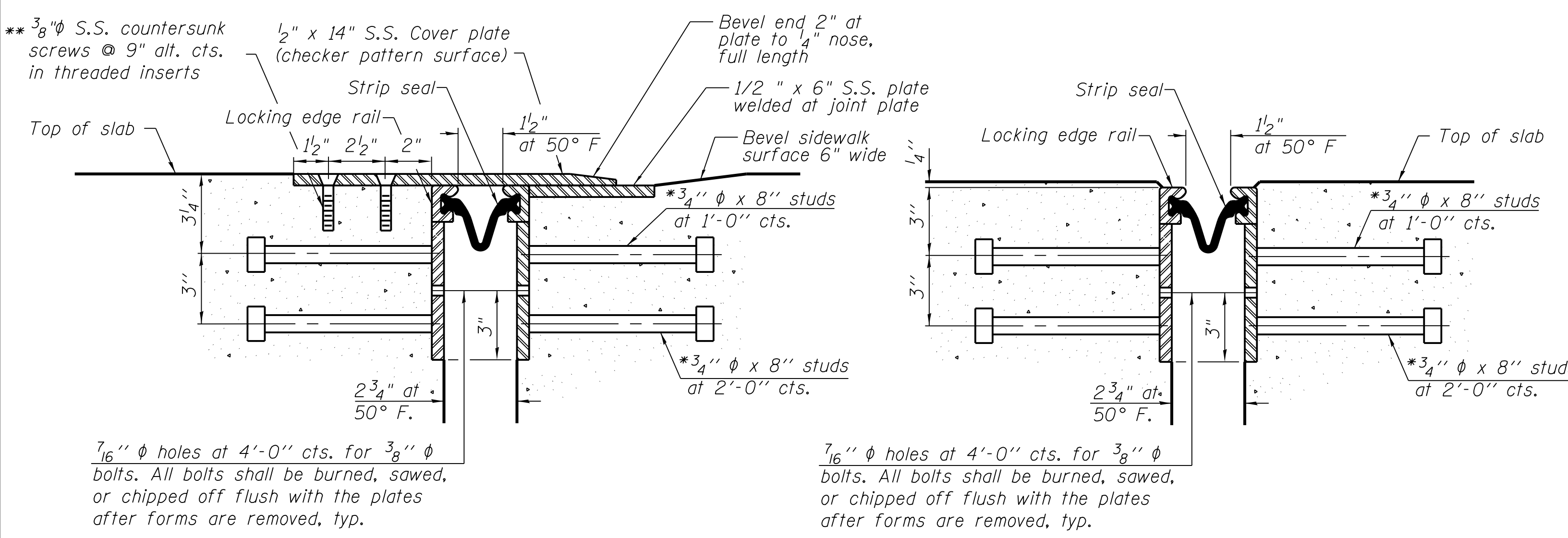
IN CHARGE	S. KADERBEK
APPROVED BY	D.ZROKA
CHECKED BY	P.MAID
DESIGNED BY	L.SOMMER
DRAWN BY	T.SLEDDER
PROJECT NO.	2012-0021
FILE NAME	File Name

MARK	DATE	DESCRIPTION
	2/9/17	ISSUED FOR CONSTRUCTION

LOCATION IDENTIFIER: **DR095**  
95TH ST. BRIDGE  
BRIDGE RAILING DETAILS  
SN 016-0437

**95TH-S-121**

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**SECTION THRU WELDED RAIL JOINT AT SIDEWALK**

**SECTION THRU WELDED RAIL JOINT**

**ROLLED EXTRUDED RAIL WELDED RAIL**

**LOCKING EDGE RAIL SPLICE**

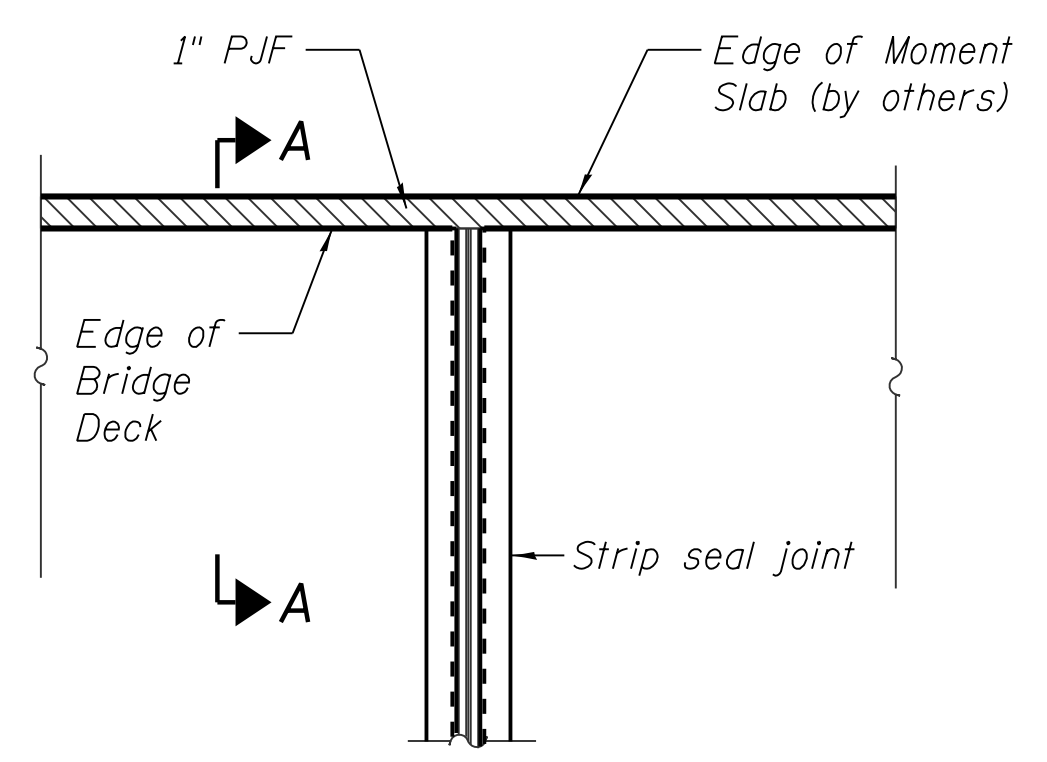
\*\*\* Back gouge not required if complete joint penetration is verified by mock-up.

The inside of the locking edge rail groove shall be free of weld residue.  
Rolled rail shown, welded rail similar.

\*\* At sidewalk only

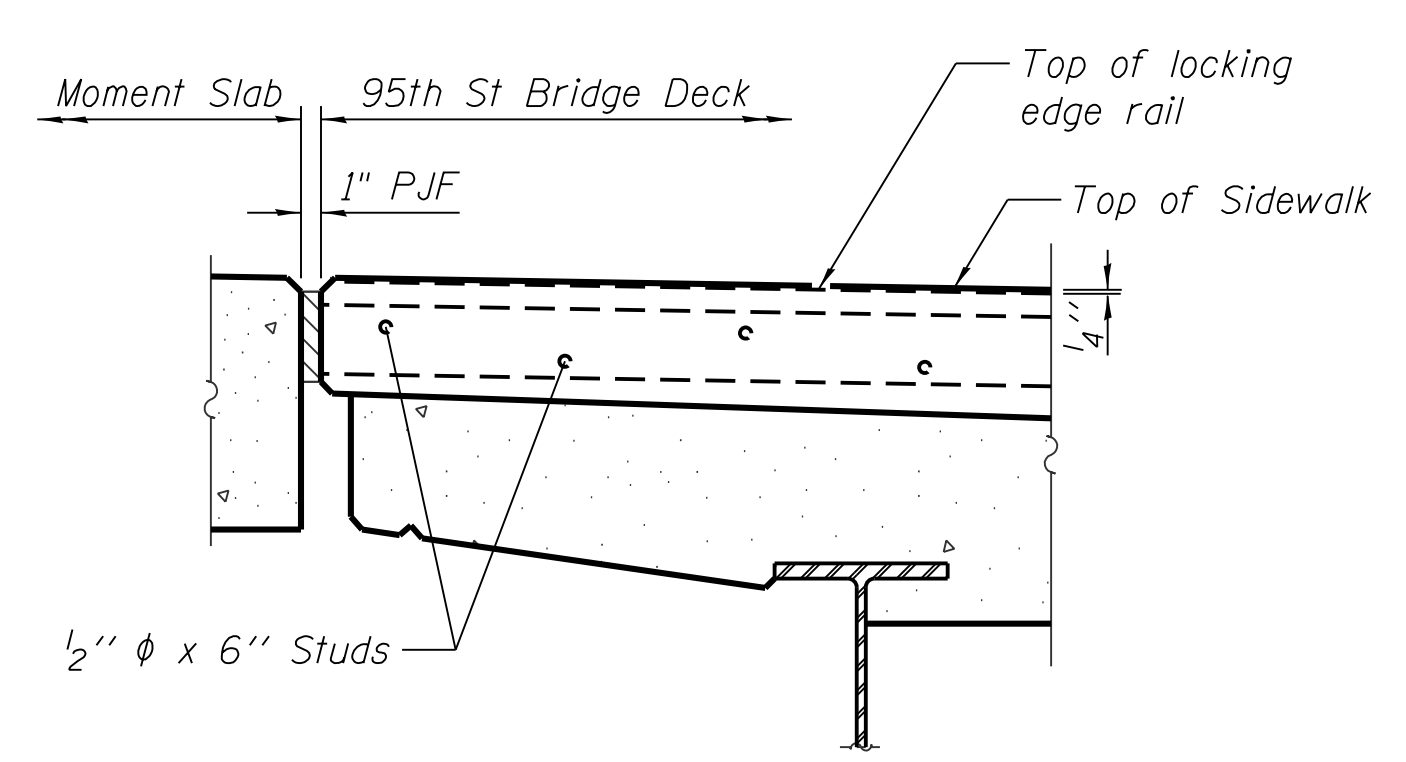
\* Granular or solid flux filled headed studs conforming to Article 1006.32 of the Std. Specs., automatically end welded.

**LOCKING EDGE RAILS**

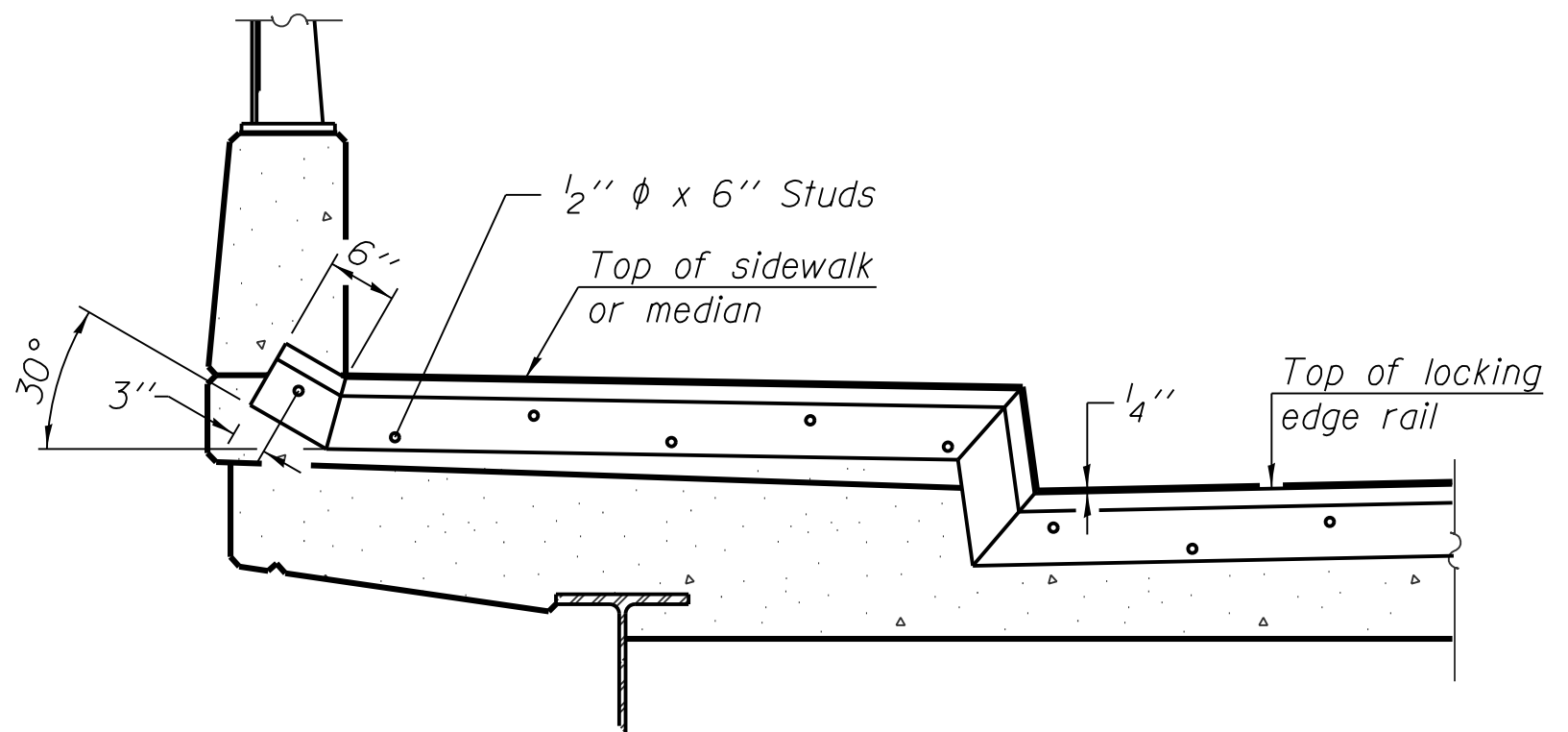


**PLAN**

(East abutment north end shown, similar other locations)



**SECTION A-A**



**TYPICAL END TREATMENT AT SIDEWALK OR MEDIAN**

Shorter plates with a single row of studs at 12\"/>

Notes:

The strip seal shall be made continuous and shall have a minimum thickness of 1/4\"/>

The Locking Edge Rails depicted are conceptual only, except for the minimum dimensions shown. The actual configuration of the Locking Edge Rails and matching strip seal may vary from manufacturer to manufacturer. Flanged edge rails will not be allowed. Locking Edge Rails may be spliced at slope discontinuities and stage construction joints.

The manufacturer's recommended installation methods shall be followed.

The inside of the Locking Edge Rail groove shall be free of weld residue.

The joint opening and deck dimensions detailed on the superstructure are based on a rolled rail expansion joint. If the Contractor elects to use the welded rail expansion joint, the opening and deck dimensions shall be modified according to the dimensions detailed on this sheet at no additional cost.

All stainless steel cover plates to be stainless diamond floor plate conforming to the requirements of ASTM A793, Type 304.

All steel components shall be galvanized after fabrication according to Article 520.03 of the IDOT Standard Specifications.

Maximum space between rail segments shall be 3/6\"/>



**CHICAGO TRANSIT AUTHORITY ENGINEERING**

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**95th Terminal Improvement**  
15 West 95th Street (South Terminal)  
Chicago, IL 60628

IN CHARGE	S. KADERBEK
APPROVED BY	D.ZROKA
CHECKED BY	P.MAID
DESIGNED BY	L.SOMMER
DRAWN BY	T.SLEDGE
PROJECT NO.	2012-0021
FILE NAME	File Name

MARK	DATE	DESCRIPTION
	2/9/17	ISSUED FOR CONSTRUCTION

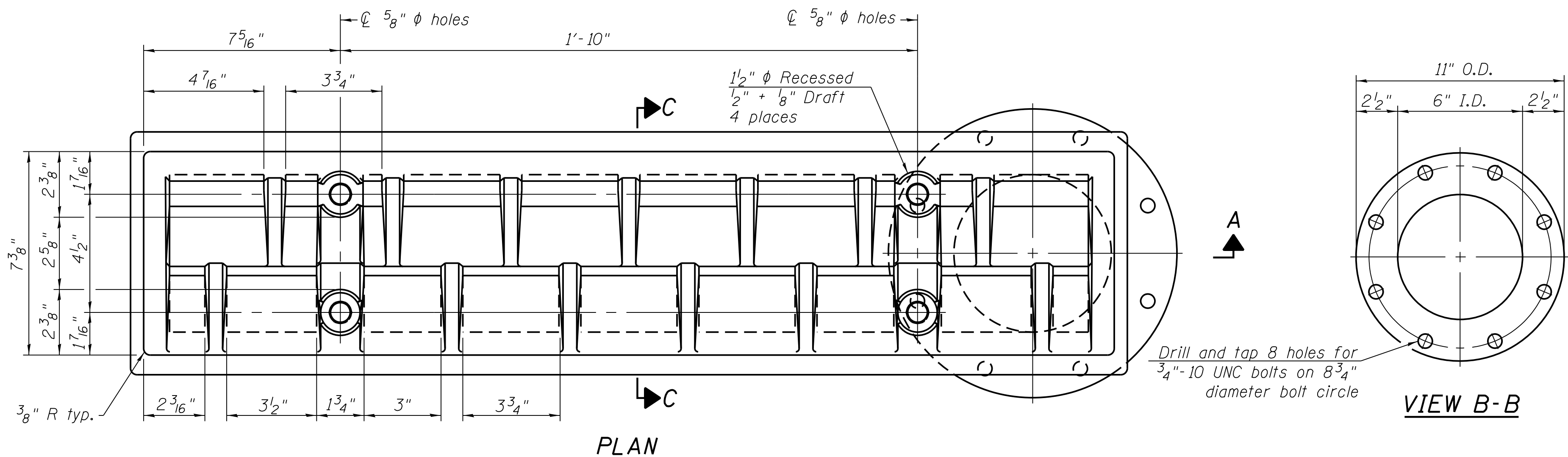
LOCATION IDENTIFIER: **DR095**

**95TH ST. BRIDGE**  
**PREFORMED JOINT STRIP SEAL**  
**SN 016-0437**

**95TH-S-122**

FILE NAME = 070\_DR095\_S-122.dgn  
PLOT DATE = 9/12/2016

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Notes:

Refer to Drainage system specification, section 33 41 10.

All cast iron parts shall be gray iron conforming to the requirements of AASHTO M 105, Class 35B.

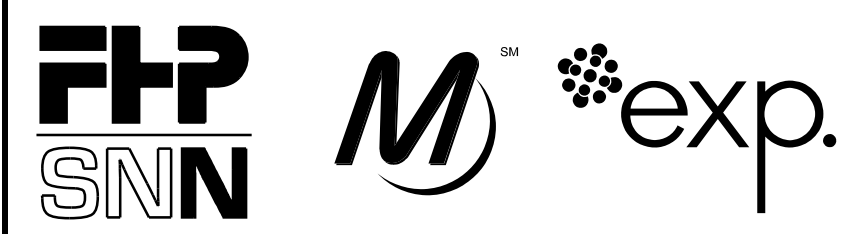
Bolts, anchor studs, washers and nuts shall conform to the requirements of ASTM A 307 and shall be galvanized according to AASHTO M 232.

As an alternate, bolts, anchor studs, washers and nuts may be stainless steel according to Article 1006.29(d) of the IDOT Standard Specifications.

Structural steel weldments of equal sections and of the same configuration may be substituted for the cast iron scupper frame. Fillet or full penetration welds shall be used for the weldments. Details shall be submitted to the Engineer for approval. Structural steel weldments shall not be substituted for the cast iron scupper grate. Structural steel frames and downspouts shall be galvanized according to AASHTO M111.

The Contractor shall take appropriate measures to assure that Protective Coat is not applied to the scupper.

Alternate fiberglass downspout conforming to ASTM D 2996 with a short-time rupture strength hoop tensile stress of 30,000 psi min. may be used in lieu of the cast iron or steel equivalent.



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Chicago, IL 60628

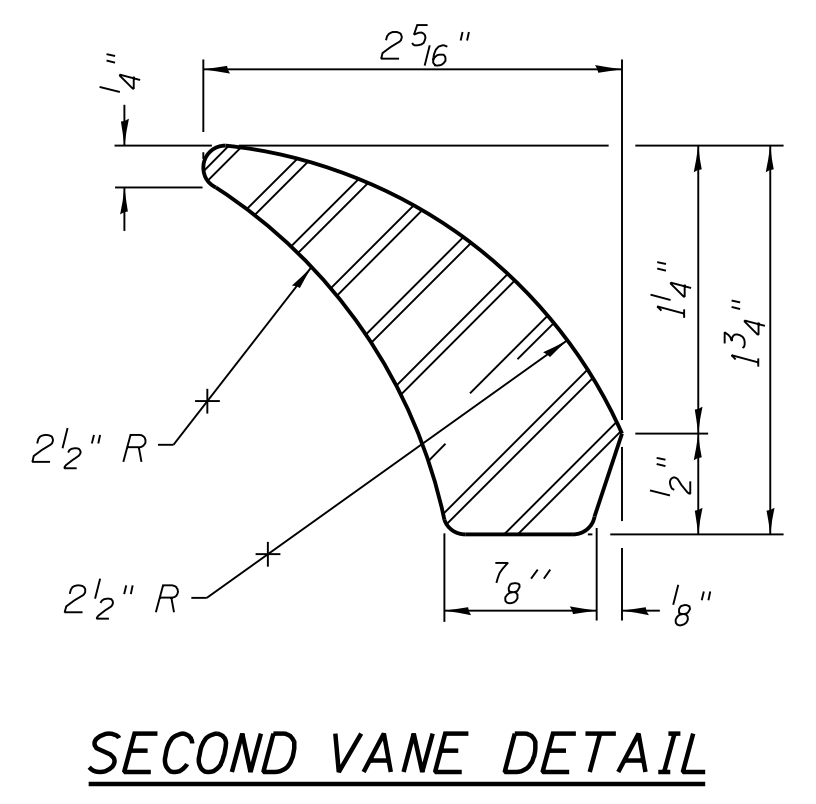
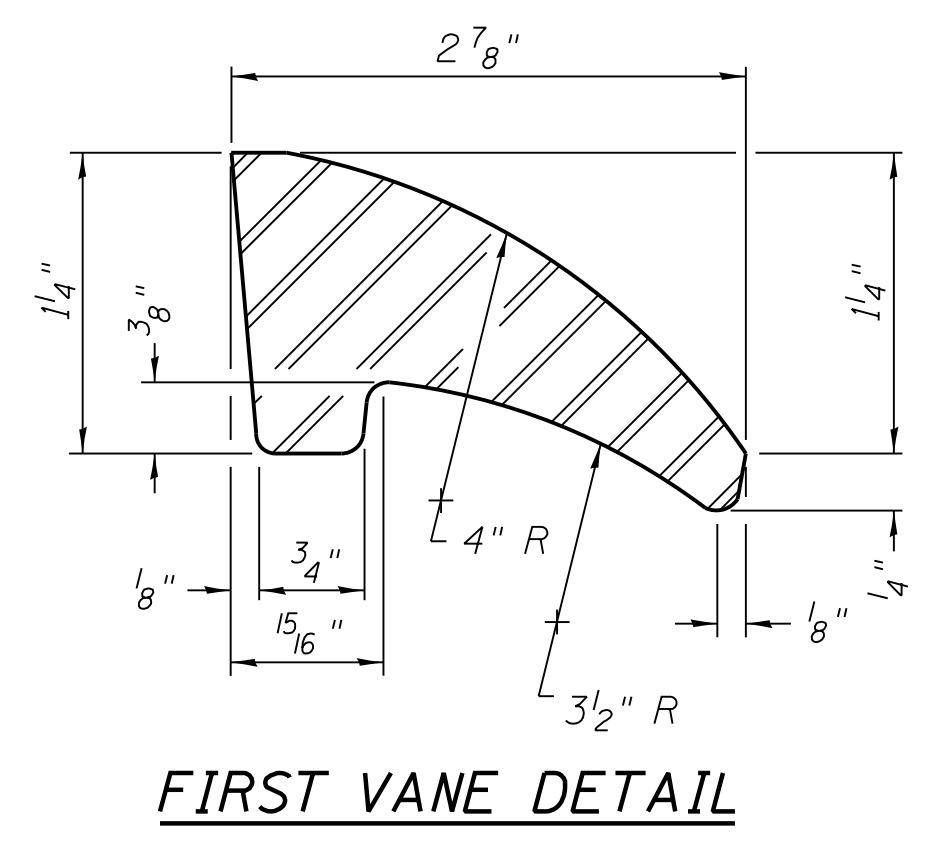
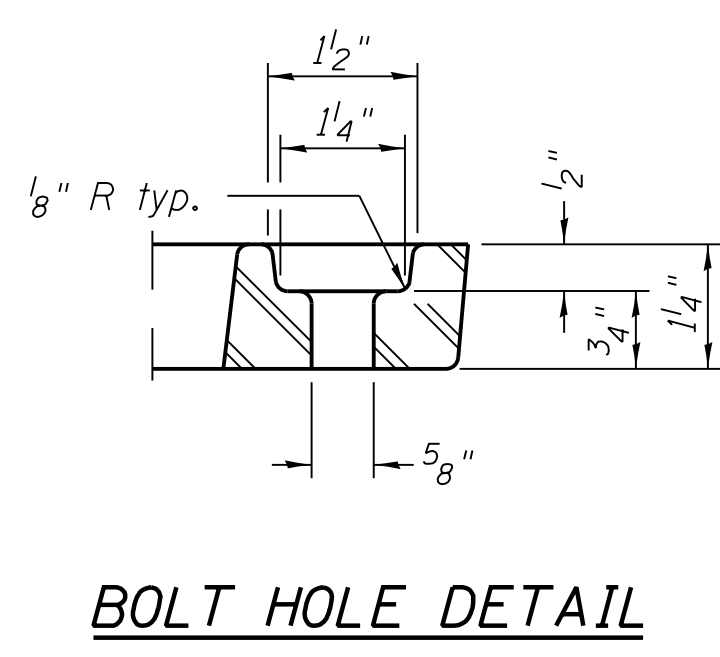
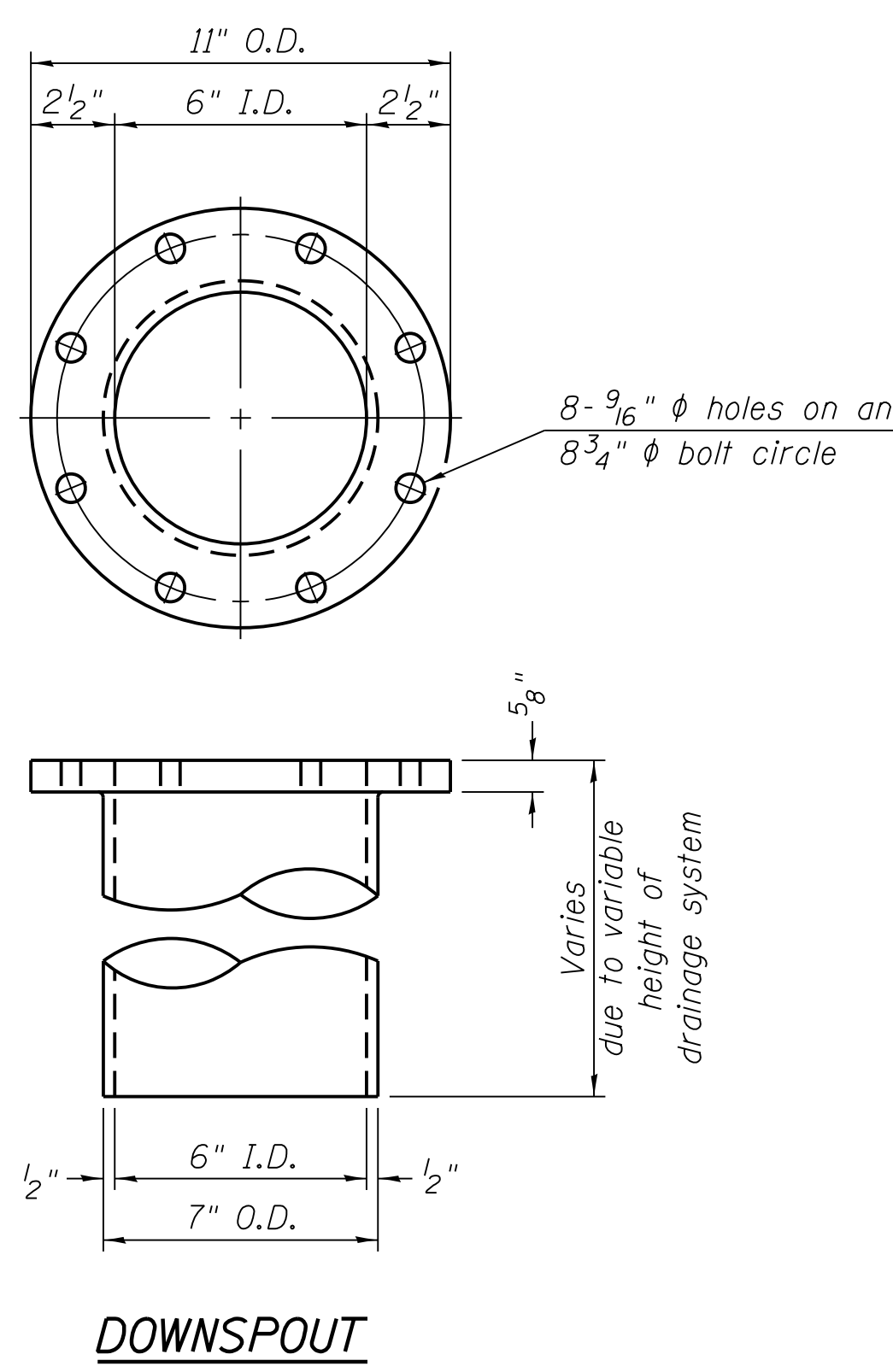
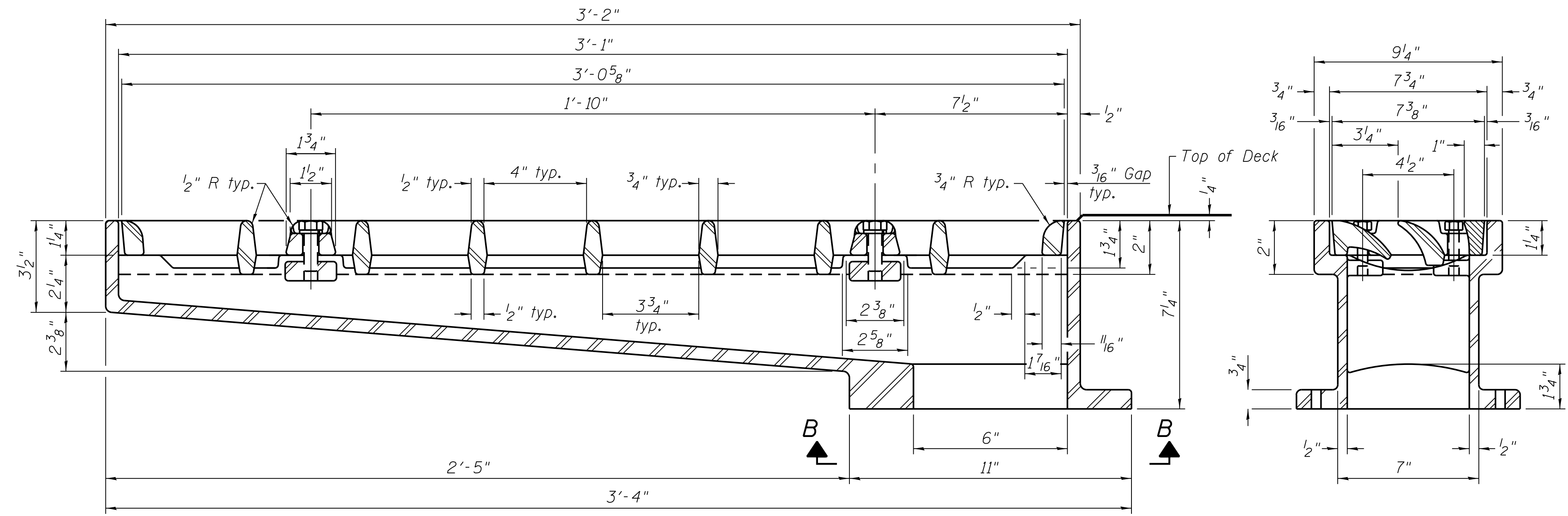
IN CHARGE	S. KADERBEK
APPROVED BY	D.ZROKA
CHECKED BY	P.MAID
DESIGNED BY	L.SOMMER
DRAWN BY	T.SLEDGE
PROJECT NO.	2012-0021
FILE NAME	File Name

2/9/17	ISSUED FOR CONSTRUCTION	
MARK	DATE	DESCRIPTION

LOCATION IDENTIFIER: **DR095**

95TH ST. BRIDGE  
DRAINAGE SCUPPER, DS-33  
SN 016-0437

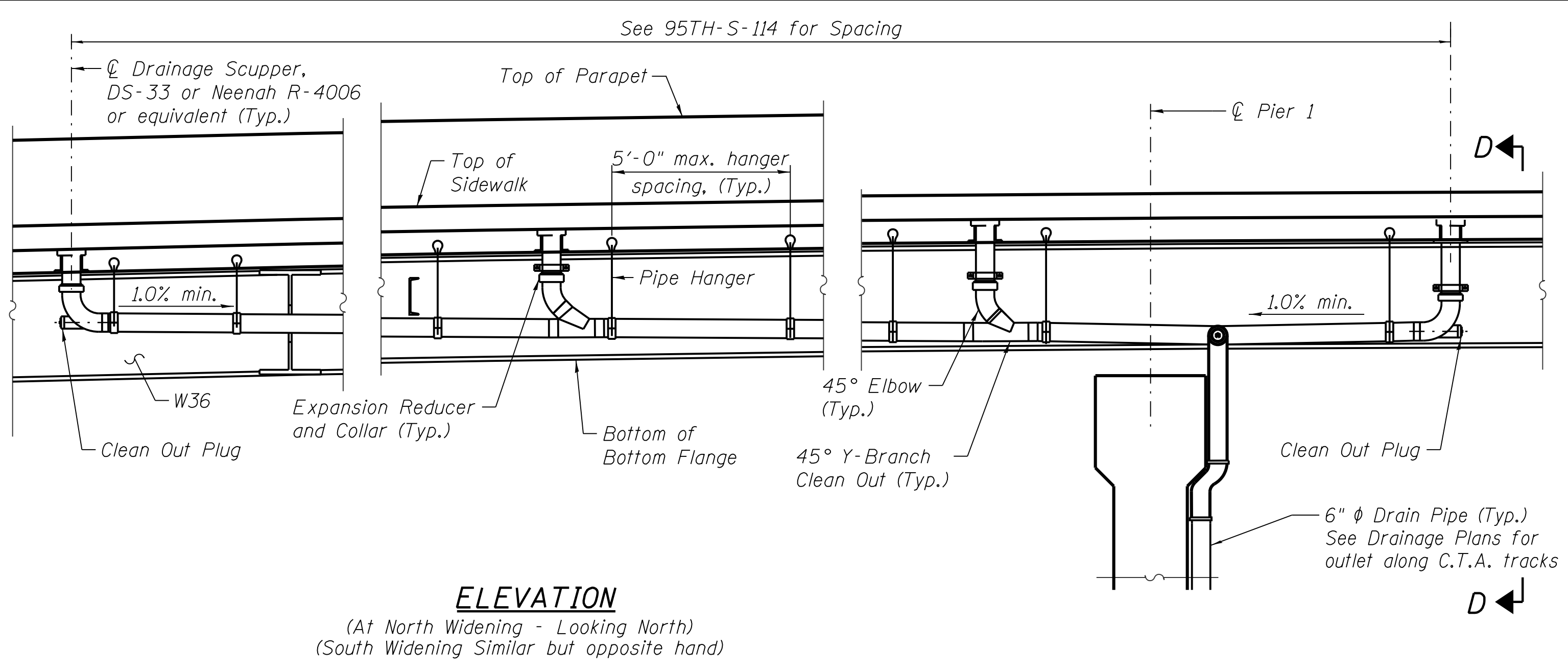
**95TH-S-123**



FILE NAME = 070\_DR095\_S-123.dgn  
PLOT DATE = 8/31/2016

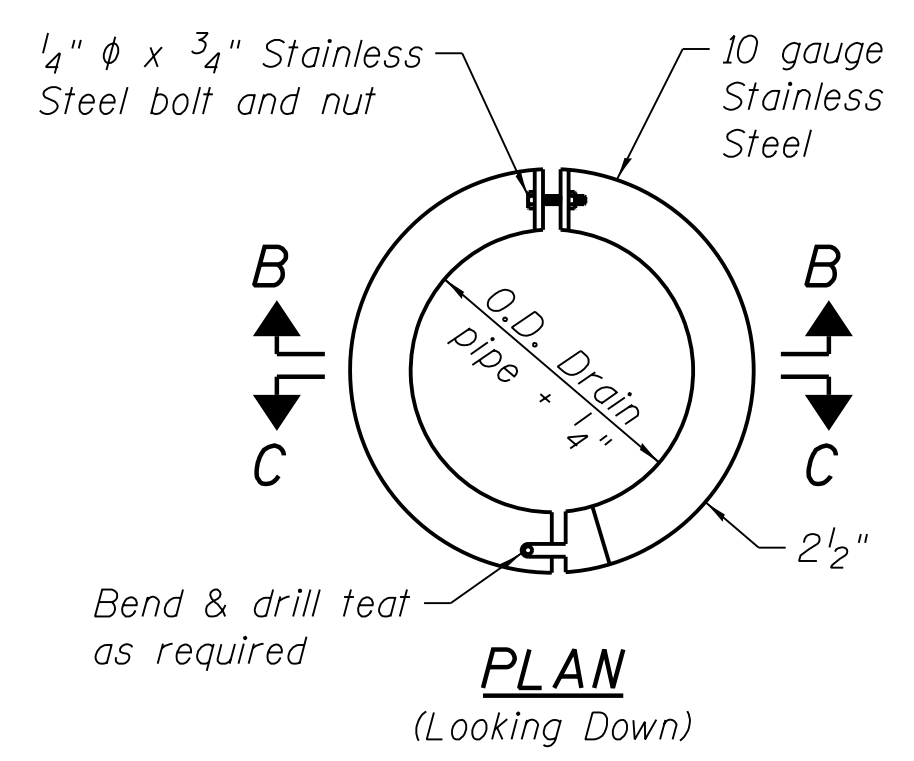


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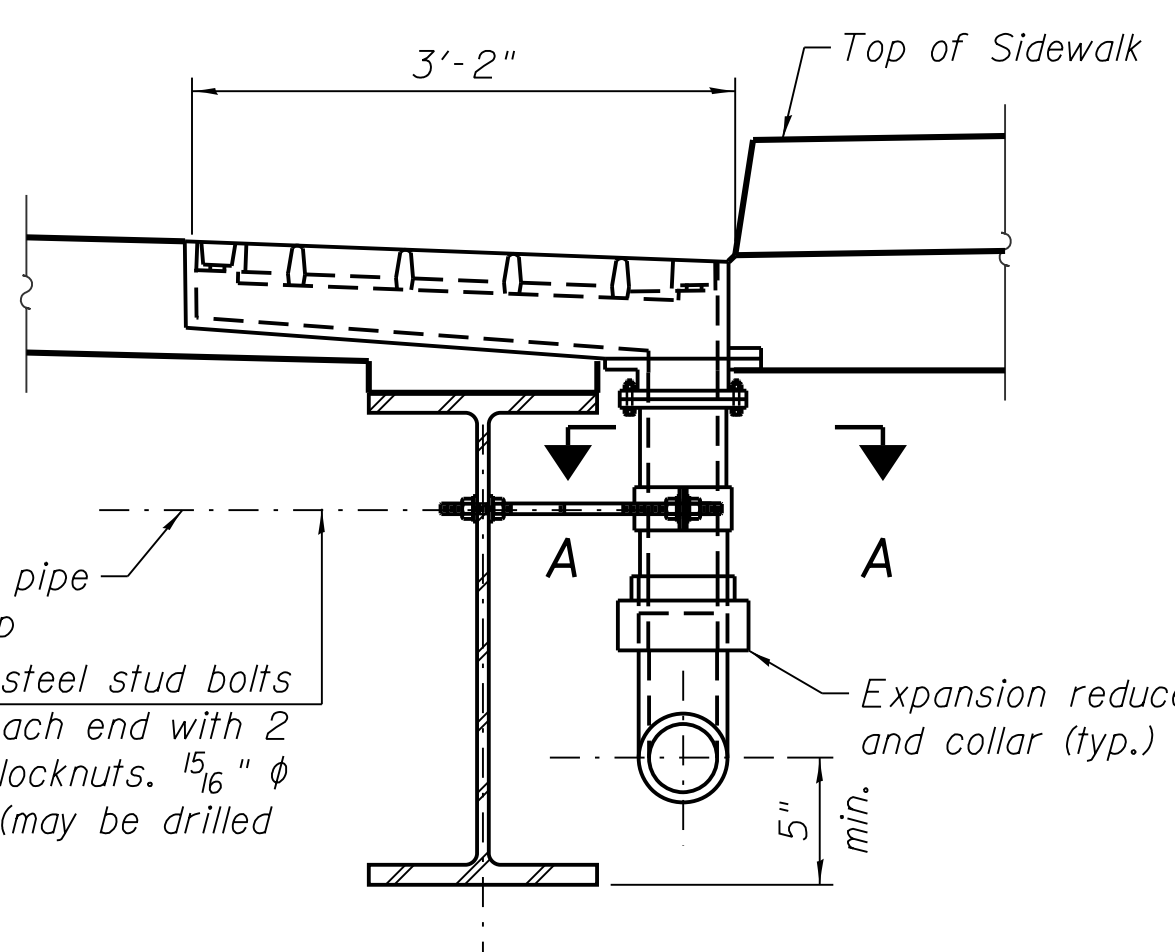
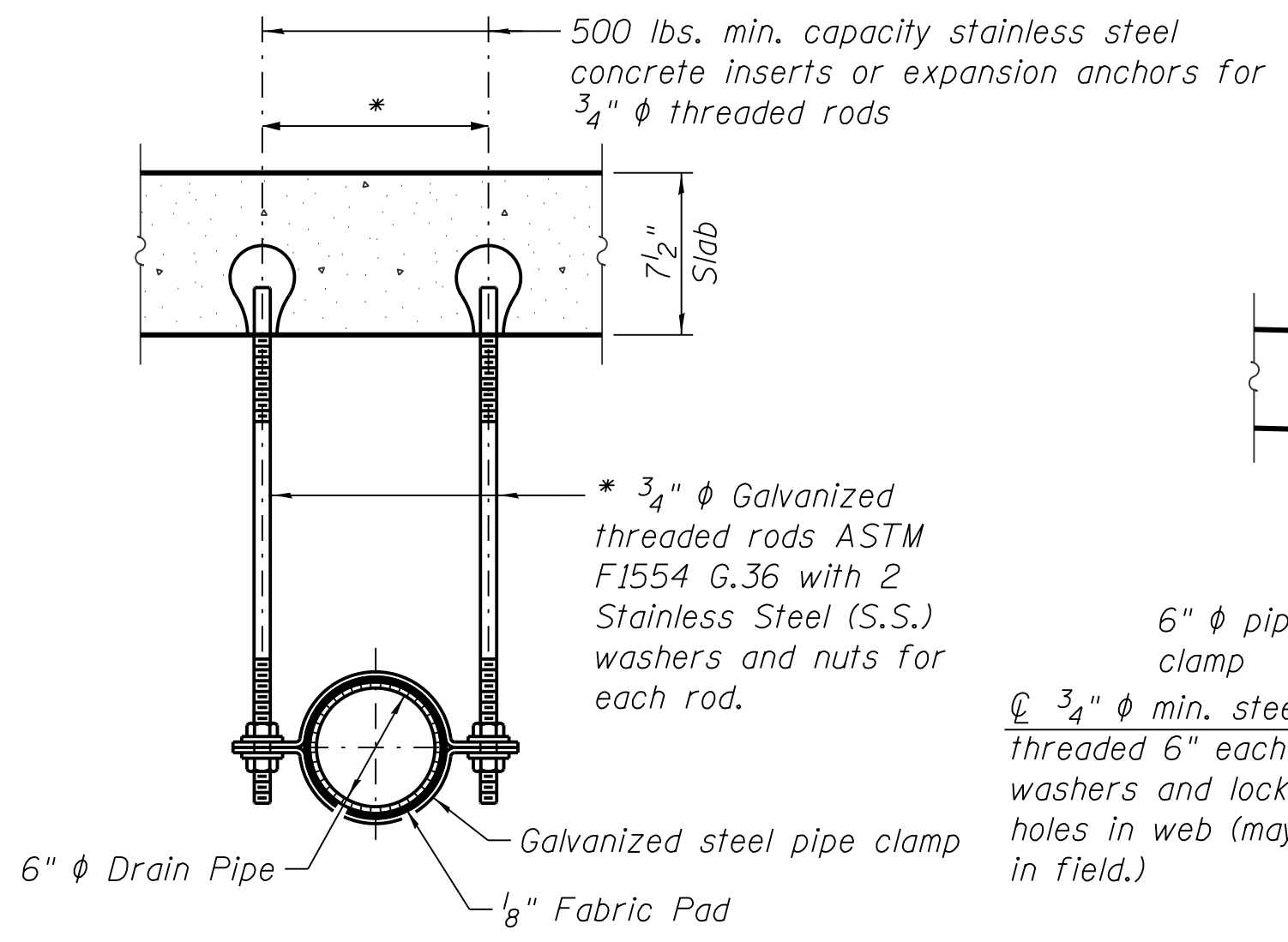
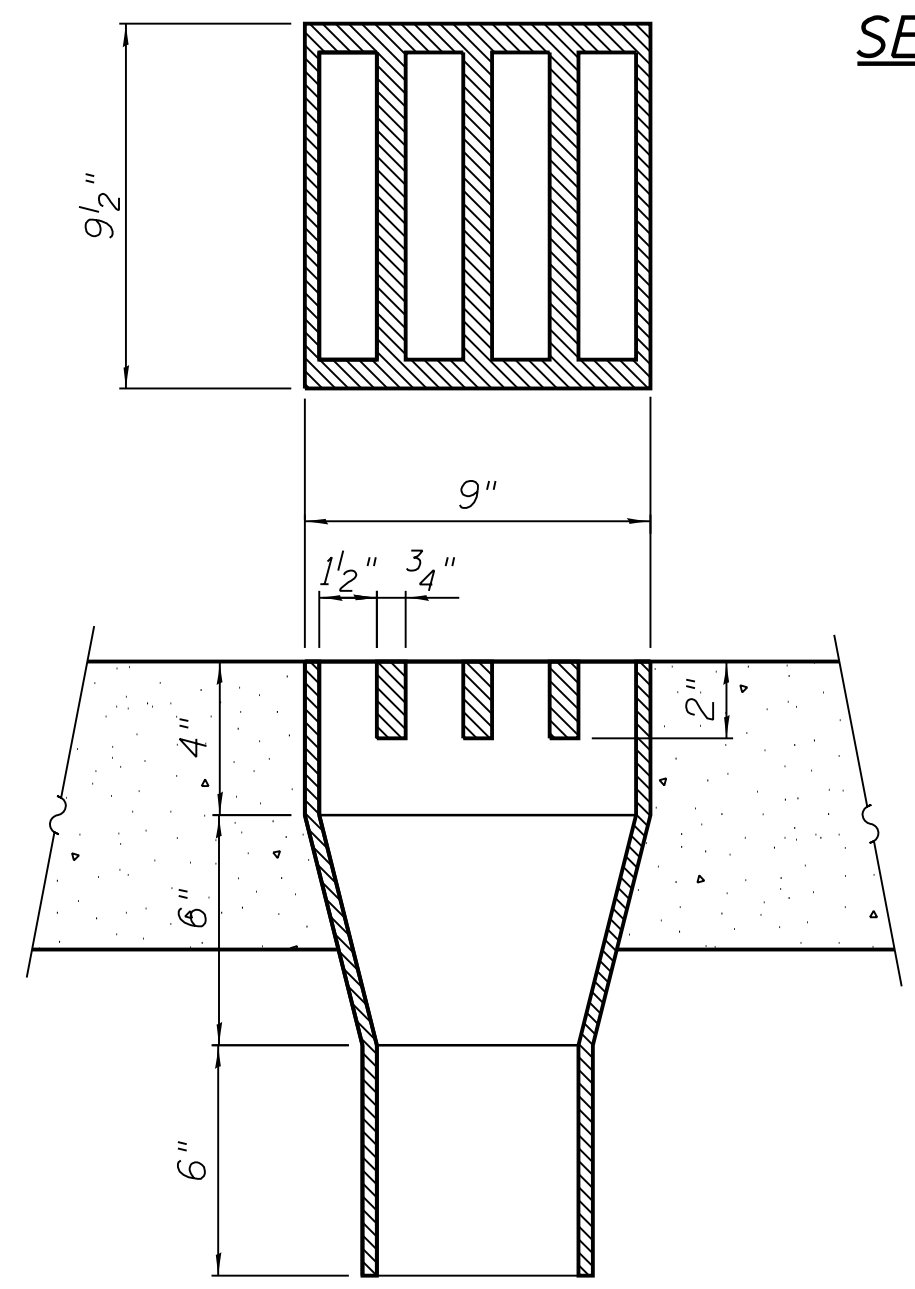
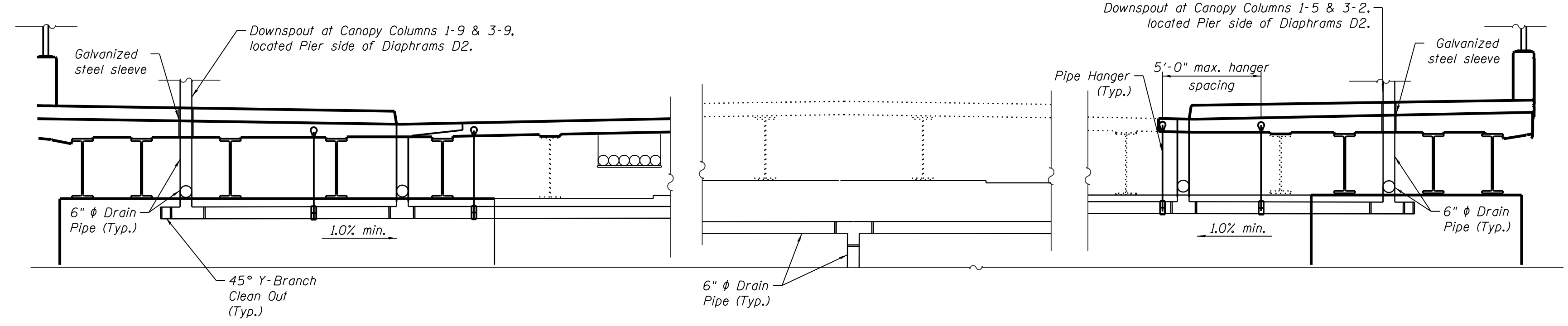
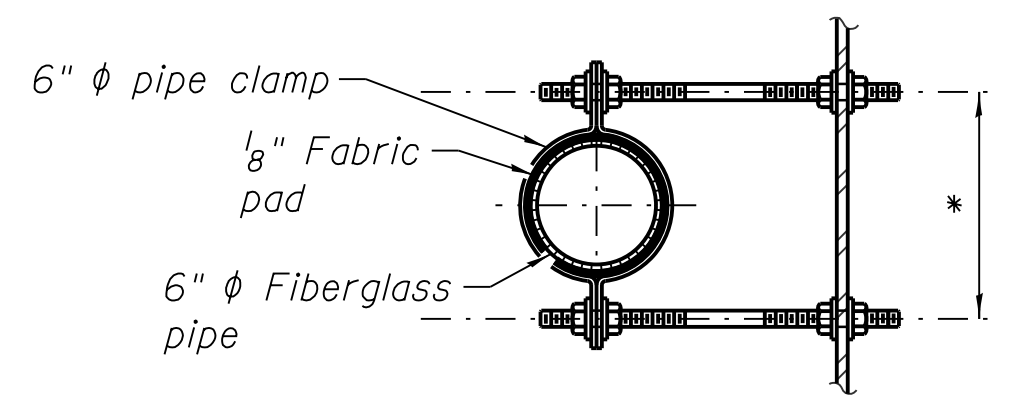
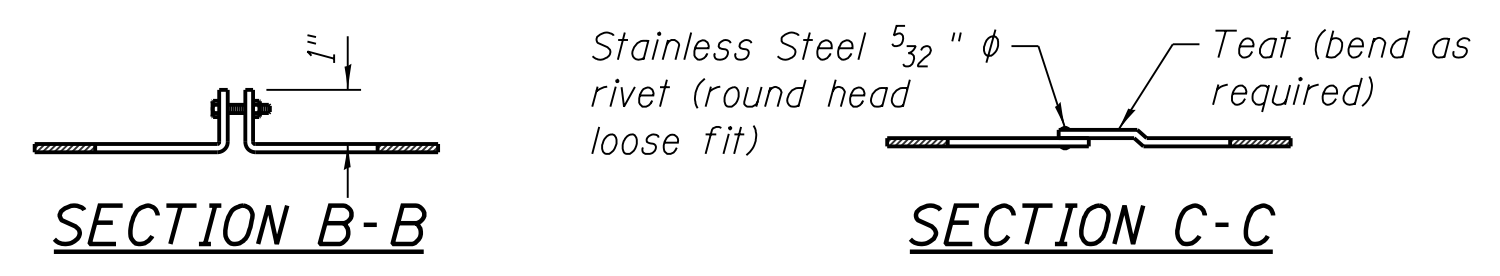


**ELEVATION**

(At North Widening - Looking North)  
(South Widening Similar but opposite hand)



**EXPANSION COLLAR DETAILS**



**Notes:**

- See sheet S-123 for drainage scupper details.
- Scuppers & bridge drainage system shall be located clear of all diaphragms.
- Fiberglass pipe shall conform to ASTM D 2996, RTRP short-time rupture strength hoop tensile stress of 30,000 p.s.i. minimum.
- Pipe supports shall be provided on all pipes at 5'-0" max. spacing.
- All pipe hangers, supports and hardware shall be hot-dipped galvanized in accordance with AASHTO M232 (ASTM A153).
- All bolts, nuts and washers shall be Stainless Steel, Type 304.
- All exposed surfaces of scuppers, hangers, brackets and piping shall be painted.
- Refer to Drainage system specification, section 33 41 10.

FILE NAME = 070\_DR095\_S-124.dgn  
PLOT DATE = 9/12/2016



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Chicago, IL 60628

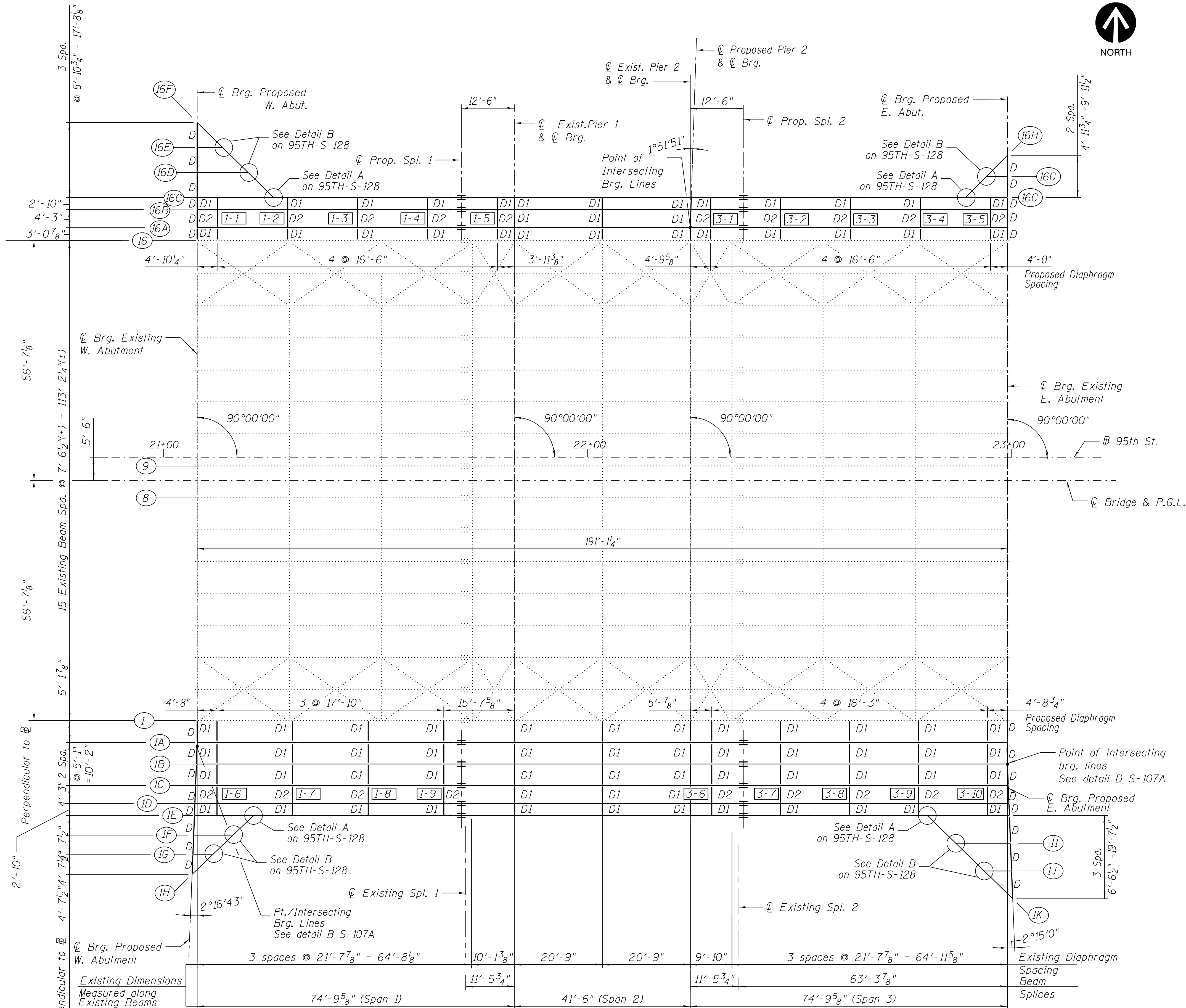
IN CHARGE	S. KADERBEK
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PROJECT NO.	2012-0021
FILE NAME	File Name

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LOCATION IDENTIFIER: **DR095**  
95TH ST. BRIDGE  
CLOSED DRAINAGE SYSTEM  
SN 016-0437

**95TH-S-124**

FILE NAME = 070\_DR095\_S-125.dgn  
 PLOT DATE = 8/31/2016



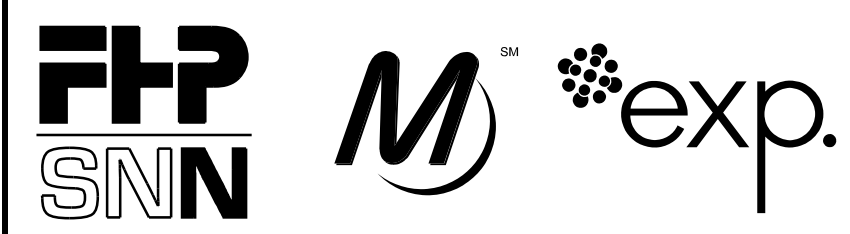
PLAN

CANOPY LOCATION	STATION	NORTH OFFSET
1-1	21+12.75	-56.580
1-2	21+29.25	-56.580
1-3	21+45.75	-56.580
1-4	21+62.25	-56.580
1-5	21+78.75	-56.580
1-6	21+12.57	79.580
1-7	21+30.40	79.580
1-8	21+48.24	79.580
1-9	21+66.07	79.580
3-1	22+29.00	-56.580
3-2	22+45.50	-56.580
3-3	22+62.00	-56.580
3-4	22+78.50	-56.580
3-5	22+95.00	-56.580
3-6	22+29.27	79.580
3-7	22+45.52	79.580
3-8	22+61.77	79.580
3-9	22+78.02	79.580
3-10	22+94.27	79.580

Note:  
 1) Station and Offset with respect to 95th Street Baseline.

**LEGEND**

- ..... Existing members to remain
- New members
- I [Canopy No.]



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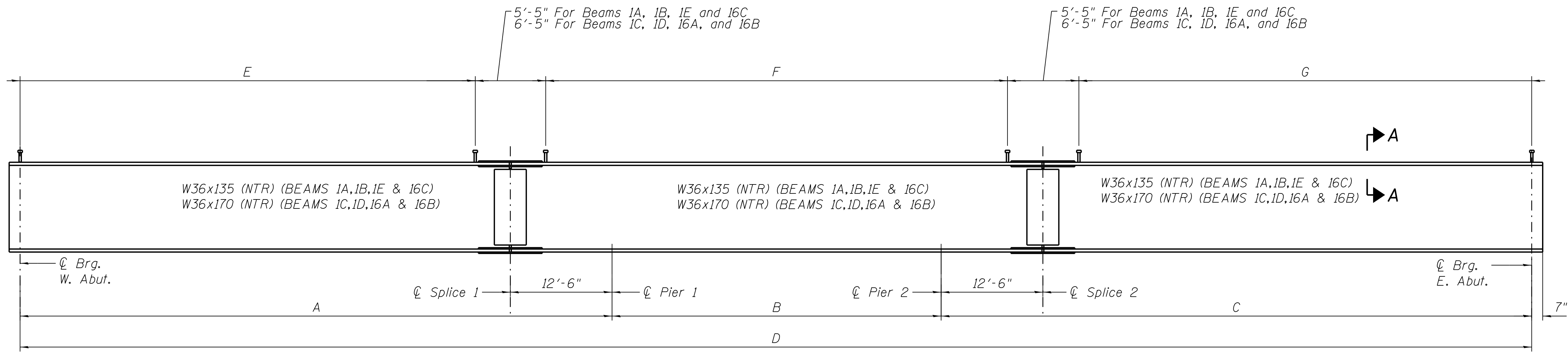
IN CHARGE	S. KADERBEK
APPROVED BY	D.ZROKA
CHECKED BY	P.MAID
DESIGNED BY	L.SOMMER
DRAWN BY	T.SLEDGE
PROJECT NO.	2012-0021
FILE NAME	File Name

MARK	DATE	DESCRIPTION
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LOCATION IDENTIFIER: **DR095**  
 95TH ST. BRIDGE  
 FRAMING PLAN  
 SN 016-0437

**95TH-S-125**

vscarp



**BEAM ELEVATION**  
(Beams 1A thru 1E, 16A thru 16C)  
Scale: N.T.S.

**BEAM DIMENSIONS**

Beam No.	Shear Stud Spacing						
	A	B	C	D	E	F	G
1E	75'-5 <sup>5</sup> / <sub>8</sub> "	41'-6"	75'-3 <sup>1</sup> / <sub>16</sub> "	192'-2 <sup>11</sup> / <sub>16</sub> "	104 sp @ 7" (-) = 60'-3 <sup>3</sup> / <sub>8</sub> "	105 sp @ 7" (-) = 61'-1"	103 sp @ 7" (-) = 60'-0 <sup>9</sup> / <sub>16</sub> "
1D	75'-4 <sup>1</sup> / <sub>16</sub> "	41'-6"	75'-1 <sup>5</sup> / <sub>16</sub> "	192'-0"	104 sp @ 7" (-) = 60'-1 <sup>9</sup> / <sub>16</sub> "	105 sp @ 7" (-) = 61'-1"	103 sp @ 7" (-) = 59'-11 <sup>7</sup> / <sub>16</sub> "
1C	75'-2"	41'-6"	74'-11 <sup>5</sup> / <sub>16</sub> "	191'-7 <sup>15</sup> / <sub>16</sub> "	102 sp @ 7" (-) = 59'-5 <sup>1</sup> / <sub>2</sub> "	103 sp @ 7" (-) = 60'-1"	102 sp @ 7" (-) = 59'-3 <sup>7</sup> / <sub>16</sub> "
1B	74'-11 <sup>9</sup> / <sub>16</sub> "	41'-6"	74'-9 <sup>9</sup> / <sub>16</sub> "	191'-3 <sup>1</sup> / <sub>8</sub> "	102 sp @ 7" (-) = 59'-3 <sup>1</sup> / <sub>16</sub> "	103 sp @ 7" (-) = 60'-1"	102 sp @ 7" (-) = 59'-1 <sup>1</sup> / <sub>16</sub> "
1A	74'-9 <sup>5</sup> / <sub>8</sub> "	41'-6"	74'-9 <sup>5</sup> / <sub>8</sub> "	191'-1 <sup>1</sup> / <sub>4</sub> "	103 sp @ 7" (-) = 59'-7 <sup>1</sup> / <sub>8</sub> "	105 sp @ 7" (-) = 61'-1"	103 sp @ 7" (-) = 59'-7 <sup>1</sup> / <sub>8</sub> "
16A	74'-9 <sup>5</sup> / <sub>8</sub> "	41'-6"	74'-9 <sup>5</sup> / <sub>8</sub> "	191'-1 <sup>1</sup> / <sub>4</sub> "	102 sp @ 7" (-) = 59'-1 <sup>1</sup> / <sub>8</sub> "	103 sp @ 7" (-) = 60'-1"	102 sp @ 7" (-) = 59'-1 <sup>1</sup> / <sub>8</sub> "
16B	74'-9 <sup>5</sup> / <sub>8</sub> "	41'-7 <sup>5</sup> / <sub>8</sub> "	74'-8"	191'-1 <sup>1</sup> / <sub>4</sub> "	102 sp @ 7" (-) = 59'-1 <sup>1</sup> / <sub>8</sub> "	104 sp @ 7" (-) = 60'-2 <sup>5</sup> / <sub>8</sub> "	101 sp @ 7" (-) = 58'-11 <sup>1</sup> / <sub>2</sub> "
16C	74'-9 <sup>5</sup> / <sub>8</sub> "	41'-8 <sup>3</sup> / <sub>4</sub> "	74'-6 <sup>7</sup> / <sub>8</sub> "	191'-1 <sup>1</sup> / <sub>4</sub> "	103 sp @ 7" (-) = 59'-7 <sup>1</sup> / <sub>8</sub> "	106 sp @ 7" (-) = 61'-3 <sup>3</sup> / <sub>4</sub> "	102 sp @ 7" (-) = 59'-4 <sup>3</sup> / <sub>8</sub> "

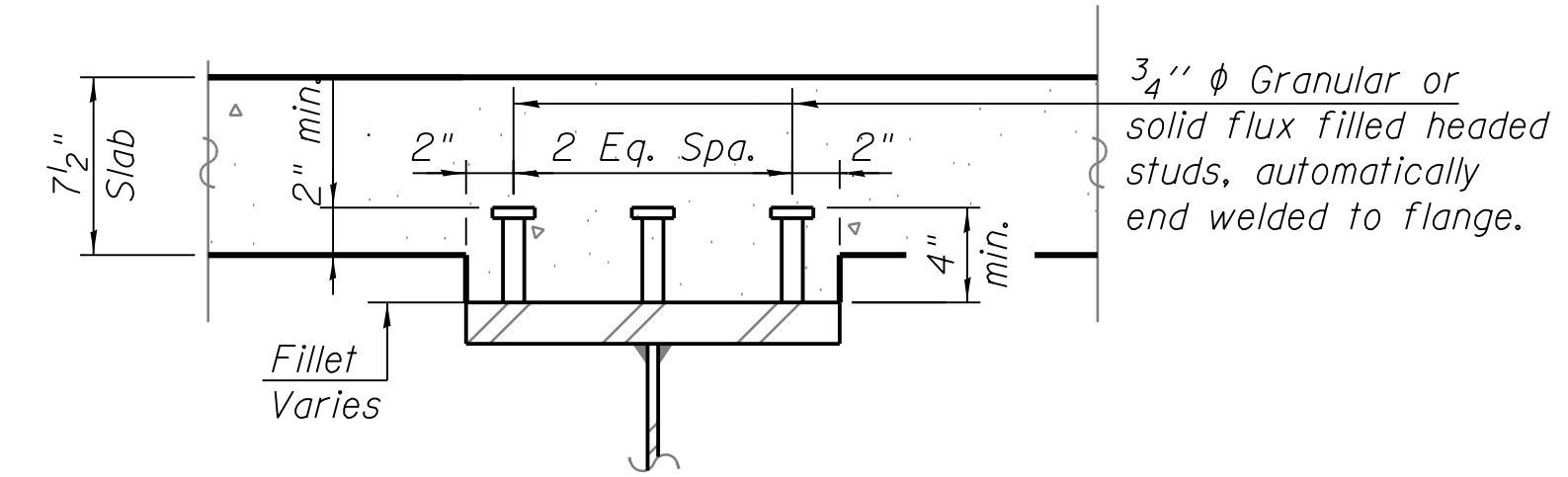
All dimensions are measured along the centerline of beam.

**TOP OF BEAM ELEVATIONS**  
(For Fabrication only)

Beam No.	∅ Brg. W Abut.	∅ Field Splice #1	∅ Pier 1	∅ Pier 2	∅ Field Splice #2	∅ Brg. E Abut.
1A	16.307	17.439	17.439	17.440	17.440	16.309
1B	16.257	17.394	17.394	17.395	17.395	16.264
1C	16.251	17.394	17.394	17.395	17.395	16.258
1D	16.247	17.394	17.394	17.395	17.395	16.253
1E	16.243	17.394	17.394	17.395	17.395	16.251
16A	16.466	17.640	17.640	17.639	17.639	16.426
16B	16.466	17.640	17.639	17.638	17.638	16.426
16C	16.466	17.640	17.639	17.637	17.637	16.426

Beam No.	∅ Brg. W Abut.	End
1F	16.238	16.488
1G	16.233	16.361
1H	16.228	16.607
16D	16.466	16.766
16E	16.466	16.616
16F	16.466	16.915

Beam No.	Start	∅ Brg. E Abut.
1I	16.582	16.243
1J	16.412	16.236
1K	16.739	16.229
16G	16.571	16.426
16H	16.708	16.426



**SECTION A-A**

Notes:

- Load carrying components designated "NTR" shall conform to the Impact Testing requirement, Zone 2.
- Shear Studs shall not be placed on top of connection plate for diaphragm D2.



**CHICAGO TRANSIT AUTHORITY ENGINEERING**

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**95th Terminal Improvement**  
15 West 95th Street (South Terminal)  
Chicago, IL 60628

IN CHARGE	S. KADERBEK
APPROVED BY	D.ZROKA
CHECKED BY	P.MAID
DESIGNED BY	L.SOMMER
DRAWN BY	T.SLEDGE
PROJECT NO.	2012-0021
FILE NAME	File Name

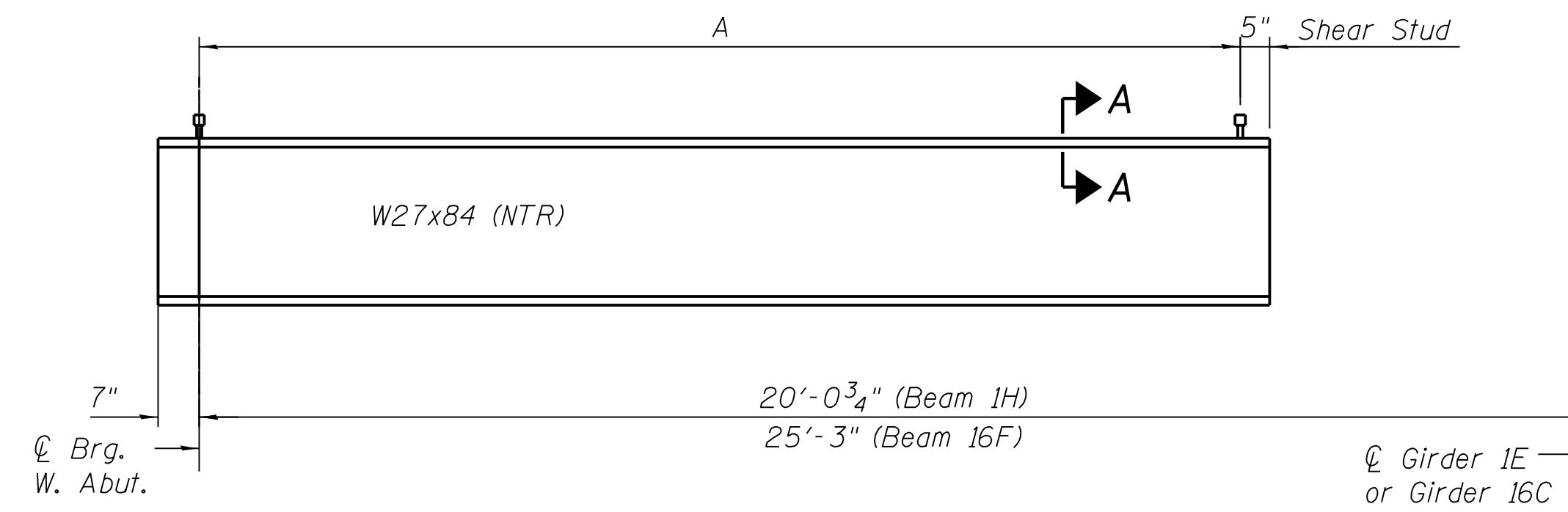
MARK	DATE	DESCRIPTION
	2/9/17	ISSUED FOR CONSTRUCTION

LOCATION IDENTIFIER: **DR095**  
95TH ST. BRIDGE  
STRUCTURAL STEEL DETAILS  
(SHEET 1 OF 5)  
SN 016-0437

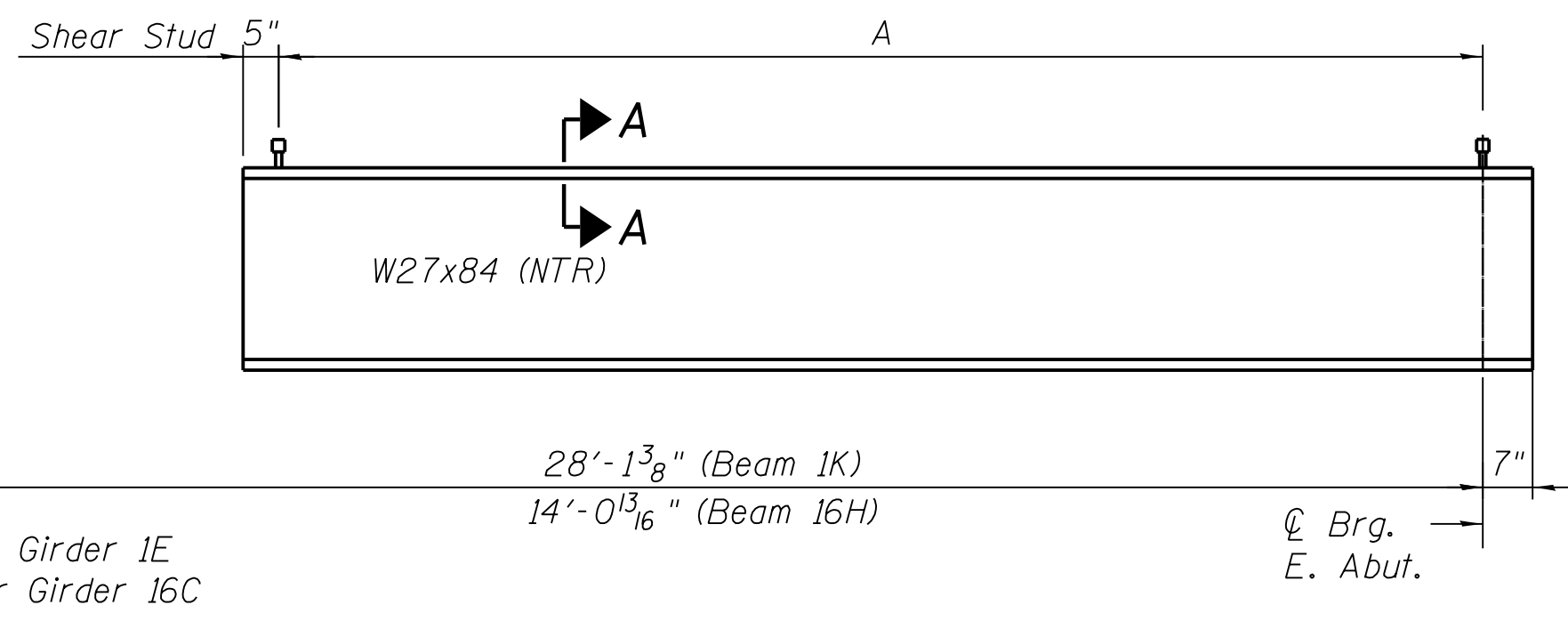
**95TH-S-126**

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PLOT DATE = 8/31/2016

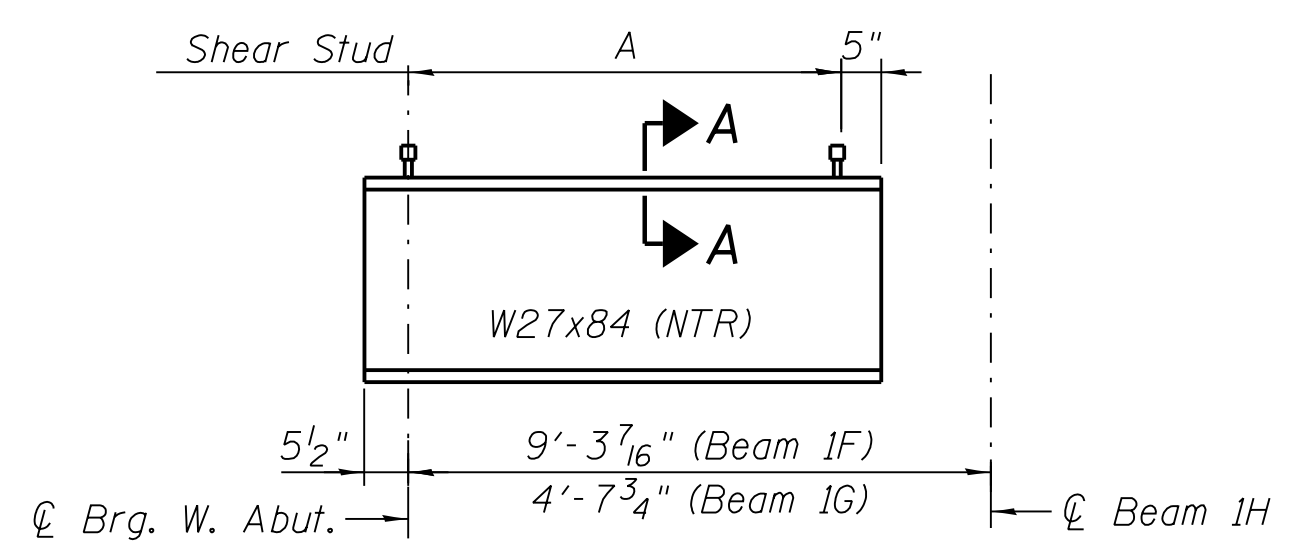
vscarp



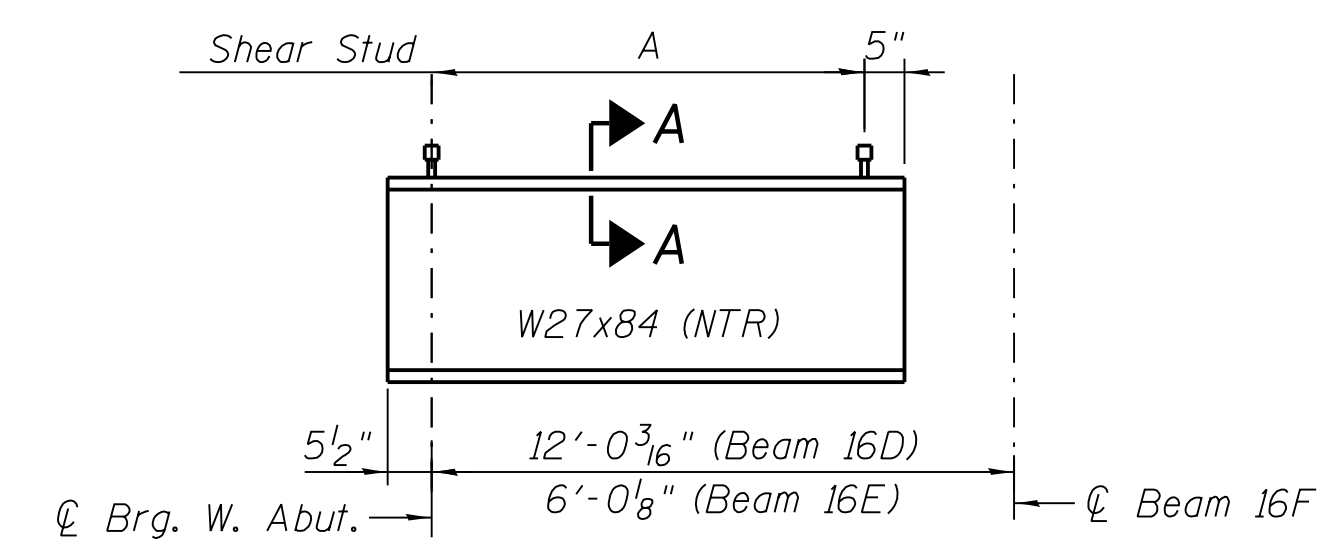
**BEAMS 1H & 16F**



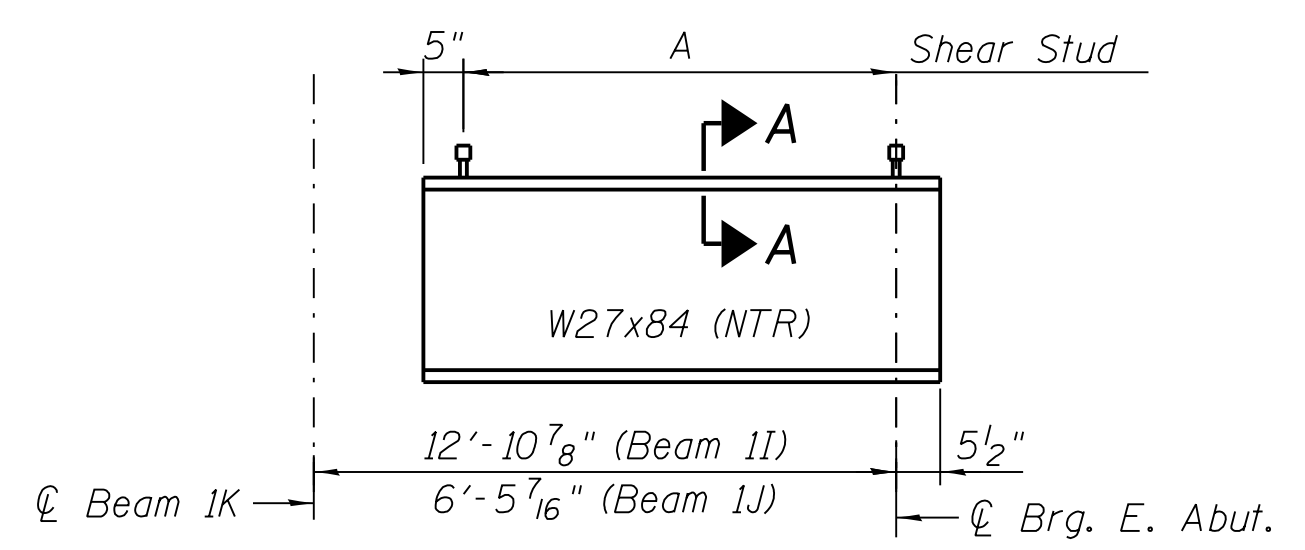
**BEAMS 1K & 16H**



**BEAMS 1F & 1G**



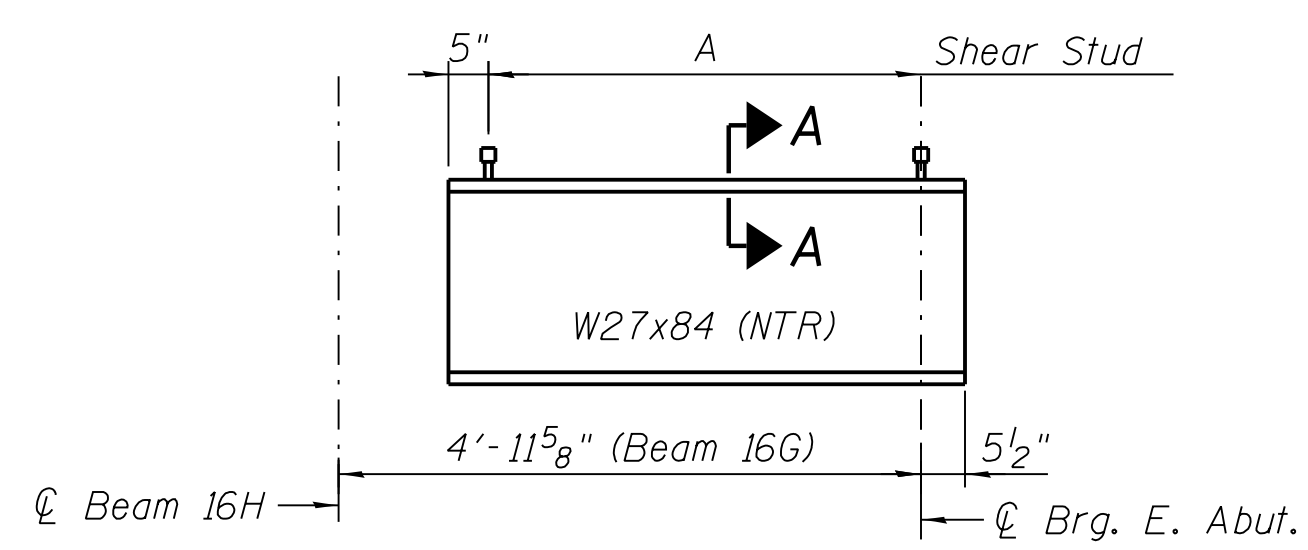
**BEAMS 16D & 16E**



**BEAMS 1I & 1J**

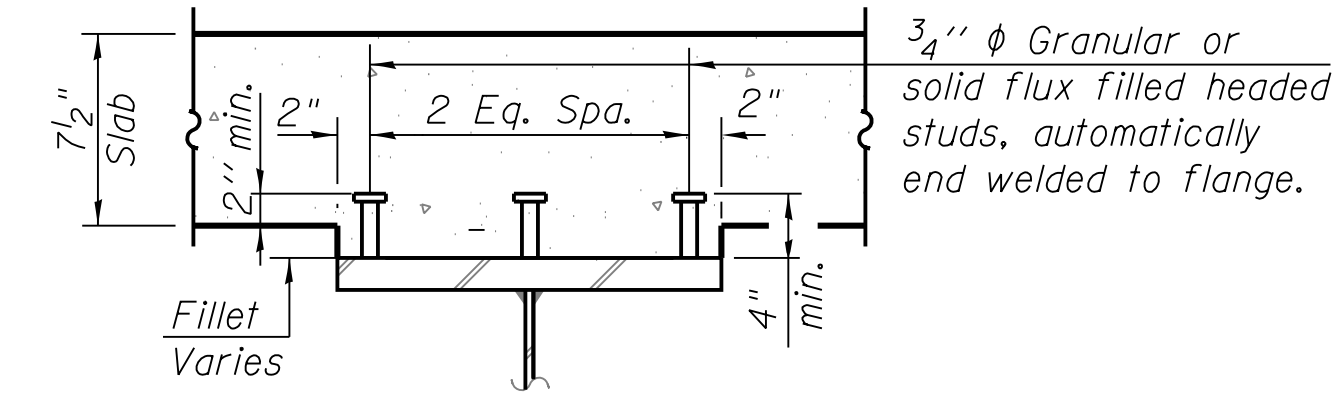
**SHEAR STUD DIMENSIONS**

Beam	A
16D	20 Spa. @ 6" (-) = 10'-0"
16E	8 Spa. @ 6" (-) = 3'-11 7/8"
16F	46 Spa. @ 6" (-) = 23'-1 1/4"
16G	6 Spa. @ 6" (-) = 2'-11 1/2"
16H	24 Spa. @ 6" (-) = 11'-11"
1F	15 Spa. @ 6" (-) = 7'-3"
1G	5 Spa. @ 6" (-) = 3'-8 1/8"
1H	36 Spa. @ 6" (-) = 17'-10 3/4"
1I	22 Spa. @ 6" (-) = 10'-10 5/8"
1J	9 Spa. @ 6" (-) = 4'-5 1/4"
1K	52 Spa. @ 6" (-) = 25'-11 1/2"



**BEAM 16G**

**BEAM ELEVATIONS**



**SECTION A-A**

Note:  
1. Load carrying components designated "NTR" shall conform to the Impact Testing requirement, Zone 2.



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Chicago, IL 60628

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APPROVED BY	D.ZROKA
CHECKED BY	P.MAID
DESIGNED BY	L.SOMMER
DRAWN BY	T.SLEDGE
PROJECT NO.	2012-0021
FILE NAME	File Name

MARK	DATE	DESCRIPTION
	2/9/17	ISSUED FOR CONSTRUCTION

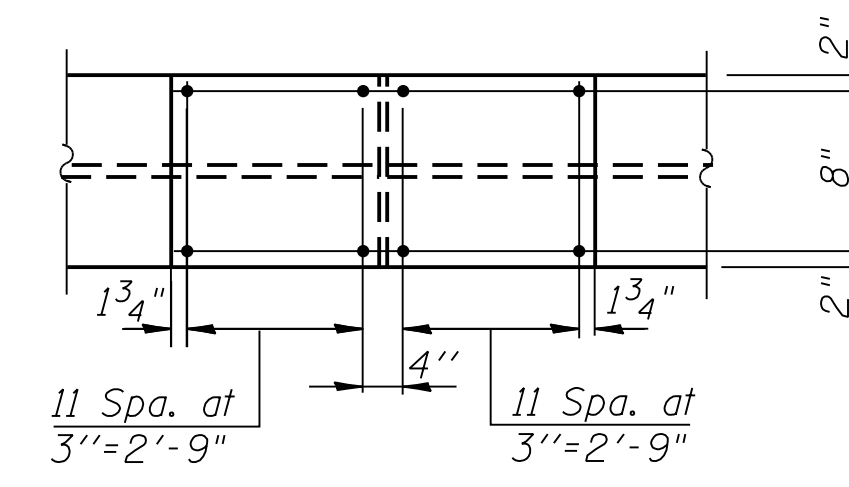
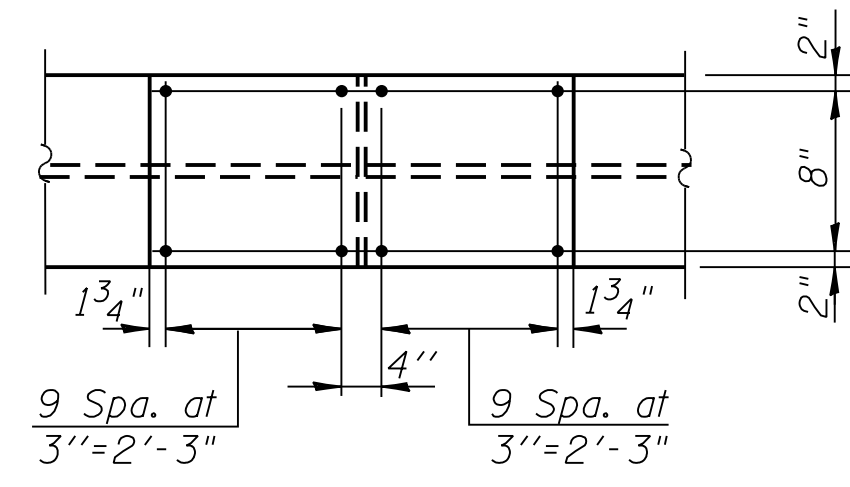
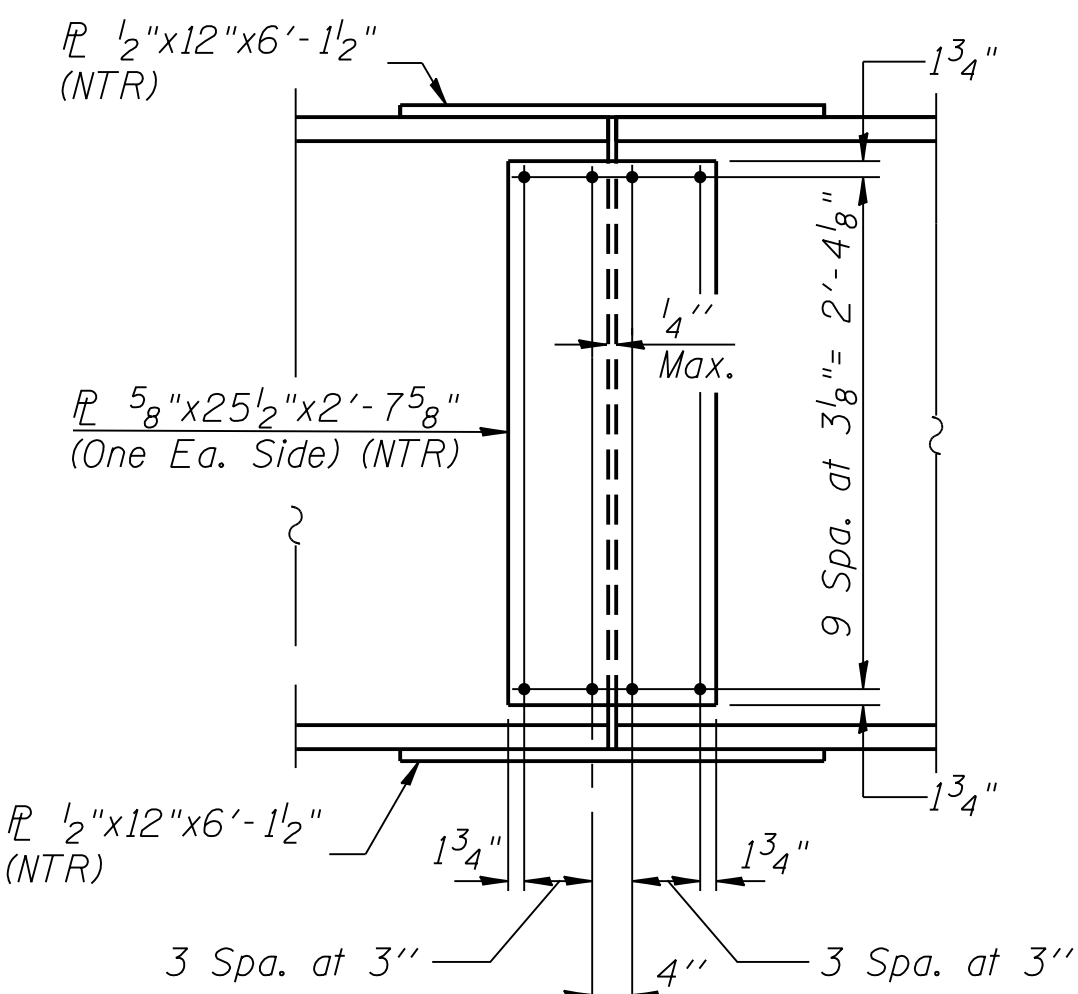
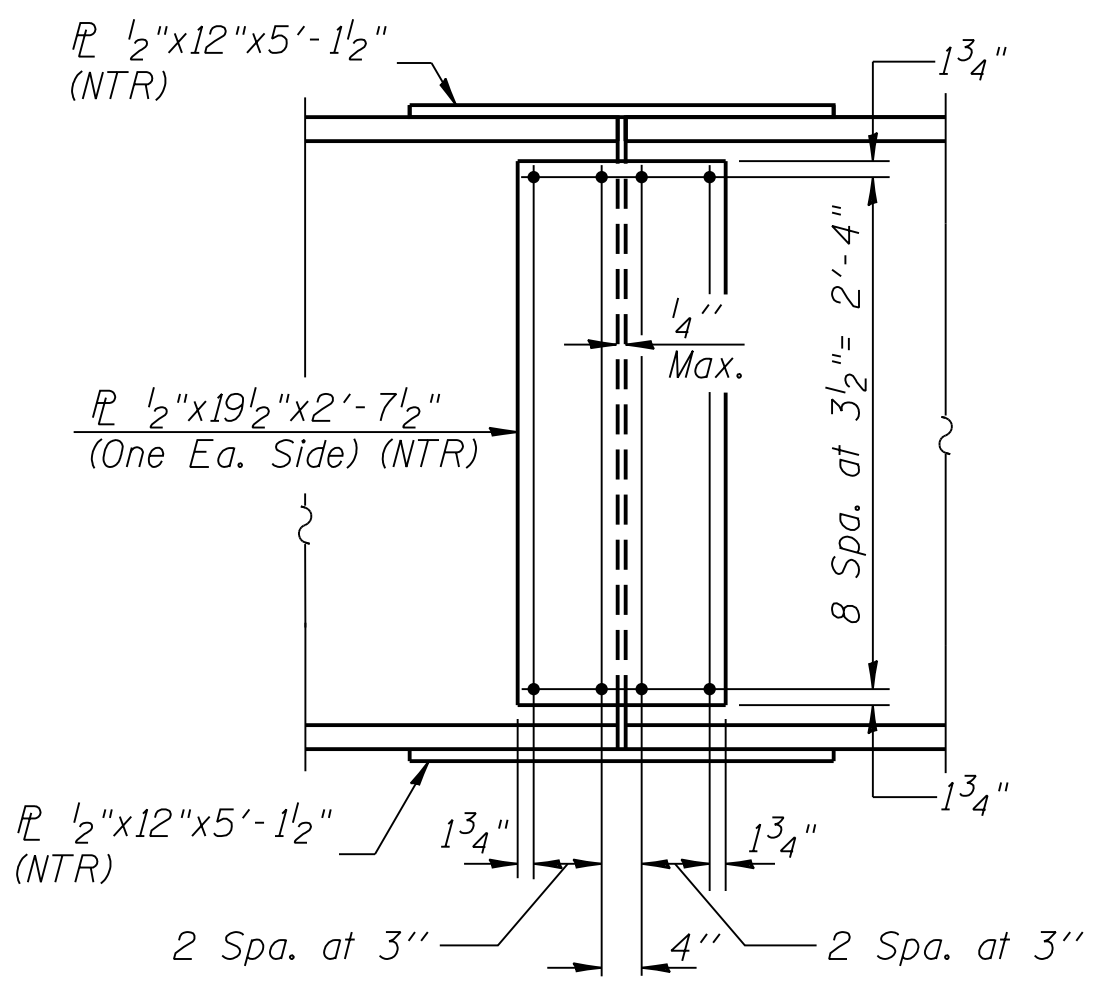
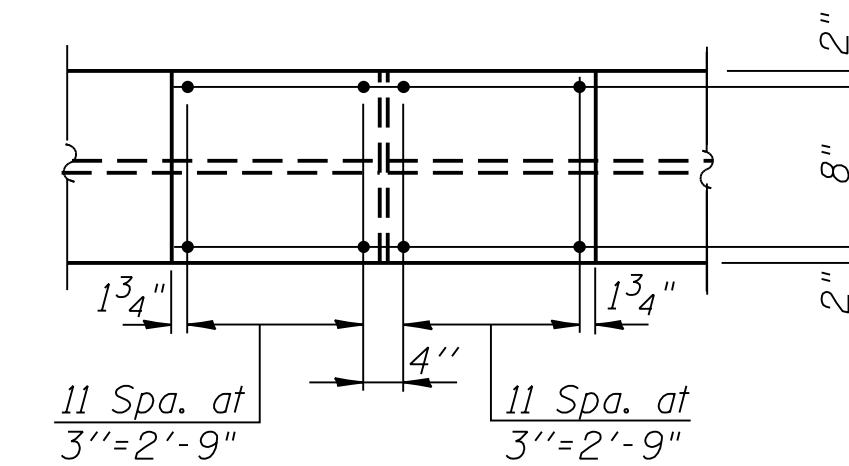
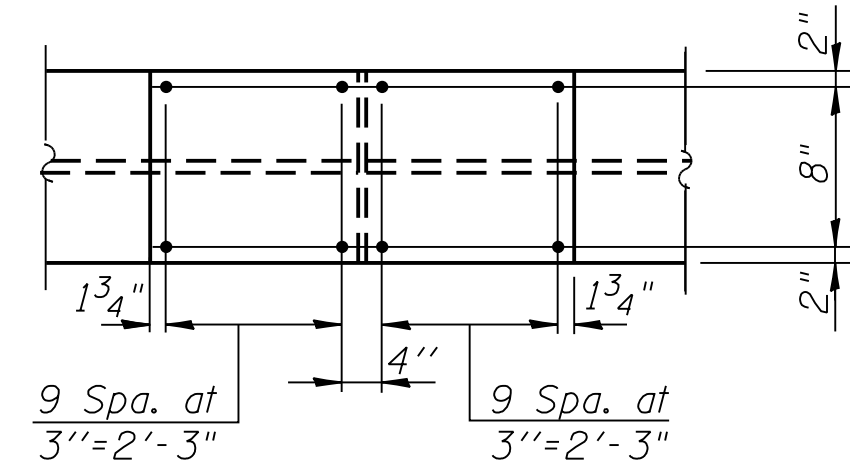
LOCATION IDENTIFIER: **DR095**

**95TH ST. BRIDGE**  
**STRUCTURAL STEEL DETAILS**  
(SHEET 2 OF 5)  
SN 016-0437

**95TH-S-127**

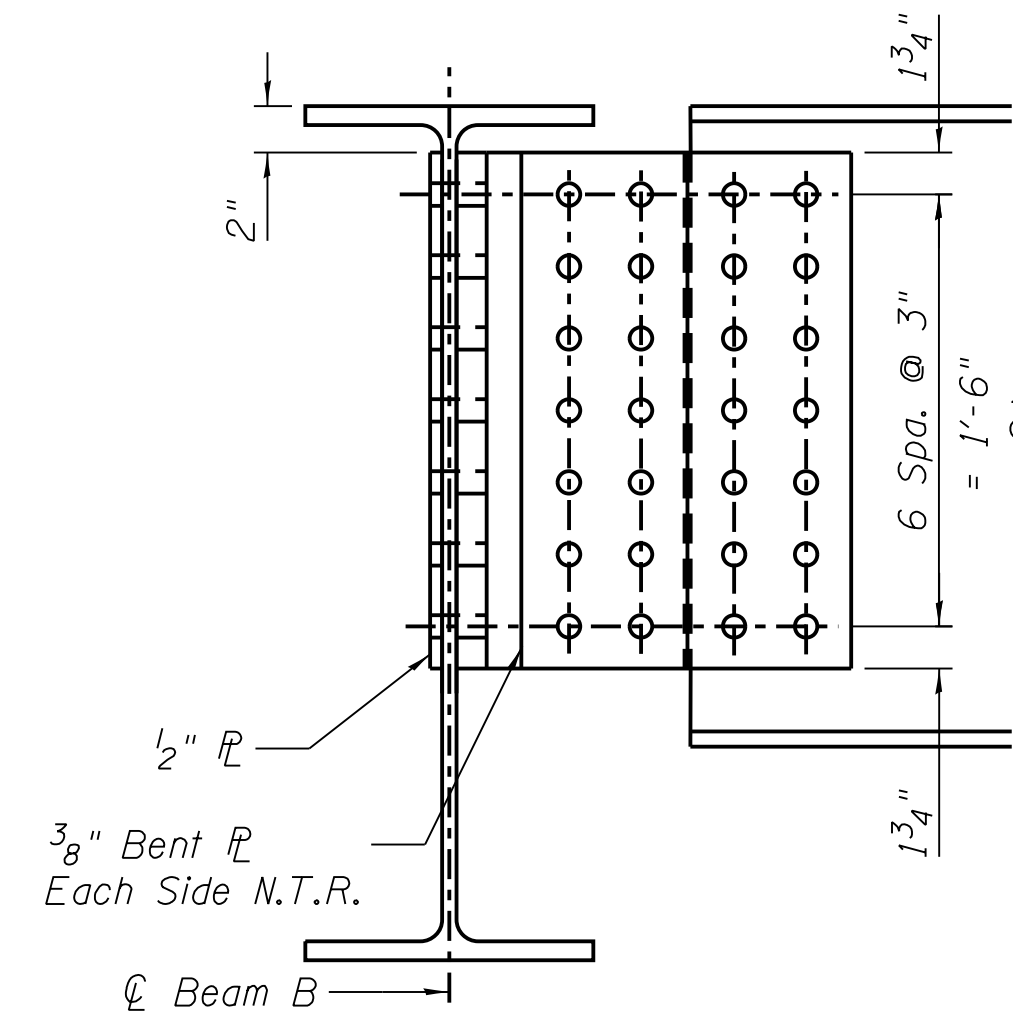
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PLOT DATE = 8/31/2016

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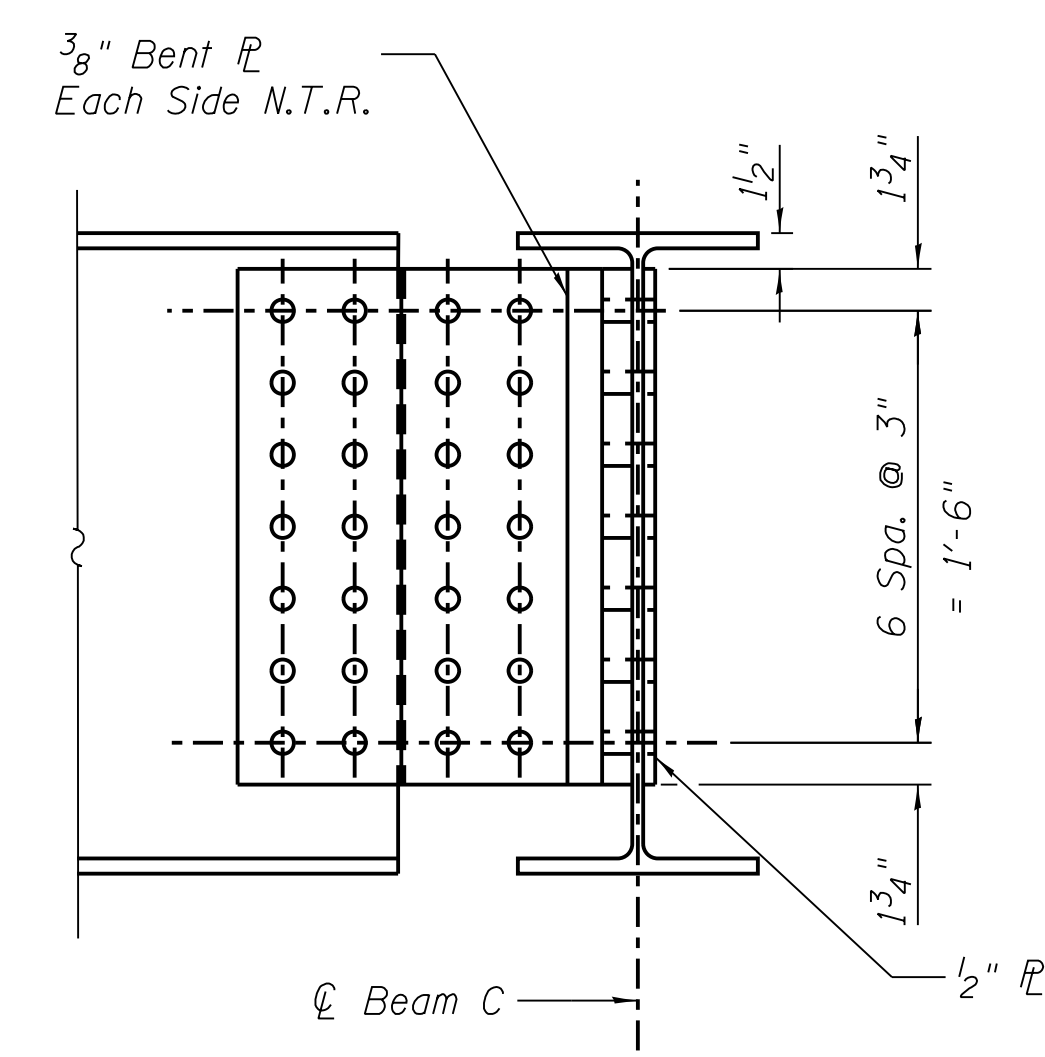


**FIELD SPLICE DETAIL**  
Beams 1A, 1B, 1E & 16C

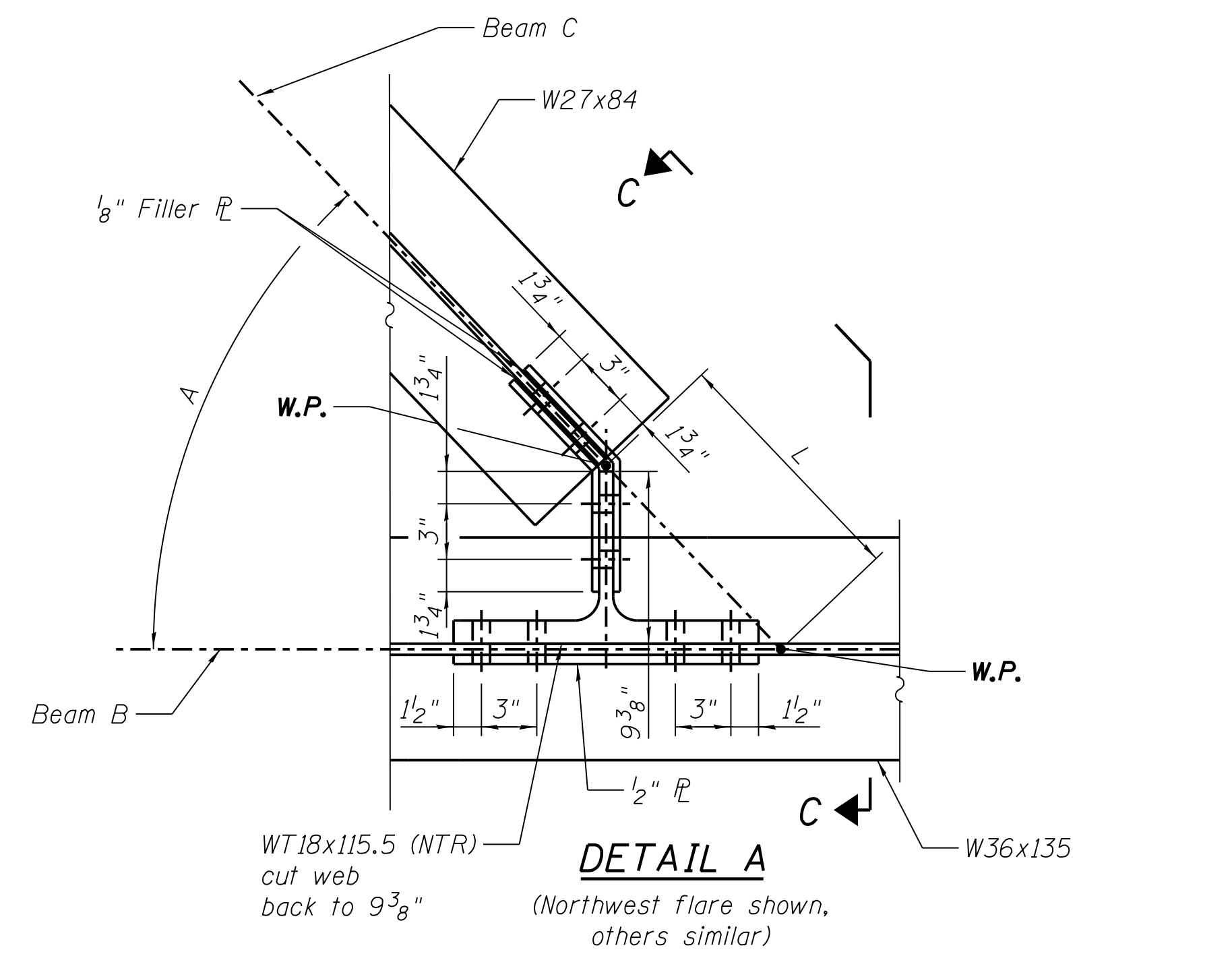
**FIELD SPLICE DETAIL**  
Beams 1C, 1D, 16A & 16B



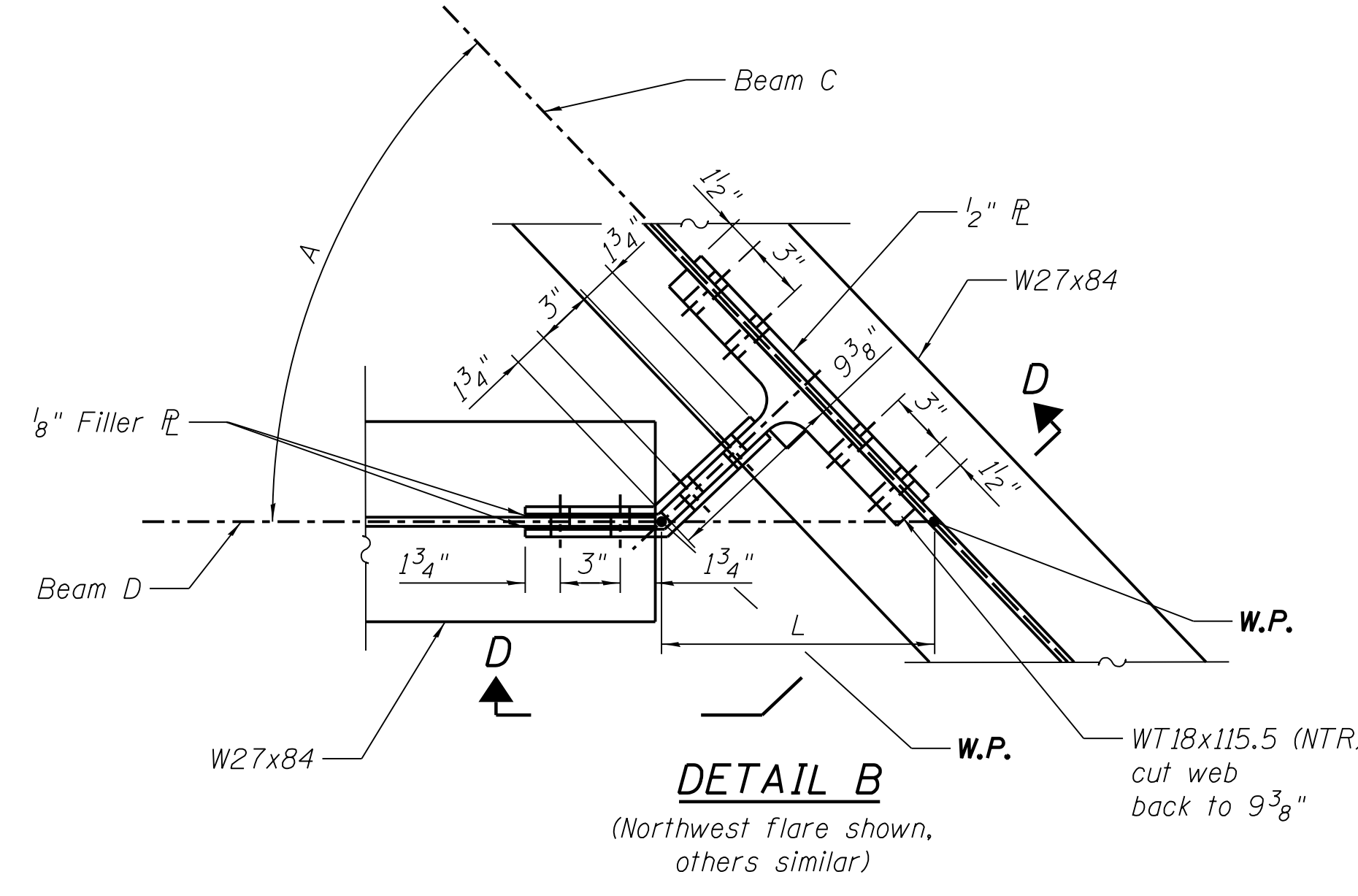
**SECTION C**



**SECTION D**



**DETAIL A**  
(Northwest flare shown, others similar)



**DETAIL B**  
(Northwest flare shown, others similar)

Beam B	Beam C	Beam D	Angle A (degrees)	Length L
16C	16F	-	44.4592	1'-1 3/4"
-	16F	16E	44.4592	1'-1 3/4"
-	16F	16D	44.4592	1'-1 3/4"
16C	16H	-	45.0575	1'-1 5/8"
-	16H	16G	45.0575	1'-1 5/8"
1E	1H	-	43.7672	1'-1 5/16"
-	1H	1F	43.7672	1'-1 5/16"
-	1H	1G	43.7672	1'-1 5/16"
1E	1K	-	44.2706	1'-1 3/16"
-	1K	1I	44.2706	1'-1 3/16"
-	1K	1J	44.2706	1'-1 3/16"



**CHICAGO TRANSIT AUTHORITY ENGINEERING**

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15 West 95th Street (South Terminal)  
Chicago, IL 60628

**IN CHARGE** S. KADERBEK  
**APPROVED BY** D. ZROKA  
**CHECKED BY** P. MAID  
**DESIGNED BY** L. SOMMER  
**DRAWN BY** T. SLEDGE  
**PROJECT NO.** 2012-0021  
**FILE NAME** File Name

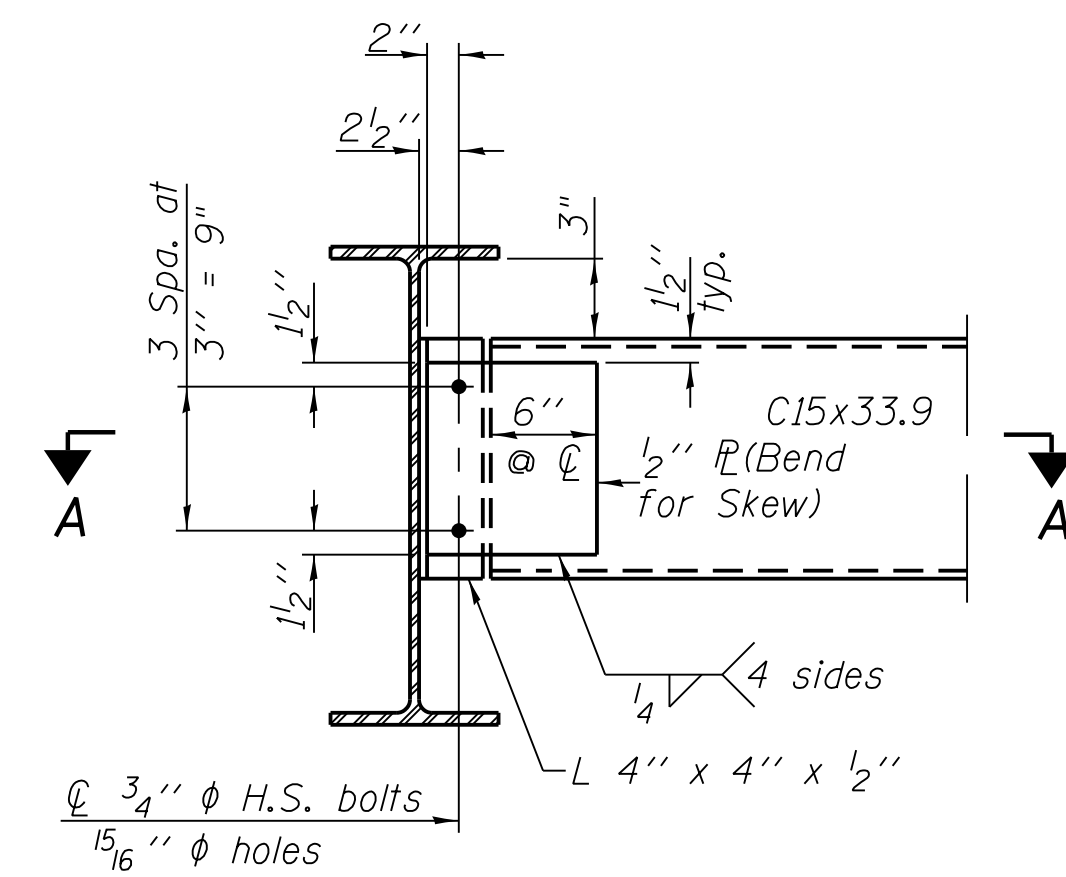
MARK	DATE	DESCRIPTION
	2/9/17	ISSUED FOR CONSTRUCTION

**LOCATION IDENTIFIER: DR095**  
**95TH ST. BRIDGE**  
**STRUCTURAL STEEL DETAILS**  
(SHEET 3 OF 5)  
SN 016-0437

**95TH-S-128**

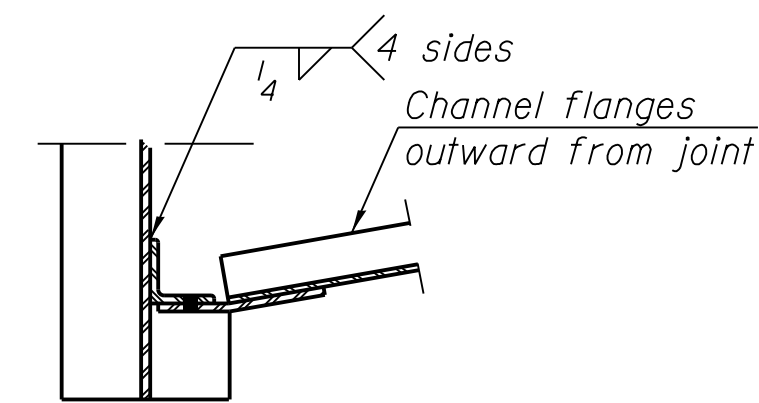
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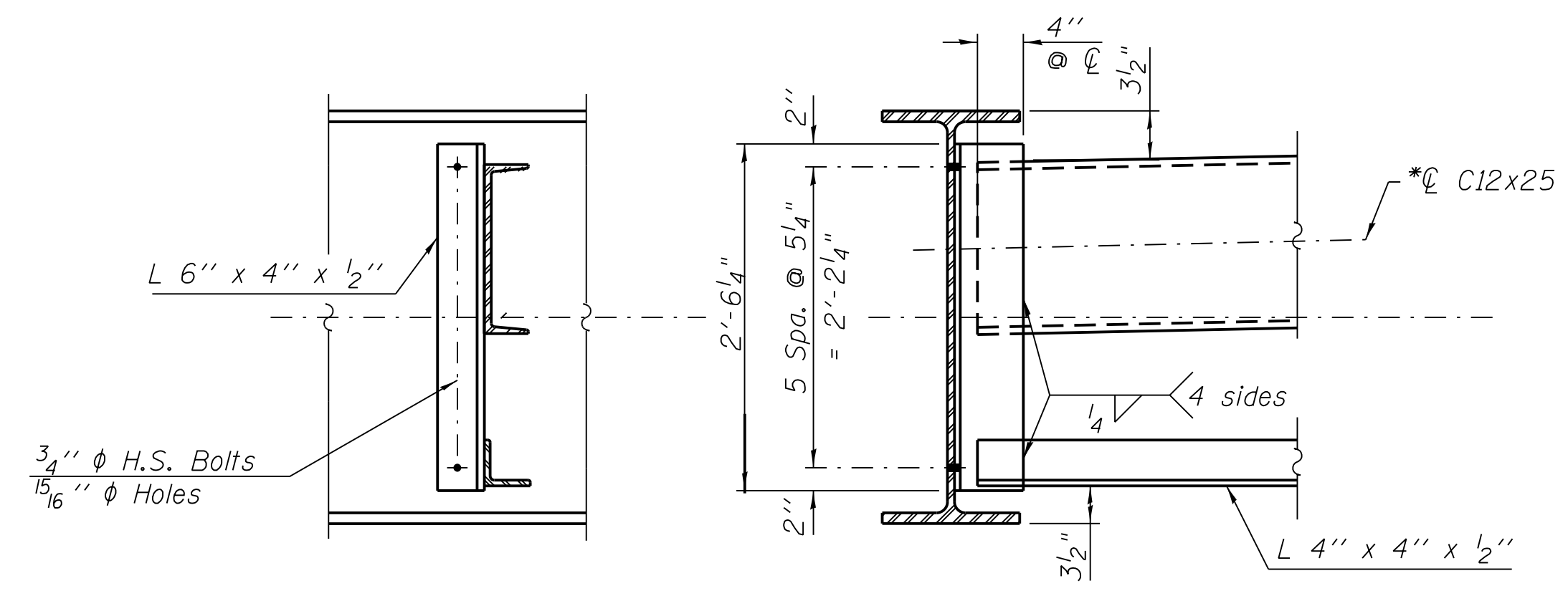


**END DIAPHRAGM, D**

Note:  
Two hardened washers required for each set of oversized holes.



**SECTION A-A**



**INTERIOR DIAPHRAGM, D1**

Note:  
Two hardened washers required for each set of oversized holes.  
\*Alternate channels are permitted to facilitate material acquisition. The alternative, if utilized, shall be provided at no extra cost to the Authority.  
Where new structural steel elements are connected to an existing steel member, all new fasteners shall be high strength bolts. Holes shall be subpunched or subdrilled 1/16" dia. and reamed in the field to 13/16" dia. for 3/4" dia. bolts, unless otherwise noted. Holes shall be subpunched or subdrilled 13/16" dia. and reamed in the field to 15/16" dia. for 7/8" dia. bolts, unless noted otherwise.

Note:  
For Diaphragm Supporting Canopy Column, D2, see Architectural Drawings.



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LOCATION IDENTIFIER: **DR095**  
95TH ST. BRIDGE  
STRUCTURAL STEEL DETAILS  
(SHEET 4 OF 5)  
SN 016-0437

**95TH-S-129**

FILE NAME = 070\_DR095\_S-129.dgn  
PLOT DATE = 8/31/2016

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INTERIOR GIRDER MOMENT TABLE (BEAMS 1A, 1B, 1E, 16C)				
		0.4 Sp. 1 or 0.6 Sp. 3	Pier	.5 Sp. 2
$I_s$	(in <sup>4</sup> )	7,800	7,800	7,800
$I_c(n)$	(in <sup>4</sup> )	20,451	20,451	20,451
$I_c(3n)$	(in <sup>4</sup> )	14,908	14,908	14,908
$S_s$	(in <sup>3</sup> )	439	439	439
$S_c(n)$	(in <sup>3</sup> )	641	641	641
$S_c(3n)$	(in <sup>3</sup> )	577	577	577
$\rho$	(k/')	.676	.676	.676
$M \rho$	('k)	333	301	148
$s \rho$	(k/')	.310	.310	.310
$M_s \rho$	('k)	153	138	68
$M \zeta$	('k)	373	298	170
$M_{IM}$	('k)	94	80	51
$\rho_3 [M \zeta + i]$	('k)	778	630	368
$M_a$	('k)	1,643	1,390	759
$M_u$	('k)	2,439	-	-
$f_s \rho$ non-comp	(ksi)	9.3	8.4	4.1
$f_s \rho$ (comp)	(ksi)	3.2	3.9	1.9
$f_s \rho_3 [M \zeta + M_I]$	(ksi)	14.6	17.5	10.3
$f_s$ (Overload)	(ksi)	27.1	29.8	16.3
$f_s$ (Total)	(ksi)	-	38.7	21.2
VR	(k)	36	39.9	39.9

INTERIOR GIRDER REACTION TABL (BEAMS 1A, 1B, 1E, 16C)			
		Abut.	Pier
$R \rho$	(k)	31.8	63.7
$R \zeta$	(k)	27.5	35.4
$R_I$	(k)	6.9	9.6
$R_{Total}$	(k)	66.2	108.7

\* Compact section  
 \*\* Braced non-compact and partially braced section

INTERIOR GIRDER MOMENT TABLE (BEAMS 1C, 1D, 16A, 16B)				
		0.4 Sp. 1 or 0.6 Sp. 3	Pier	.5 Sp. 2
$I_s$	(in <sup>4</sup> )	10,500	10,500	10,500
$I_c(n)$	(in <sup>4</sup> )	22,678	22,678	22,678
$I_c(3n)$	(in <sup>4</sup> )	16,323	16,323	16,323
$S_s$	(in <sup>3</sup> )	580	580	580
$S_c(n)$	(in <sup>3</sup> )	794	794	794
$S_c(3n)$	(in <sup>3</sup> )	706	706	706
$\rho$	(k/')	.642	.642	.642
$M \rho$	('k)	709	578	439
$s \rho$	(k/')	.310	.310	.310
$M_s \rho$	('k)	178	139	87
$M \zeta$	('k)	194	207	137
$M_w$	('k)	883	643	639
$M_s$	('k)	602	437	435
$1.3(\rho_3 M \zeta)$	('k)	420	449	297
$1.3 M_w + .5 M_s$	('k)	1,449	1,054	1,048
$1.3 (M \zeta + .3 M_w) + 1.6 M_s$	('k)	1,560	1,219	1,123
$M_a$	('k)	2,713	2,151	1,807
$M_u$	('k)	2,940	-	-
$f_s \rho$ non-comp	(ksi)	14.7	12.0	9.1
$f_s \rho$ (comp)	(ksi)	3.0	2.9	1.8
$f_s \zeta + s \cdot w$	(ksi)	18.1	19.4	17.9
$f_s$ (Overload)	(ksi)	35.8	34.3	28.8
$f_s$ (Total)	(ksi)	-	44.6	37.4
VR	(k)	37.8	47.2	47.2

INTERIOR GIRDER REACTION TABLE (BEAMS 1C, 1D, 16A, 16B)			
		Abut.	Pier
$R \rho$	(k)	49.1	86.9
$R \zeta + .3 M_w + s$	(k)	61.2	95.7
$R_{Total}$	(k)	110.3	182.6

\* Compact section  
 \*\* Braced non-compact and partially braced section

$I_s, S_s$ : Non-composite moment of inertia and section modulus of the steel section used for computing  $f_s$  (Total and Overload) due to non-composite dead loads (in.4 and in.3).  
 $I_c(n), S_c(n)$ : Composite moment of inertia and section modulus of the steel and deck based upon the modular ratio, "n", used for computing  $f_s$  (Total and Overload) due to short-term composite live loads (in.4 and in.3).  
 $I_c(3n), S_c(3n)$ : Composite moment of inertia and section modulus of the steel and deck based upon 3 times the modular ratio, "3n", used for computing  $f_s$  (Total and Overload) due to long-term composite (superimposed) dead loads (in.4 and in.3).  
 $\rho$ : Un-factored non-composite dead load (kips/ft.).  
 $M \rho$ : Un-factored moment due to non-composite dead load (kip-ft.).  
 $s \rho$ : Un-factored long-term composite (superimposed) dead load (kips/ft.).  
 $M_s \rho$ : Un-factored moment due to long-term composite (superimposed) dead load (kip-ft.).  
 $M \zeta$ : Un-factored live load moment (kip-ft.).  
 $M_I$ : Un-factored moment due to impact (kip-ft.).  
 $M \zeta, M_s, M_w$ : Factored Moment due to Pedestrian Live Load, Snow Load, and Wind Load.  
 $M_a$ : Factored design moment (kip-ft.).  
 $1.3 [ M \rho + M_s \rho + \frac{5}{3} (M \zeta + M_I) ]$   
 OR  $1.3 ( M \rho + M_s \rho + M_w ) + 0.5 M_s$   
 OR  $1.3 ( M \rho + M_s \rho + M \zeta + .3 M_w ) + 1.6 M_s$   
 $M_u$ : Compact composite moment capacity according to AASHTO LFD 10.50.1.1 or compact non-composite moment capacity according to AASHTO LFD 10.48.1 (kip-ft.).  
 $f_s$  (Overload): Sum of stresses as computed from the moments below (ksi).  
 $M \rho + M_s \rho + \frac{5}{3} (M \zeta + M_I)$   
 $f_s$  (Total): Sum of stresses as computed from the moments below on non-compact section (ksi).  
 $1.3 [ M \rho + M_s \rho + \frac{5}{3} (M \zeta + M_I) ]$   
 OR  $1.3 ( M \rho + M_s \rho + M_w ) + 0.5 M_s$   
 OR  $1.3 ( M \rho + M_s \rho + M \zeta + .3 M_w ) + 1.6 M_s$   
 $VR$ : Maximum  $\zeta$  + impact shear range within the composite portion of the span for stud shear connector design (kips).

Note:  
 Moments in Span 2 are negative.



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**95th Terminal Improvement**  
 15 West 95th Street (South Terminal)  
 Chicago, IL 60628

IN CHARGE	S. KADERBEK
APPROVED BY	D.ZROKA
CHECKED BY	P.MAID
DESIGNED BY	L.SOMMER
DRAWN BY	T.SLEDGE
PROJECT NO.	2012-0021
FILE NAME	File Name

MARK	DATE	DESCRIPTION
	2/9/17	ISSUED FOR CONSTRUCTION

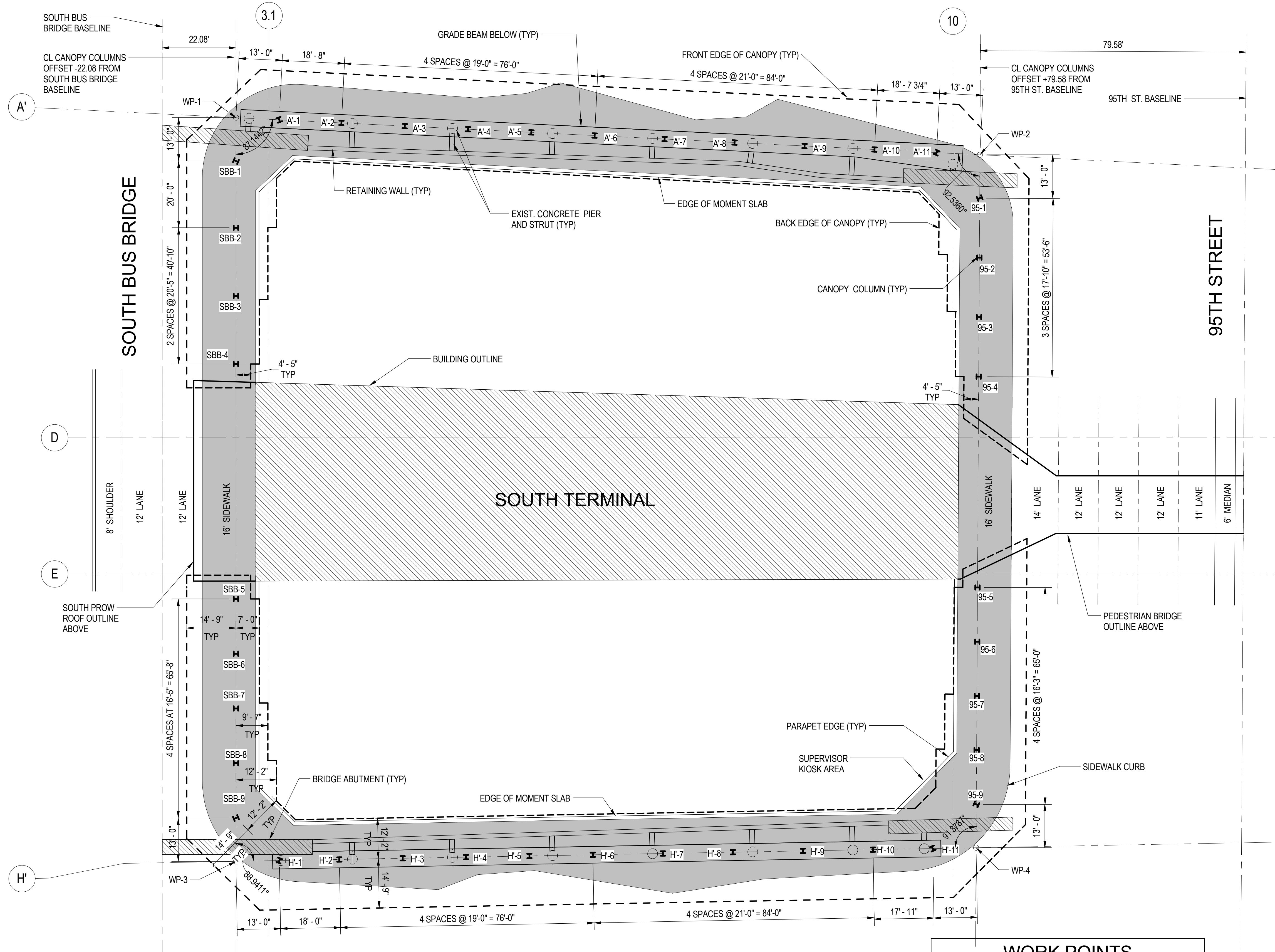
LOCATION IDENTIFIER: **DR095**  
 95TH ST. BRIDGE  
 STRUCTURAL STEEL DETAILS  
 (SHEET 5 OF 5)  
 SN 016-0437

**95TH-S-130**

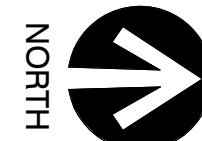
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 PLOT DATE = 8/31/2016

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1 SOUTH TERMINAL - CANOPY GEOMETRY PLAN  
S-1301 SCALE: 1/16" = 1'-0"



WORK POINTS					
LOCATION	NORTHING	EASTING	BASELINE	STATION	OFFSET
WP - 1	1841677.614	1177685.607	SBB	300+83.62	-22.08
WP - 2	1841900.890	1177689.578	95TH	20+99.59	+79.58
WP - 3	1841684.760	1177908.422	SBB	303+06.54	-22.08
WP - 4	1841906.389	1177897.211	95TH	23+07.30	+79.58



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**95th Terminal Improvement**  
 15 West 95th Street (South Terminal)  
 Chicago, IL 60628

IN CHARGE	J. JAKALSKI
APPROVED BY	J. JAKALSKI
CHECKED BY	R.S. NAIR
DESIGNED BY	S. CITKO
DRAWN BY	C. BOVE
PROJECT NO.	2012-0021.02
FILE NAME	D095_S-RoofCENTRAL

2/9/17	ISSUED FOR CONSTRUCTION	
MARK	DATE	DESCRIPTION

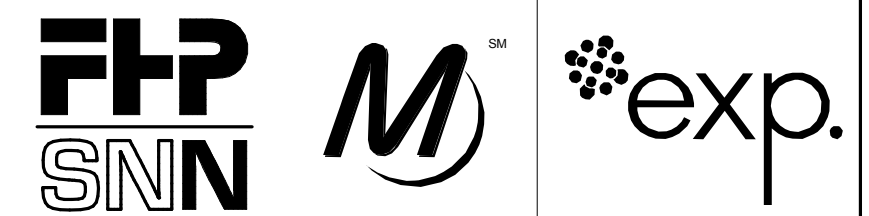
LOCATION IDENTIFIER: DR095

**SOUTH TERMINAL BUS CANOPY GEOMETRY PLAN**

**S-1301**



**NOT FOR CONSTRUCTION**



**SENSITIVE SECURITY INFORMATION**  
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**95th Terminal Improvement**  
 15 West 95th Street (South Terminal)  
 Chicago, IL 60628

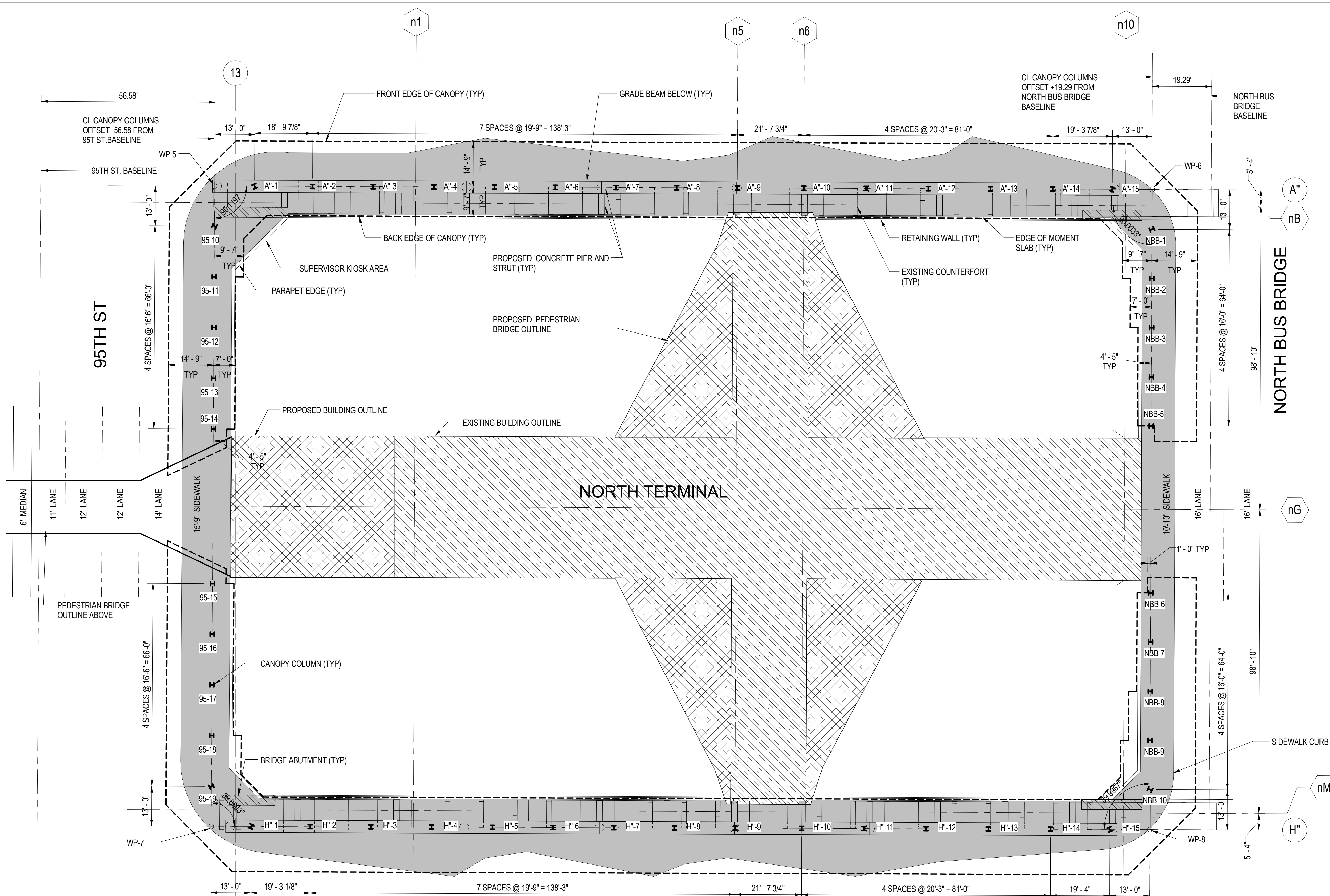
IN CHARGE	J. JAKALSKI
APPROVED BY	J. JAKALSKI
CHECKED BY	R.S. NAIR
DESIGNED BY	S. CITKO
DRAWN BY	C. BOVE
PROJECT NO.	2012-0021.02
FILE NAME	D095_S-RooFCENTRAL

MARK	DATE	DESCRIPTION
	1/24/17	ISSUED FOR PERMIT D16013-03
	9/27/16	ISSUED FOR SOLICITATION
	6/17/16	ISSUED FOR IDOT PERMIT

LOCATION IDENTIFIER: **DR095**

**NORTH TERMINAL BUS CANOPY GEOMETRY PLAN**

**S-1302**

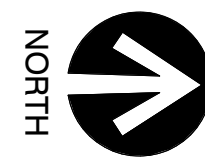


WORK POINTS					
LOCATION	NORTHING	EASTING	BASELINE	STATION	OFFSET
WP - 5	1842036.995	1177685.721	95TH	20+99.34	-56.58
WP - 6	1842341.916	1177677.008	NBB	70+77.65	+19.29
WP - 7	1842042.511	1177893.979	95TH	23+07.67	-56.58
WP - 8	1842347.879	1177885.253	NBB	72+85.98	+19.29

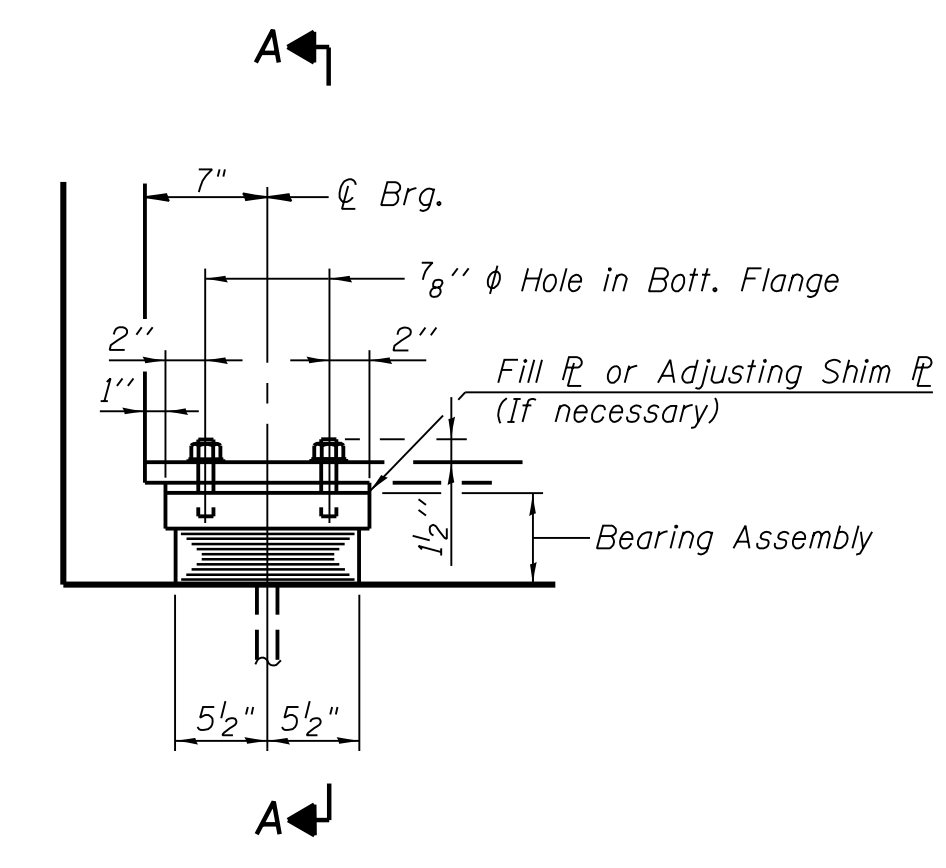
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**1** NORTH TERMINAL BUS CANOPY GEOMETRY PLAN  
 S-1302 SCALE: 1/16" = 1'-0"

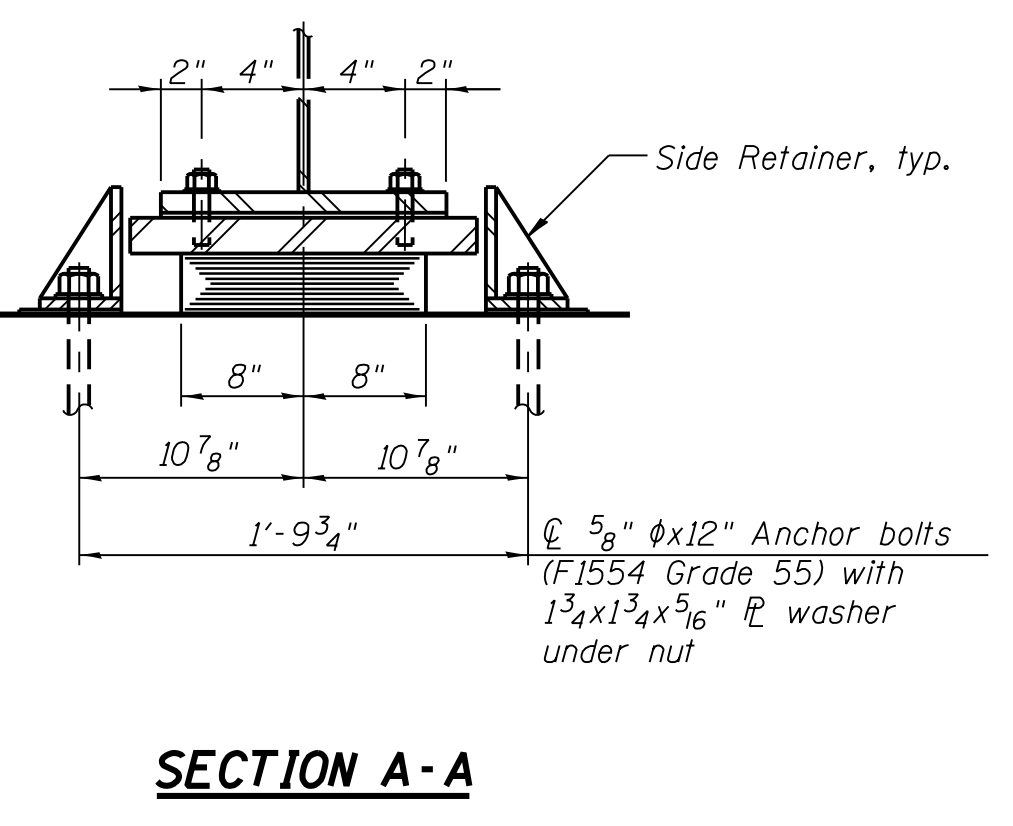


vscarpa

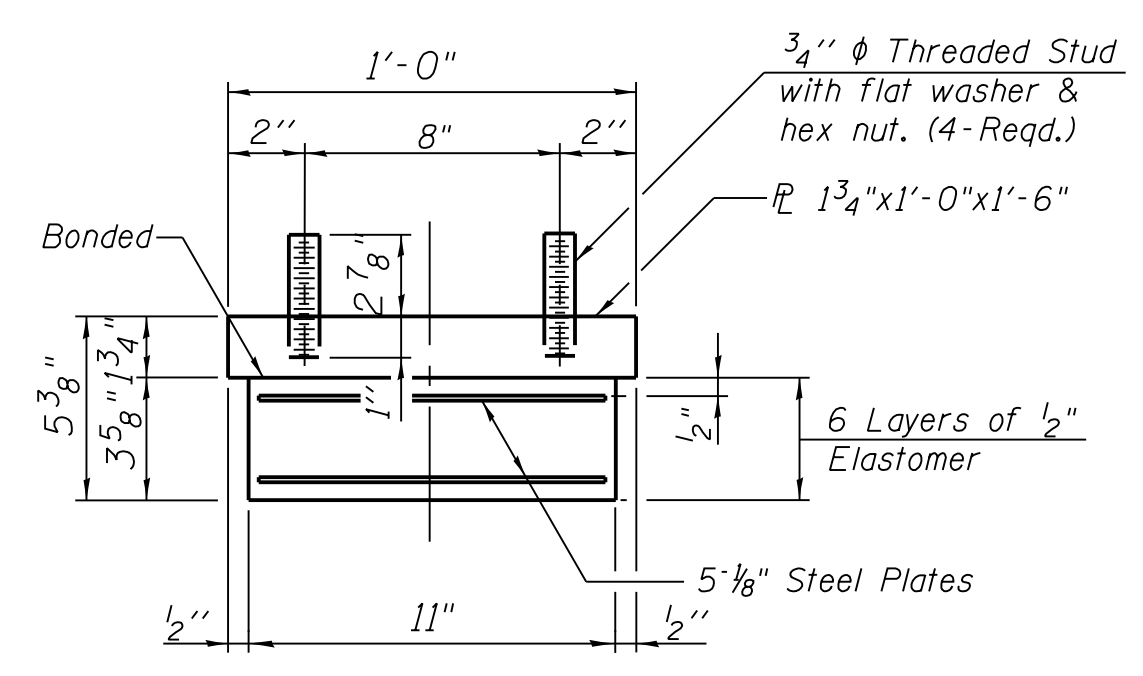


**ELEVATION AT ABUT.**

**TYPE I ELASTOMERIC EXP. BRG.**

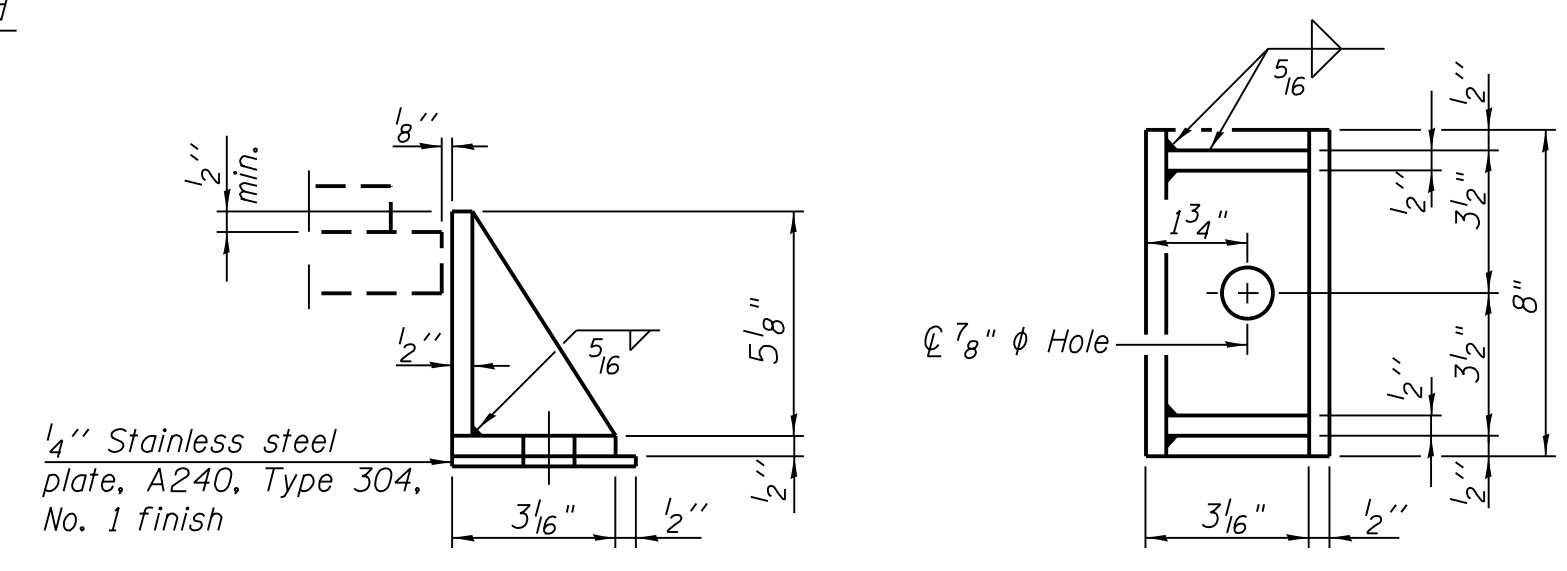


**SECTION A-A**



**BEARING ASSEMBLY**

Note:  
Fill plates and shim plates shall not be placed under Bearing Assembly.



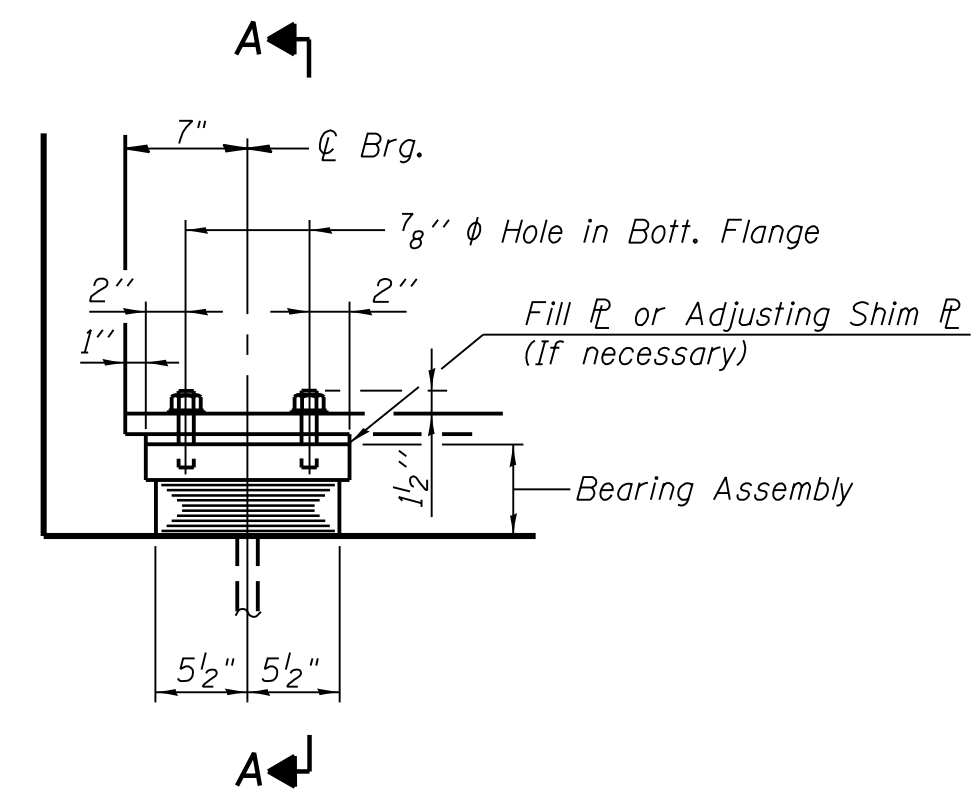
**SIDE RETAINER**

Equivalent rolled angle with stiffeners will be allowed in lieu of welded plates.

**EAST ABUTMENT BEARINGS**

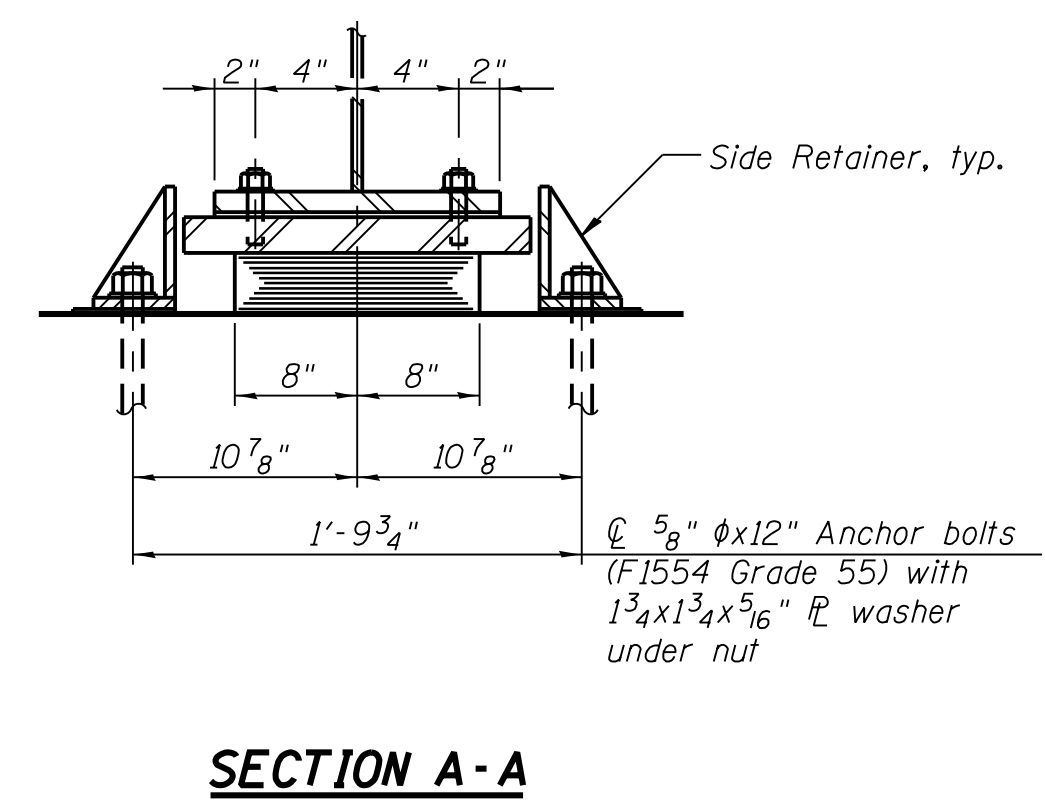
Beams IC, ID, 16A & 16B

Note:  
At East Abutment, add 3/16" x 1'-0" x 1'-6" fill plate for beam 16A

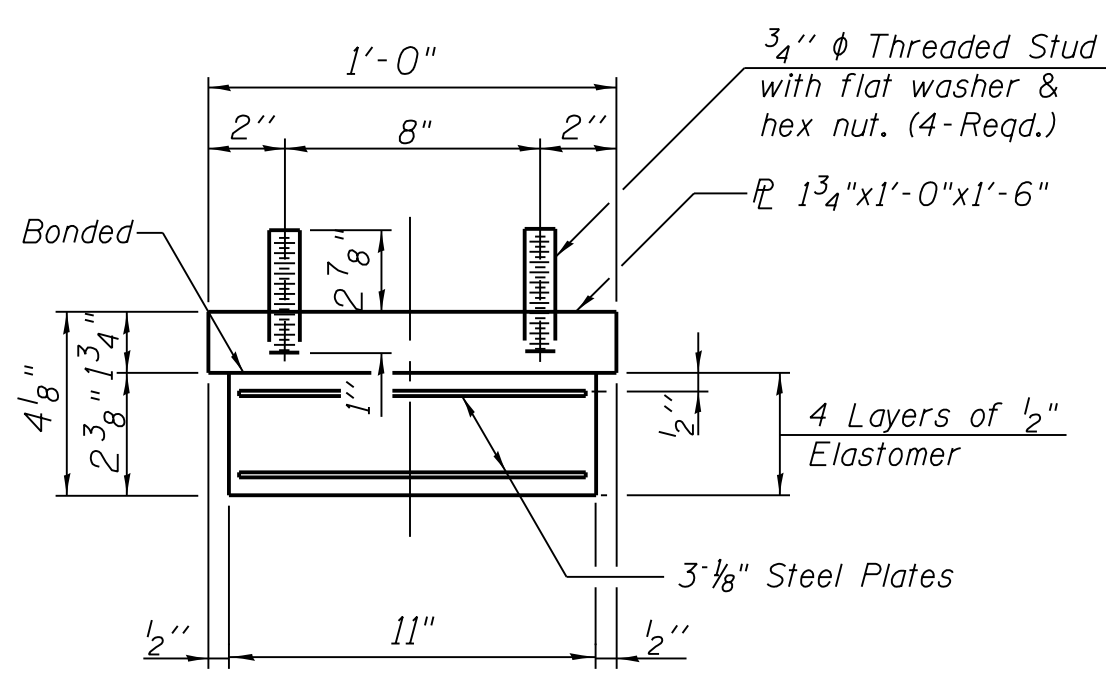


**ELEVATION AT ABUT.**

**TYPE I ELASTOMERIC EXP. BRG.**

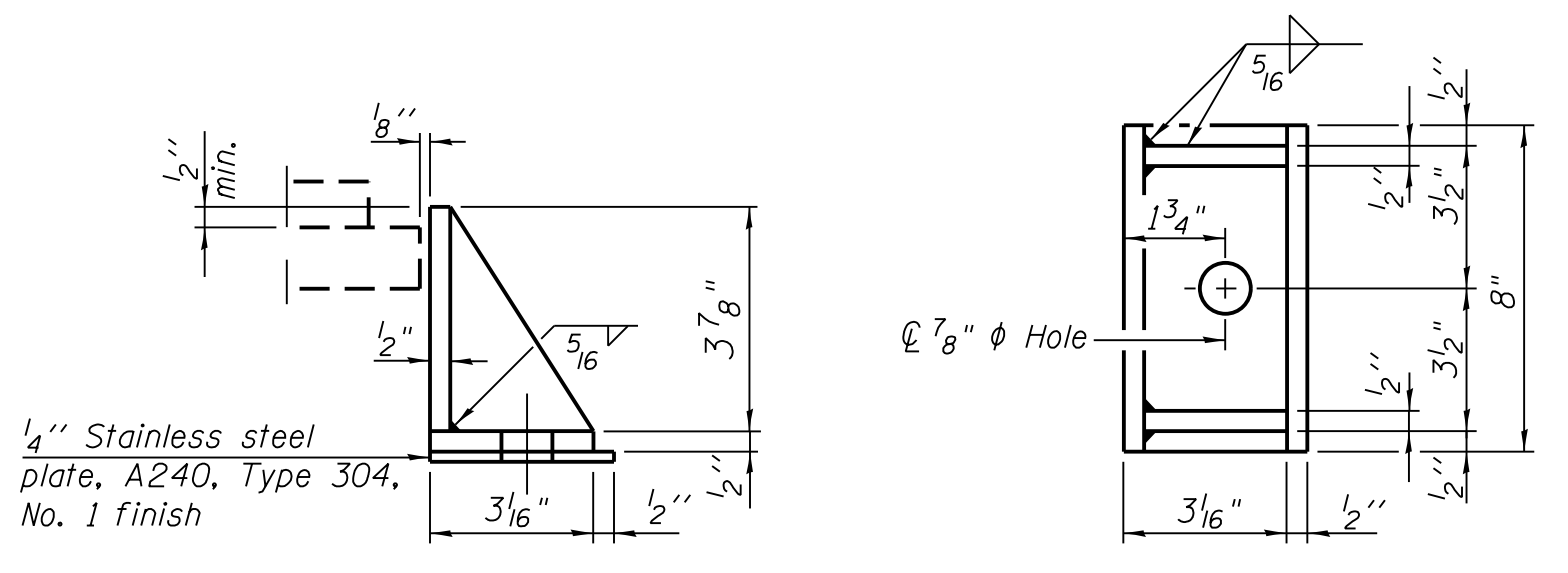


**SECTION A-A**



**BEARING ASSEMBLY**

Note:  
Fill plates and shim plates shall not be placed under Bearing Assembly.



**SIDE RETAINER**

Equivalent rolled angle with stiffeners will be allowed in lieu of welded plates.

**WEST ABUTMENT BEARINGS**

Beams IC, ID, 16A & 16B

Note:  
At West Abutment, Beam 16B add 3/16" x 1'-0" x 1'-6" fill plate.

Notes:  
Anchor bolts shall be ASTM F1554 all-thread (or an Engineer-approved alternate material) of the grade(s) and diameter(s) specified. The corresponding specified grade of AASHTO M314 anchor bolts may be used in lieu of ASTM F1554.  
Side retainers and other steel members required for the elastomeric bearing assembly shall be included in the cost of Elastomeric Bearing Assembly, Type I.  
Beams shall be braced for stability during erection and remain braced until deck is poured and cured.  
Anchor bolts and side retainers at all supports shall be installed as each member is erected unless an equivalent temporary means of lateral restraint is used.



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**95th Terminal Improvement**  
15 West 95th Street (South Terminal)  
Chicago, IL 60628

IN CHARGE	S. KADERBEK
APPROVED BY	D.ZROKA
CHECKED BY	P.MAID
DESIGNED BY	L.SOMMER
DRAWN BY	T.SLEDGE
PROJECT NO.	2012-0021
FILE NAME	File Name

MARK	DATE	DESCRIPTION
	2/9/17	ISSUED FOR CONSTRUCTION

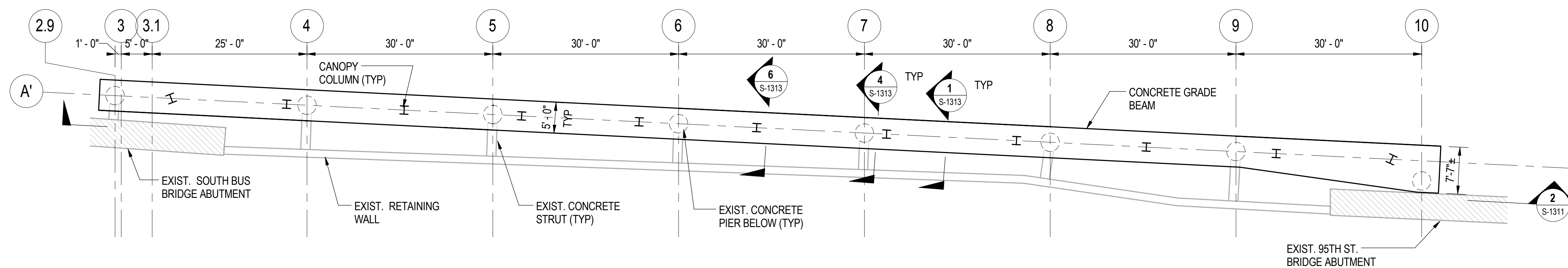
LOCATION IDENTIFIER: **DR095**  
**95TH ST. BRIDGE BEARING DETAILS (SHEET 1 OF 5) SN 016-0437**

**95TH-S-131**

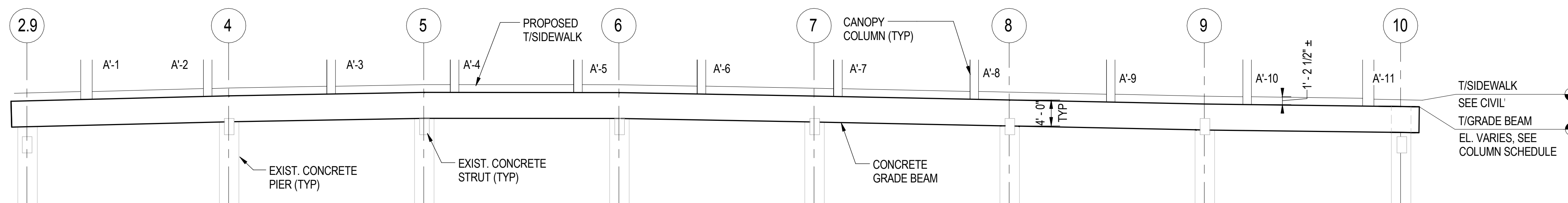
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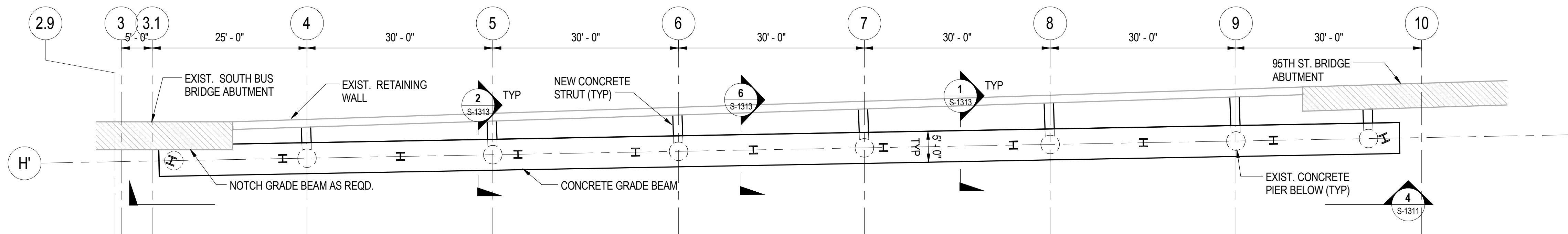
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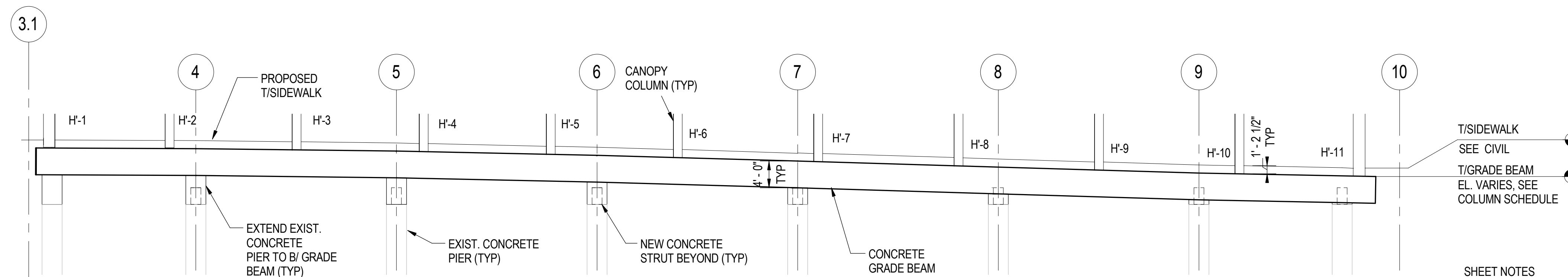
**1** SOUTH TERMINAL BUS CANOPY WEST FOUNDATION PLAN  
S-1311 SCALE: 3/32" = 1'-0"



**2** SOUTH TERMINAL BUS CANOPY WEST FOUNDATION ELEVATION  
S-1311 SCALE: 3/32" = 1'-0"



**3** SOUTH TERMINAL BUS CANOPY EAST FOUNDATION PLAN  
S-1311 SCALE: 3/32" = 1'-0"



**4** SOUTH TERMINAL BUS CANOPY EAST FOUNDATION ELEVATION  
S-1311 SCALE: 3/32" = 1'-0"

- SHEET NOTES**
- FOR GENERAL NOTES, SEE DWGS S-001 TO S-004
  - FOR CANOPY COLUMN LOCATIONS, SEE SHEET S-1301



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**95th Terminal Improvement**  
15 West 95th Street (South Terminal)  
Chicago, IL 60628

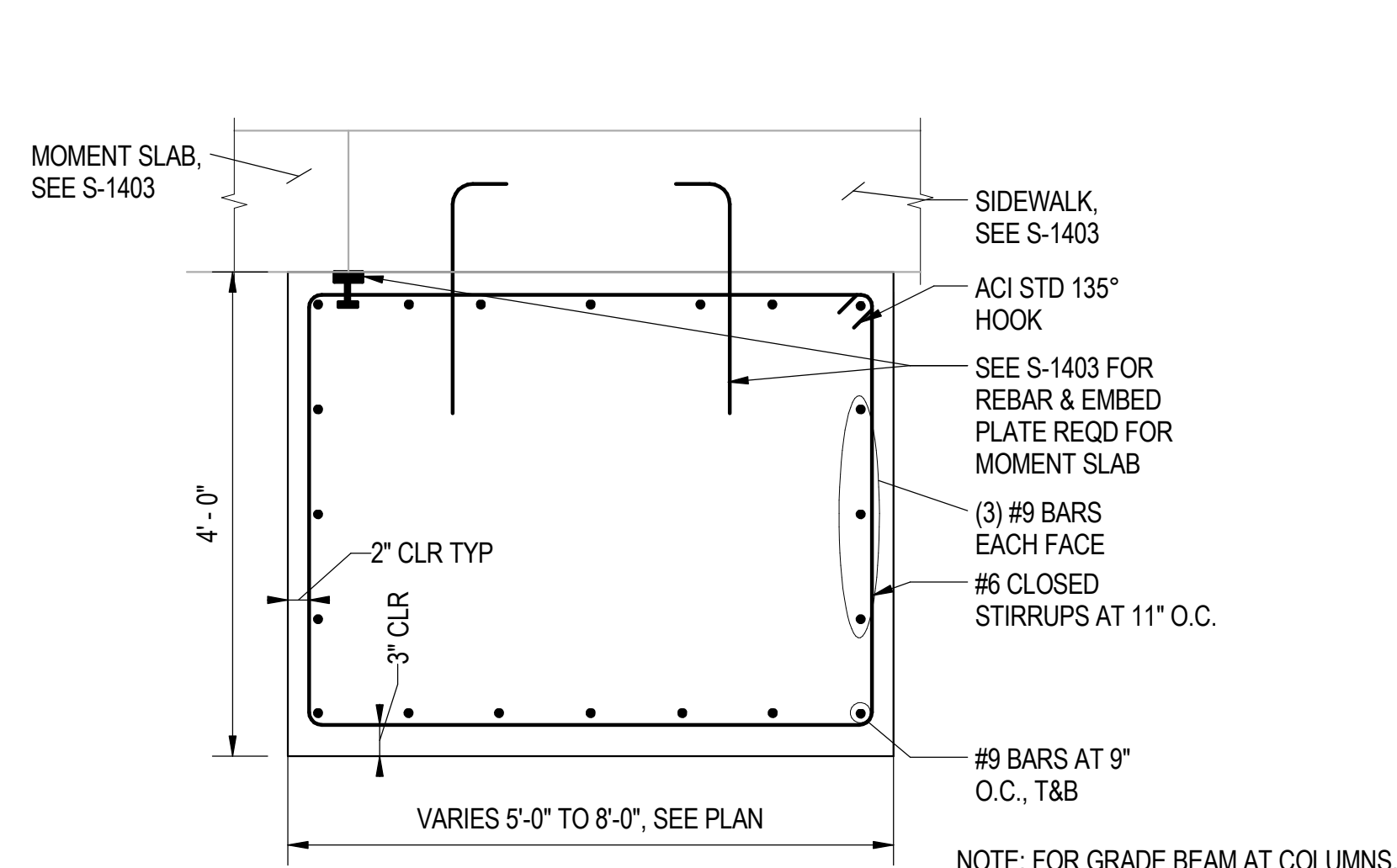
IN CHARGE	J. JAKALSKI
APPROVED BY	J. JAKALSKI
CHECKED BY	R.S. NAIR
DESIGNED BY	S. CITKO
DRAWN BY	C. BOVE
PROJECT NO.	2012-0021.02
FILE NAME	D095_S-RoofCENTRAL

MARK	DATE	DESCRIPTION
	2/9/17	ISSUED FOR CONSTRUCTION

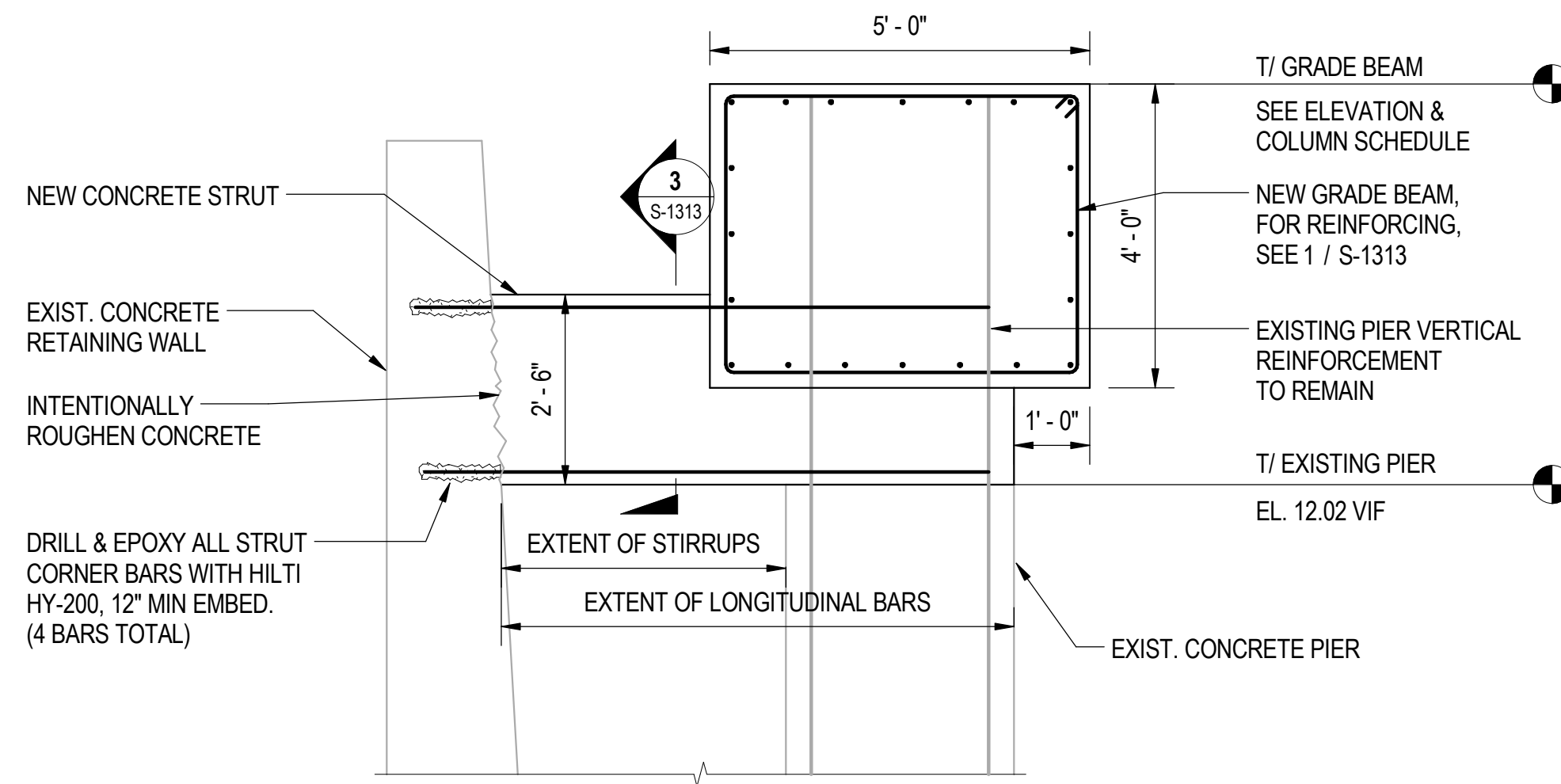
LOCATION IDENTIFIER: DR095

**SOUTH TERMINAL BUS CANOPY FOUNDATION PLANS & ELEVATIONS**

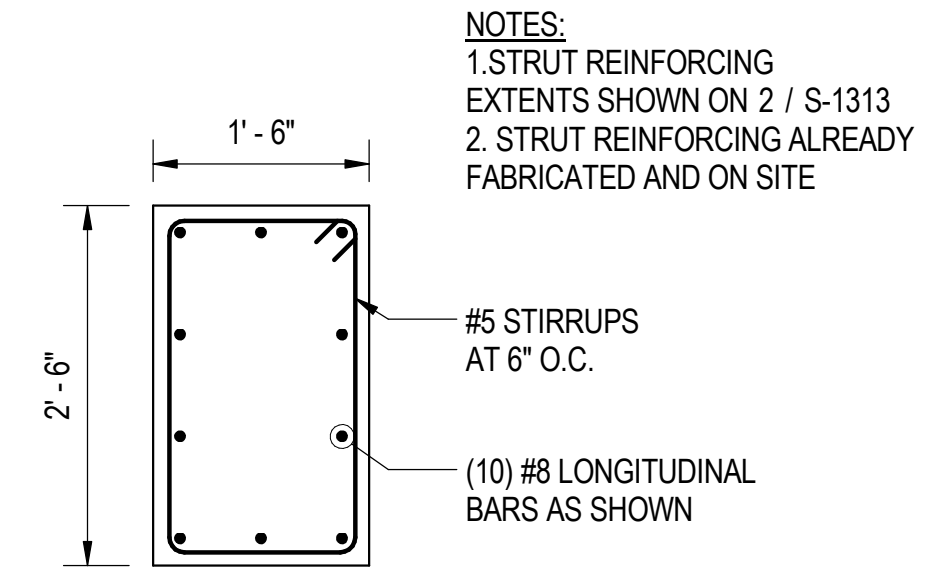
**S-1311**



**1** TYPICAL GRADE BEAM SECTION  
S-1313 SCALE: 3/4" = 1'-0"

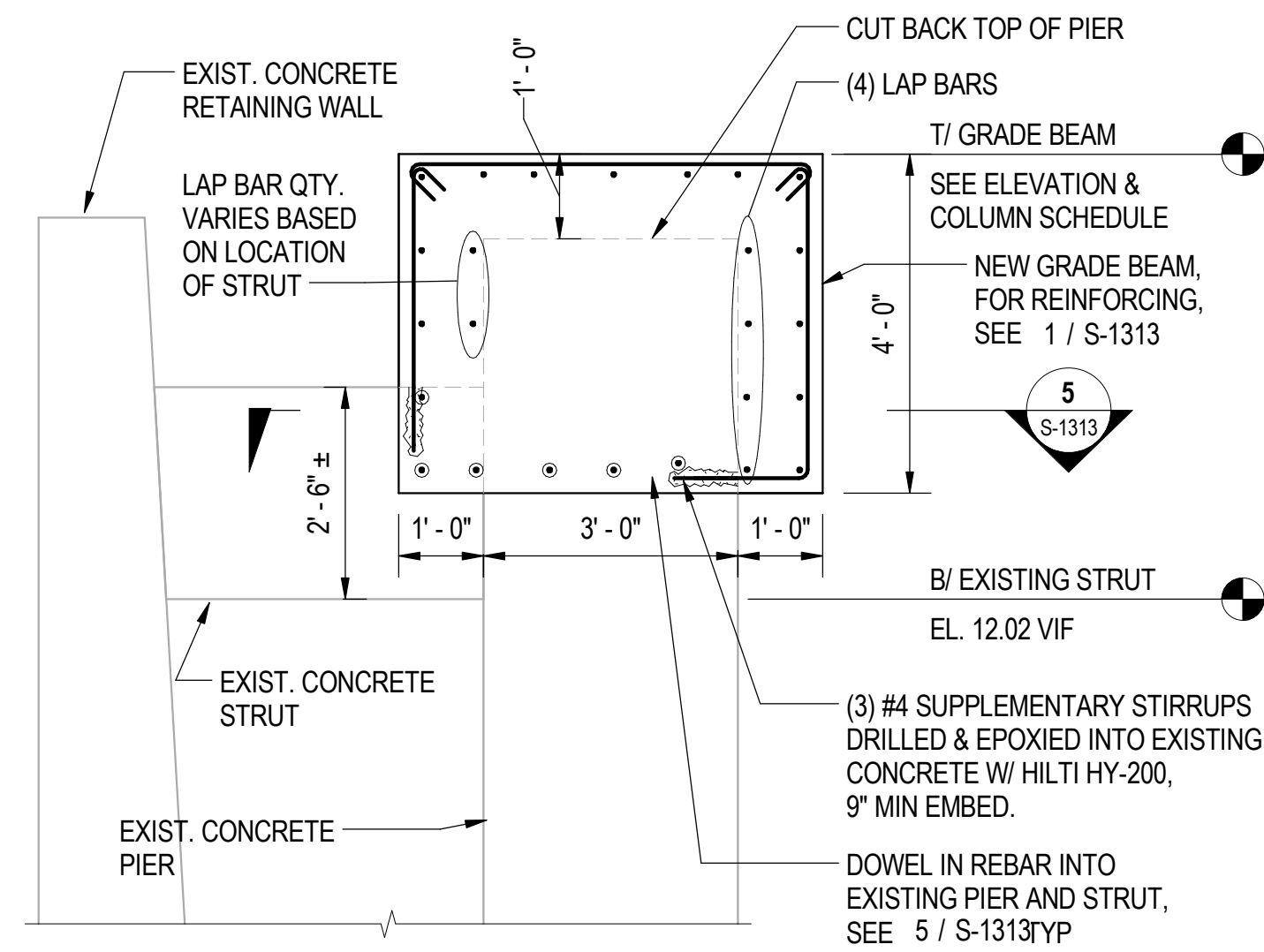


**2** TYPICAL FOUNDATION SECTION - EAST  
S-1313 SCALE: 1/2" = 1'-0"

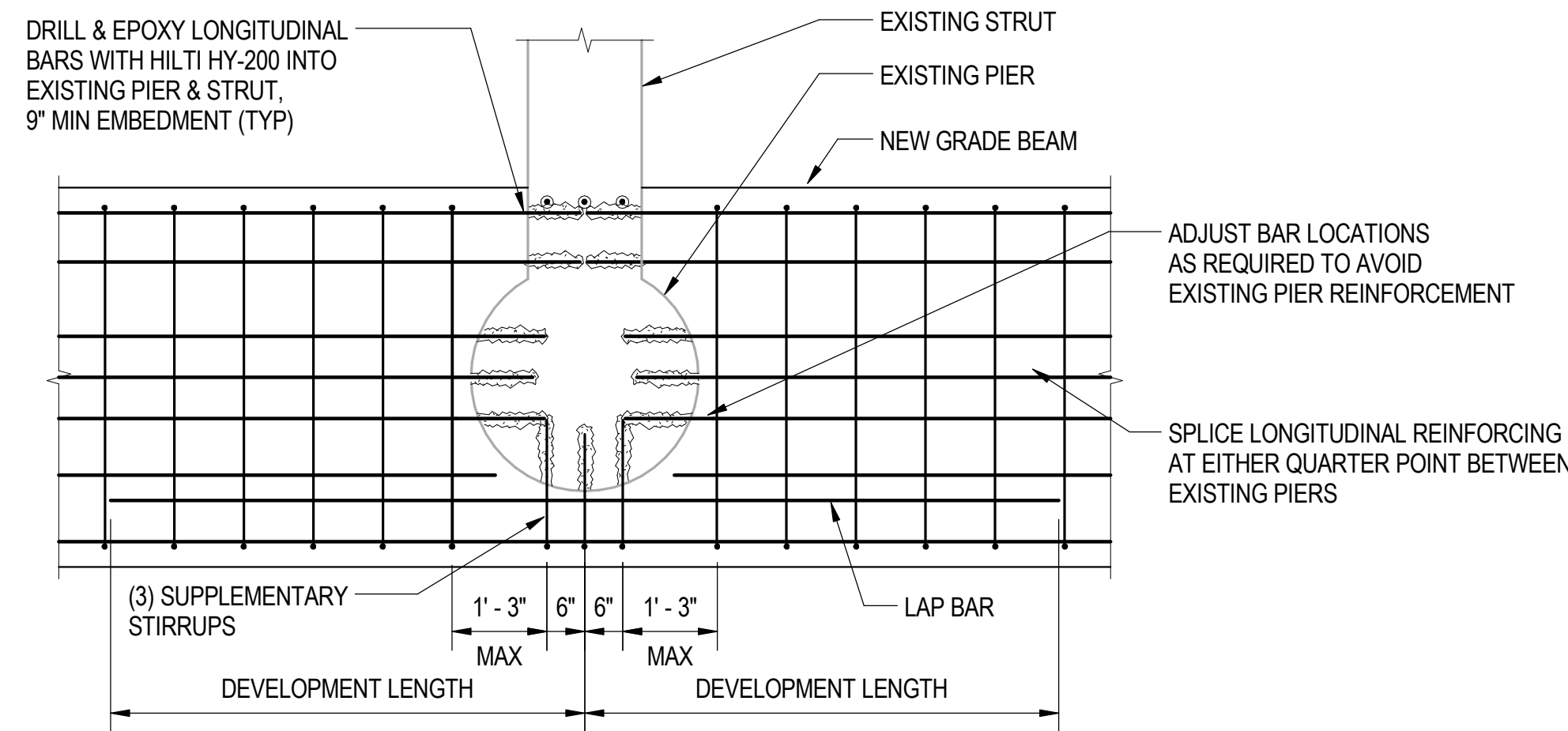


**3** TYPICAL STRUT SECTION  
S-1313 SCALE: 3/4" = 1'-0"

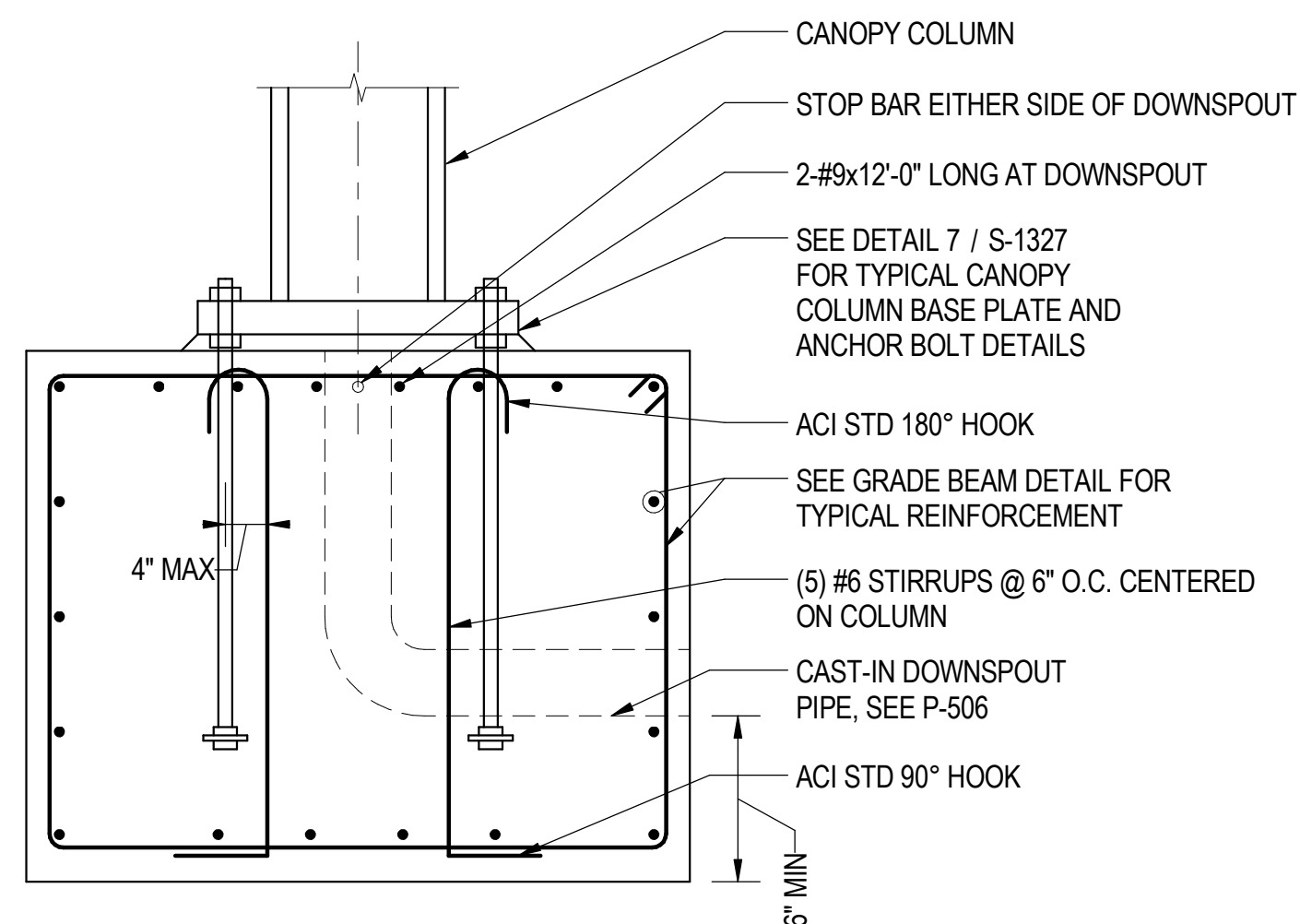
NOTES:  
1. STRUT REINFORCING EXTENTS SHOWN ON 2 / S-1313  
2. STRUT REINFORCING ALREADY FABRICATED AND ON SITE



**4** TYPICAL FOUNDATION SECTION - WEST  
S-1313 SCALE: 1/2" = 1'-0"

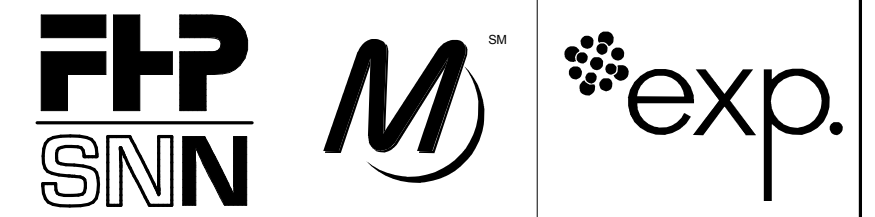


**5** DETAIL - BOTTOM REINFORCEMENT PLAN  
S-1313 SCALE: 1/2" = 1'-0"



**6** REINFORCEMENT AT CANOPY COLUMN BASE PLATE  
S-1313 SCALE: 3/4" = 1'-0"

SHEET NOTES  
1. FOR GENERAL NOTES, SEE DWGS S-001 TO S-004



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**95th Terminal Improvement**  
15 West 95th Street (South Terminal)  
Chicago, IL 60628

IN CHARGE	J. JAKALSKI
APPROVED BY	J. JAKALSKI
CHECKED BY	R.S. NAIR
DESIGNED BY	S. CITKO
DRAWN BY	C. BOVE
PROJECT NO.	2012-0021.02
FILE NAME	D095_S-RoofCENTRAL

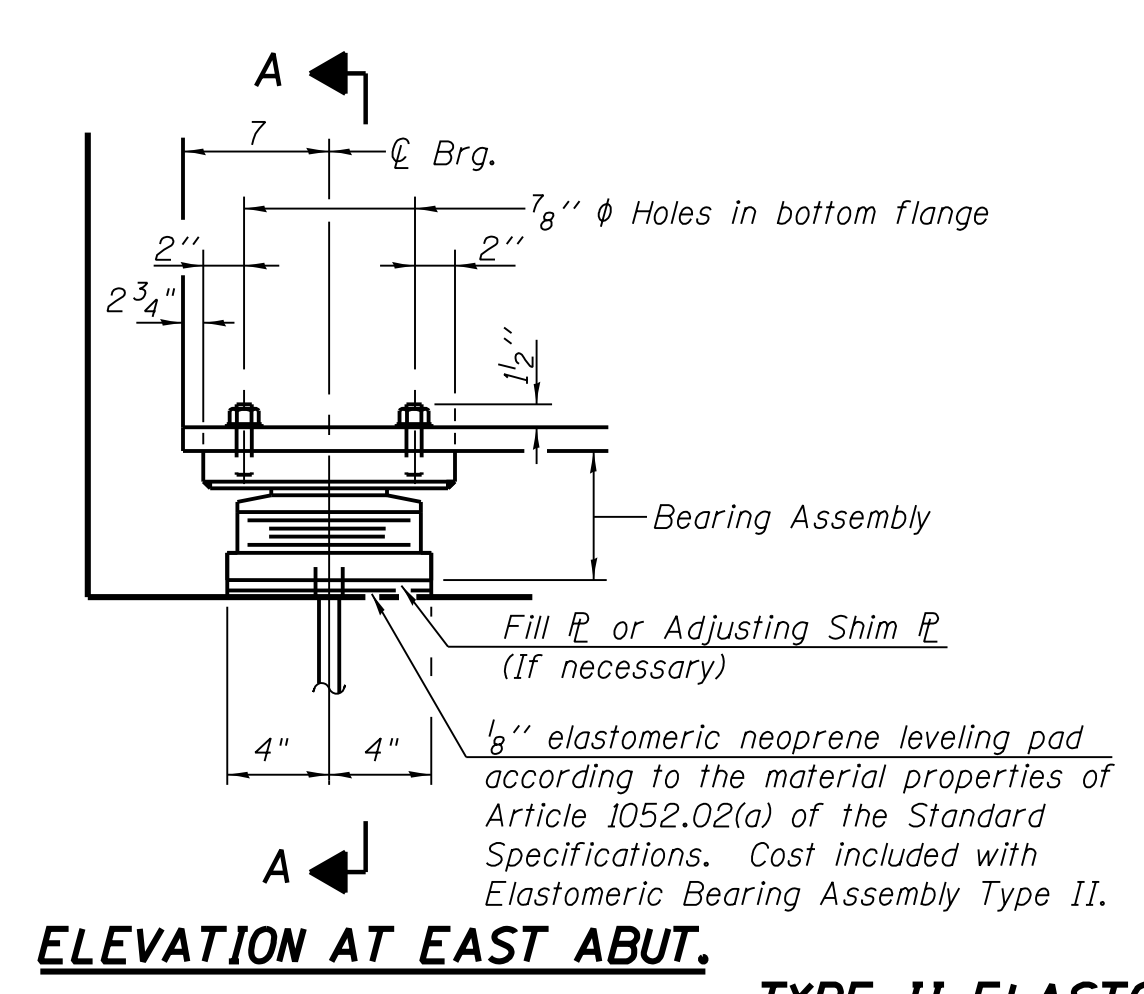
MARK	DATE	DESCRIPTION
	2/9/17	ISSUED FOR CONSTRUCTION

LOCATION IDENTIFIER: **DR095**

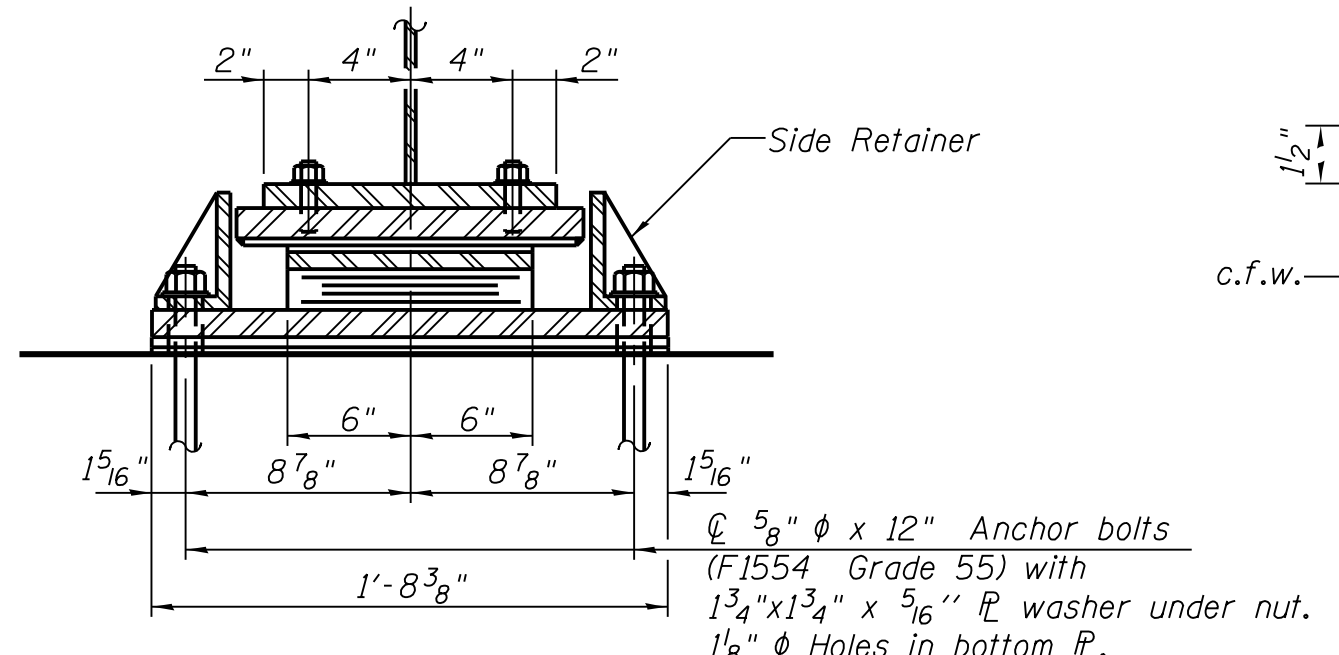
**SOUTH TERMINAL BUS CANOPY FOUNDATION DETAILS**

**S-1313**

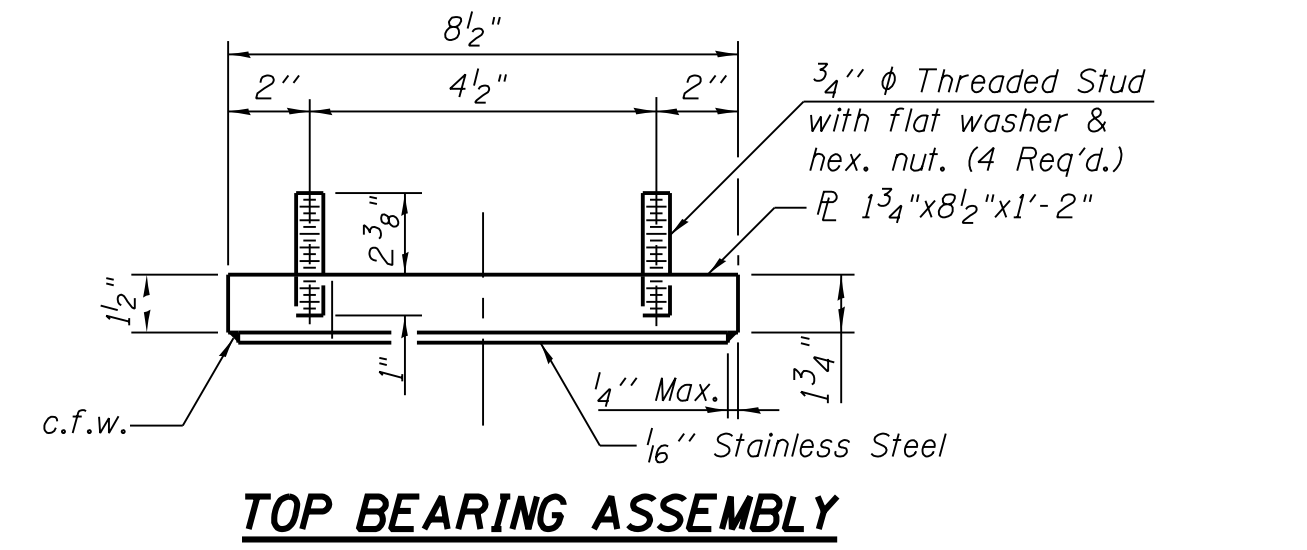
vscarpa



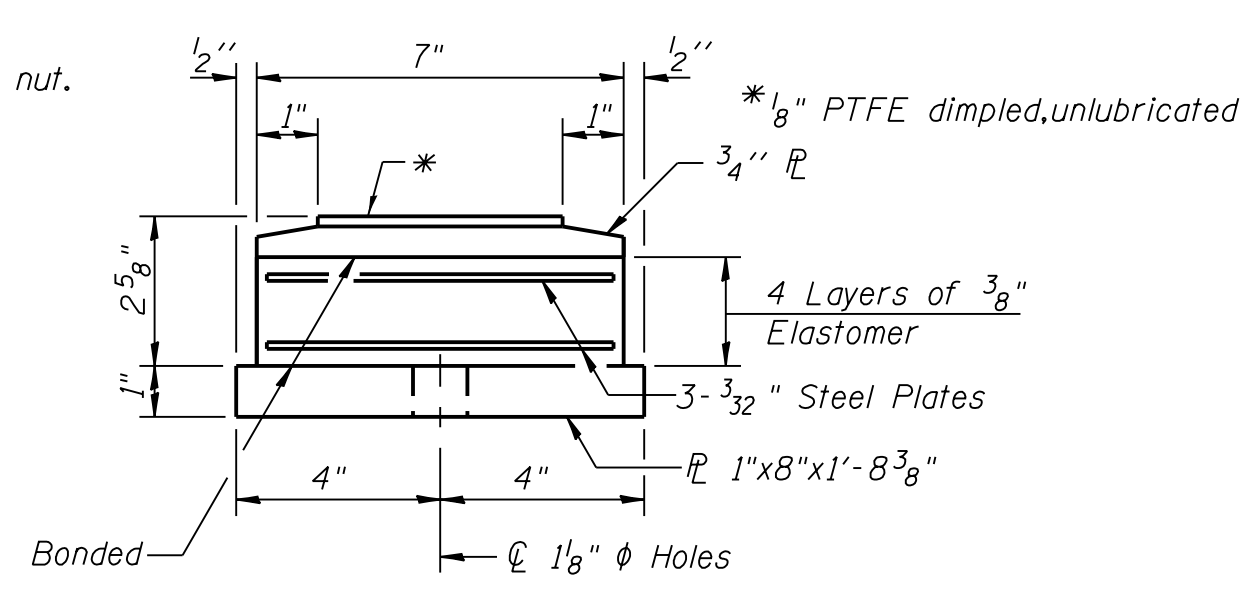
**ELEVATION AT EAST ABUT.**  
**TYPE II ELASTOMERIC EXP. BRG.**



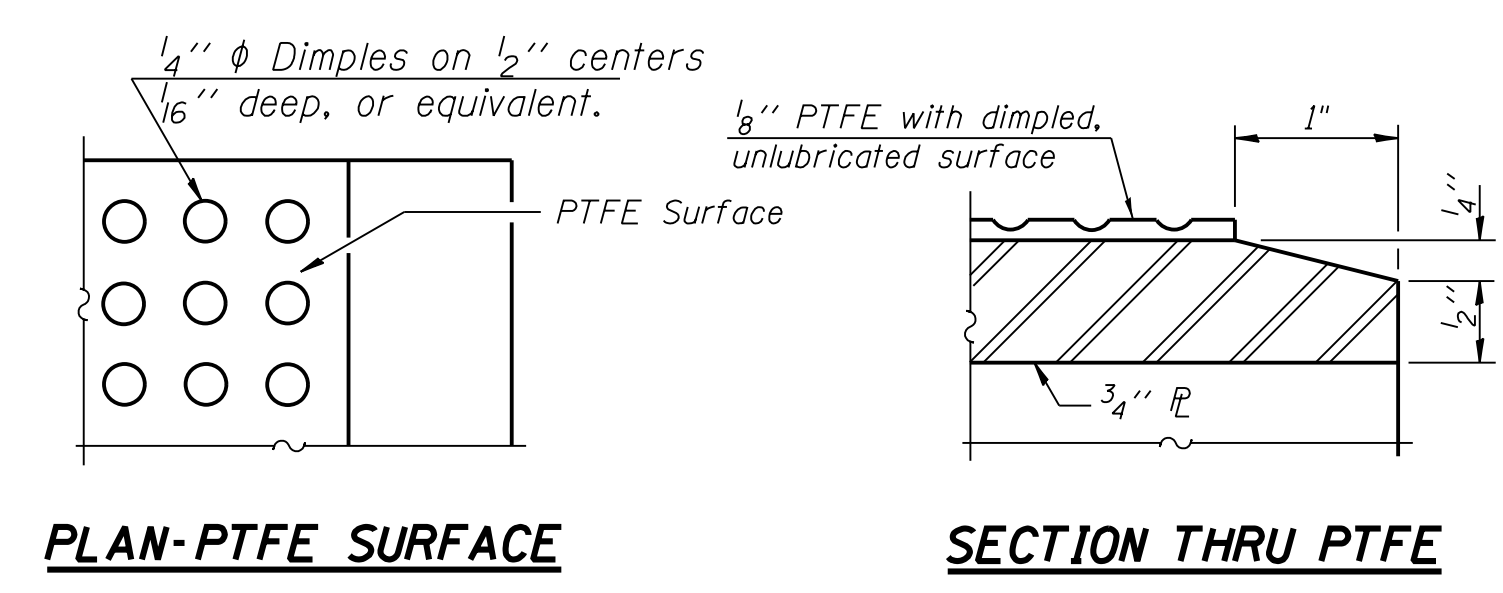
**SECTION A-A**  
Anchor bolts (F1554 Grade 55) with 1 3/4" x 1 3/4" x 5/16" P washer under nut. 1 1/8" φ Holes in bottom P.



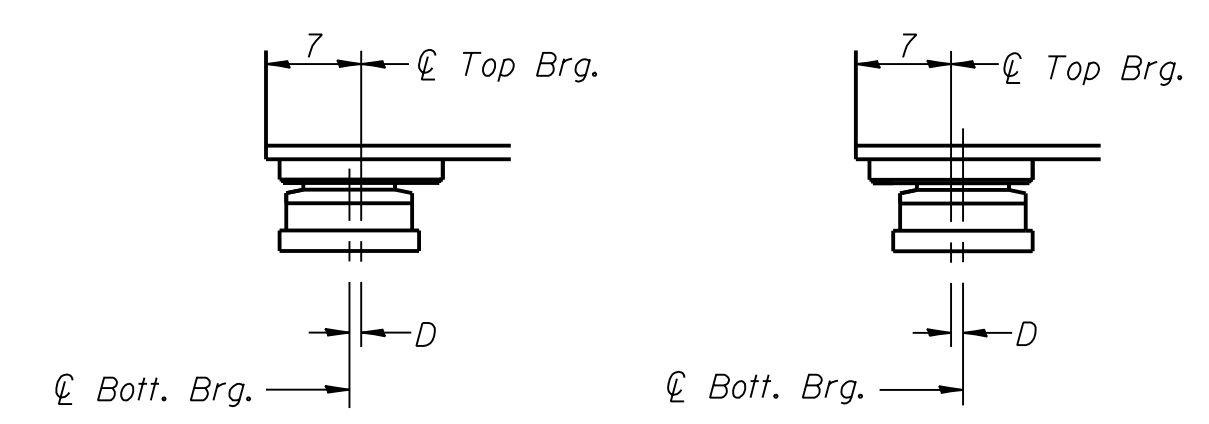
**TOP BEARING ASSEMBLY**



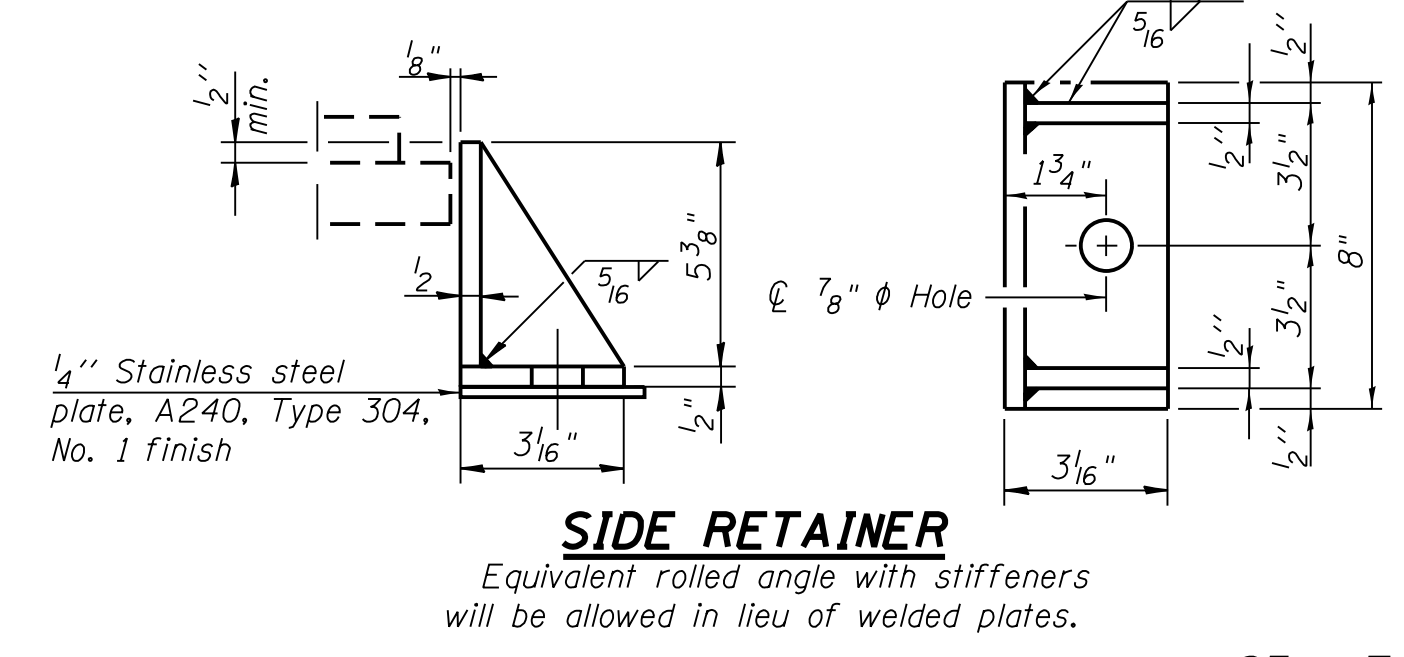
**BOTTOM BEARING ASSEMBLY**



**PLAN-PTFE SURFACE**  
**SECTION THRU PTFE**

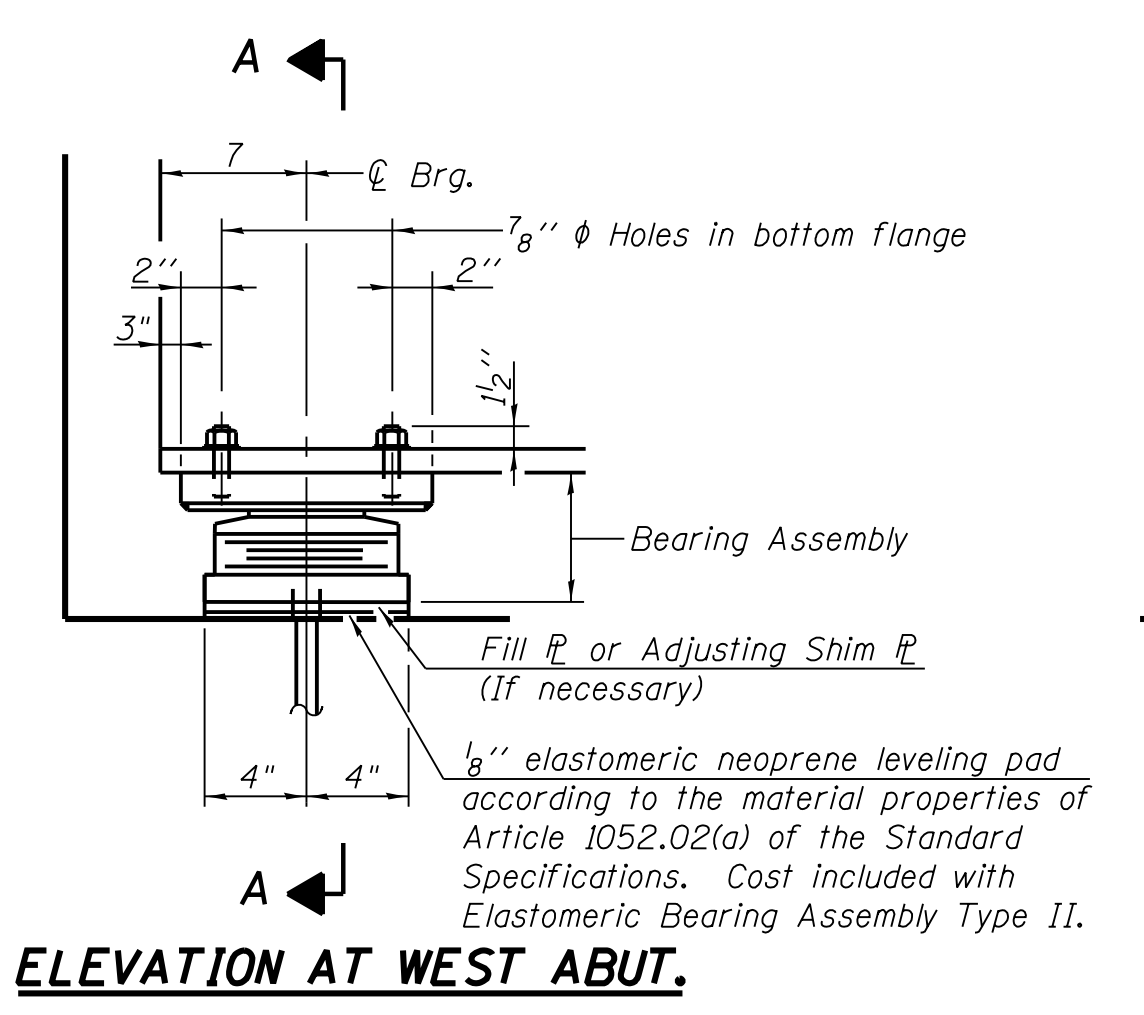


**BELOW 50°F.**  
**ABOVE 50°F.**  
D = 1/8" per each 100' of expansion for every 15° temp. change from the normal temp. of 50°F.  
**EXPANSION BEARING ORIENTATION**  
The above diagrams are for informational purposes only to show the amount of expected offset "D" for the current temperature in the field.

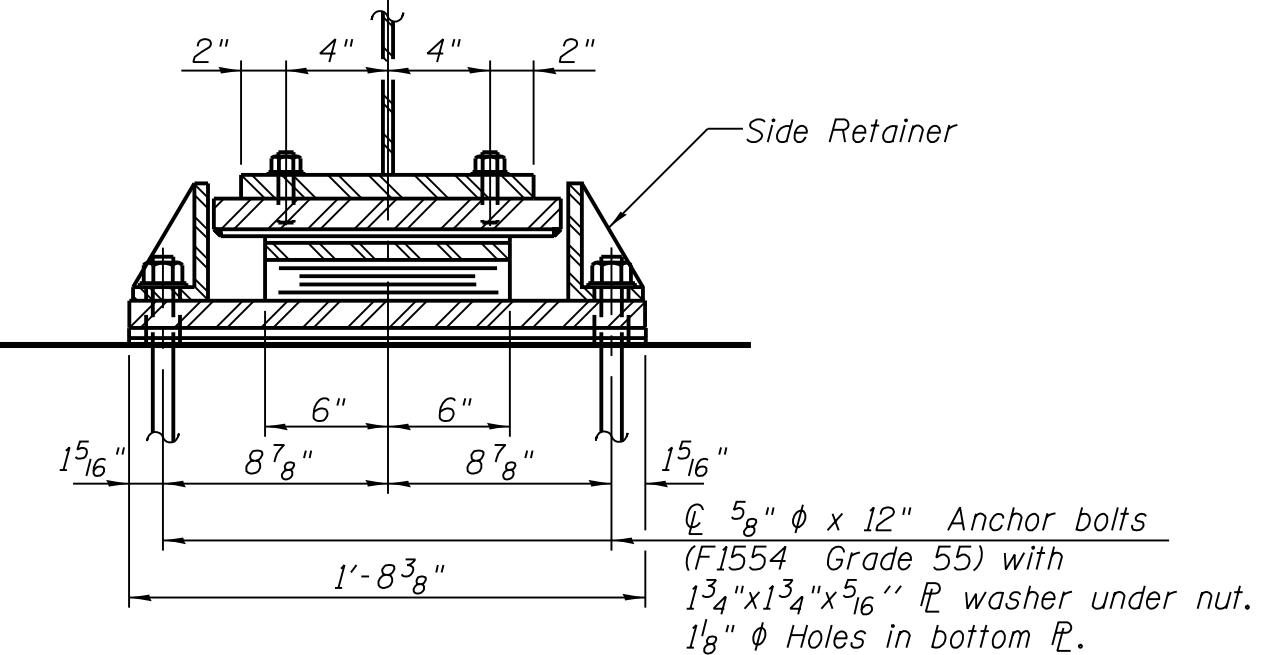


**SIDE RETAINER**  
Equivalent rolled angle with stiffeners will be allowed in lieu of welded plates.

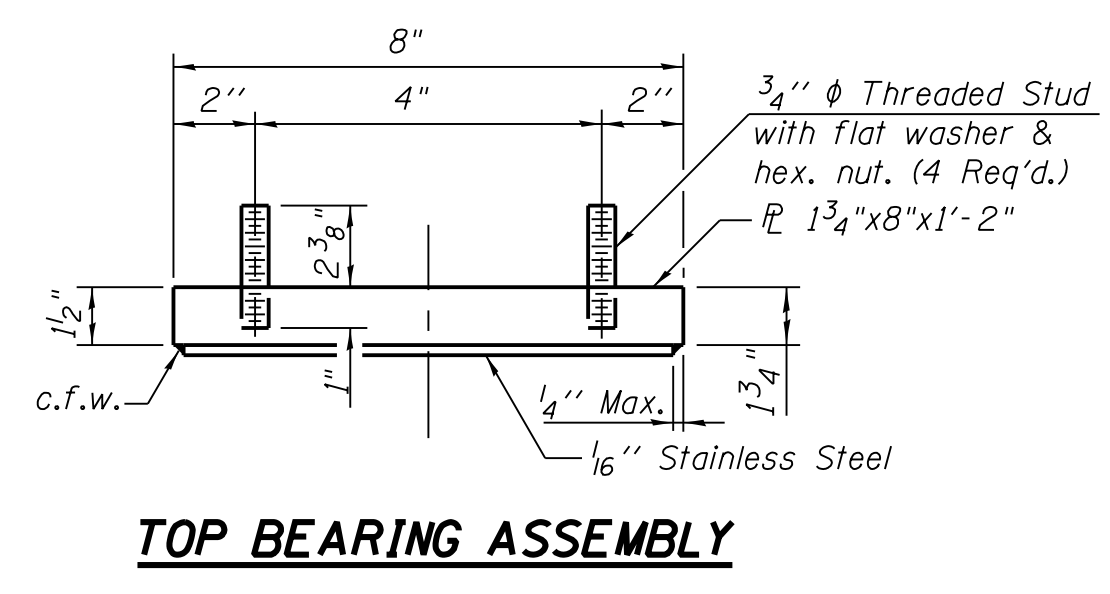
**95th EAST ABUTMENT**  
Beams 1A, 1B, 1E, 1K, 16C & 16H



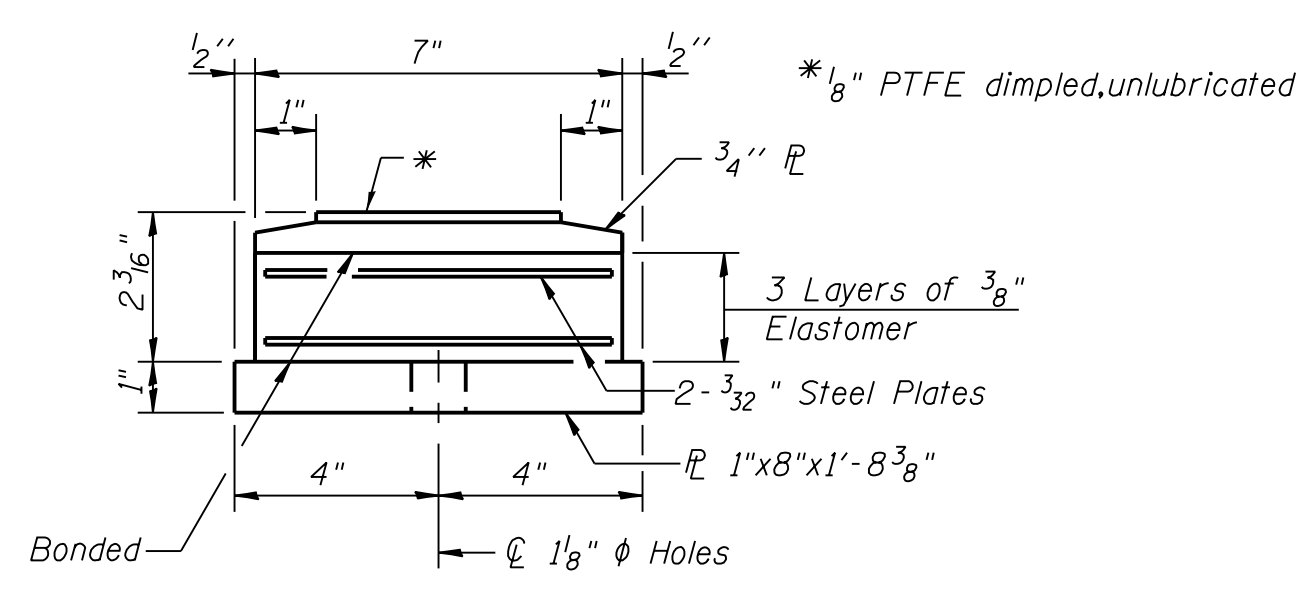
**ELEVATION AT WEST ABUT.**  
**TYPE II ELASTOMERIC EXP. BRG.**



**SECTION A-A**  
Anchor bolts (F1554 Grade 55) with 1 3/4" x 1 3/4" x 5/16" P washer under nut. 1 1/8" φ Holes in bottom P.



**TOP BEARING ASSEMBLY**

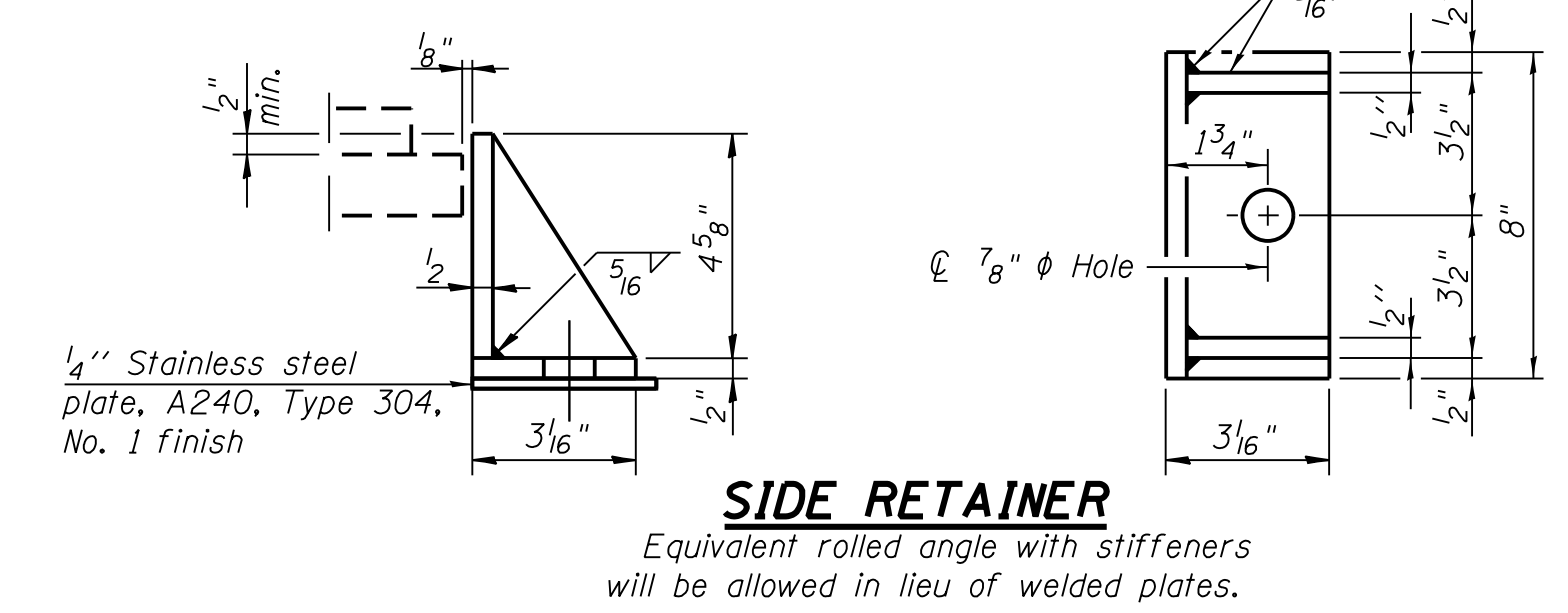


**BOTTOM BEARING ASSEMBLY**

Notes:  
Anchor bolts shall be ASTM F1554 all-thread (or an Engineer-approved alternate material) of the grade(s) and diameter(s) specified. The corresponding specified grade of AASHTO M314 anchor bolts may be used in lieu of ASTM F1554.  
Side retainers and other steel members required for the elastomeric bearing assembly shall be included in the cost of Elastomeric Bearing Assembly, Type I.  
Beams shall be braced for stability during erection and remain braced until deck is poured and cured.  
Anchor bolts and side retainers at all supports shall be installed as each member is erected unless an equivalent temporary means of lateral restraint is used.  
For steel extensions see sheet S-132.B.

**FILL PLATE TABLE**

West Abutment		East Abutment	
Beam	Dimensions	Beam	Dimensions
1B	7/16" x 8" x 1'-2"	1B	5/16" x 8 1/2" x 1'-2"
1H	9/16" x 8" x 1'-2"	16C	5/8" x 8 1/2" x 1'-2"
16F	1/2" x 8" x 1'-2"	16H	1/6" x 8 1/2" x 1'-2"



**SIDE RETAINER**  
Equivalent rolled angle with stiffeners will be allowed in lieu of welded plates.

**95th WEST ABUTMENT**  
Beams 1A, 1B, 1E, 1H, 16C & 16F



**CHICAGO TRANSIT AUTHORITY ENGINEERING**

**SENSITIVE SECURITY INFORMATION**  
WARNING: THIS RECORD CONTAINS SENSITIVE SECURITY INFORMATION THAT IS CONTROLLED UNDER 49 CFR PARTS 15 AND 1550. NO PART OF THIS RECORD MAY BE DISCLOSED TO PERSONS WITHOUT A "NEED TO KNOW" AS DEFINED IN CFR PARTS 15 AND 1550. EXCEPT WITH THE WRITTEN PERMISSION OF THE ADMINISTRATOR OF THE TRANSPORTATION SECURITY ADMINISTRATION OR THE SECRETARY OF TRANSPORTATION. UNAUTHORIZED RELEASE MAY RESULT IN CIVIL PENALTY OR OTHER ACTION. FOR U.S. GOVERNMENT AGENCIES, PUBLIC DISCLOSURE IS GOVERNED BY 5 U.S.C. 552 AND 49 CFR PARTS 15 AND 1550.

**DRAWING SCALE IS NOT GUARANTEED. CTA ASSUMES NO RISK OF LIABILITY FOR ERRORS CAUSED, DIRECTLY OR INDIRECTLY, BY SCALING OF THIS DRAWING.**

**95th Terminal Improvement**  
15 West 95th Street (South Terminal)  
Chicago, IL 60628

IN CHARGE	S. KADERBEK
APPROVED BY	D.ZROKA
CHECKED BY	P.MAID
DESIGNED BY	L.SOMMER
DRAWN BY	T.SLEDGE
PROJECT NO.	2012-0021
FILE NAME	File Name

MARK	DATE	DESCRIPTION
	2/9/17	ISSUED FOR CONSTRUCTION

LOCATION IDENTIFIER: **DR095**

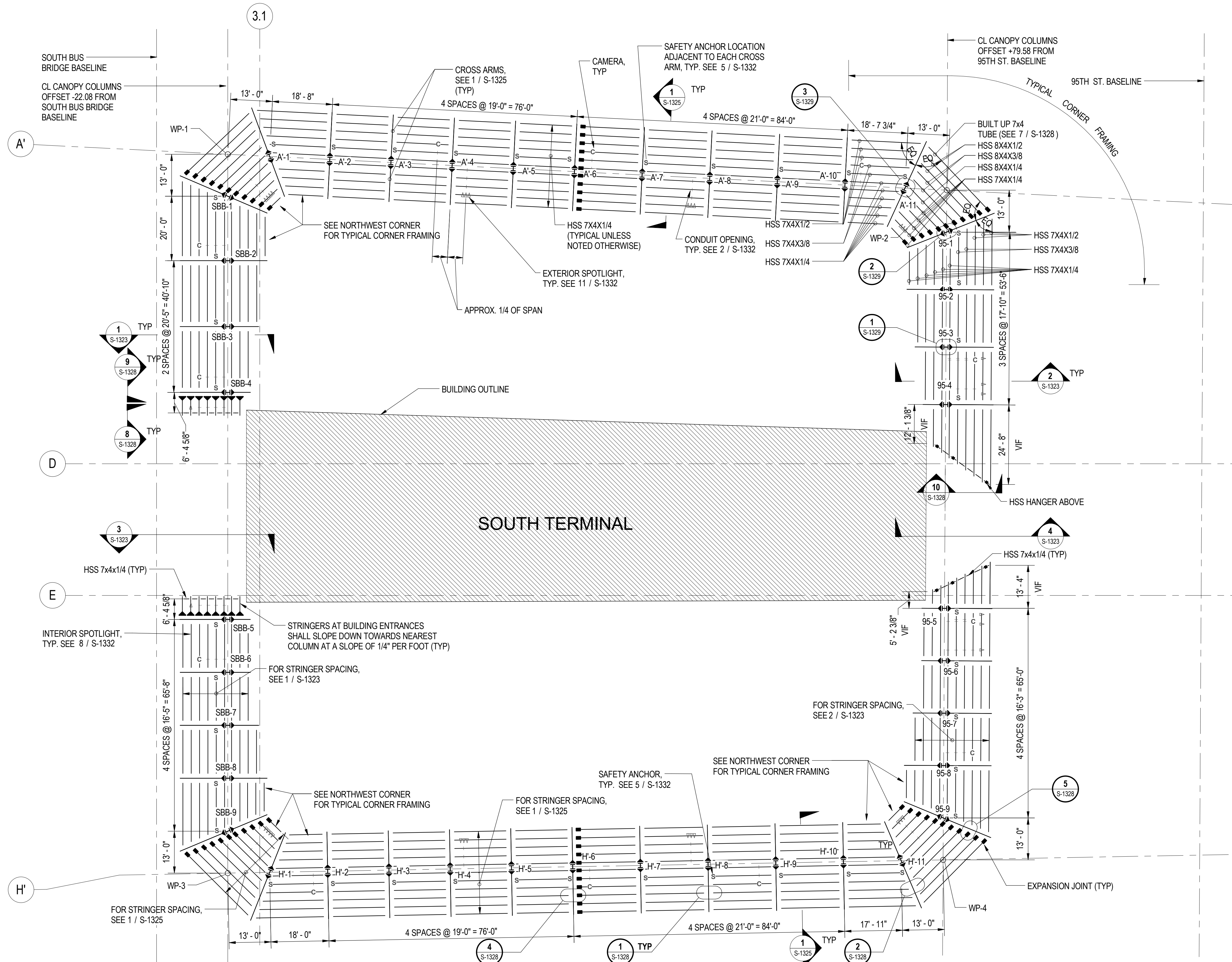
95TH ST. BRIDGE  
BEARING DETAILS  
(SHEET 2 OF 5)  
SN 016-0437

**95TH-S-132**

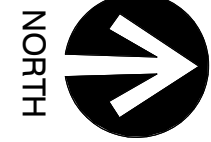
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PLOT DATE = 8/31/2016

01/12/2016 4:47:33 PM

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**1** SOUTH TERMINAL - CANOPY FRAMING PLAN  
 S-1321 SCALE: 1/16" = 1'-0"



**SHEET NOTES**

- 1. FOR GENERAL NOTES, SEE DWGS S-001 TO S-004
- 2. ALL CANOPY STEEL IS TO BE HOT-DIP GALVANIZED.



**cta CHICAGO TRANSIT AUTHORITY ENGINEERING**

**SENSITIVE SECURITY INFORMATION**  
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**95th Terminal Improvement**  
 15 West 95th Street (South Terminal)  
 Chicago, IL 60628

IN CHARGE	J. JAKALSKI
APPROVED BY	J. JAKALSKI
CHECKED BY	R.S. NAIR
DESIGNED BY	S. CITKO
DRAWN BY	C. BOVE
PROJECT NO.	2012-0021.02
FILE NAME	D095_S-RoofCENTRAL

MARK	DATE	DESCRIPTION
	2/9/17	ISSUED FOR CONSTRUCTION

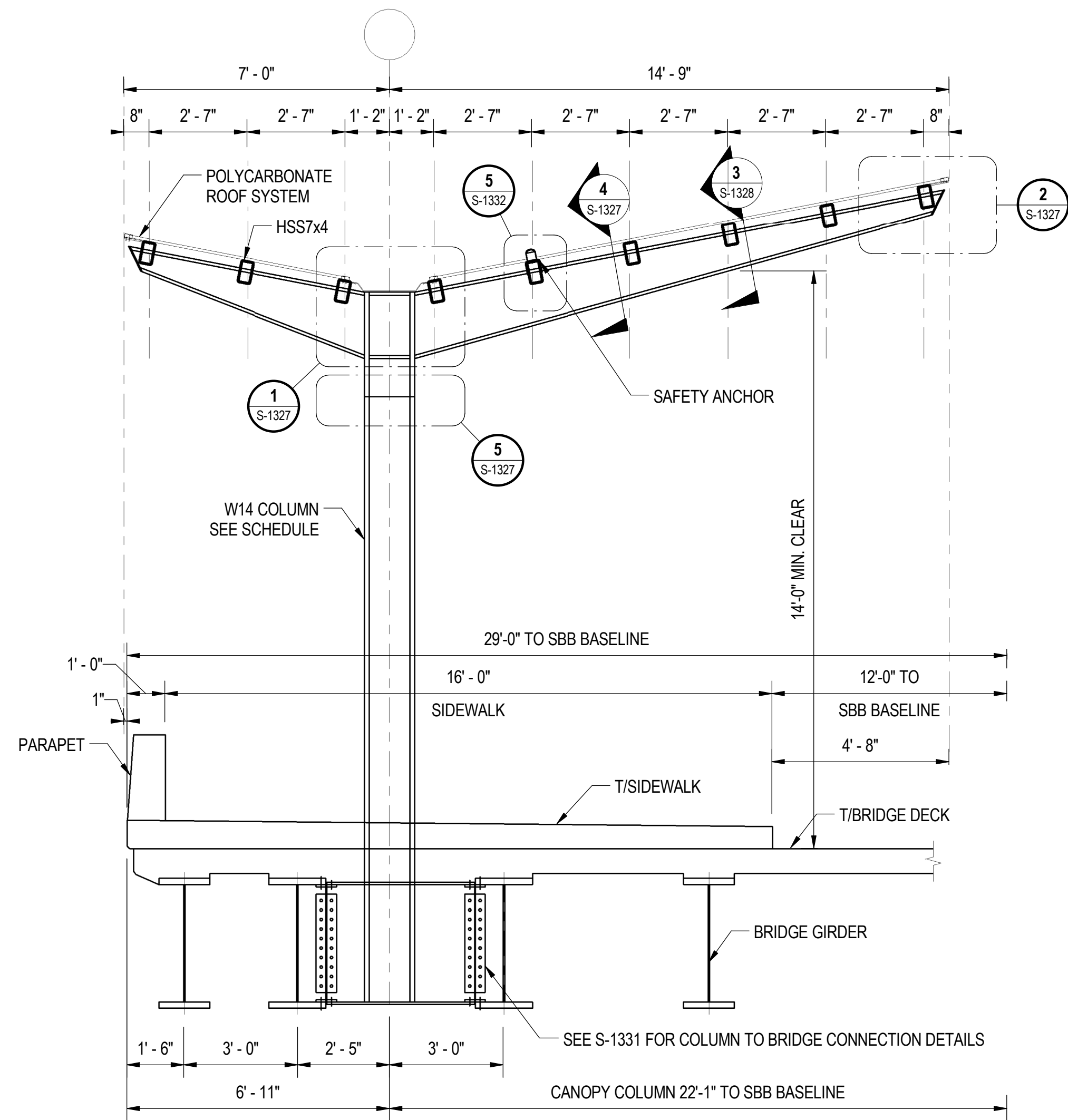
LOCATION IDENTIFIER: **DR095**

**SOUTH TERMINAL BUS CANOPY FRAMING PLAN**

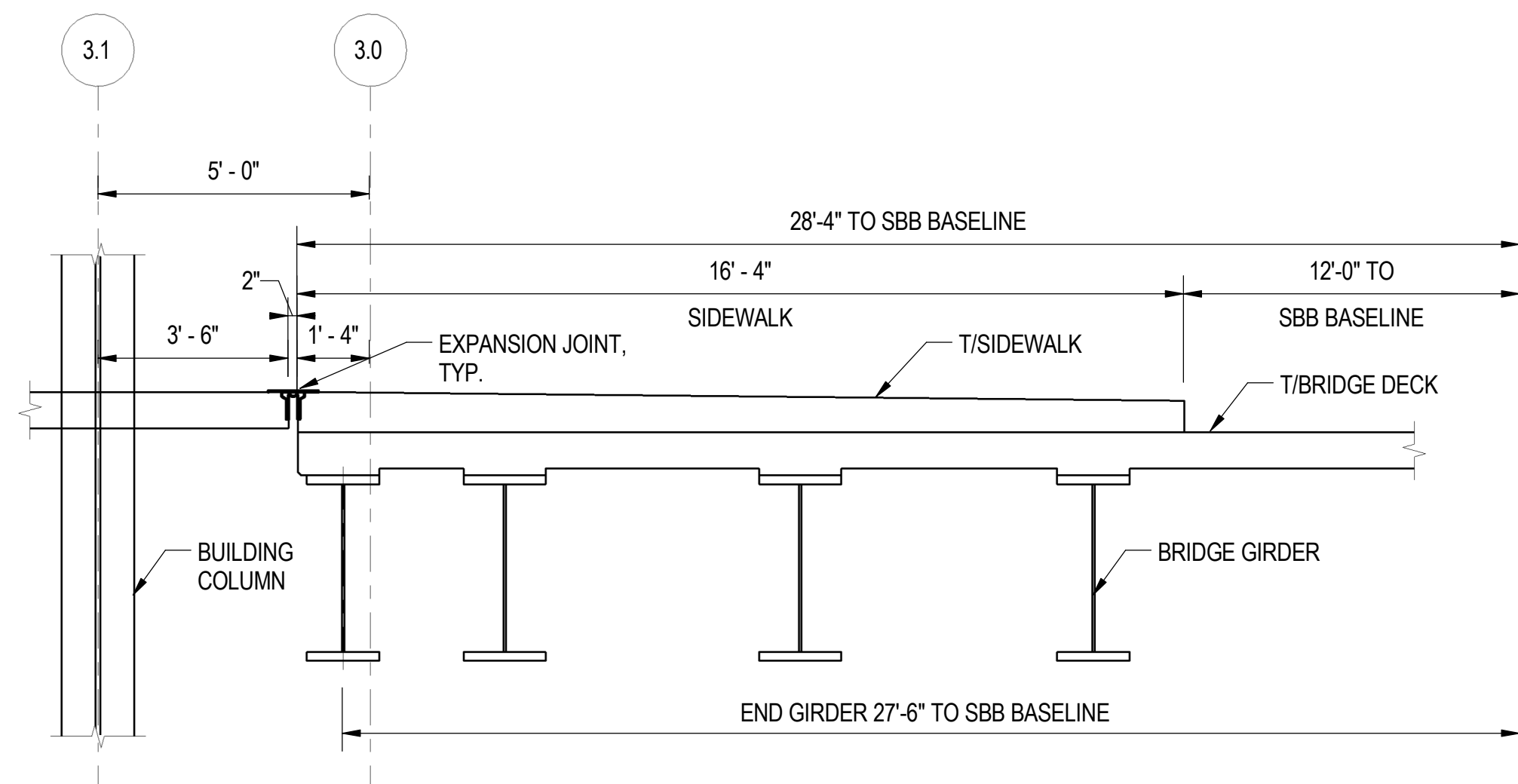
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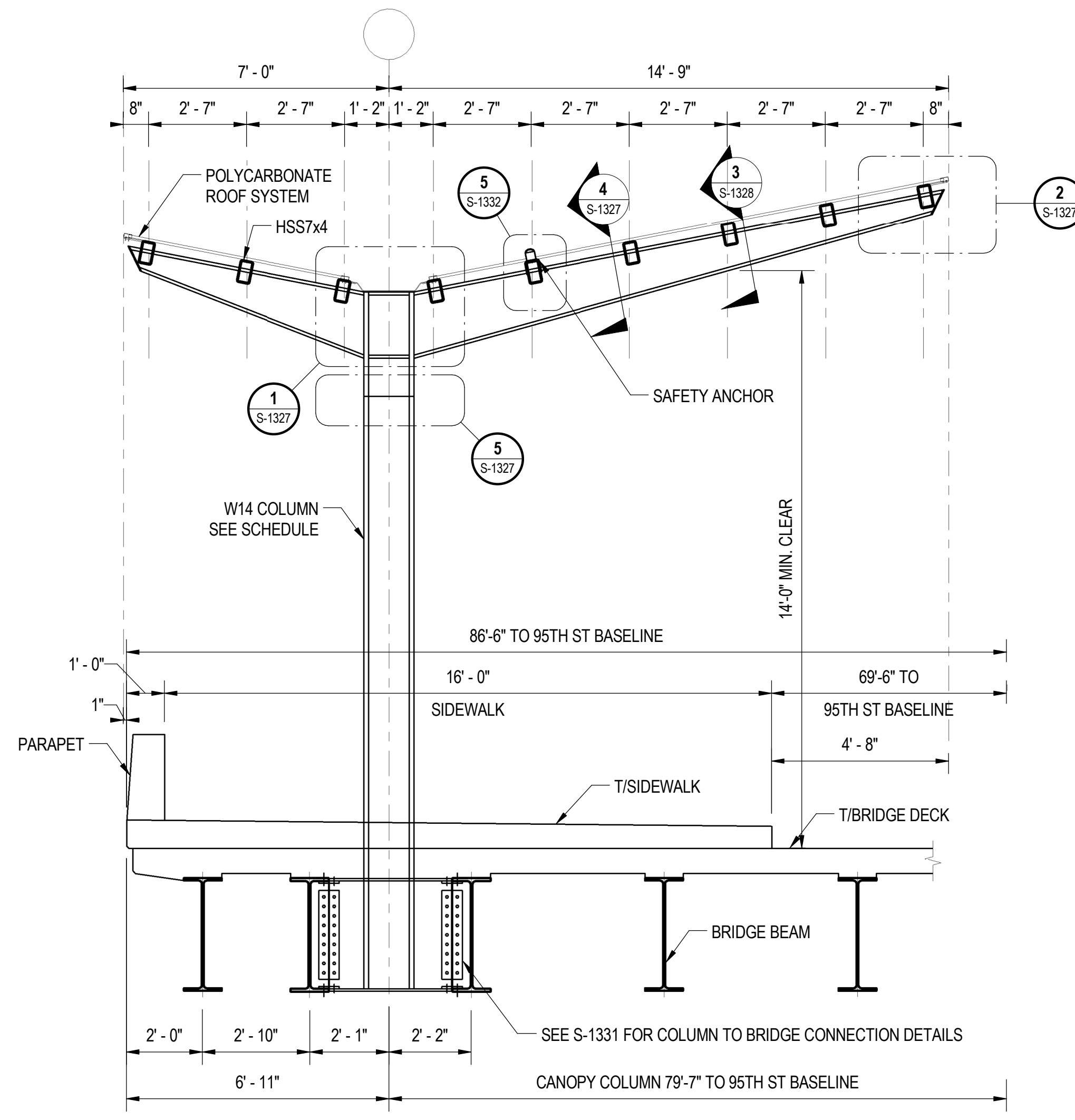
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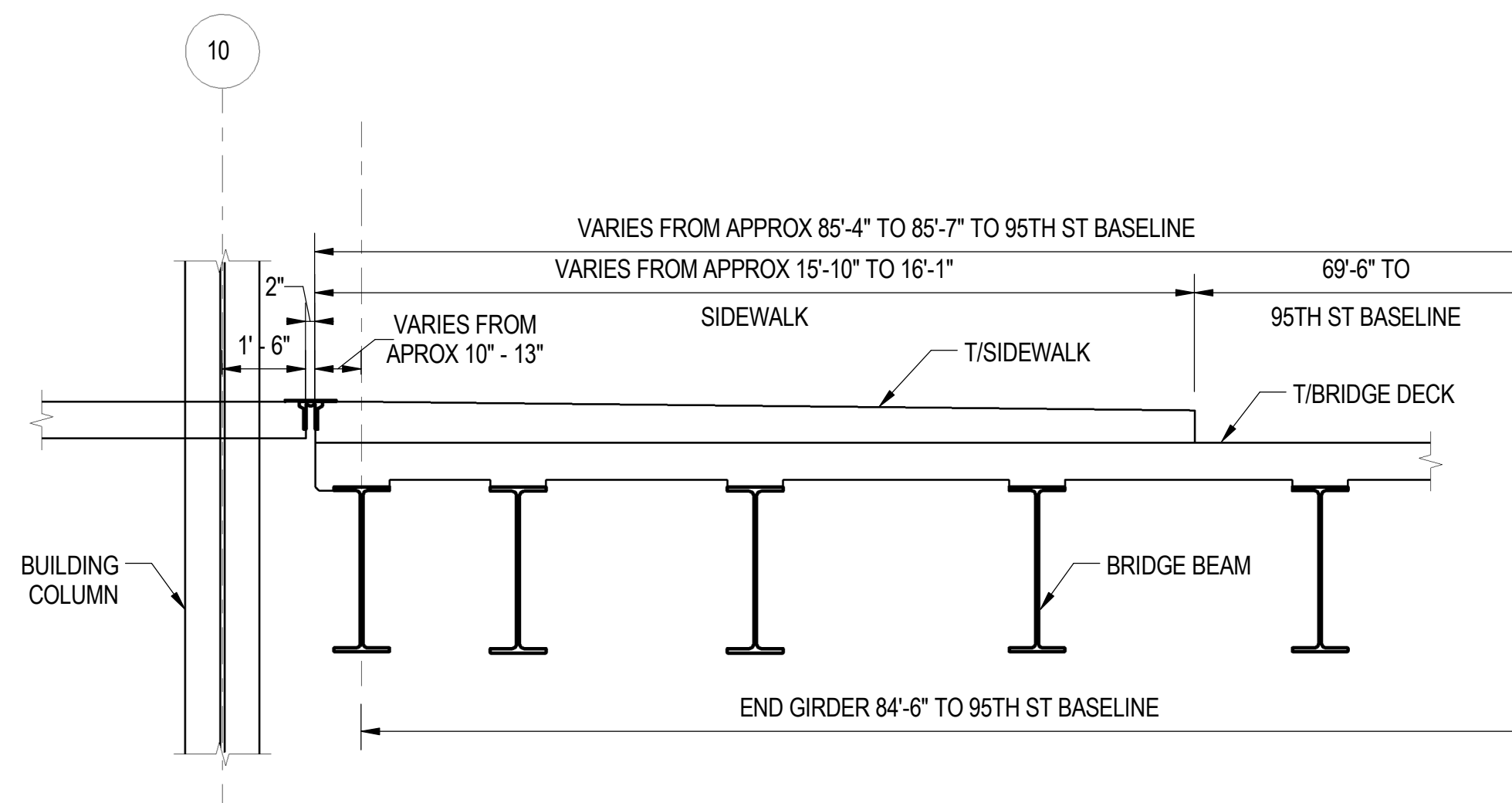
**1** TYPICAL SECTION SOUTH BUS BRIDGE AT PARAPET  
S-1323 SCALE: 3/8" = 1'-0"



**3** TYPICAL SECTION SOUTH BUS BRIDGE AT ENTRY  
S-1323 SCALE: 3/8" = 1'-0"



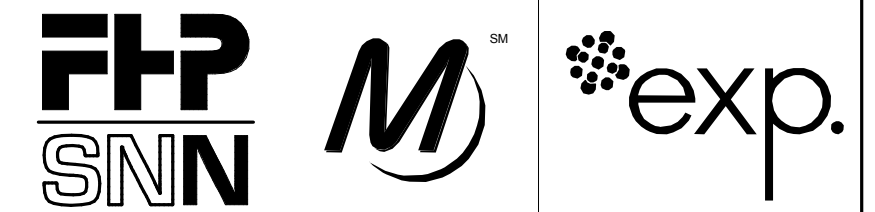
**2** TYPICAL SECTION 95TH ST AND SOUTH TERMINAL AT PARAPET  
S-1323 SCALE: 3/8" = 1'-0"



**4** TYPICAL SECTION 95TH ST AND SOUTH TERMINAL AT ENTRY  
S-1323 SCALE: 3/8" = 1'-0"

**SHEET NOTES**

- 1. FOR POLYCARBONATE ROOF SEE ARCHITECTURAL AS SERIES SHEETS.
- 2. FOR EXPANSION JOINTS SEE BRIDGE PLANS.



**SENSITIVE SECURITY INFORMATION**  
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**95th Terminal Improvement**  
15 West 95th Street (South Terminal)  
Chicago, IL 60628

IN CHARGE	J. JAKALSKI
APPROVED BY	J. JAKALSKI
CHECKED BY	R.S. NAIR
DESIGNED BY	S. CITKO
DRAWN BY	D. LALLY
PROJECT NO.	2012-0021.02
FILE NAME	D095_S-RoofCENTRAL

MARK	DATE	DESCRIPTION
	2/9/17	ISSUED FOR CONSTRUCTION

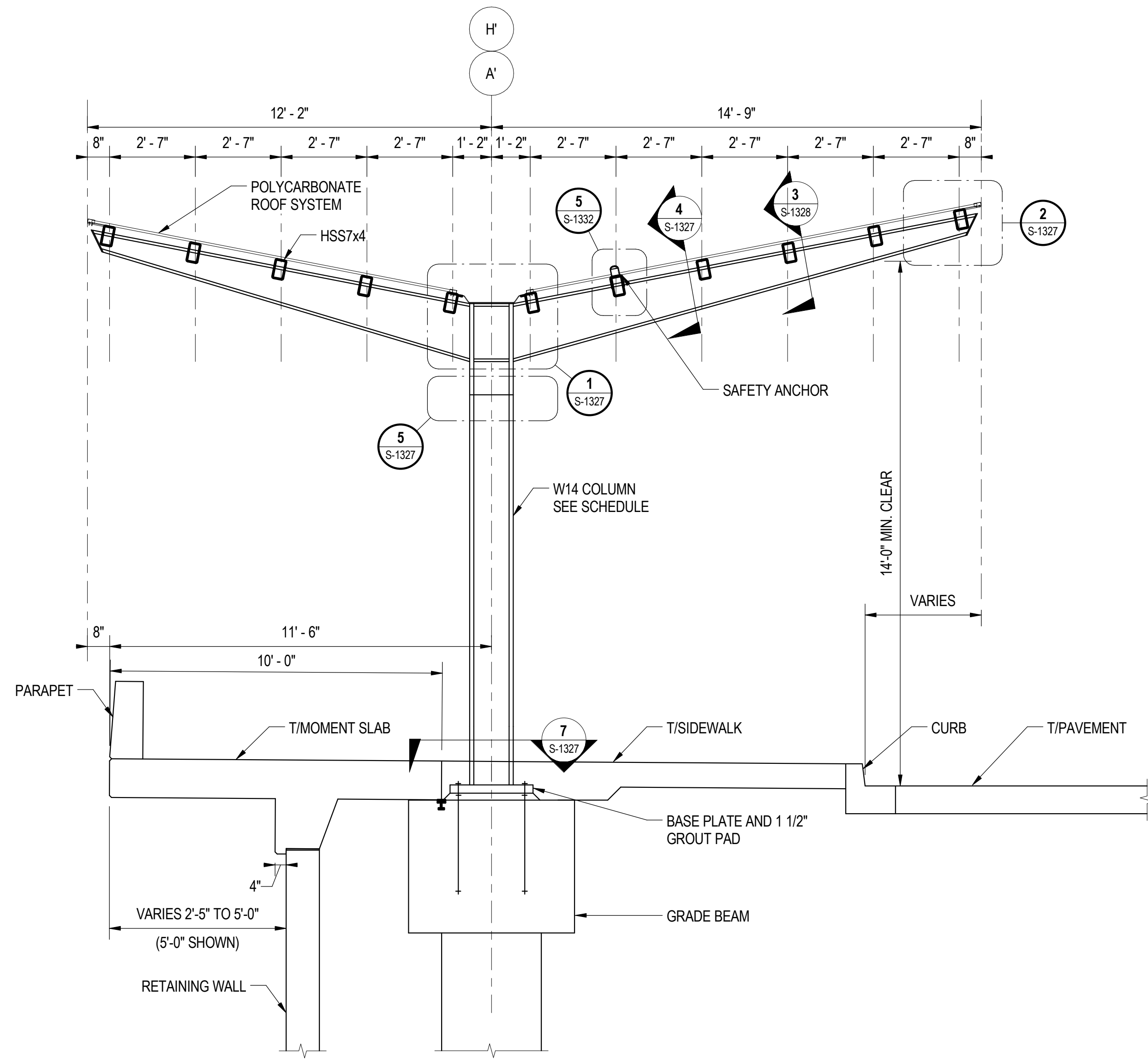
LOCATION IDENTIFIER: **DR095**

**BUS CANOPY SECTIONS AT SOUTH TERMINAL**

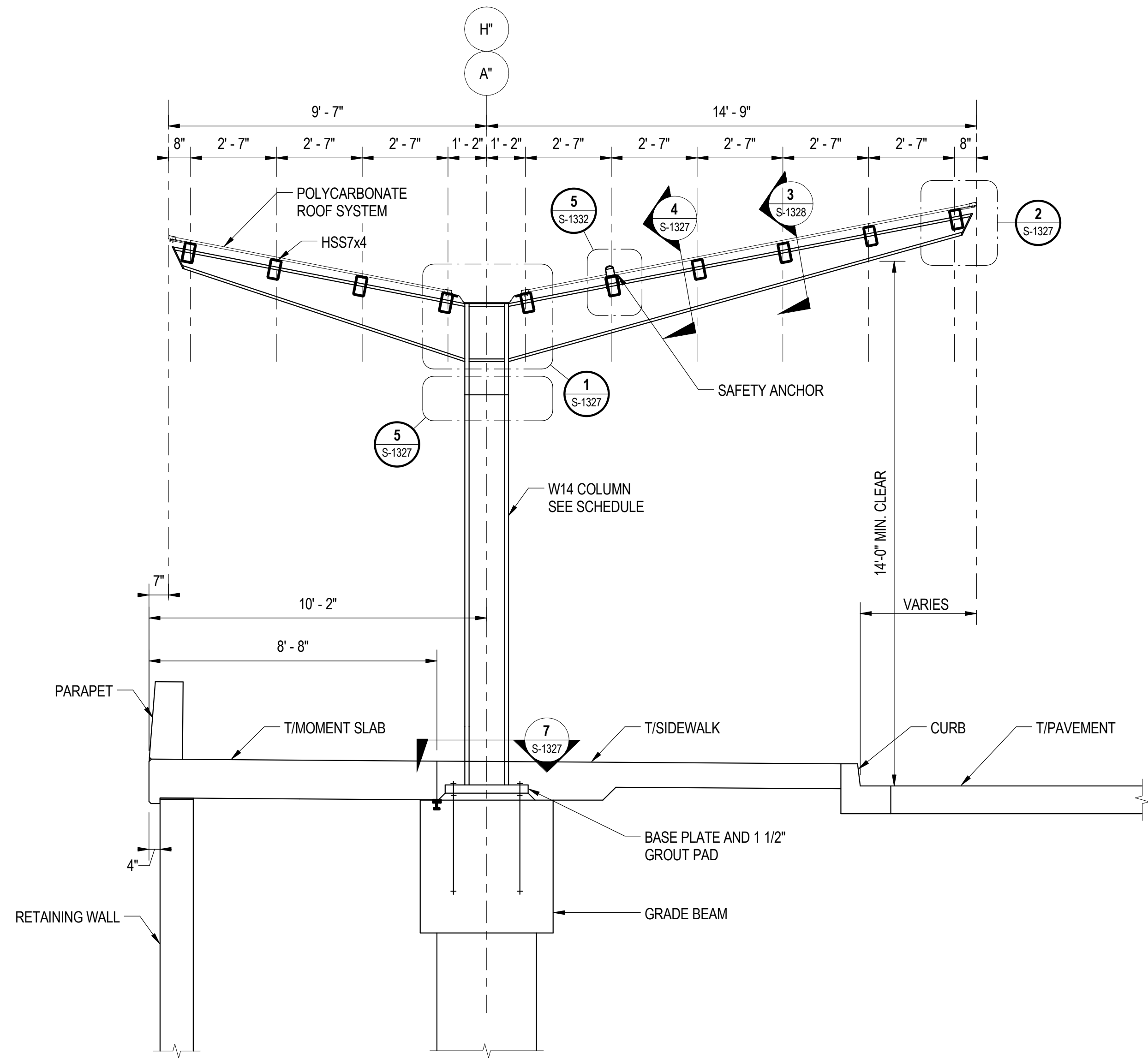
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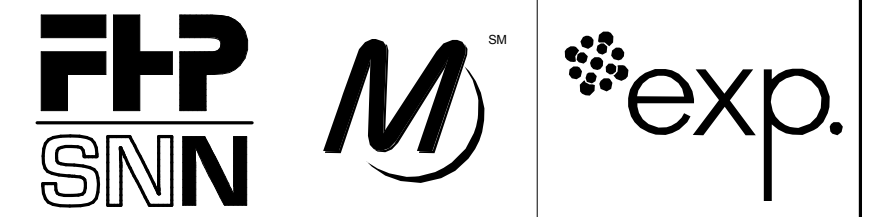
**1** TYPICAL SECTION AT SOUTH TERMINAL RETAINING WALLS  
S-1325 SCALE: 3/8" = 1'-0"



**2** TYPICAL SECTION AT NORTH TERMINAL RETAINING WALLS  
S-1325 SCALE: 3/8" = 1'-0"

**SHEET NOTES**

- 1. FOR POLYCARBONATE ROOF SEE ARCHITECTURAL AS SERIES SHEETS.
- 2. FOR EXPANSION JOINTS SEE BRIDGE PLANS.



**SENSITIVE SECURITY INFORMATION**  
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**95th Terminal Improvement**  
15 West 95th Street (South Terminal)  
Chicago, IL 60628

IN CHARGE	J. JAKALSKI
APPROVED BY	J. JAKALSKI
CHECKED BY	R.S. NAIR
DESIGNED BY	S. CITKO
DRAWN BY	D. LALLY
PROJECT NO.	2012-0021.02
FILE NAME	D095_S-RoofCENTRAL

MARK	DATE	DESCRIPTION
	2/9/17	ISSUED FOR CONSTRUCTION

LOCATION IDENTIFIER: **DR095**

**BUS CANOPY SECTIONS AT RETAINING WALLS**

**S-1325**



**SOUTH CANOPY - WEST SIDE**

COLUMN LINE	COLUMN NUMBER	COLUMN SIZE	T/ COLUMN ELEVATION	BOTTOM ELEVATION	BOTTOM ELEVATION DESCRIPTION
SBB	4	W14X211	34.30'	19.70'	T/ BRIDGE GIRDER
SBB	3	W14X211	34.45'	18.97'	T/ BRIDGE GIRDER
SBB	2	W14X211	34.60'	18.15'	T/ BRIDGE GIRDER
SBB	1	W14X257	34.75'	17.35'	T/ BRIDGE GIRDER
A'	1	W14X257	34.60'	17.51'	T/ GRADE BEAM
A'	2	W14X211	34.34'	17.88'	T/ GRADE BEAM
A'	3	W14X211	34.08'	18.27'	T/ GRADE BEAM
A'	4	W14X211	33.82'	18.49'	T/ GRADE BEAM
A'	5	W14X211	33.56'	18.48'	T/ GRADE BEAM
A'	6	W14X211	33.30'	18.25'	T/ GRADE BEAM
A'	7	W14X211	33.02'	17.82'	T/ GRADE BEAM
A'	8	W14X211	32.73'	17.39'	T/ GRADE BEAM
A'	9	W14X211	32.44'	16.96'	T/ GRADE BEAM
A'	10	W14X211	32.16'	16.53'	T/ GRADE BEAM
A'	11	W14X257	31.90'	15.67'	T/ GRADE BEAM
95	1	W14X257	32.05'	16.34'	T/ BRIDGE GIRDER
95	2	W14X211	31.90'	16.67'	T/ BRIDGE GIRDER
95	3	W14X211	31.75'	16.99'	T/ BRIDGE GIRDER
95	4	W14X211	31.60'	17.32'	T/ BRIDGE GIRDER

**NORTH CANOPY - WEST SIDE**

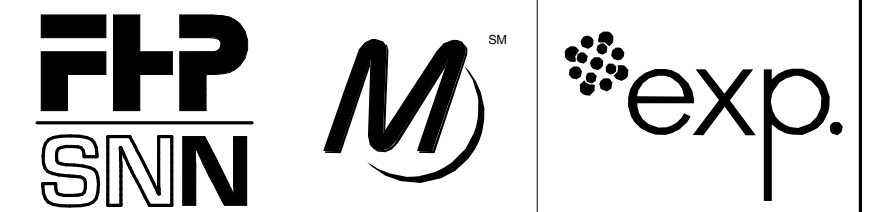
COLUMN LINE	COLUMN NUMBER	COLUMN SIZE	T/ COLUMN ELEVATION	BOTTOM ELEVATION	BOTTOM ELEVATION DESCRIPTION
95	14	W14X211	31.80'	17.64'	T/ BRIDGE GIRDER
95	13	W14X211	31.91'	17.49'	T/ BRIDGE GIRDER
95	12	W14X211	32.03'	17.18'	T/ BRIDGE GIRDER
95	11	W14X211	32.14'	16.87'	T/ BRIDGE GIRDER
95	10	W14X257	32.25'	16.56'	T/ BRIDGE GIRDER
A"	1	W14X257	32.10'	15.95'	T/ GRADE BEAM
A"	2	W14X211	32.31'	16.75'	T/ GRADE BEAM
A"	3	W14X211	32.52'	17.04'	T/ GRADE BEAM
A"	4	W14X211	32.74'	17.35'	T/ GRADE BEAM
A"	5	W14X211	32.95'	17.64'	T/ GRADE BEAM
A"	6	W14X211	33.17'	17.94'	T/ GRADE BEAM
A"	7	W14X211	33.38'	18.18'	T/ GRADE BEAM
A"	8	W14X211	33.60'	18.32'	T/ GRADE BEAM
A"	9	W14X211	33.46'	18.35'	T/ GRADE BEAM
A"	10	W14X211	33.31'	18.32'	T/ GRADE BEAM
A"	11	W14X211	33.16'	18.22'	T/ GRADE BEAM
A"	12	W14X211	33.02'	17.99'	T/ GRADE BEAM
A"	13	W14X211	32.88'	17.79'	T/ GRADE BEAM
A"	14	W14X211	32.74'	17.46'	T/ GRADE BEAM
A"	15	W14X257	32.60'	17.13'	T/ GRADE BEAM
NBB	1	W14X257	32.75'	TBD	T/ BRIDGE GIRDER
NBB	2	W14X211	32.64'	TBD	T/ BRIDGE GIRDER
NBB	3	W14X211	32.53'	TBD	T/ BRIDGE GIRDER
NBB	4	W14X211	32.41'	TBD	T/ BRIDGE GIRDER
NBB	5	W14X211	32.30'	TBD	T/ BRIDGE GIRDER

**SOUTH CANOPY - EAST SIDE**

COLUMN LINE	COLUMN NUMBER	COLUMN SIZE	T/ COLUMN ELEVATION	BOTTOM ELEVATION	BOTTOM ELEVATION DESCRIPTION
SBB	5	W14X211	34.30'	20.21'	T/ BRIDGE GIRDER
SBB	6	W14X211	34.66'	20.28'	T/ BRIDGE GIRDER
SBB	7	W14X211	35.03'	20.35'	T/ BRIDGE GIRDER
SBB	8	W14X211	35.39'	20.42'	T/ BRIDGE GIRDER
SBB	9	W14X257	35.75'	20.49'	T/ BRIDGE GIRDER
H'	1	W14X257	35.60'	20.50'	T/ GRADE BEAM
H'	2	W14X211	35.43'	20.43'	T/ GRADE BEAM
H'	3	W14X211	35.25'	20.25'	T/ GRADE BEAM
H'	4	W14X211	35.06'	19.97'	T/ GRADE BEAM
H'	5	W14X211	34.88'	19.58'	T/ GRADE BEAM
H'	6	W14X211	34.70'	19.08'	T/ GRADE BEAM
H'	7	W14X211	34.12'	18.46'	T/ GRADE BEAM
H'	8	W14X211	33.55'	17.83'	T/ GRADE BEAM
H'	9	W14X211	32.97'	17.24'	T/ GRADE BEAM
H'	10	W14X211	32.39'	16.76'	T/ GRADE BEAM
H'	11	W14X257	31.90'	16.14'	T/ GRADE BEAM
95	9	W14X257	32.05'	16.35'	T/ BRIDGE GIRDER
95	8	W14X211	31.94'	16.64'	T/ BRIDGE GIRDER
95	7	W14X211	31.83'	16.94'	T/ BRIDGE GIRDER
95	6	W14X211	31.71'	17.23'	T/ BRIDGE GIRDER
95	5	W14X211	31.60'	17.39'	T/ BRIDGE GIRDER

**NORTH CANOPY - EAST SIDE**

COLUMN LINE	COLUMN NUMBER	COLUMN SIZE	T/ COLUMN ELEVATION	BOTTOM ELEVATION	BOTTOM ELEVATION DESCRIPTION
95	15	W14X211	31.80'	17.64'	T/ BRIDGE GIRDER
95	16	W14X211	31.91'	17.47'	T/ BRIDGE GIRDER
95	17	W14X211	32.03'	17.15'	T/ BRIDGE GIRDER
95	18	W14X211	32.14'	16.83'	T/ BRIDGE GIRDER
95	19	W14X257	32.25'	16.50'	T/ BRIDGE GIRDER
H"	1	W14X257	32.10'	16.04'	T/ GRADE BEAM
H"	2	W14X211	32.31'	16.75'	T/ GRADE BEAM
H"	3	W14X211	32.52'	17.04'	T/ GRADE BEAM
H"	4	W14X211	32.74'	17.34'	T/ GRADE BEAM
H"	5	W14X211	32.95'	17.64'	T/ GRADE BEAM
H"	6	W14X211	33.17'	17.94'	T/ GRADE BEAM
H"	7	W14X211	33.38'	18.16'	T/ GRADE BEAM
H"	8	W14X211	33.60'	18.30'	T/ GRADE BEAM
H"	9	W14X211	33.46'	18.34'	T/ GRADE BEAM
H"	10	W14X211	33.31'	18.32'	T/ GRADE BEAM
H"	11	W14X211	33.16'	18.21'	T/ GRADE BEAM
H"	12	W14X211	33.02'	18.01'	T/ GRADE BEAM
H"	13	W14X211	32.88'	17.73'	T/ GRADE BEAM
H"	14	W14X211	32.74'	17.43'	T/ GRADE BEAM
H"	15	W14X257	32.60'	17.24'	T/ GRADE BEAM
NBB	10	W14X257	32.75'	TBD	T/ BRIDGE GIRDER
NBB	9	W14X211	32.64'	TBD	T/ BRIDGE GIRDER
NBB	8	W14X211	32.53'	TBD	T/ BRIDGE GIRDER
NBB	6	W14X211	32.41'	TBD	T/ BRIDGE GIRDER
NBB	7	W14X211	32.30'	TBD	T/ BRIDGE GIRDER



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**95th Terminal Improvement**  
 15 West 95th Street (South Terminal)  
 Chicago, IL 60628

<b>IN CHARGE</b>	J. JAKALSKI
<b>APPROVED BY</b>	J. JAKALSKI
<b>CHECKED BY</b>	R.S. NAIR
<b>DESIGNED BY</b>	S. CITKO
<b>DRAWN BY</b>	C. BOVE
<b>PROJECT NO.</b>	2012-0021.02
<b>FILE NAME</b>	D095_S-RoofCENTRAL

	2/9/17	ISSUED FOR CONSTRUCTION
<b>MARK</b>	<b>DATE</b>	<b>DESCRIPTION</b>

LOCATION IDENTIFIER: **DR095**

**BUS CANOPY COLUMN SCHEDULE**

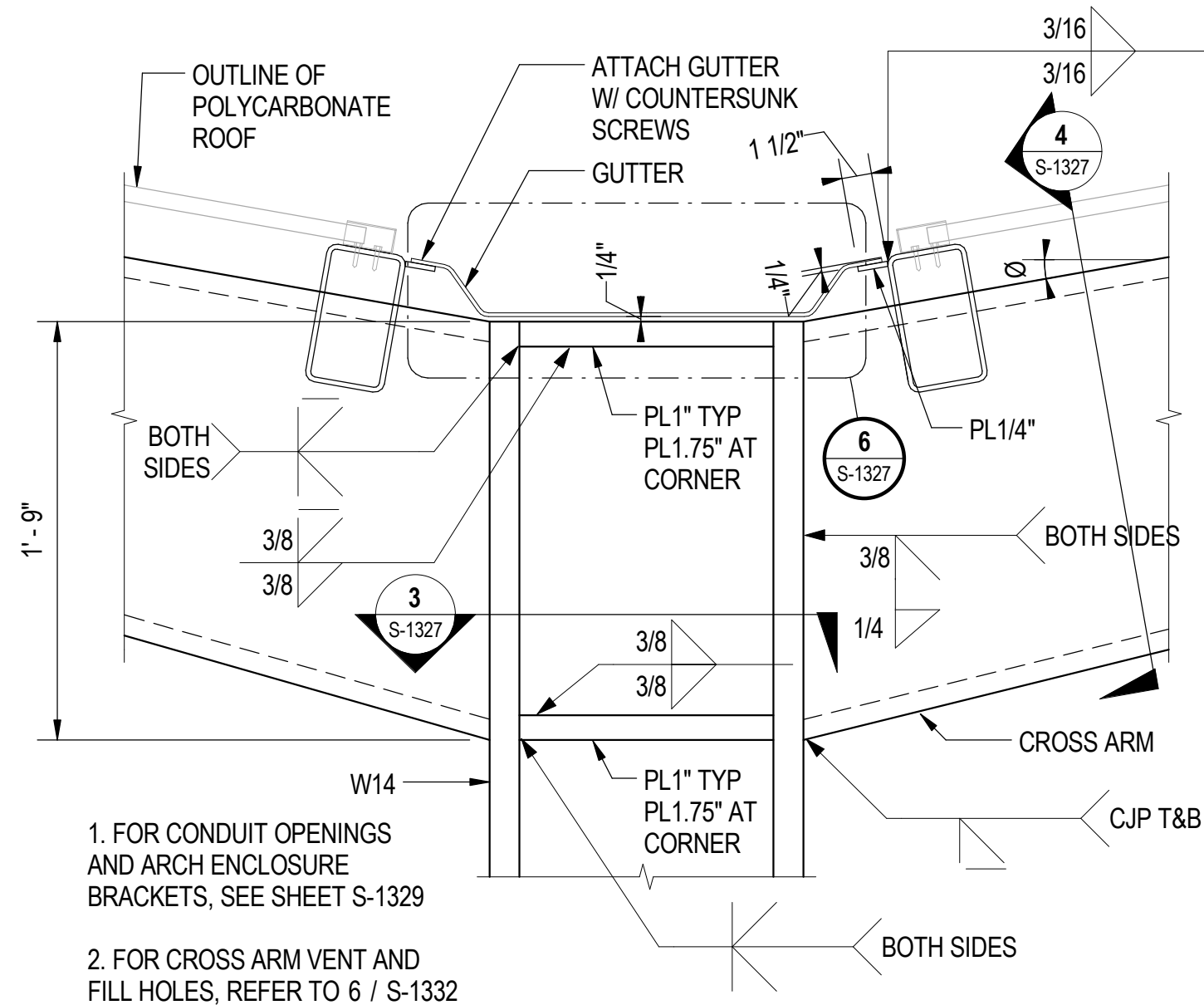
**S-1326**

**SHEET NOTES**

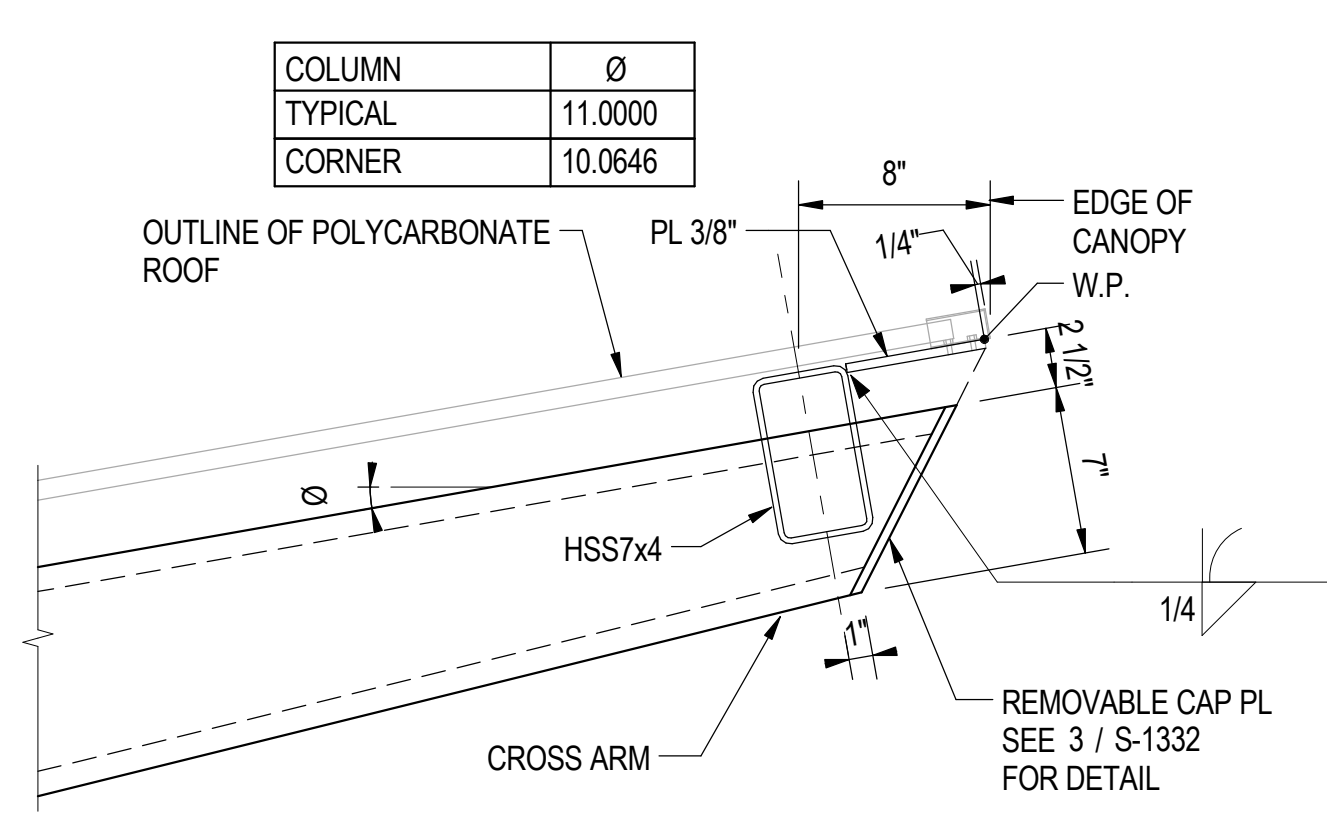
- BOTTOM ELEVATIONS ARE APPROXIMATE. VERIFY IN FIELD ACTUAL GRADE BEAM AND BRIDGE GIRDER ELEVATIONS
- ERECTING COLUMN TO CORRECT T/COL. ELEVATION IS IMPORTANT FOR PROPER DRAINAGE OF CANOPY SYSTEM

30/01/2017 2:03:49 PM

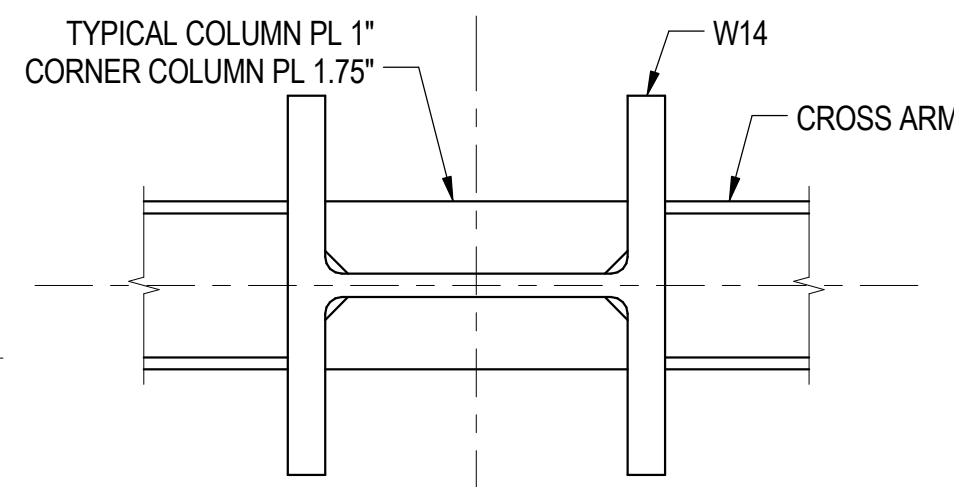
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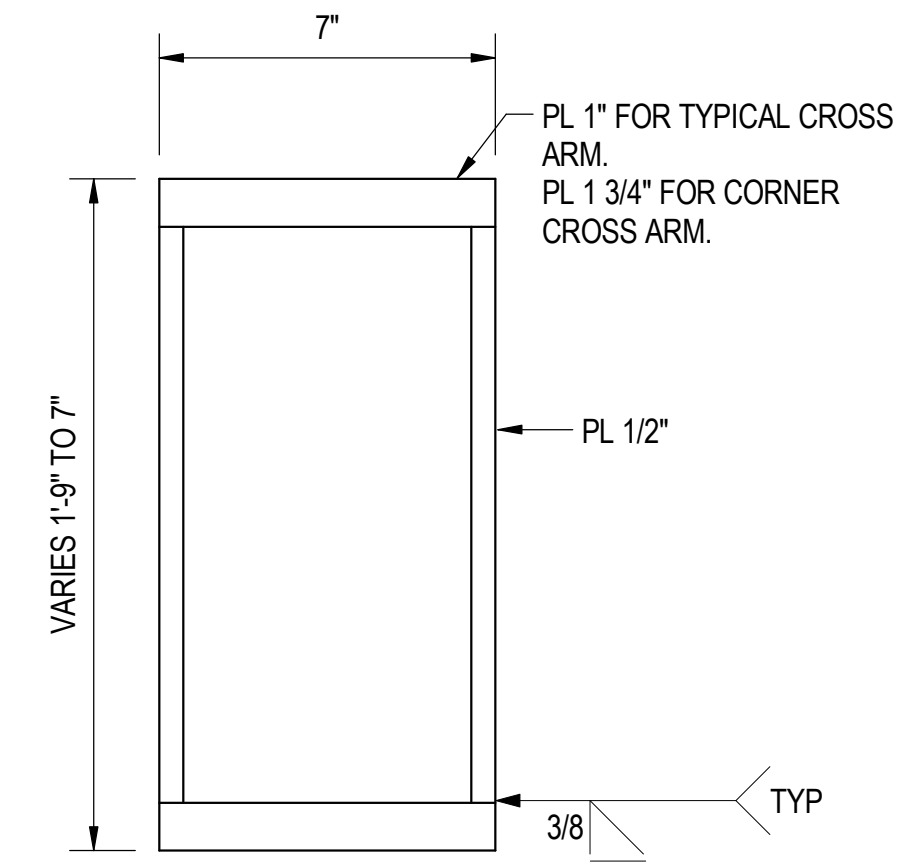
**1** ENLARGED SECTION AT COLUMN  
S-1327 SCALE: 1 1/2" = 1'-0"



**2** ENLARGED SECTION AT EDGE  
S-1327 SCALE: 1 1/2" = 1'-0"

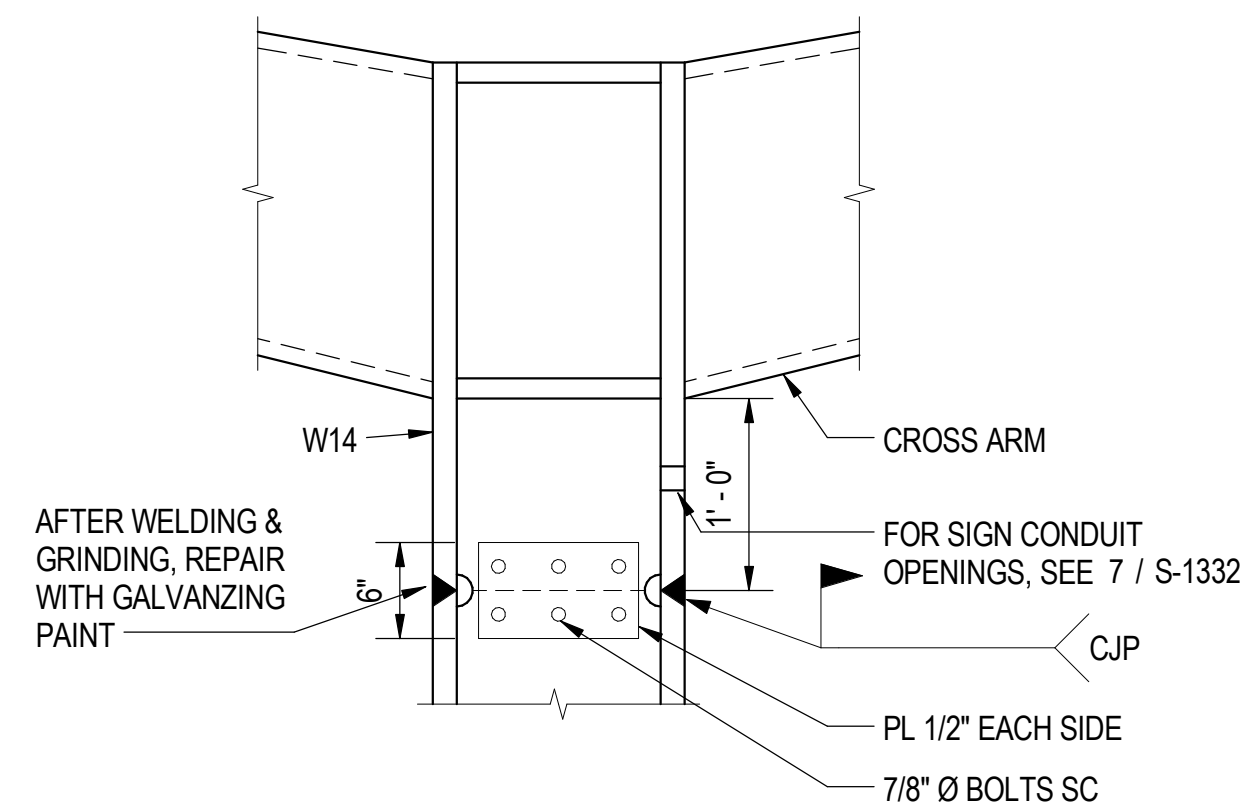


**3** SECTION  
S-1327 SCALE: 1 1/2" = 1'-0"

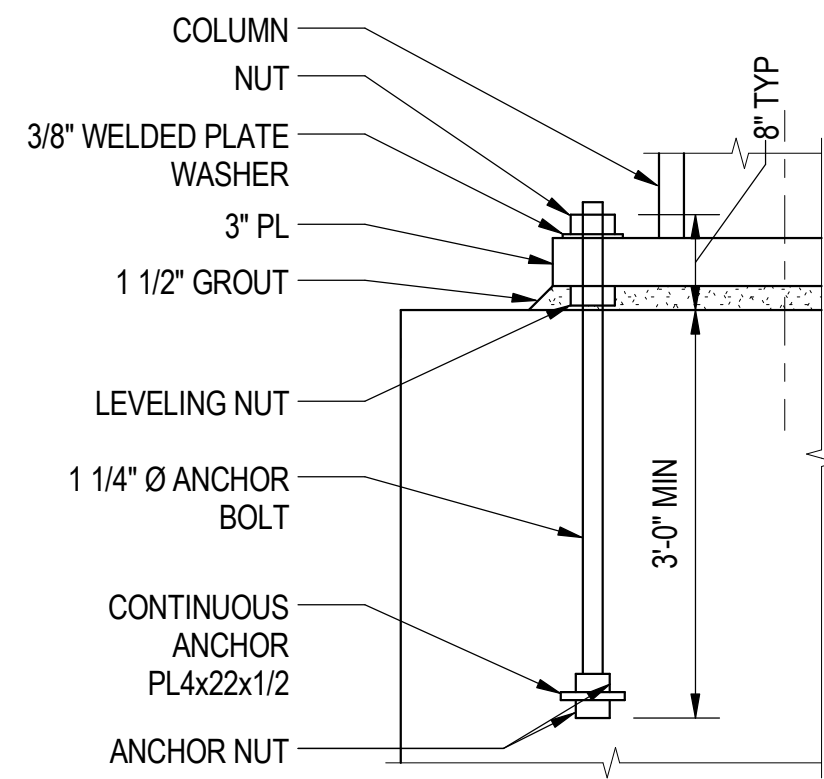


**4** SECTION THROUGH CROSS ARM  
S-1327 SCALE: 3" = 1'-0"

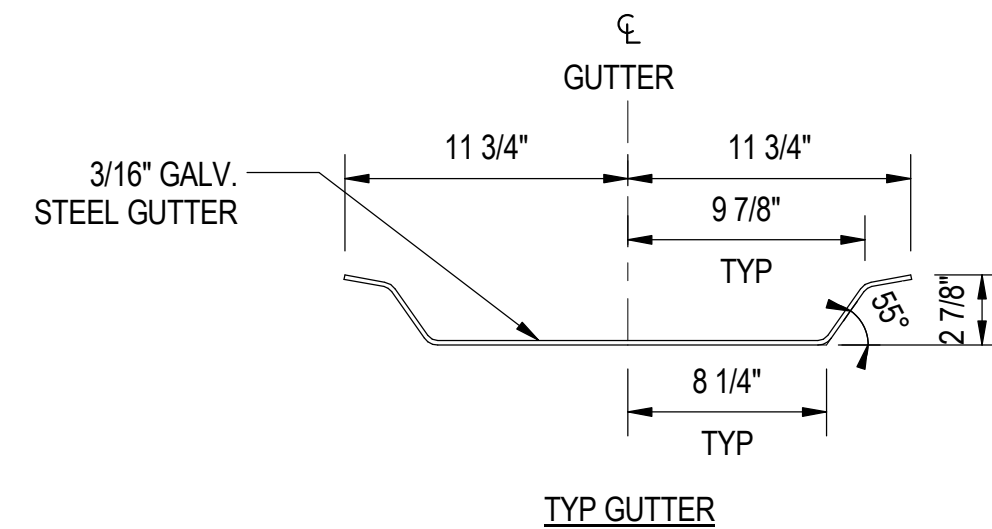
1. FOR CONDUIT OPENINGS AND ARCH ENCLOSURE BRACKETS, SEE SHEET S-1329
2. FOR CROSS ARM VENT AND FILL HOLES, REFER TO 6 / S-1332



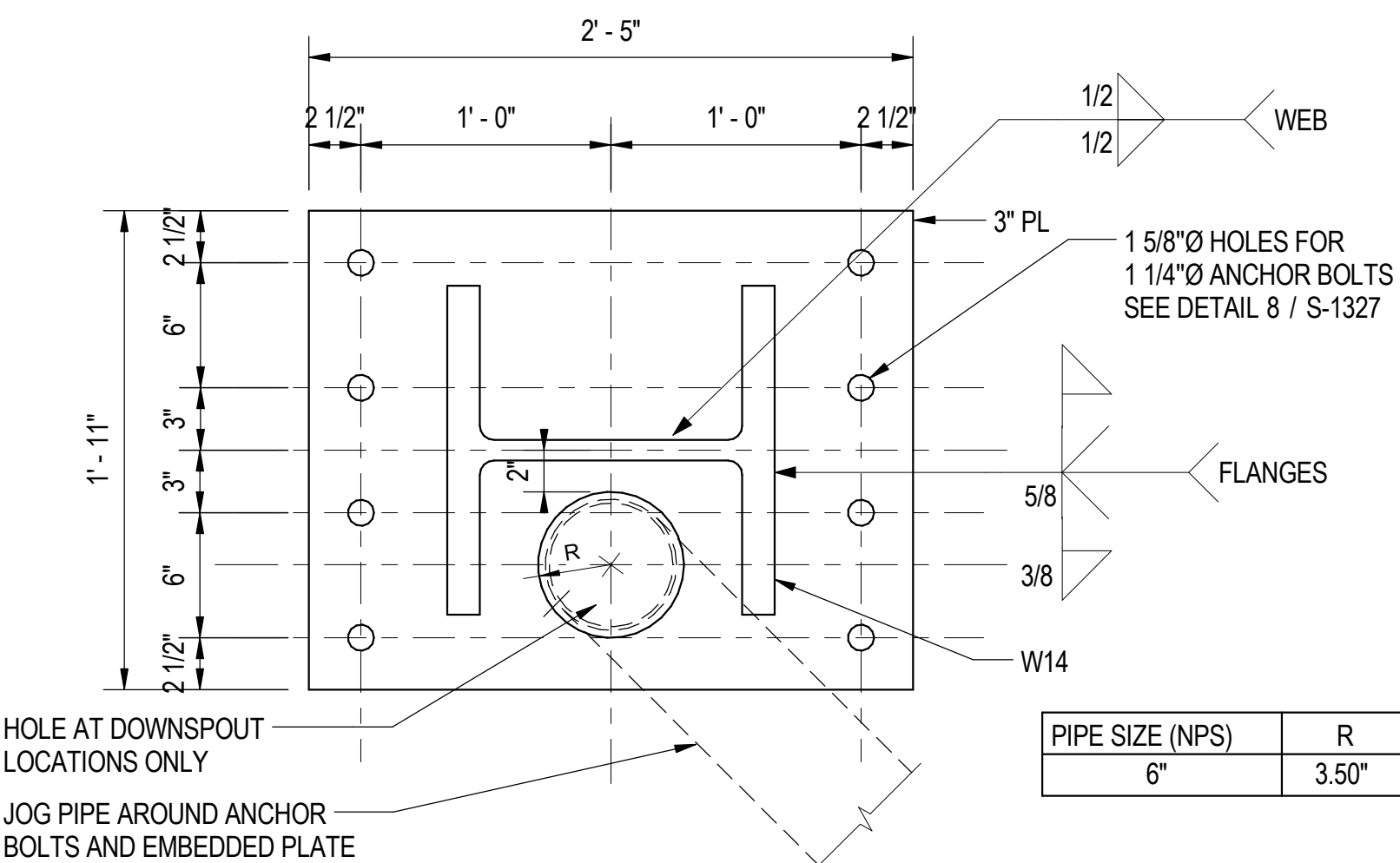
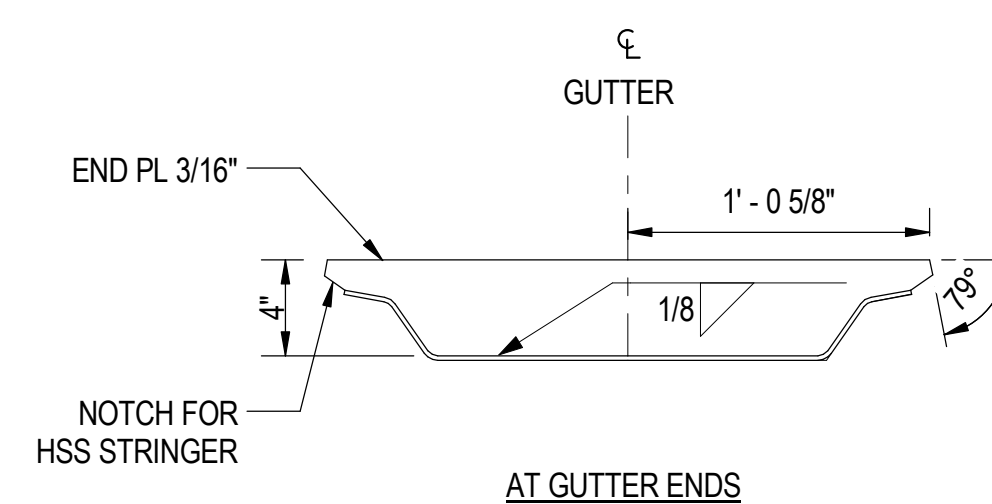
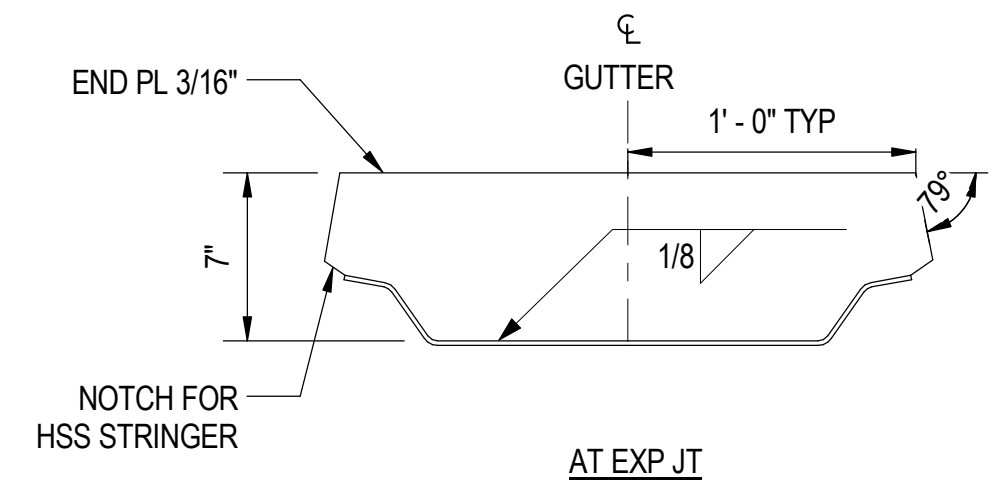
**5** SPLICE DETAIL  
S-1327 SCALE: 1" = 1'-0"



**8** ANCHOR BOLT DETAIL  
S-1327 SCALE: 1" = 1'-0"



**6** GUTTER PROFILE  
S-1327 SCALE: 1 1/2" = 1'-0"



**7** BASEPLATE DETAIL  
S-1327 SCALE: 1 1/2" = 1'-0"

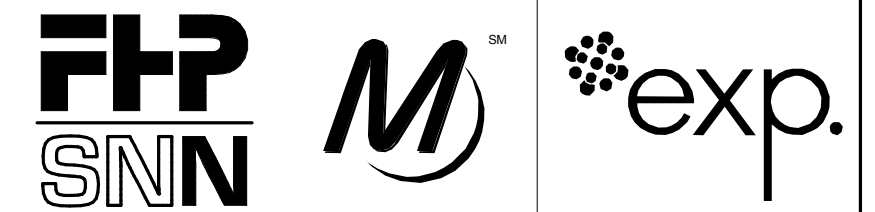
DOWNSPOUT PENETRATIONS IN BASEPLATES ON GRADE	
COLUMN	SIDE OF COLUMN
A'-1, H'-1	N
A'-3, H'-3	N
A'-5, H'-5	S
A'-6, H'-6	S
A'-7, H'-7	N
A'-9, H'-9	N
A'-11, H'-11	S
A'-1, H'-1	N
A'-2, H'-2	N
A'-4, H'-4	N
A'-6, H'-6	N
A'-10, H'-10	N
A'-12, H'-12	S
A'-14, H'-14	S
A'-15, H'-15	S

PIPE SIZE (NPS)	R
6"	3.50"

HOLE AT DOWNSPOUT LOCATIONS ONLY  
JOG PIPE AROUND ANCHOR BOLTS AND EMBEDDED PLATE

**SHEET NOTES**

1. FOR GENERAL NOTES, SEE DWGS S-001 TO S-004
2. ALL CANOPY STEEL IS TO BE HOT-DIP GALVANIZED.
3. RECOMMENDED ERECTION SEQUENCE
  - a. COMPLETE WELDED COLUMN SPLICE BEFORE ERECTING STRINGERS
  - b. ERECT STRINGERS PRIOR TO GROUTING BASE PLATE AND WELDING WELD PLATES
  - c. GROUT BASE PLATE AND WELD WELD PLATE BEFORE POLYCARBONATE ROOF INSTALLATION



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**95th Terminal Improvement**  
15 West 95th Street (South Terminal)  
Chicago, IL 60628

IN CHARGE	J. JAKALSKI
APPROVED BY	J. JAKALSKI
CHECKED BY	R.S. NAIR
DESIGNED BY	S. CITKO
DRAWN BY	C. BOVE
PROJECT NO.	2012-0021.02
FILE NAME	D095_S-RoofCENTRAL

MARK	DATE	DESCRIPTION
	2/9/17	ISSUED FOR CONSTRUCTION

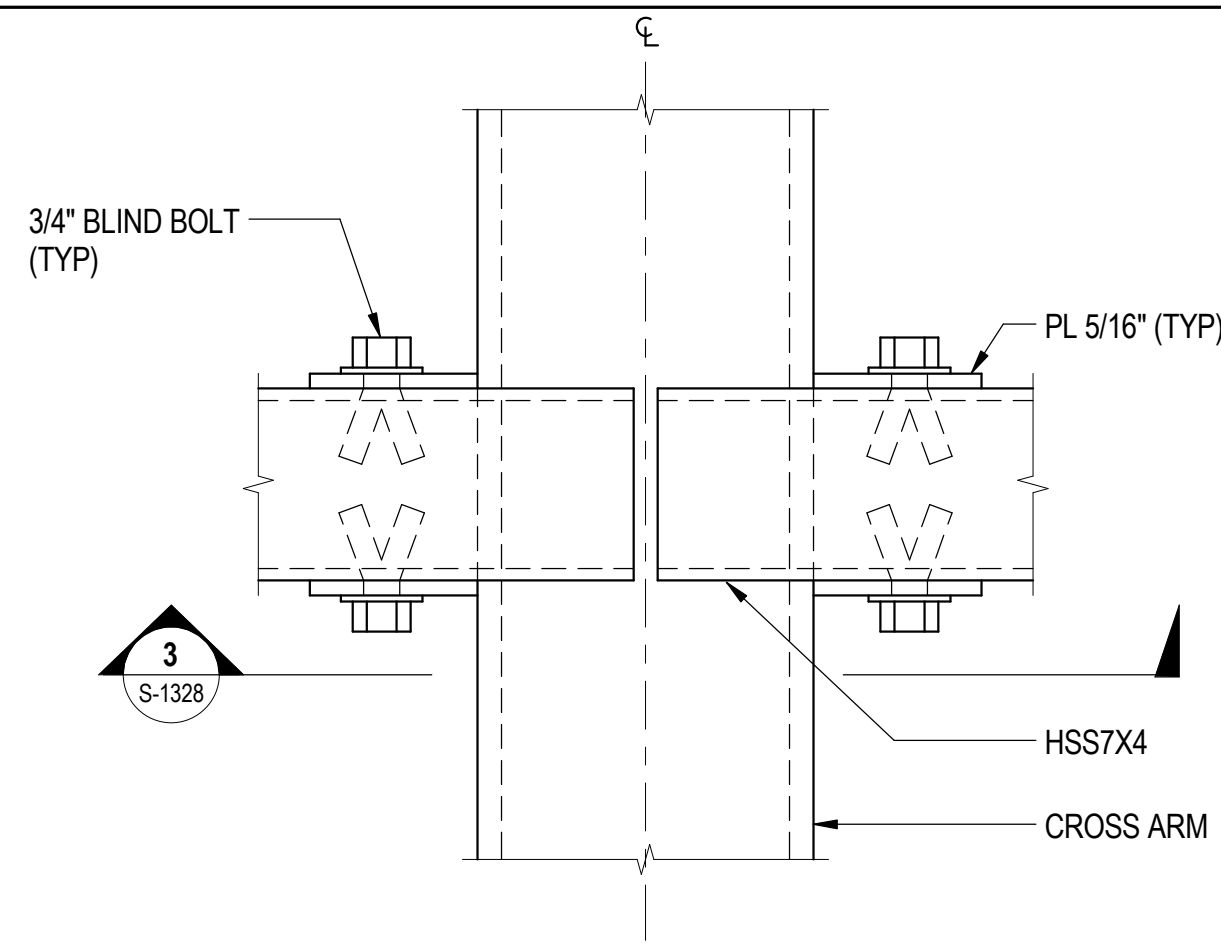
LOCATION IDENTIFIER: **DR095**

**BUS CANOPY DETAILS - 1**

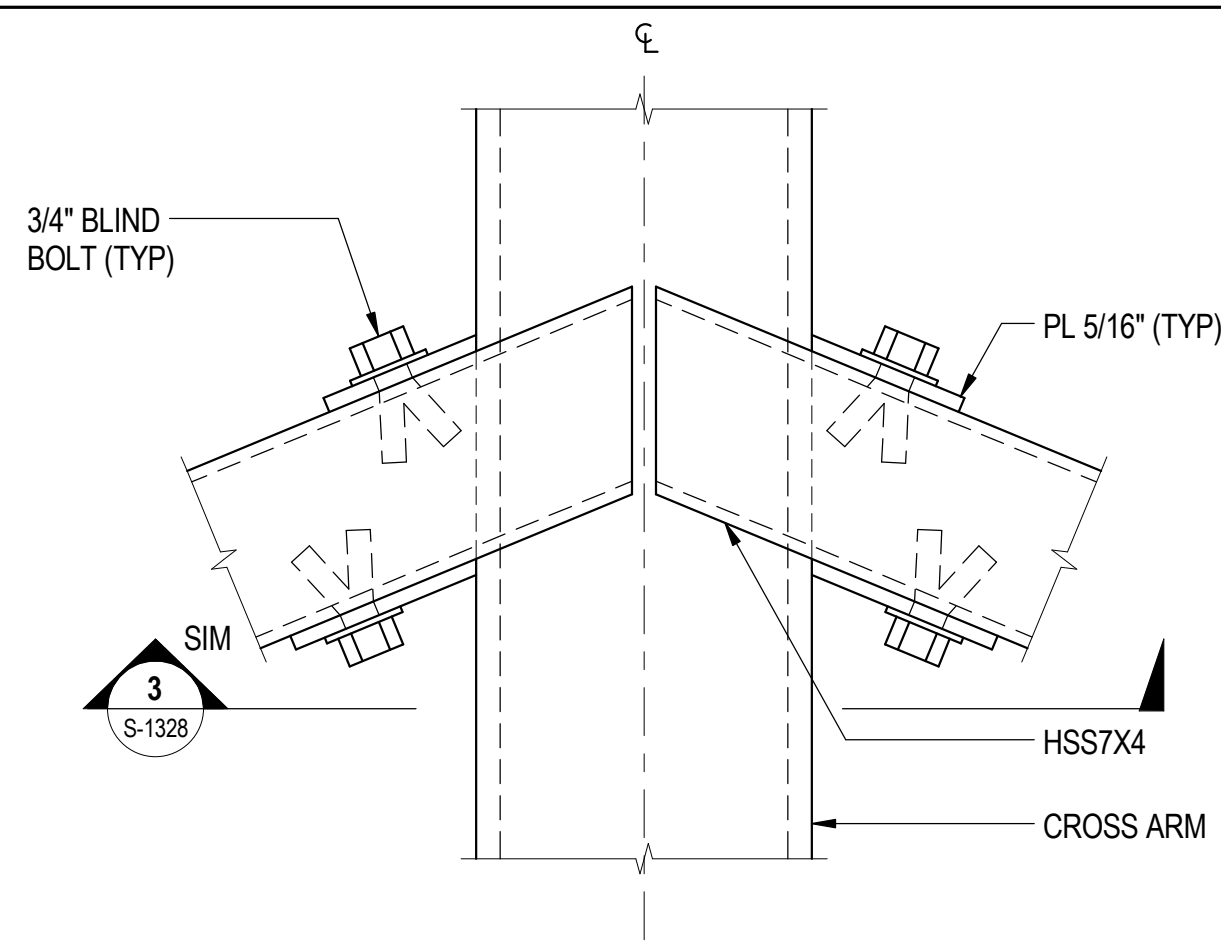
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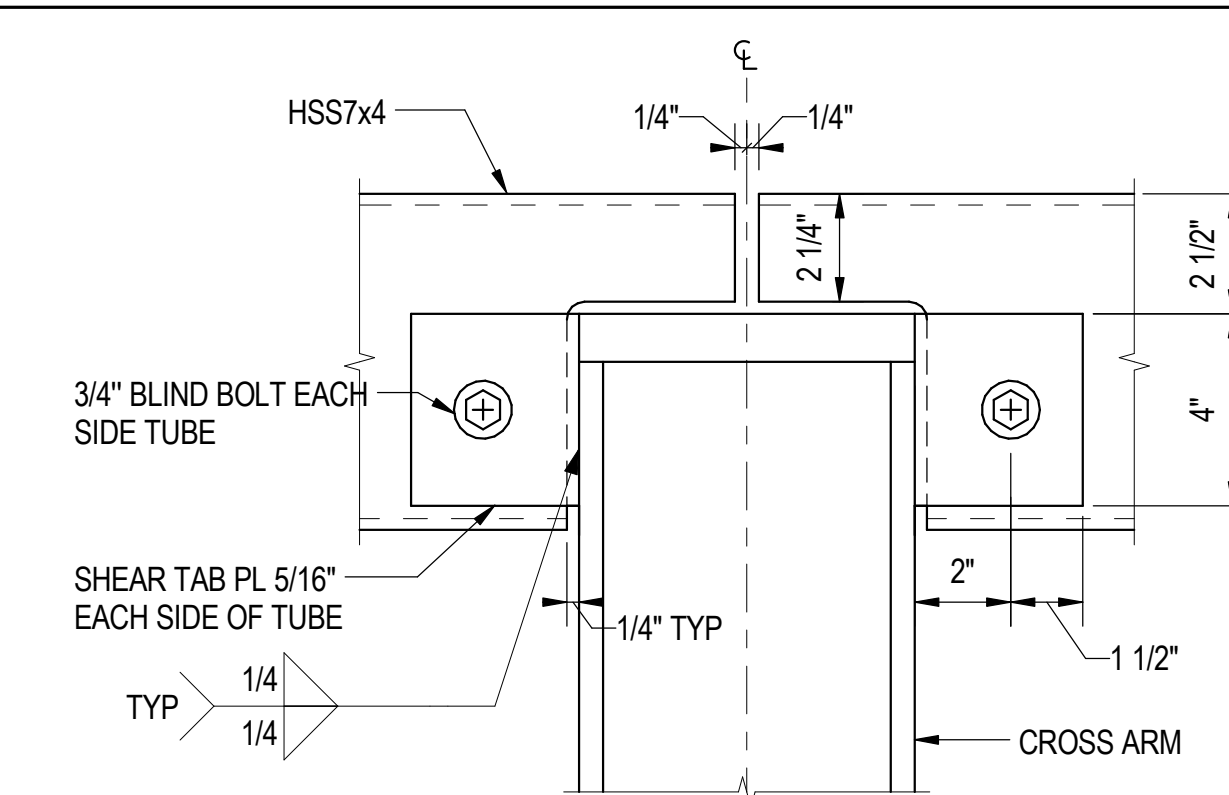
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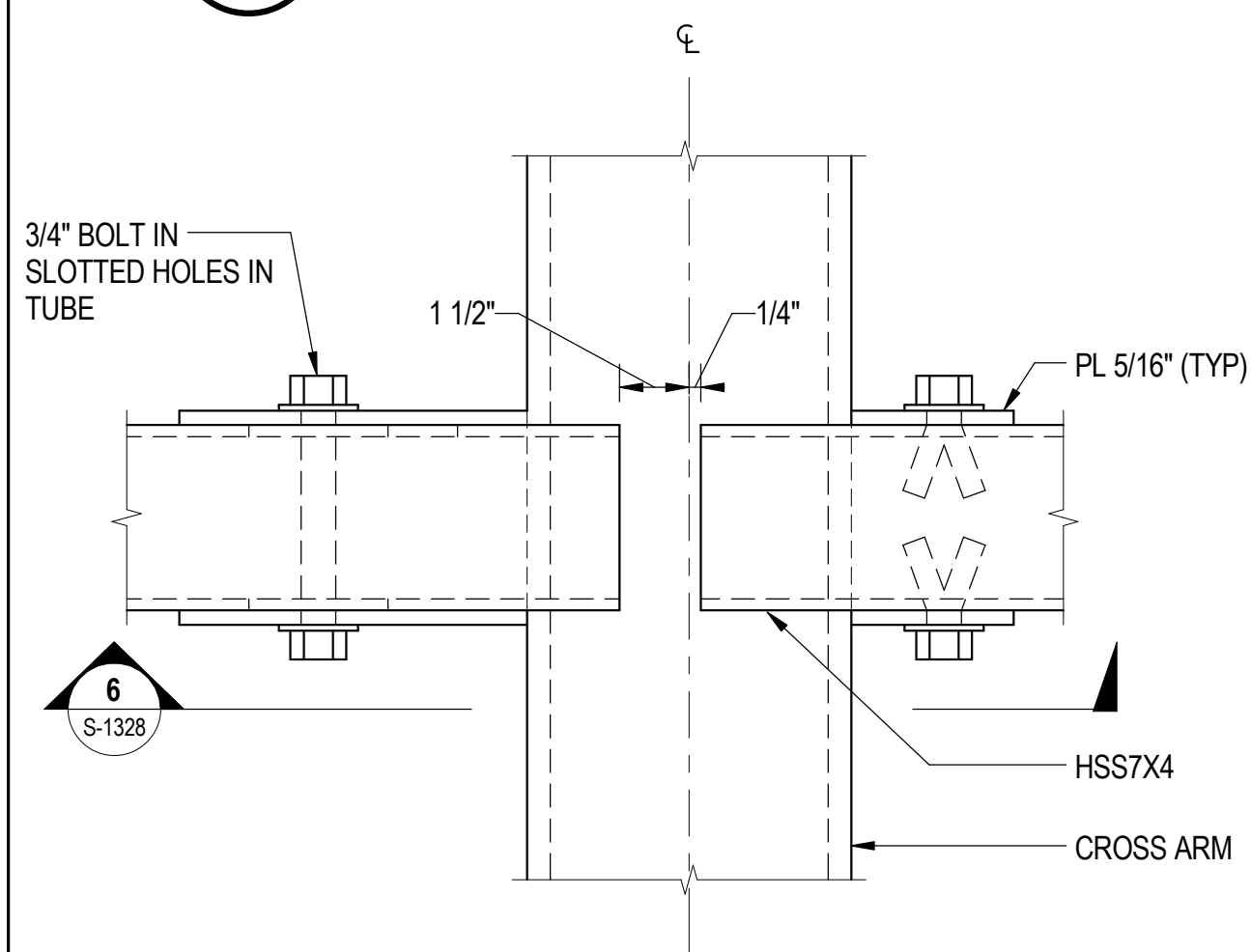
**1** TYPICAL STRINGER CONNECTION PLAN  
S-1328 SCALE: 3" = 1'-0"



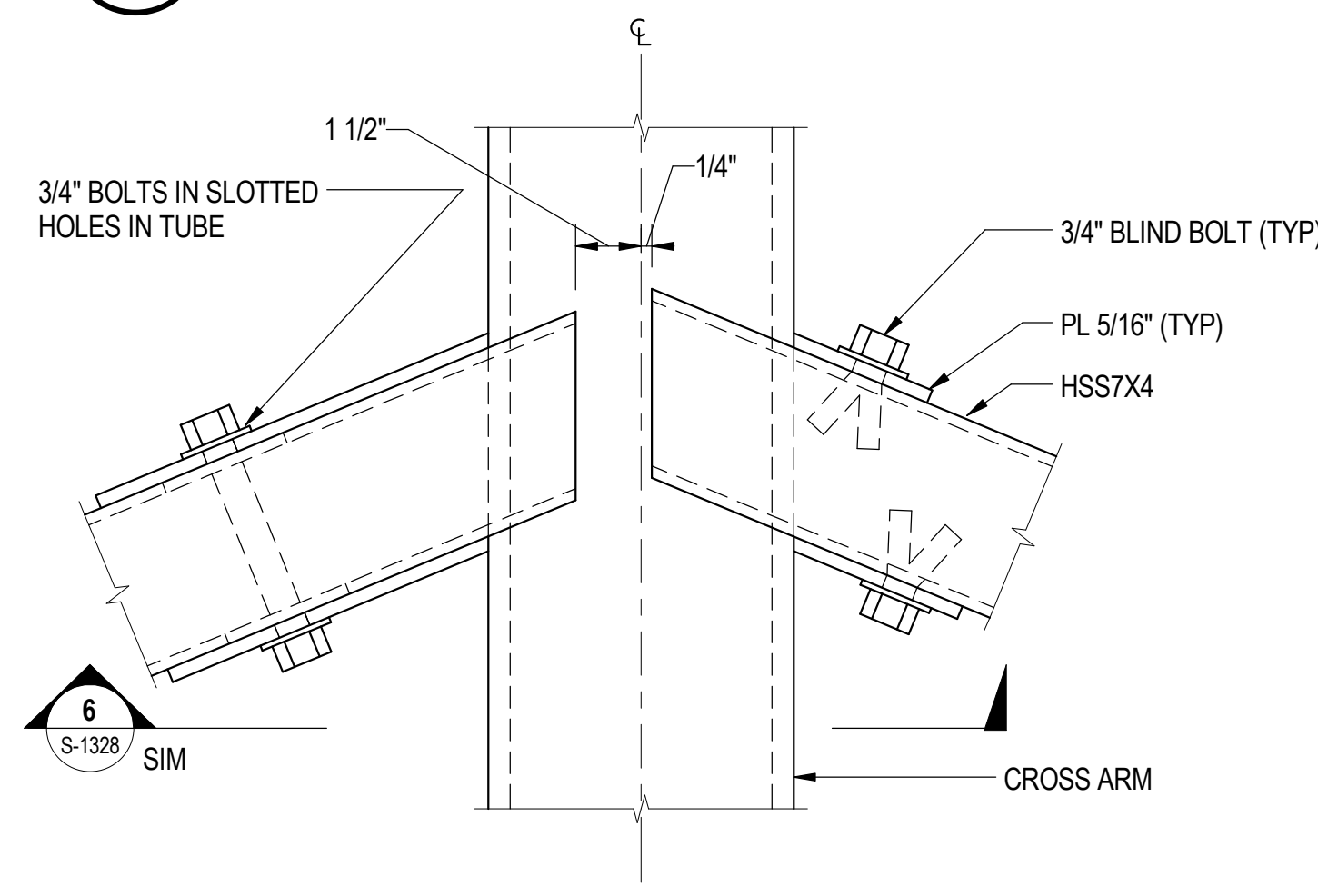
**2** STRINGER CONNECTION PLAN AT CORNERS  
S-1328 SCALE: 3" = 1'-0"



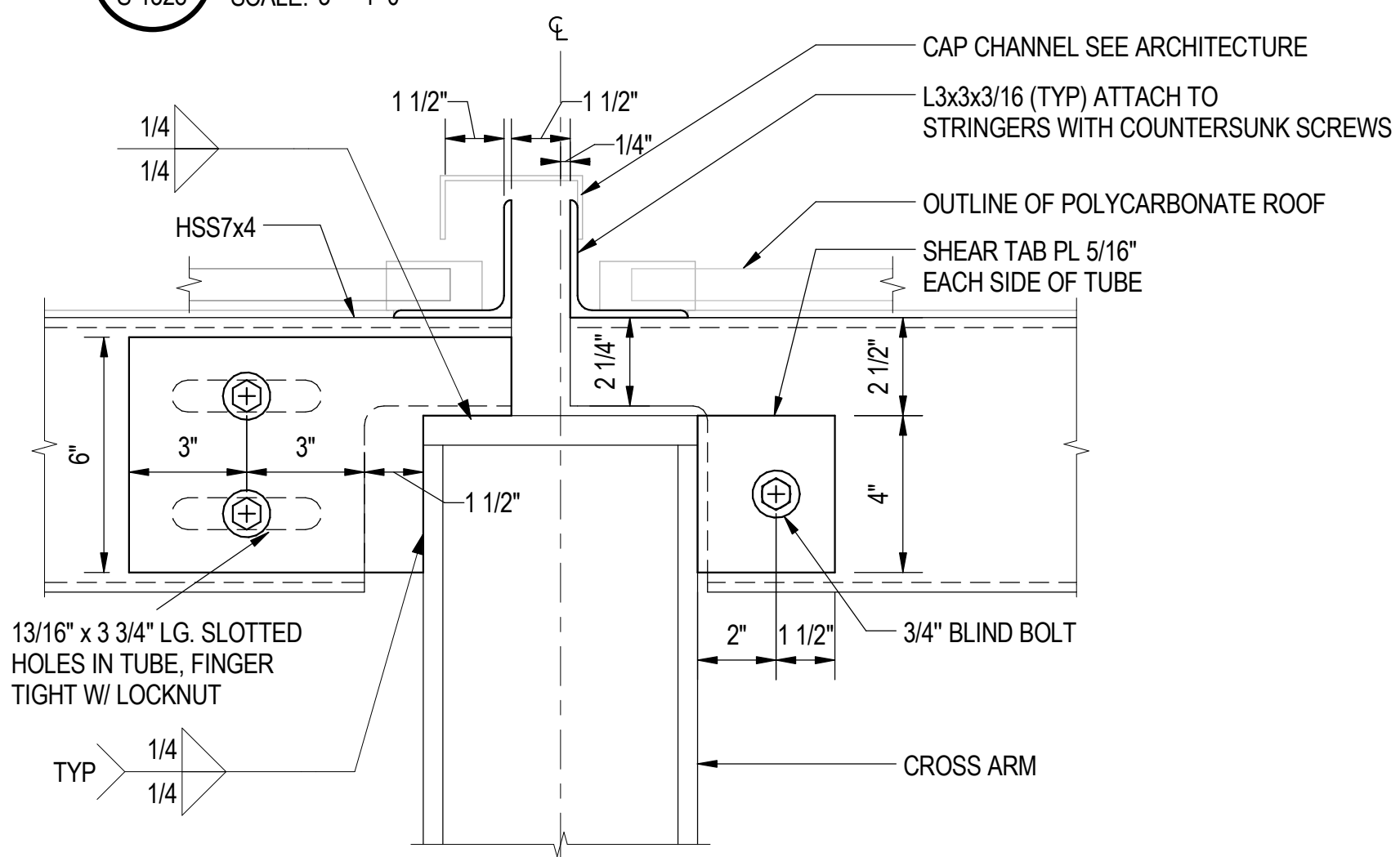
**3** STRINGER CONNECTION SECTION  
S-1328 SCALE: 3" = 1'-0"



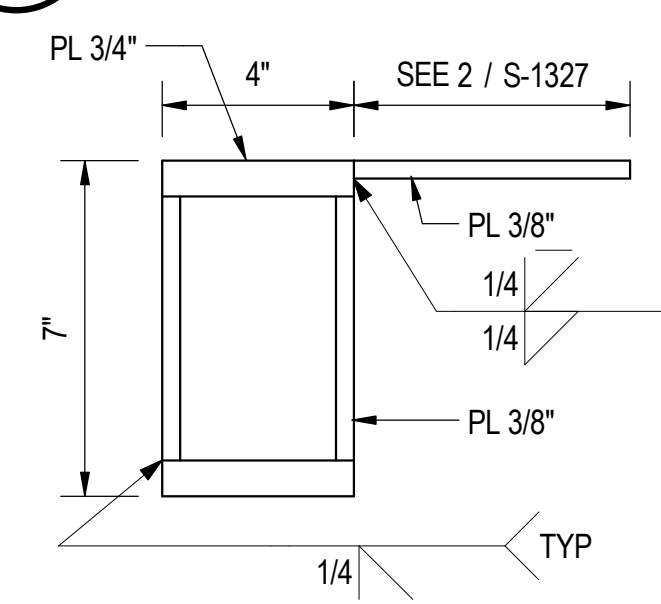
**4** EXPANSION STRINGER CONNECTION PLAN  
S-1328 SCALE: 3" = 1'-0"



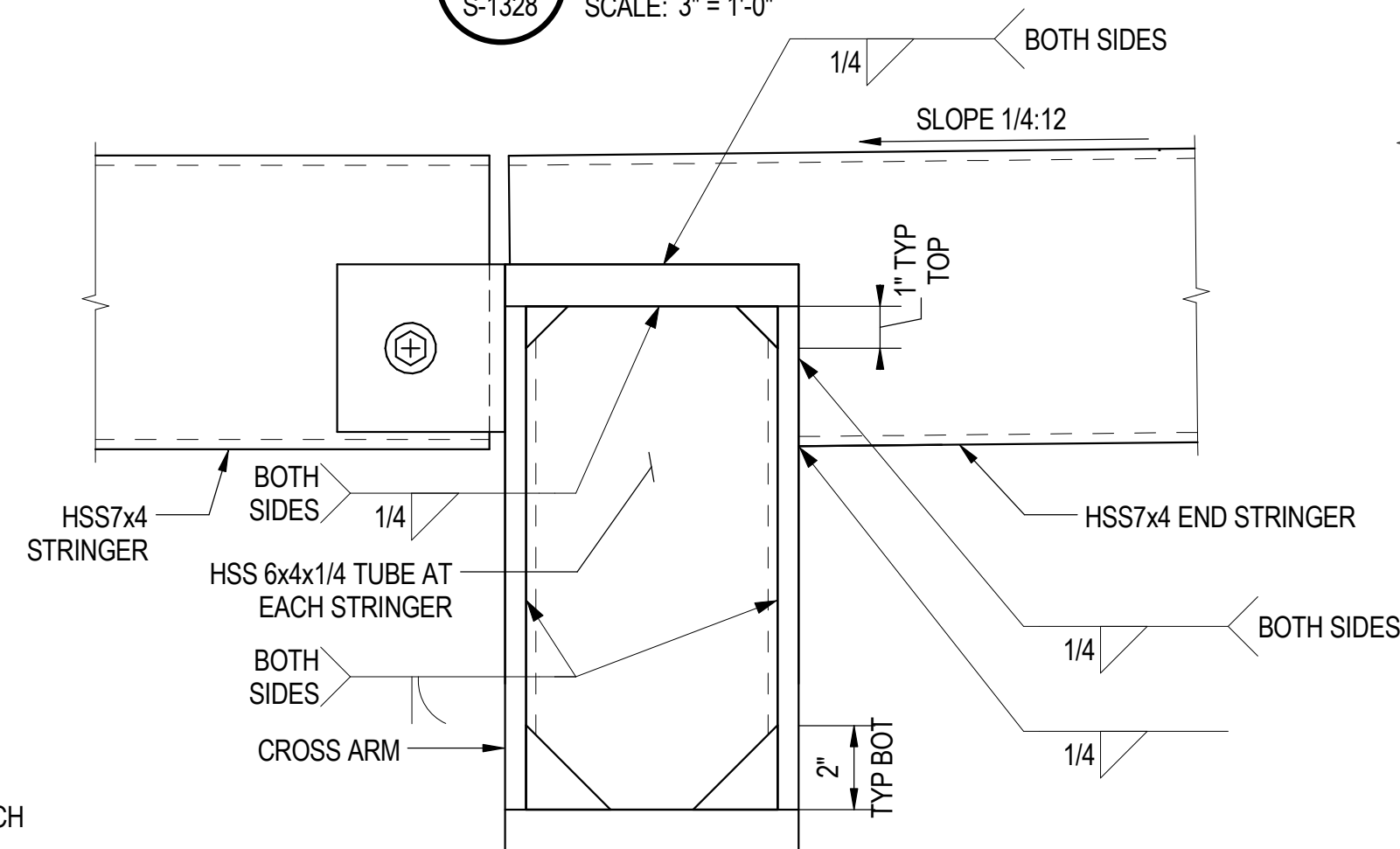
**5** EXPANSION STRINGER CONNECTION PLAN AT CORNERS  
S-1328 SCALE: 3" = 1'-0"



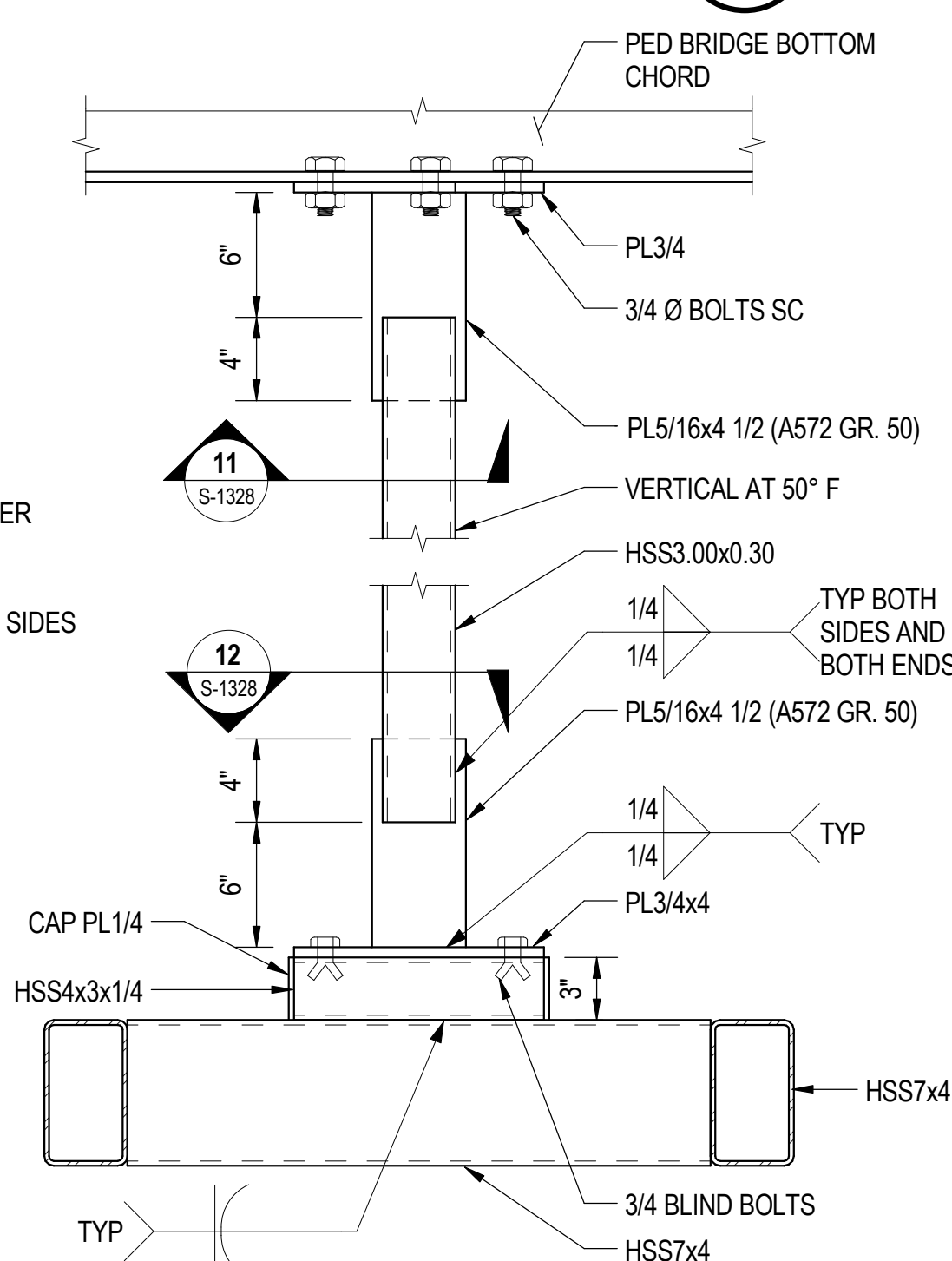
**6** EXPANSION STRINGER CONNECTION SECTION  
S-1328 SCALE: 3" = 1'-0"



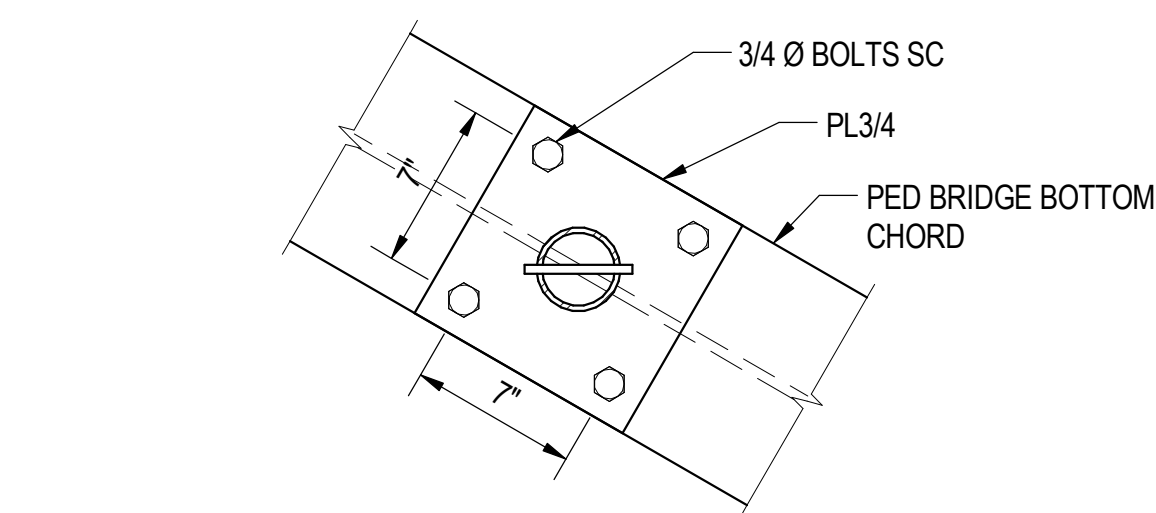
**7** BUILT UP HSS 7X4  
S-1328 SCALE: 3" = 1'-0"



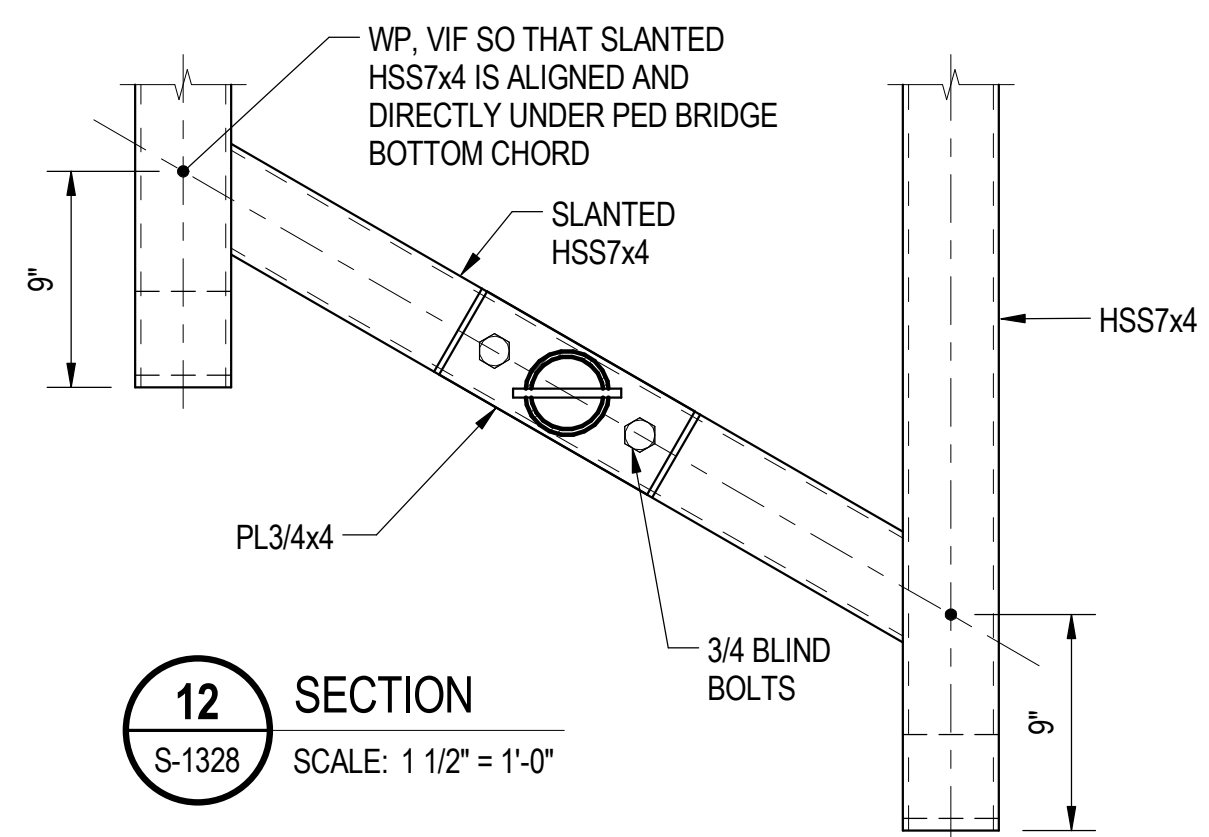
**9** STRINGER MOMENT CONNECTION DETAIL  
S-1328 SCALE: 3" = 1'-0"



**10** SUSPENDED STRINGER DETAIL  
S-1328 SCALE: 1 1/2" = 1'-0"



**11** REFLECTED SECTION  
S-1328 SCALE: 1 1/2" = 1'-0"



**12** SECTION  
S-1328 SCALE: 1 1/2" = 1'-0"



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**95th Terminal Improvement**  
15 West 95th Street (South Terminal)  
Chicago, IL 60628

IN CHARGE	J. JAKALSKI
APPROVED BY	J. JAKALSKI
CHECKED BY	R.S. NAIR
DESIGNED BY	S. CITKO
DRAWN BY	C. BOVE
PROJECT NO.	2012-0021.02
FILE NAME	D095_S-RoofCENTRAL

2/9/17	ISSUED FOR CONSTRUCTION	
MARK	DATE	DESCRIPTION

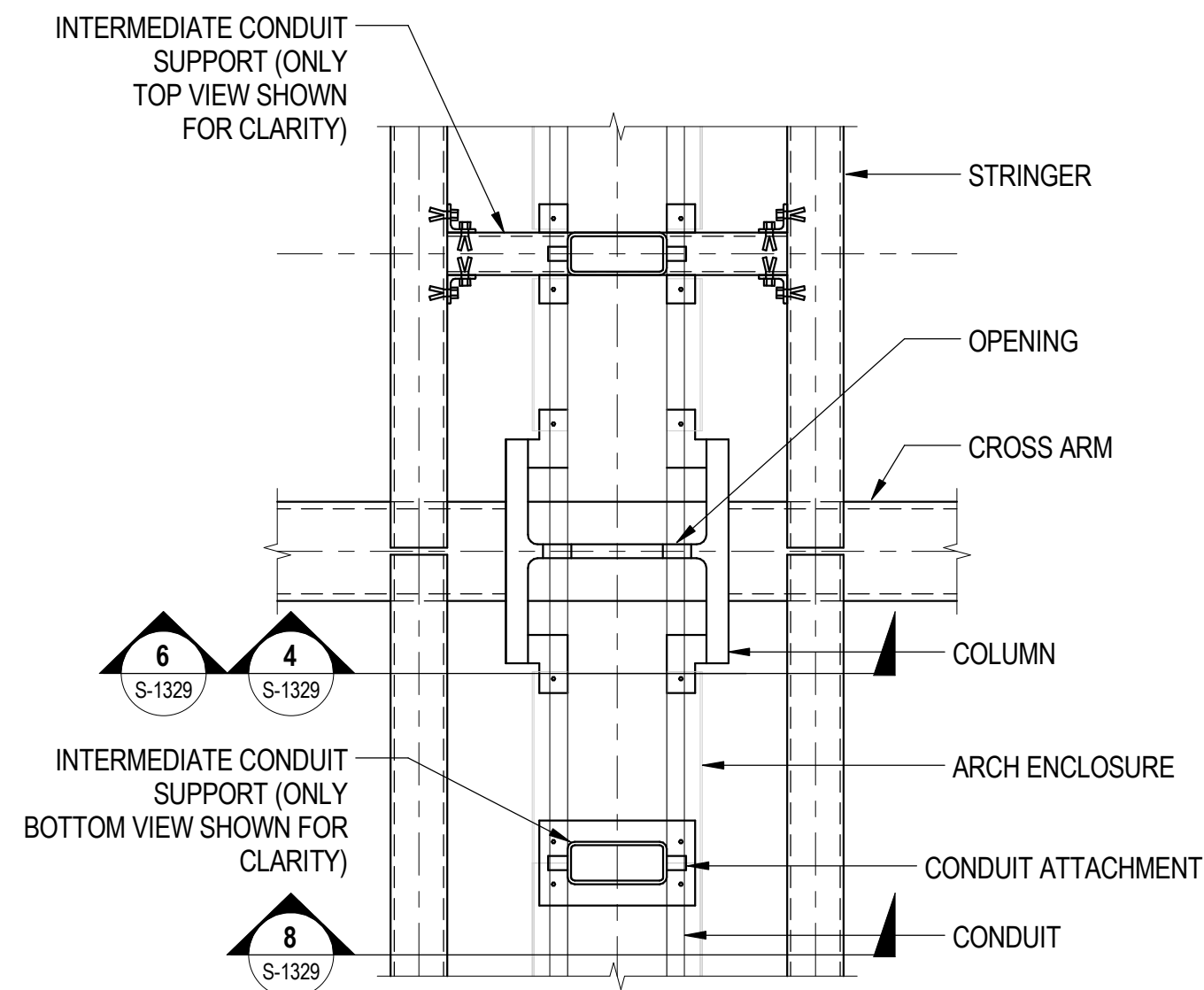
LOCATION IDENTIFIER: **DR095**

**BUS CANOPY DETAILS - 2**

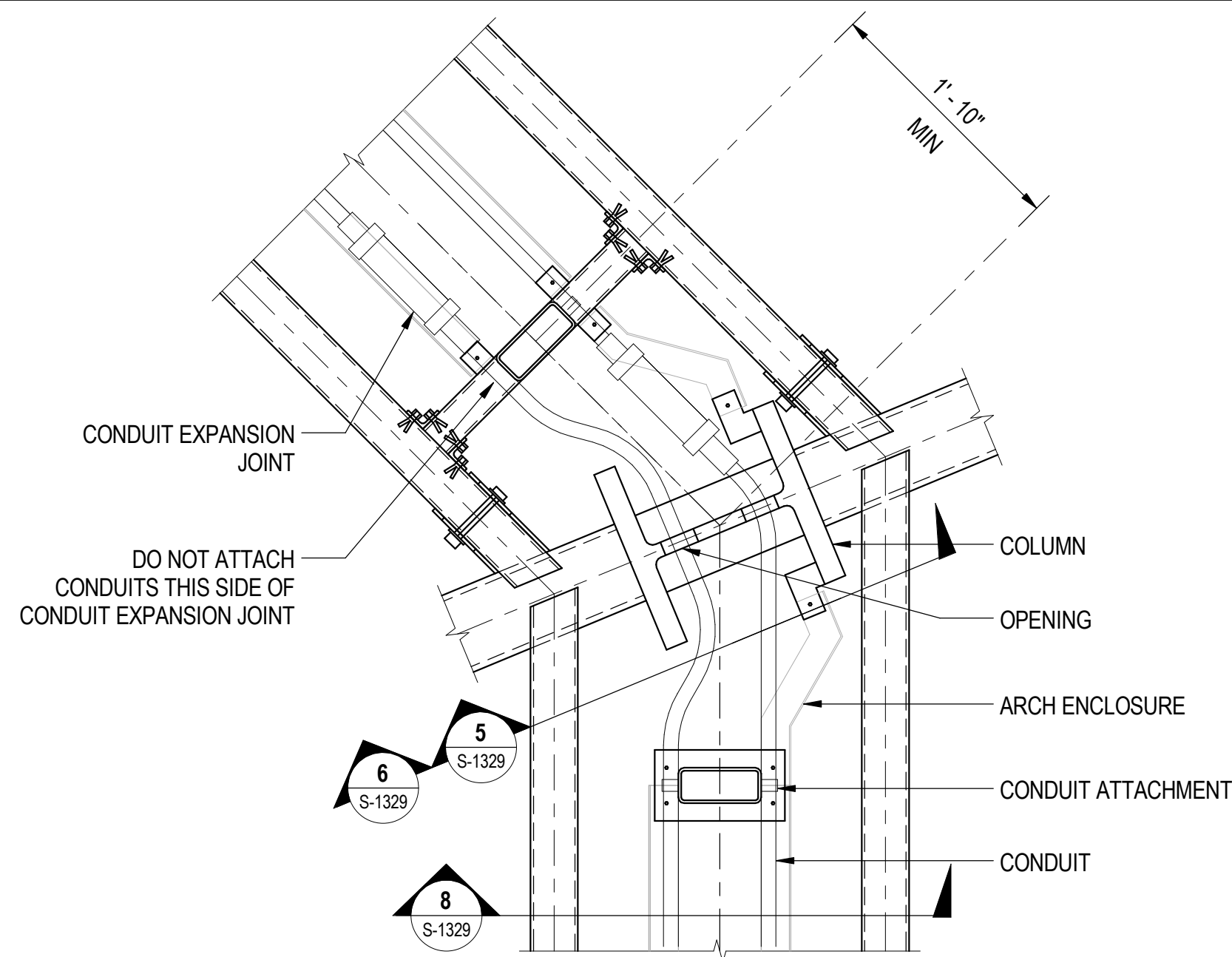
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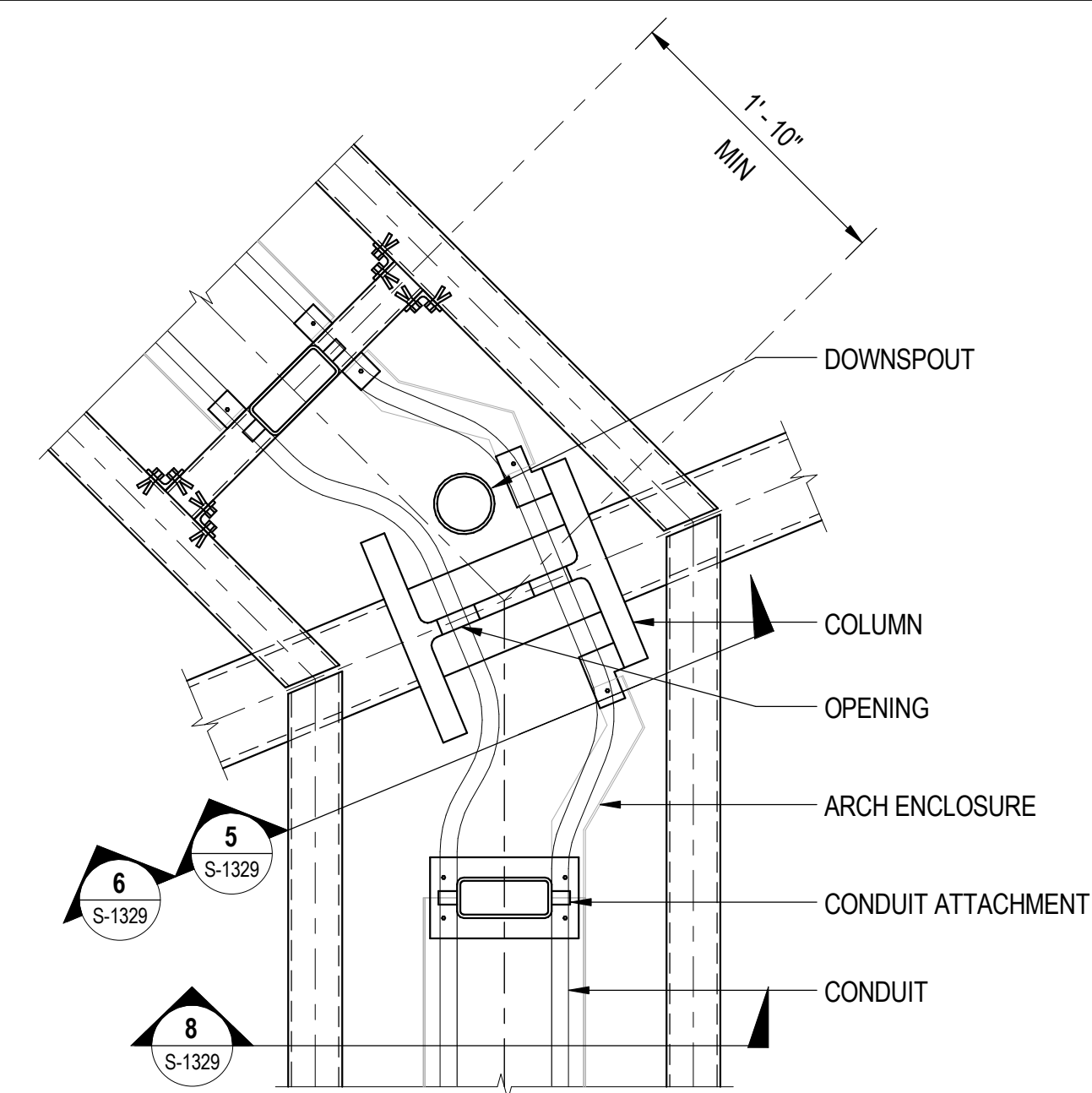
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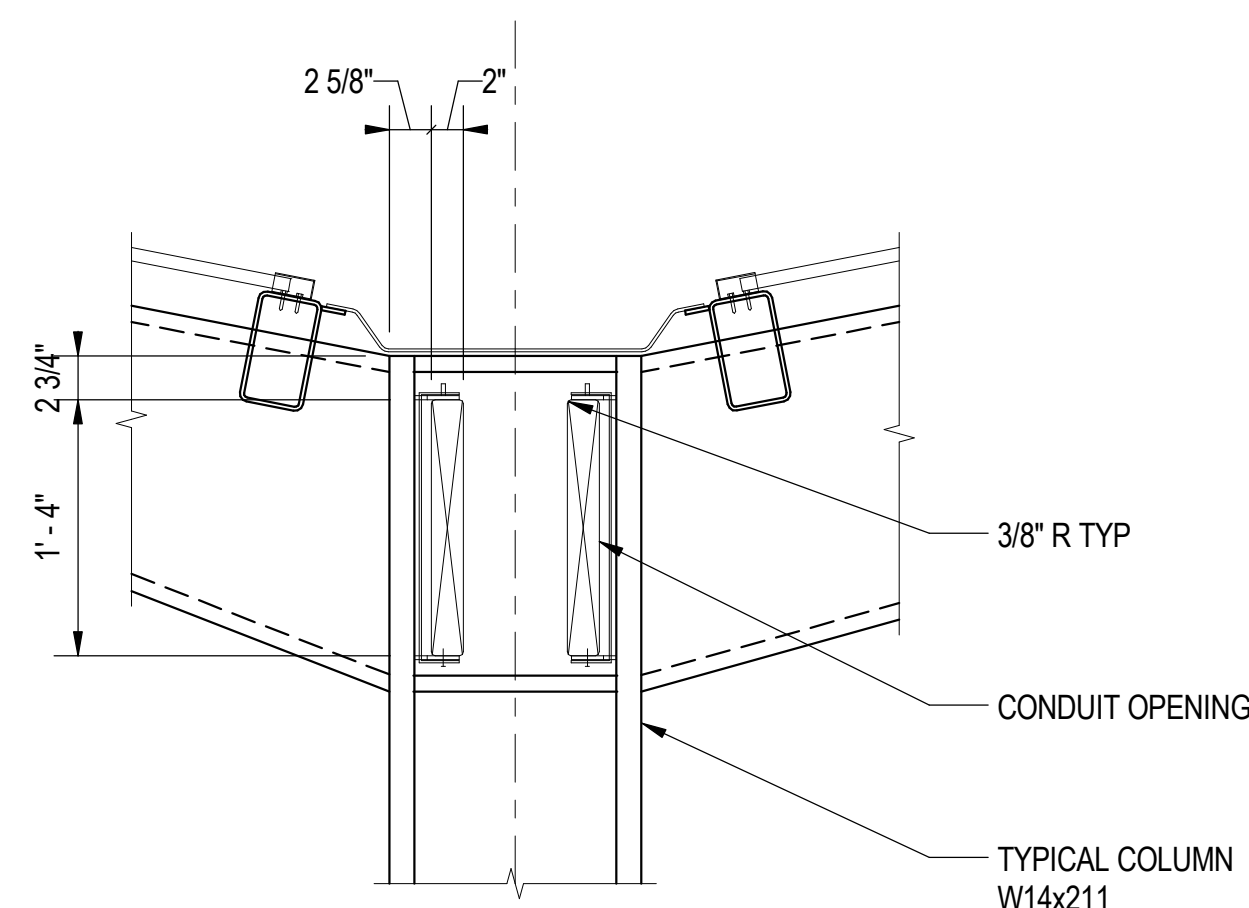
**1** CONDUIT PLAN AT TYPICAL COLUMN  
S-1329 SCALE: 1" = 1'-0"



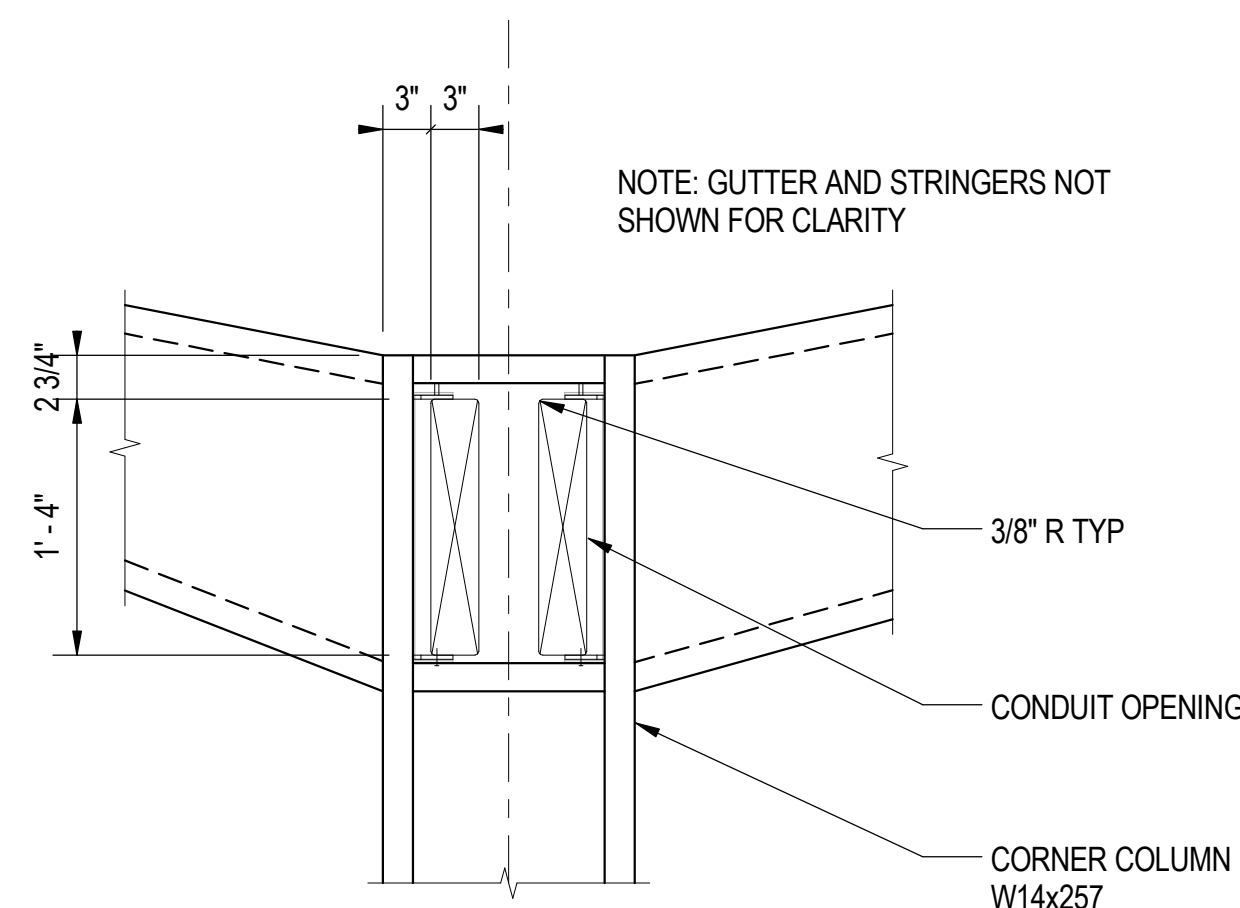
**2** CONDUIT PLAN AT CORNER COLUMN WITH EXP JT  
S-1329 SCALE: 1" = 1'-0"



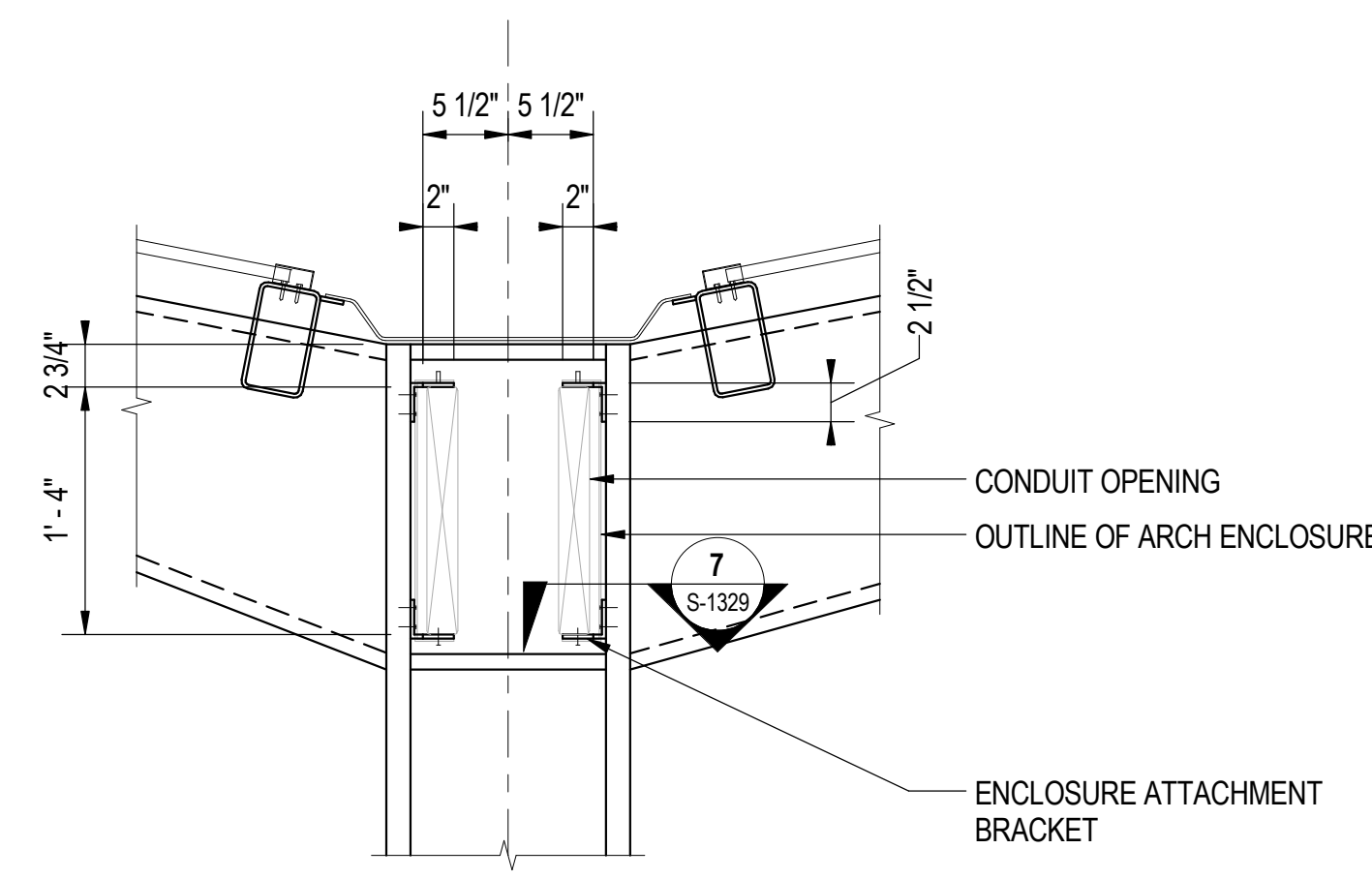
**3** CONDUIT PLAN AT CORNER COLUMN WITH DOWNSPOUT  
S-1329 SCALE: 1" = 1'-0"



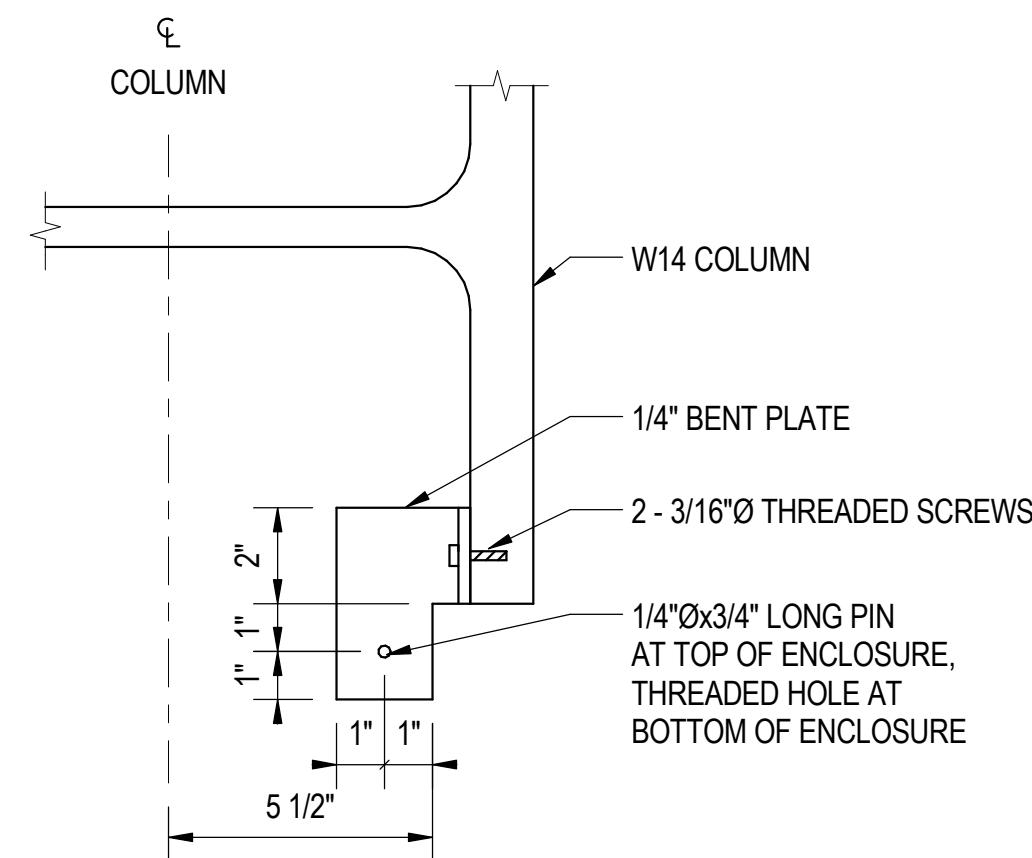
**4** CONDUIT OPENINGS AT TYPICAL COLUMN  
S-1329 SCALE: 1" = 1'-0"



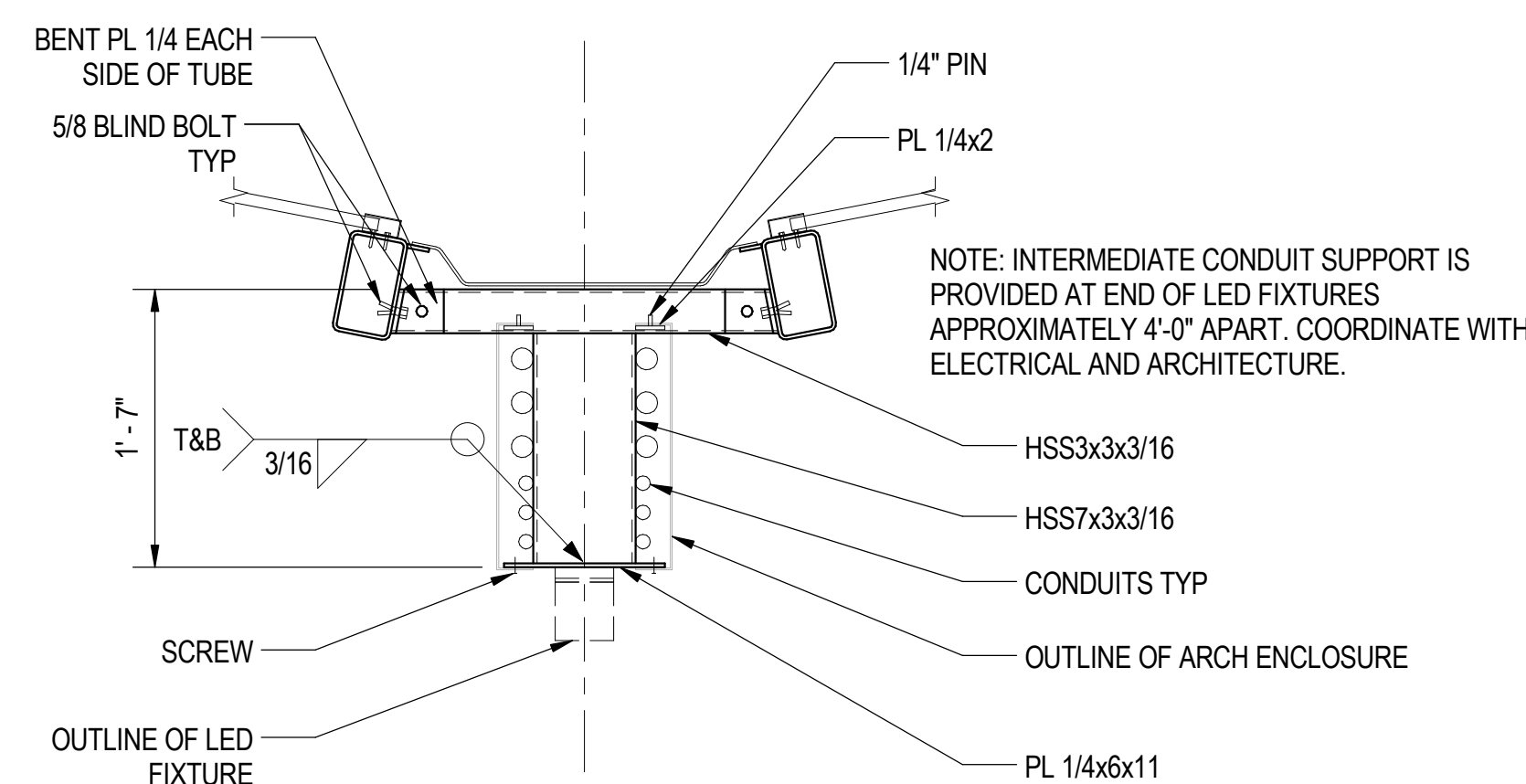
**5** CONDUIT OPENINGS AT CORNER COLUMN  
S-1329 SCALE: 1" = 1'-0"



**6** ENCLOSURE SUPPORT AT COLUMN  
S-1329 SCALE: 1" = 1'-0"



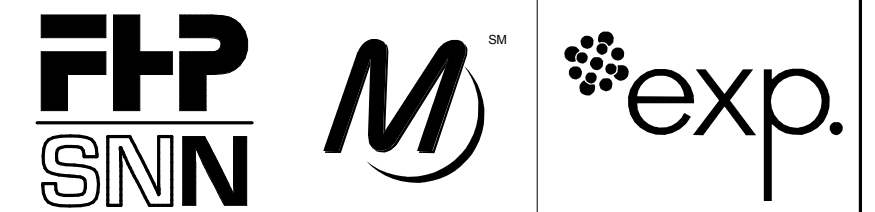
**7** ENCLOSURE ATTACHMENT AT COLUMN  
S-1329 SCALE: 3" = 1'-0"



**8** INTERMEDIATE CONDUIT SUPPORT  
S-1329 SCALE: 1" = 1'-0"

**SHEET NOTES**

- FOR GENERAL NOTES, SEE SWGS S-001 TO S-004
- ALL CANOPY STEEL IS TO BE HOT-DIP GALVANIZED.
- FOR CONDUITS REFER TO ELECTRICAL AND TELECOM SHEETS.
- FOR LIGHT FIXTURES REFER TO ELECTRICAL SHEETS.
- FOR ARCHITECTURAL ENCLOSURES REFER TO AS SERIES SHEETS.



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**95th Terminal Improvement**  
15 West 95th Street (South Terminal)  
Chicago, IL 60628

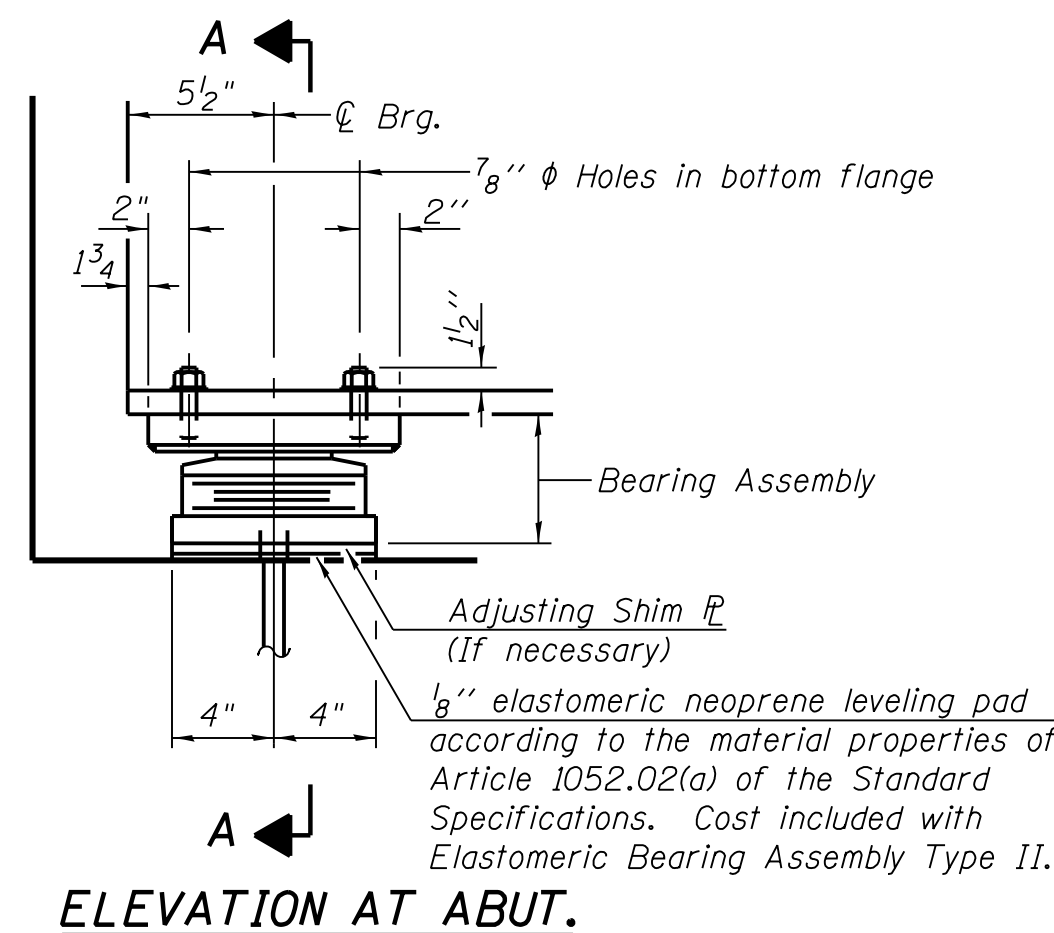
IN CHARGE	J. JAKALSKI
APPROVED BY	J. JAKALSKI
CHECKED BY	R.S. NAIR
DESIGNED BY	S. CITKO
DRAWN BY	D. LALLY
PROJECT NO.	2012-0021.02
FILE NAME	D095_S-RoofCENTRAL

MARK	DATE	DESCRIPTION
	2/9/17	ISSUED FOR CONSTRUCTION

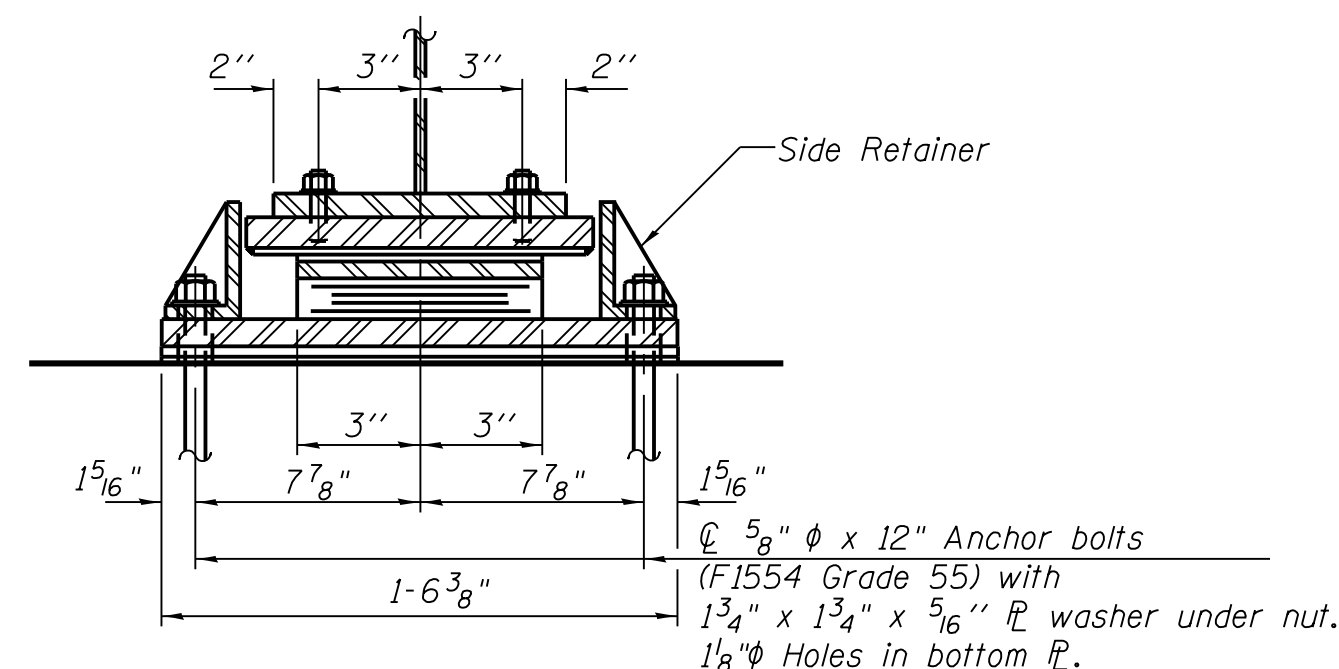
LOCATION IDENTIFIER: **DR095**

**BUS CANOPY DETAILS - 3**

**S-1329**



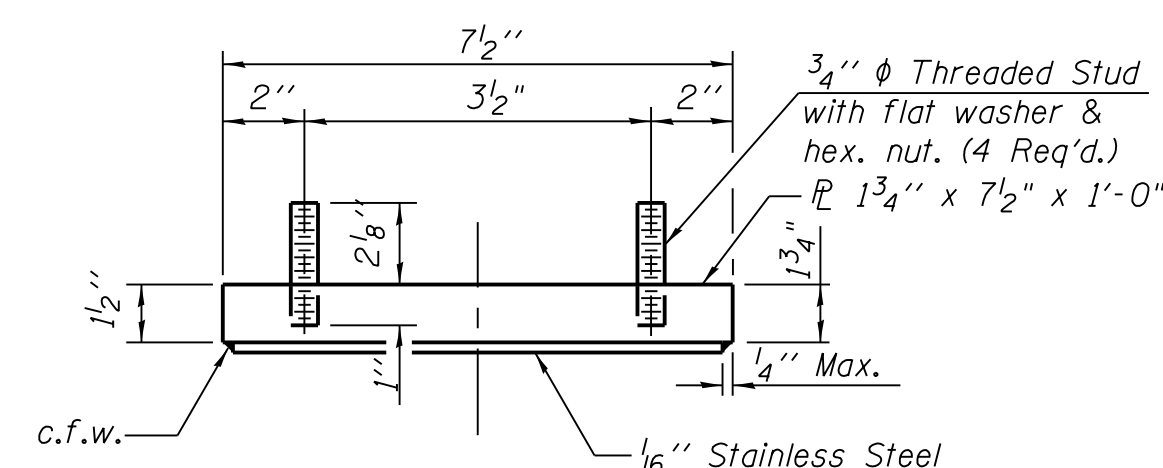
**ELEVATION AT ABUT.**



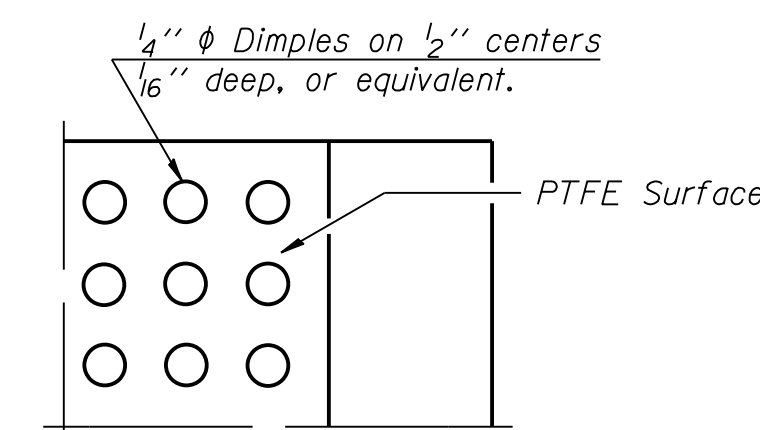
**SECTION A-A**

**TYPE II ELASTOMERIC EXP. BRG.**

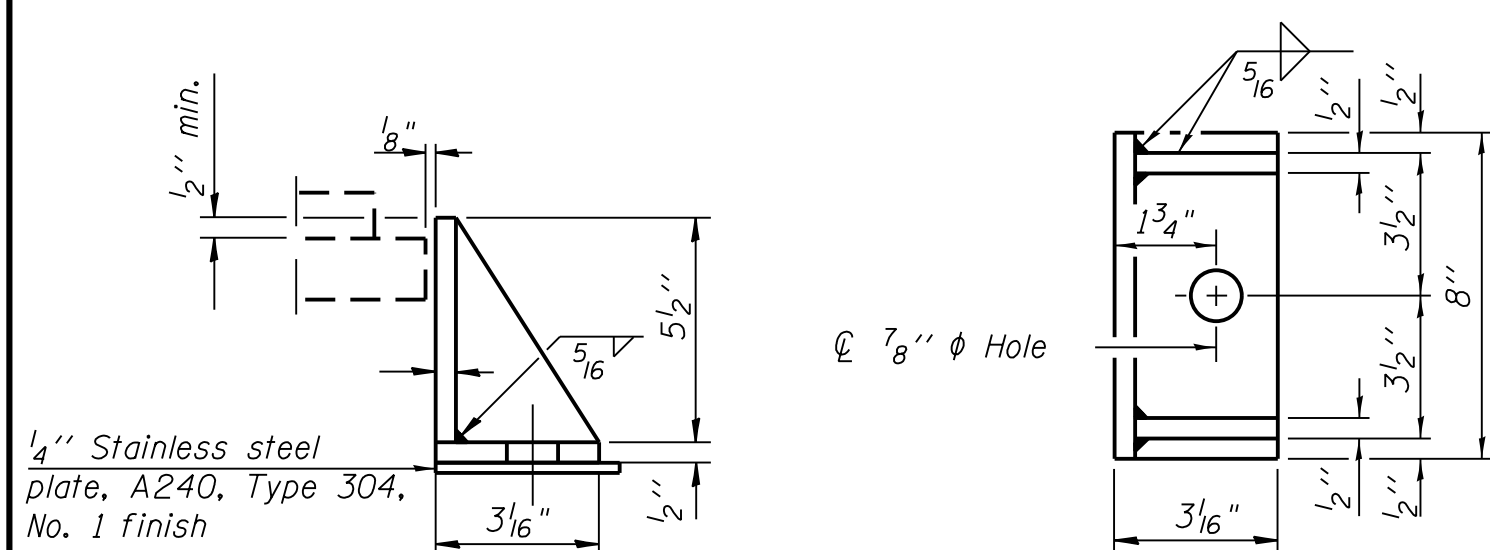
Beams 1F, 1G, 1I, 1J, 16D, 16E, & 16G



**TOP BEARING ASSEMBLY**

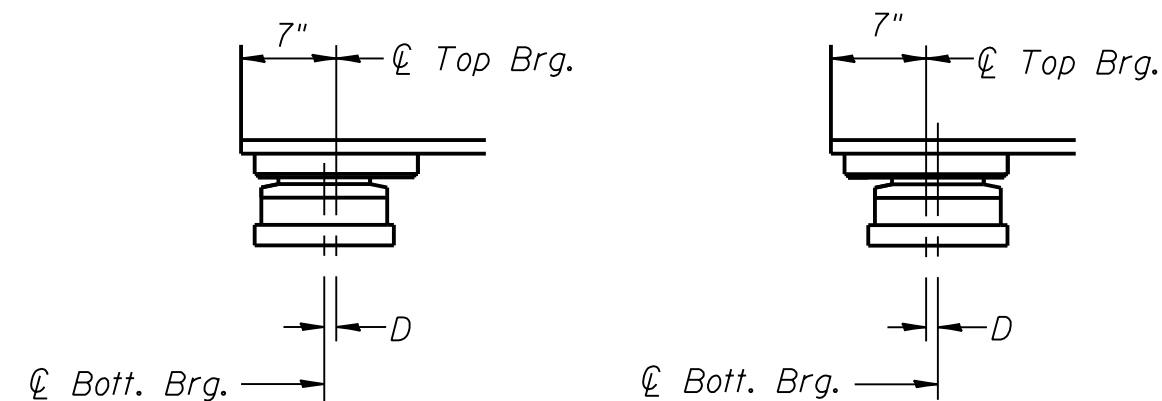


**PLAN-PTFE SURFACE**



**SIDE RETAINER**

Equivalent rolled angle with stiffeners will be allowed in lieu of welded plates.



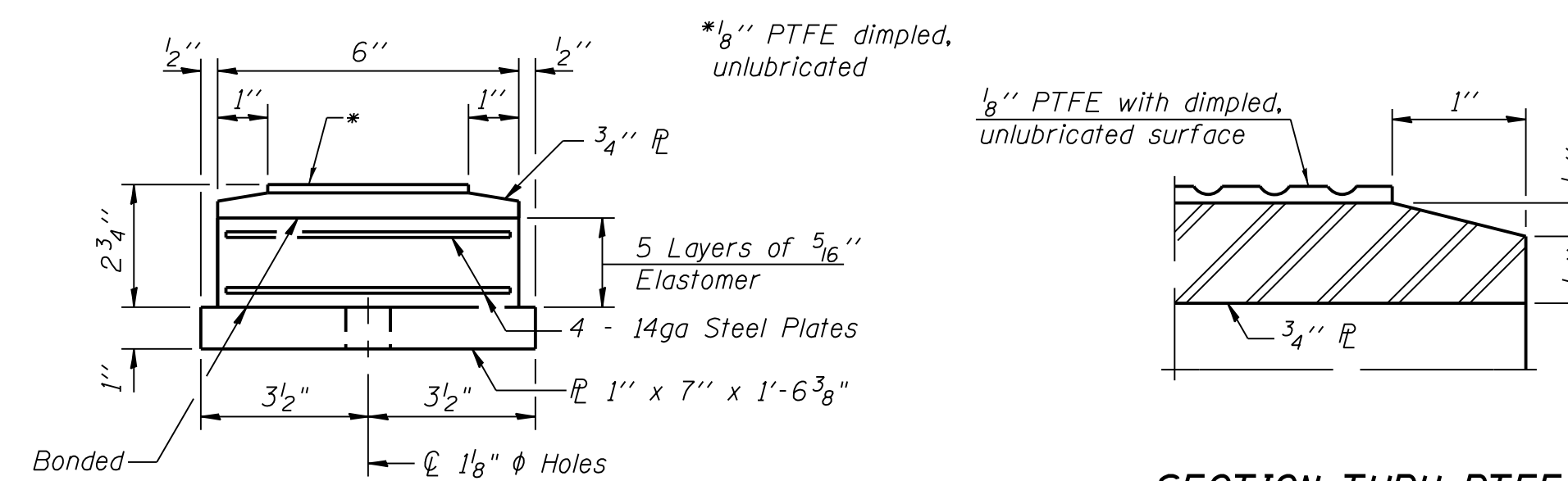
**BELOW 50°F.**

**ABOVE 50°F.**

D = 1/8" per each 100' of expansion for every 15° temp. change from the normal temp. of 50°F.

**EXPANSION BEARING ORIENTATION**

The above diagrams are for informational purposes only to show the amount of expected offset "D" for the current temperature in the field.



**BOTTOM BEARING ASSEMBLY**

**SECTION THRU PTFE**

**Notes:**

Anchor bolts shall be ASTM F1554 all-thread (or an Engineer-approved alternate material) of the grade(s) and diameter(s) specified. The corresponding specified grade of AASHTO M314 anchor bolts may be used in lieu of ASTM F1554.

Side retainers and other steel members required for the elastomeric bearing assembly shall be included in the cost of Elastomeric Bearing Assembly, Type I.

Beams shall be braced for stability during erection and remain braced until deck is poured and cured.

Anchor bolts and side retainers at all supports shall be installed as each member is erected unless an equivalent temporary means of lateral restraint is used. For Steel Extensions, see Sheet S-132B.



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**95th Terminal Improvement**

15 West 95th Street (South Terminal)  
Chicago, IL 60628

<b>IN CHARGE</b>	S. KADERBEK
<b>APPROVED BY</b>	D.ZROKA
<b>CHECKED BY</b>	P.MAID
<b>DESIGNED BY</b>	L.SOMMER
<b>DRAWN BY</b>	T.SLEDGE
<b>PROJECT NO.</b>	2012-0021
<b>FILE NAME</b>	File Name

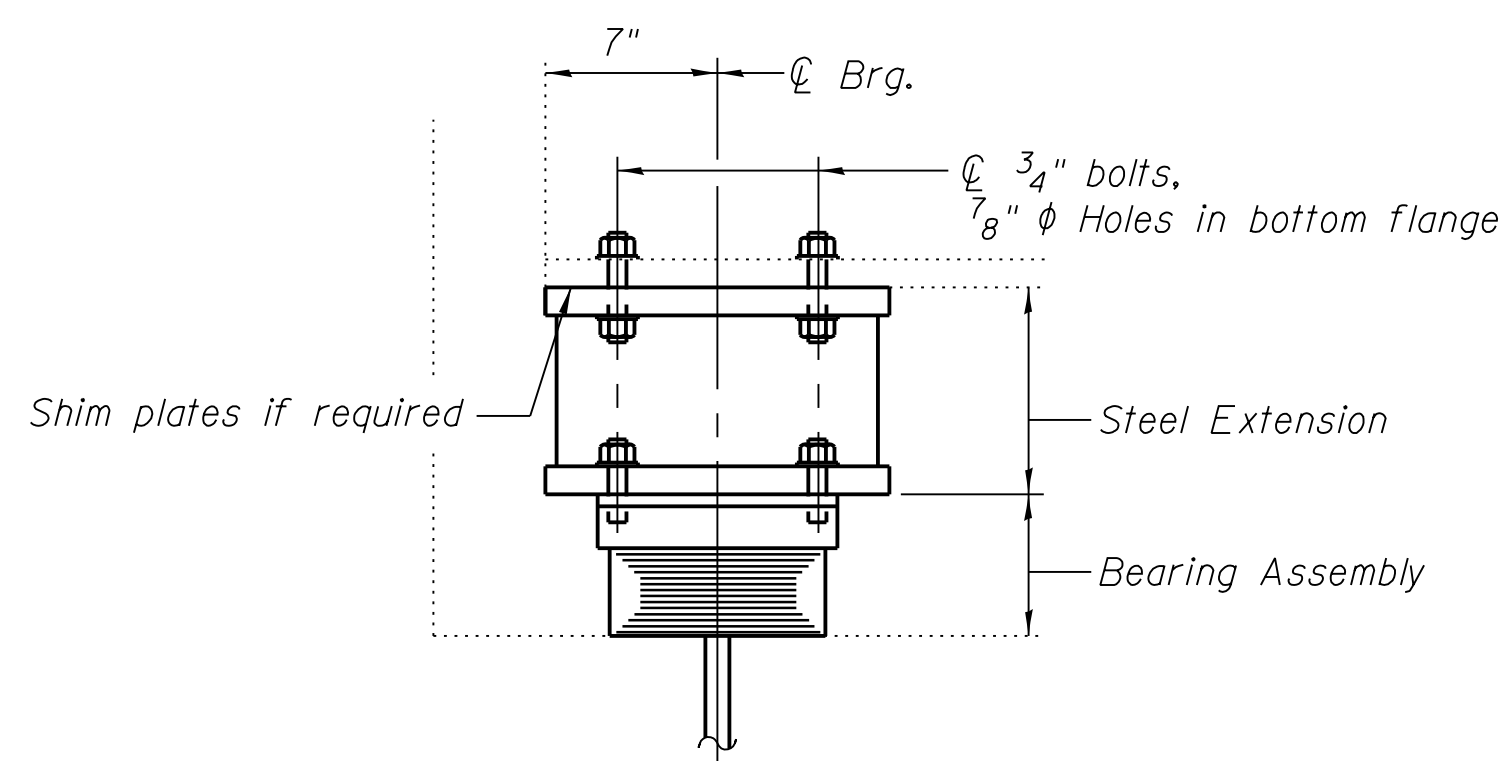
	2/9/17	ISSUED FOR CONSTRUCTION
<b>MARK</b>	<b>DATE</b>	<b>DESCRIPTION</b>

**LOCATION IDENTIFIER: DR095**

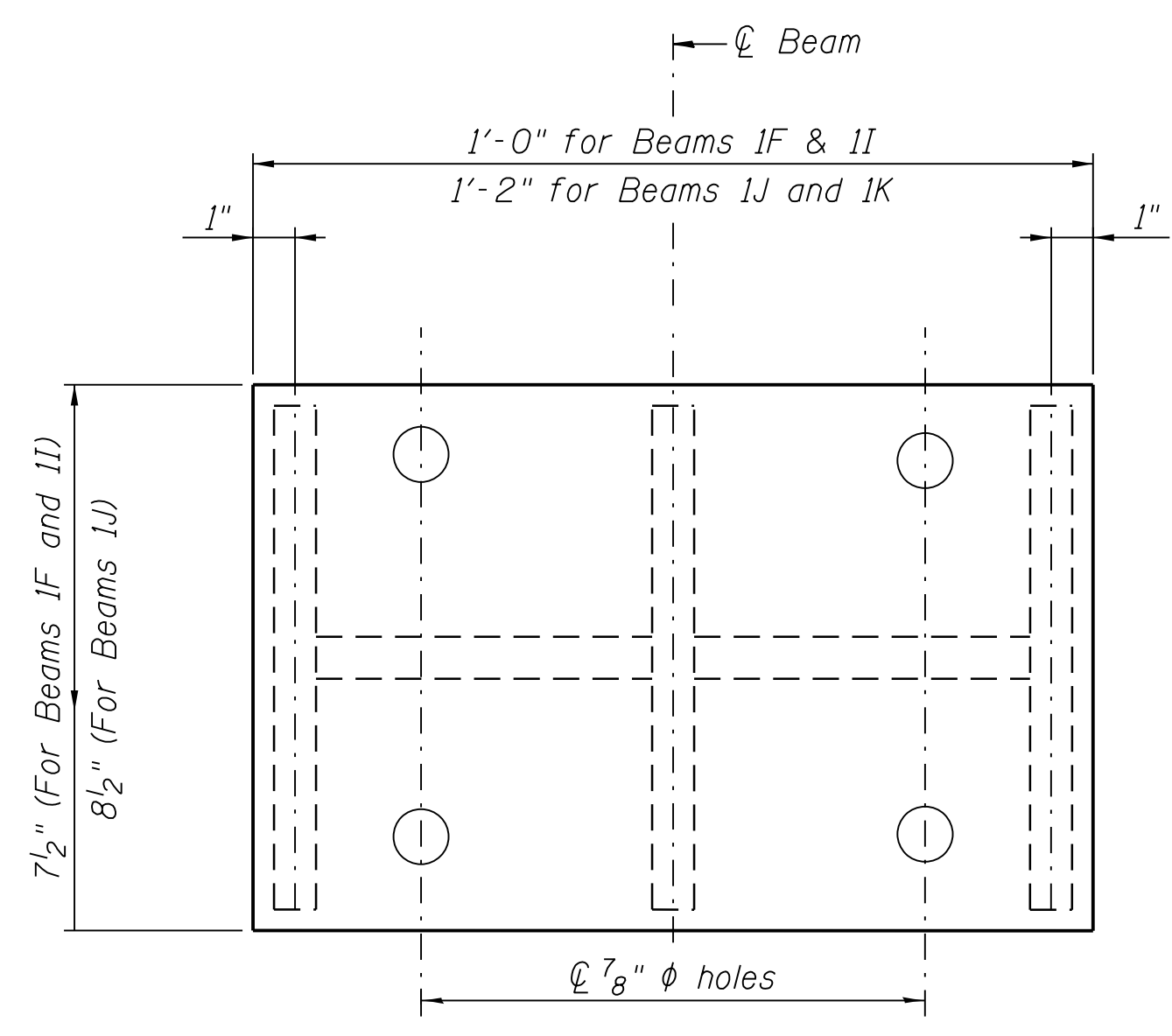
95TH ST. BRIDGE  
BEARING DETAILS  
(SHEET 3 OF 5)  
SN 016-0437

**95TH-S-132A**

vscarpa



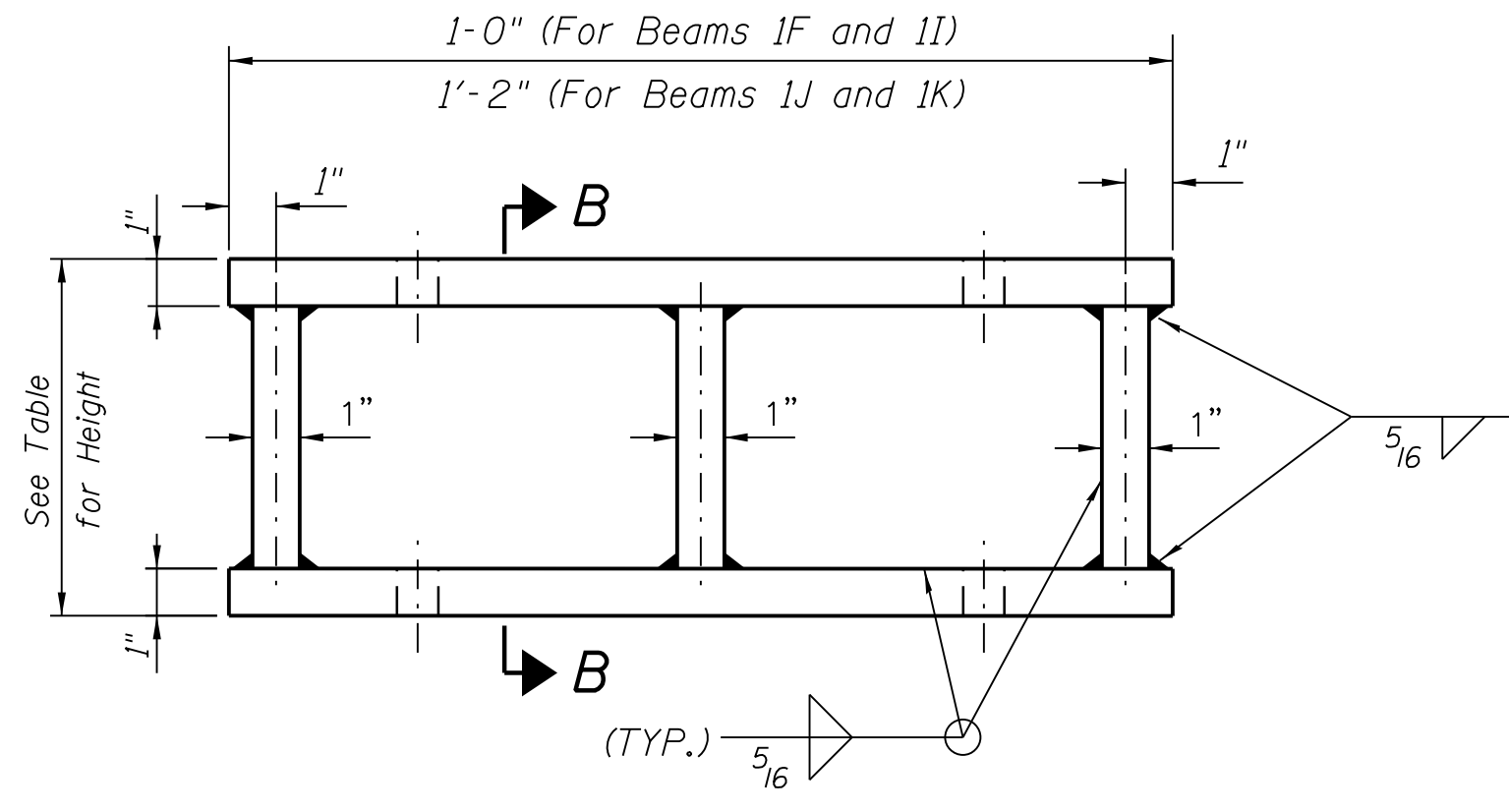
**ELEVATION AT SOUTH ABUTMENT WIDENING**  
 (Beams 1F, 1I, 1J & 1K)



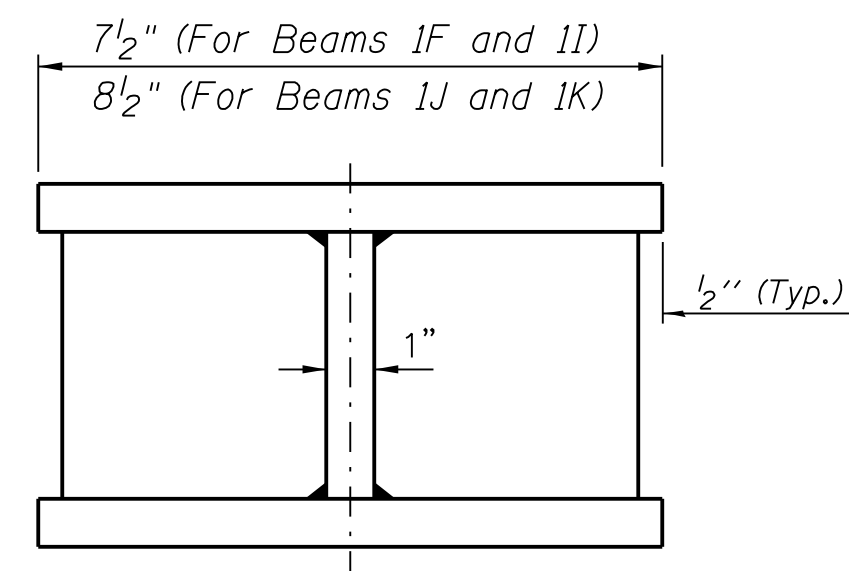
**PLAN TALL STEEL EXTENSION**

**TABLE OF TALL EXTENSION HEIGHTS**

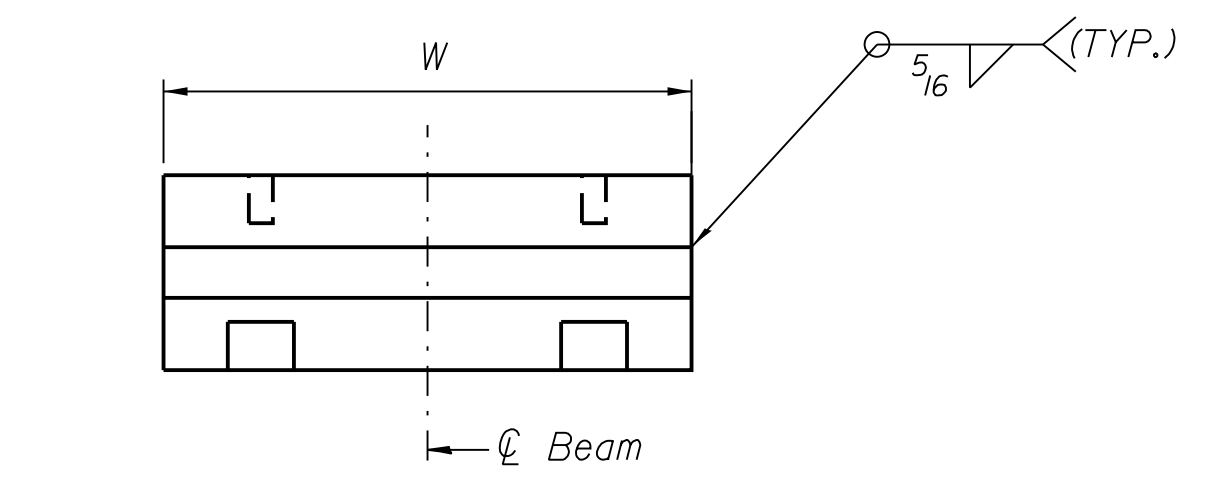
Girder Number	Height
1F	10 13/16"
1J	10 3/4"
1K	10 13/16"



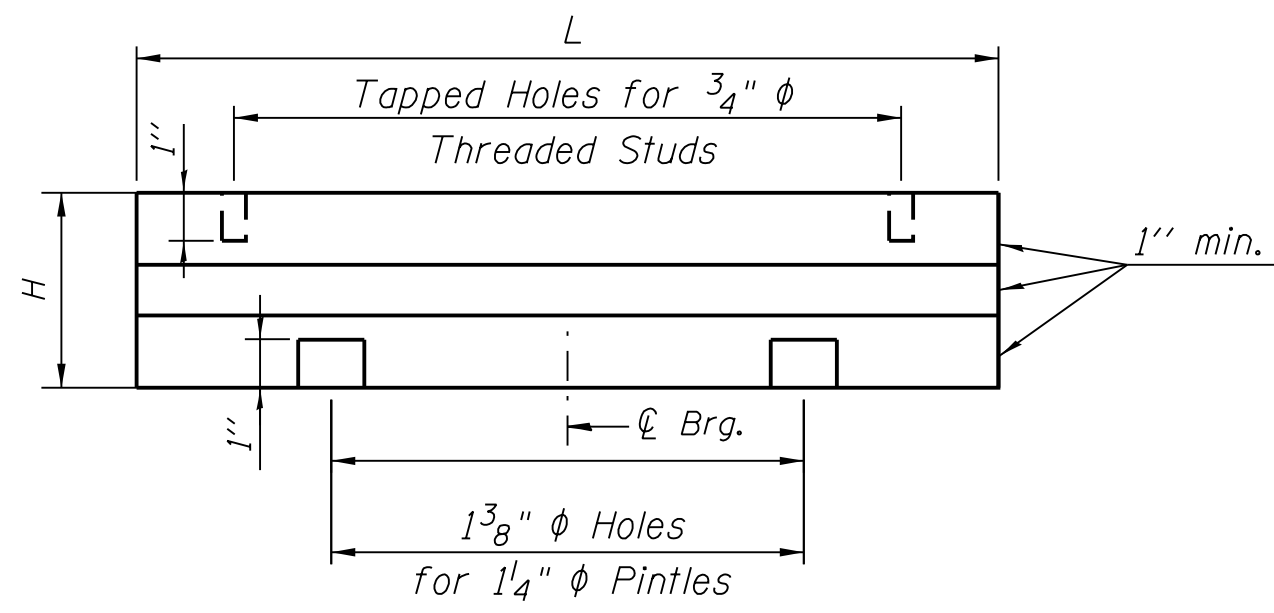
**ELEVATION TALL STEEL EXTENSION**



**SECTION B-B**



**END VIEW SHORT STEEL EXTENSION**



**ELEVATION SHORT STEEL EXTENSION**

**TABLE OF SHORT EXTENSION DIMENSIONS**

Girder Number	L	W	H
<i>West Abutment</i>			
1C	1'-6"	1'-0"	2 5/16"
1D	1'-6"	1'-0"	2 7/8"
1E	1'-2"	8"	2 9/16"
16A	1'-6"	1'-0"	1 7/8"
<i>East Abutment</i>			
1C	1'-6"	1'-0"	1 3/4"
1D	1'-6"	1'-0"	1 1/16"
1E	1'-2"	8 1/2"	2 1/4"

Notes:  
 Prior to ordering any materials, the Contractor shall verify in the field all bearing height and shim thickness dimensions.



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**APPROVED BY** D.ZROKA  
**CHECKED BY** P.MAID  
**DESIGNED BY** L.SOMMER  
**DRAWN BY** T.SLEDGE  
**PROJECT NO.** 2012-0021  
**FILE NAME** File Name

MARK	DATE	DESCRIPTION
	2/9/17	ISSUED FOR CONSTRUCTION

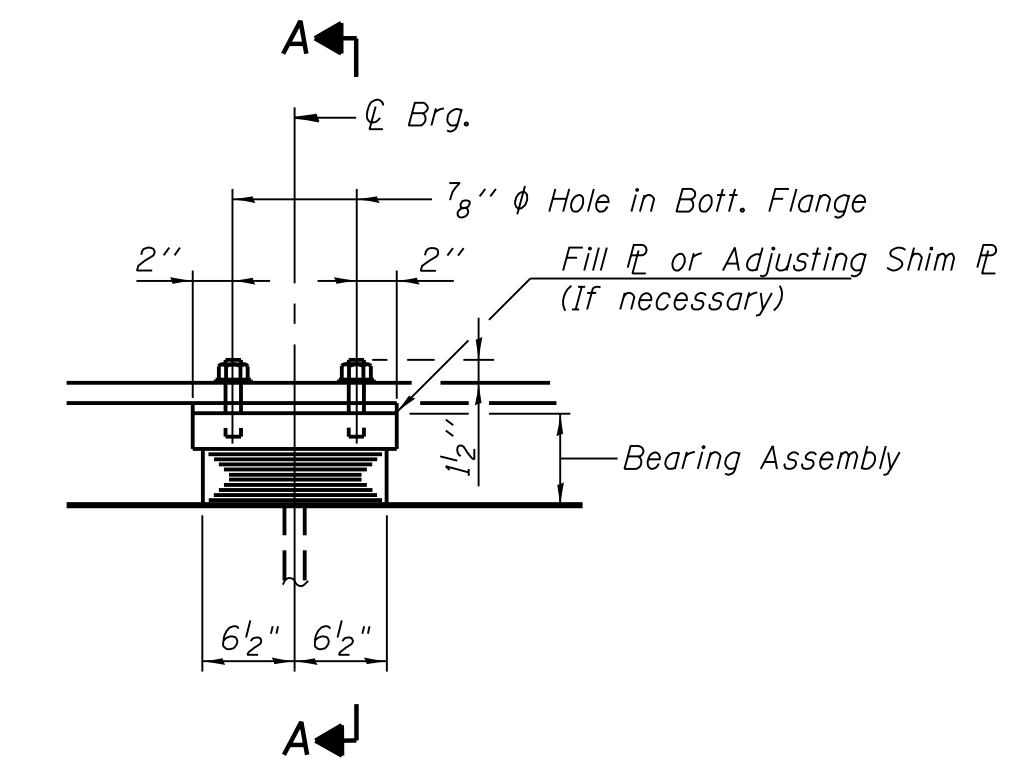
**LOCATION IDENTIFIER: DR095**

95TH ST. BRIDGE  
 BEARING DETAILS  
 (SHEET 4 OF 5)  
 SN 016-0437

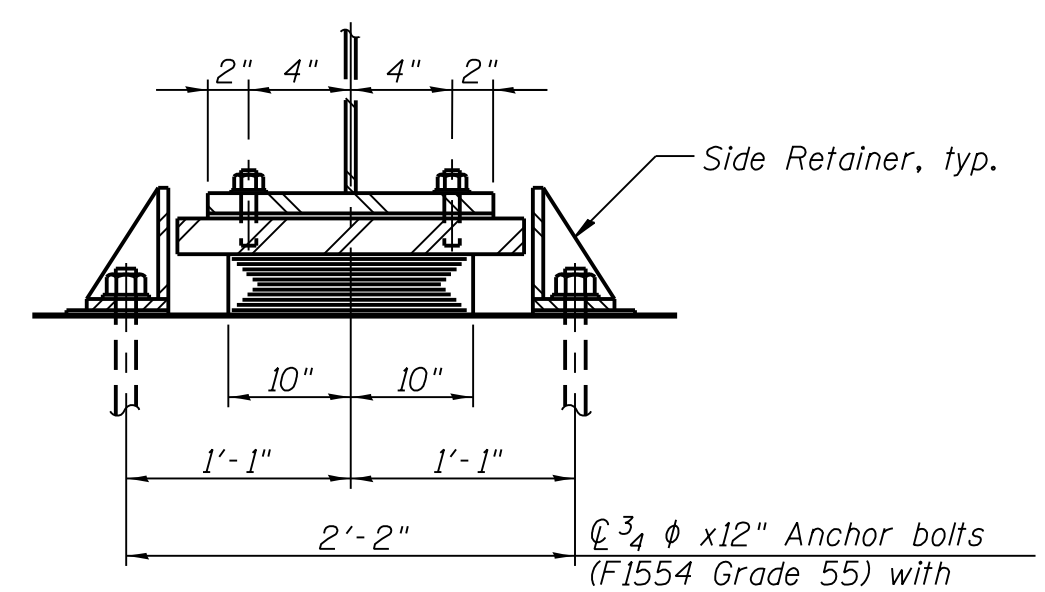
**95TH-S-132B**

FILE NAME = 070\_DR095\_S-132B.dgn  
 PLOT DATE = 8/31/2016

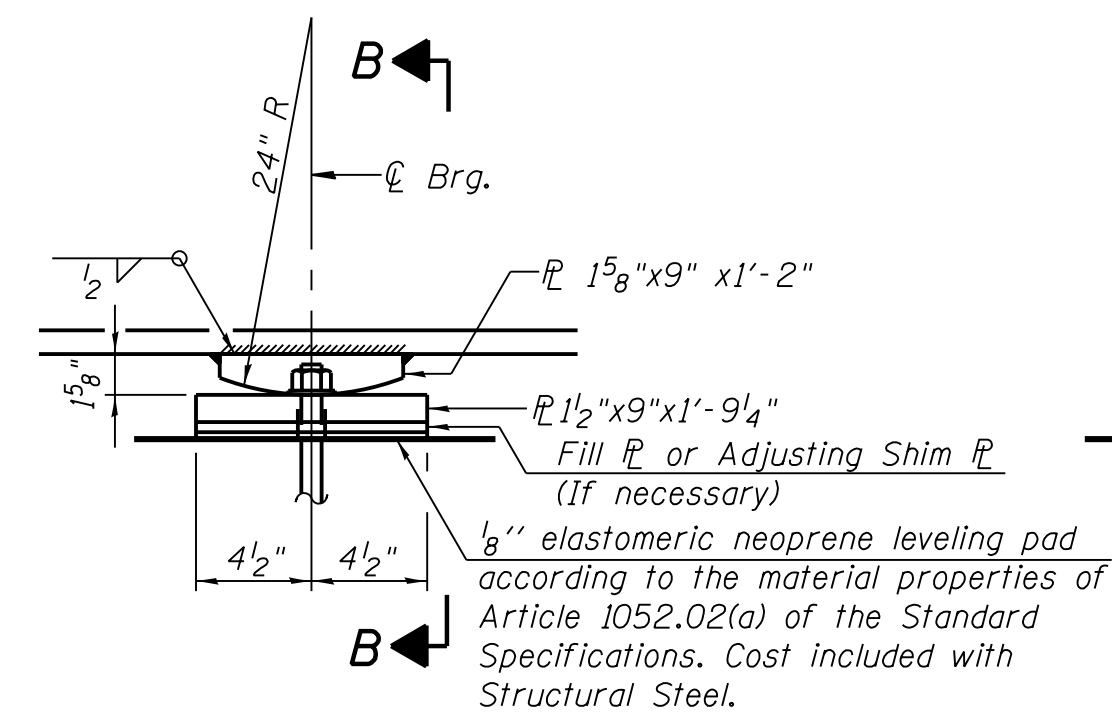
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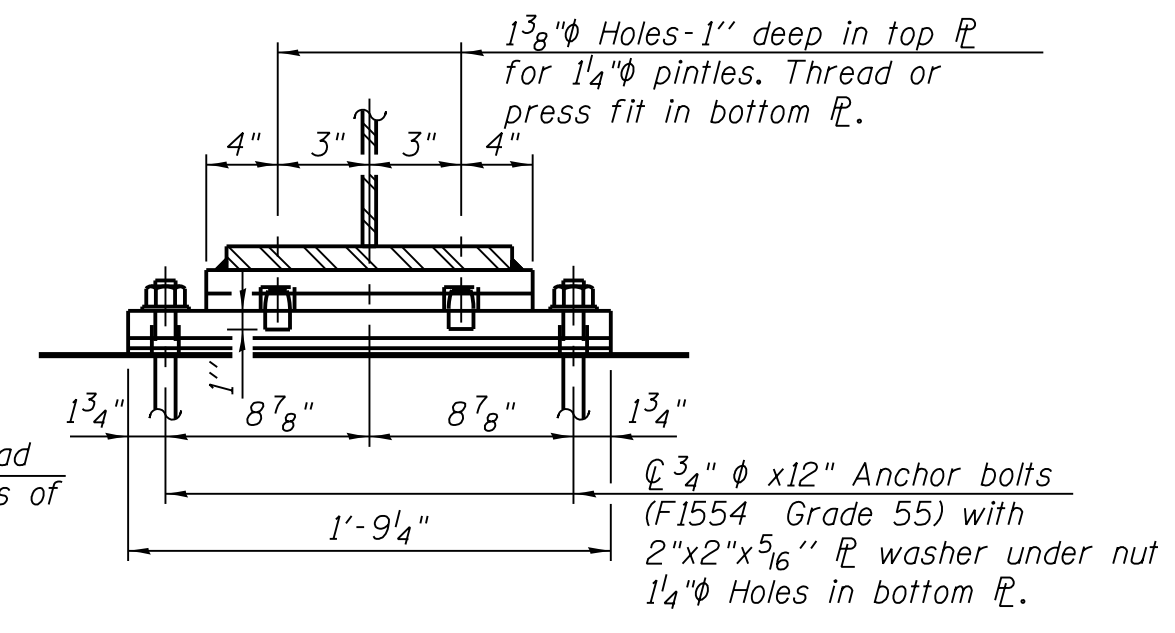
ELEVATION AT PIER 2



SECTION A-A



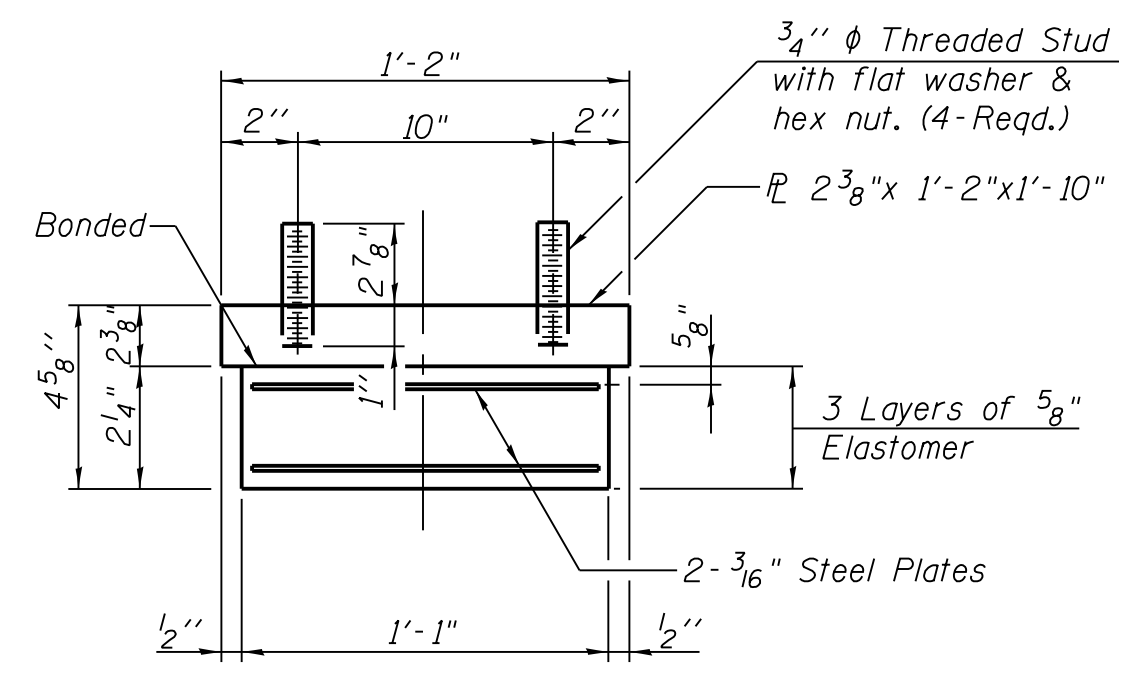
ELEVATION AT PIER 1



SECTION B-B

**TYPE I ELASTOMERIC EXP. BRG.**

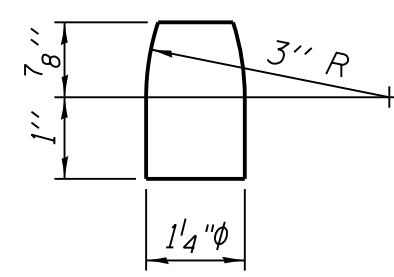
**FIXED BEARING**



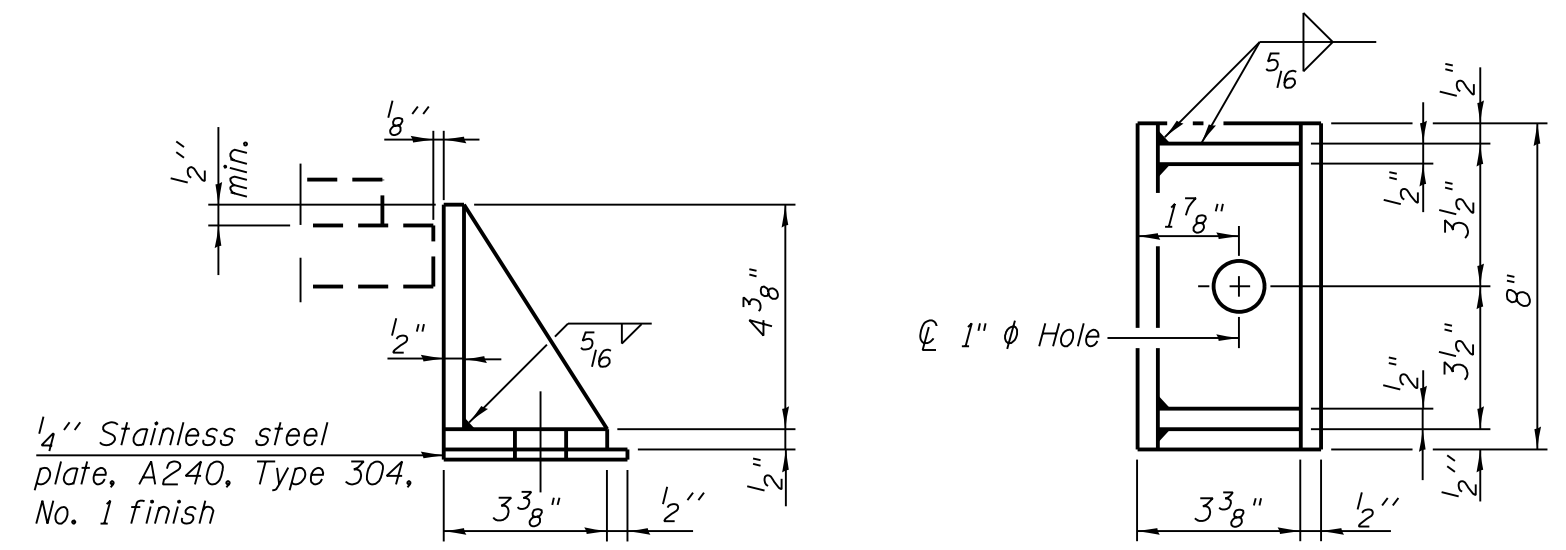
**BEARING ASSEMBLY**

Note:  
Fill plates and shim plates shall not be placed under Bearing Assembly.

Notes:  
Anchor bolts shall be ASTM F1554 all-thread (or an Engineer-approved alternate material) of the grade(s) and diameter(s) specified. The corresponding specified grade of AASHTO M314 anchor bolts may be used in lieu of ASTM F1554.  
Side retainers and other steel members required for the elastomeric bearing assembly shall be included in the cost of Elastomeric Bearing Assembly, Type I.  
Beams shall be braced for stability during erection and remain braced until deck is poured and cured.  
Anchor bolts and side retainers at all supports shall be installed as each member is erected unless an equivalent temporary means of lateral restraint is used.



**PINTLE**



**SIDE RETAINER**  
Equivalent rolled angle with stiffeners will be allowed in lieu of welded plates.

**FILL PLATE TABLE**

Beam No.	Pier 1	Pier 2
1A	1 1/8" x 9" x 1'-9 1/4"	1 1/4" x 1'-2" x 1'-10"
1B	5/8" x 9" x 1'-9 1/4"	9/16" x 1'-2" x 1'-10"
1E	5/8" x 9" x 1'-9 1/4"	9/16" x 1'-2" x 1'-10"
16C	1/16" x 9"-x 1'-9 1/4"	9/16" x 1'-2" x 1'-10"



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Chicago, IL 60628

IN CHARGE	S. KADERBEK
APPROVED BY	D.ZROKA
CHECKED BY	P.MAID
DESIGNED BY	L.SOMMER
DRAWN BY	T.SLEDGE
PROJECT NO.	2012-0021
FILE NAME	File Name

MARK	DATE	DESCRIPTION
	2/9/17	ISSUED FOR CONSTRUCTION

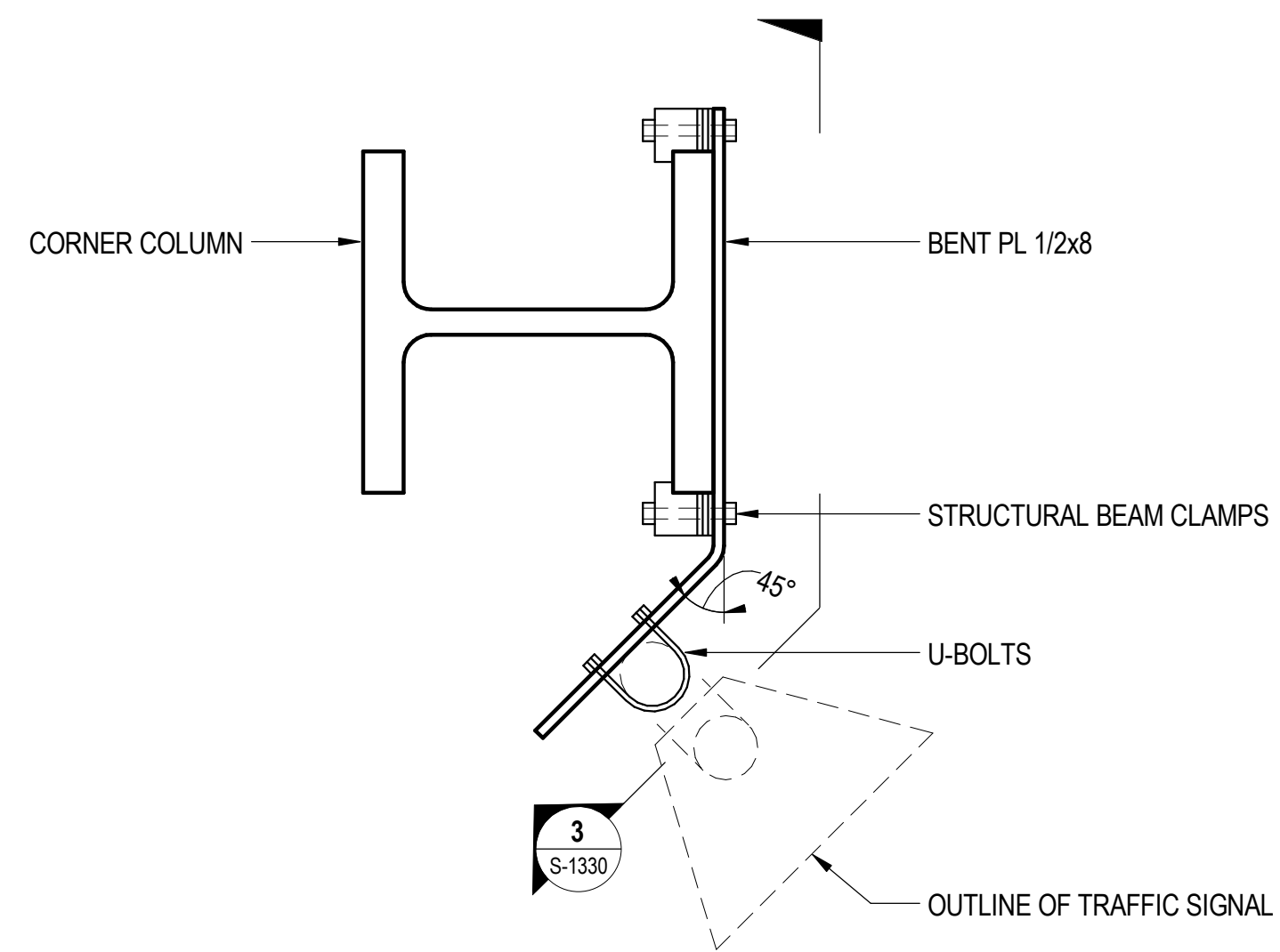
LOCATION IDENTIFIER: **DR095**  
95TH ST. BRIDGE  
BEARING DETAILS  
(SHEET 5 OF 5)  
SN 016-0437

**95TH-S-133**

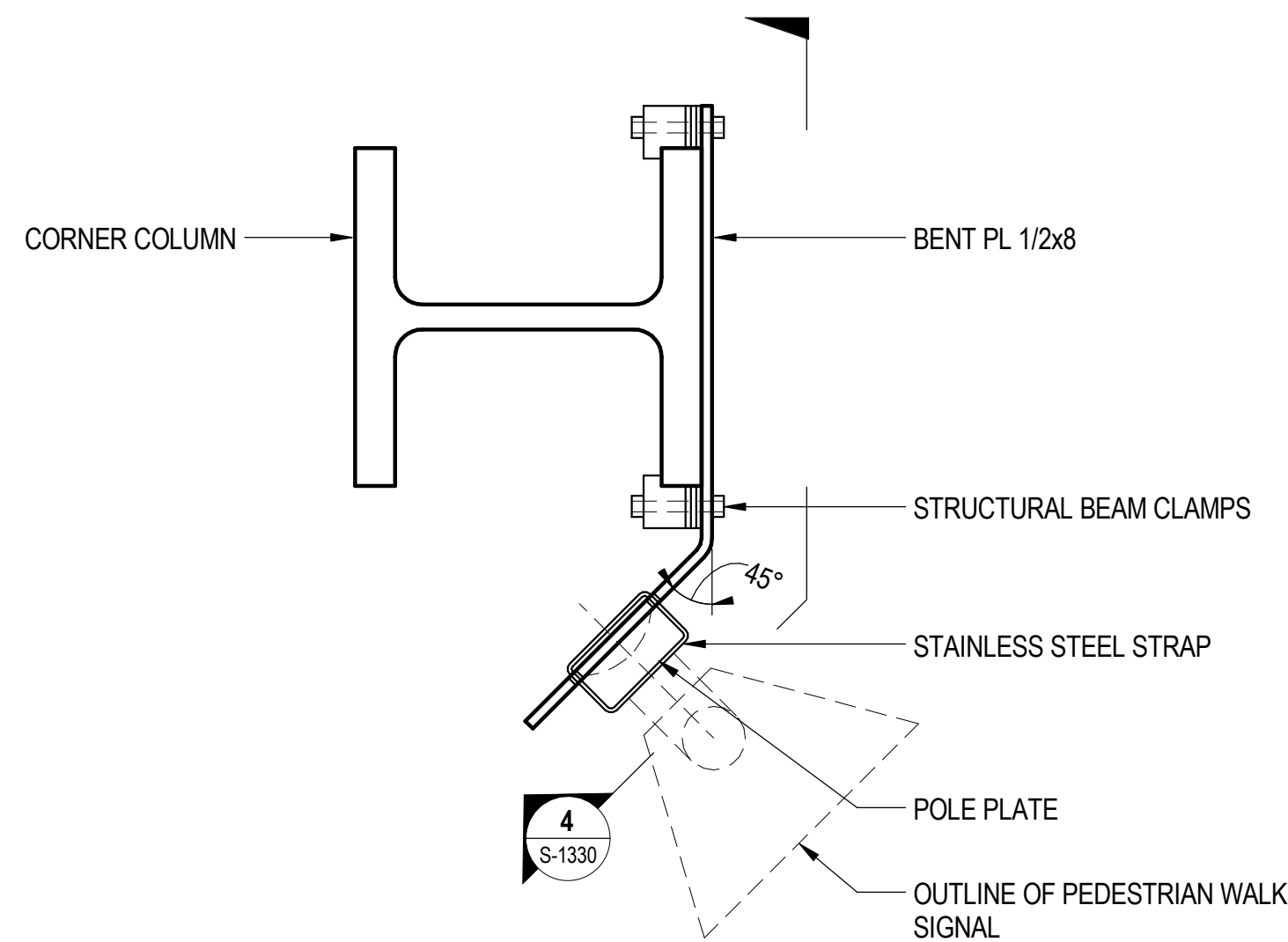
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PLOT DATE = 8/31/2016

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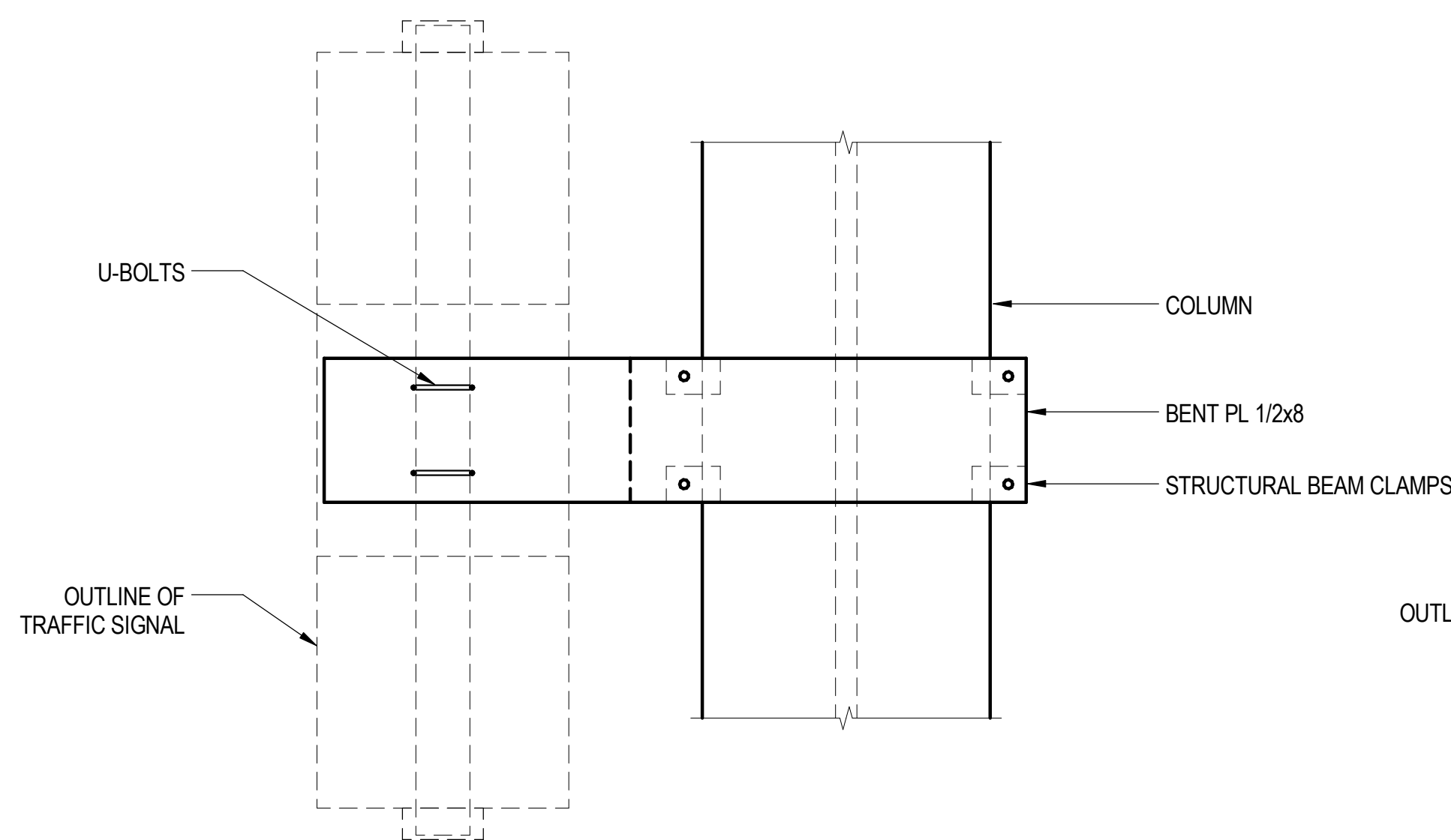
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**1** TRAFFIC SIGNAL MOUNTING DETAIL  
S-1330 SCALE: 1 1/2" = 1'-0"

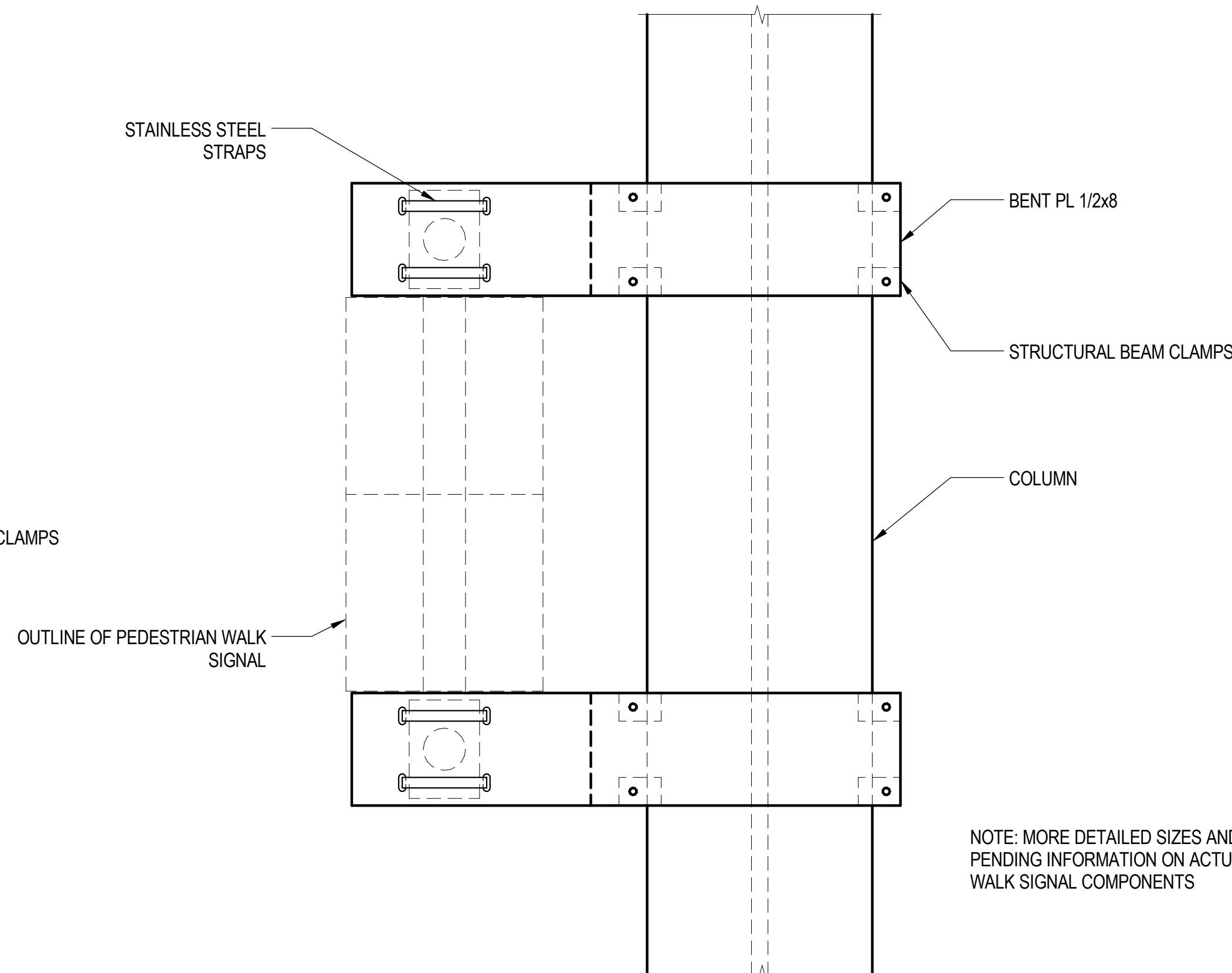


**2** PEDESTRIAN WALK SIGNAL MOUNTING DETAIL  
S-1330 SCALE: 1 1/2" = 1'-0"



NOTE: MORE DETAILED SIZES AND DIMENSIONS PENDING INFORMATION ON ACTUAL TRAFFIC SIGNAL COMPONENTS

**3** SECTION  
S-1330 SCALE: 1 1/2" = 1'-0"

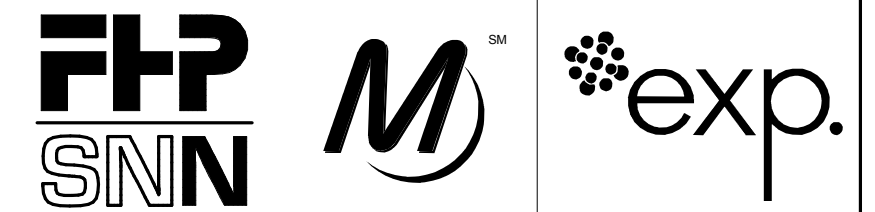


NOTE: MORE DETAILED SIZES AND DIMENSIONS PENDING INFORMATION ON ACTUAL PEDESTRIAN WALK SIGNAL COMPONENTS

**4** SECTION  
S-1330 SCALE: 1 1/2" = 1'-0"

**SHEET NOTES**

- 1. FOR GENERAL NOTES, SEE SWGS S-001 TO S-004
- 2. ALL CANOPY STEEL IS TO BE HOT-DIP GALVANIZED.
- 3. FOR TRAFFIC SIGNAL LOCATION REFER TO TRAFFIC SIGNAL CIVIL SHEETS.



**SENSITIVE SECURITY INFORMATION**

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**95th Terminal Improvement**  
15 West 95th Street (South Terminal)  
Chicago, IL 60628

IN CHARGE	J. JAKALSKI
APPROVED BY	J. JAKALSKI
CHECKED BY	R.S. NAIR
DESIGNED BY	S. CITKO
DRAWN BY	D. LALLY
PROJECT NO.	2012-0021.02
FILE NAME	D095_S-RooF\CENTRAL

MARK	DATE	DESCRIPTION
	2/9/17	ISSUED FOR CONSTRUCTION

LOCATION IDENTIFIER: **DR095**

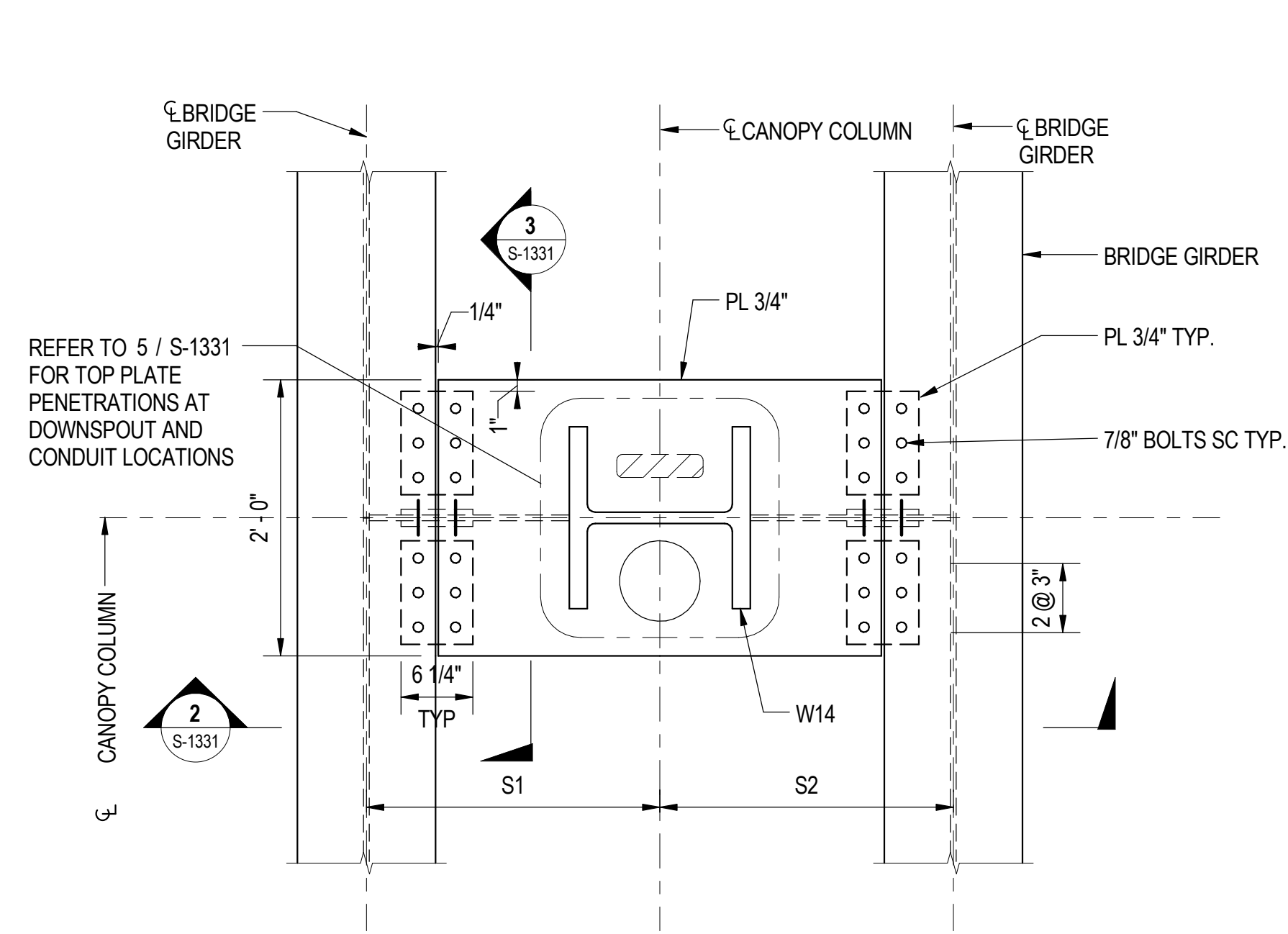
**BUS CANOPY DETAILS - 4**

**S-1330**

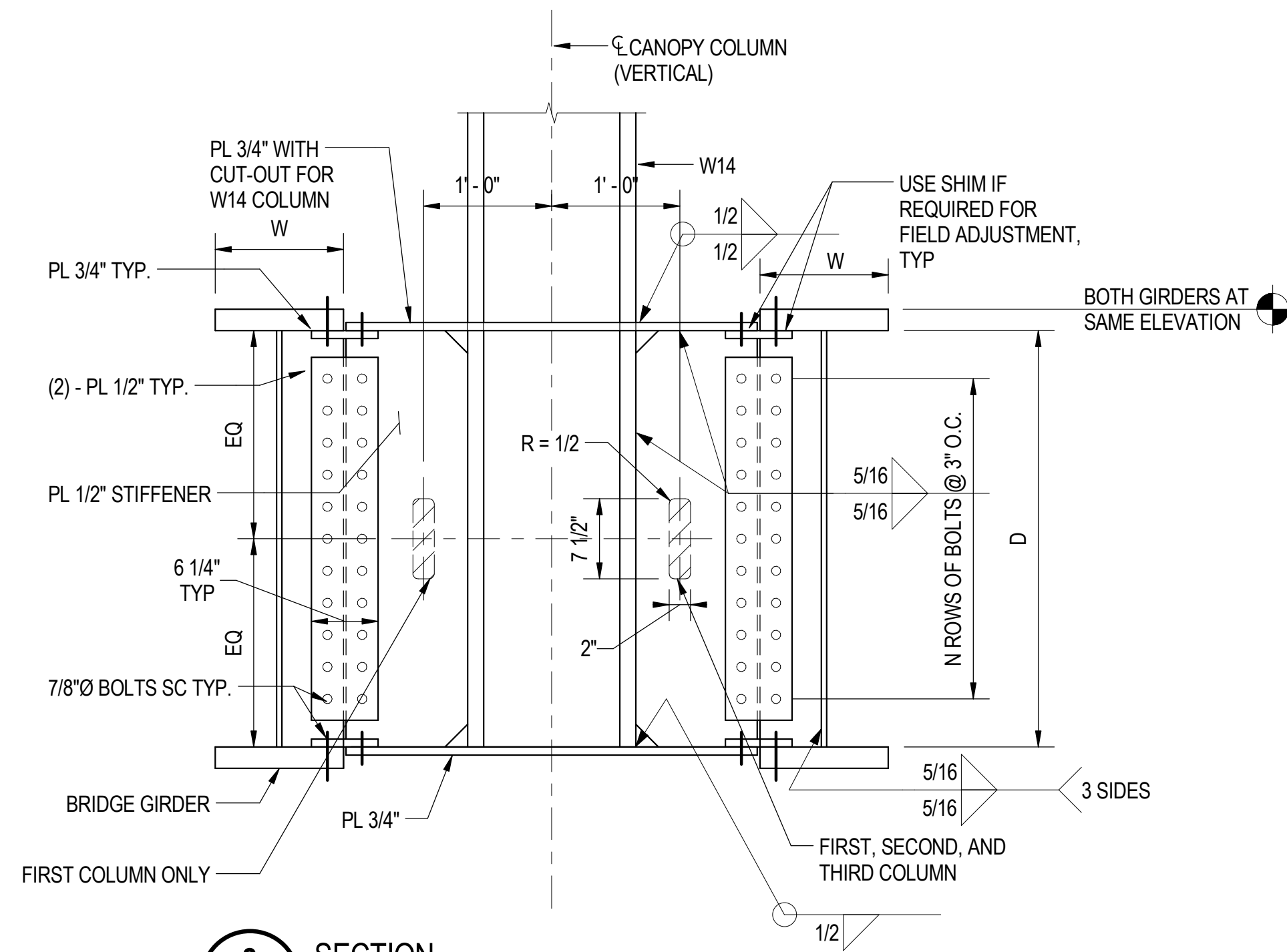


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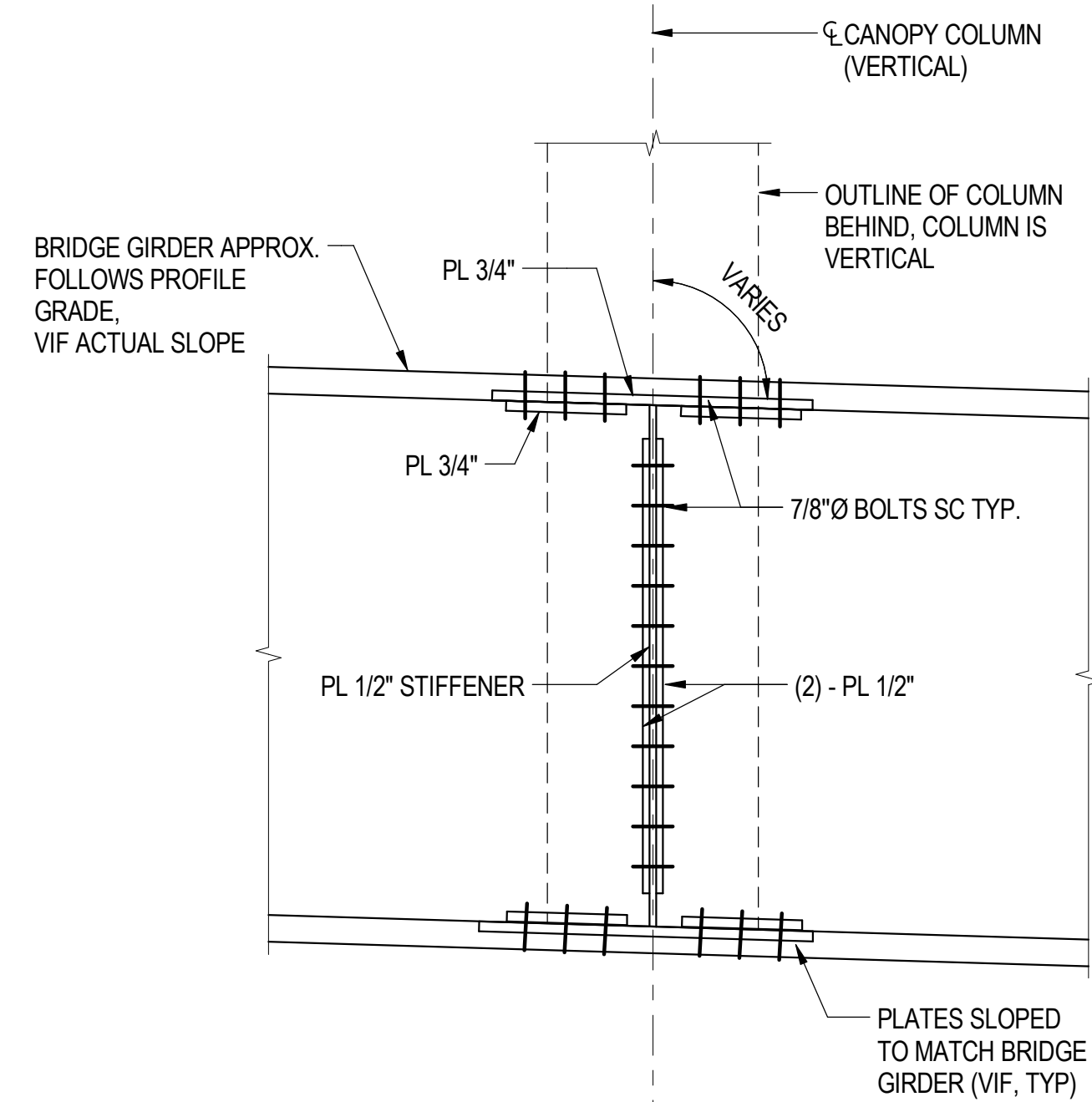


**1** TYPICAL COLUMN TO BRIDGE CONNECTION DETAIL  
S-1331 SCALE: 1" = 1'-0"

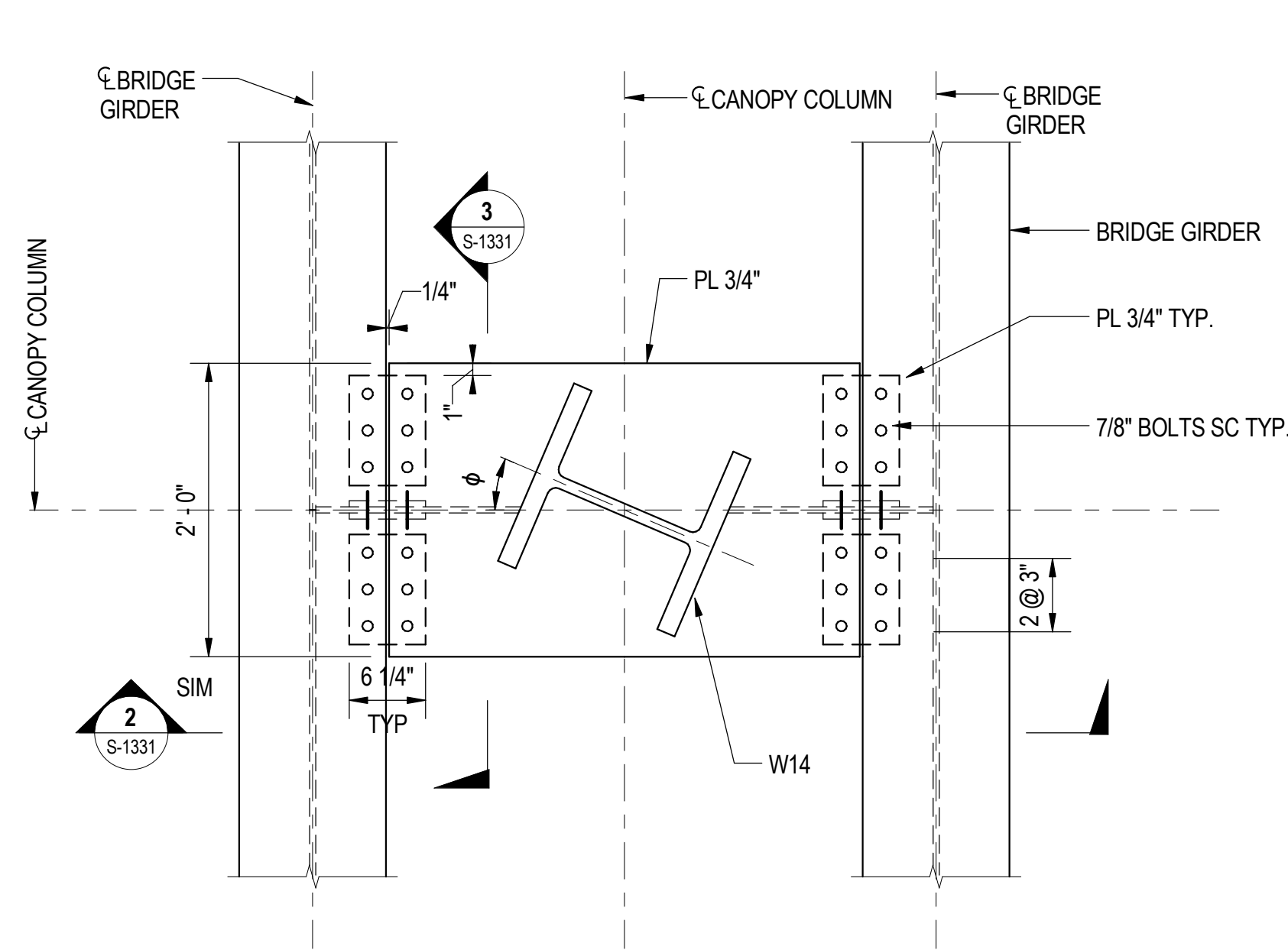


**2** SECTION  
S-1331 SCALE: 1" = 1'-0"

TYPICAL BRIDGE CONNECTIONS					
BRIDGE	D	W	S1	S2	N
SBB	39"	18"	2'-5"	3'-0"	9
95TH-S	34"	12"	2'-1"	2'-2"	8
95TH-N	34"	12"	1'-10"	2'-5"	8
NBB	TBD	TBD	2'-1 1/2"	1'-10 1/2"	9

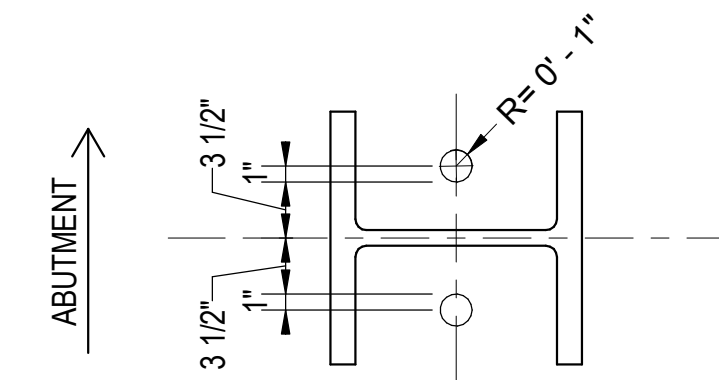


**3** SECTION  
S-1331 SCALE: 1" = 1'-0"



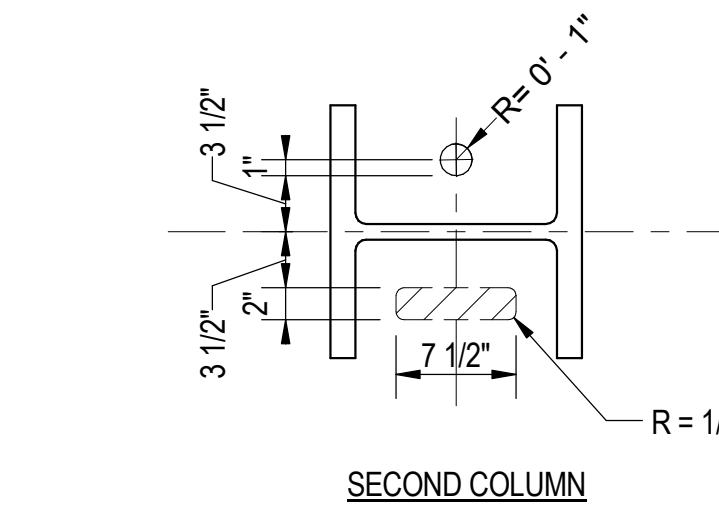
**4** TYPICAL CORNER COLUMN TO BRIDGE CONNECTION DETAIL  
S-1331 SCALE: 1" = 1'-0"

CORNER BRIDGE CONNECTIONS		
COLUMN	ANGLE (φ)	S,W,D,N
SBB-1	23.2°	SEE TYP
SBB-9	22.8°	SEE TYP
95-1	21.9°	SEE TYP
95-9	22.1°	SEE TYP
95-10	22.5°	SEE TYP
95-19	22.5°	SEE TYP
NBB-1	22.5°	SEE TYP
NBB-10	22.5°	SEE TYP

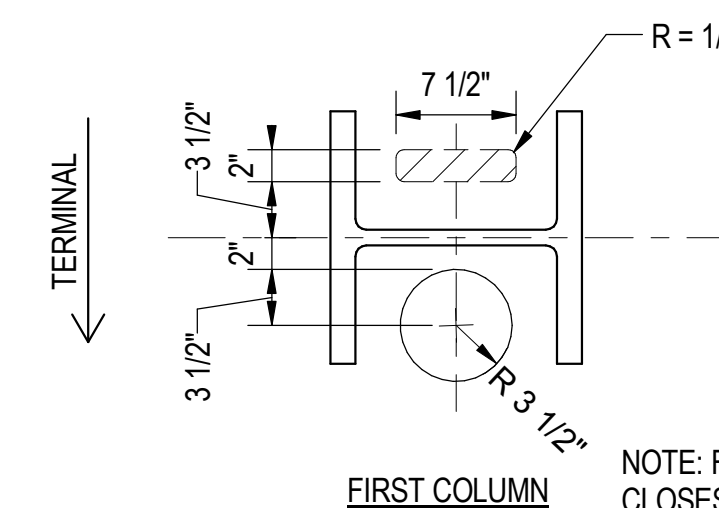


**5** DOWNSPOUT AND CONDUIT PENETRATIONS IN TOP PLATE  
S-1331 SCALE: 1" = 1'-0"

THIRD COLUMN



SECOND COLUMN



FIRST COLUMN

NOTE: FIRST COLUMN IS ONE CLOSEST TO TERMINAL BUILDING

**SHEET NOTES**

- FOR GENERAL NOTES, SEE DWGS S-001 TO S-004
- ALL CANOPY STEEL IS TO BE HOT-DIP GALVANIZED.



**SENSITIVE SECURITY INFORMATION**

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**95th Terminal Improvement**  
15 West 95th Street (South Terminal)  
Chicago, IL 60628

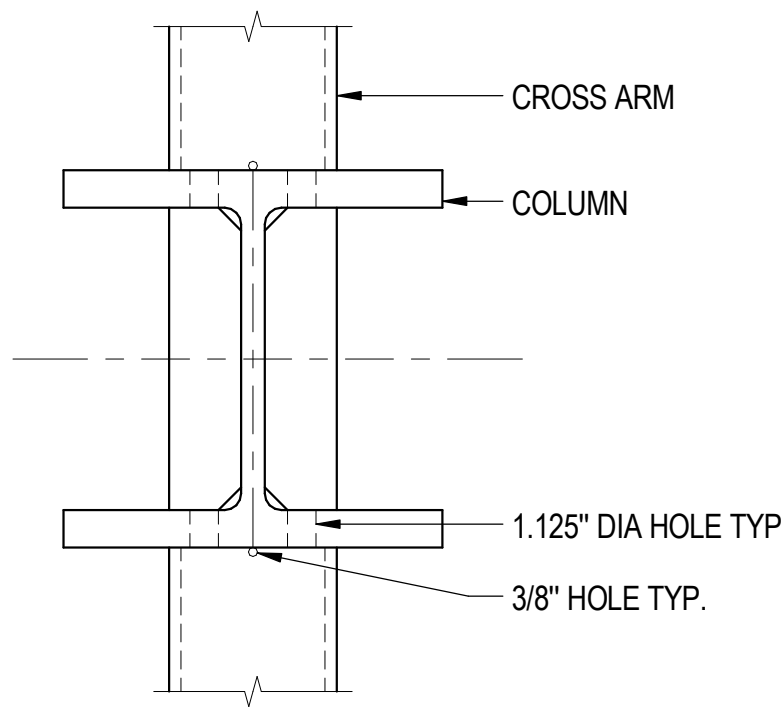
IN CHARGE	J. JAKALSKI
APPROVED BY	J. JAKALSKI
CHECKED BY	R.S. NAIR
DESIGNED BY	S. CITKO
DRAWN BY	C. BOVE
PROJECT NO.	2012-0021.02
FILE NAME	D095_S-RoofCENTRAL

MARK	DATE	DESCRIPTION
	2/9/17	ISSUED FOR CONSTRUCTION

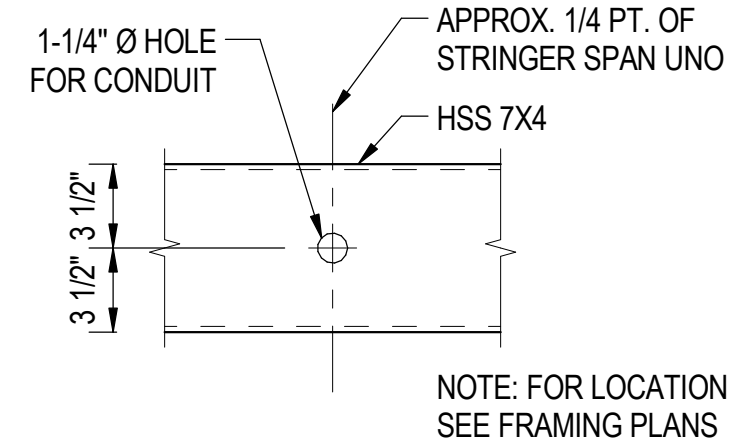
LOCATION IDENTIFIER: **DR095**

**BUS CANOPY DETAILS - 5**

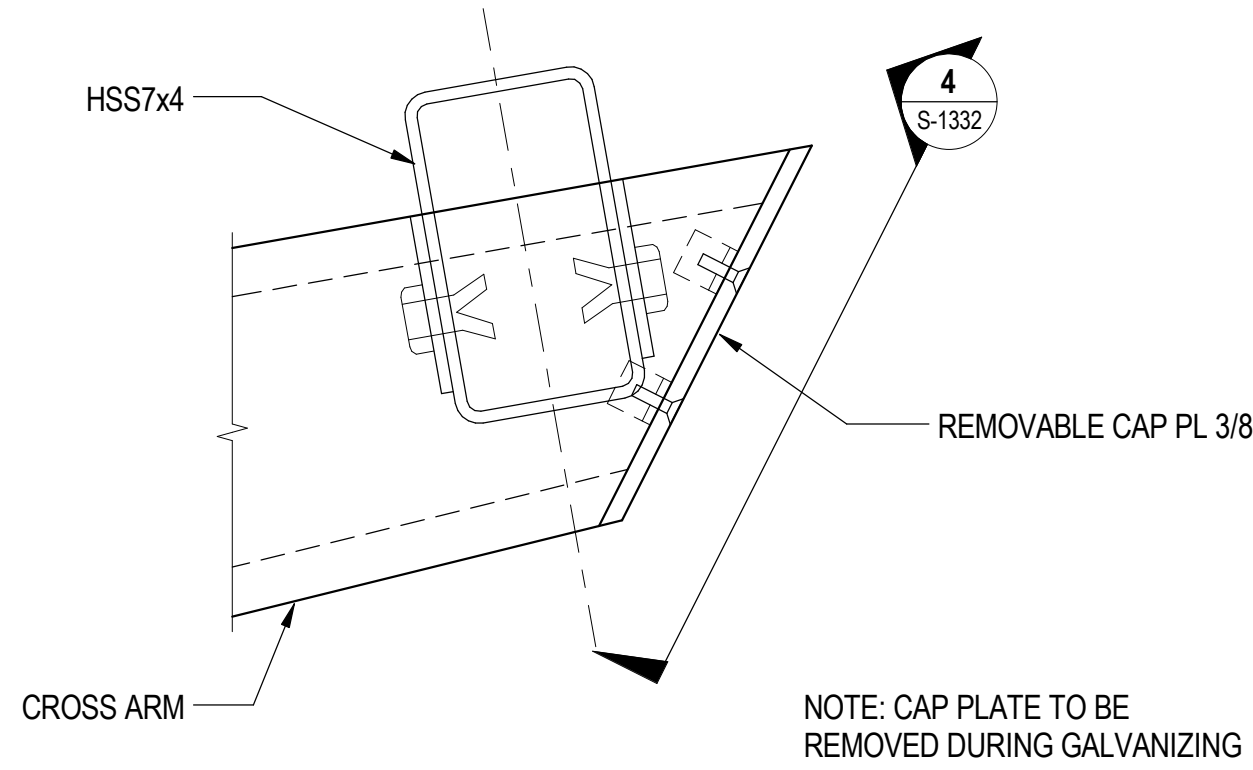
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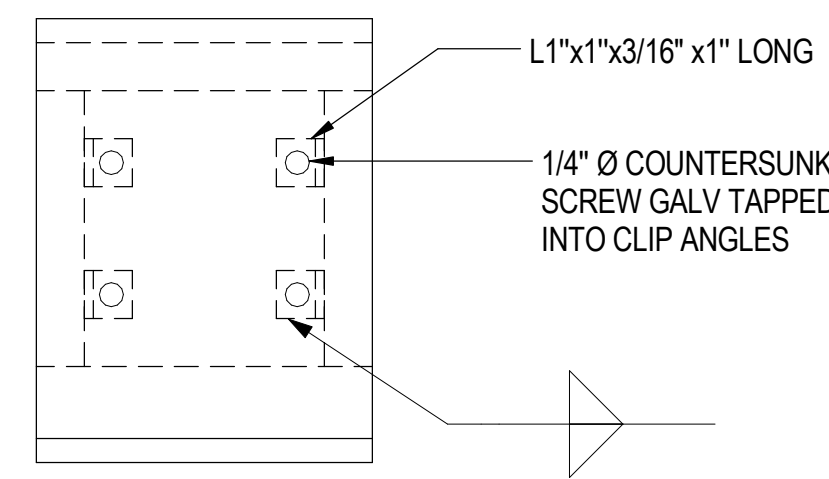
**1** TOP VIEW  
S-1332 SCALE: 1 1/2" = 1'-0"



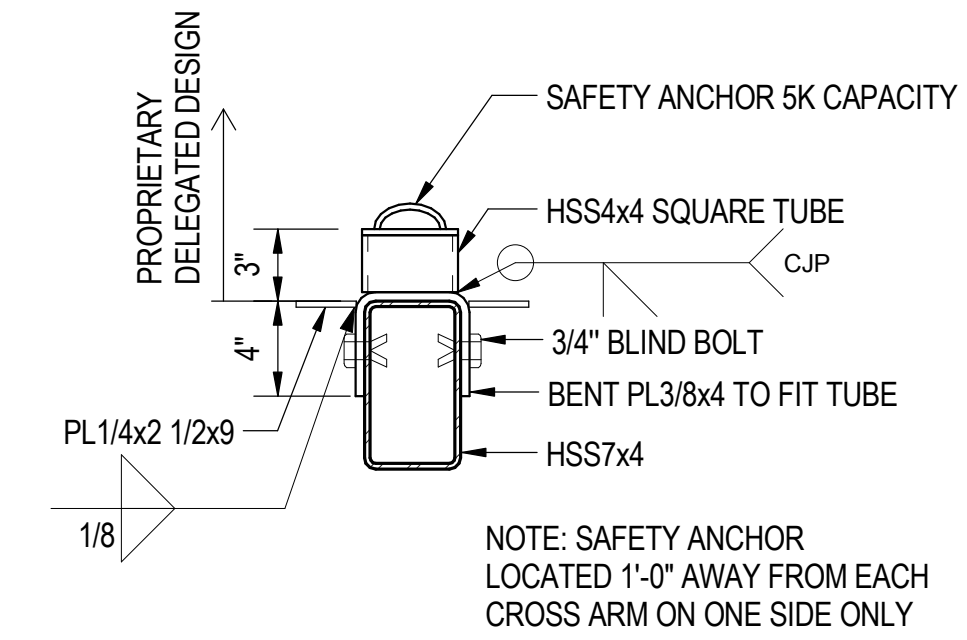
**2** STRINGER CONDUIT OPENING  
S-1332 SCALE: 1 1/2" = 1'-0"



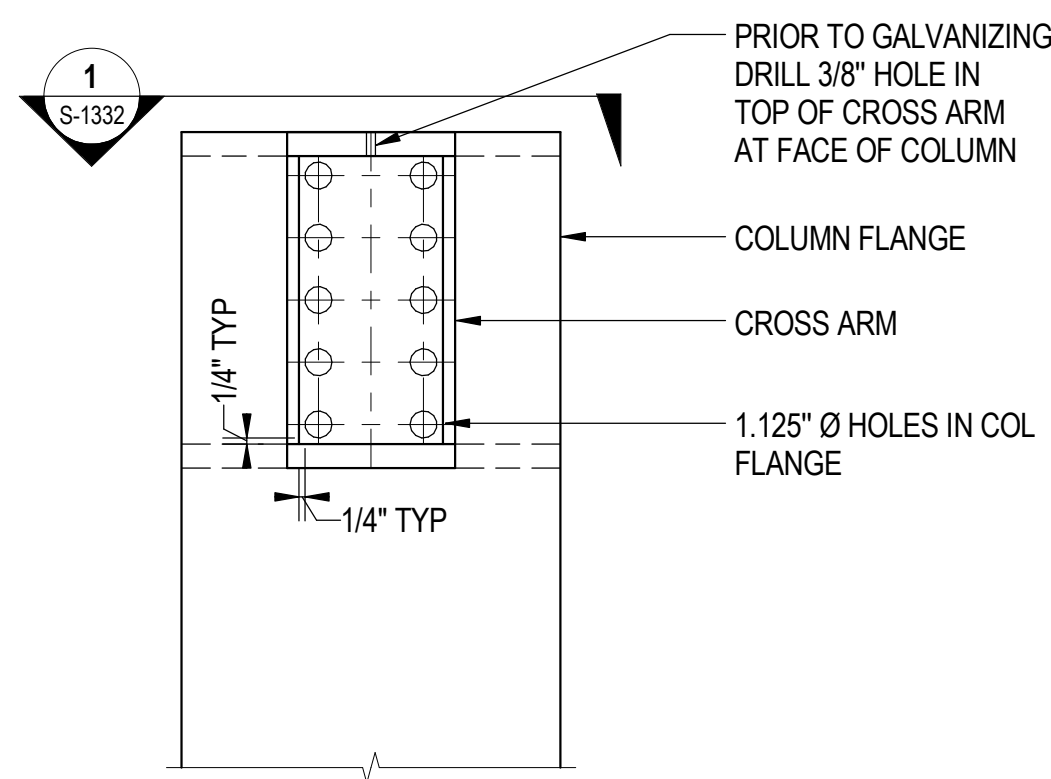
**3** CAP PLATE DETAIL  
S-1332 SCALE: 3" = 1'-0"



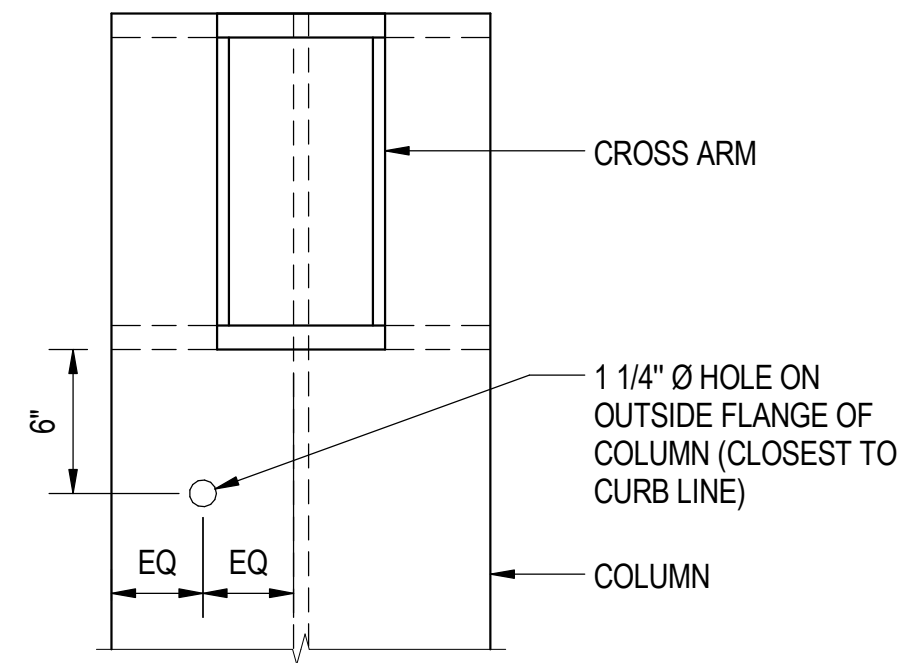
**4** END VIEW  
S-1332 SCALE: 3" = 1'-0"



**5** SAFETY ANCHOR DETAIL  
S-1332 SCALE: 1 1/2" = 1'-0"



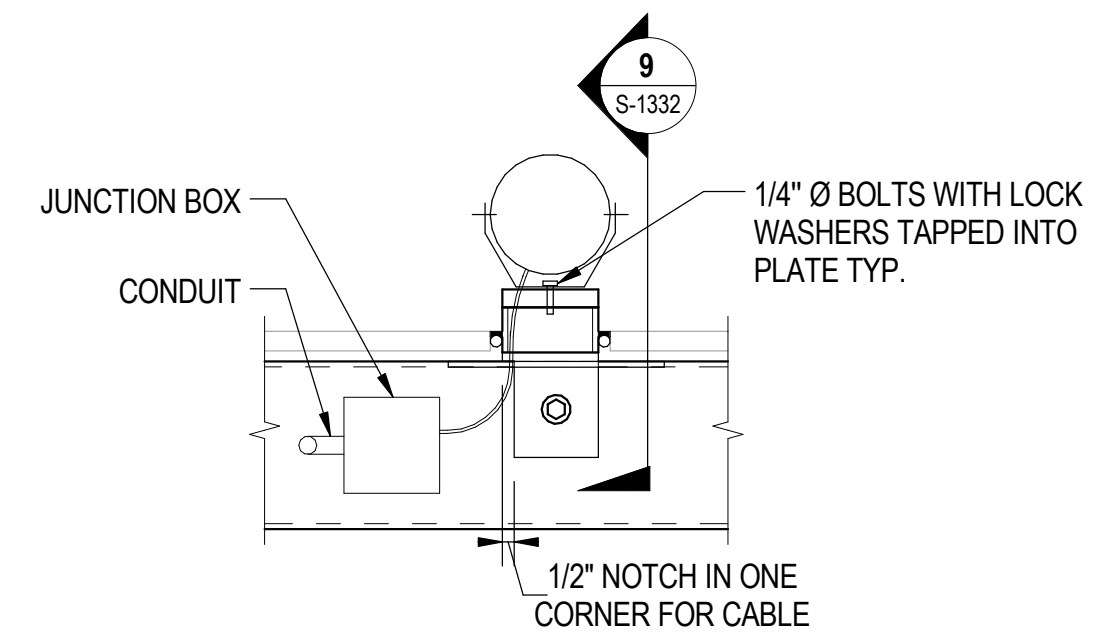
**6** CROSS ARM VENT AND FILL HOLES  
S-1332 SCALE: 1 1/2" = 1'-0"



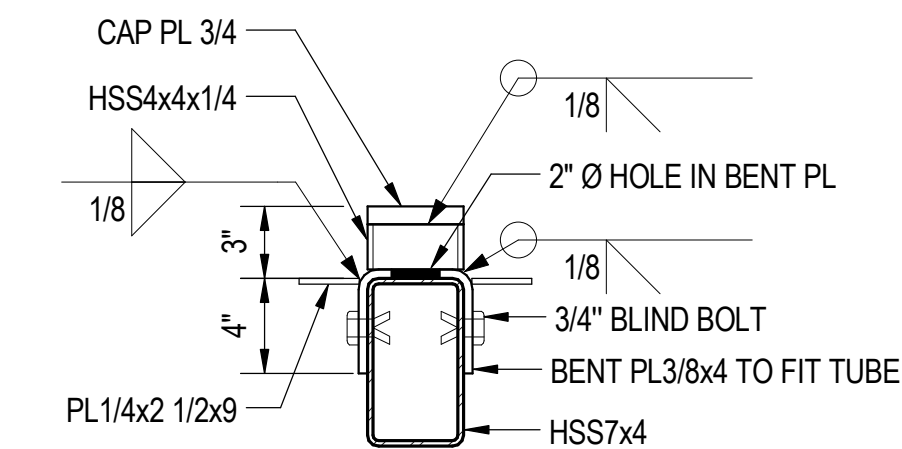
**7** ILLUMINATED SIGN CONDUIT OPENING  
S-1332 SCALE: 1 1/2" = 1'-0"

COLUMNS WITH SIGN CONDUIT OPENING

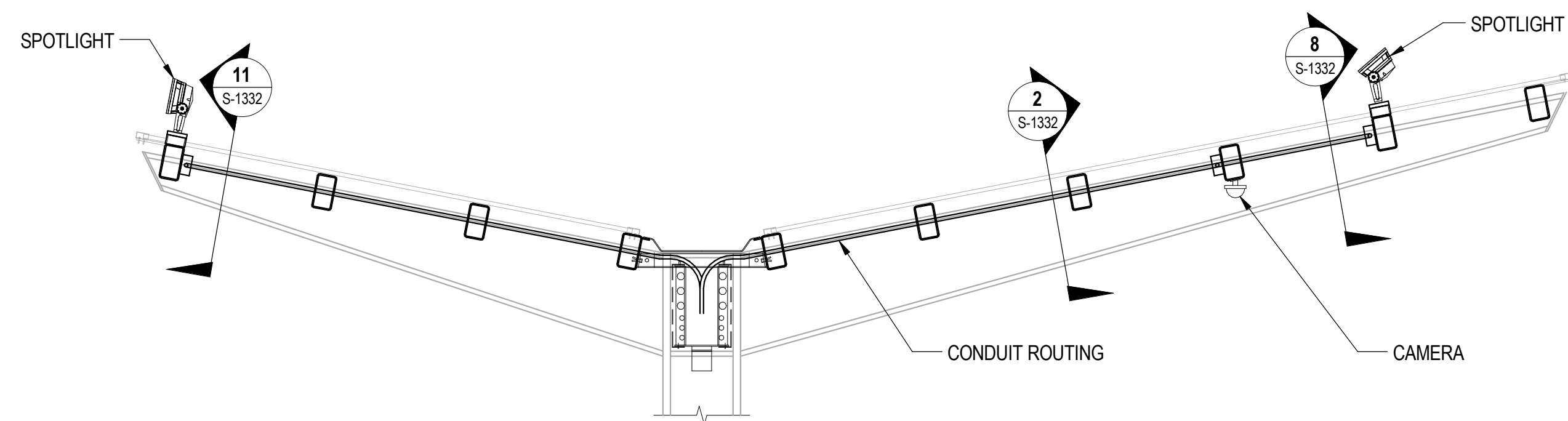
SBB-4	SBB-6	95-14	95-15
SBB-1	H'-2	95-11	H'-2
A'-4	H'-5	A"-3	H'-6
A'-7	H'-8	A"-8	H'-9
A'-10	95-8	A"-12	H'-12
95-4	95-5	A"-15	NBB-9
		NBB-4	NBB-6



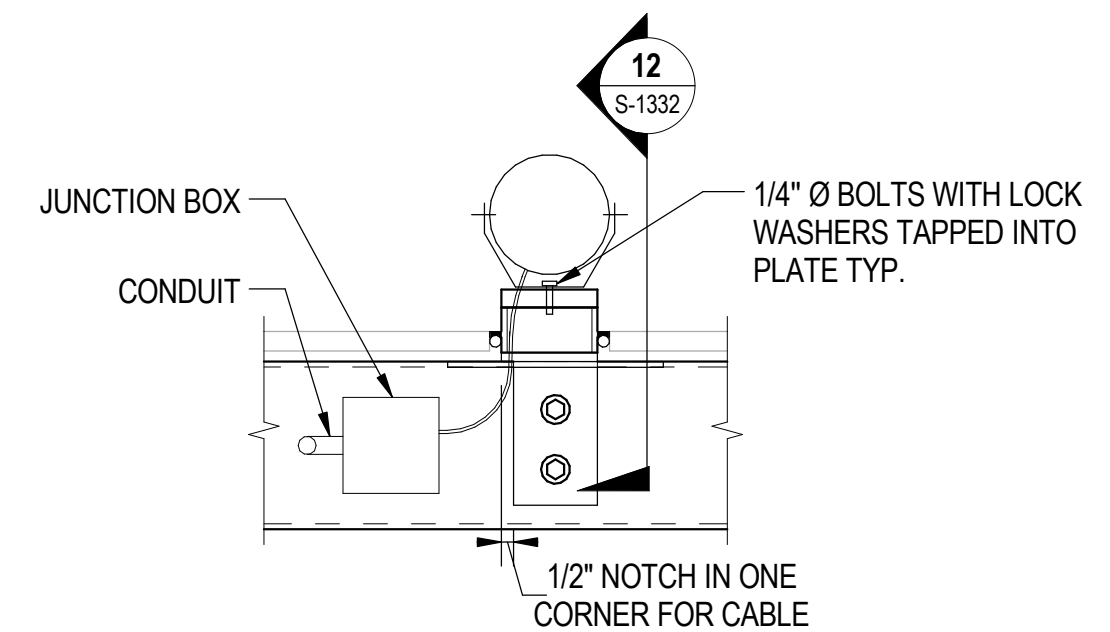
**8** SPOTLIGHT BRACKET - INTERIOR STRINGER  
S-1332 SCALE: 1 1/2" = 1'-0"



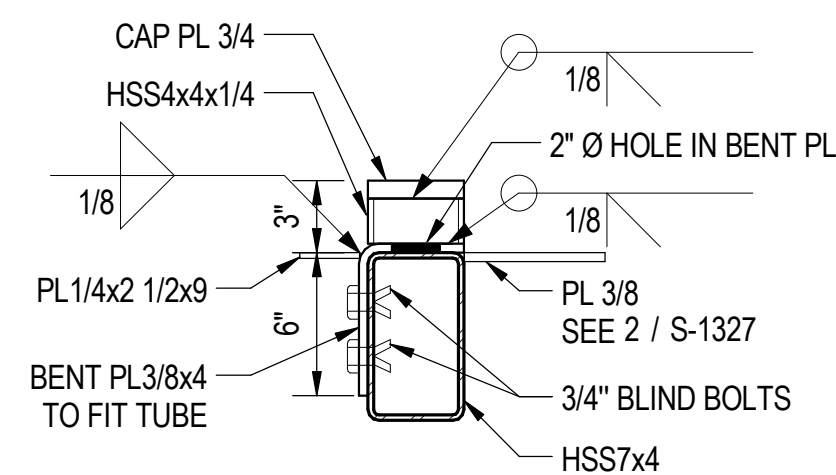
**9** SECTION  
S-1332 SCALE: 1 1/2" = 1'-0"



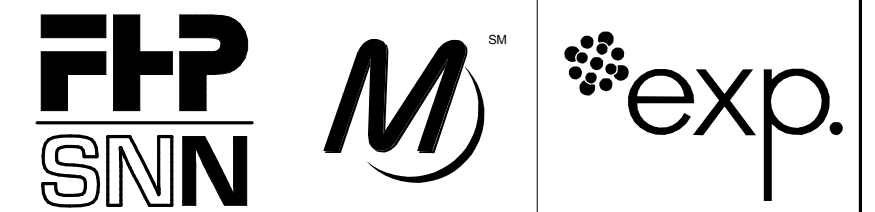
**10** SPOTLIGHT AND CAMERA CONDUIT ROUTING  
S-1332 SCALE: 1/2" = 1'-0"



**11** SPOTLIGHT BRACKET - EXTERIOR STRINGER  
S-1332 SCALE: 1 1/2" = 1'-0"



**12** SECTION  
S-1332 SCALE: 1 1/2" = 1'-0"



**SENSITIVE SECURITY INFORMATION**  
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**95th Terminal Improvement**  
15 West 95th Street (South Terminal)  
Chicago, IL 60628

IN CHARGE	J. JAKALSKI
APPROVED BY	J. JAKALSKI
CHECKED BY	R.S. NAIR
DESIGNED BY	S. CITKO
DRAWN BY	D. LALLY
PROJECT NO.	2012-0021.02
FILE NAME	D095_S-RoofCENTRAL

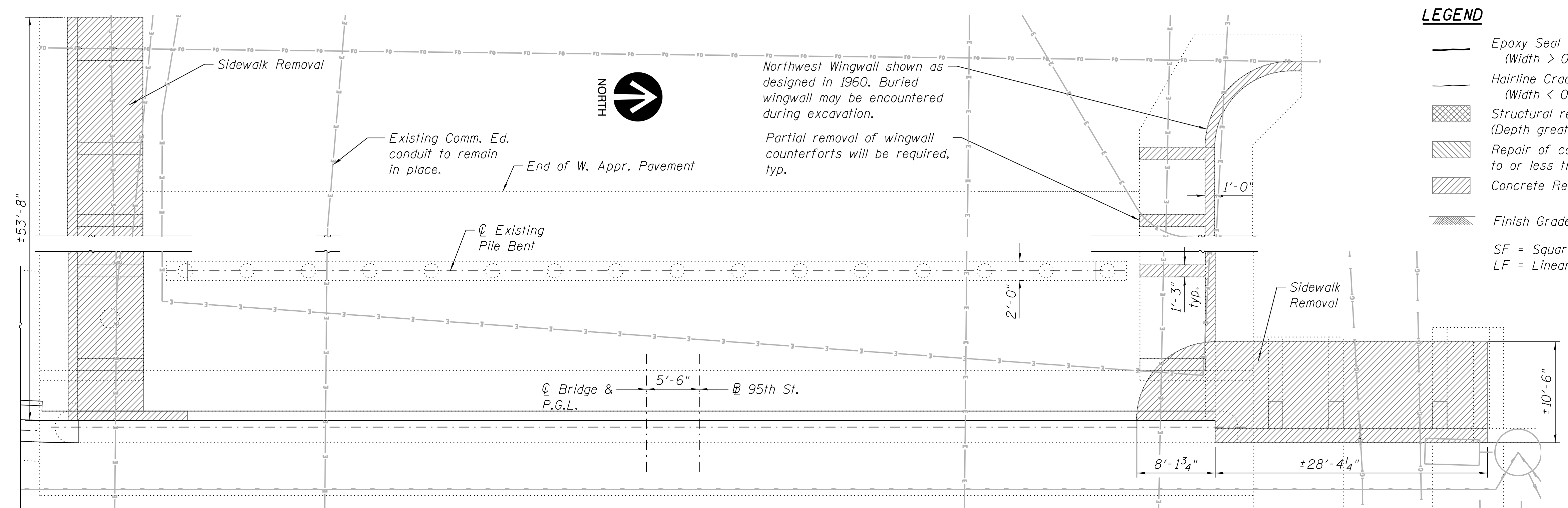
MARK	DATE	DESCRIPTION
	2/9/17	ISSUED FOR CONSTRUCTION

LOCATION IDENTIFIER: **DR095**

**BUS CANOPY DETAILS - 6**

**S-1332**

vscarpa

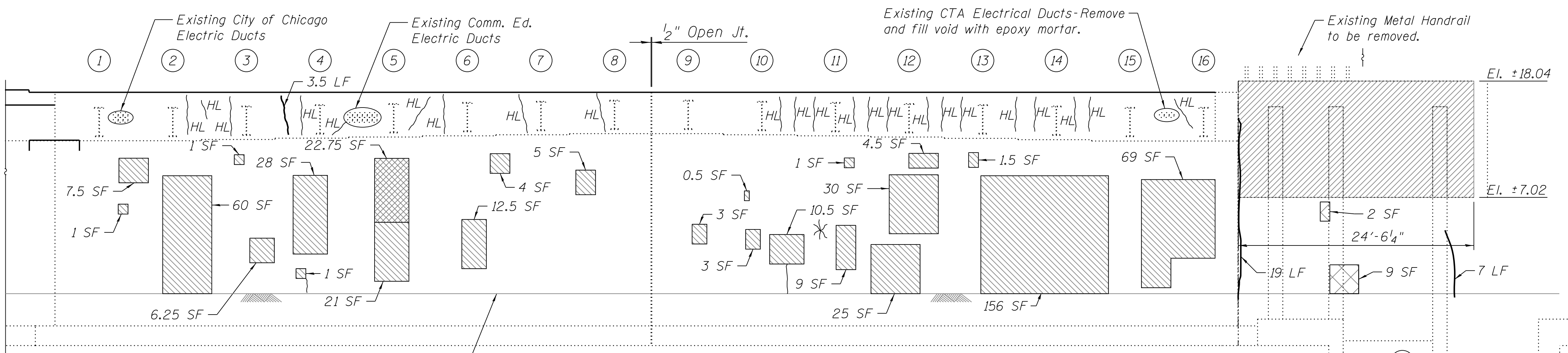


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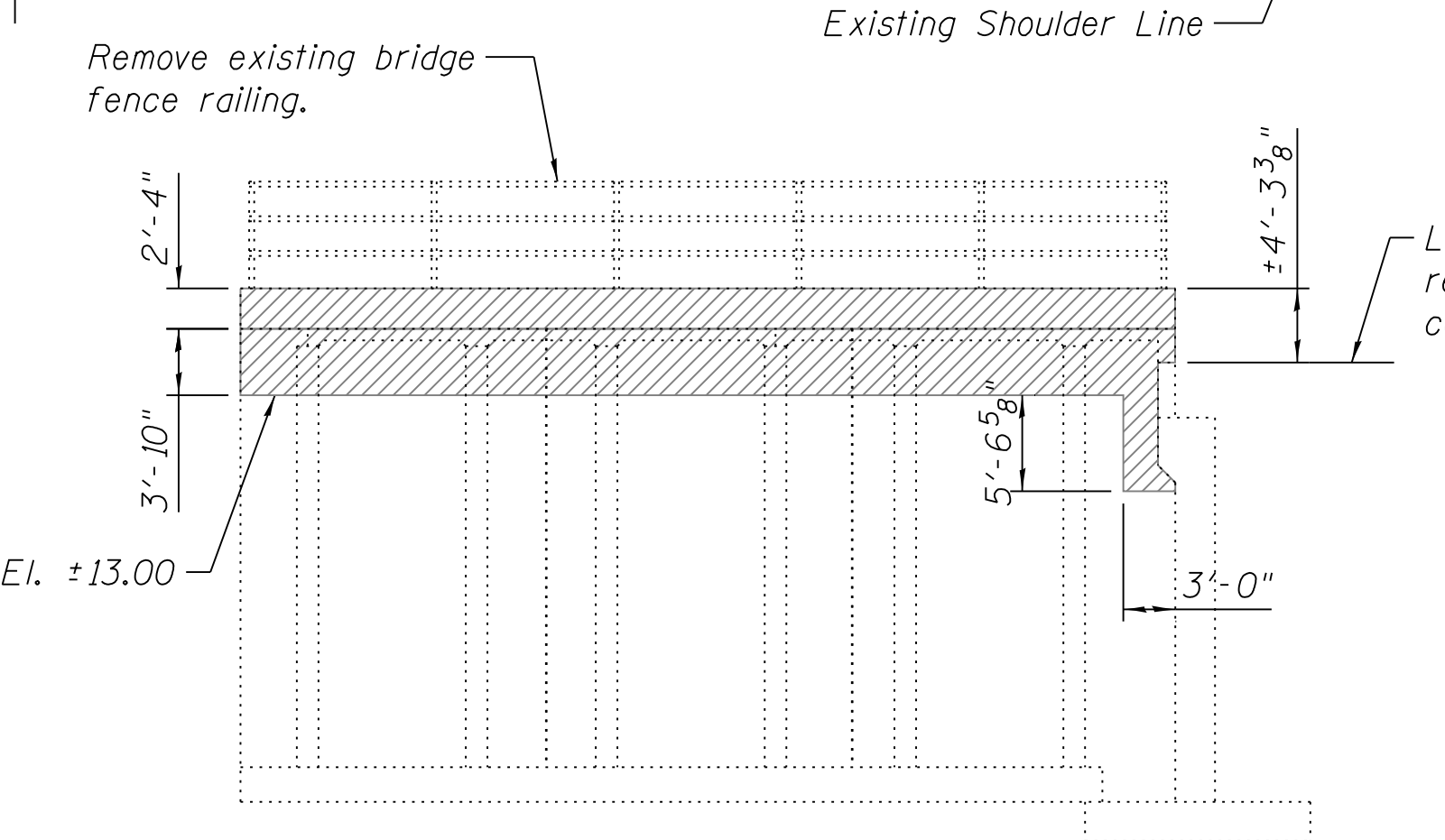
- Epoxy Seal (Width > 0.06")
- Hairline Crack not to be sealed (Width < 0.06")
- Structural repair of concrete (Depth greater than 5 inches)
- Repair of concrete (Depth equal to or less than 5 inches)
- Concrete Removal
- Finish Grade

SF = Square Foot  
LF = Linear Foot

**PLAN**



**ELEVATION**  
(Looking West)



**ELEVATION - SOUTHWEST WINGWALL**  
(Looking North)

Deteriorated areas on the wingwall are shown for information only.

**Note:**

1. For additional information on existing utilities, see Utility Plans.
2. Areas of repair and lengths of sealant are approximate. Contractor shall verify areas and include a 10 percent increase in all quantities to allow for variations in plan areas against actual areas.
3. Coordinate the replacement of manhole with traffic signal drawings. See 95th-S-117 for detail.



**CHICAGO TRANSIT AUTHORITY ENGINEERING**

**SENSITIVE SECURITY INFORMATION**

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**95th Terminal Improvement**  
15 West 95th Street (South Terminal)  
Chicago, IL 60628

<b>IN CHARGE</b>	S. KADERBEK
<b>APPROVED BY</b>	D.ZROKA
<b>CHECKED BY</b>	P.MAID
<b>DESIGNED BY</b>	L.SOMMER
<b>DRAWN BY</b>	T.SLEDGE
<b>PROJECT NO.</b>	2012-0021
<b>FILE NAME</b>	File Name

MARK	DATE	DESCRIPTION
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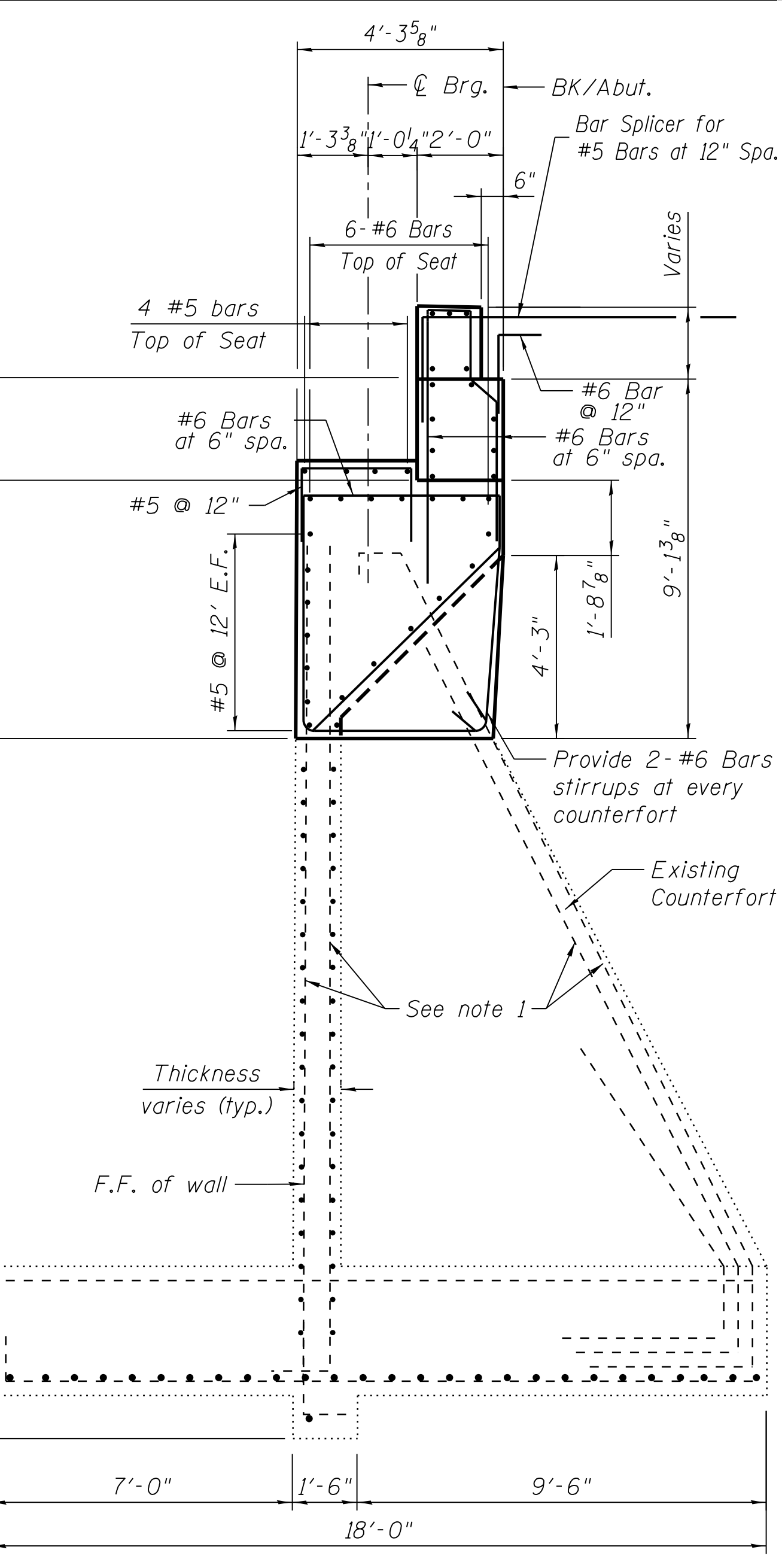
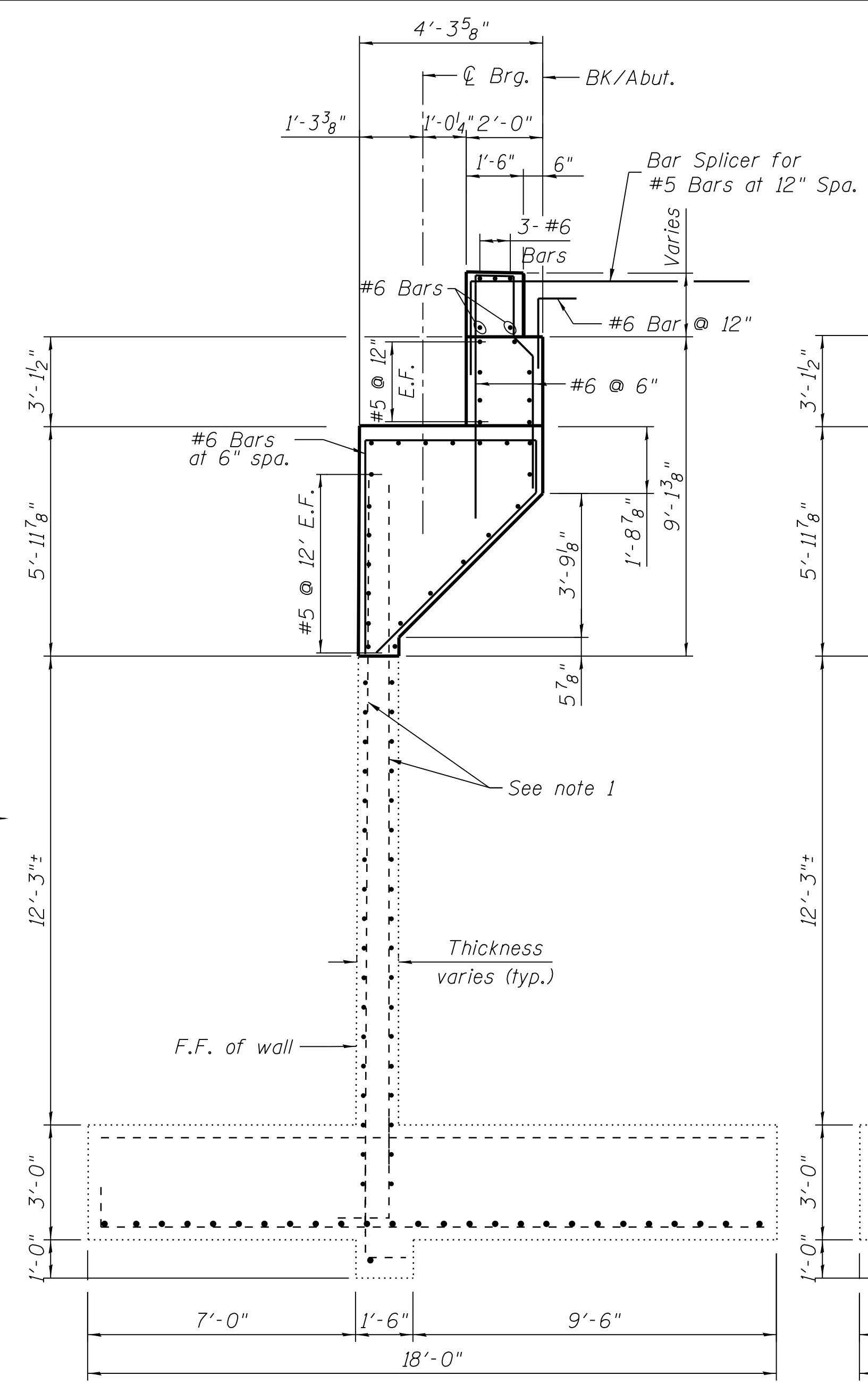
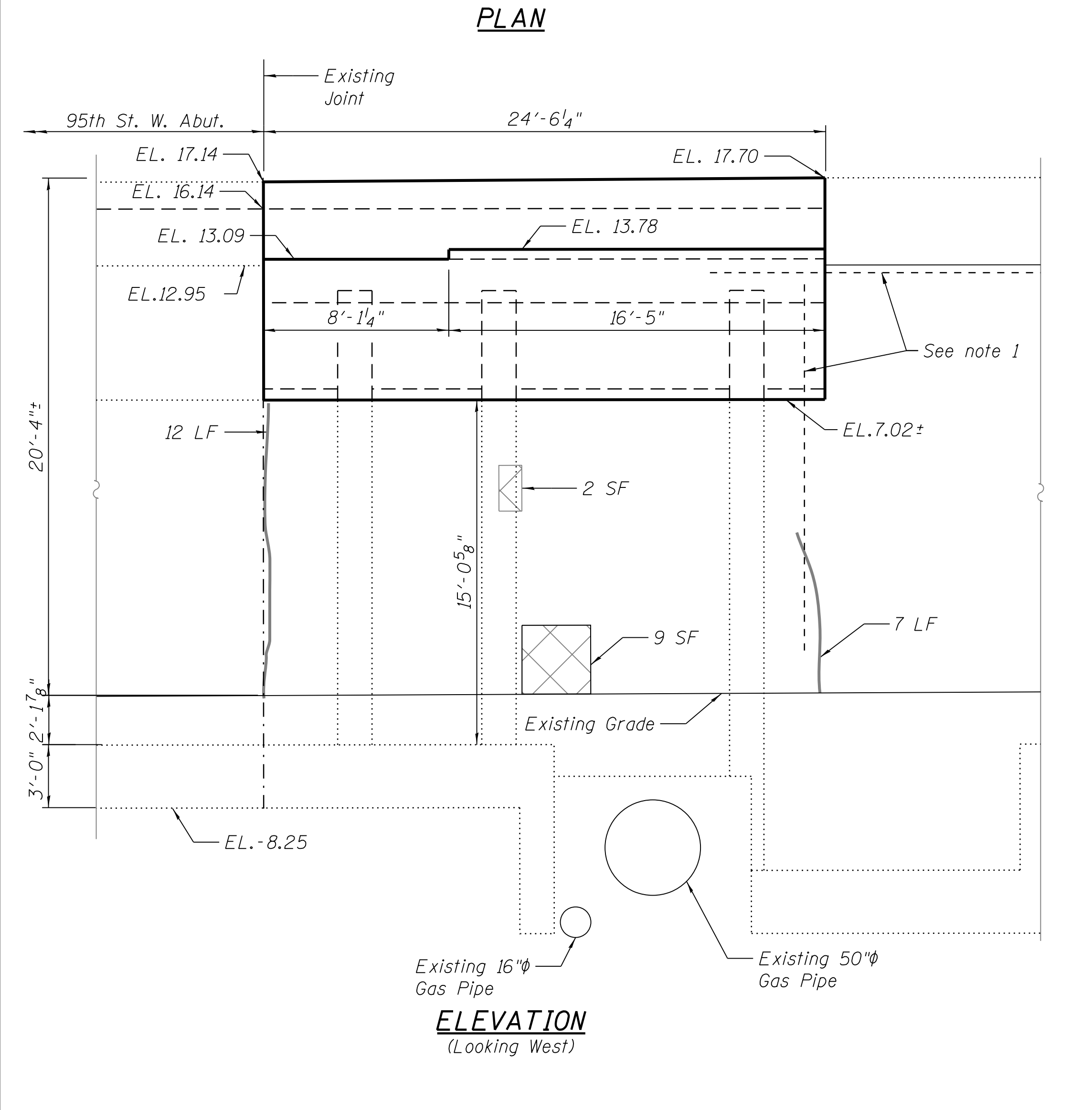
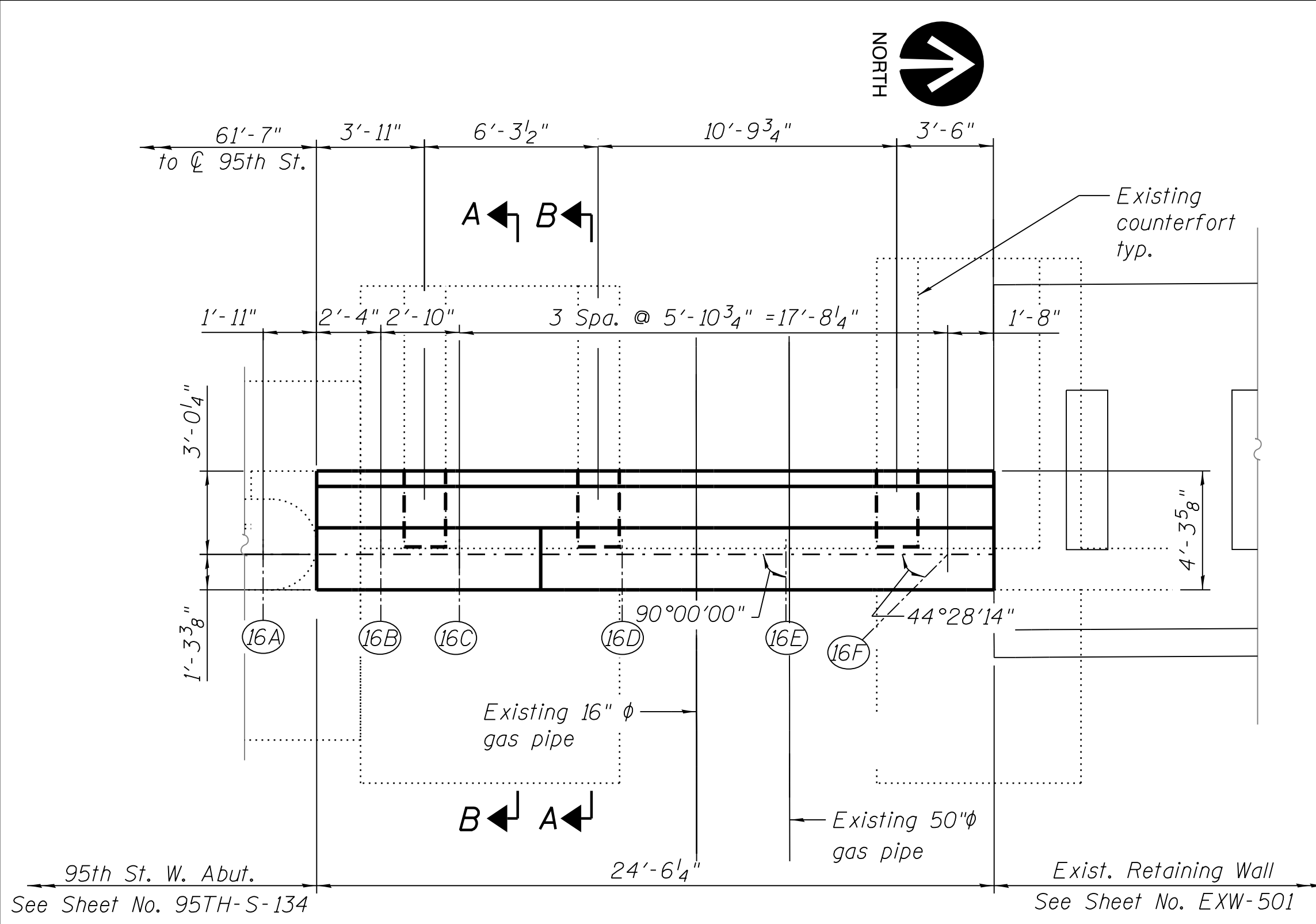
**LOCATION IDENTIFIER: DR095**

**95TH ST. BRIDGE**  
**WEST ABUTMENT REPAIR DETAILS**  
SN 016-0437

**95TH-S-134**

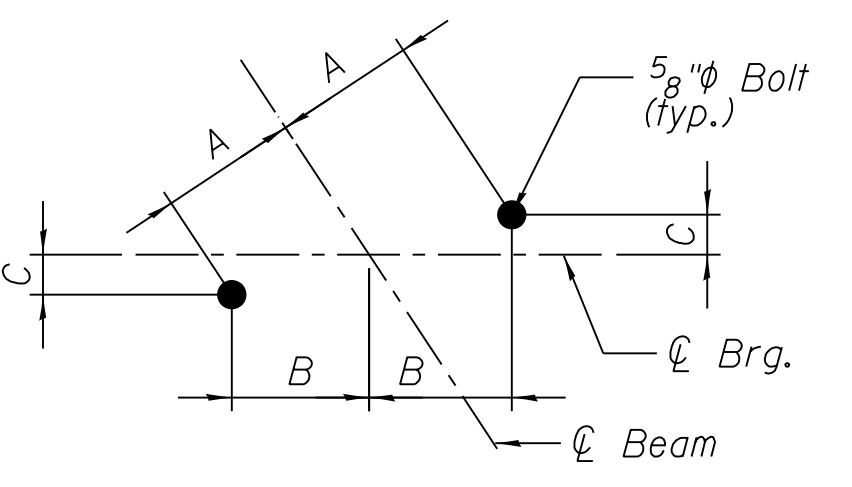
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vscarpa

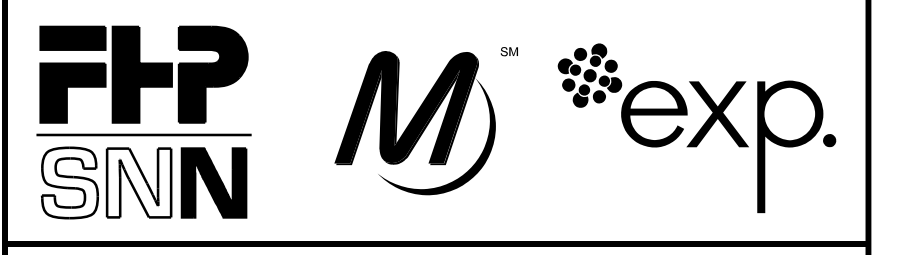


- LEGEND**
- Epoxy seal (Width > 0.06")
  - Hairline Crack not to be sealed (Width < 0.06")
  - Structural repair of concrete (Depth greater than 5 inches)
  - Repair of concrete (Depth equal to or less than 5 inches)
  - Finish Grade
  - SF = Square Foot
  - LF = Linear Foot

- Notes:**
1. Existing reinforcement shall be cleaned and incorporated into the new construction.
  2. For Concrete Removal, see Sheet No. 95TH-S-134.
  3. For backfill details, see Sheet 95TH-S-140A.
  4. Areas of repair and lengths of sealant are approximate. Contractor shall verify areas and include a 10 percent increase in all quantities to allow for variations in plan areas against actual areas.



Beam	A	B	C
16A, 16B	10 7/8"	10 7/8"	0
16C	8 7/8"	8 7/8"	0
16D, 16E	7 7/8"	7 7/8"	0
16F	8 7/8"	6 5/16"	6 3/16"



**CHICAGO TRANSIT AUTHORITY ENGINEERING**

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**95th Terminal Improvement**  
15 West 95th Street (South Terminal)  
Chicago, IL 60628

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<b>APPROVED BY</b>	D.ZROKA
<b>CHECKED BY</b>	P.MAID
<b>DESIGNED BY</b>	L.SOMMER
<b>DRAWN BY</b>	T.SLEDGE
<b>PROJECT NO.</b>	2012-0021
<b>FILE NAME</b>	File Name

MARK	DATE	DESCRIPTION
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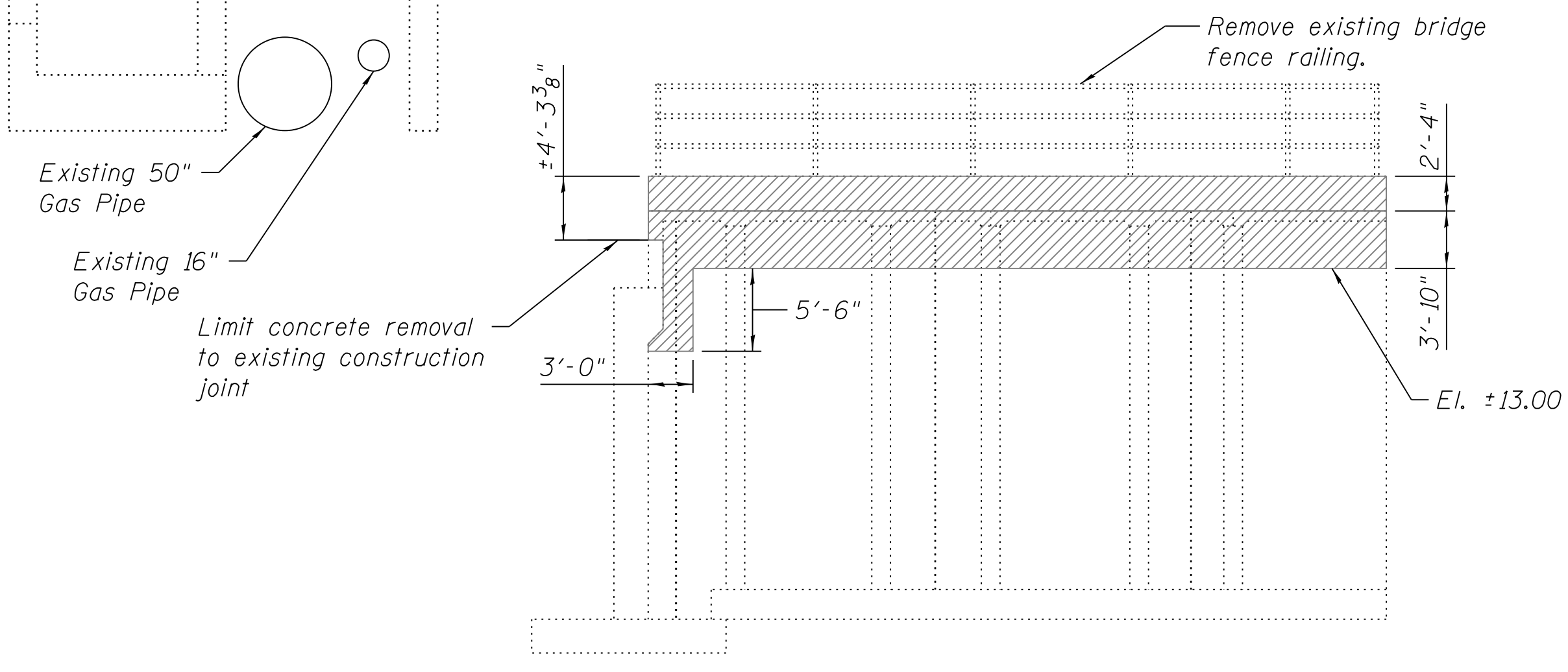
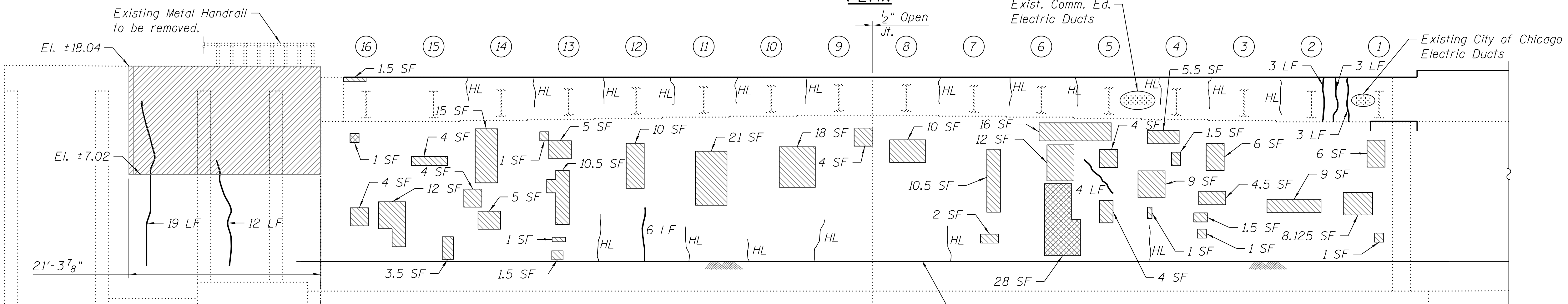
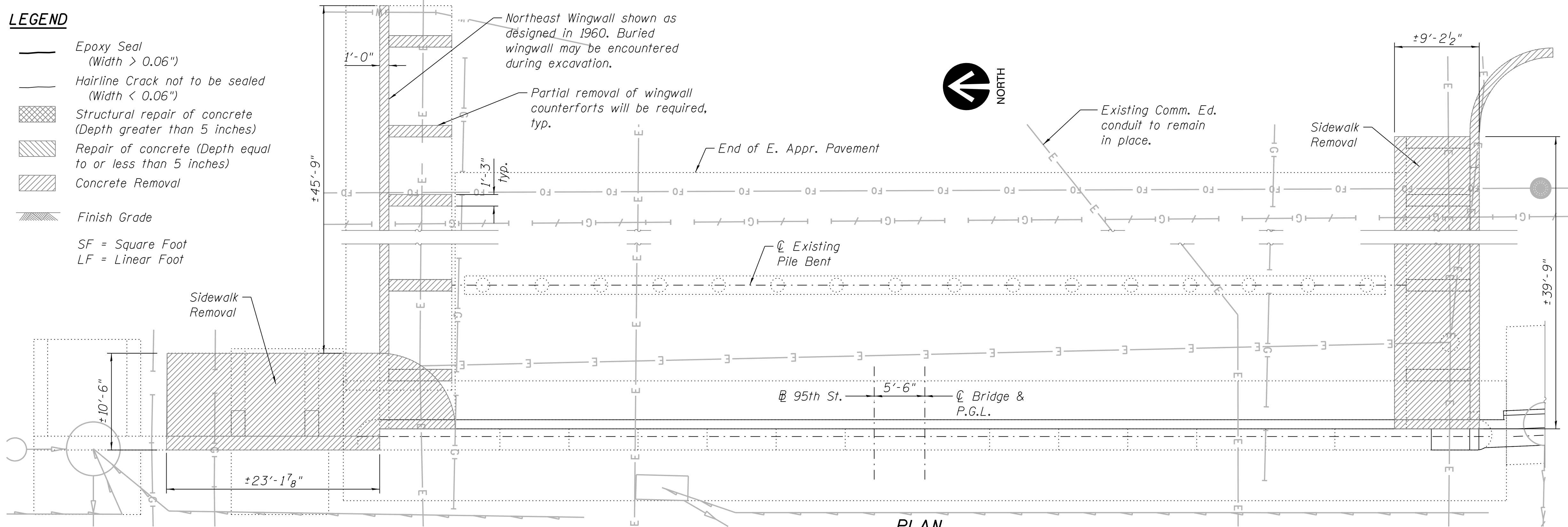
**LOCATION IDENTIFIER: DR095**  
**95TH ST. BRIDGE**  
**WEST ABUTMENT NORTH WIDENING**  
**SN 016-0437**

**95TH-S-135**

FILE NAME = 070\_DR095\_S-135.dgn  
PLOT DATE = 8/31/2016

**LEGEND**

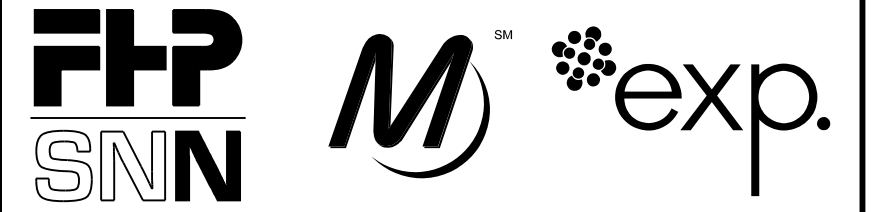
- Epoxy Seal (Width > 0.06")
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- Structural repair of concrete (Depth greater than 5 inches)
- Repair of concrete (Depth equal to or less than 5 inches)
- Concrete Removal
- Finish Grade
- SF = Square Foot
- LF = Linear Foot



**ELEVATION - SOUTHEAST WINGWALL**  
(Looking North)  
*Deteriorated areas on the wingwall are shown for information only.*

**Notes:**

1. For additional information on existing utilities, see Utility Plans.
2. Areas of repair and lengths of sealant are approximate. Contractor shall verify areas and include a 10 percent increase in all quantities to allow for variations in plan areas against actual areas.
3. Coordinate the replacement of manhole with traffic signal drawings. See 95th-S-117 for detail.



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**95th Terminal Improvement**  
15 West 95th Street (South Terminal)  
Chicago, IL 60628

<b>IN CHARGE</b>	S. KADERBEK
<b>APPROVED BY</b>	D.ZROKA
<b>CHECKED BY</b>	P.MAID
<b>DESIGNED BY</b>	L.SOMMER
<b>DRAWN BY</b>	T.SLEDGE
<b>PROJECT NO.</b>	2012-0021
<b>FILE NAME</b>	File Name

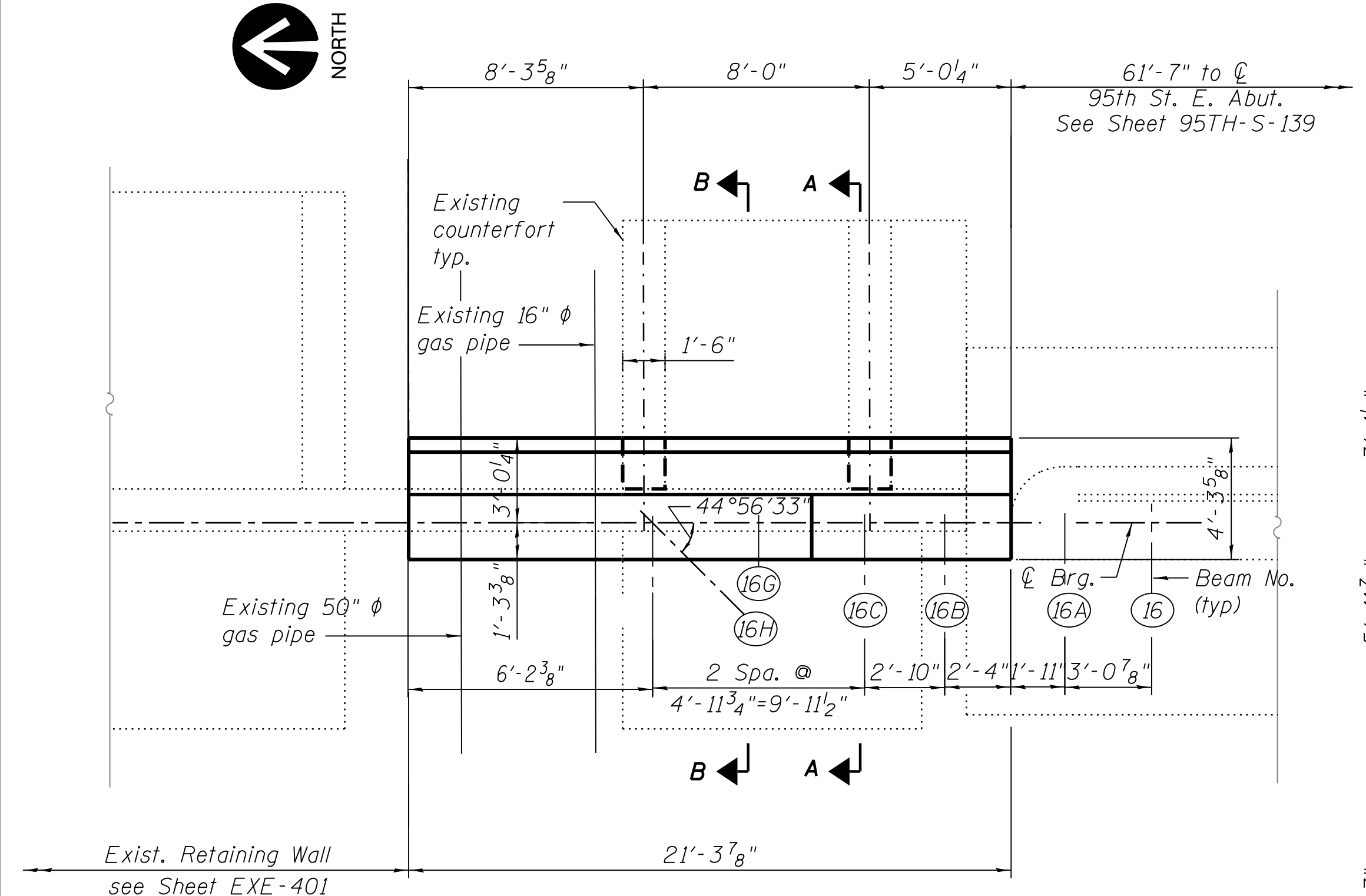
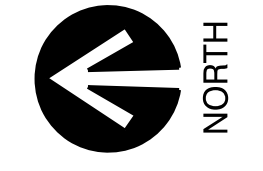
<b>MARK</b>	<b>DATE</b>	<b>DESCRIPTION</b>
	2/9/17	ISSUED FOR CONSTRUCTION

**LOCATION IDENTIFIER: DR095**

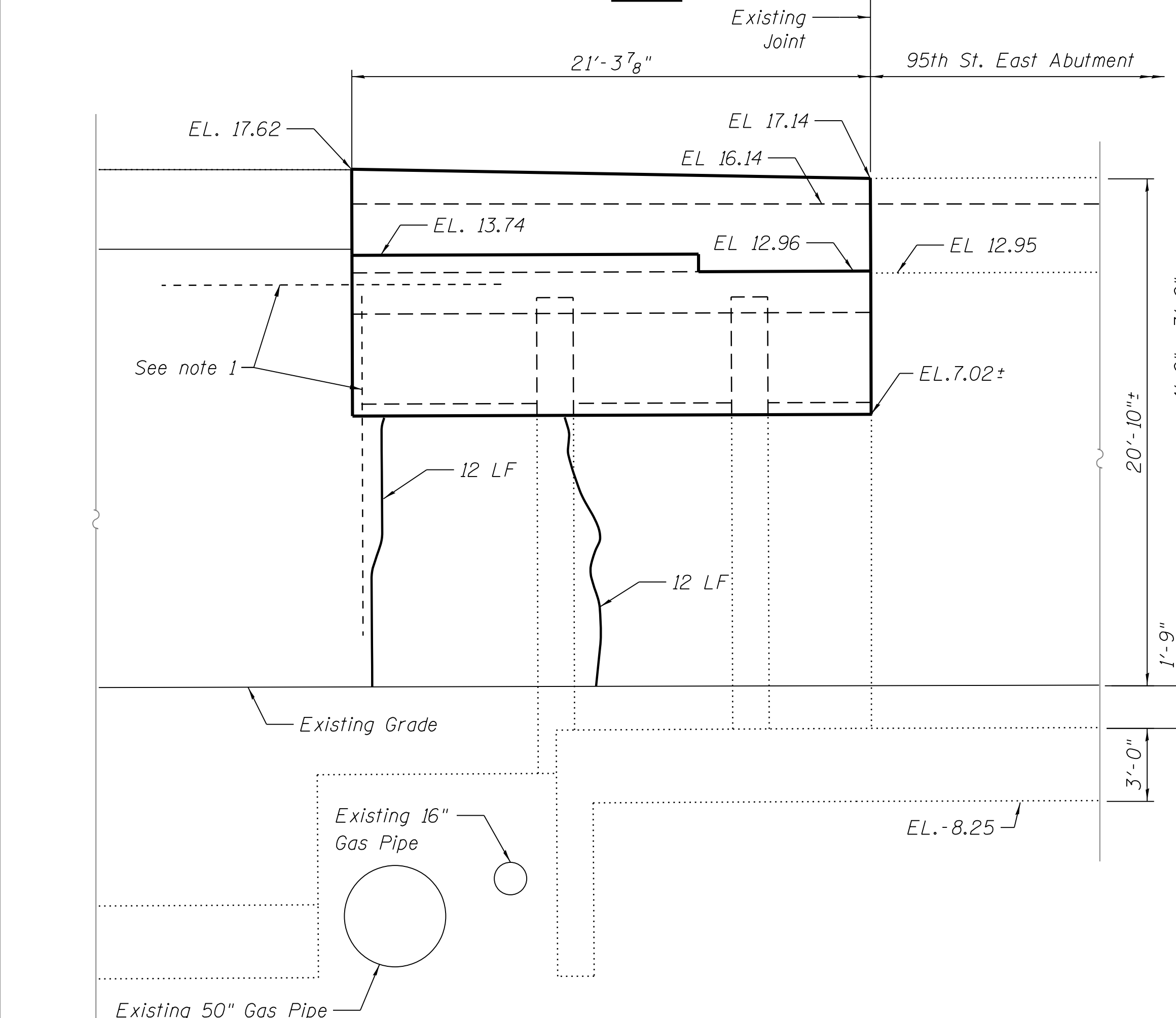
**95TH ST. BRIDGE**  
**EAST ABUTMENT REPAIR DETAILS**  
SN 016-0437

**95TH-S-139**

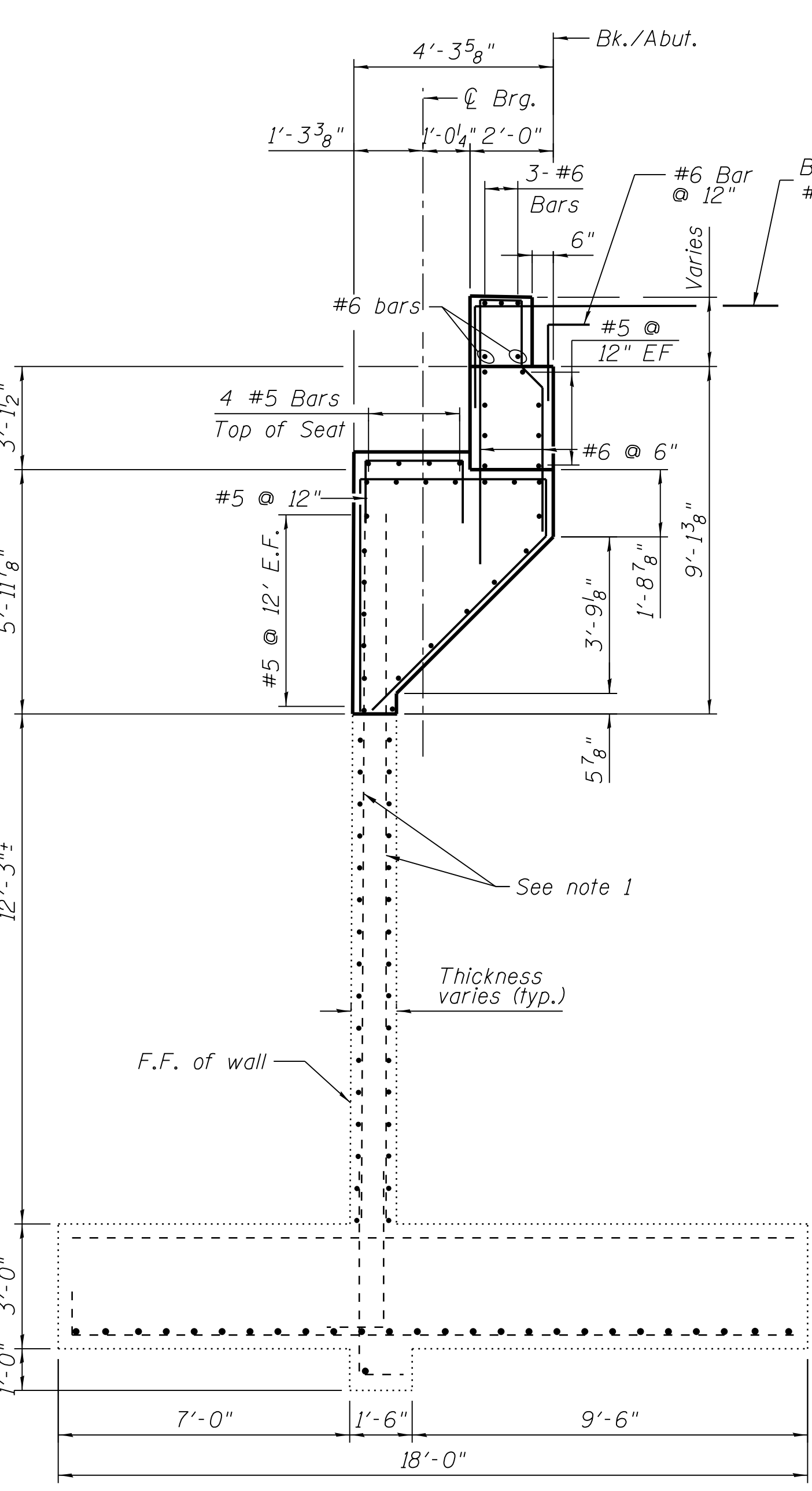
vscarpa



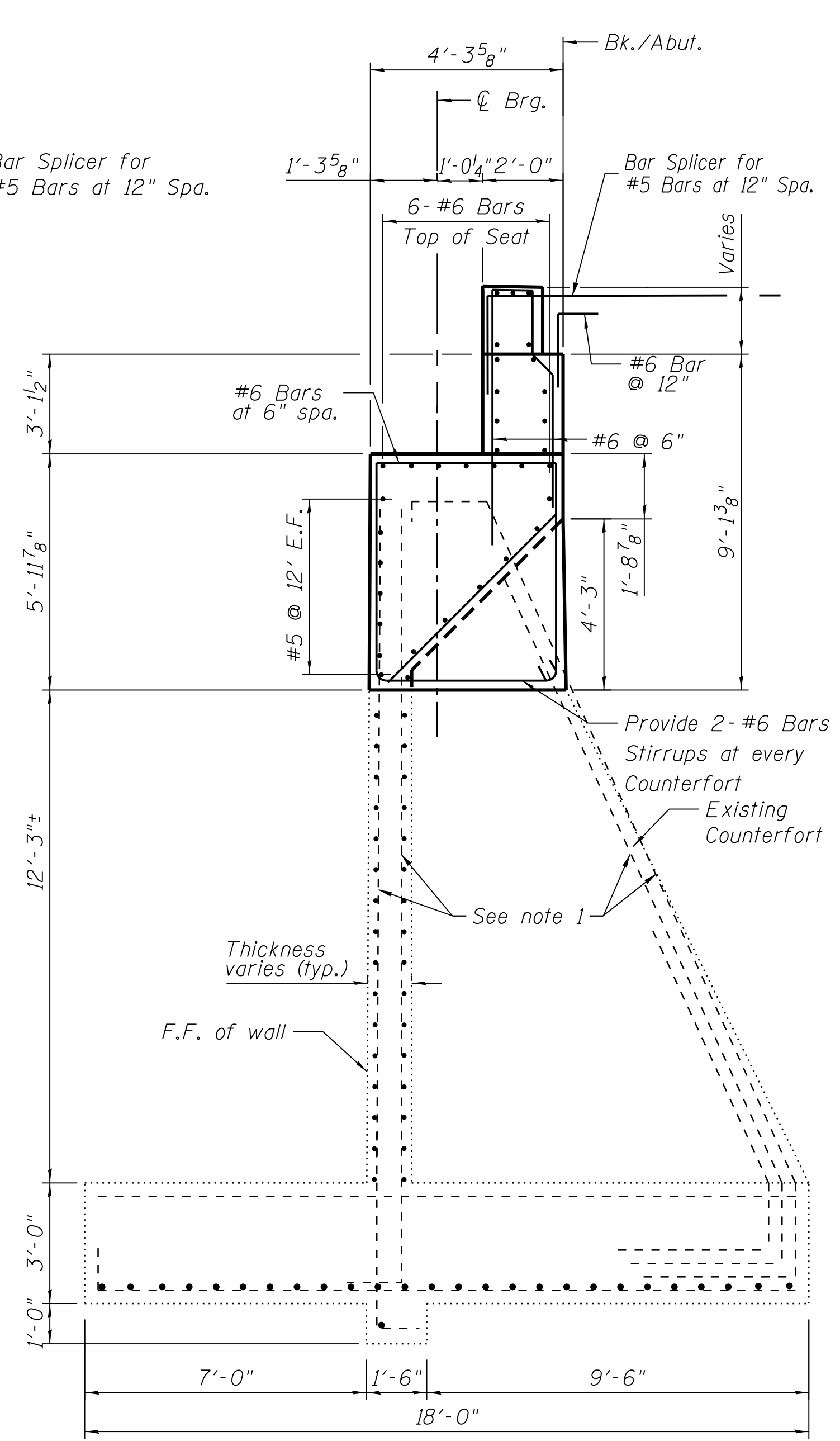
**PLAN**



**ELEVATION**  
(Looking East)



**SECTION A-A**  
(Looking North)

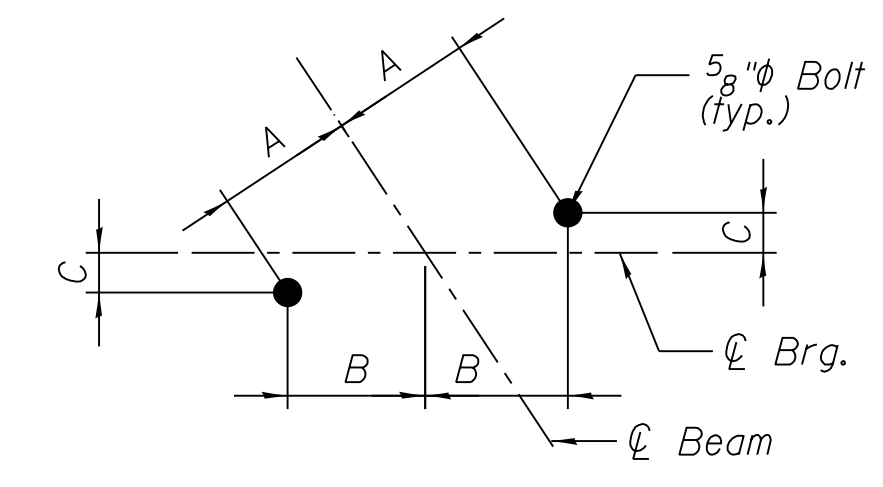


**SECTION B-B**  
(Looking North)

**LEGEND**

- Epoxy seal (Width > 0.06")
- Hairline Crack not to be sealed (Width < 0.06")
- Structural repair of concrete (Depth greater than 5 inches)
- Repair of concrete (Depth equal to or less than 5 inches)
- Finish Grade
- SF = Square Foot
- LF = Linear Foot

- Notes:
- Existing reinforcement shall be cleaned and incorporated into the new construction.
  - For Concrete Removal, see Sheet No. 95TH-S-139.
  - For backfill details, see Sheet 95TH-S-140A.
  - Areas of repair and lengths of sealant are approximate. Contractor shall verify areas and include a 10 percent increase in all quantities to allow for variations in plan areas against actual areas.



**ANCHOR BOLT DETAIL**

Beam	A	B	C
16A, 16B	10 7/8"	10 7/8"	0
16C	8 7/8"	8 7/8"	0
16G	7 7/8"	7 7/8"	0
16H	8 7/8"	6 5/16"	6 1/4"



**SENSITIVE SECURITY INFORMATION**  
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**95th Terminal Improvement**  
15 West 95th Street (South Terminal)  
Chicago, IL 60628

IN CHARGE	S. KADERBEK
APPROVED BY	D.ZROKA
CHECKED BY	P.MAID
DESIGNED BY	L.SOMMER
DRAWN BY	T.SLEDGE
PROJECT NO.	2012-0021
FILE NAME	File Name

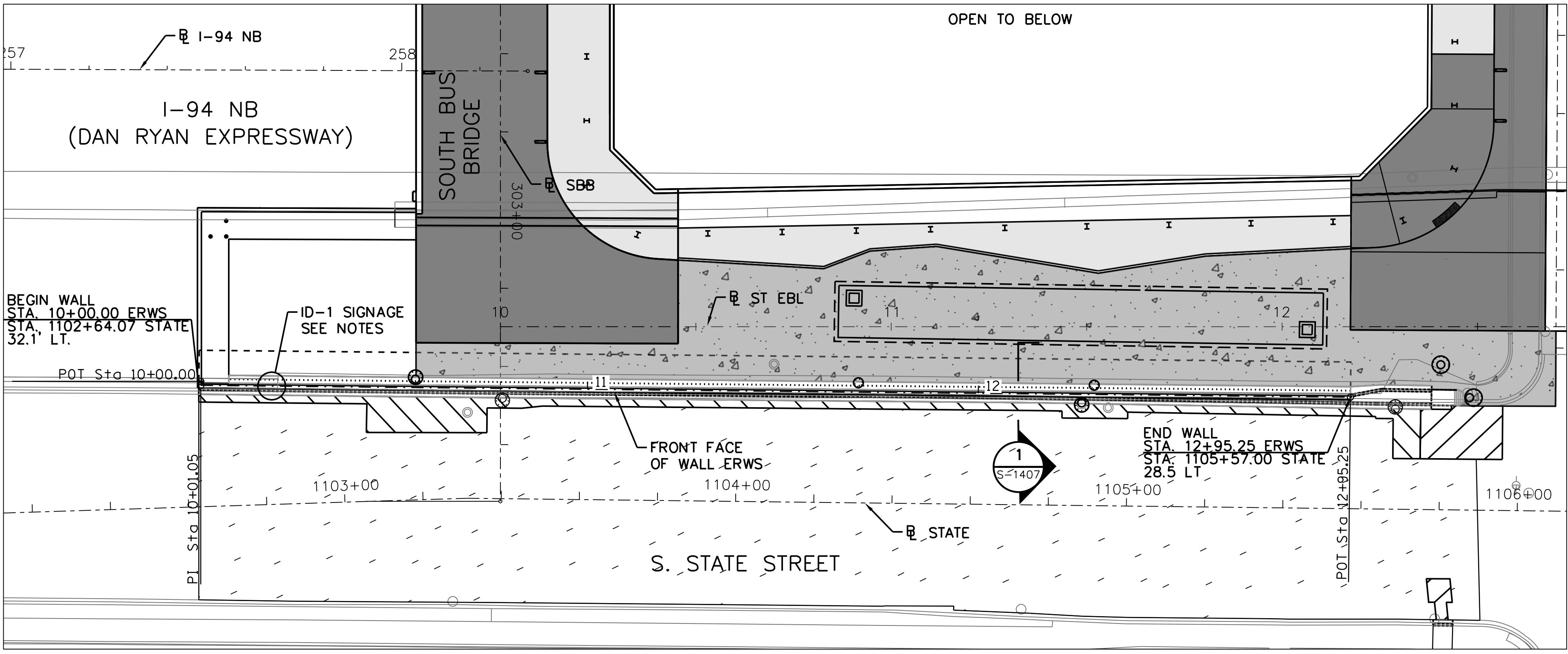
MARK	DATE	DESCRIPTION
	2/9/17	ISSUED FOR CONSTRUCTION

LOCATION IDENTIFIER: **DR095**  
**95TH ST. BRIDGE**  
**EAST ABUTMENT NORTH WIDENING**  
SN 016-0437

**95TH-S-140**

FILE NAME = 070\_DR095\_S-140.dgn  
PLOT DATE = 8/31/2016

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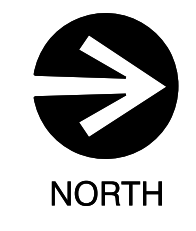
**NOTES:**

- FOR ADDITIONAL INFORMATION:
- EXISTING & PROPOSED DRAINAGE, SEE SHEETS C-136 TO C-145
- RETAINING WALL & BARRIER DETAILS, SEE SHEETS S-1403 & S-1407
- LANDSCAPE PLAN, SEE "L" DRAWINGS
- SEE SHEET C-137 FOR PIPE PENETRATION LOCATION.
- SEE SHEET S-1407 FOR PIPE PENETRATION DETAIL.
- SEE AG101, AS507 & S-1408 FOR ID-1 SIGNAGE DETAILS AND LOCATION.

**LEGEND:**

- PROPOSED PAVEMENT
- PEDESTRIAN WALKWAY
- ▨ CLASS C PATCH
- ▤ HMA RESURFACING
- ▩ EXISTING BRIDGE DECK REPAIR
- PROPOSED BRIDGE DECK

**1 EAST RETAINING WALL - SOUTH TERMINAL**



0 10' 20' 40'  
SCALE: 1"=20'-0"



**CHICAGO TRANSIT AUTHORITY ENGINEERING**

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**95th Terminal Improvement**  
15 West 95th Street (South Terminal)  
Chicago, IL 60628

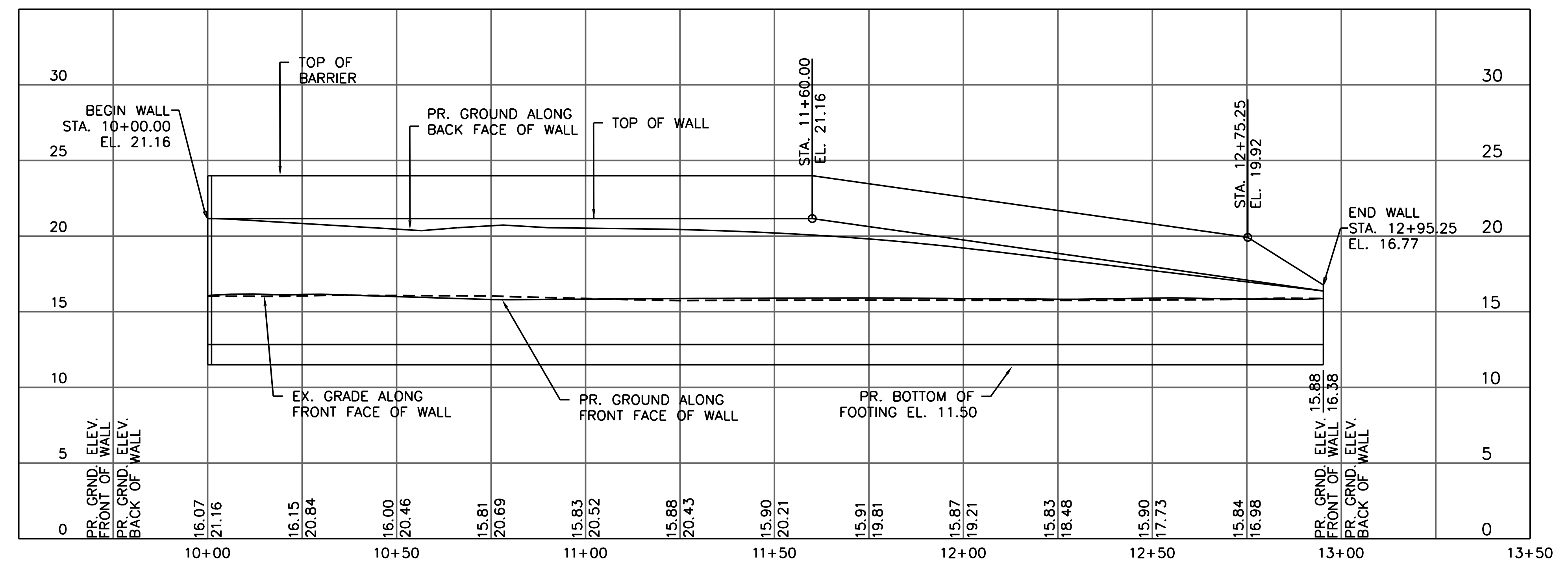
IN CHARGE	S. KADERBEK
APPROVED BY	D.ZROKA
CHECKED BY	P.MAID
DESIGNED BY	L.SOMMER
DRAWN BY	T.SLEDGE
PROJECT NO.	2012-0021
FILE NAME	File Name

MARK	DATE	DESCRIPTION
	2/9/17	ISSUED FOR CONSTRUCTION

LOCATION IDENTIFIER: **DR095**

**PLAN & PROFILE SOUTH TERMINAL EAST RETAINING WALL**

**S-1400**



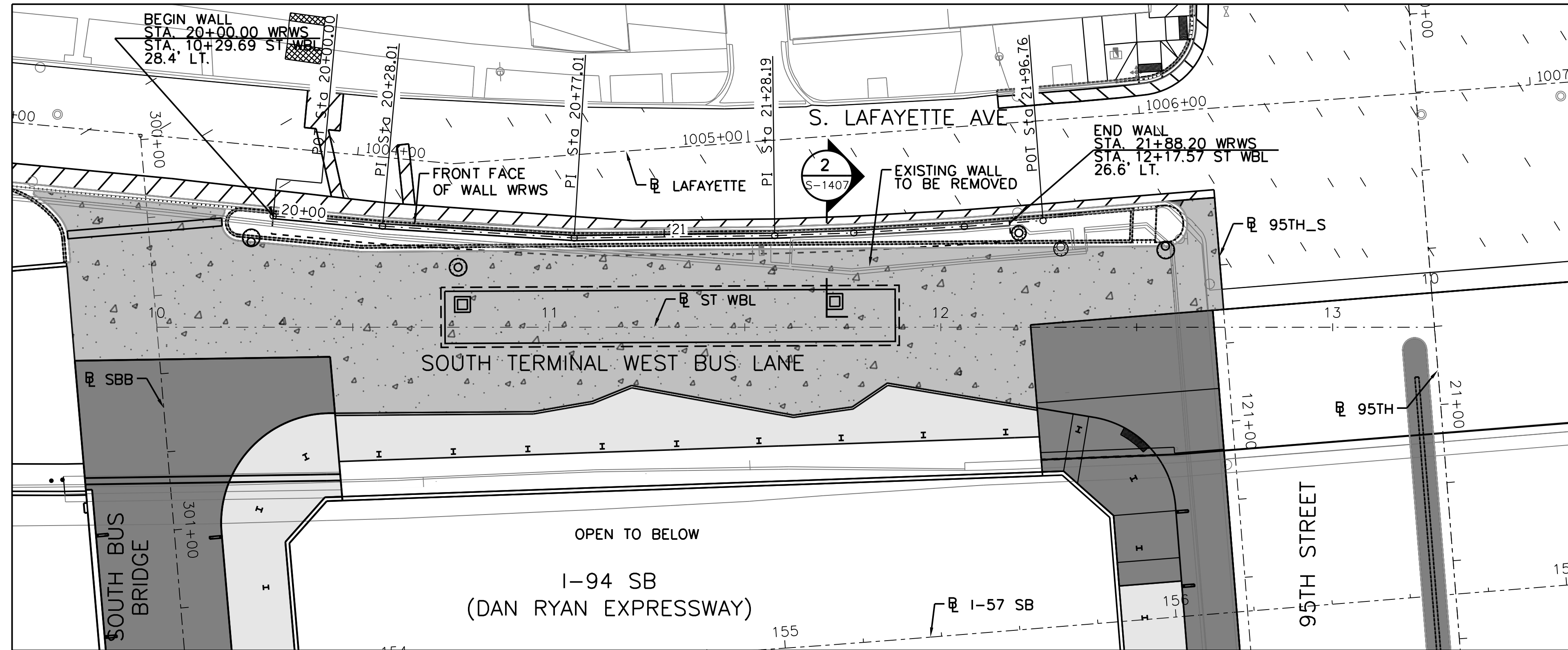
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SCALE: 1"=5'-0"

HORIZONTAL  
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SCALE: 1"=20'-0"

**2 EAST RETAINING WALL PROFILE - SOUTH TERMINAL**

FILE NAME = 070\_DR095\_S-1400.DGN  
PLOT DATE = 12/14/2016

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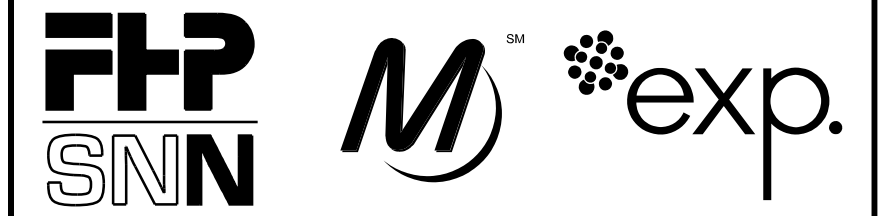
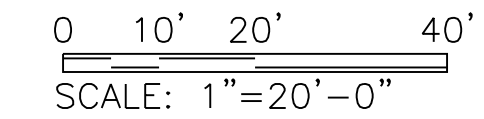
**NOTES:**

- FOR ADDITIONAL INFORMATION:
- EXISTING & PROPOSED DRAINAGE, SEE SHEETS C-136 TO C-145
- RETAINING WALL & BARRIER DETAILS, SEE SHEET S-1403 & S-1407
- FOR RETAINING WALL REMOVAL LIMITS AND DETAILS, SEE SHEETS S-1407, C-100 AND C-101
- LANDSCAPE PLAN, SEE "L" DRAWINGS
- COORDINATE INSTALLATION OF COMED DUCT AT WALL SEE ELECTRICAL PLANS

**LEGEND:**

- PROPOSED PAVEMENT
- PEDESTRIAN WALKWAY
- ▨ CLASS C PATCH
- ▤ HMA RESURFACING
- ▩ EXISTING BRIDGE DECK REPAIR
- PROPOSED BRIDGE DECK

**1**  
S-1401 WEST RETAINING WALL - SOUTH TERMINAL



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**95th Terminal Improvement**  
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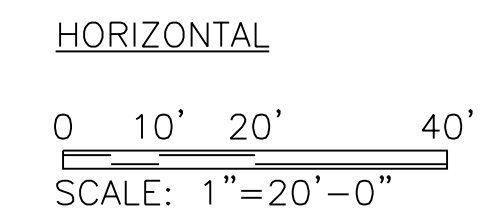
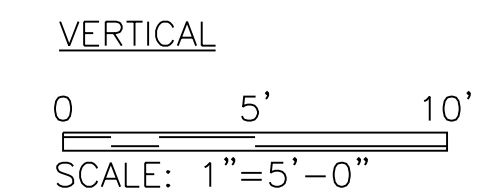
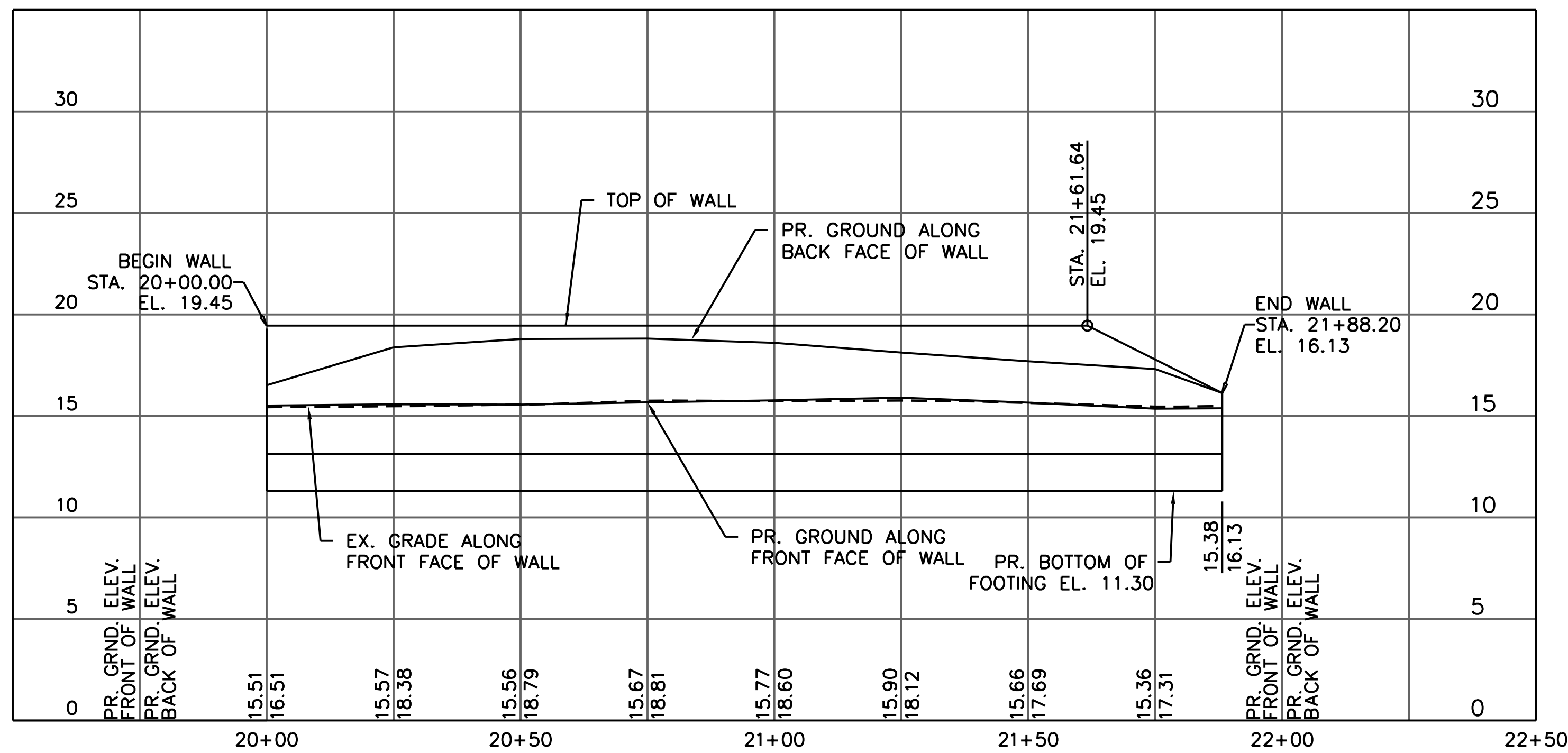
IN CHARGE	S. KADERBEK
APPROVED BY	D.ZROKA
CHECKED BY	P.MAID
DESIGNED BY	L.SOMMER
DRAWN BY	T.SLEDGE
PROJECT NO.	2012-0021
FILE NAME	File Name

MARK	DATE	DESCRIPTION
	2/9/17	ISSUED FOR CONSTRUCTION

LOCATION IDENTIFIER: **DR095**

**PLAN & PROFILE SOUTH TERMINAL WEST RETAINING WALL**

**S-1401**

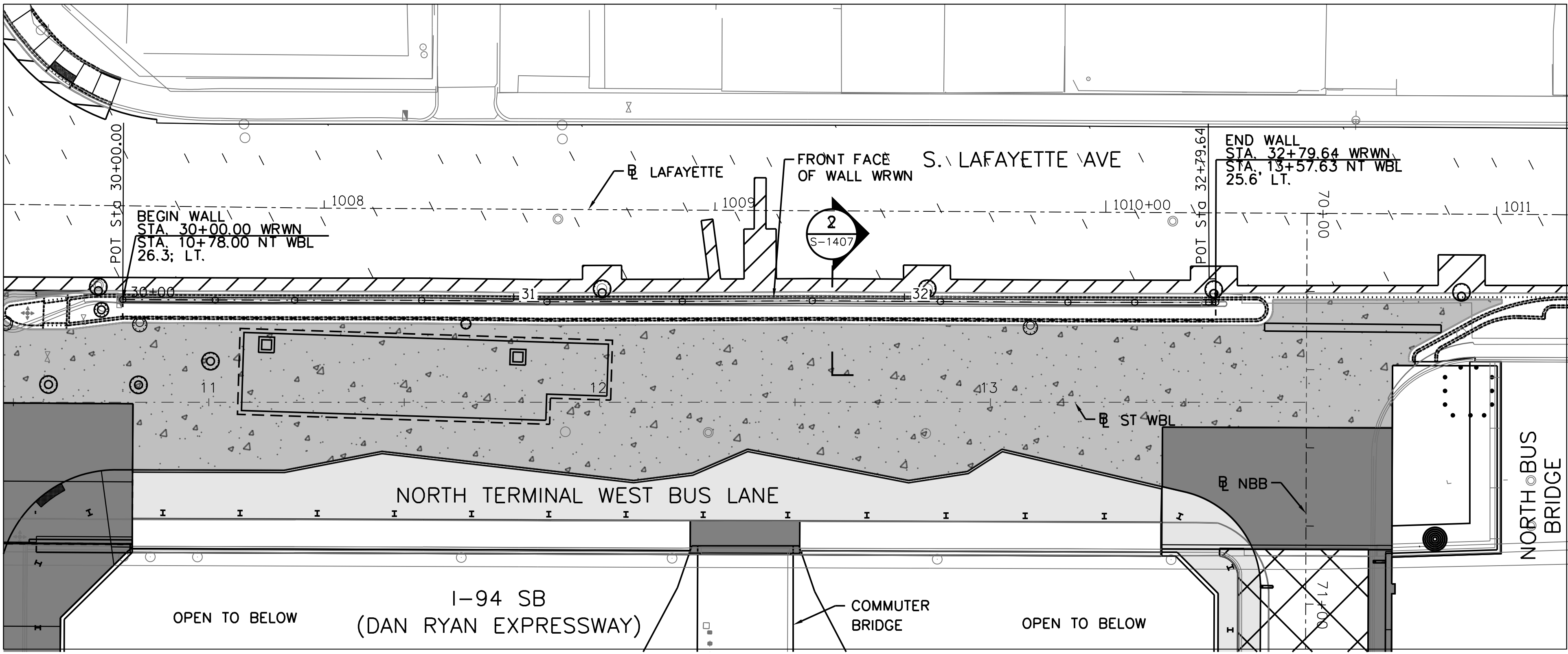


**2**  
S-1401 WEST RETAINING WALL PROFILE - SOUTH TERMINAL

FILE NAME = 070\_DR095\_S-1401.dgn  
PLOT DATE = 12/14/2016



vscarp



- NOTES:**
- FOR ADDITIONAL INFORMATION:
  - EXISTING & PROPOSED DRAINAGE, SEE SHEETS C-136 TO C-145
  - RETAINING WALL & BARRIER DETAILS, SEE SHEET S-1403A & S-1407
  - LANDSCAPE PLAN, SEE "L" DRAWINGS
  - FRONT FACE OF RETAINING WALL ALIGNED TO BACK OF CURB

- LEGEND:**
- PROPOSED PAVEMENT
  - PEDESTRIAN WALKWAY
  - CLASS C PATCH
  - HMA RESURFACING
  - EXISTING BRIDGE DECK REPAIR
  - PROPOSED BRIDGE DECK



**CHICAGO TRANSIT AUTHORITY ENGINEERING**

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**95th Terminal Improvement**  
 15 West 95th Street (South Terminal)  
 Chicago, IL 60628

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PROJECT NO.	2012-0021
FILE NAME	File Name

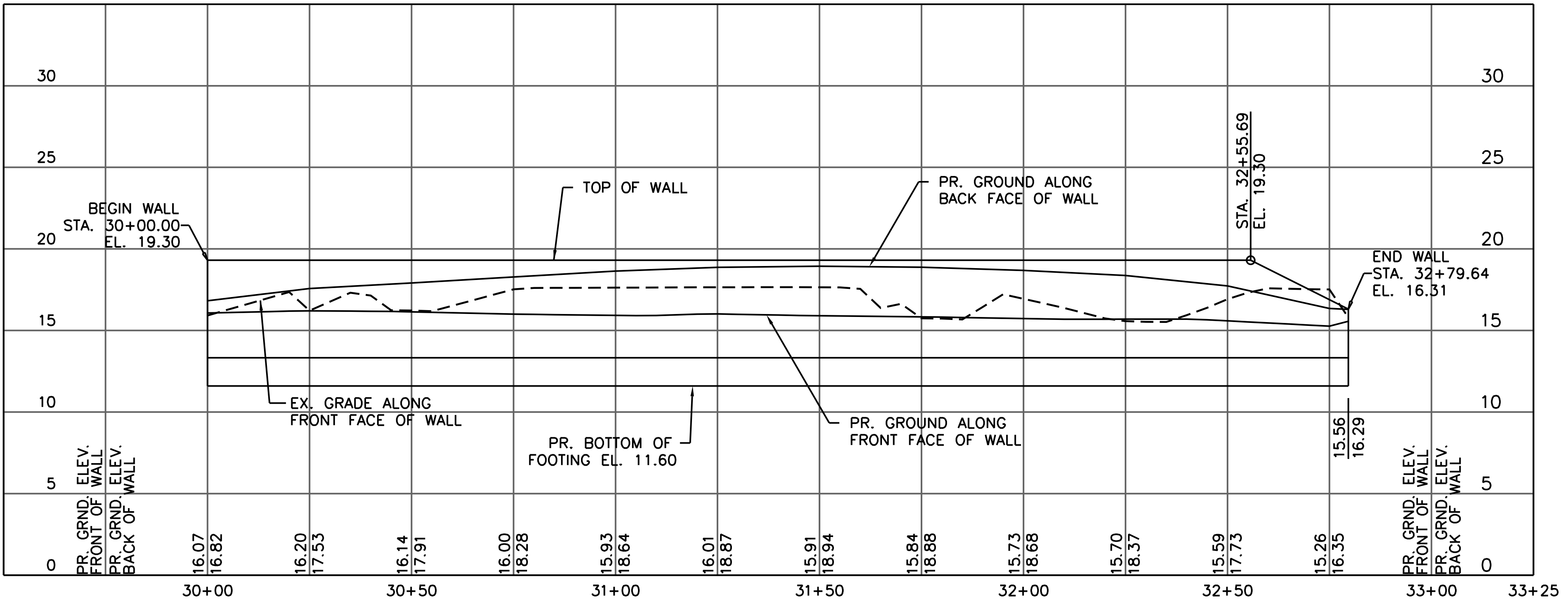
MARK	DATE	DESCRIPTION
	2/9/17	ISSUED FOR CONSTRUCTION

LOCATION IDENTIFIER: **DR095**  
 PLAN & PROFILE  
 NORTH TERMINAL  
 WEST RETAINING WALL

**S-1402**

**1 WEST RETAINING WALL - NORTH TERMINAL**  
 S-1402

0 10' 20' 40'  
 SCALE: 1"=20'-0"



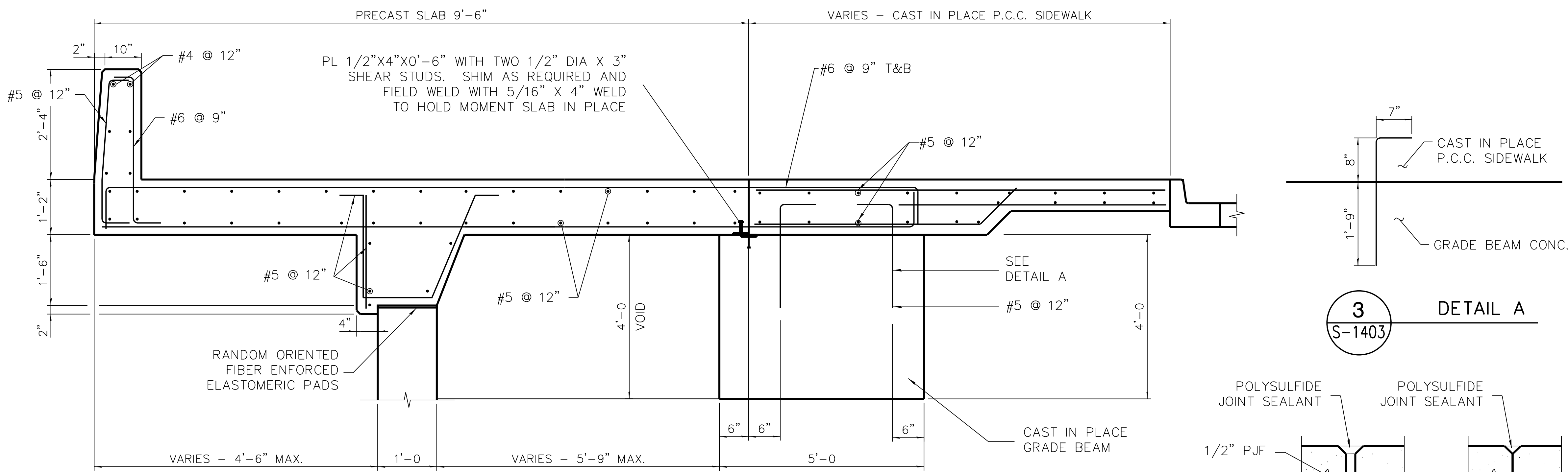
**2 WEST RETAINING WALL PROFILE - NORTH TERMINAL**  
 S-1402

VERTICAL  
 0 5' 10'  
 SCALE: 1"=5'-0"

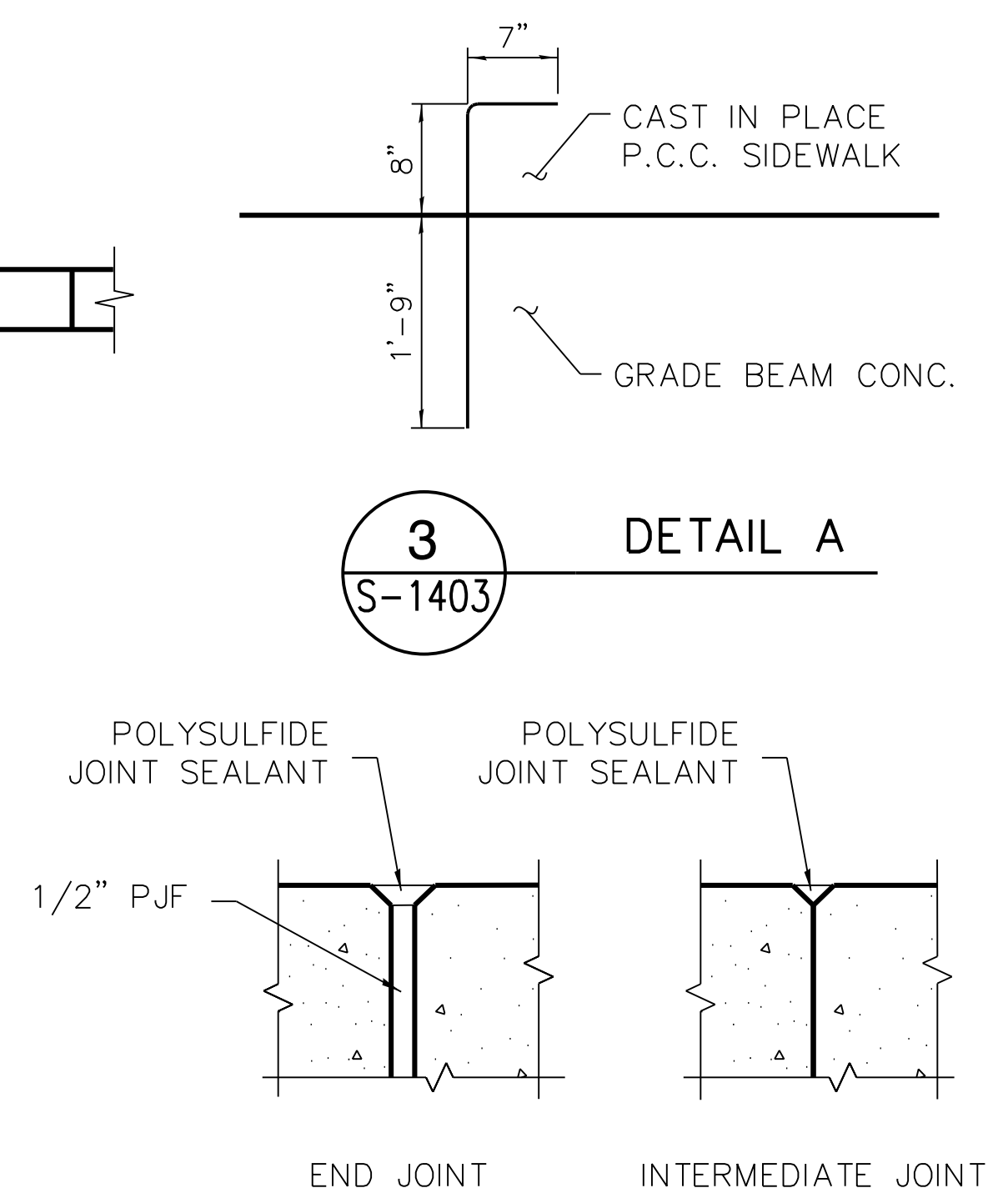
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 SCALE: 1"=20'-0"

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 PLOT DATE = 12/14/2016

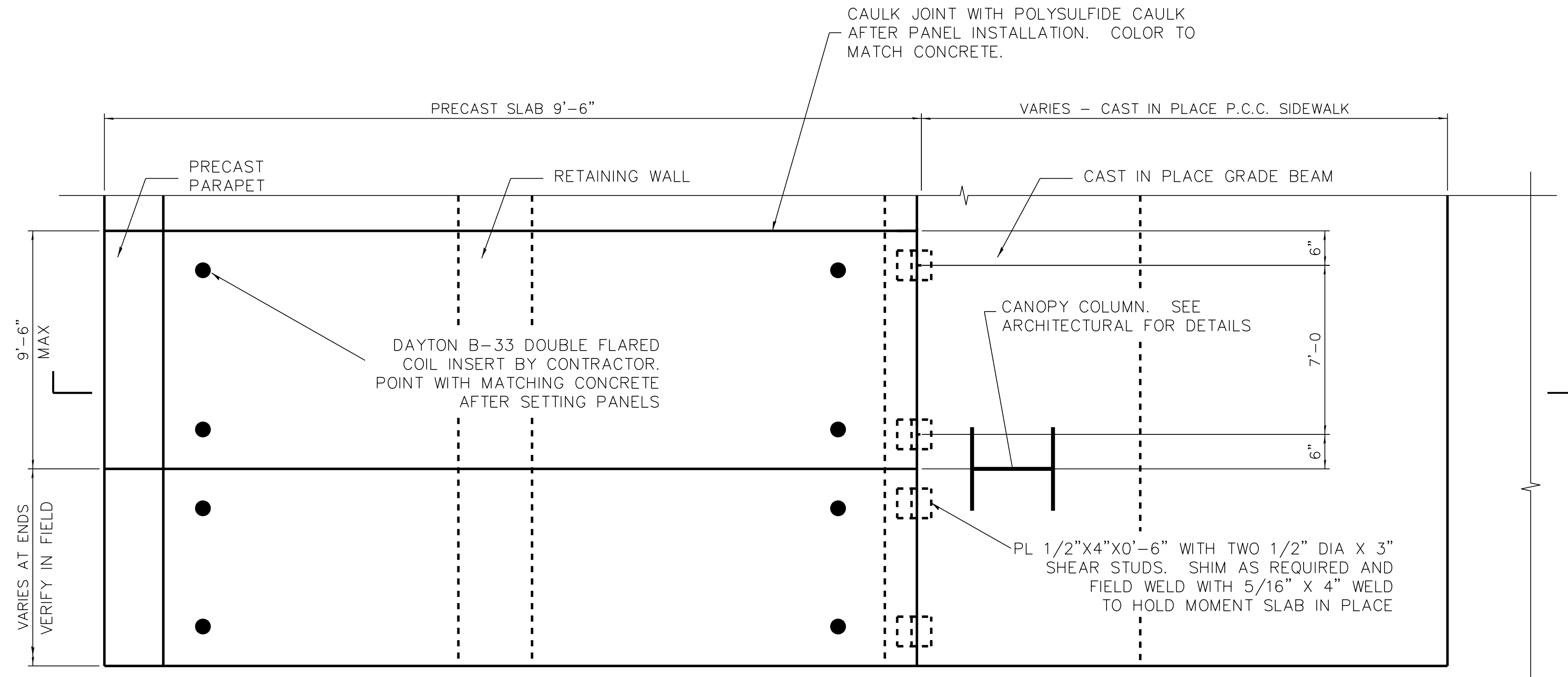
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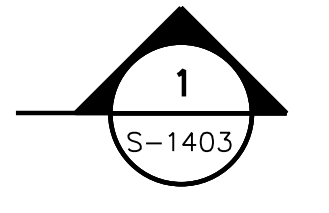
**1** SECTION 1 - SOUTH TERMINAL  
S-1403



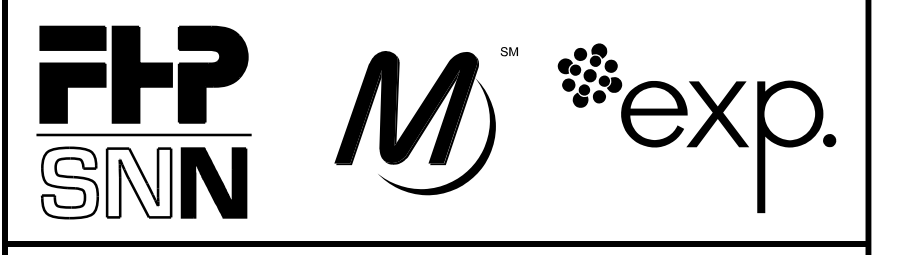
**4** JOINTING DETAIL  
S-1403



**2** PLAN VIEW - SOUTH TERMINAL  
S-1403



- NOTES:
1. PRECASTER'S STRUCTURAL ENGINEER TO PROVIDE ADDITIONAL REINFORCING AS REQUIRED TO CONTROL CRACKING.
  2. PRECASTER'S STRUCTURAL ENGINEER DESIGN LIFTING INSERTS.
  3. SHIM PANELS AS REQUIRED AT GRADE BEAM TO MATCH GRADE.
  4. FIELD WELD PANELS TO GRADE BEAM EMBEDDED PLATES TO STABILIZE PRE-CAST UNTIL CLOSURE POUR IS MADE.
  5. SEE C-163 FOR SLAB LAYOUT.



**cta** CHICAGO TRANSIT AUTHORITY ENGINEERING

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15 West 95th Street (South Terminal)  
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APPROVED BY	D.ZROKA
CHECKED BY	P.MAID
DESIGNED BY	L.SOMMER
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PROJECT NO.	2012-0021
FILE NAME	File Name

MARK	DATE	DESCRIPTION
	2/9/17	ISSUED FOR CONSTRUCTION

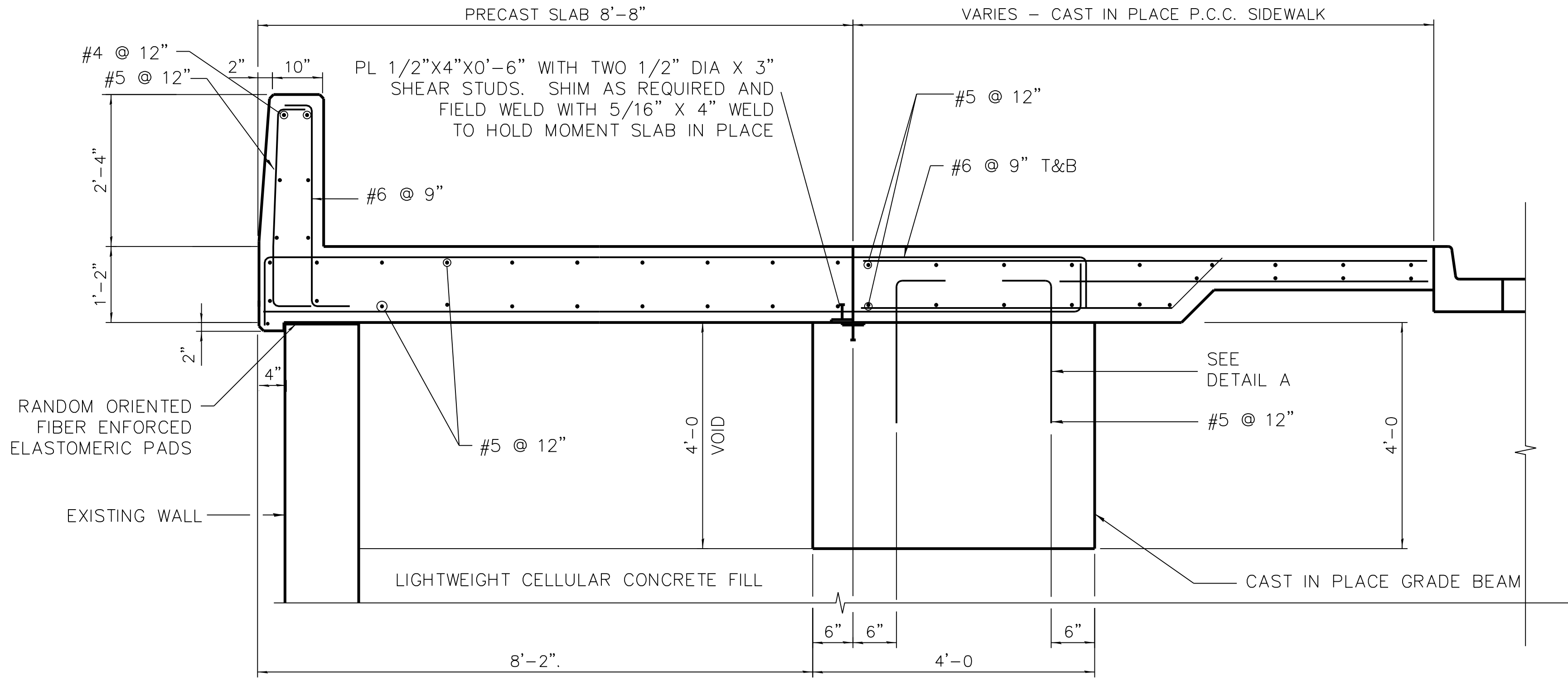
LOCATION IDENTIFIER: **DR095**

SOUTH TERMINAL  
MOMENT SLAB DETAILS

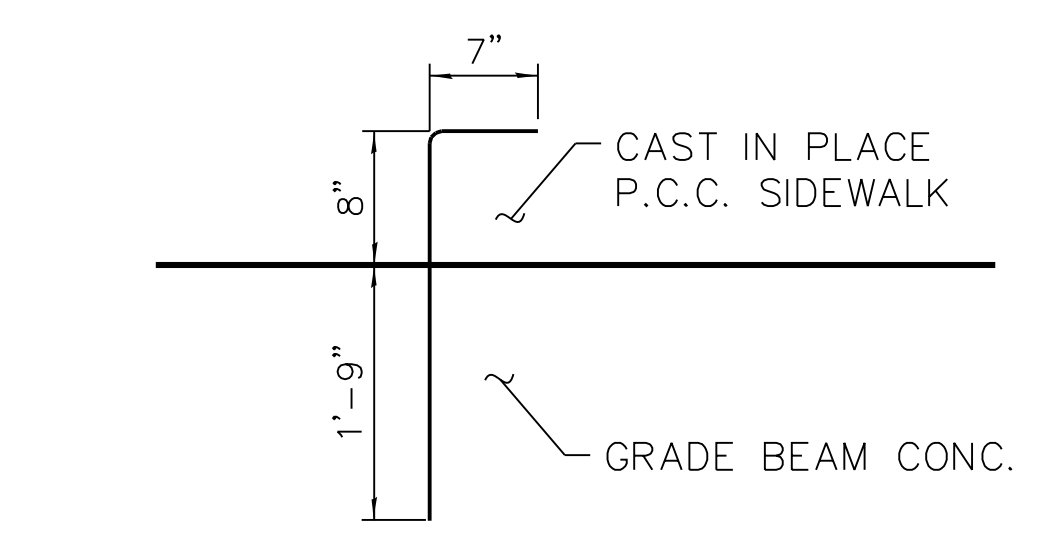
**S-1403**

FILE NAME = 070\_DR095\_S-1403.dgn  
PLOT DATE = 12/14/2016

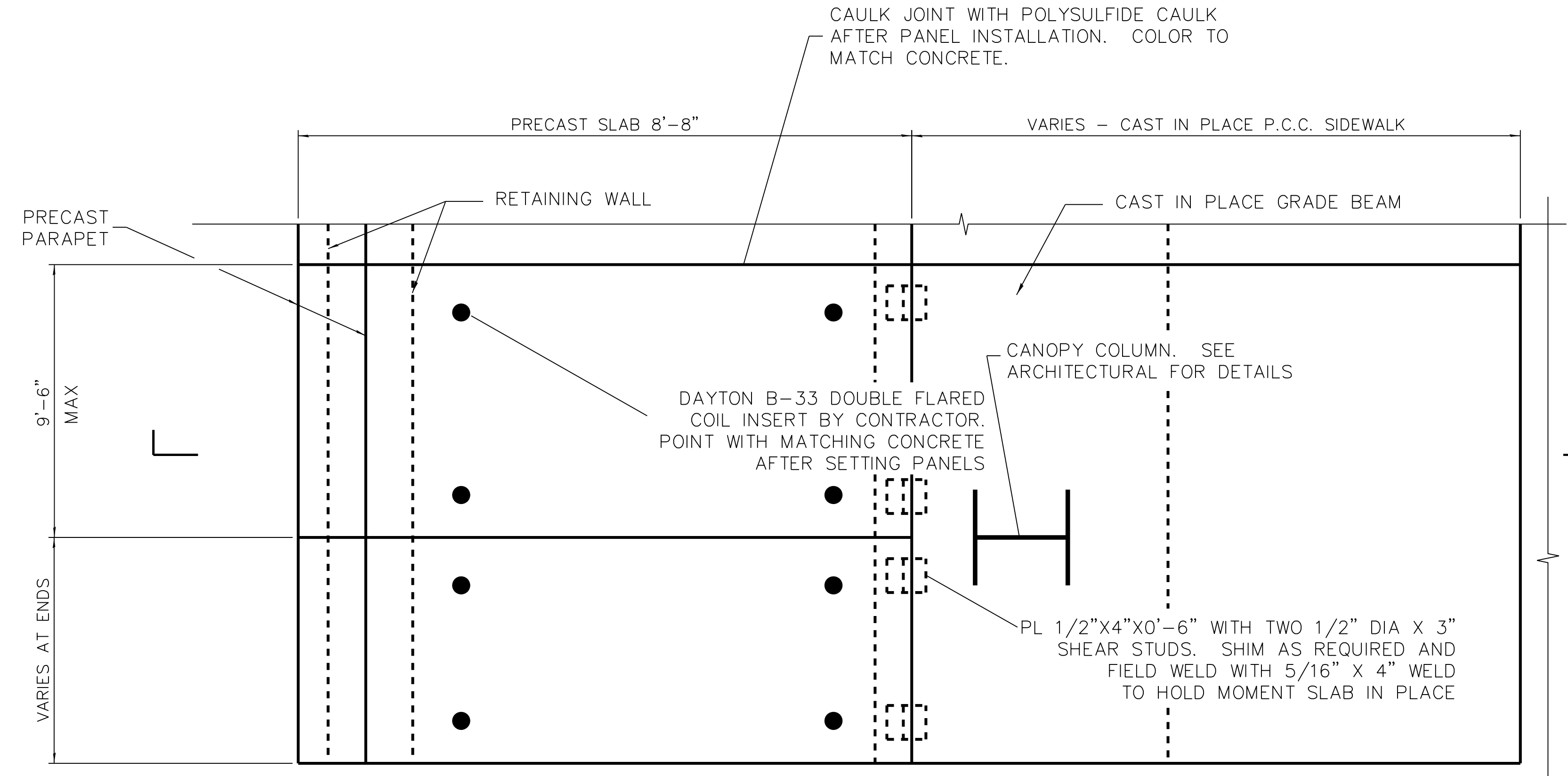
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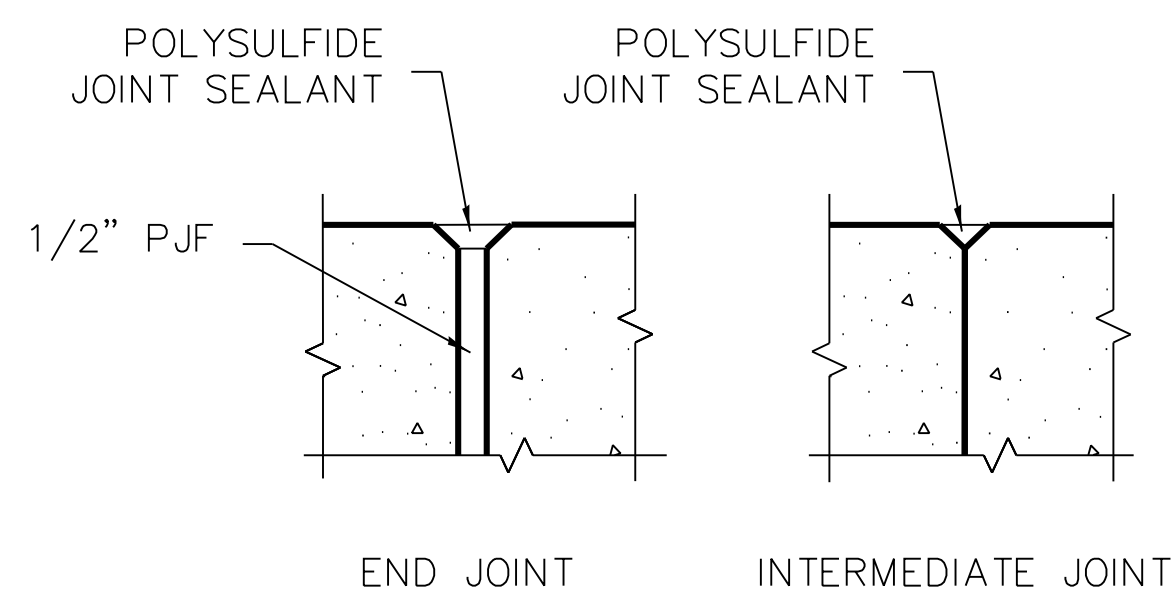
**1** SECTION 1 - NORTH TERMINAL  
S-1403A



**3** DETAIL A  
S-1403A



**2** PLAN VIEW - NORTH TERMINAL  
S-1403A



**4** JOINTING DETAIL  
S-1403A

- NOTES:
1. PRECASTER'S STRUCTURAL ENGINEER TO PROVIDE ADDITIONAL REINFORCING AS REQUIRED TO CONTROL CRACKING.
  2. PRECASTER'S STRUCTURAL ENGINEER DESIGN LIFTING INSERTS.
  3. SHIM PANELS AS REQUIRED AT GRADE BEAM TO MATCH GRADE.
  4. FIELD WELD PANELS TO GRADE BEAM EMBEDDED PLATES TO STABILIZE PRE-CAST UNTIL CLOSURE POUR IS MADE.
  5. SEE C-164 FOR SLAB LAYOUT.



**cta** CHICAGO TRANSIT AUTHORITY ENGINEERING

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DRAWN BY	T.SLEDGE
PROJECT NO.	2012-0021
FILE NAME	File Name

MARK	DATE	DESCRIPTION
	2/9/17	ISSUED FOR CONSTRUCTION

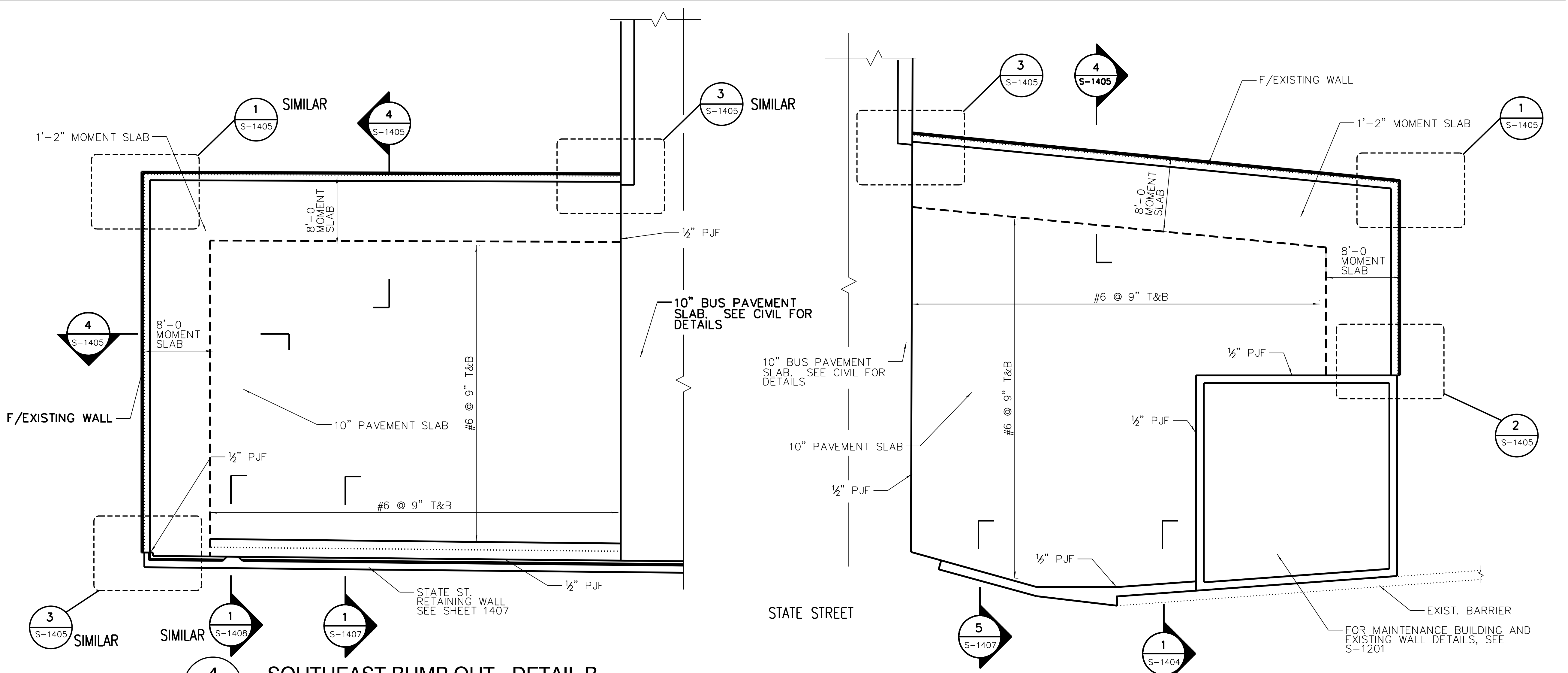
LOCATION IDENTIFIER: **DR095**

NORTH TERMINAL  
MOMENT SLAB DETAILS

**S-1403A**

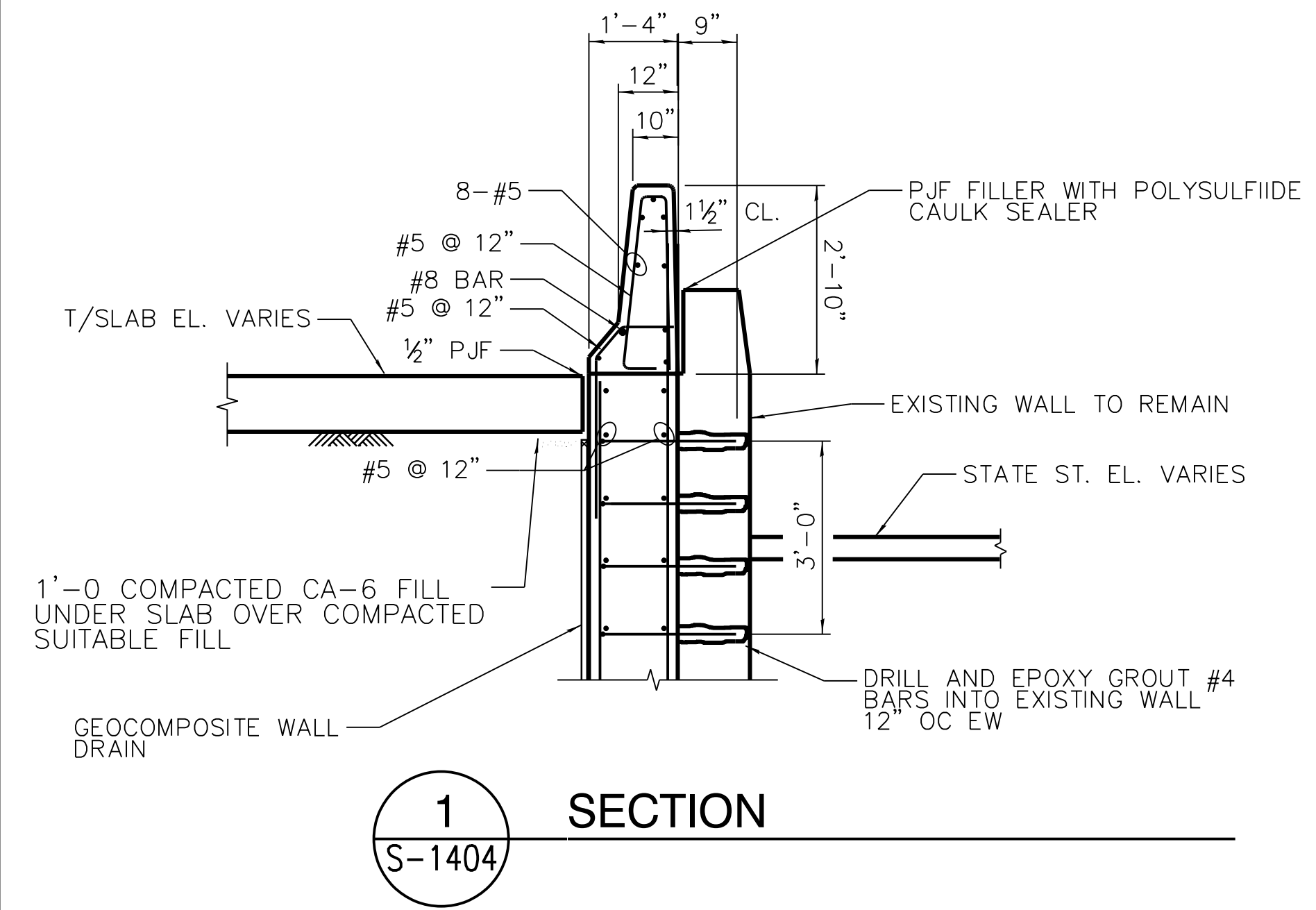
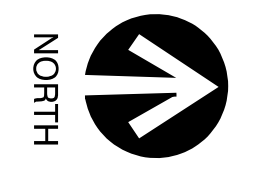
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PLOT DATE = 12/14/2016

vscorpa

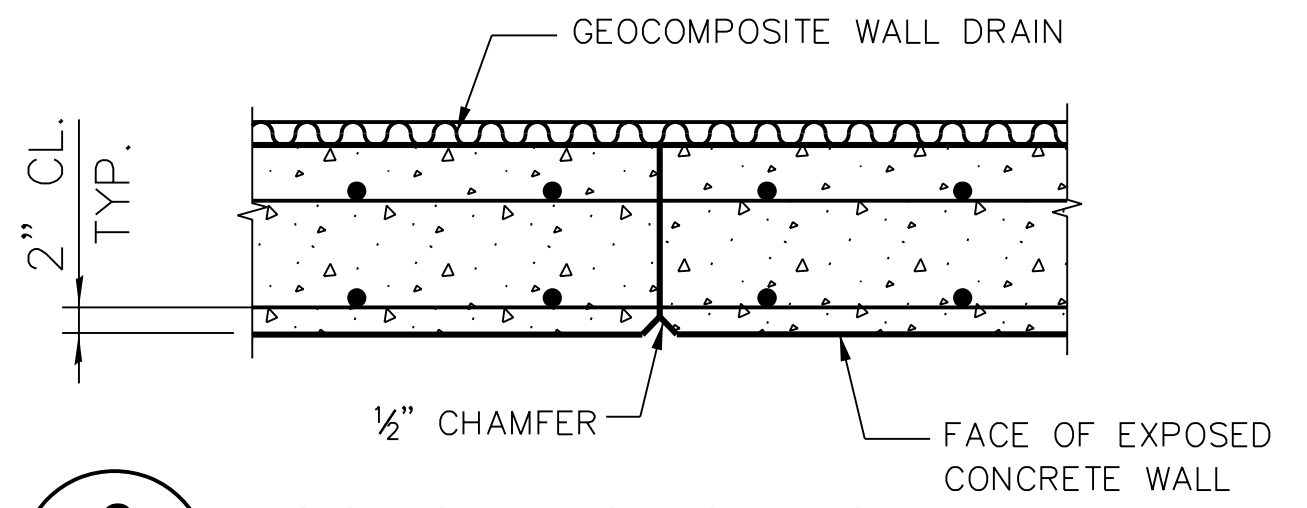


**4** SOUTHEAST BUMP OUT - DETAIL B  
S-1404

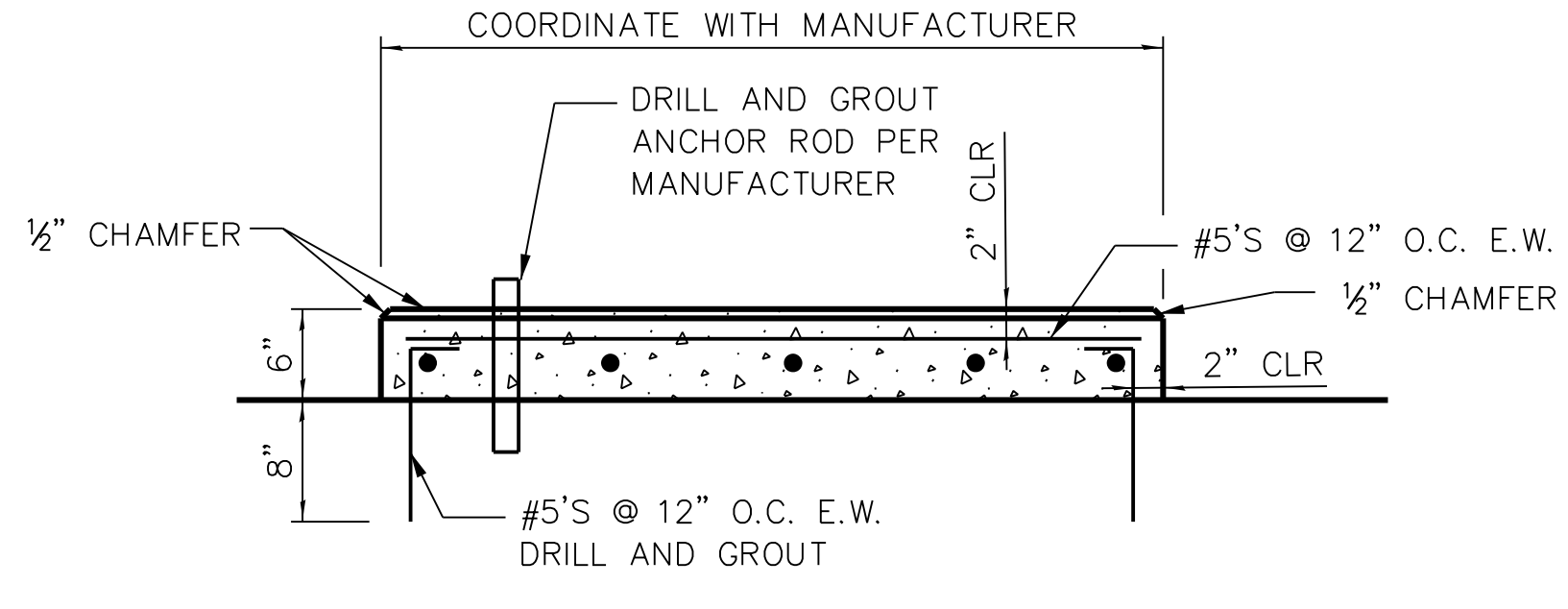
**3** NORTHEAST BUMP OUT - DETAIL A  
S-1404



**1** SECTION  
S-1404

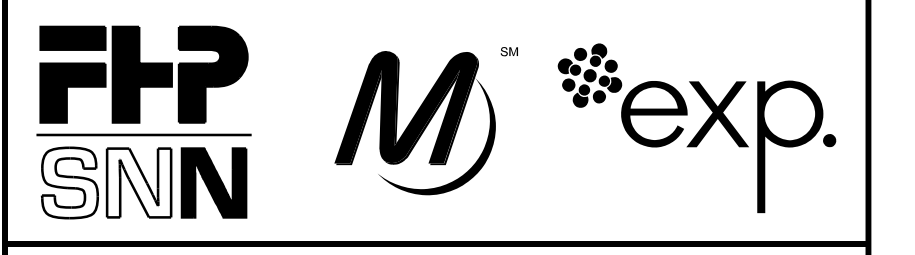


**2** CONSTRUCTION JOINT DETAIL  
S-1404



**5** EQUIPMENT PAD DETAIL  
S-1404

- NOTES
1. CONCRETE: 5,000 PSI F'c AT 28 DAYS
  2. REINFORCING: ASTM A615 GRADE 60 EPOXY COATED
  3. SEE CIVIL PLANS FOR SLAB ELEVATIONS AND DIMENSIONS
  4. SEE ARCHITECTURAL PLANS FOR MAINTENANCE BUILDING SIZE AND DETAILS
  5. PROVIDE CONSTRUCTION JOINTS AT SPACINGS NOT TO EXCEED 30 FEET.
  6. AT CONTRACTOR'S OPTION, MOMENT SLABS MAY BE PRECAST.
  7. COORDINATE EQUIPMENT PADS PLACEMENTS AND DUCTS WITH ELECTRICAL PLANS.
  8. COORDINATE ID-1 SIGNAGE WITH AG101.



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 15 West 95th Street (South Terminal)  
 Chicago, IL 60628

IN CHARGE	S. KADERBEK
APPROVED BY	D.ZROKA
CHECKED BY	P.MAID
DESIGNED BY	L.SOMMER
DRAWN BY	T.SLEDGE
PROJECT NO.	2012-0021
FILE NAME	File Name

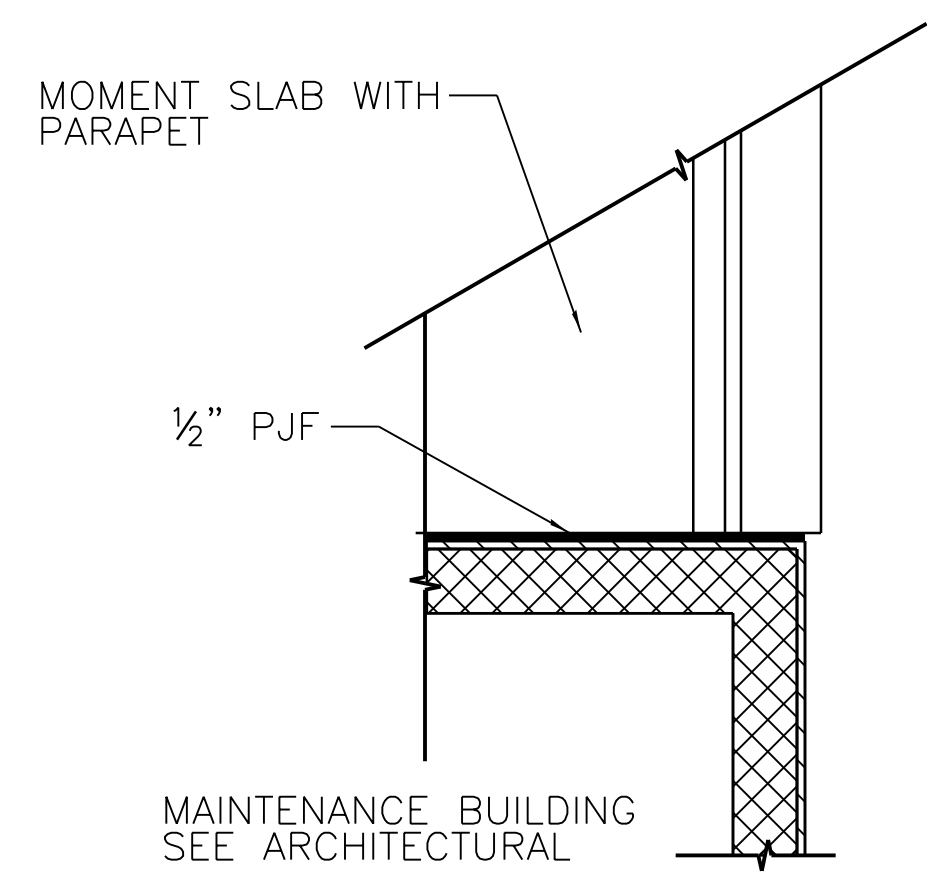
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	2/9/17	ISSUED FOR CONSTRUCTION

LOCATION IDENTIFIER: **DR095**  
 NORTHEAST AND SOUTHEAST BUMP-OUTS, PLANS AND MOMENT SLAB DETAILS

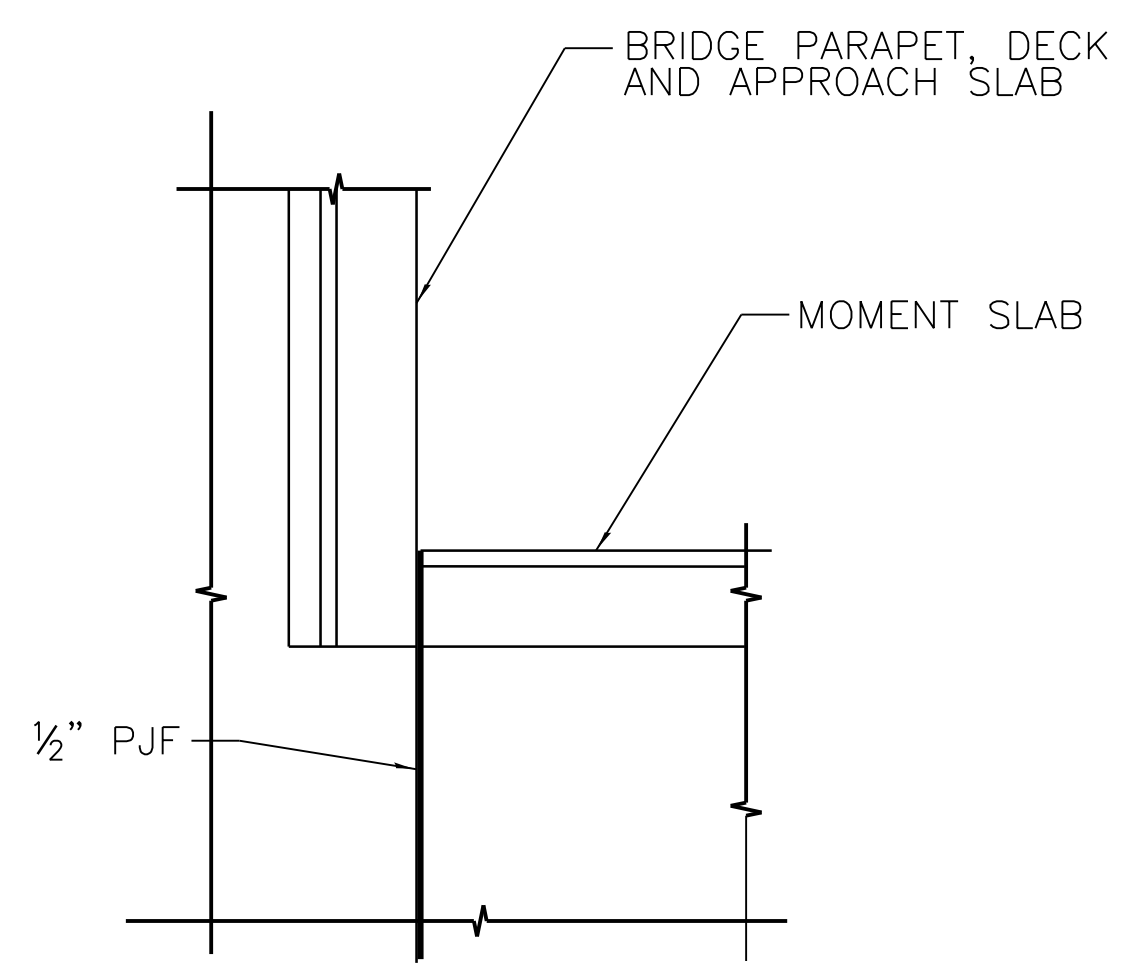
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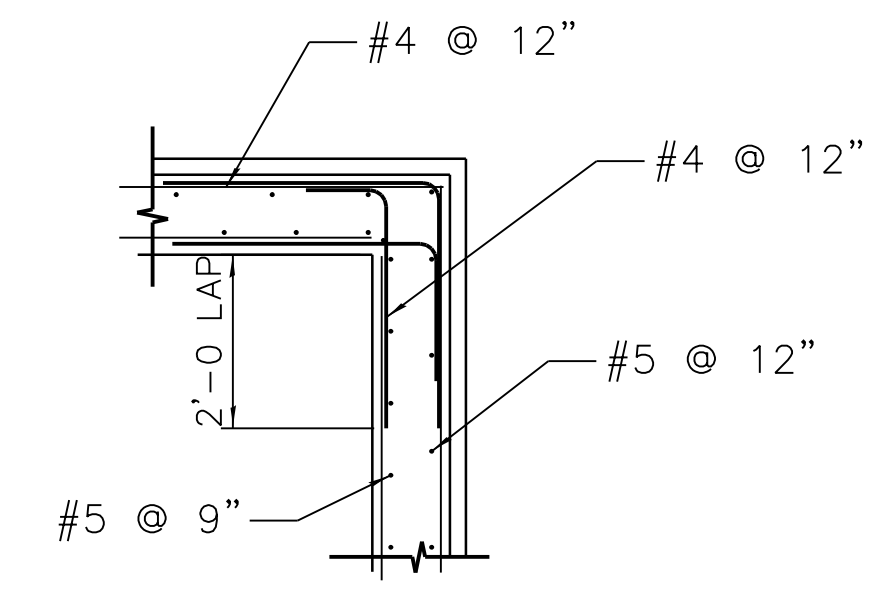
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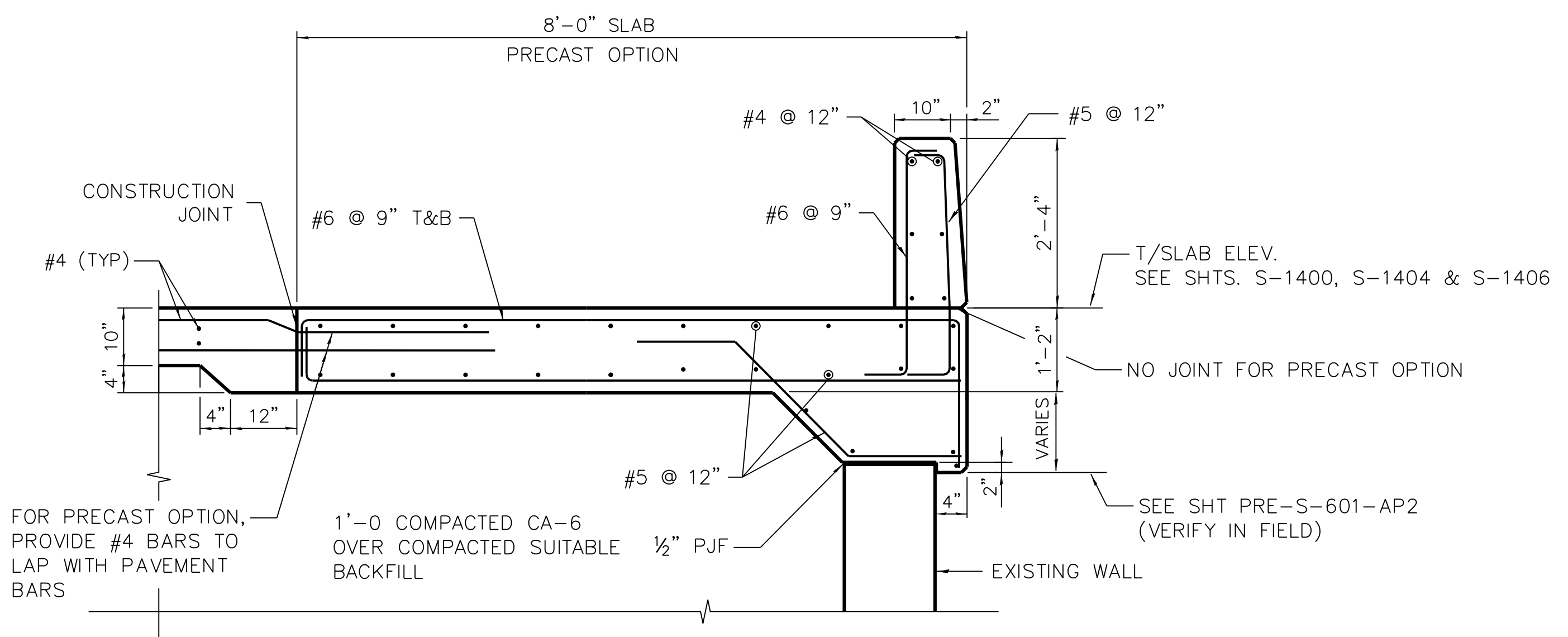
**2** DETAIL  
S-1405



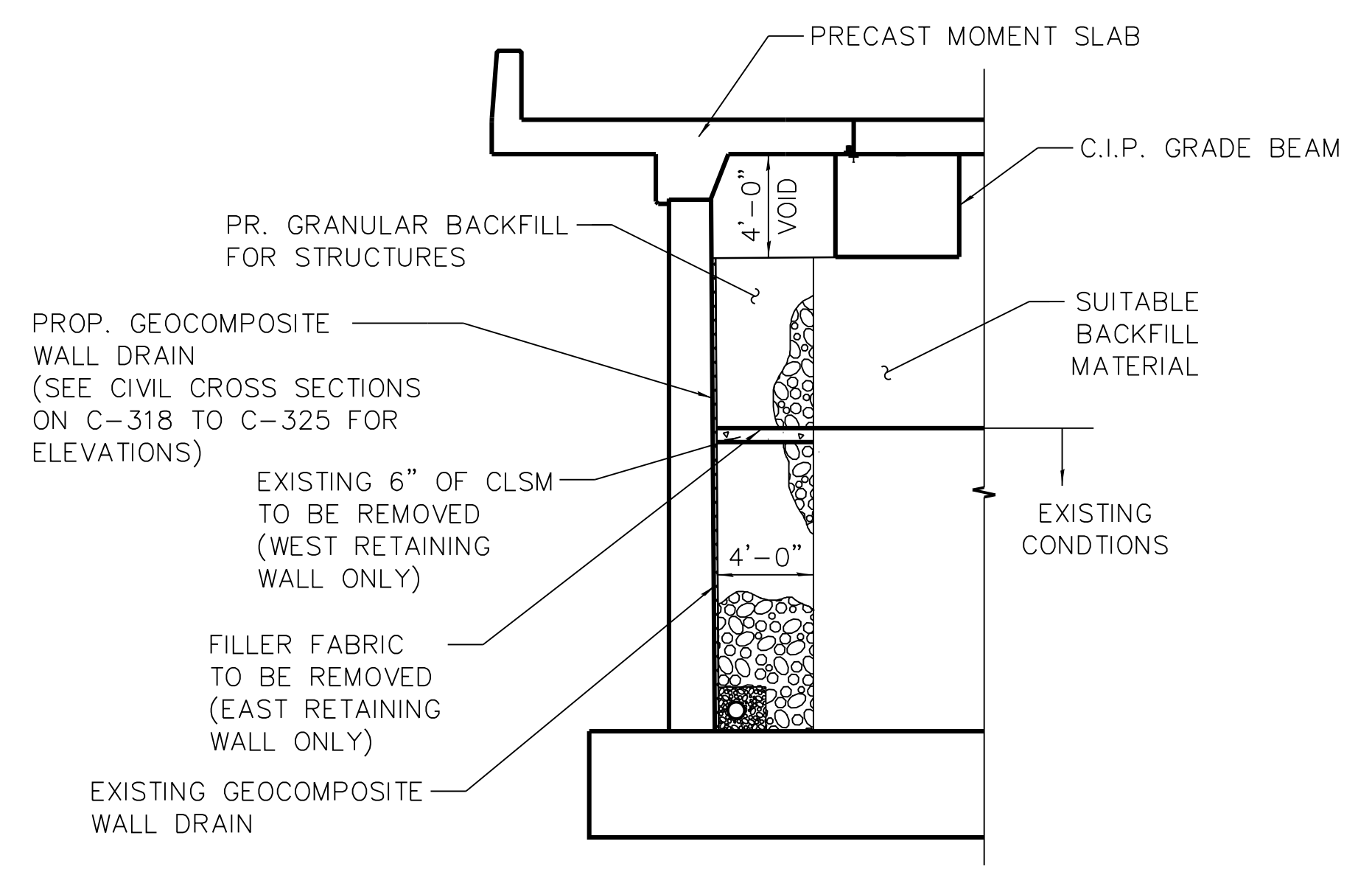
**3** DETAIL  
S-1405



**1** DETAIL  
S-1405



**4** MOMENT SLAB SECTION  
S-1405



**5** SOUTH TERMINAL RETAINING WALL BACKFILL  
S-1405



**SENSITIVE SECURITY INFORMATION**  
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**95th Terminal Improvement**  
15 West 95th Street (South Terminal)  
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PROJECT NO.	2012-0021
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MARK	DATE	DESCRIPTION
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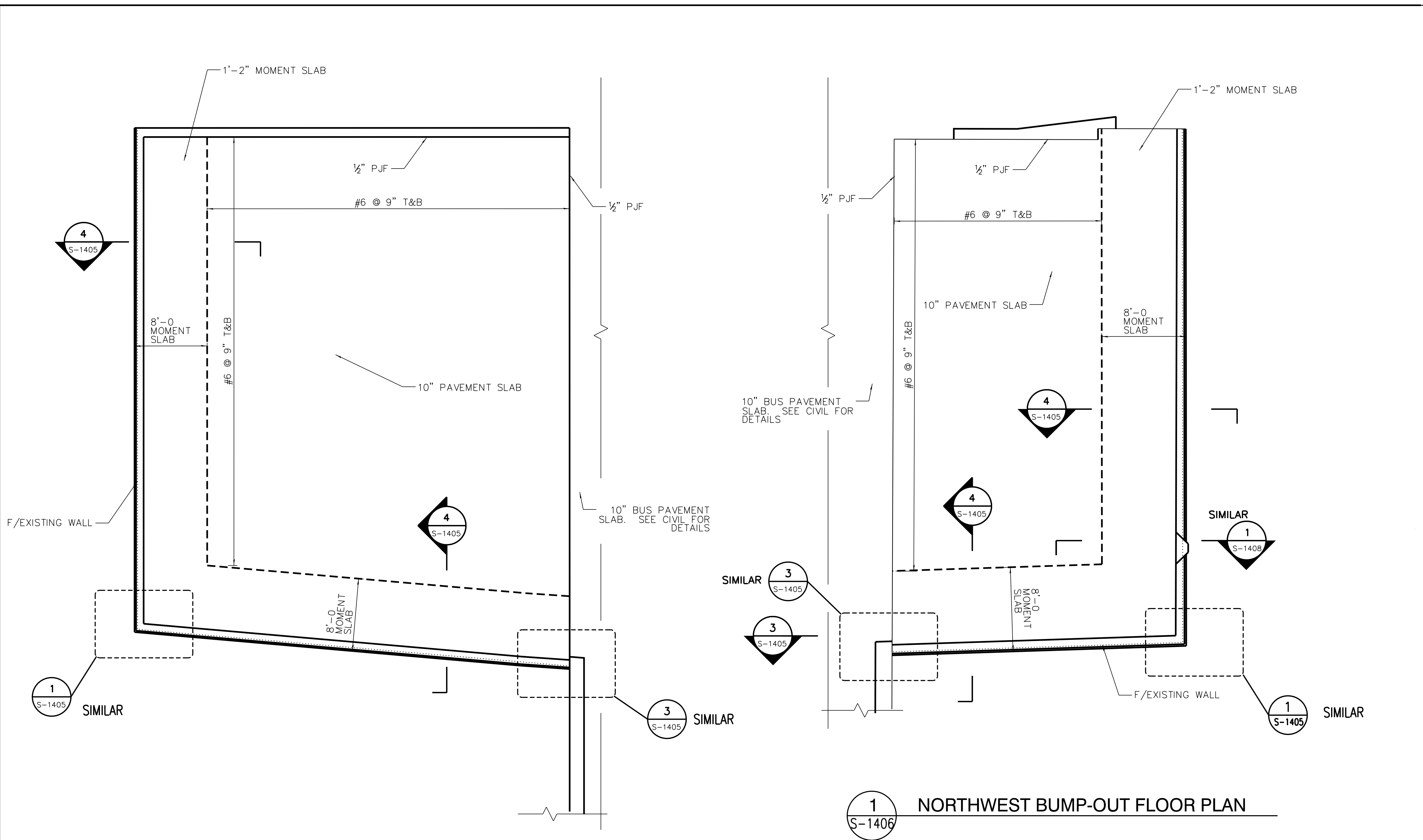
LOCATION IDENTIFIER: **DR095**

BUMP-OUTS, MOMENT SLABS AND MISC. DETAILS

**S-1405**

FILE NAME = 070\_DR095\_S-1405.dgn  
PLOT DATE = 12/14/2016

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2  
S-1406  
SOUTHWEST BUMP-OUT FLOOR PLAN

1  
S-1406  
NORTHWEST BUMP-OUT FLOOR PLAN



- NOTES
1. CONCRETE: 5,000 PSI F'c AT 28 DAYS
  2. REINFORCING: ASTM A615 GRADE 60 EPOXY COATED
  3. SEE CIVIL PLANS FOR SLAB ELEVATIONS AND DIMENSIONS
  4. SEE ARCHITECTURAL PLANS FOR MAINTENANCE BUILDING SIZE AND DETAILS.
  5. PROVIDE CONSTRUCTION JOINTS AT SPACINGS NOT TO EXCEED 30 FEET.
  6. AT CONTRACTOR'S OPTION, MOMENT SLABS MAY BE PRECAST.
  7. COORDINATE EQUIPMENT PADS PLACEMENTS, AND DUCTS WITH ELECTRICAL PLANS. SEE SHEET E-003, E-004, AND E-406.
  8. COORDINATE ID-1 SIGNAGE WITH AG102.

FILE NAME = 070\_DR095\_S-1406.dgn  
PLOT DATE = 12/14/2016



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FILE NAME	File Name

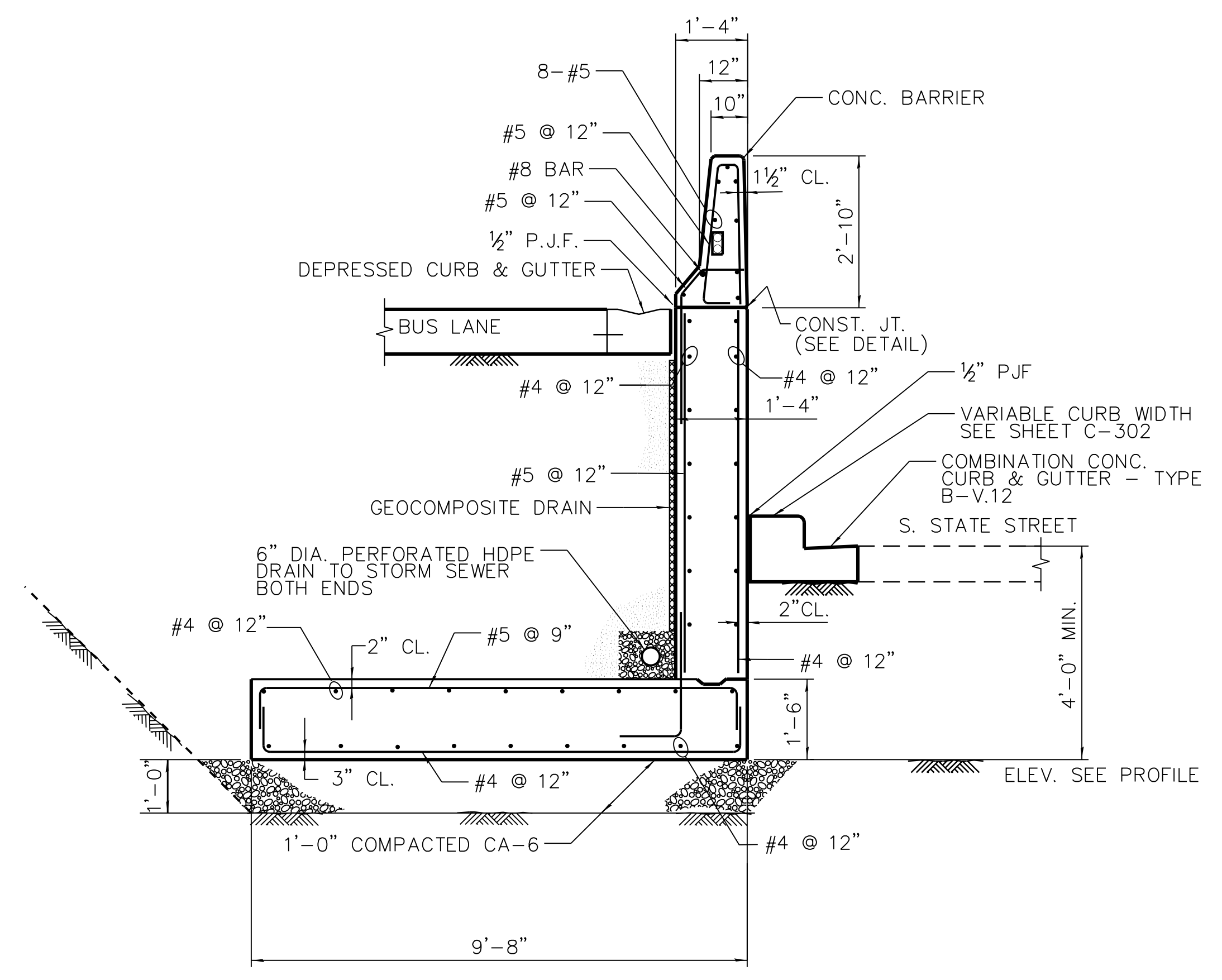
MARK	DATE	DESCRIPTION
	2/9/17	ISSUED FOR CONSTRUCTION

LOCATION IDENTIFIER: **DR095**

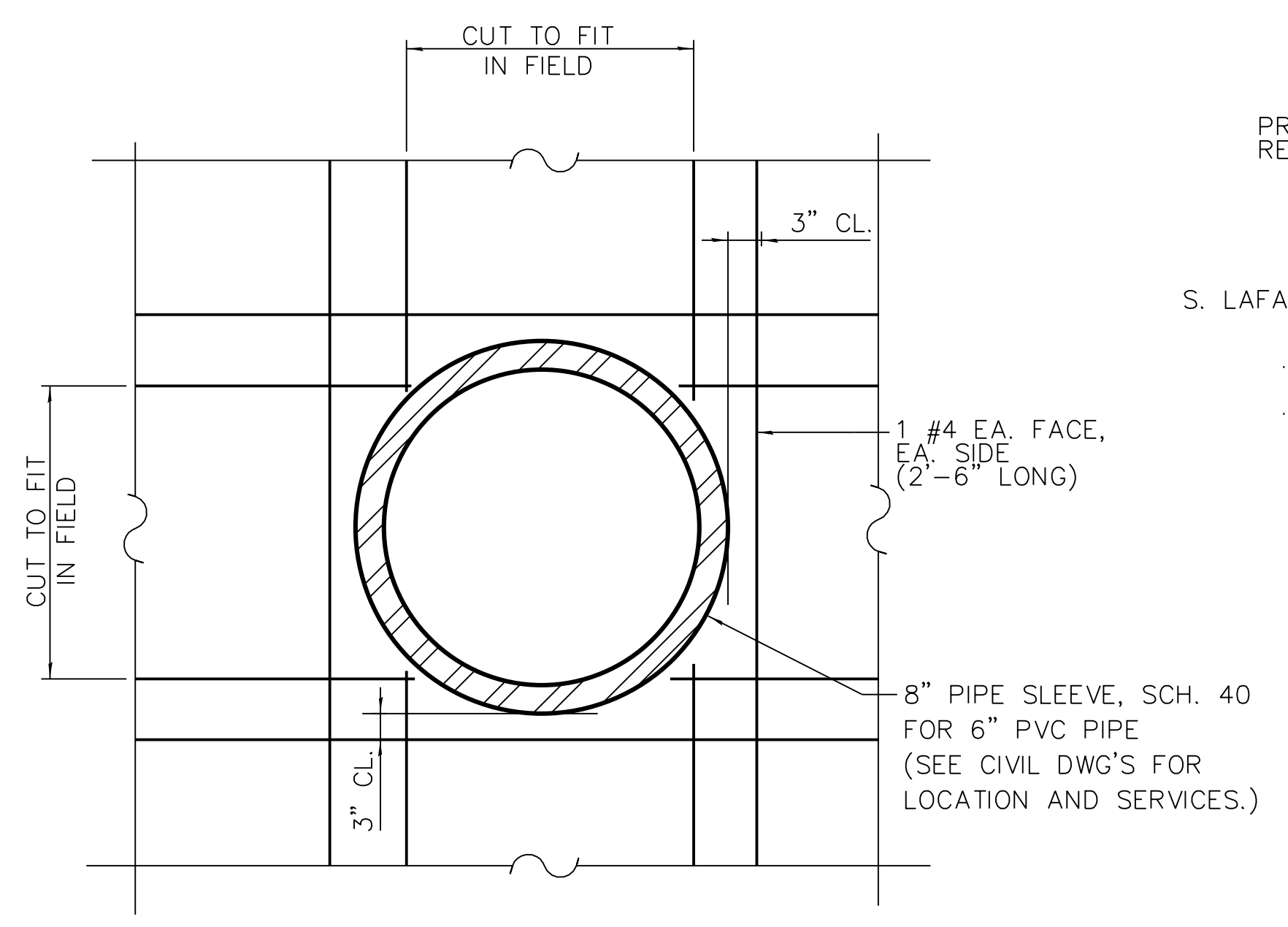
UTILITY ENCLOSURE PLANS - 1

**S-1406**

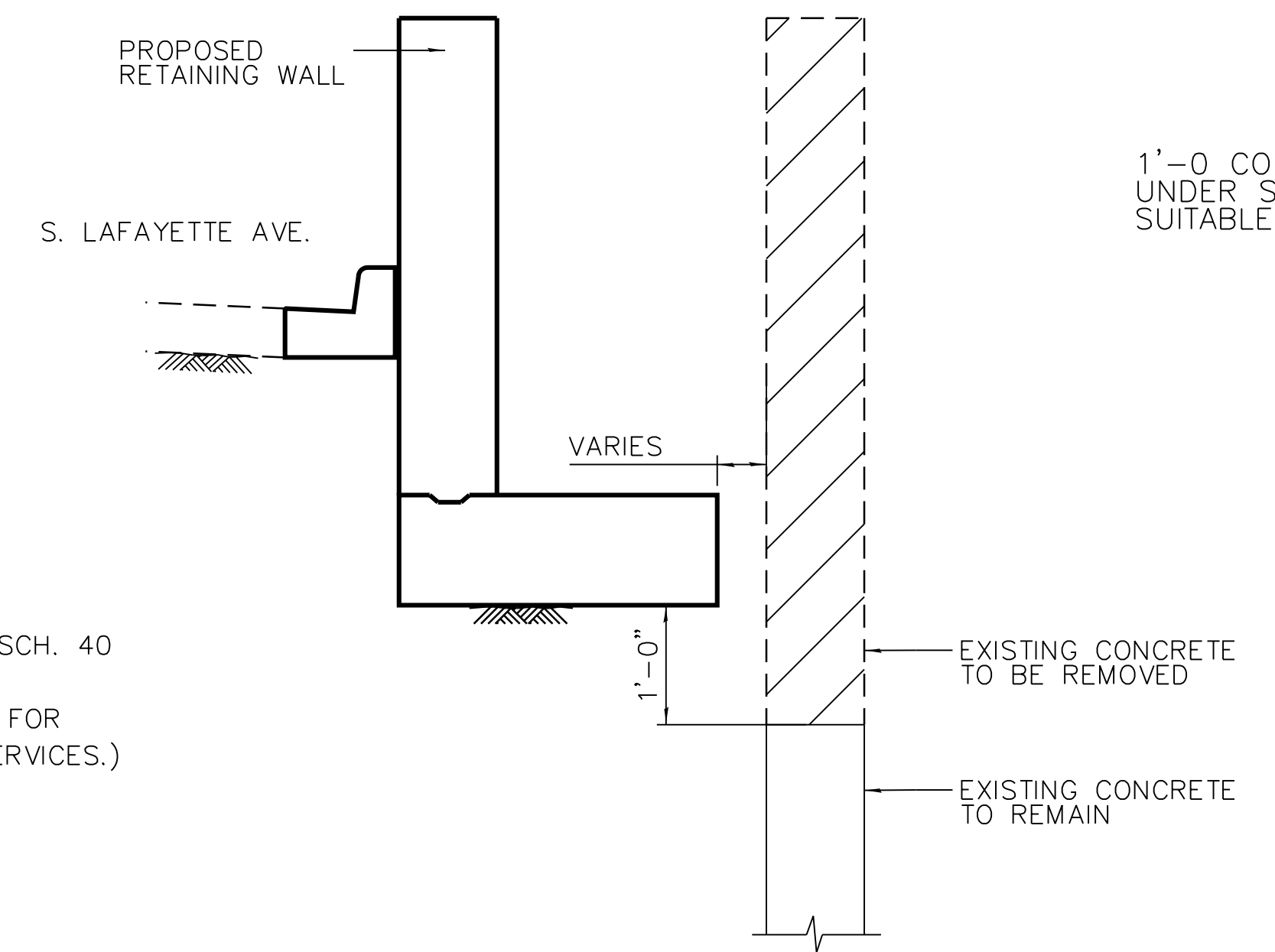
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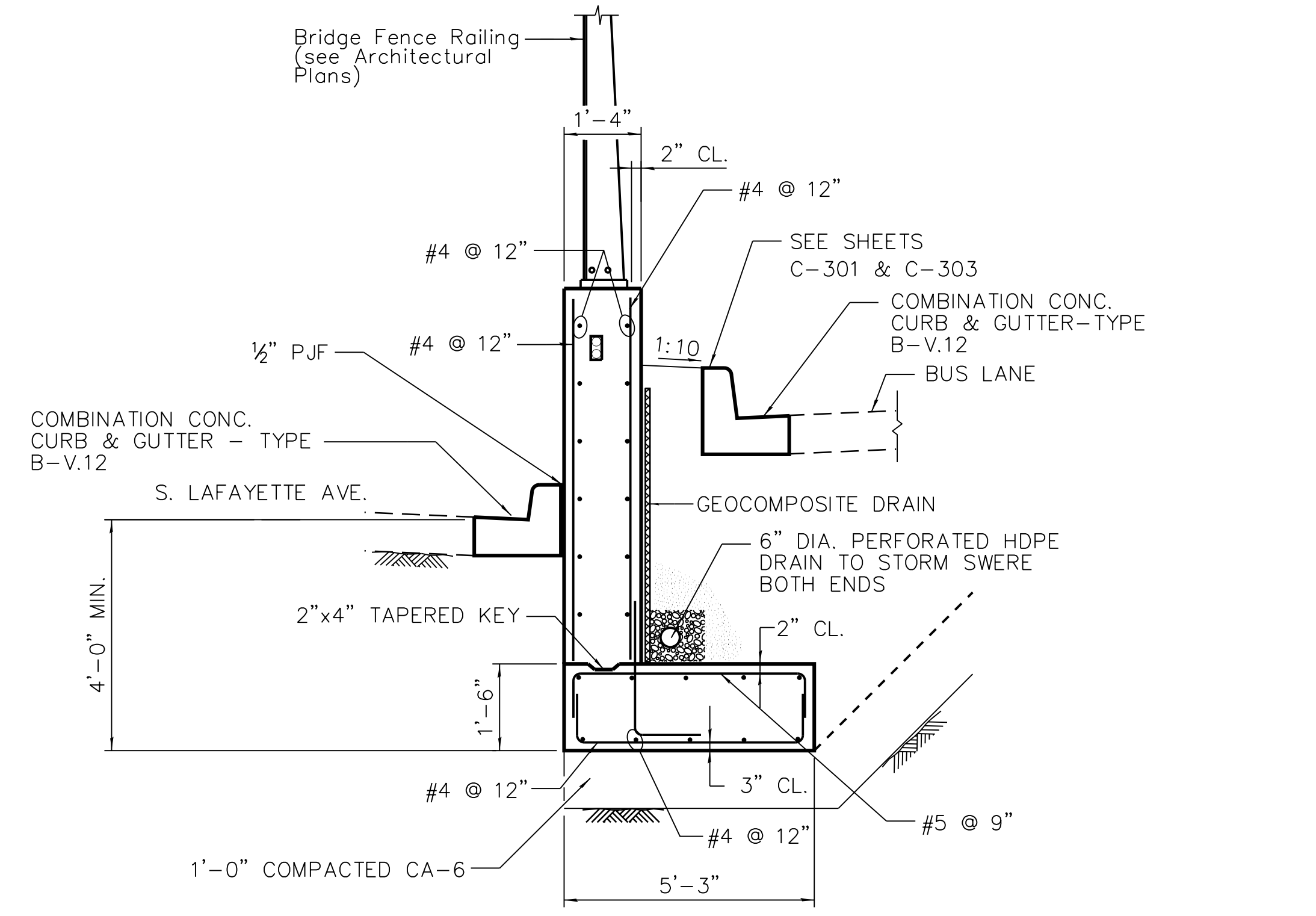
**1** SOUTH RETAINING WALL ALONG STATE STREET  
S-1407



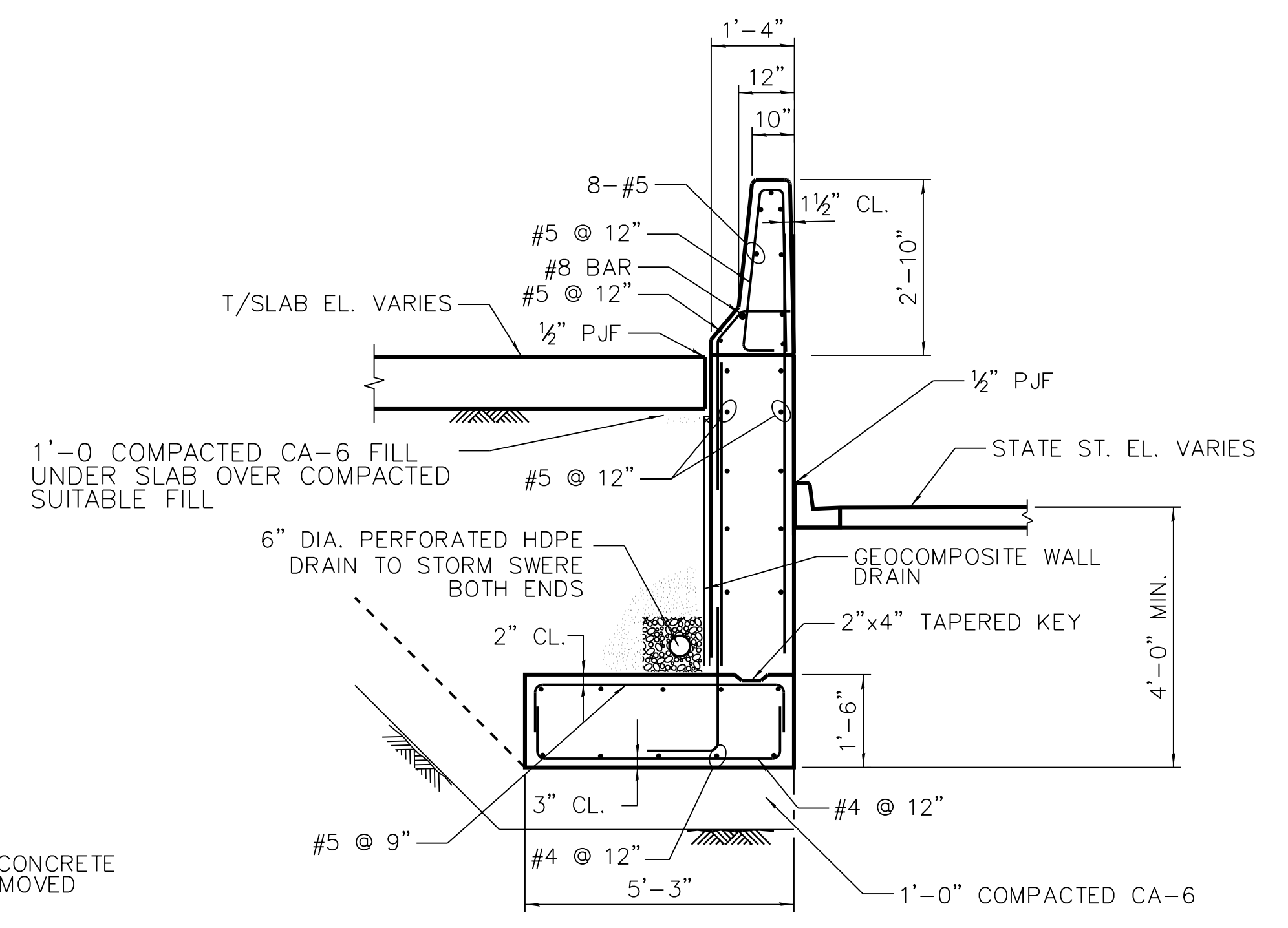
**3** PIPE PENETRATION DETAIL  
S-1407



**4** LAFAYETTE AVENUE EXISTING RETAINING WALL REMOVAL  
S-1407



**2** LAFAYETTE AVENUE RETAINING WALL NORTH AND SOUTH TERMINAL  
S-1407



**5** NORTH RETAINING WALL ALONG STATE STREET  
S-1407

- NOTES:
1. CONCRETE: 5,000 PSI F'C (28 DAYS)
  2. REINFORCING: ASTM A615 GRADE 60 EPOXY COATED.
  3. FOR PIPE DETAIL AND ELEVATION, SEE CIVIL DRAWINGS.
  4. FOR CONDUIT DETAIL, SEE S-1408.



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15 West 95th Street (South Terminal)  
Chicago, IL 60628

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APPROVED BY	D.ZROKA
CHECKED BY	P.MAID
DESIGNED BY	M.SAFFARI
DRAWN BY	T.SLEDGE
PROJECT NO.	2012-0021
FILE NAME	File Name

MARK	DATE	DESCRIPTION
	2/9/17	ISSUED FOR CONSTRUCTION

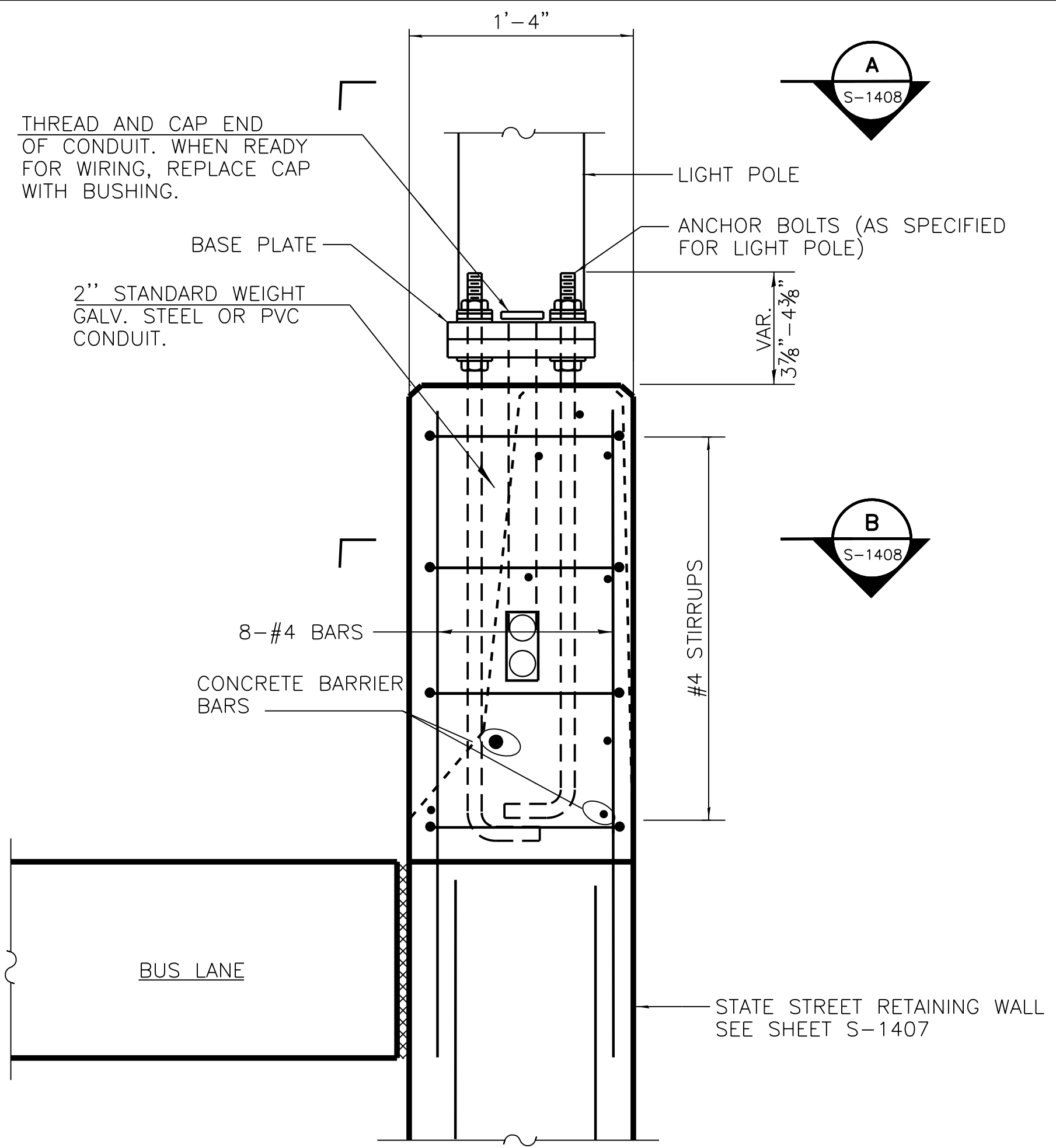
LOCATION IDENTIFIER: **DR095**

LAFAYETTE AVENUE AND STATE STREET RETAINING WALLS

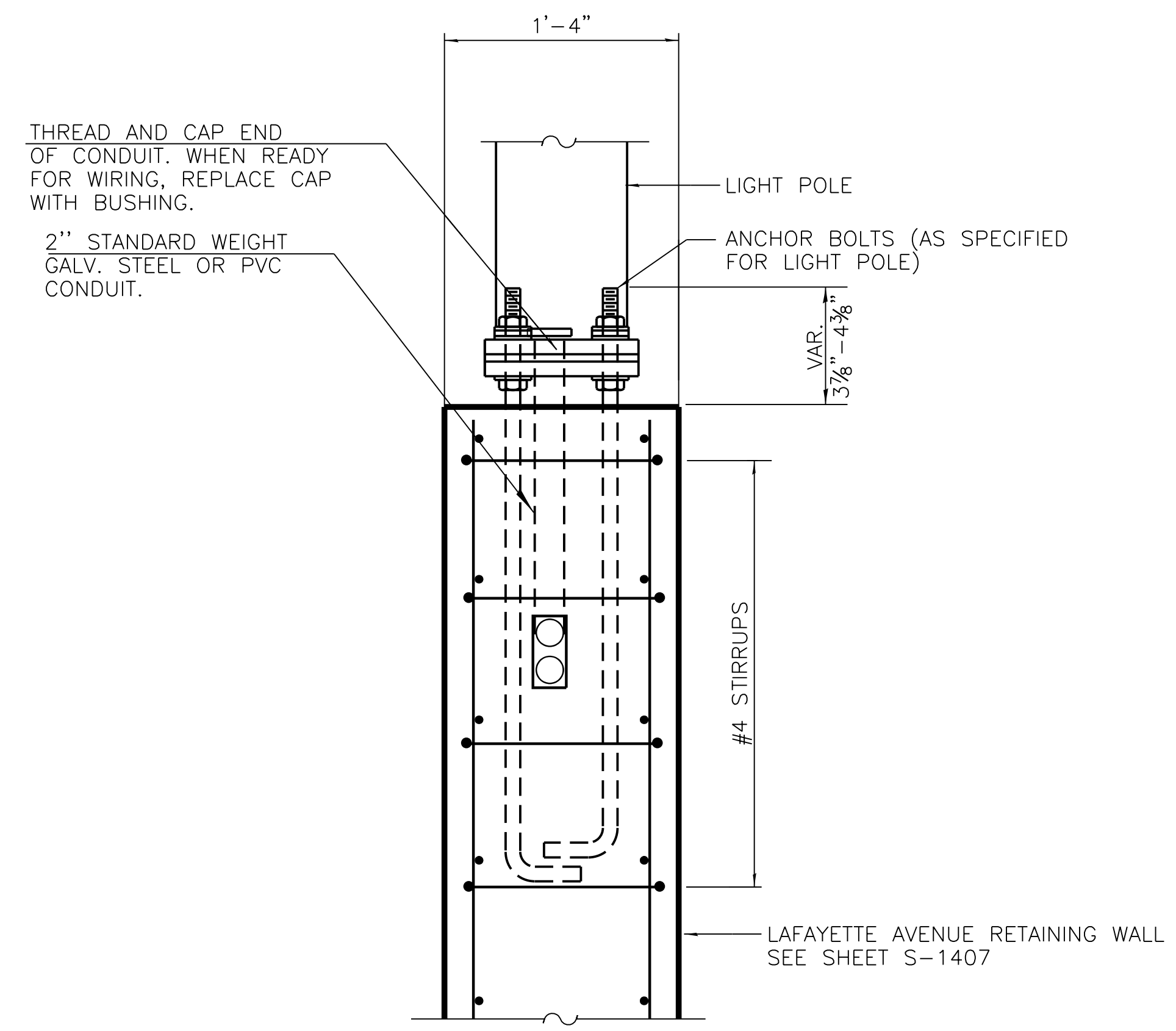
**S-1407**

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PLOT DATE = 10/21/2016

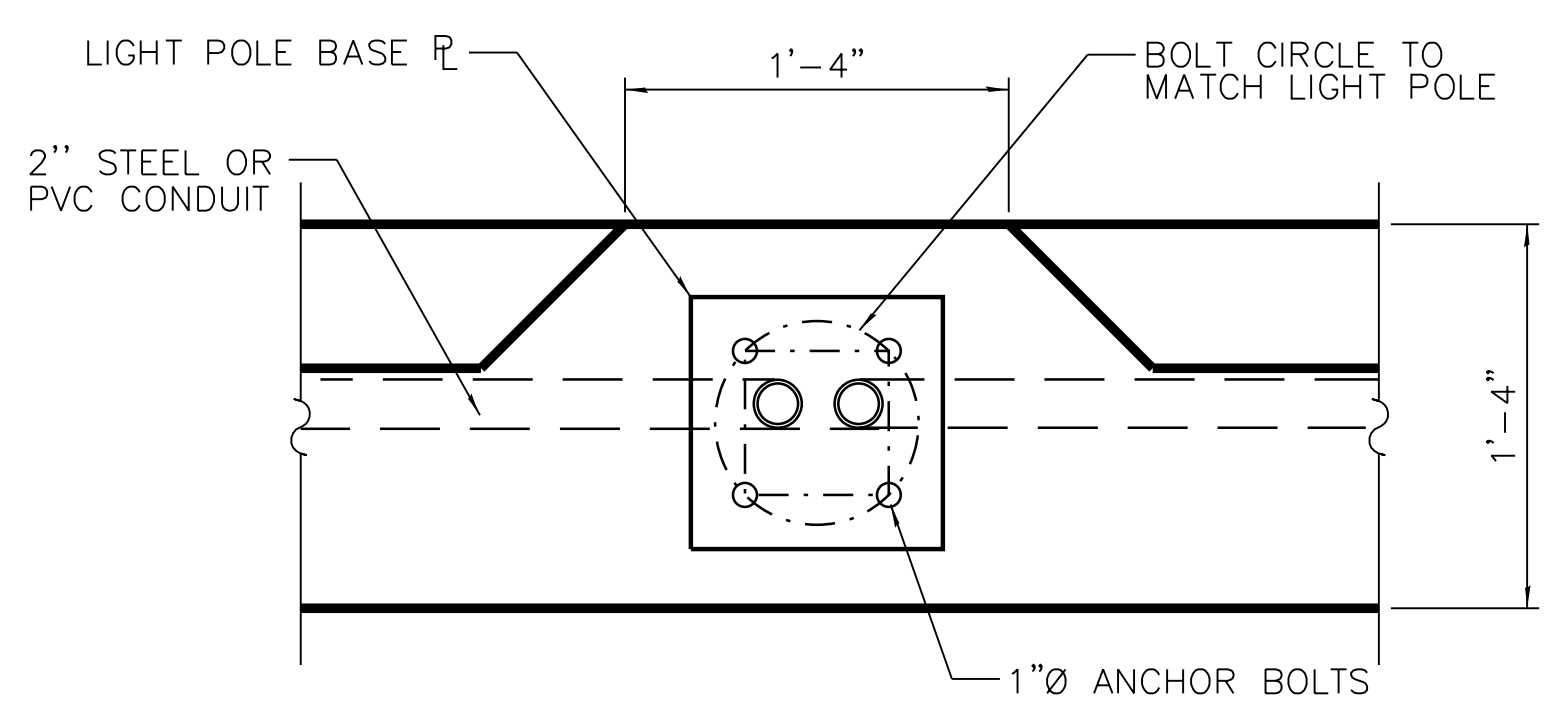
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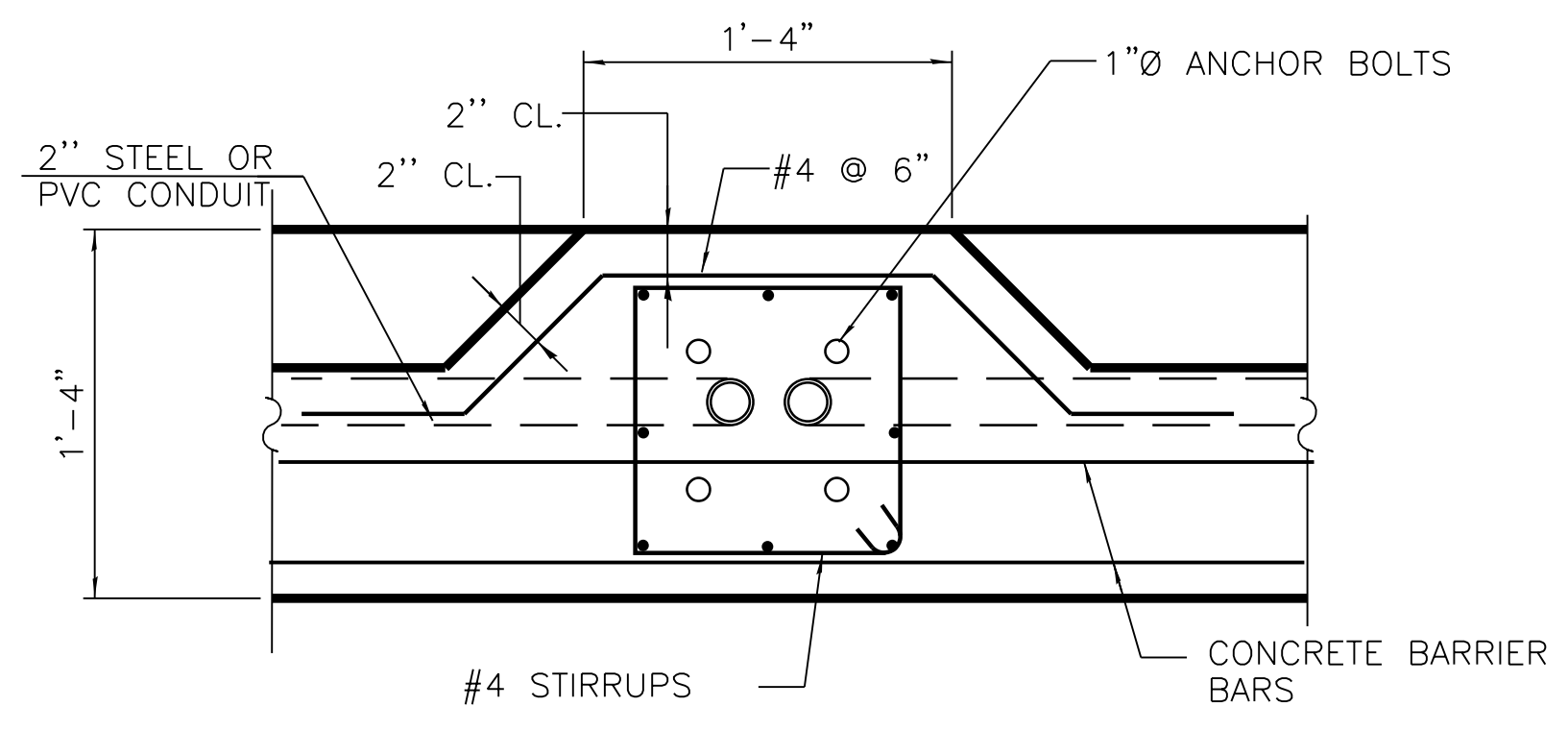
**1** LIGHT STANDARD DETAIL  
S-1408 AT BARRIER



**2** LIGHT STANDARD DETAIL  
S-1408 AT RETAINING WALL



**1** SECTION A-A  
S-1408



**2** SECTION B-B  
S-1408

**NOTE:**

- FOR LIGHT POLE STRUCTURE AND CONDUIT DETAIL, SEE ELECTRICAL DRAWINGS.
- FOR ID-1 SIGNAGE ADDITIONAL DETAILS SEE AS507.

FILE NAME = 070\_DR095\_S-1408.dgn  
PLOT DATE = 12/14/2016



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**95th Terminal Improvement**  
15 West 95th Street (South Terminal)  
Chicago, IL 60628

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APPROVED BY	D.ZROKA
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DRAWN BY	T.SLEDGE
PROJECT NO.	2012-0021
FILE NAME	File Name

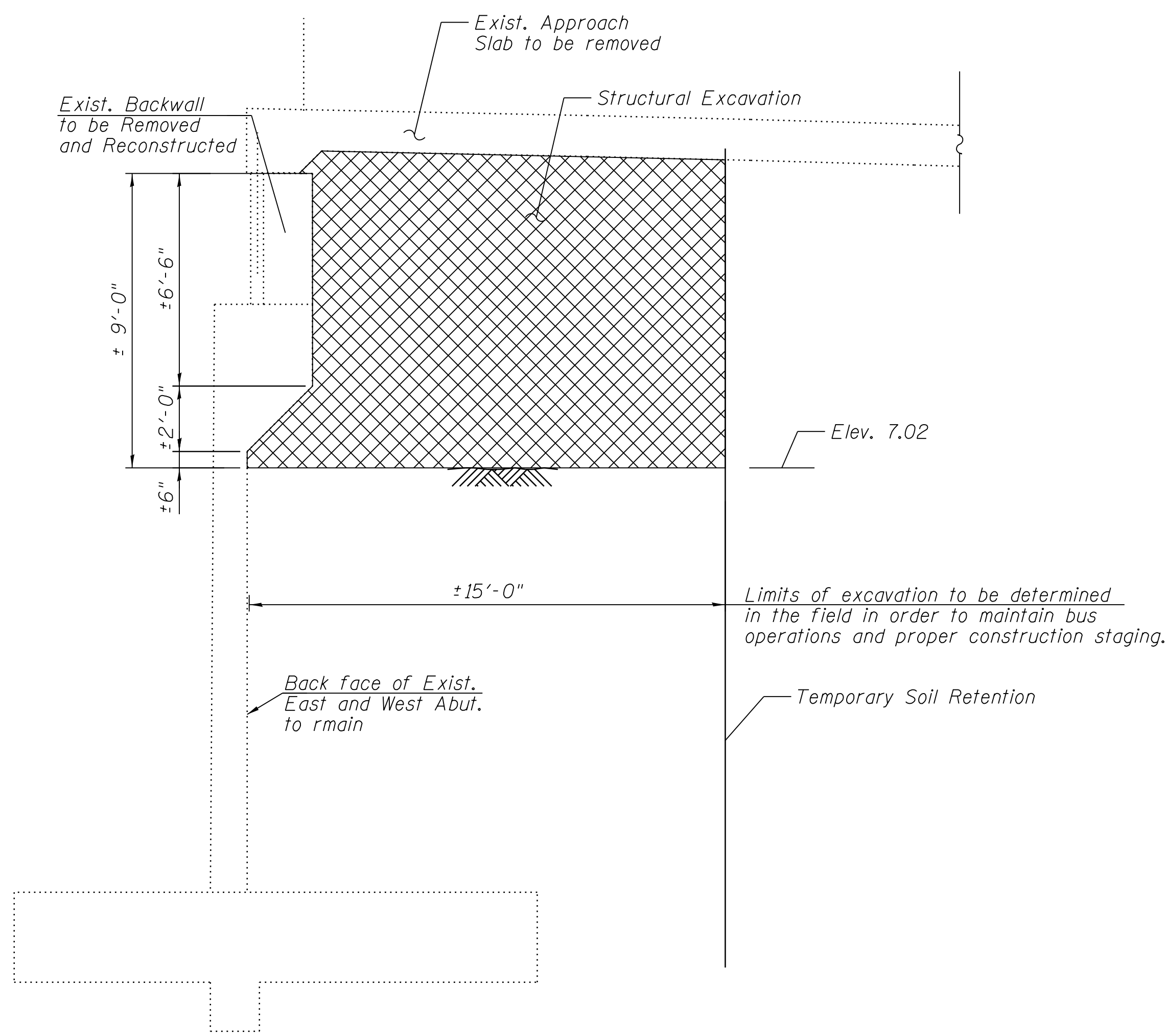
MARK	DATE	DESCRIPTION
	2/9/17	ISSUED FOR CONSTRUCTION

LOCATION IDENTIFIER: **DR095**  
LAFAYETTE AVENUE  
AND STATE STREET  
LIGHTING DETAILS

**S-1408**

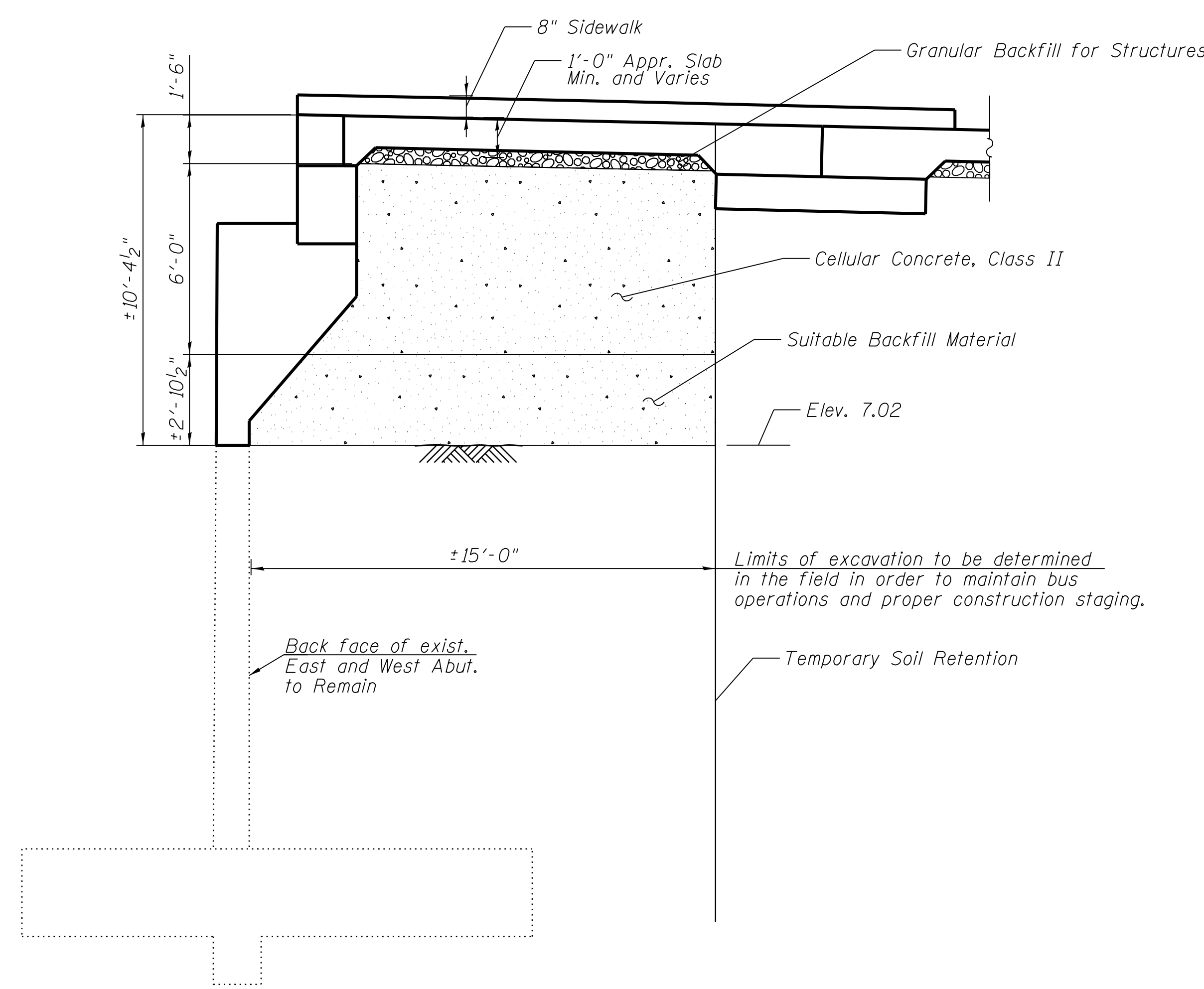


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**SECTION A - EXCAVATION**  
 (West Abutment - Looking South)  
 (East Abutment - Looking North)  
 Scale: N.T.S.

- CONSTRUCTION SEQUENCE:**
- 1) Remove Existing Approach Slab.
  - 2) Excavate behind Existing Retaining wall to the Limits shown on the plans.
  - 3) Remove Backwall.
  - 4) Reconstruct Backwall as shown on Sheets 95TH-S-135 and 95TH-S-140.
  - 5) Backfill with approved Cellular Concrete in lifts not exceeding four (4) feet.
  - 6) Backfill with Granular Backfill for Structures.
  - 7) Construct Approach Slab, Closure Pour and Sidewalk.

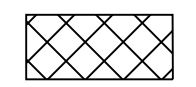
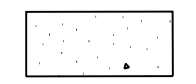


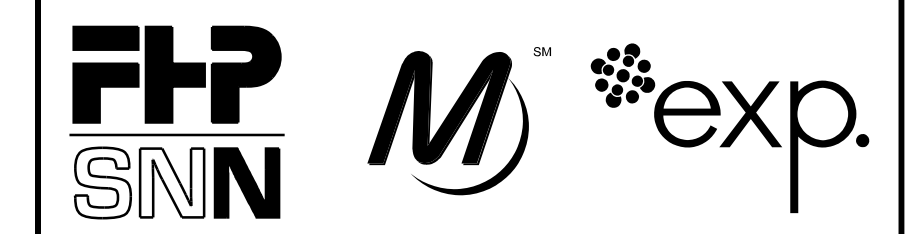
**SECTION A - CELLULAR CONCRETE**  
 (West Abutment - Looking South)  
 (East Abutment - Looking North)  
 Scale: N.T.S.

**NOTES:**

1. Cellular Concrete shall be placed in lifts not exceeding 3 Feet in thickness.

**LEGEND:**

	Structure Excavation
	Cellular Concrete



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 15 West 95th Street (South Terminal)  
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<b>DRAWN BY</b>	T.SLEDGE
<b>PROJECT NO.</b>	2012-0021
<b>FILE NAME</b>	File Name

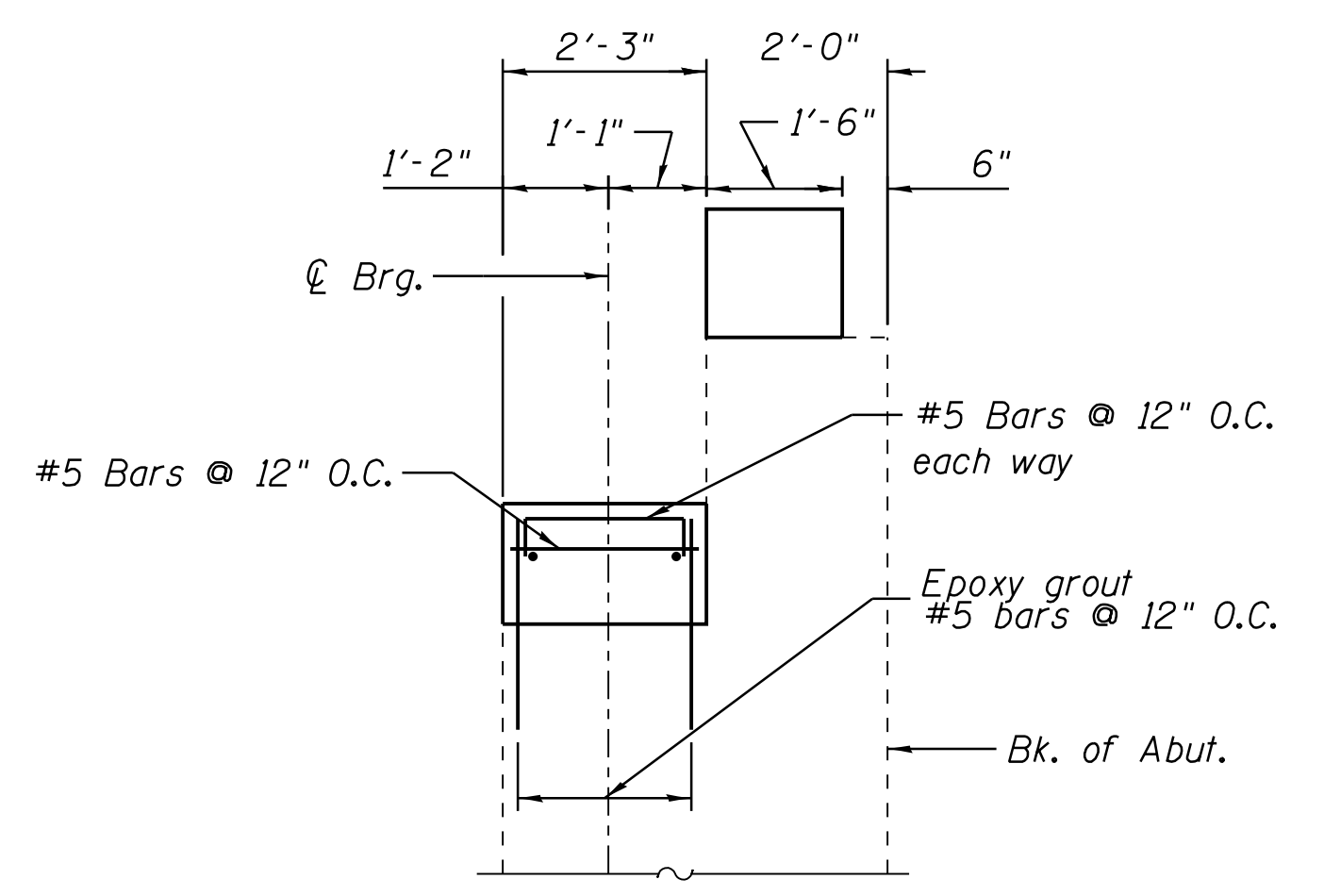
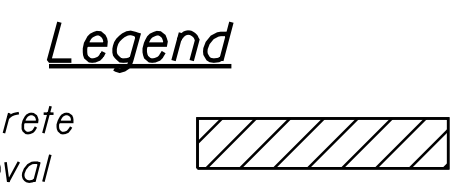
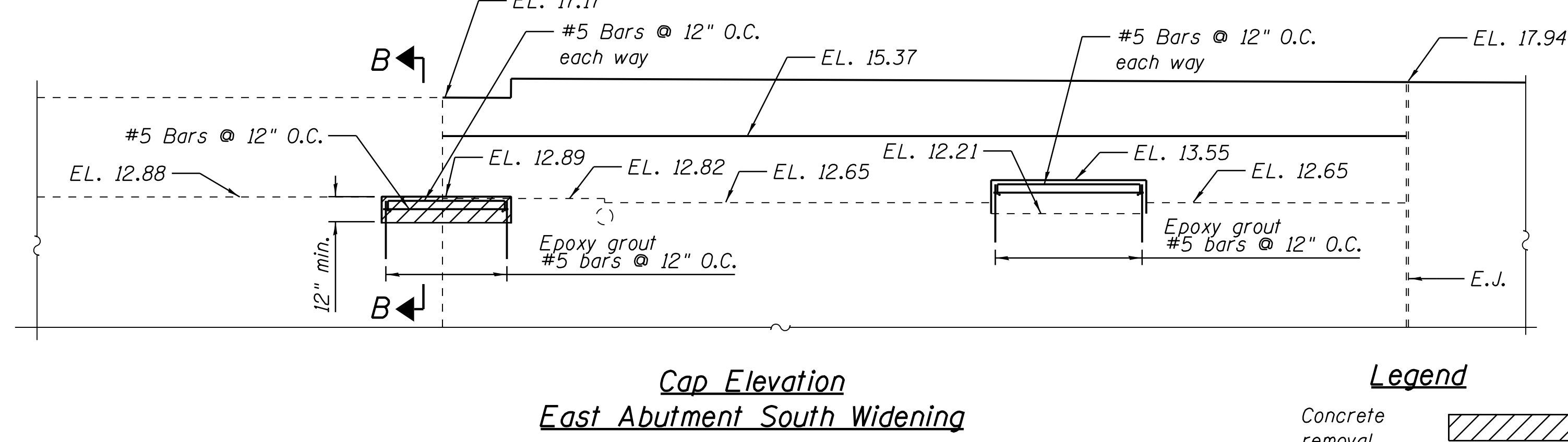
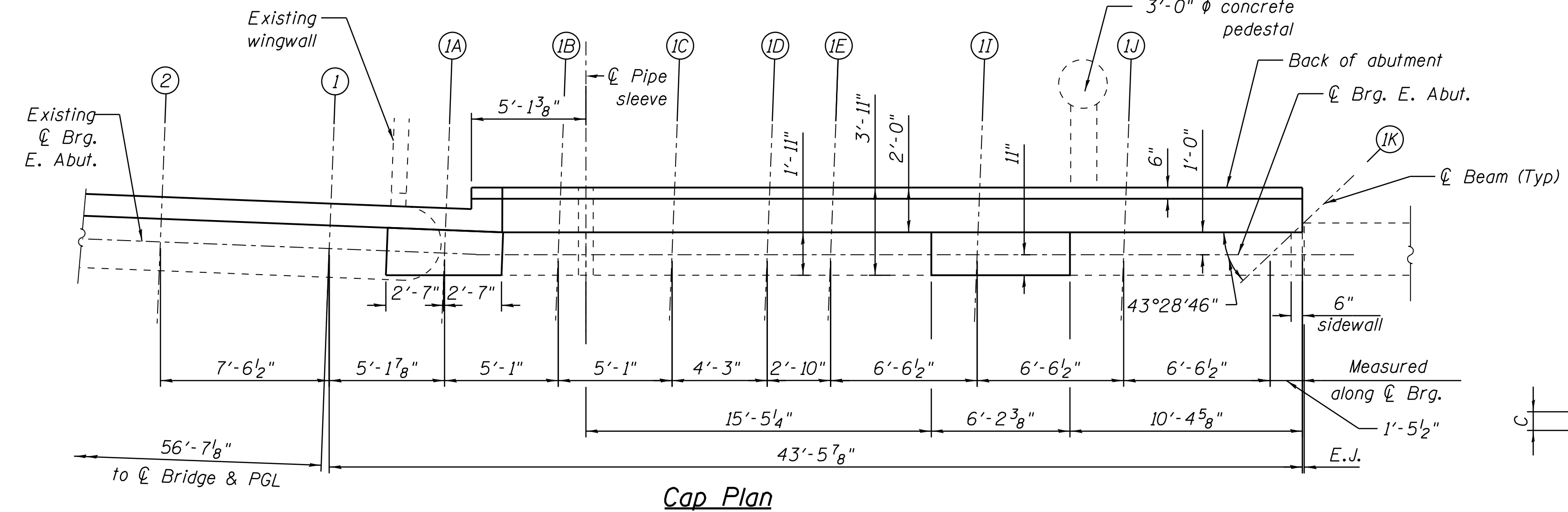
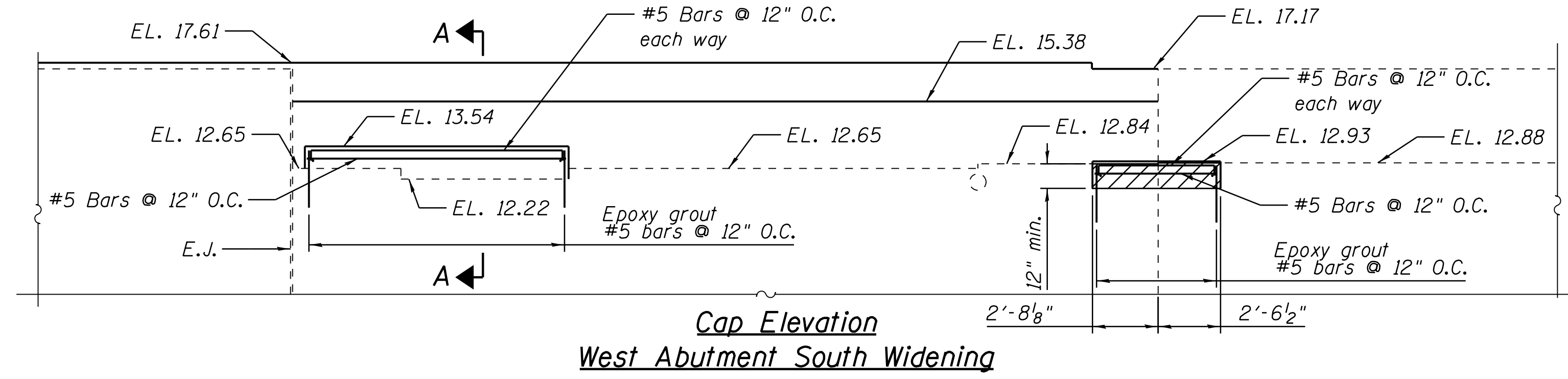
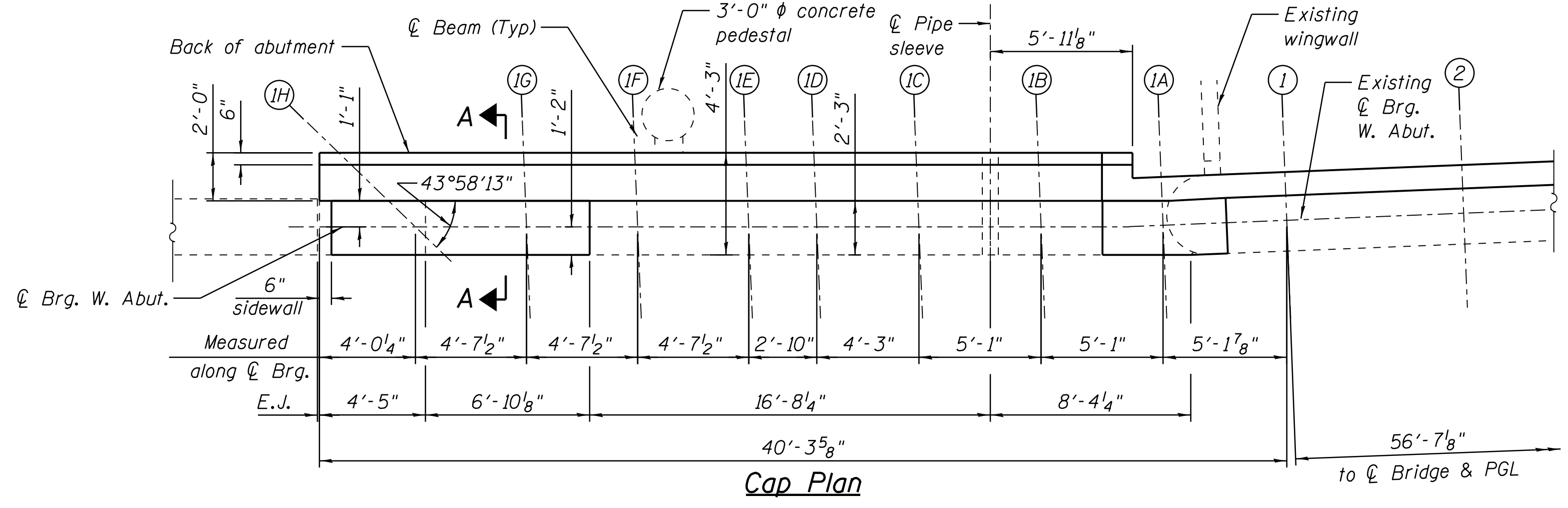
MARK	DATE	DESCRIPTION
	2/9/17	ISSUED FOR CONSTRUCTION

**LOCATION IDENTIFIER: DR095**  
 95TH ST. BRIDGE  
 EAST AND WEST EXISTING WALL  
 EXCAVATION & CELLULAR CONCRETE  
 SN 016-0437

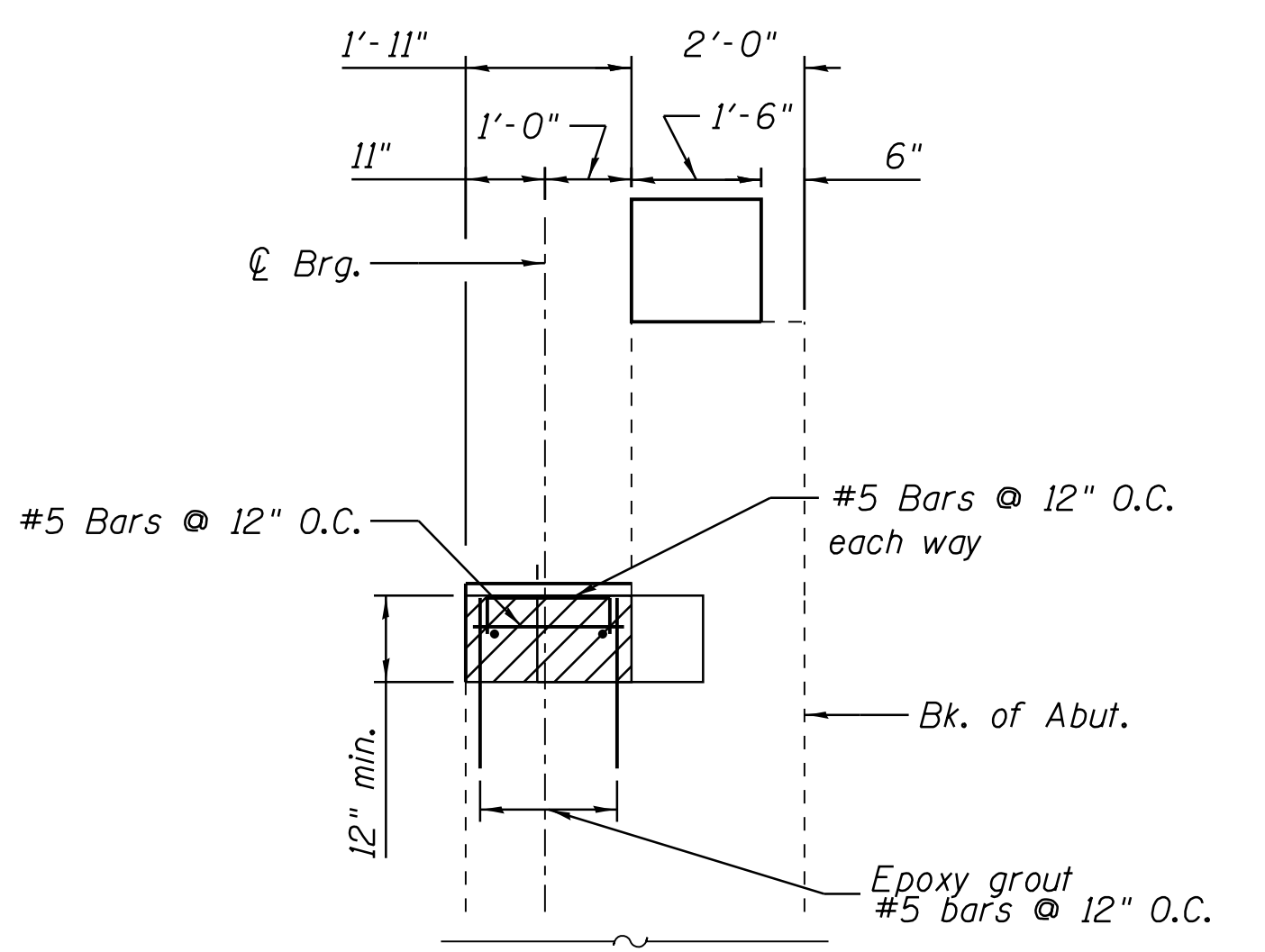
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 PLOT DATE = 1/23/2017

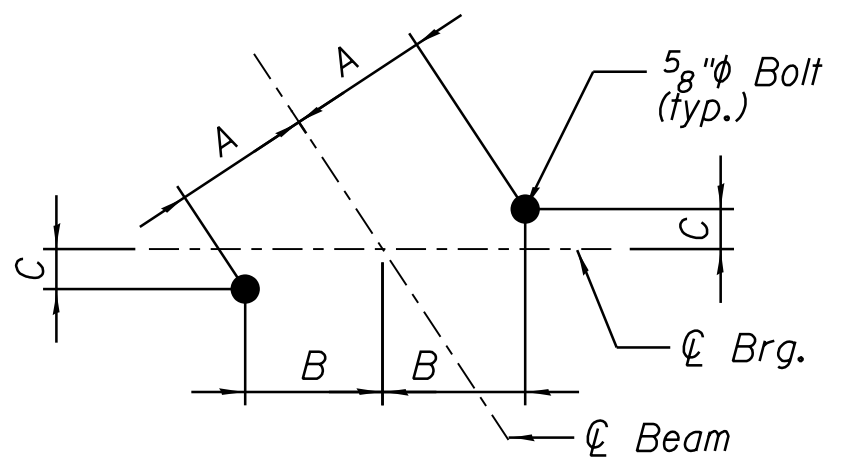
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Section A-A



Section B-B



Anchor bolt details

Beam	A	B	C
1A, 1B, 1E	8 7/8"	8 7/8"	0
1C, 1D	10 7/8"	10 7/8"	0
1F, 1G, 1I, 1J	7 7/8"	7 7/8"	0
1H	8 7/8"	5 7/8"	6 5/8"
1K	8 7/8"	6 1/8"	6 7/16"

- Notes:
- Existing reinforcement shall be cleaned and incorporated into the new construction in accordance with IDOT STD. Specification Section 501.05. Cost included with concrete removal.
  - Drilling and grouting of reinforcement into existing concrete shall be in accordance with IDOT Std Spec Article 584. Bars shall be embedded a minimum of 9". Bars shall maintain a minimum clearance of 4" from the face of concrete.



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PROJECT NO.	2012-0021
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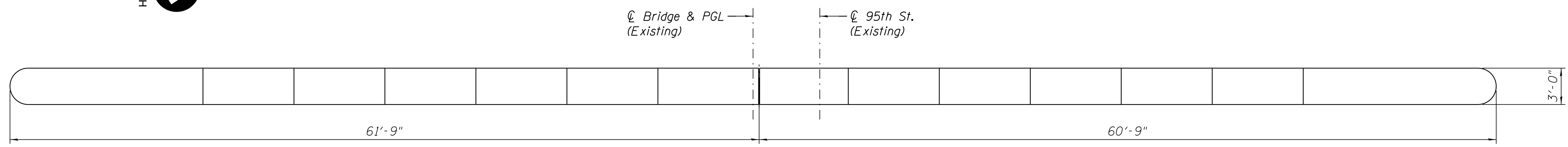
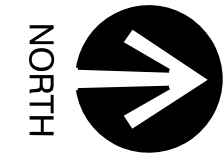
2/9/17	ISSUED FOR CONSTRUCTION	
MARK	DATE	DESCRIPTION

LOCATION IDENTIFIER: **DR095**  
 95TH ST. BRIDGE  
 EAST & WEST ABUTMENTS  
 SOUTH WIDENINGS  
 SN 016-0437

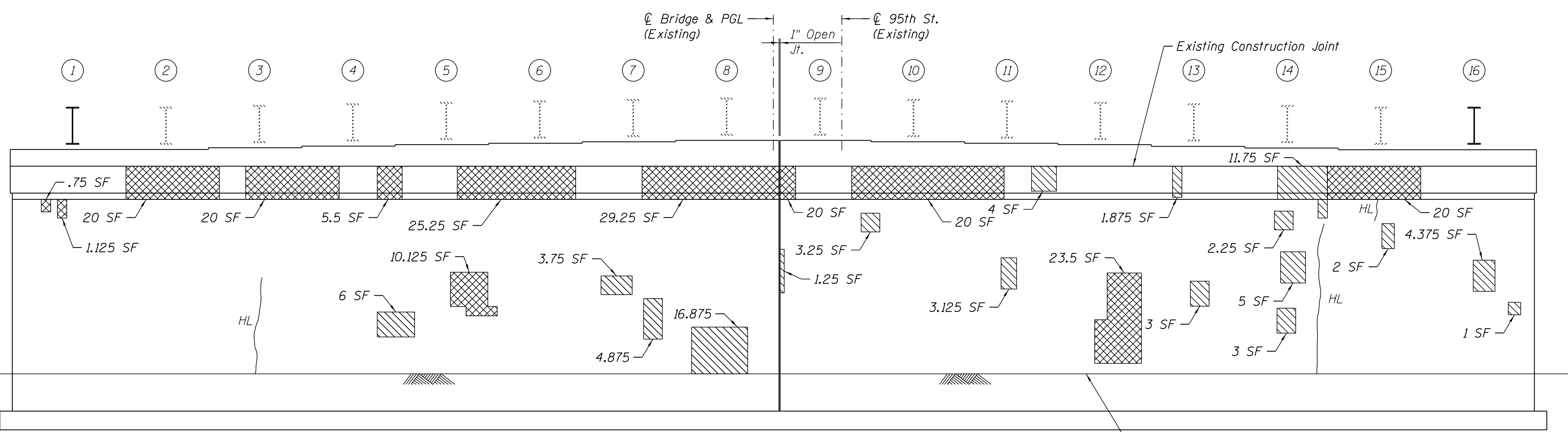
**95TH-S-140B**

FILE NAME = 070\_DR095\_S-140B.dgn  
 PLOT DATE = 8/31/2016

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PLAN



PIER 1 EAST ELEVATION  
(Looking West)

Note:

- 1. CTA electric ducts attached to the cap not shown for clarity.

Notes:

- 1. Waterproof coating shall be applied to all exposed surfaces after repairs are complete. See structural repair of concrete specification, section 03 74 00.
- 2. Areas of repair and lengths of sealant are approximate. Contractor shall verify areas and include a 10 percent increase in all quantities to allow for variations in plan areas against actual areas.

LEGEND

- Epoxy seal (Width > 0.06")
- Hairline Crack not to be sealed (Width < 0.06")
- ▨ Structural repair of concrete (Depth greater than 5 inches)
- ▨ Repair of concrete (Depth equal to or less than 5 inches)
- ▨ Finish Grade
- SF = Square Foot
- LF = Linear Foot



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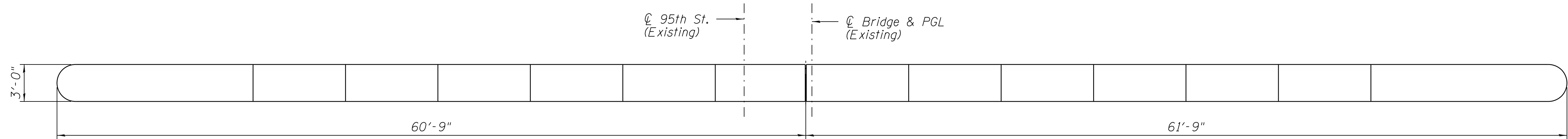
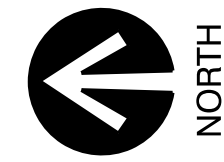
MARK	DATE	DESCRIPTION
	2/9/17	ISSUED FOR CONSTRUCTION

LOCATION IDENTIFIER: DR095  
95TH ST. BRIDGE  
PIER 1 REPAIR DETAILS  
(SHEET 1 OF 2)  
SN 016-0437

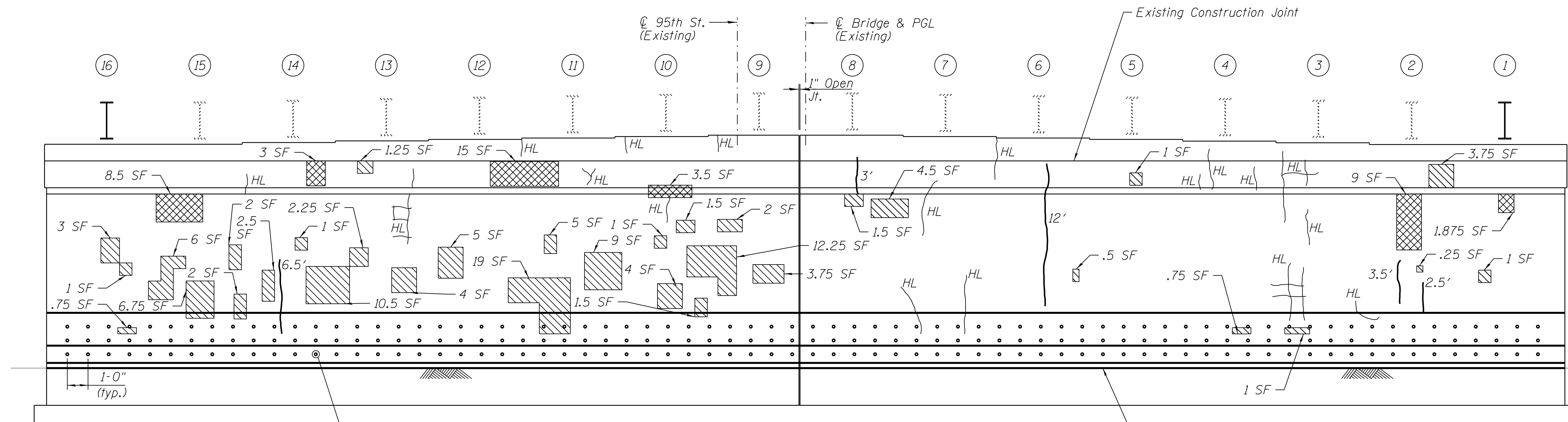
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FILE NAME = 070\_DR095\_S-144.dgn  
PLOT DATE = 8/31/2016

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PLAN



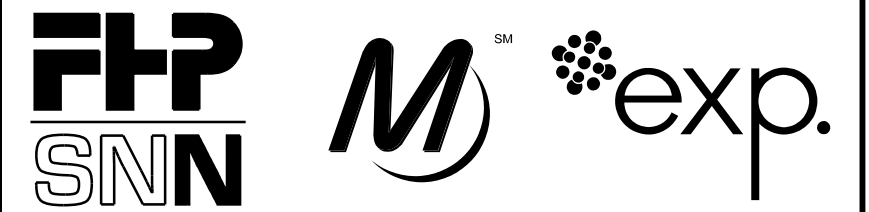
PIER 1 WEST ELEVATION  
(Looking East)

LEGEND

- Epoxy seal (Width > 0.06")
- Hairline Crack not to be sealed (Width < 0.06")
- Structural repair of concrete (Depth greater than 5 inches)
- Repair of concrete (Depth equal to or less than 5 inches)
- Finish Grade
- SF = Square Foot
- LF = Linear Foot

Notes:

1. Waterproof coating shall be applied to all exposed surfaces after repairs are complete. See structural repair of concrete specification, section 03 74 00.
2. Areas of repair and lengths of sealant are approximate. Contractor shall verify areas and include a 10 percent increase in all quantities to allow for variations in plan areas against actual areas.
3. See 95th-S-150, detail F, for barrier detail.



SENSITIVE SECURITY INFORMATION

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95th Terminal Improvement  
15 West 95th Street (South Terminal)  
Chicago, IL 60628

IN CHARGE S. KADERBEK

APPROVED BY D.ZROKA

CHECKED BY P.MAID

DESIGNED BY L.SOMMER

DRAWN BY T.SLEDGE

PROJECT NO. 2012-0021

FILE NAME File Name

MARK	DATE	DESCRIPTION
	2/9/17	ISSUED FOR CONSTRUCTION

LOCATION IDENTIFIER: DR095

95TH ST. BRIDGE

PIER 1 REPAIR DETAILS

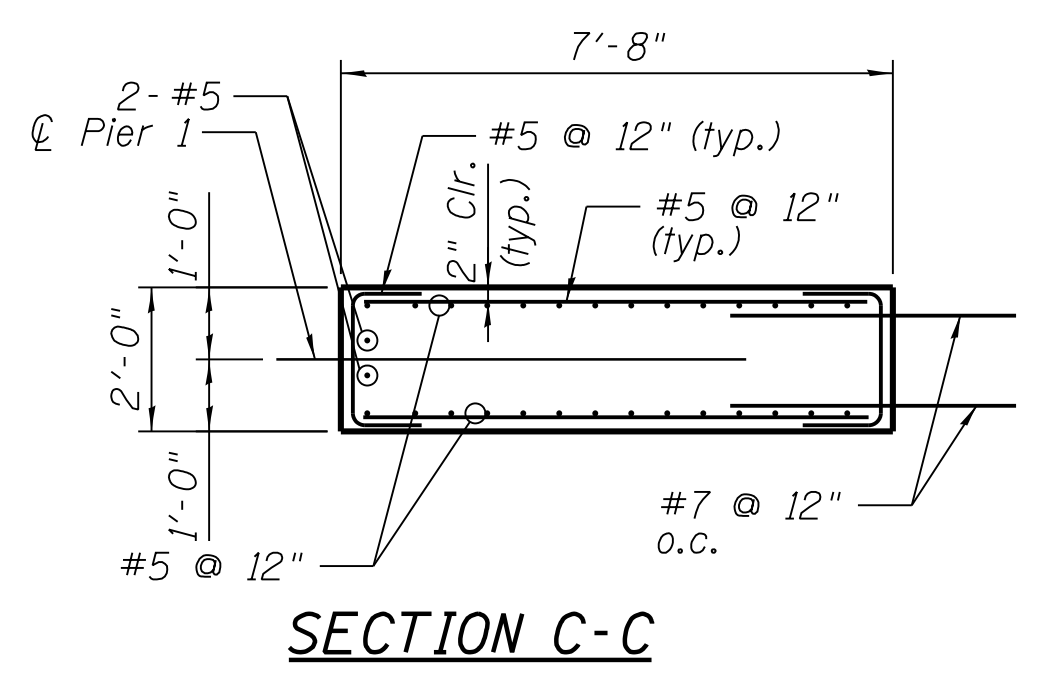
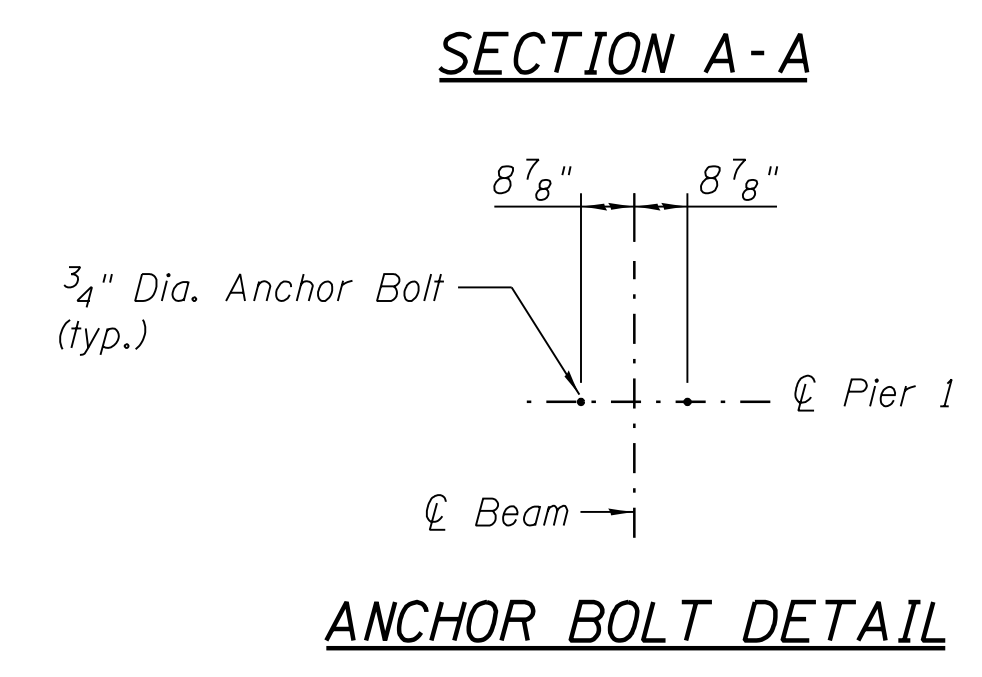
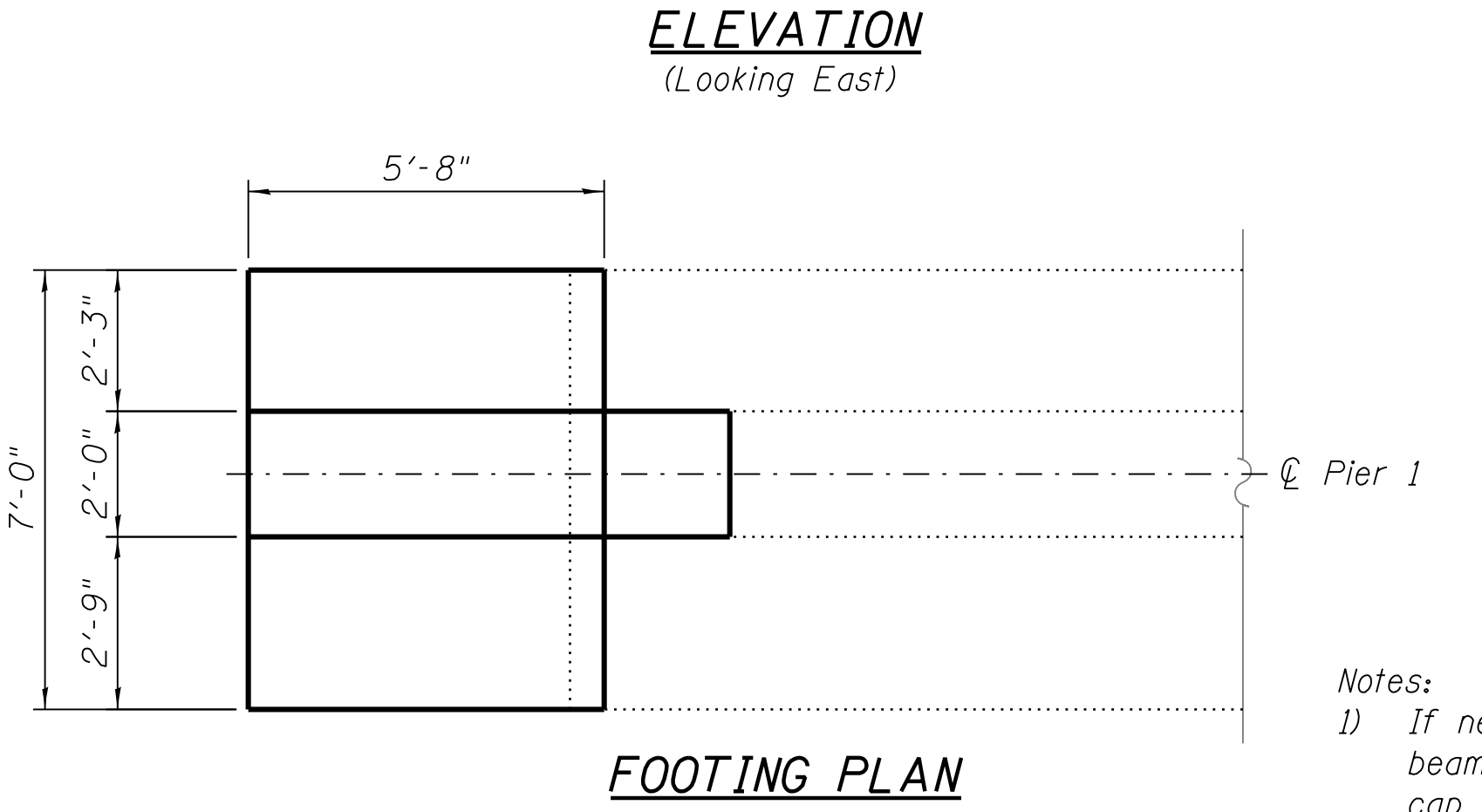
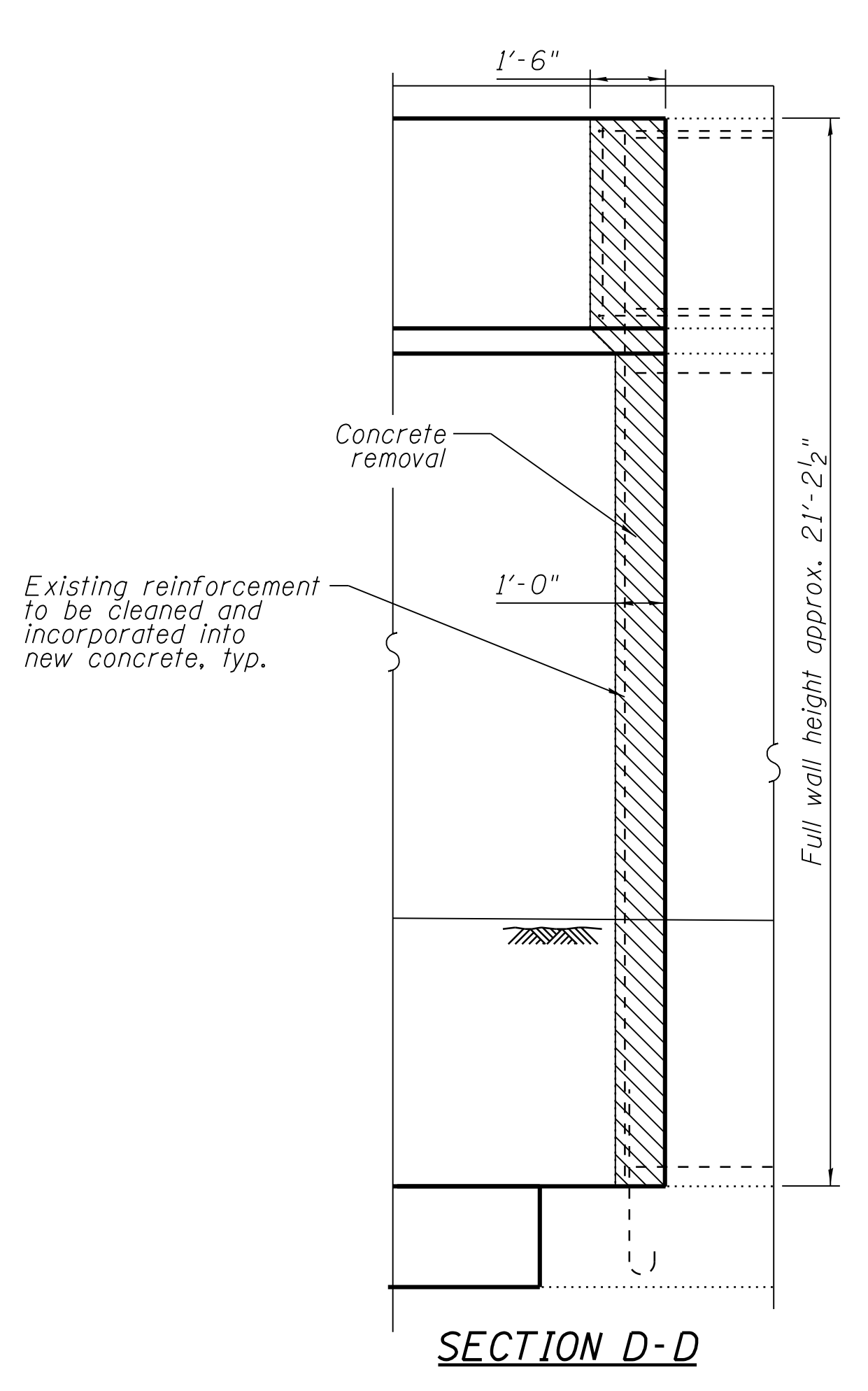
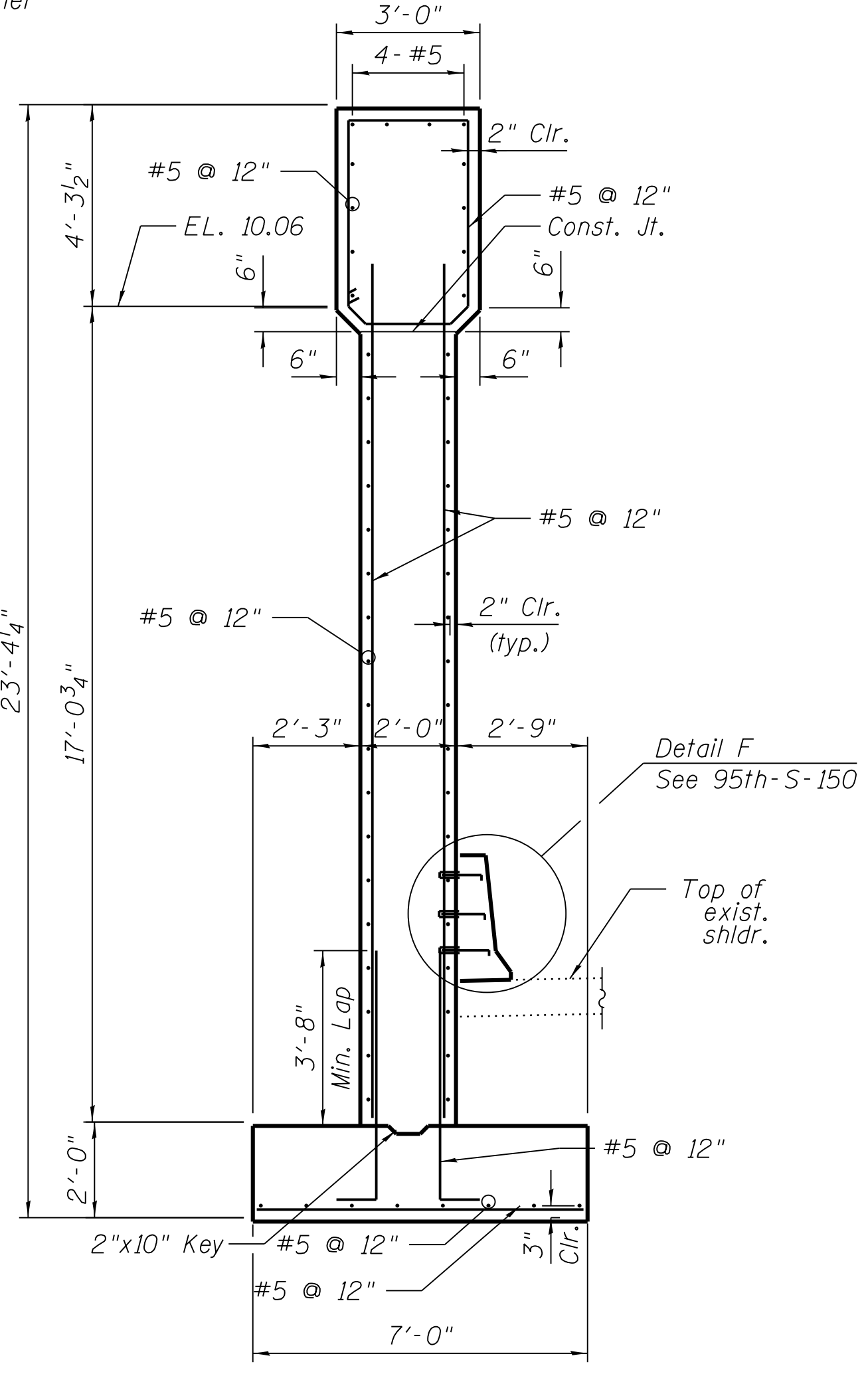
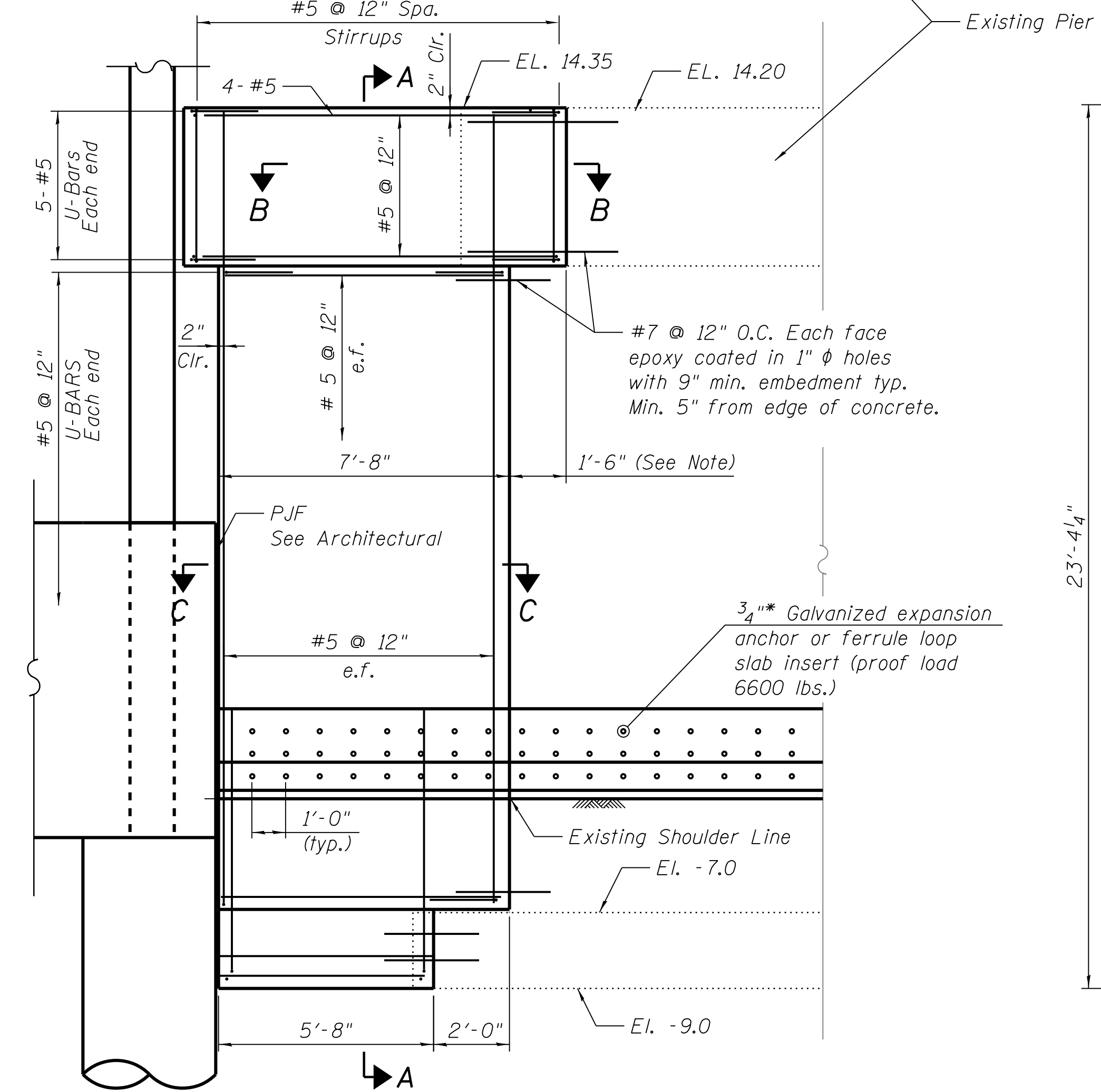
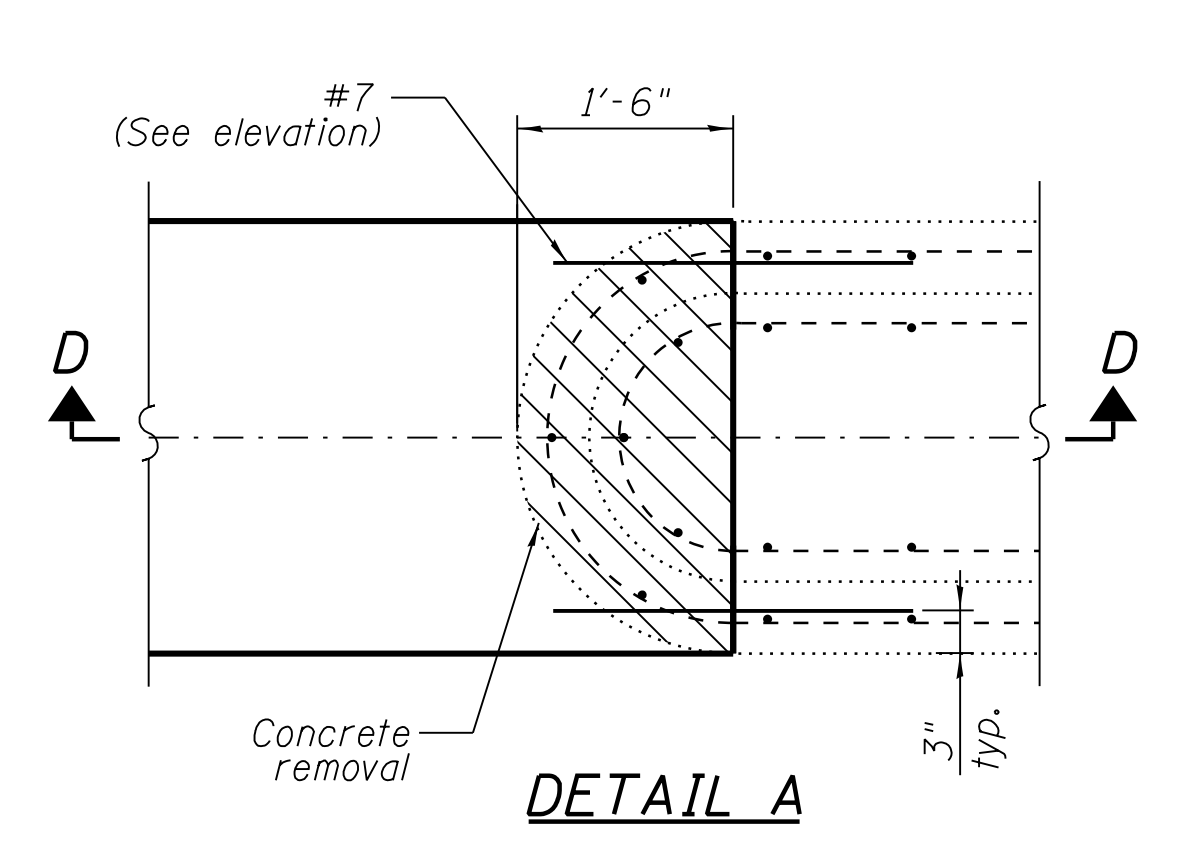
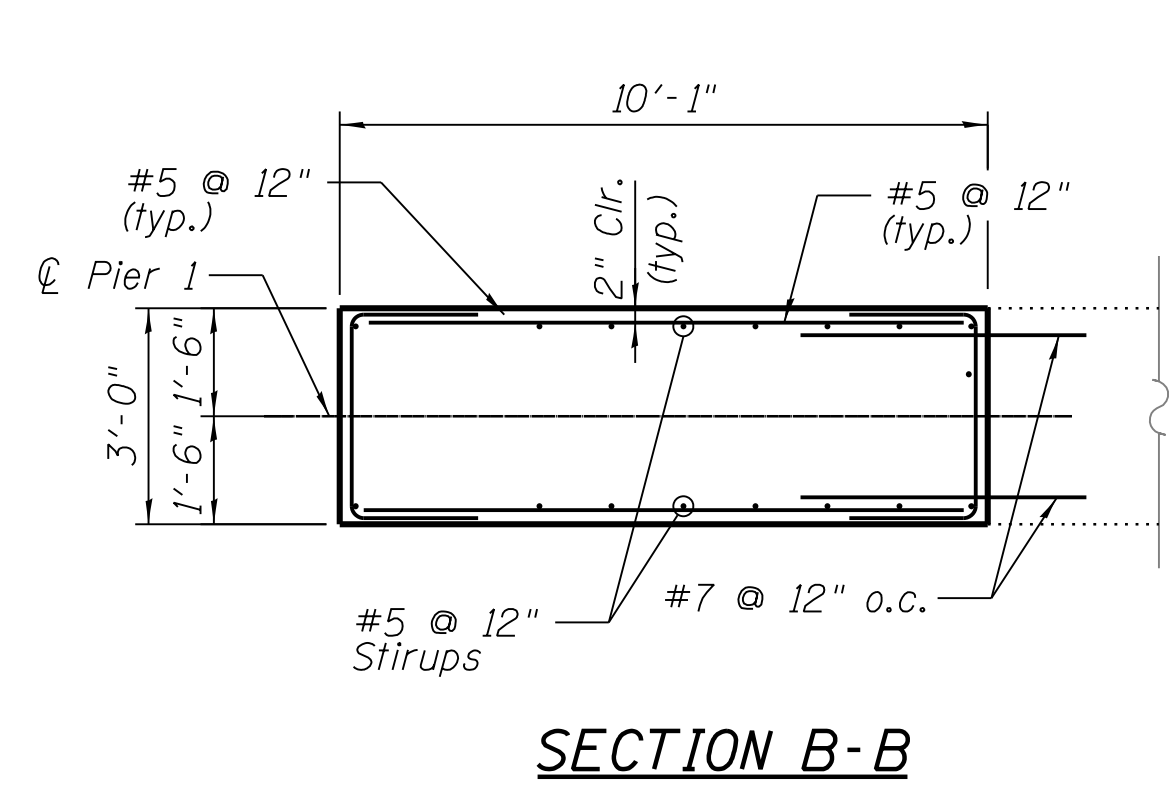
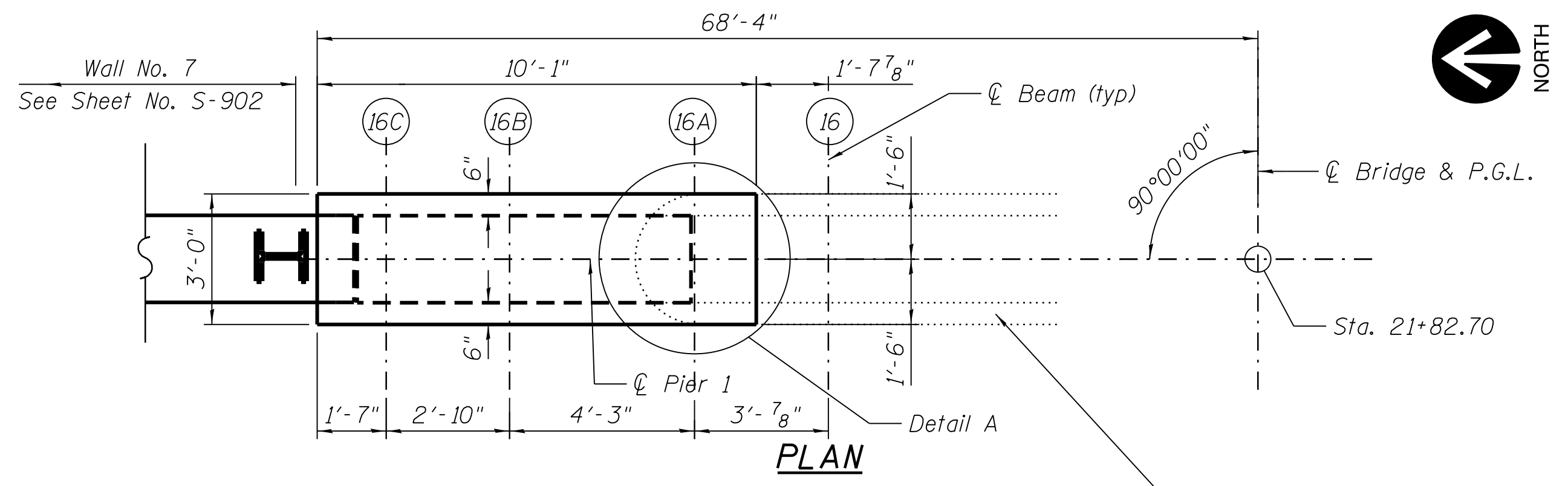
(SHEET 2 OF 2)

SN 016-0437

95TH-S-144A

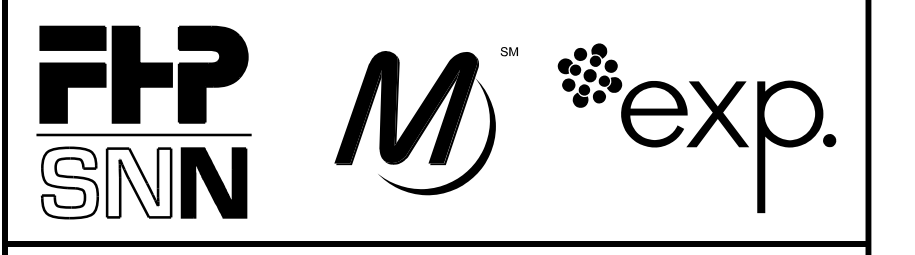
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PLOT DATE = 8/31/2016

vscarpa



- Notes:
- 1) If necessary, Contractor to support existing beam during removal of end of existing pier cap & construction of new cap.
  - 2) Contractor shall be responsible for retaining a licensed Structural Engineer in the state of Illinois to provide an SE sealed submittal for review and approval prior to implementation of support.  
 $R_{DL} = 79.8^k$        $R_{LL+1} = 68.3^k$
  - 3) Drilling and grouting of reinforcement into existing concrete shall be in accordance with IDOT Std Spec Article 584.

FILE NAME = 070\_DR095\_S-145.dgn  
 PLOT DATE = 8/31/2016



**CHICAGO TRANSIT AUTHORITY ENGINEERING**

**SENSITIVE SECURITY INFORMATION**  
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**95th Terminal Improvement**  
 15 West 95th Street (South Terminal)  
 Chicago, IL 60628

IN CHARGE	S. KADERBEK
APPROVED BY	D.ZROKA
CHECKED BY	P.MAID
DESIGNED BY	L.SOMMER
DRAWN BY	T.SLEDGE
PROJECT NO.	2012-0021
FILE NAME	File Name

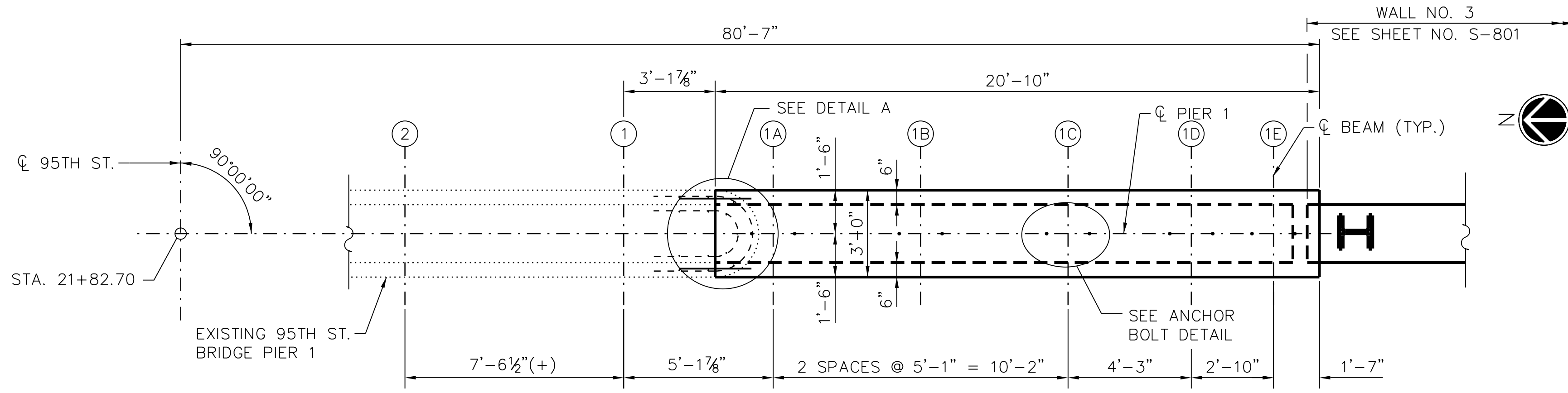
MARK	DATE	DESCRIPTION
	2/9/17	ISSUED FOR CONSTRUCTION

LOCATION IDENTIFIER: **DR095**

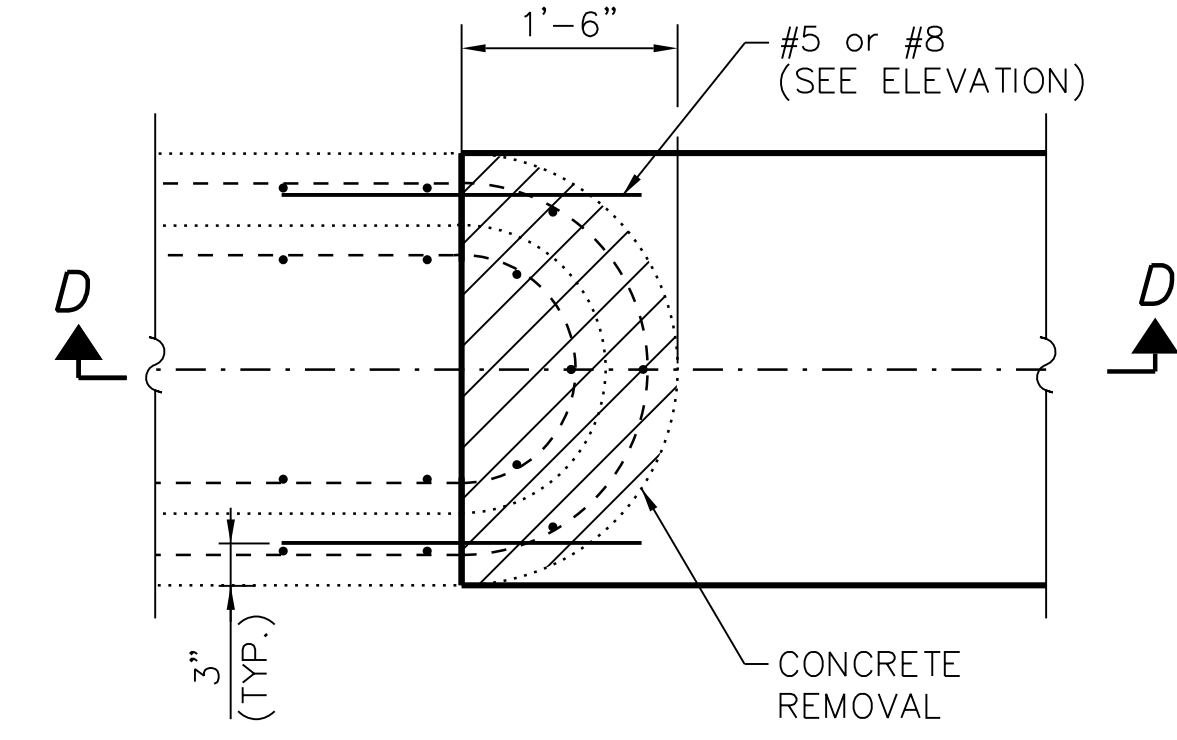
**95TH ST. BRIDGE  
 PIER 1 NORTH WIDENING  
 SN 016-0437**

**95TH-S-145**

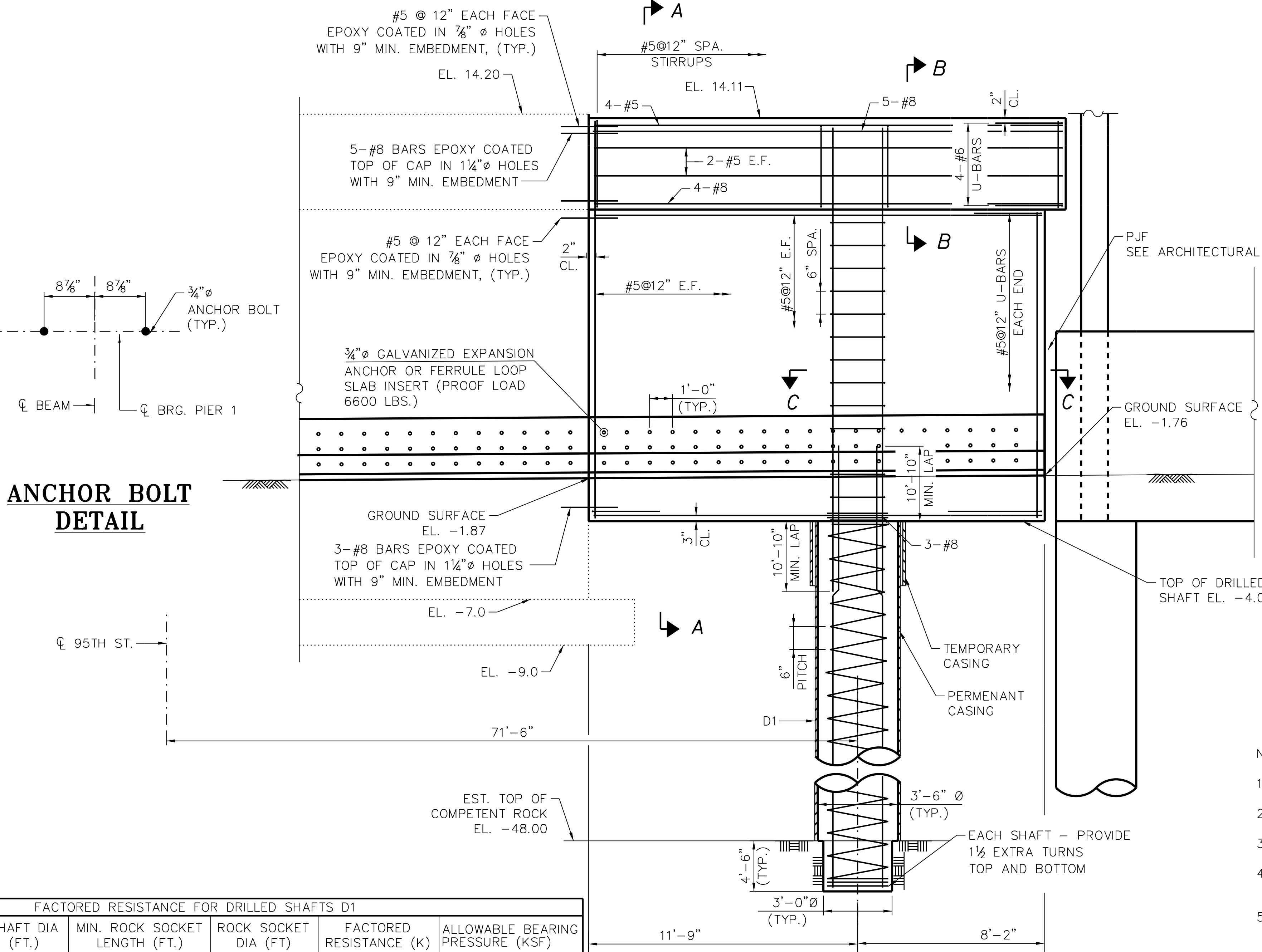
vscorppa



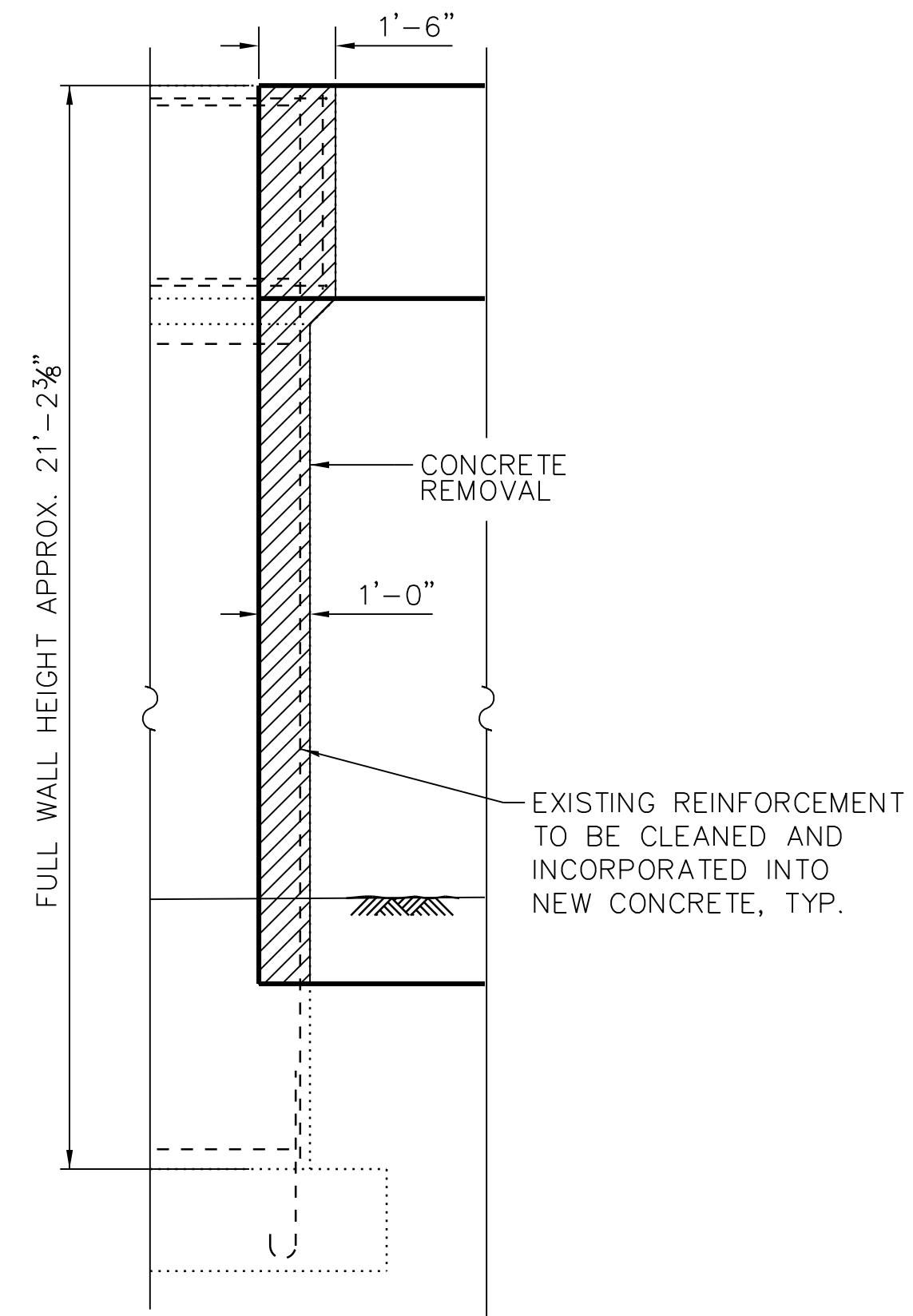
**PLAN**



**DETAIL A**



**ELEVATION**  
(LOOKING EAST)



**SECTION D-D**

**ANCHOR BOLT DETAIL**

FACTORED RESISTANCE FOR DRILLED SHAFTS D1				
SHAFT DIA (FT.)	MIN. ROCK SOCKET LENGTH (FT.)	ROCK SOCKET DIA (FT)	FACTORED RESISTANCE (K)	ALLOWABLE BEARING PRESSURE (KSF)
3'-6"	4'-6" (MIN.)	3'-0"	2400	340

**NOTES:**

- POUR STEPS MONOLITHICALLY WITH CAP.
- SPACE REINFORCEMENT TO MISS ANCHOR BOLTS.
- FOR DRILLED SHAFT LOCATIONS, SEE SHEET NO. 95TH-S-103.
- FOR DRILLED SHAFT DETAILS, PIER SECTION, AND REINFORCEMENT DETAILS, SEE SHEET NO. 95TH-S-150.
- DRILLING AND GROUTING OF REINFORCEMENT INTO EXISTING CONCRETE SHALL BE IN ACCORDANCE WITH IDOT STD SPEC ARTICLE 584.



**CHICAGO TRANSIT AUTHORITY ENGINEERING**

**SENSITIVE SECURITY INFORMATION**  
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**95th Terminal Improvement**  
14 West 95th Street (North Terminal)  
15 West 95th Street (South Terminal)  
Chicago, IL 60628

IN CHARGE	S. KADERBEK
APPROVED BY	D.ZROKA
CHECKED BY	P.MAID
DESIGNED BY	L.SOMMER
DRAWN BY	T.SLEDGE
PROJECT NO.	2012-0021
FILE NAME	File Name

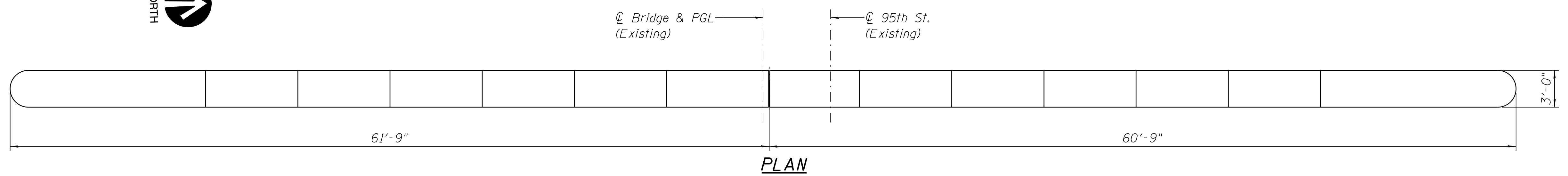
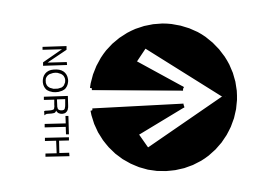
MARK	DATE	DESCRIPTION
	10/06/16	ISSUED FOR CONSTRUCTION

LOCATION IDENTIFIER: **DR095**  
**95TH ST. BRIDGE**  
**PIER 1 SOUTH WIDENING**  
**SN 016-0437**

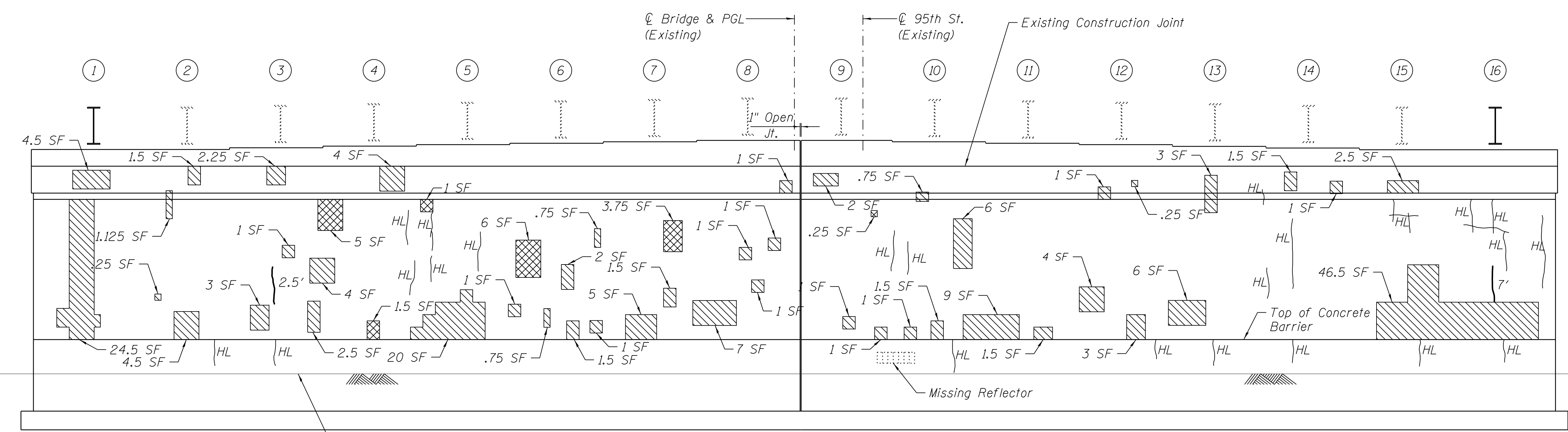
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FILE NAME = 070\_DR095\_S-146.dgn  
PLOT DATE = 10/7/2016

vscorpp



**PLAN**



**PIER 2 EAST ELEVATION**  
(Looking West)

**Notes:**

1. Waterproof coating shall be applied to all exposed surfaces after repairs are complete. See structural repair of concrete specification, section 03 74 00.
2. Areas of repair and lengths of sealant are approximate. Contractor shall verify areas and include a 10 percent increase in all quantities to allow for variations in plan areas against actual areas.

**LEGEND**

- Epoxy seal (Width > 0.06")
- Hairline Crack not to be sealed (Width < 0.06")
- ▨ Structural repair of concrete (Depth greater than 5 inches)
- ▨ Repair of concrete (Depth equal to or less than 5 inches)
- ▨ Finish Grade
- SF = Square Foot
- LF = Linear Foot



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**95th Terminal Improvement**  
15 West 95th Street (South Terminal)  
Chicago, IL 60628

IN CHARGE	S. KADERBEK
APPROVED BY	D.ZROKA
CHECKED BY	P.MAID
DESIGNED BY	L.SOMMER
DRAWN BY	T.SLEDGE
PROJECT NO.	2012-0021
FILE NAME	File Name

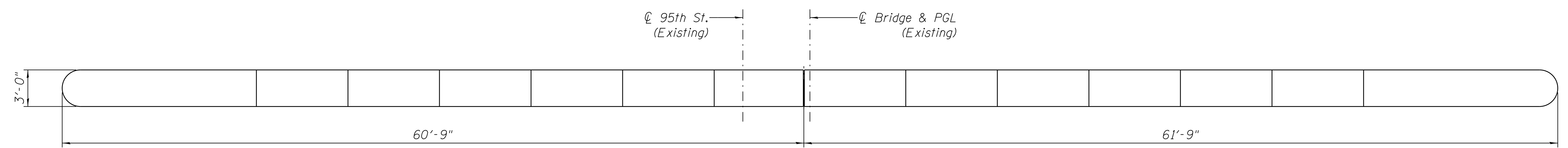
MARK	DATE	DESCRIPTION
	2/9/17	ISSUED FOR CONSTRUCTION

LOCATION IDENTIFIER: **DR095**  
95TH ST. BRIDGE  
PIER 2 REPAIR DETAILS  
(SHEET 1 OF 2)  
SN 016-0437

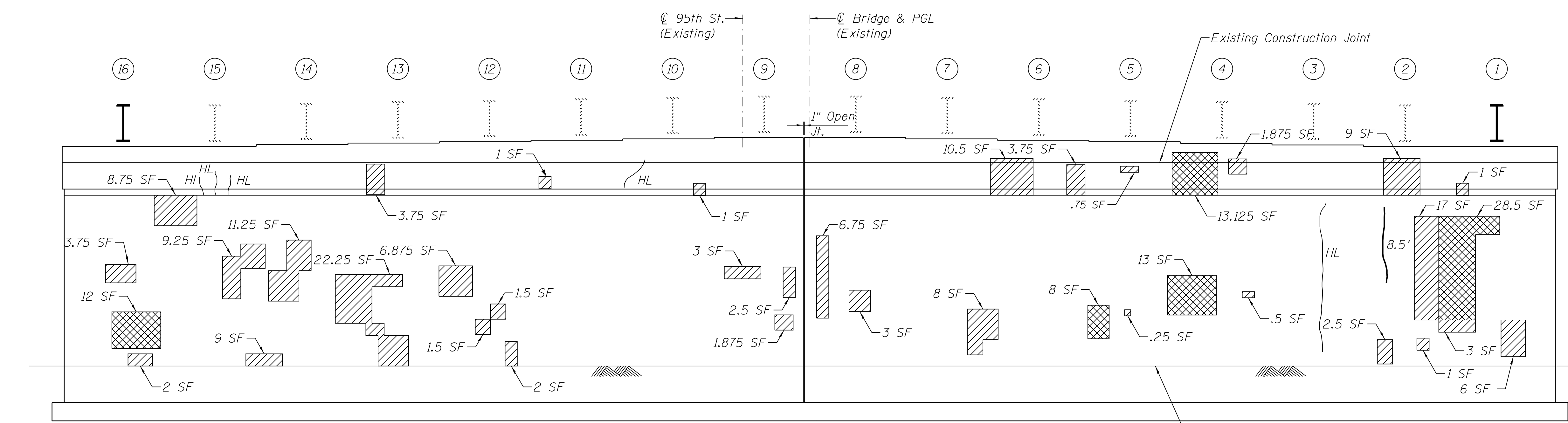
**95TH-S-147**

FILE NAME = 070\_DR095\_S-147.dgn  
PLOT DATE = 8/31/2016

vscarpa



**PIER 2 WEST ELEVATION**



**PIER 2 WEST ELEVATION**  
(Looking East)

- Notes:
- Waterproof coating shall be applied to all exposed surfaces after repairs are complete. See structural repair of concrete specification, section 03 74 00.
  - Areas of repair and lengths of sealant are approximate. Contractor shall verify areas and include a 10 percent increase in all quantities to allow for variations in plan areas against actual areas.

**LEGEND**

- Epoxy seal (Width > 0.06")
- Hairline Crack not to be sealed (Width < 0.06")
- Structural repair of concrete (Depth greater than 5 inches)
- Repair of concrete (Depth equal to or less than 5 inches)
- Finish Grade
- SF = Square Foot
- LF = Linear Foot



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**95th Terminal Improvement**  
15 West 95th Street (South Terminal)  
Chicago, IL 60628

IN CHARGE	S. KADERBEK
APPROVED BY	D.ZROKA
CHECKED BY	P.MAID
DESIGNED BY	L.SOMMER
DRAWN BY	T.SLEDGE
PROJECT NO.	2012-0021
FILE NAME	File Name

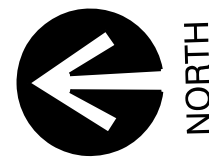
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	2/9/17	ISSUED FOR CONSTRUCTION

LOCATION IDENTIFIER: **DR095**  
95TH ST. BRIDGE  
PIER 2 REPAIR DETAILS  
(SHEET 2 OF 2)  
SN 016-0437

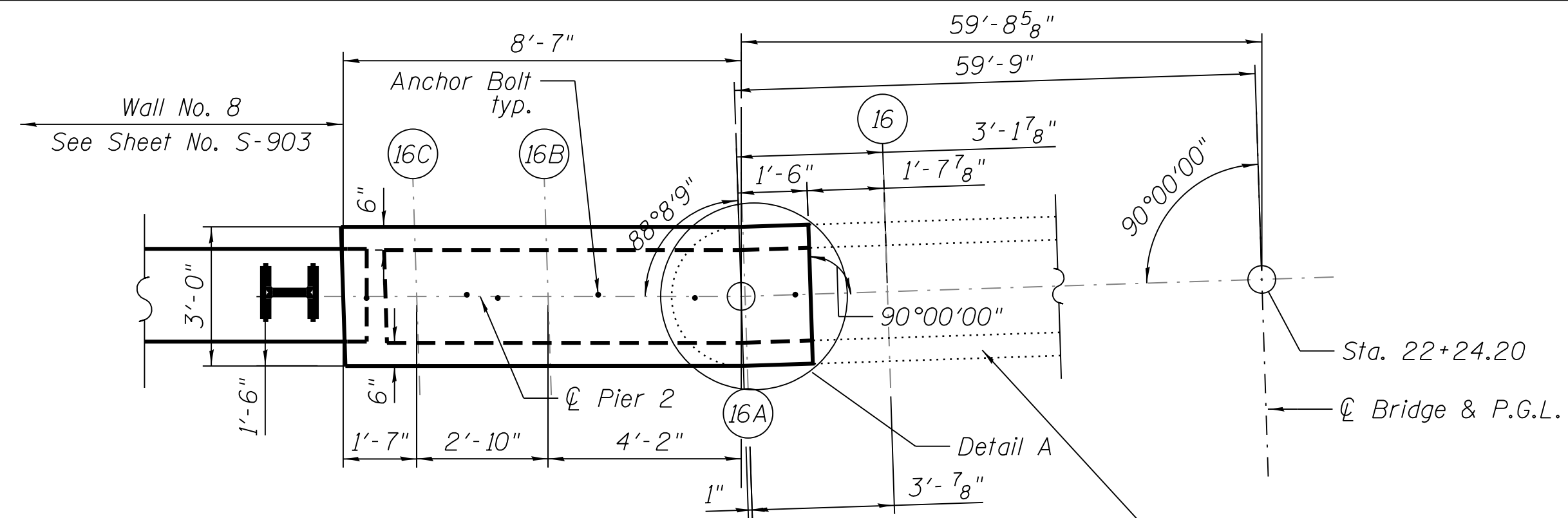
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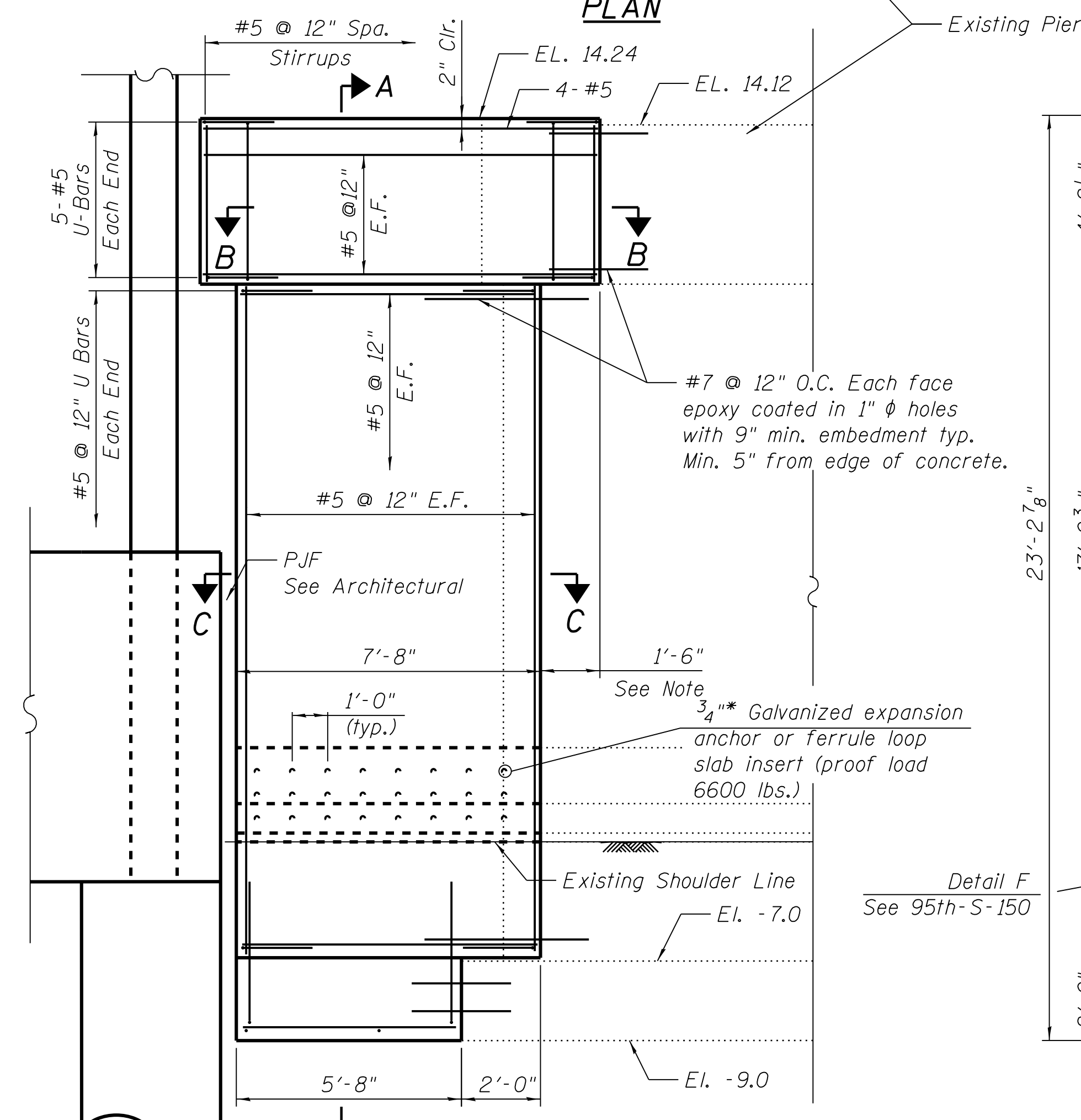




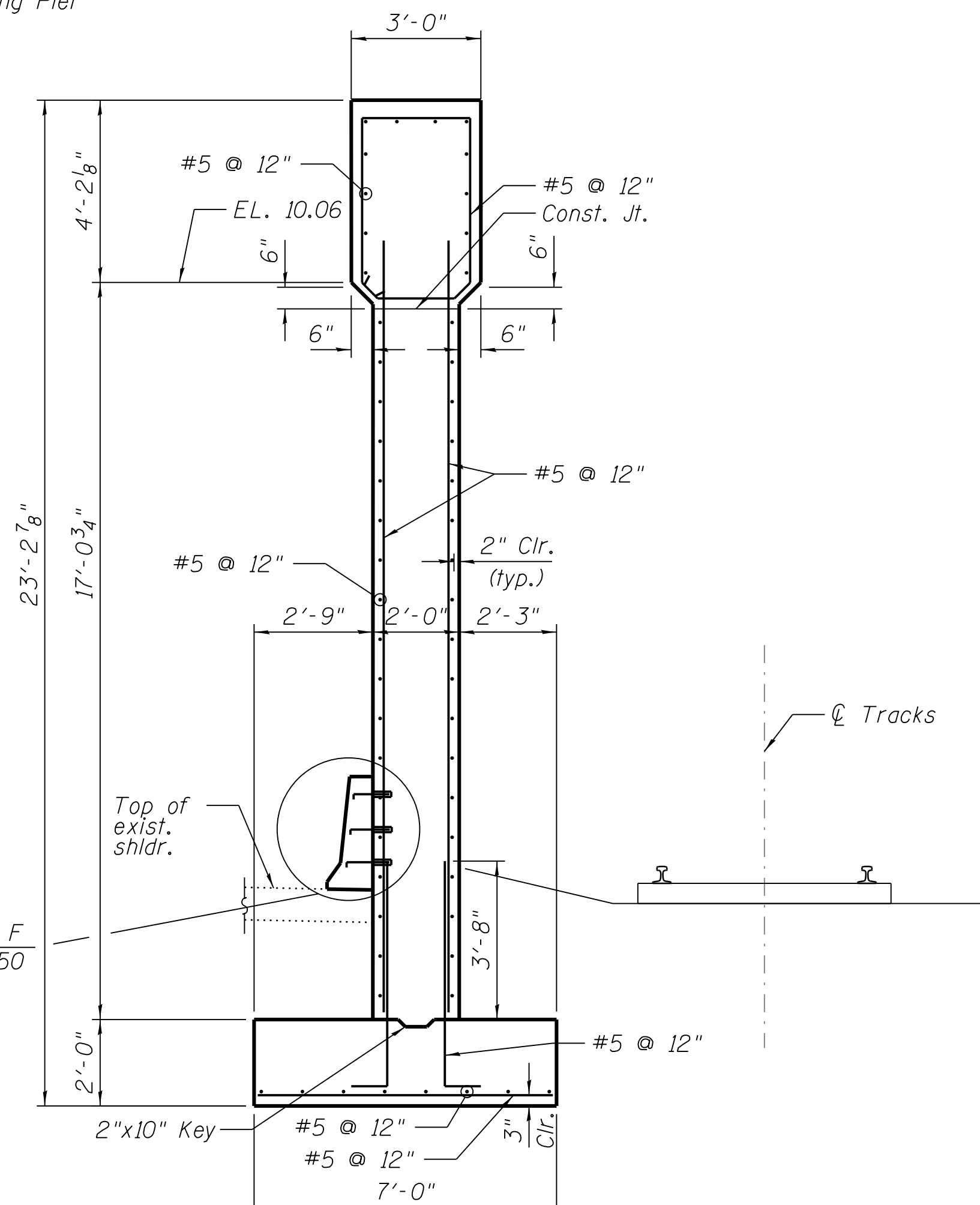
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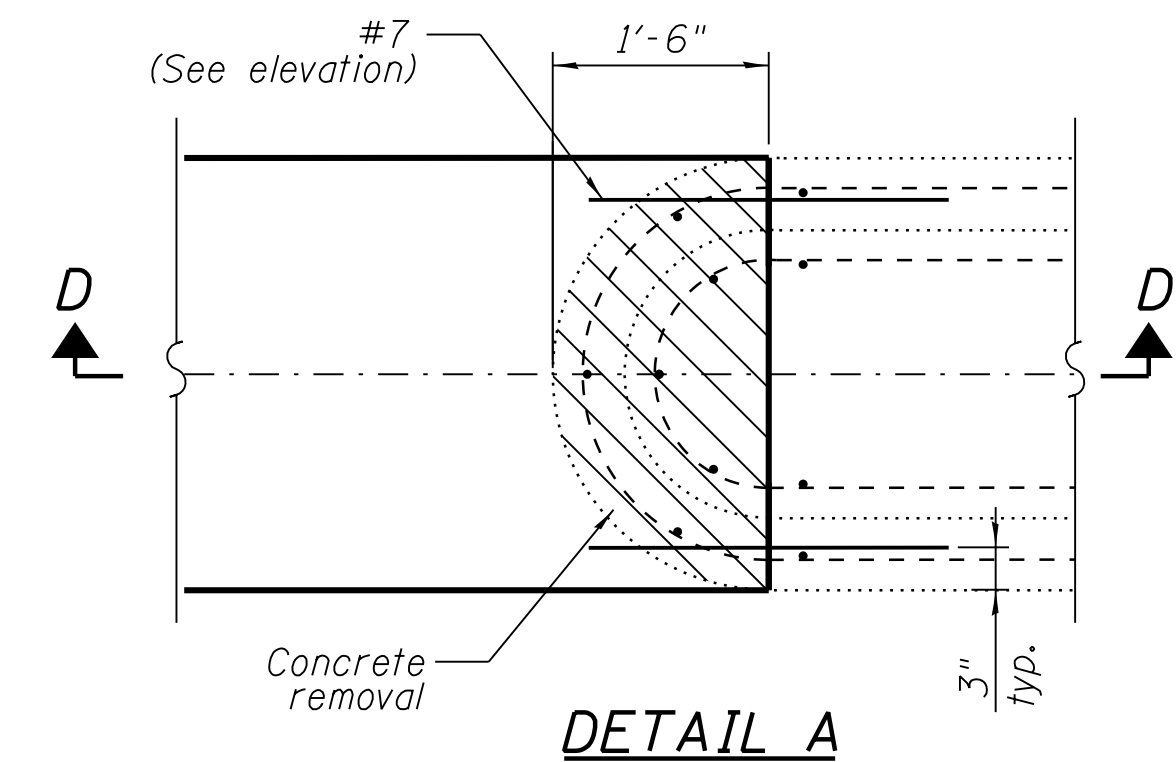
PLAN



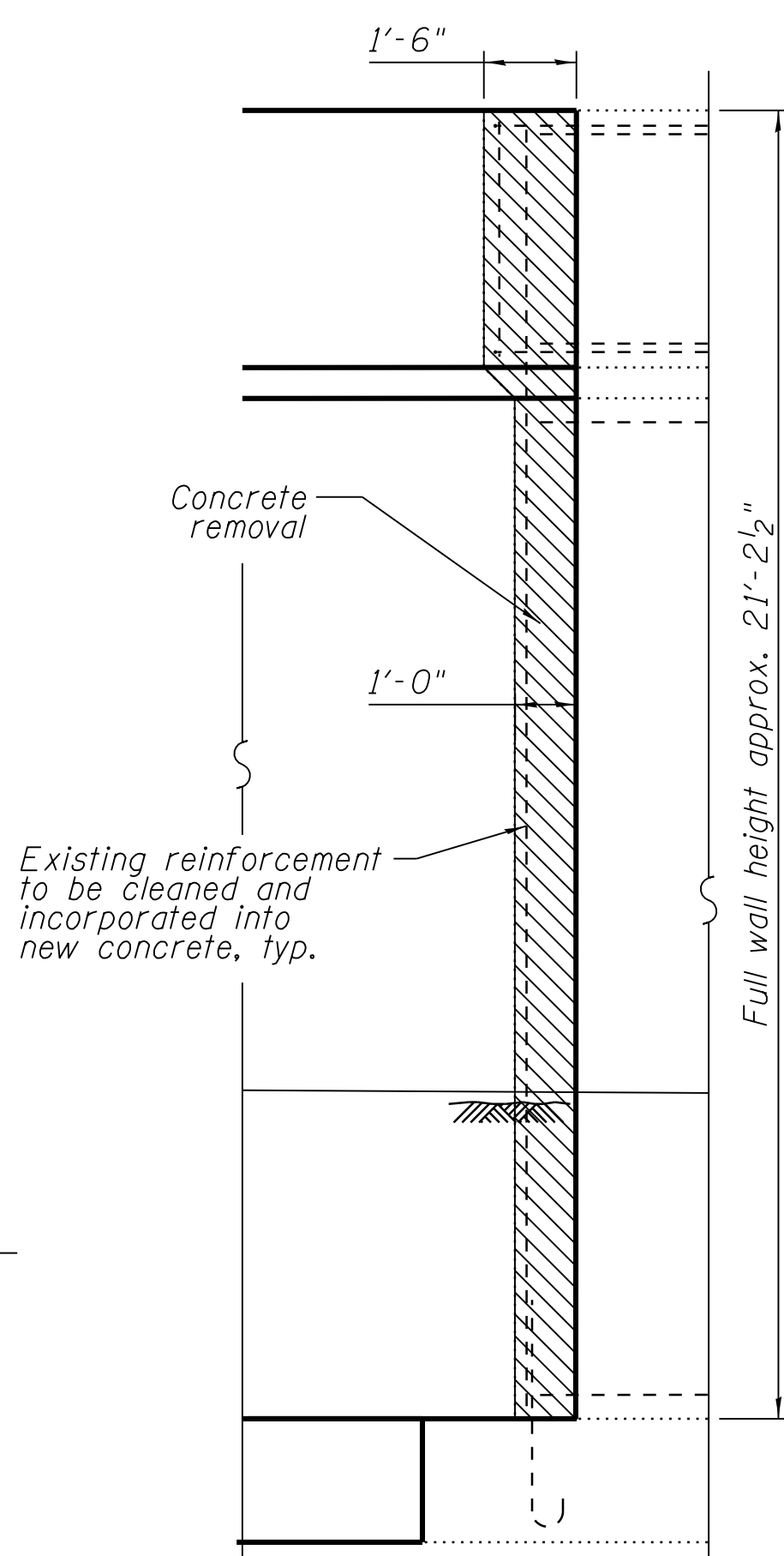
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(Looking East)



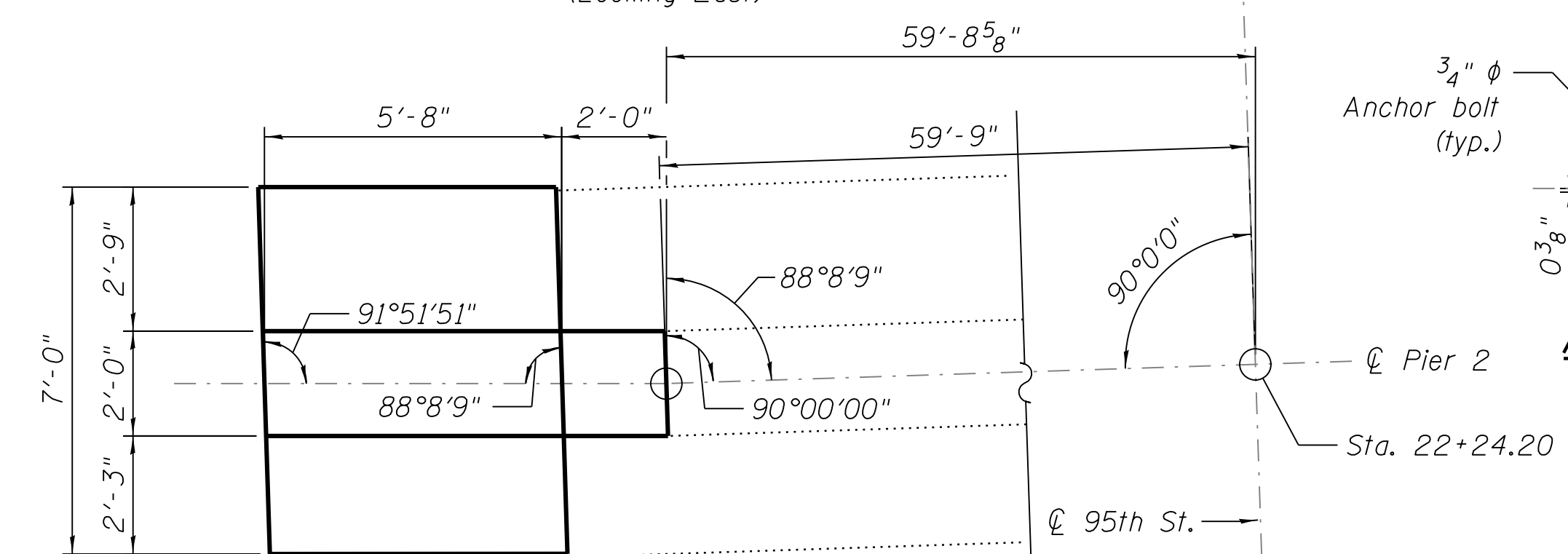
SECTION A-A



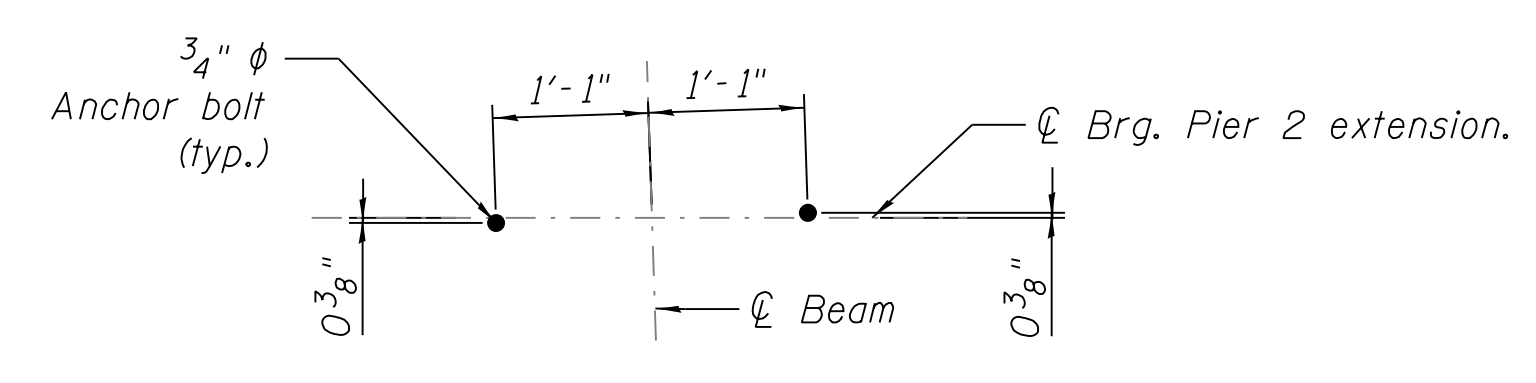
DETAIL A



SECTION D-D



FOOTING PLAN



ANCHOR BOLT DETAIL

- Note:
- 1) If necessary, Contractor to support existing beam during removal of end of existing pier cap & construction of new cap.
  - 2) See sheet S-145 for Cap Section B-B and C-C.
  - 3) Contractor shall be responsible for retaining a licensed Structural Engineer in the state of Illinois to provide an SE sealed submittal for review and approval prior to implementation of support.  
 $R_{DL} = 79.8^k$   $R_{LL+1} = 68.3^k$
  - 4) Drilling and grouting of reinforcement into existing concrete shall be in accordance with IDOT Std Spec Article 584.



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**95th Terminal Improvement**  
15 West 95th Street (South Terminal)  
Chicago, IL 60628

IN CHARGE	S. KADERBEK
APPROVED BY	D.ZROKA
CHECKED BY	P.MAID
DESIGNED BY	L.SOMMER
DRAWN BY	T.SLEDGE
PROJECT NO.	2012-0021
FILE NAME	File Name

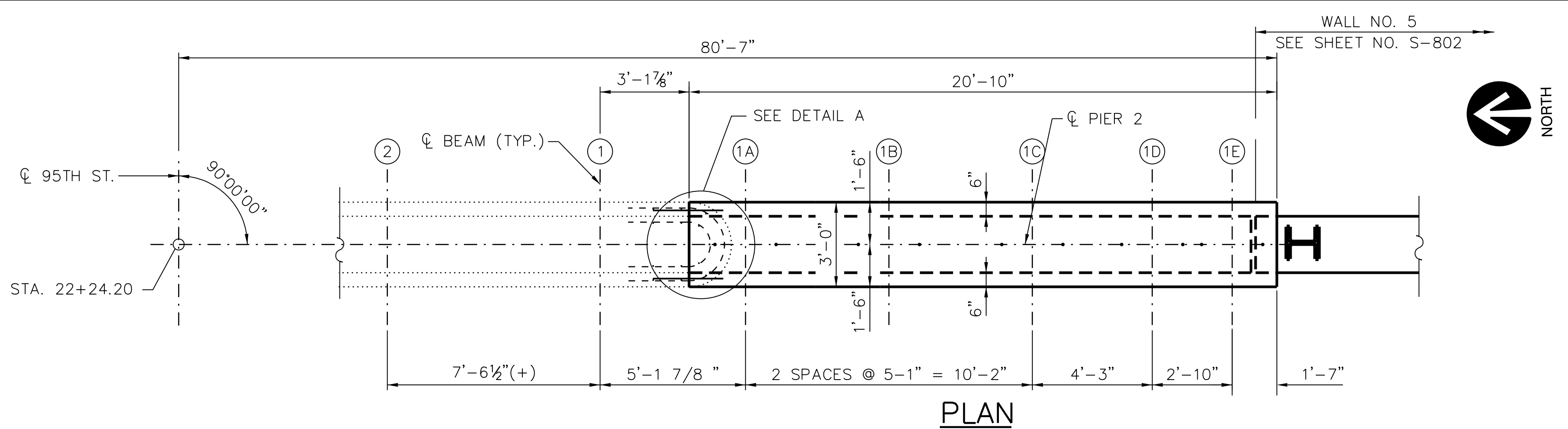
MARK	DATE	DESCRIPTION
	2/9/17	ISSUED FOR CONSTRUCTION

LOCATION IDENTIFIER: **DR095**

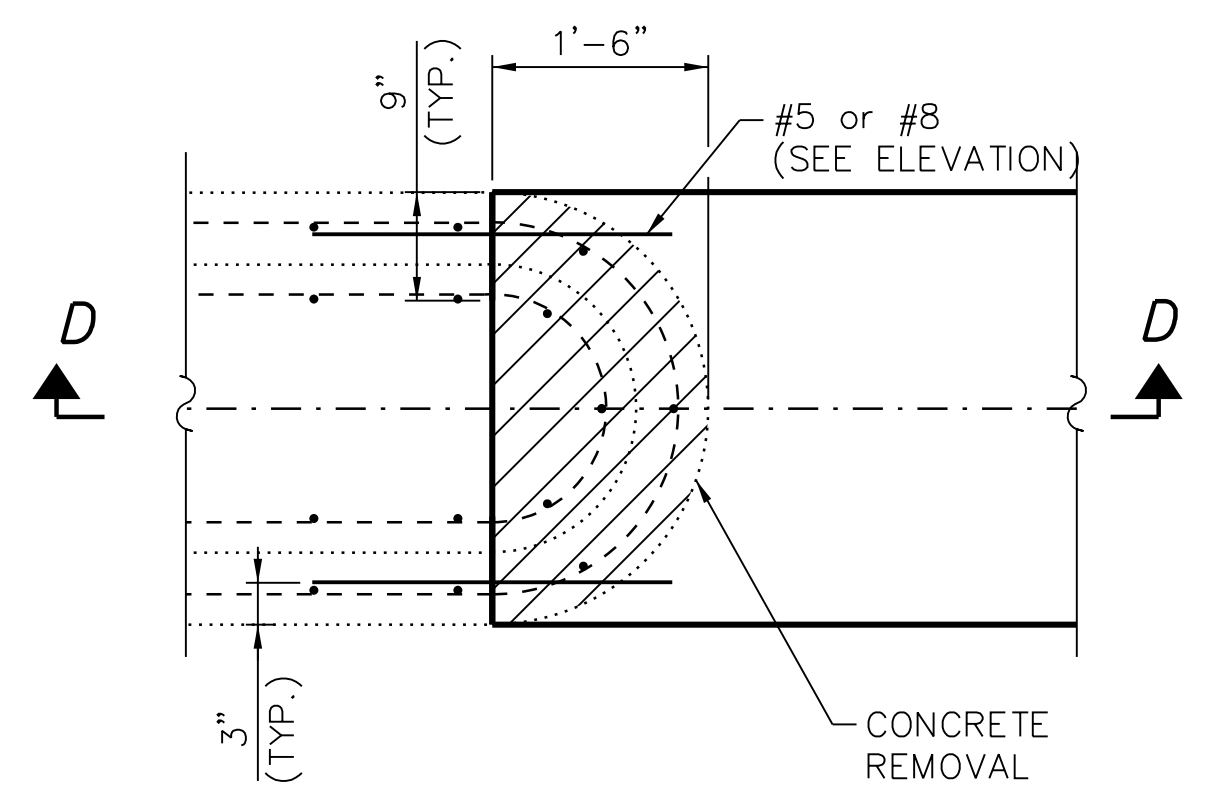
**95TH ST. BRIDGE  
PIER 2 NORTH WIDENING  
SN 016-0437**

**95TH-S-148**

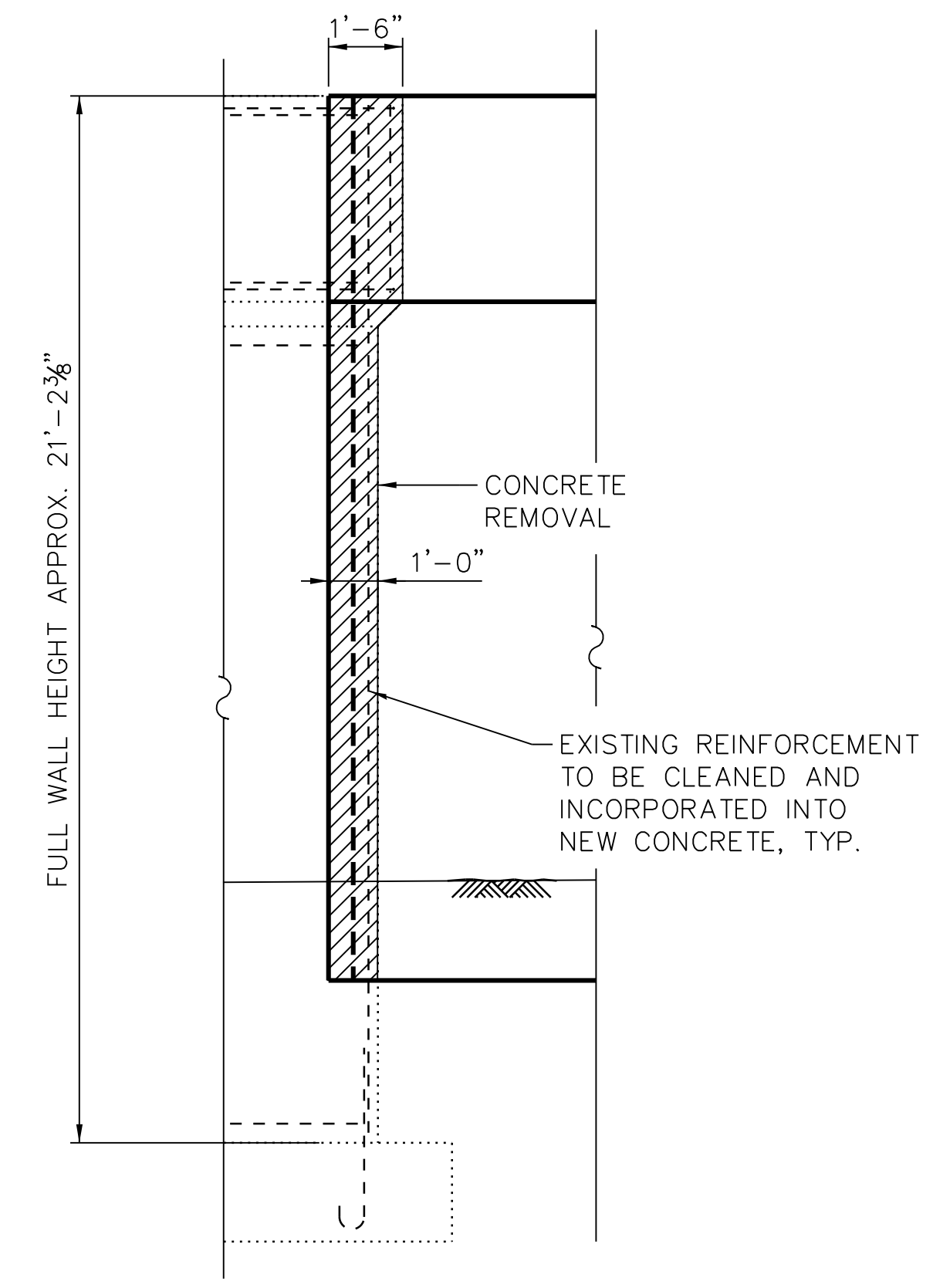
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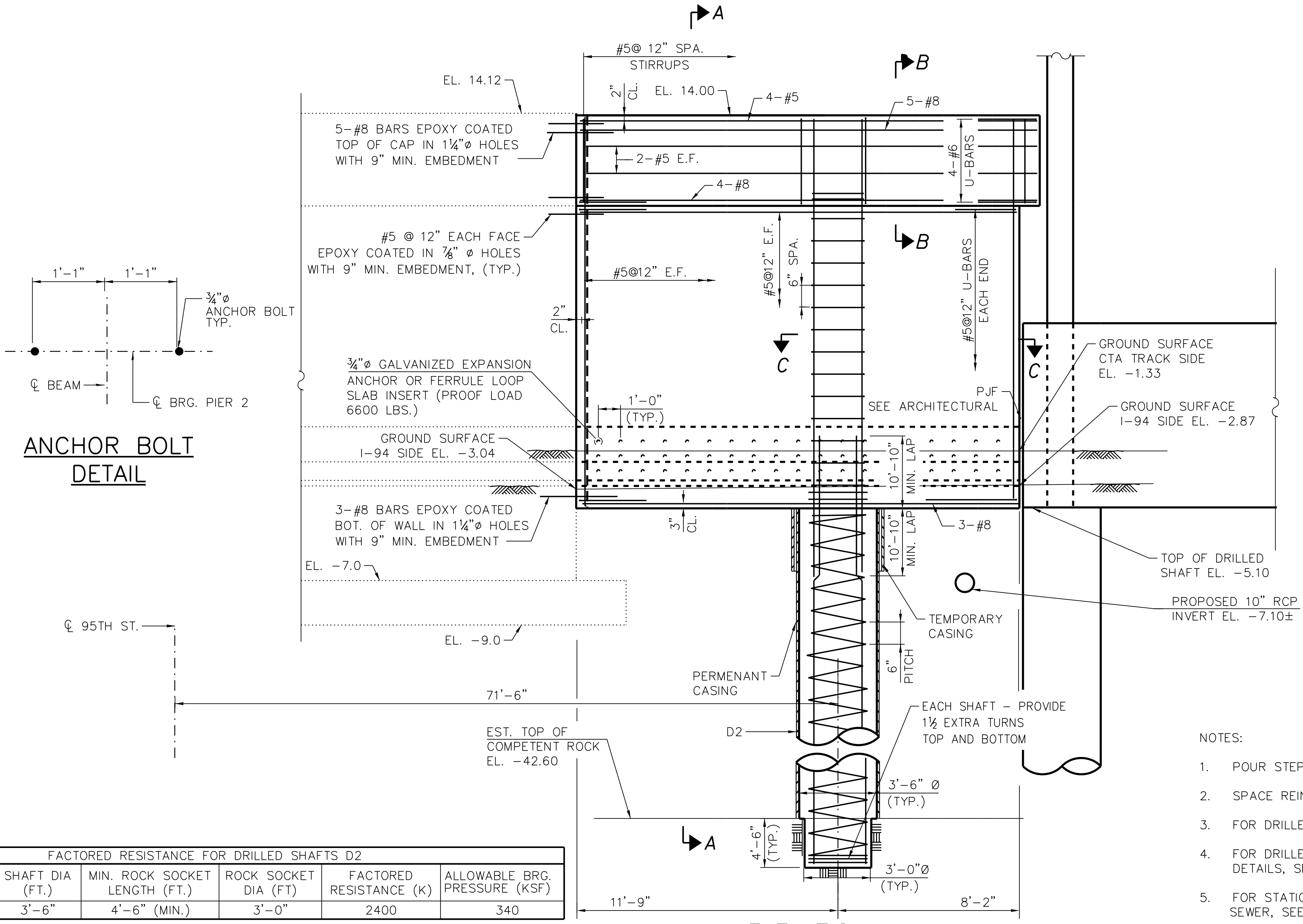
PLAN



DETAIL A



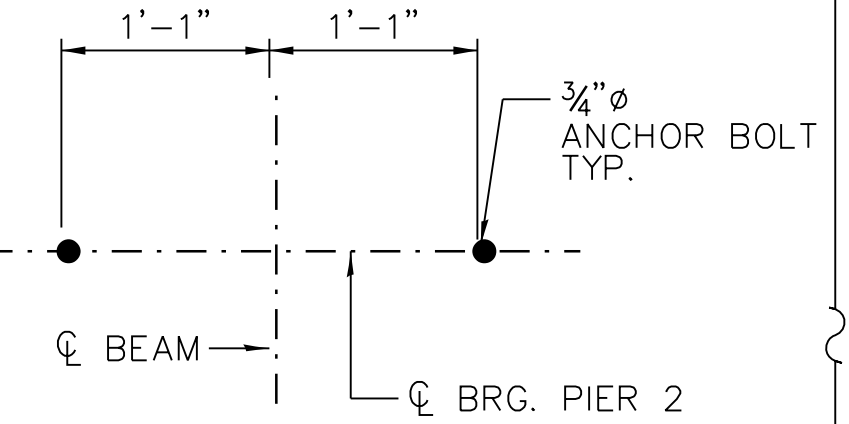
SECTION D-D



ELEVATION

(LOOKING EAST)

ANCHOR BOLT  
DETAIL



FACTORED RESISTANCE FOR DRILLED SHAFTS D2				
SHAFT DIA (FT.)	MIN. ROCK SOCKET LENGTH (FT.)	ROCK SOCKET DIA (FT)	FACTORED RESISTANCE (K)	ALLOWABLE BRG. PRESSURE (KSF)
3'-6"	4'-6" (MIN.)	3'-0"	2400	340

NOTES:

- POUR STEPS MONOLITHICALLY WITH CAP.
- SPACE REINFORCEMENT TO MISS ANCHOR BOLTS.
- FOR DRILLED SHAFT LOCATIONS, SEE SHEET 95TH-S-103.
- FOR DRILLED SHAFT DETAIL, PIER SECTION, AND REINFORCEMENT DETAILS, SEE SHEET NO. 95TH-S-150.
- FOR STATION AND OFFSET OF STORM SEWER, SEE SHEET C-143.
- DRILLING AND GROUTING OF REINFORCEMENT INTO EXISTING CONCRETE SHALL BE IN ACCORDANCE WITH IDOT STD SPEC ARTICLE 584.



**CHICAGO TRANSIT AUTHORITY ENGINEERING**

**SENSITIVE SECURITY INFORMATION**  
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**95th Terminal Improvement**  
 14 West 95th Street (North Terminal)  
 15 West 95th Street (South Terminal)  
 Chicago, IL 60628

IN CHARGE	S. KADERBEK
APPROVED BY	D.ZROKA
CHECKED BY	P.MAID
DESIGNED BY	L.SOMMER
DRAWN BY	T.SLEDGE
PROJECT NO.	2012-0021
FILE NAME	File Name

MARK	DATE	DESCRIPTION
	10/06/16	ISSUED FOR CONSTRUCTION

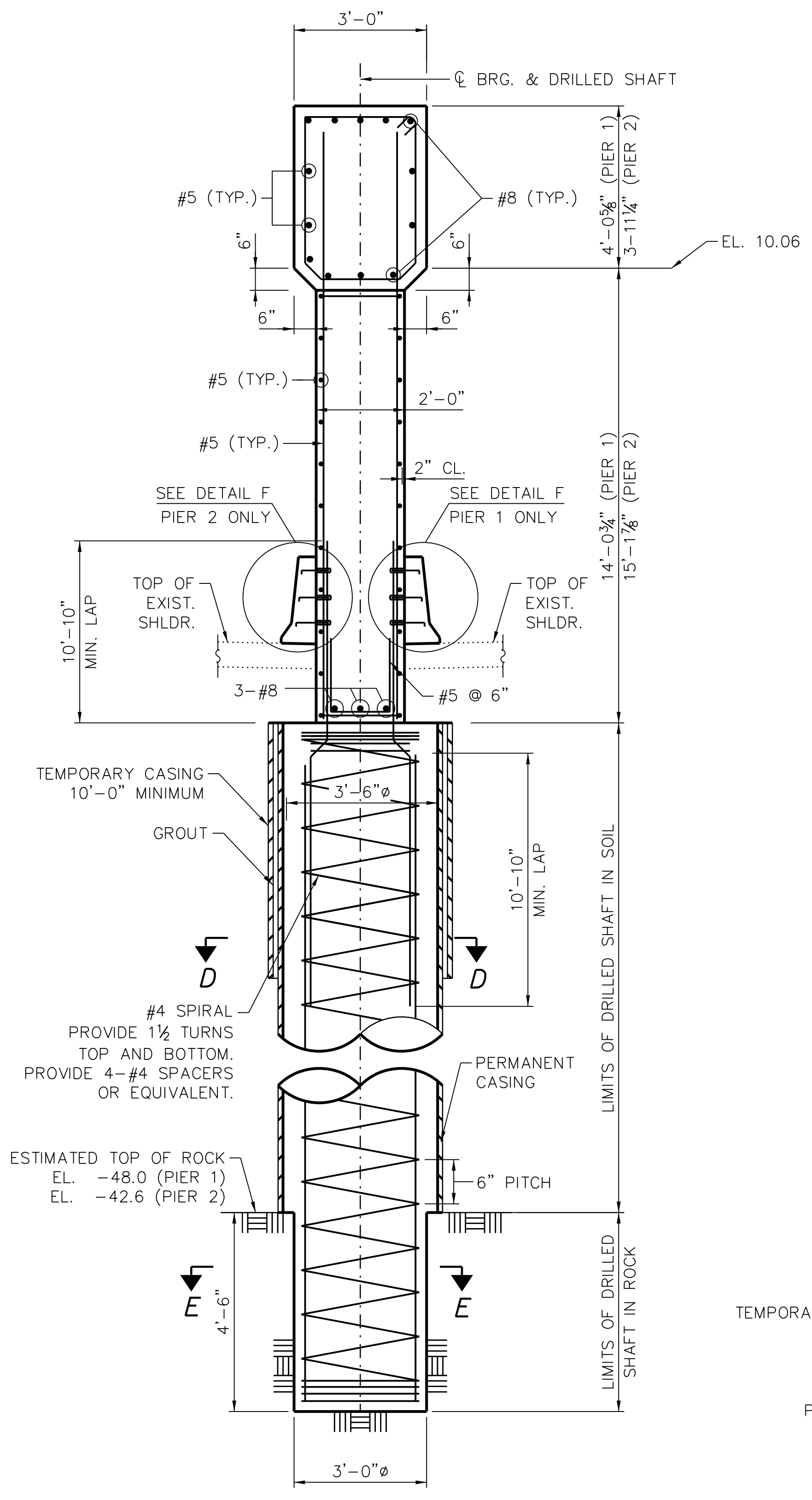
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 95TH ST. BRIDGE  
 PIER 2 SOUTH WIDENING  
 SN 016-0437

**95TH-S-149**

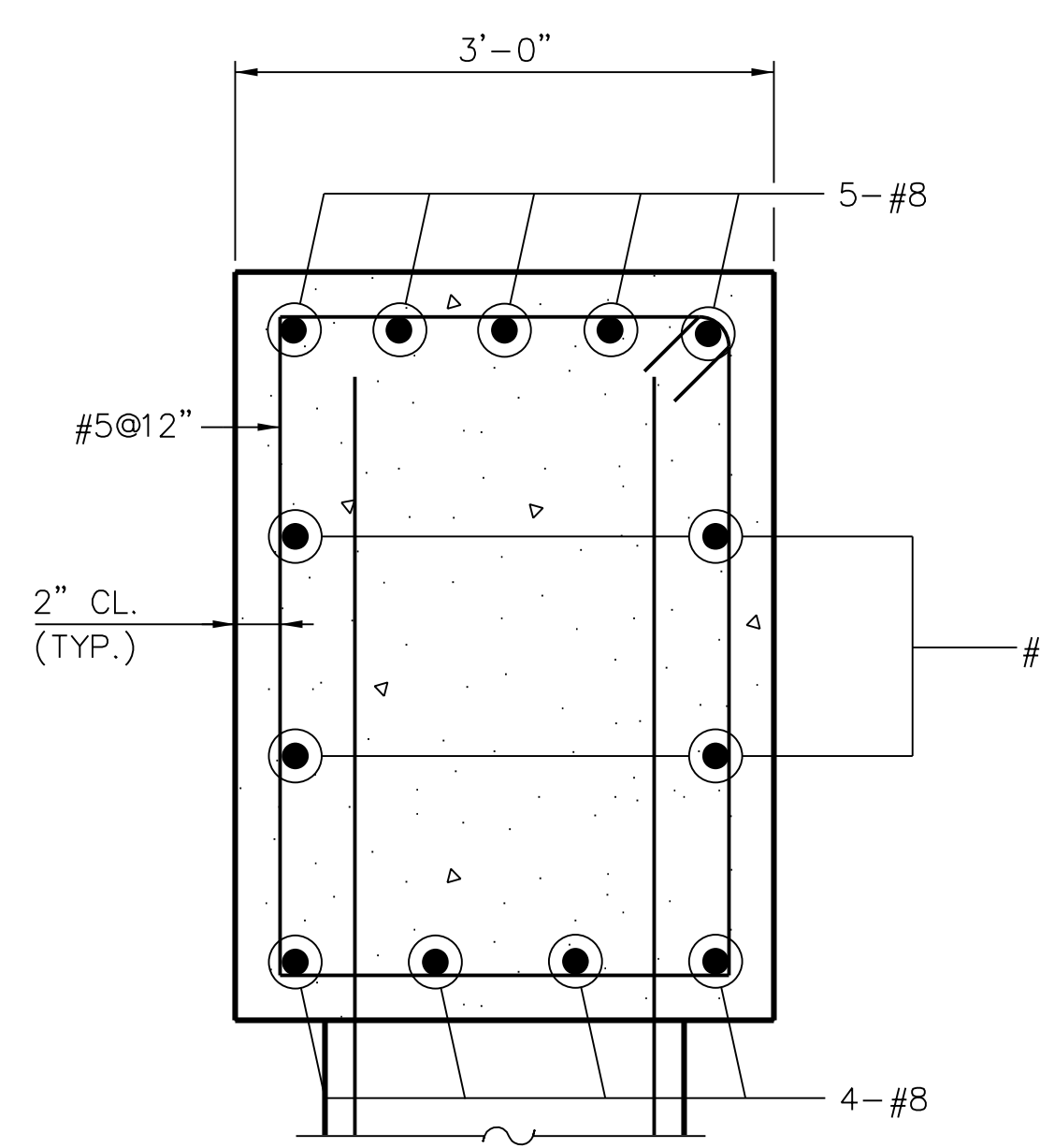
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 PLOT DATE = 10/7/2016

vscorpa

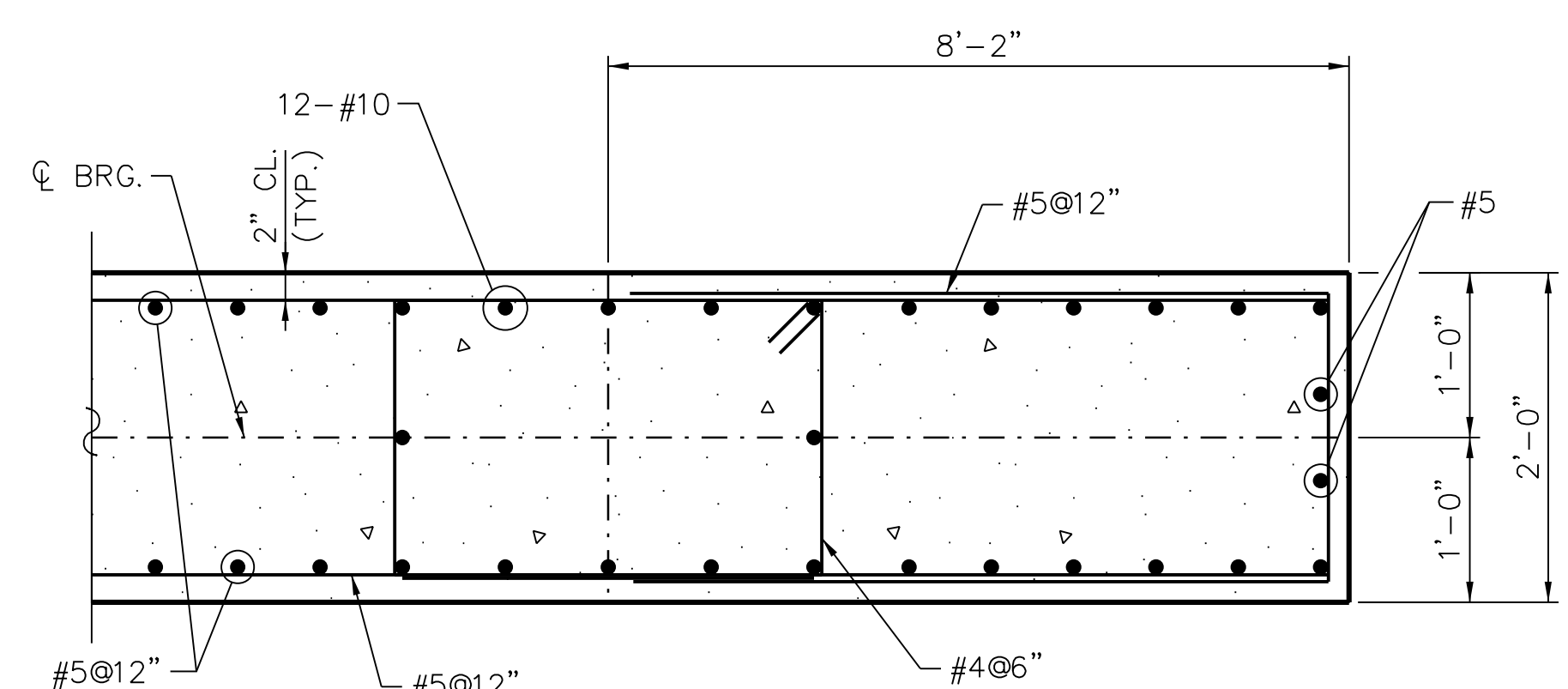
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PLOT DATE = 10/7/2016



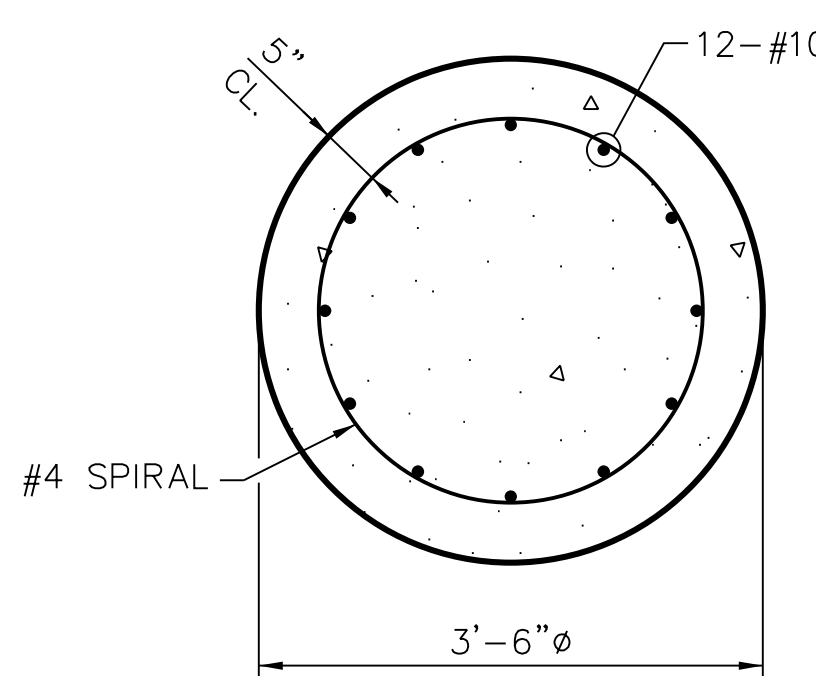
**SECTION A**  
SCALE: N.T.S.



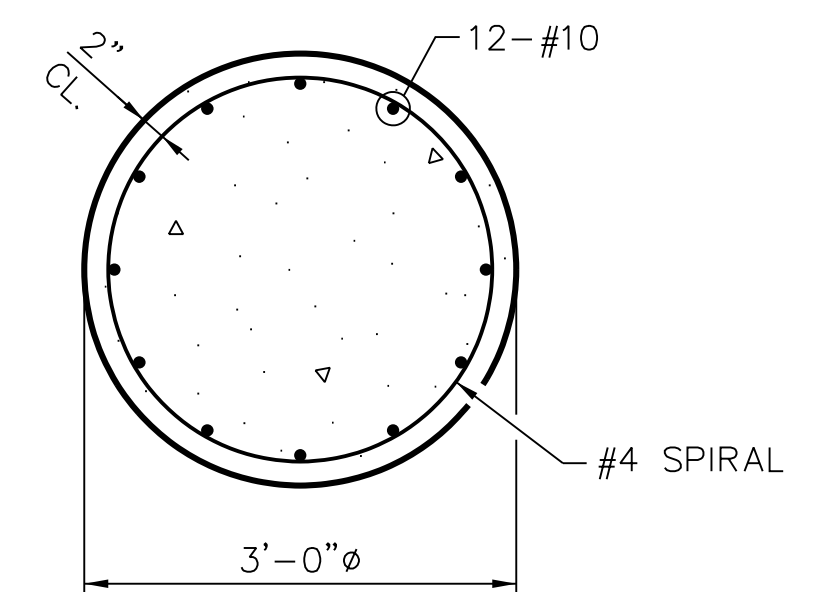
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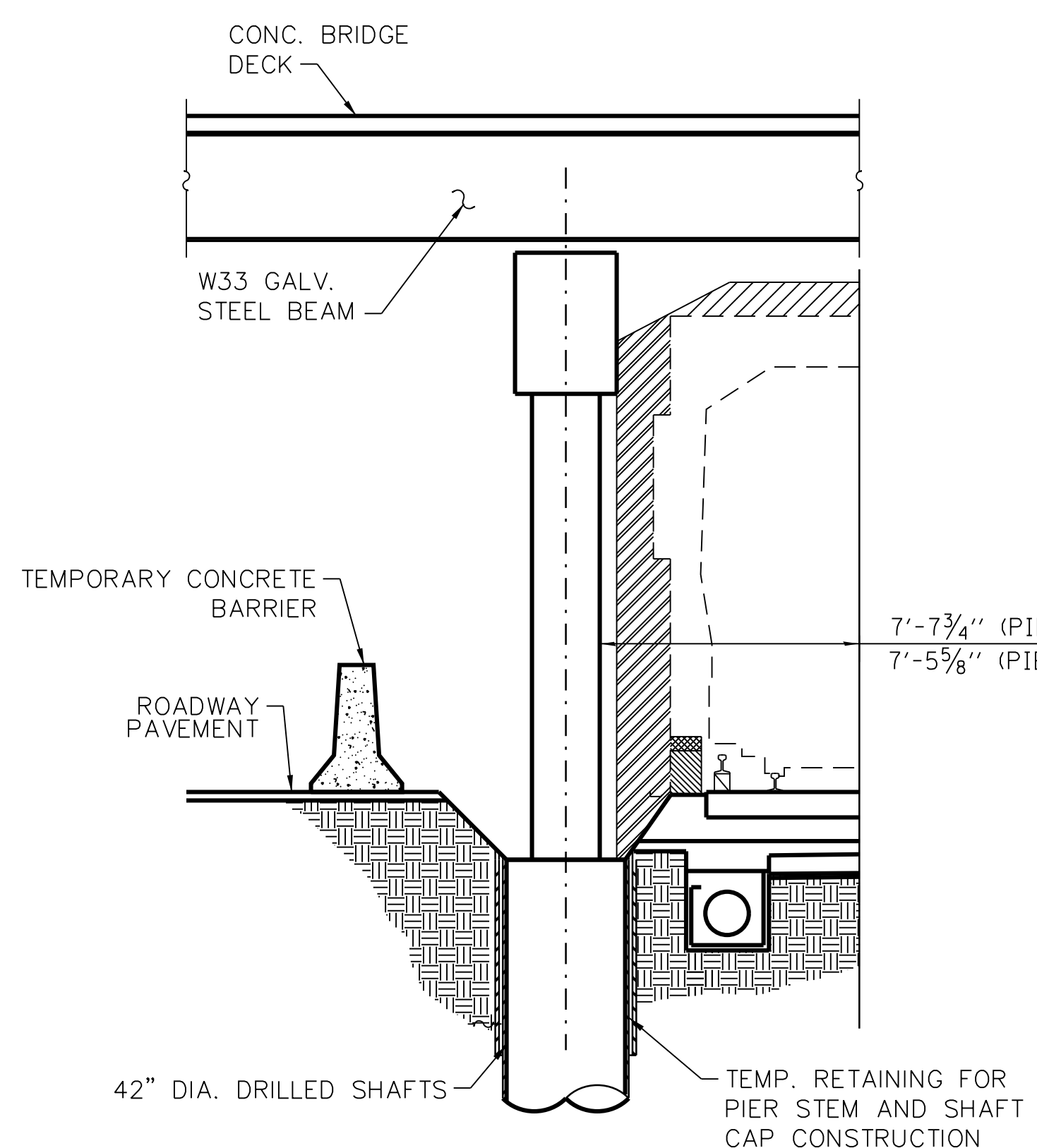
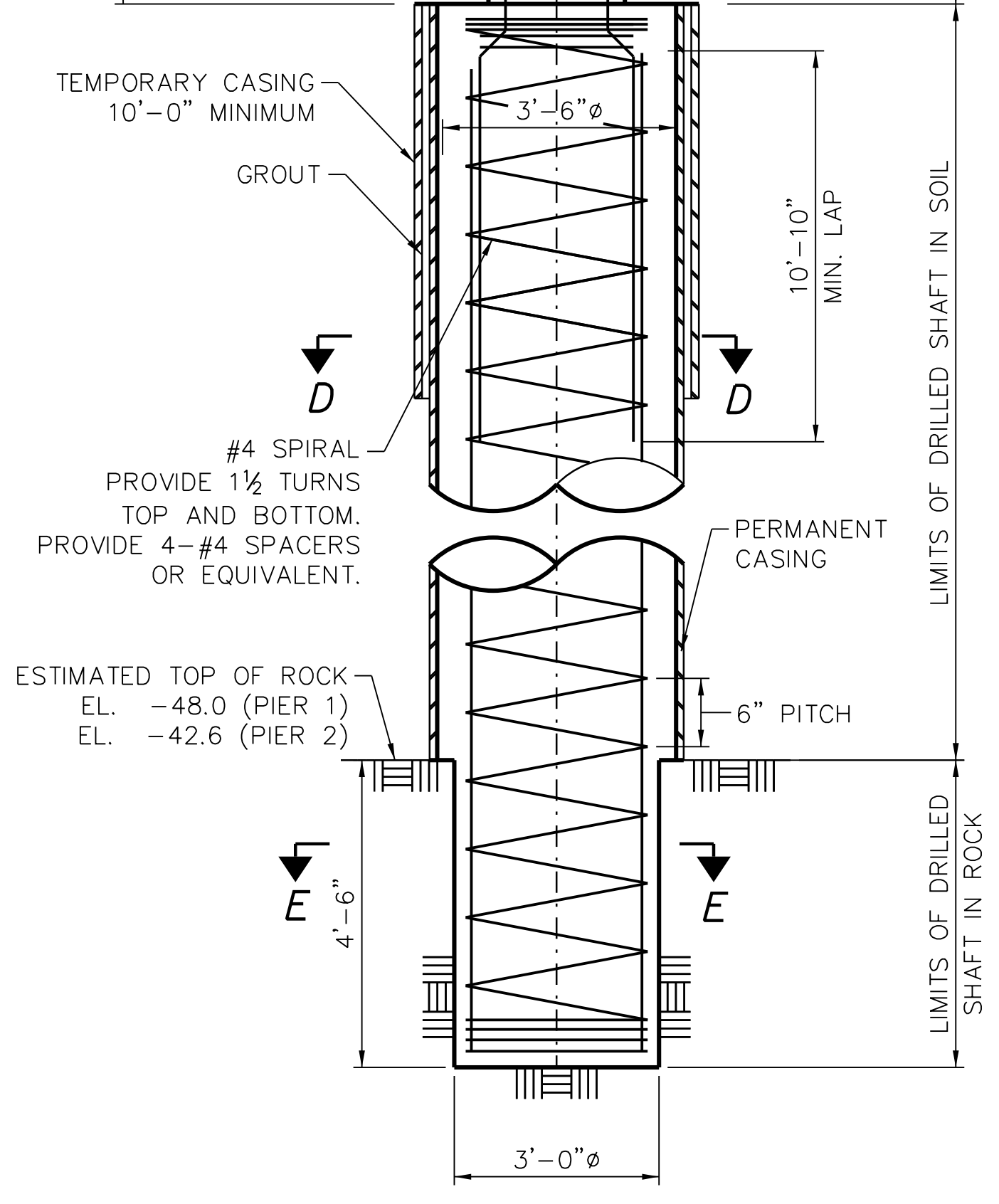
**SECTION C-C**



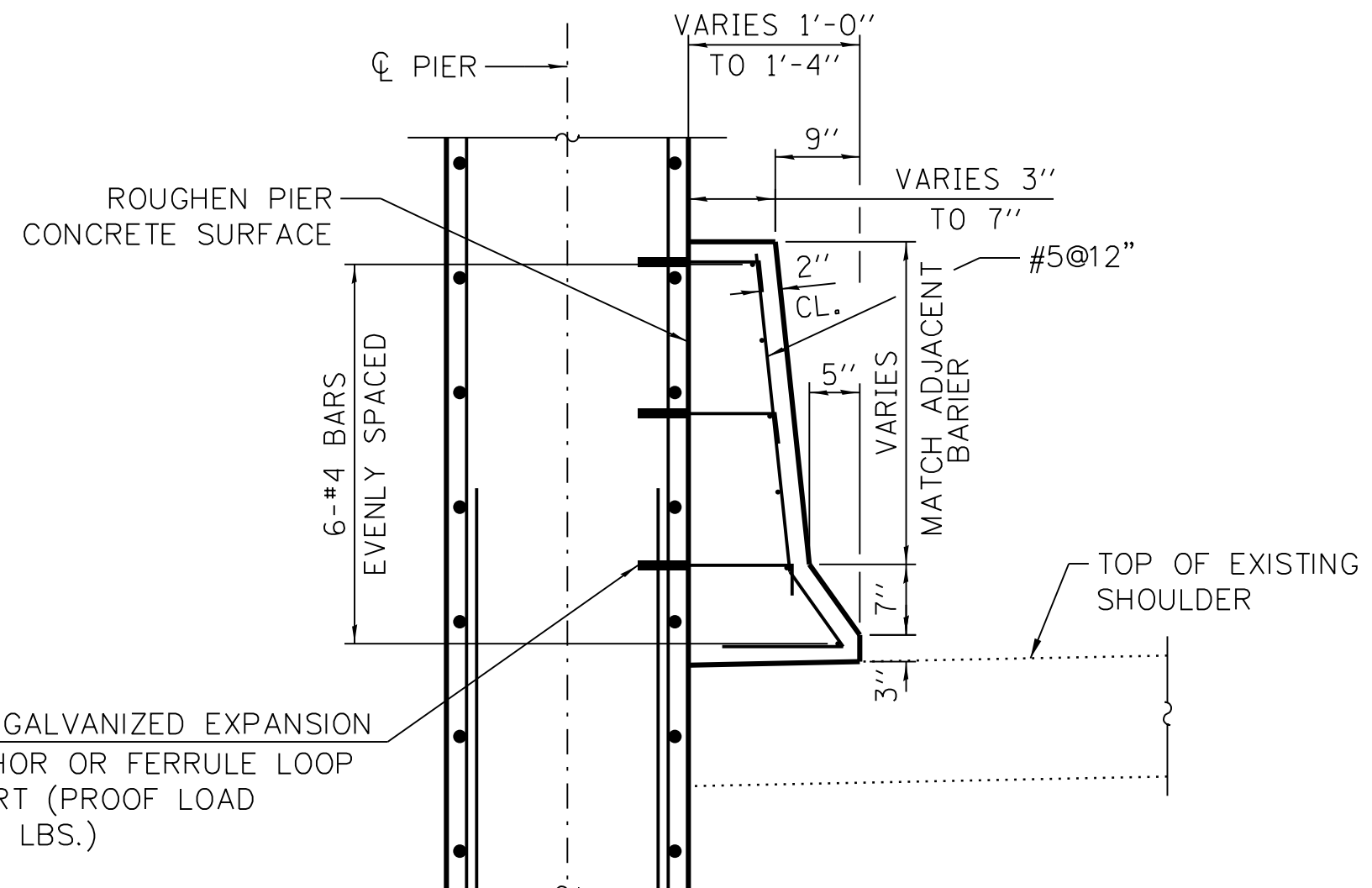
**SECTION D-D**



**SECTION E-E**

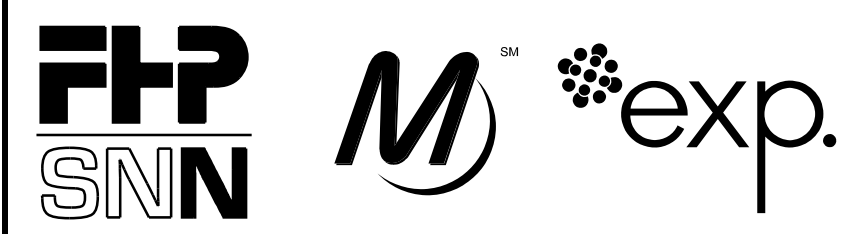


**CLEARANCE ENVELOPE**



**DETAIL F**  
SCALE: 3/4" = 1'-0"

- NOTES:
1. PROVIDE PERMANENT CASING FOR ALL ROCK SOCKETED CAISSONS. EXTEND CASING TO TOP OF ROCK. THICKNESS TO BE DETERMINED BY CONTRACTOR'S STRUCTURAL ENGINEER.
  2. USE TEMPORARY CASING TO SUPPORT ADJACENT LOADS. DEPTH 10'-0" MIN. OR AS DETERMINED BY CONTRACTOR'S STRUCTURAL ENGINEER.



**CHICAGO TRANSIT AUTHORITY ENGINEERING**

**SENSITIVE SECURITY INFORMATION**  
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**95th Terminal Improvement**  
14 West 95th Street (North Terminal)  
15 West 95th Street (South Terminal)  
Chicago, IL 60628

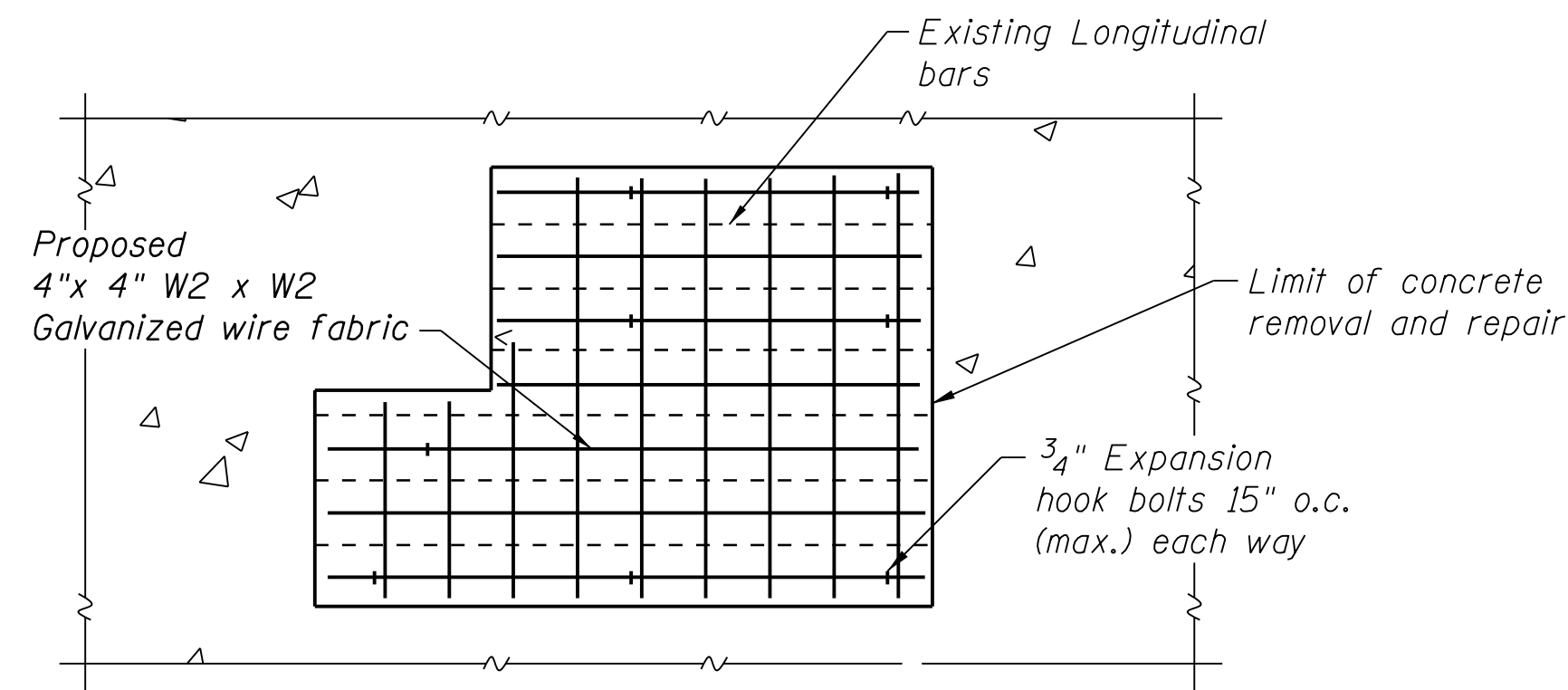
IN CHARGE	S. KADERBEK
APPROVED BY	D.ZROKA
CHECKED BY	P.MAID
DESIGNED BY	L.SOMMER
DRAWN BY	T.SLEDGE
PROJECT NO.	2012-0021
FILE NAME	File Name

MARK	DATE	DESCRIPTION
	10/06/16	ISSUED FOR CONSTRUCTION

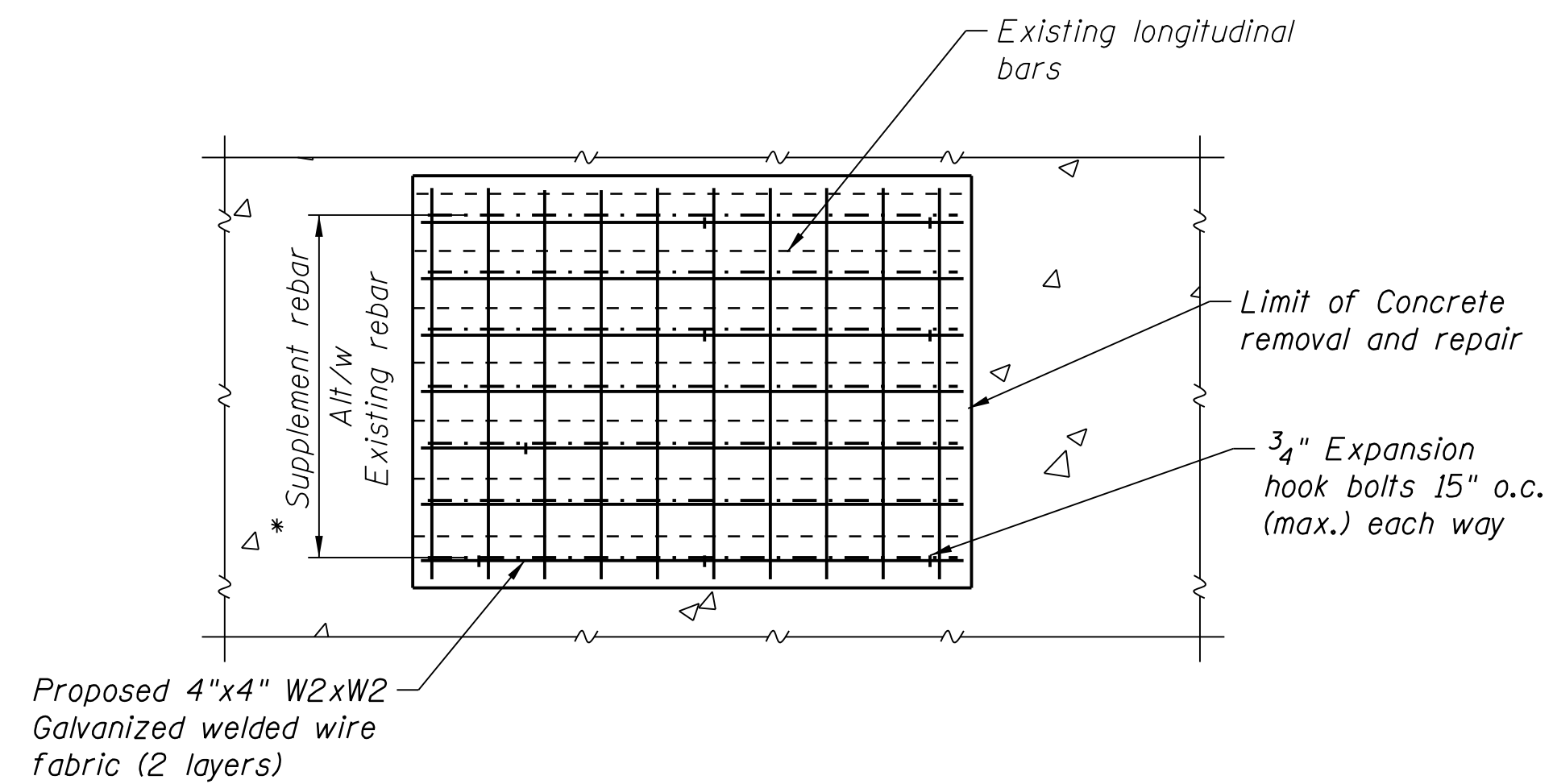
LOCATION IDENTIFIER: **DR095**  
95TH ST. BRIDGE  
PIER 1 AND 2 SOUTH  
WIDENING DETAILS  
SN 016-0437

**95TH-S-150**

vscarpa

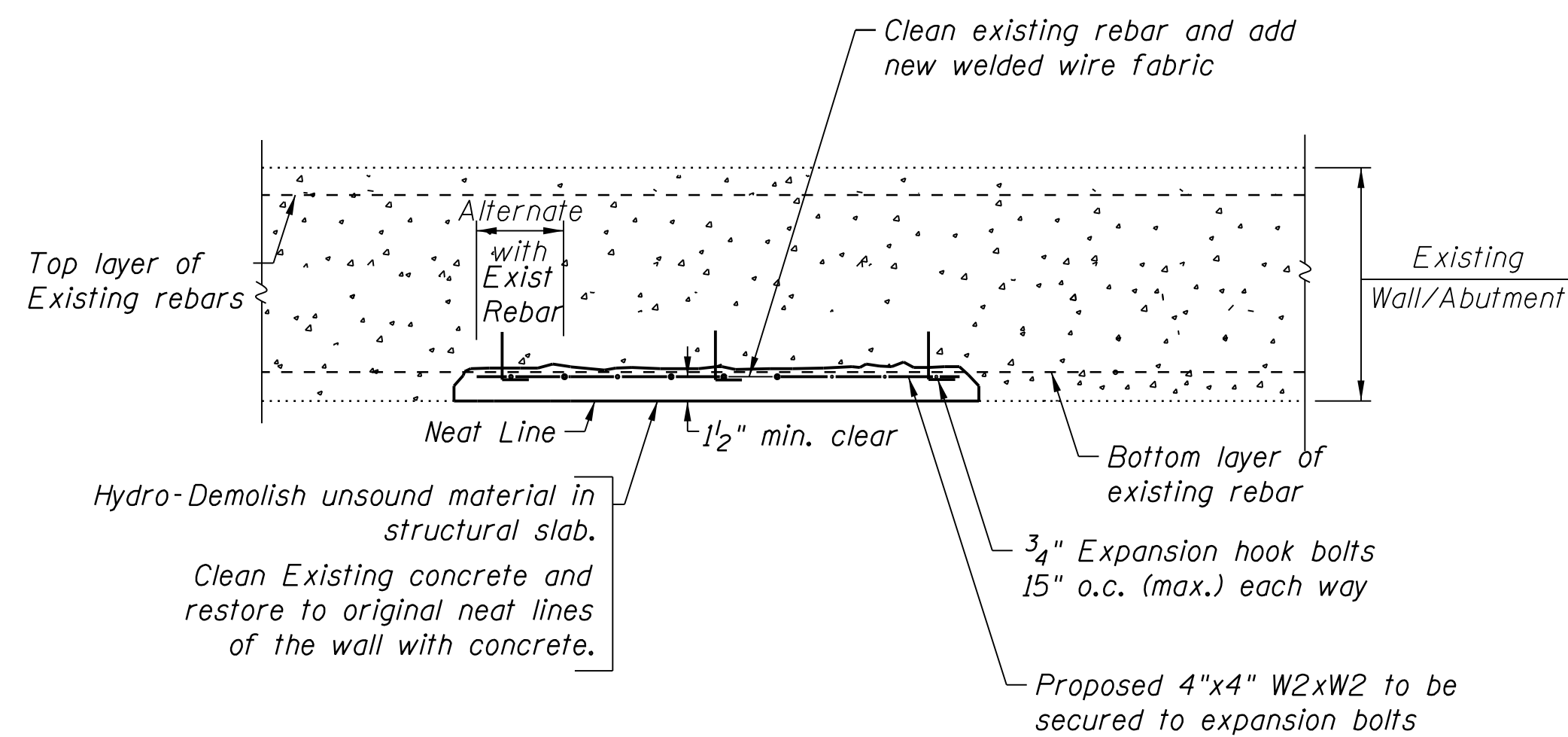


**STRUCTURAL CONCRETE REPAIR - LESS THAN 5 INCHES**  
**ELEVATION VIEW**

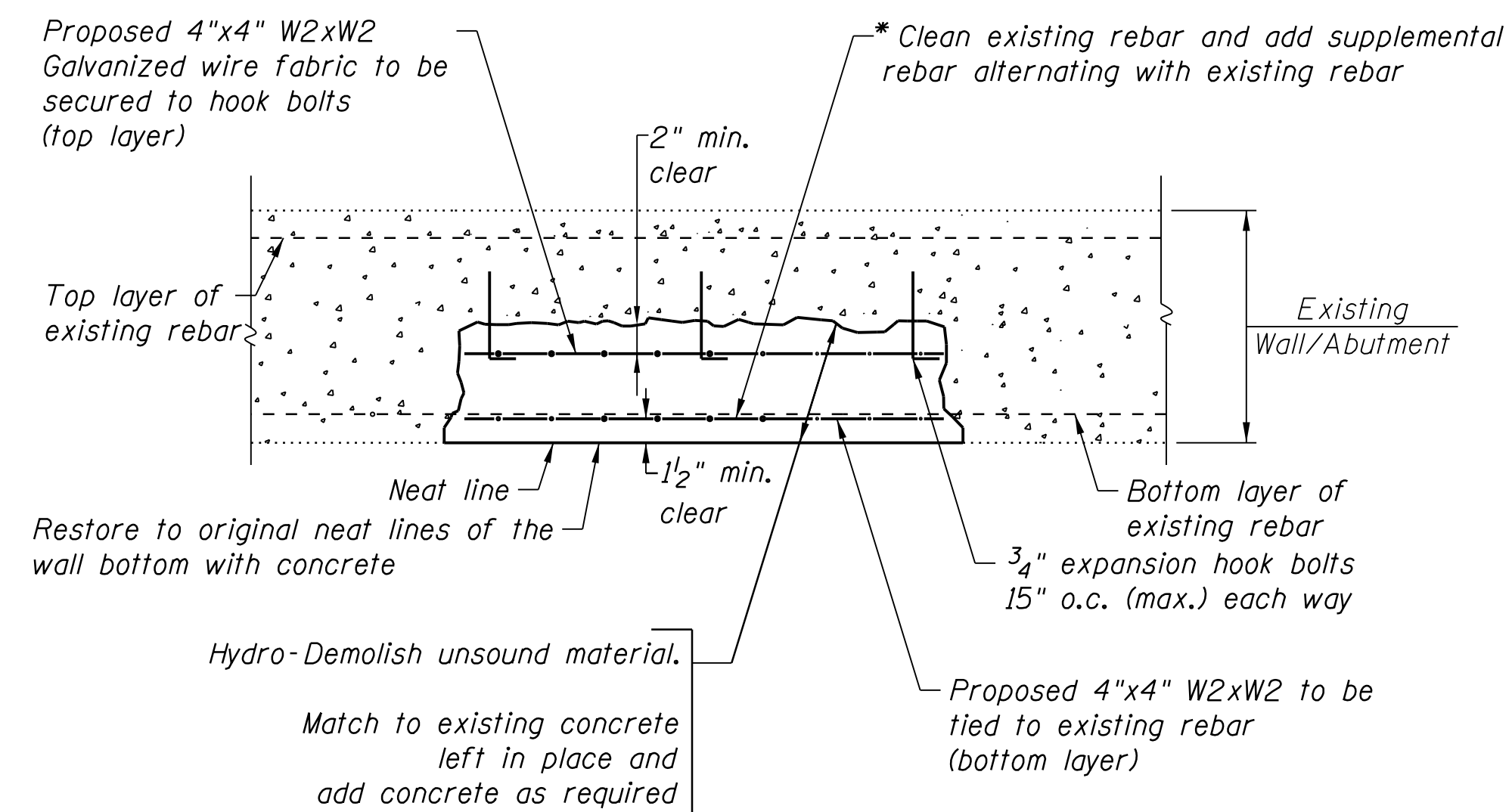


**STRUCTURAL CONCRETE REPAIR - GREATER THAN 5 INCHES**  
**ELEVATION VIEW**

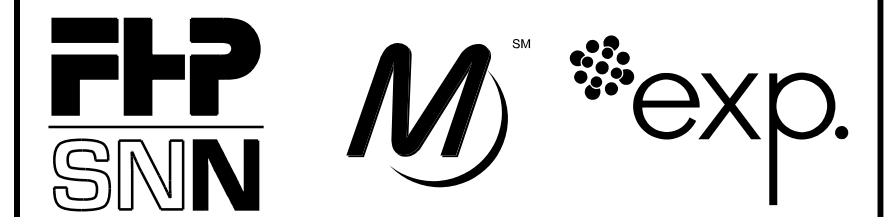
Note: Saw cut to straight lines. Saw cuts 1/2" deep or as required to avoid damage to reinforcing bars. If existing rebar is deteriorated more than 25% of cross sectional area, provide bar of equal size and lap 32 bar diameters beyond area of deterioration.



**STRUCTURAL CONCRETE REPAIR - LESS THAN 5 INCHES**



**STRUCTURAL CONCRETE REPAIR - GREATER THAN 5 INCHES**



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**95th Terminal Improvement**  
15 West 95th Street (South Terminal)  
Chicago, IL 60628

IN CHARGE	S. KADERBEK
APPROVED BY	D.ZROKA
CHECKED BY	P.MAID
DESIGNED BY	L.SOMMER
DRAWN BY	T.SLEDGE
PROJECT NO.	2012-0021
FILE NAME	File Name

MARK	DATE	DESCRIPTION
	2/9/17	ISSUED FOR CONSTRUCTION

LOCATION IDENTIFIER: **DR095**

95TH ST. BRIDGE  
STRUCTURAL REPAIR DETAILS  
SN 016-0437

**95TH-S-150A**

FILE NAME = 070\_DR095\_S-150A.dgn  
PLOT DATE = 8/31/2016

BENCH MARK: CITY OF CHICAGO BENCHMARK NO. 551. BENCH MONUMENT  
LOCATED ABOUT 50 FEET SOUTH OF THE SOUTH LINE OF W. 95TH ST., ABOUT  
11 FEET EAST OF THE WEST LINE OF S. PERRY AVE. ELEVATION 16.092.

EXISTING STRUCTURE: NONE

LOADING HL-93  
ALLOW 50#/SQ. FT. FOR FUTURE WEARING SURFACE.

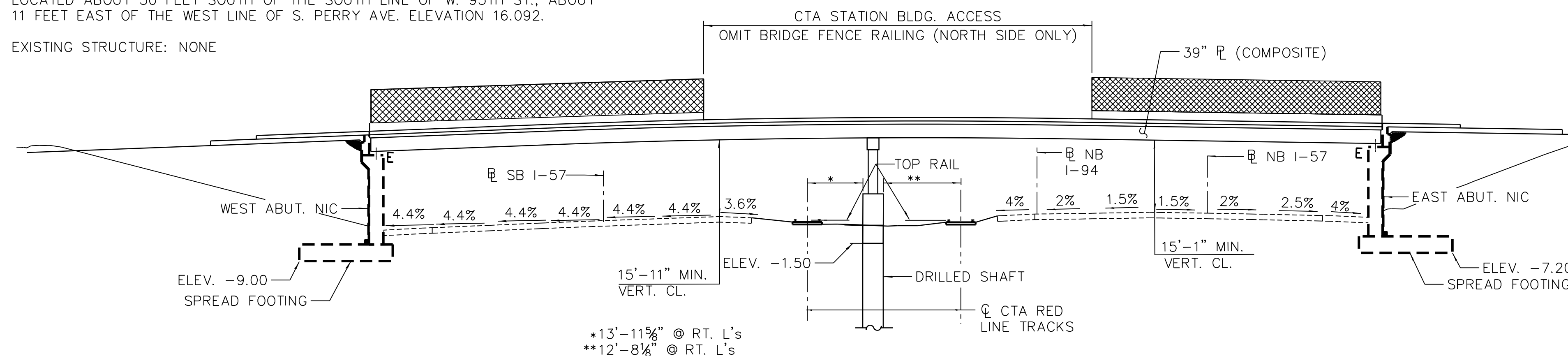
DESIGN SPECIFICATIONS  
2012 AASHTO LRFD BRIDGE  
DESIGN SPECIFICATIONS, 6TH EDITION

DESIGN STRESSES

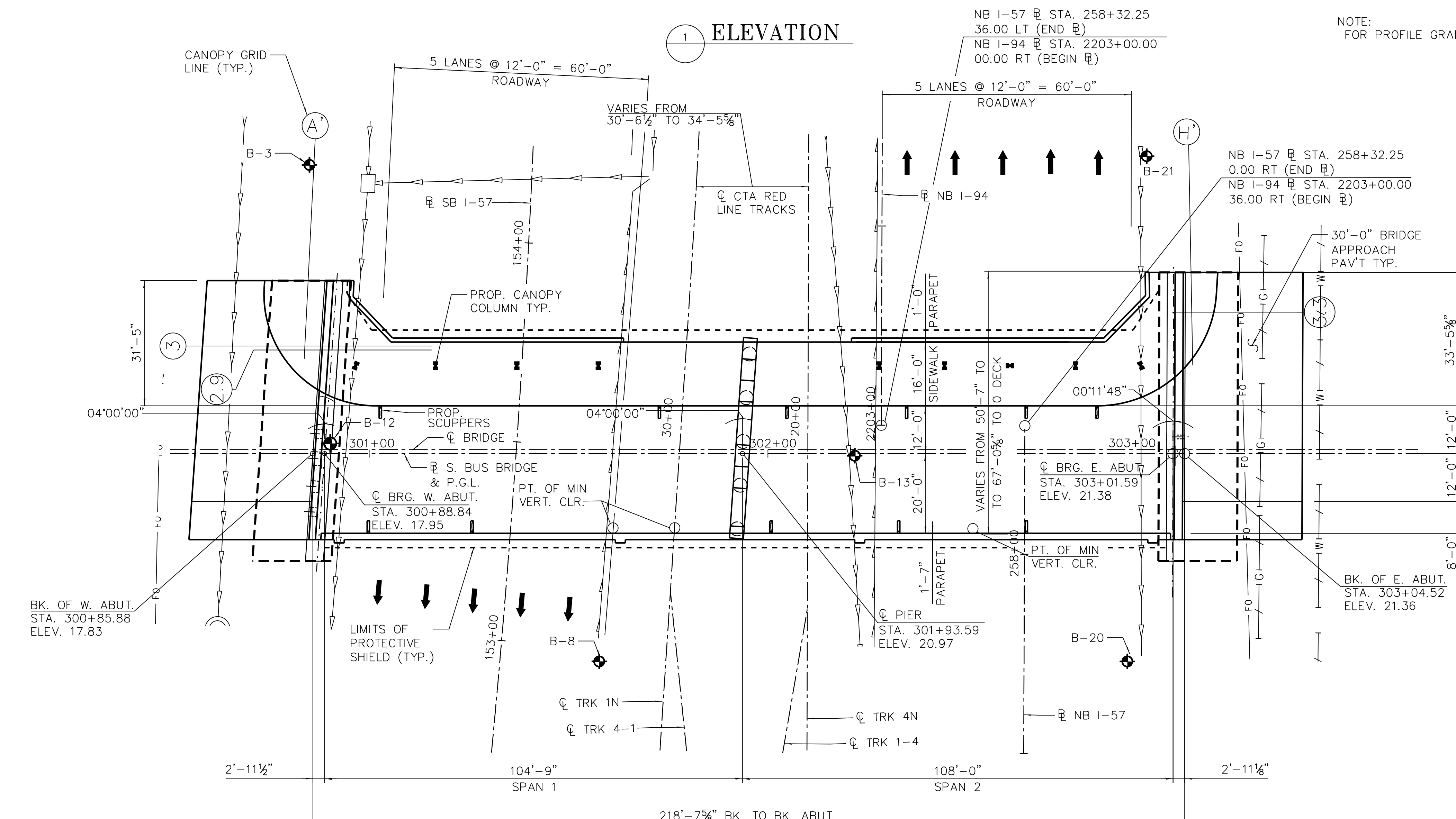
FIELD UNITS  
F'C = 5,000 PSI  
FY = 60,000 PSI (REINFORCEMENT)  
FY = 50,000 PSI (M270 GRADE 50)

SEISMIC DATA

SEISMIC PERFORMANCE ZONE (SPZ) = 1  
DESIGN SPECTRAL ACCELERATION AT 1.0 SEC. (S<sub>01</sub>) = 0.10  
DESIGN SPECTRAL ACCELERATION AT 0.2 SEC. (S<sub>05</sub>) = 0.14  
SOIL SITE CLASS = D



1 ELEVATION

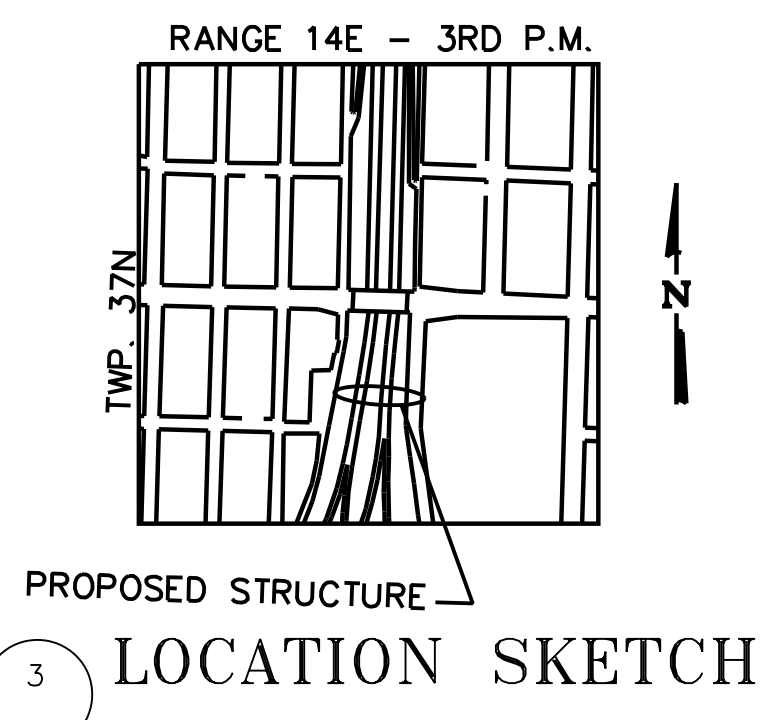


2 PLAN

NOTE:  
FOR PROFILE GRADE DATA, SEE SHEET S-202.

LEGEND

INDICATES BORING LOCATION



3 LOCATION SKETCH



CHICAGO TRANSIT  
AUTHORITY  
ENGINEERING

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SECURITY ADMINISTRATION OR THE SECRETARY OF TRANSPORTATION. UNAUTHORIZED  
RELEASE MAY RESULT IN CIVIL PENALTY OR OTHER ACTION. FOR U.S. GOVERNMENT  
AGENCIES, PUBLIC DISCLOSURE IS GOVERNED BY U.S.C. 552 AND 49 CFR PARTS 15  
AND 1520.

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ERRORS CAUSED, DIRECTLY OR INDIRECTLY,  
BY SCALING OF THIS DRAWING.

95th Terminal Improvement  
15 West 95th Street (South Terminal)  
Chicago, IL 60628

IN CHARGE	S. KADERBEK
APPROVED BY	D. ZROKA
CHECKED BY	P. MAID
DESIGNED BY	L. SOMMER
DRAWN BY	T. SLEDGE
PROJECT NO.	2012-0021
FILE NAME	File Name

MARK	DATE	DESCRIPTION
	2/9/17	ISSUED FOR CONSTRUCTION

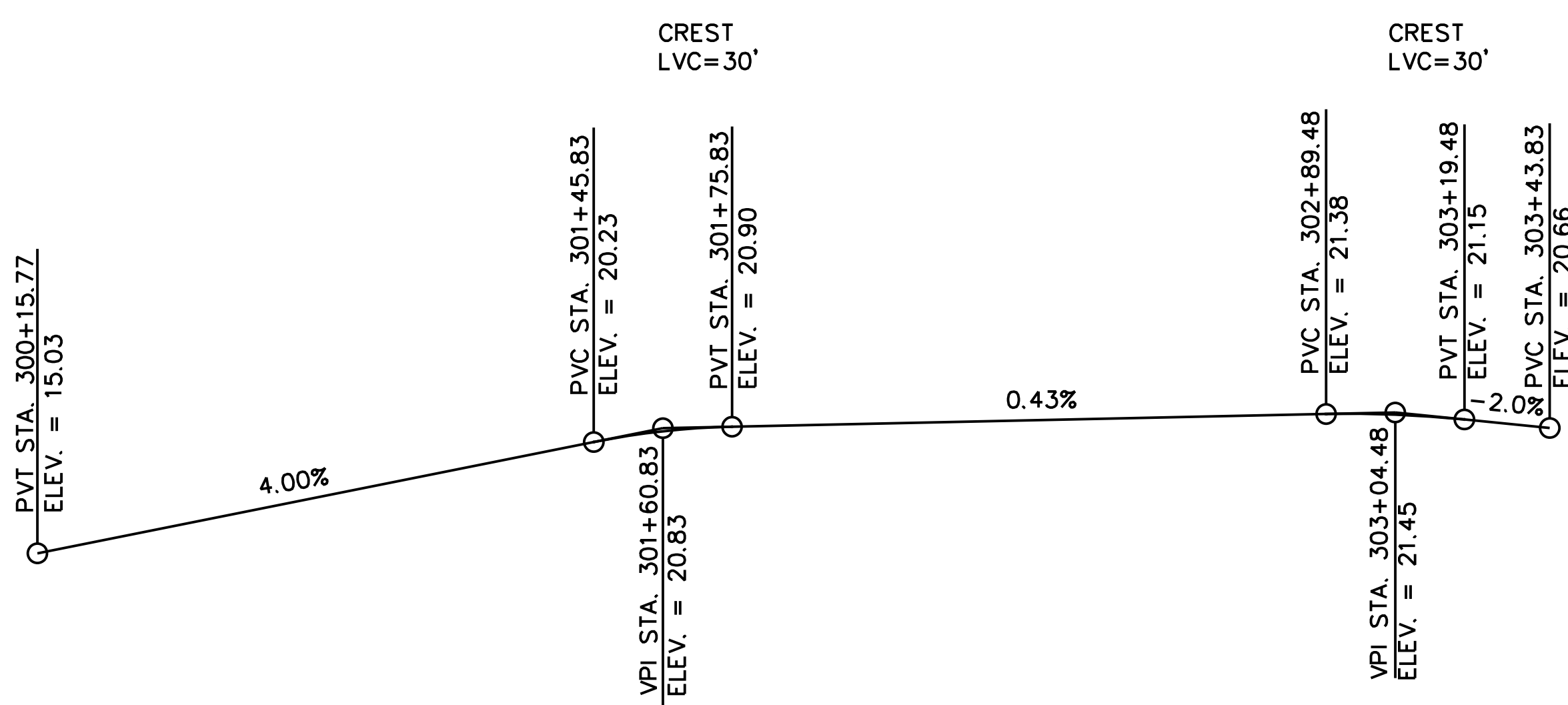
LOCATION IDENTIFIER: DR095

SOUTH BUS BRIDGE  
GENERAL PLAN  
AND ELEVATION

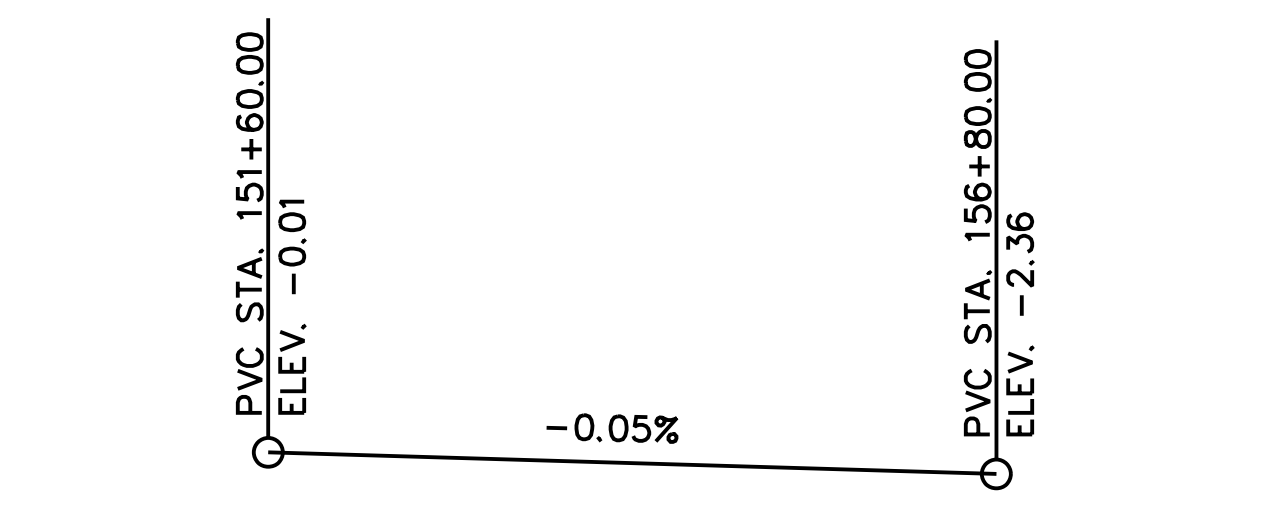
SBB-S-201

FILE NAME = 070\_DR095\_S-201.dgn  
PLOT DATE = 7/1/2016

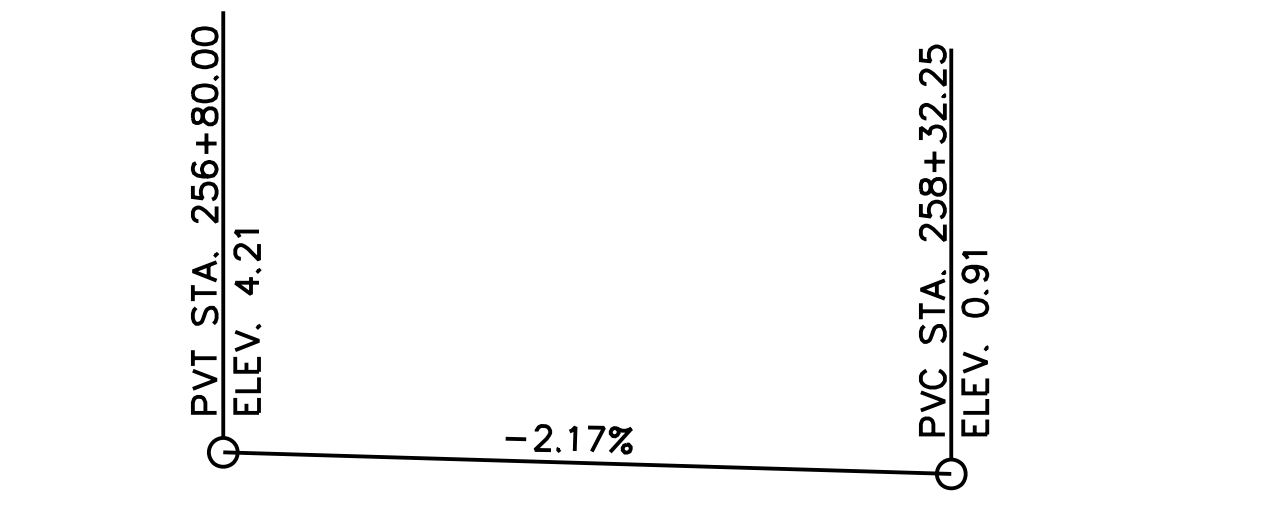
vscorppa



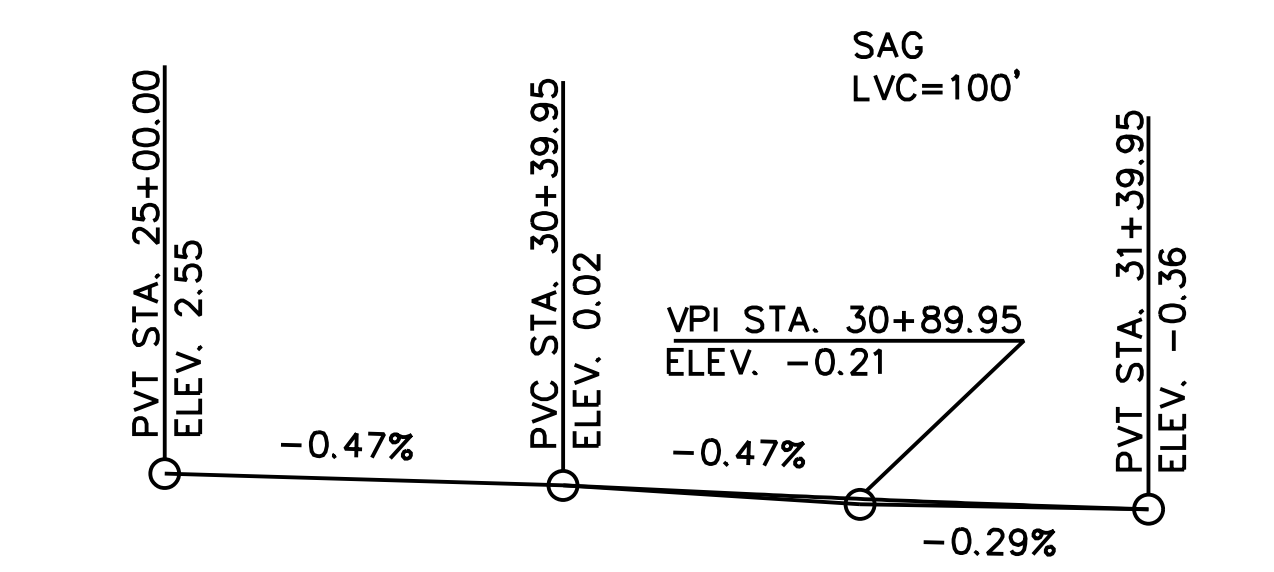
1 SOUTH BUS BRIDGE PROFILE GRADE  
SCALE: N.T.S.



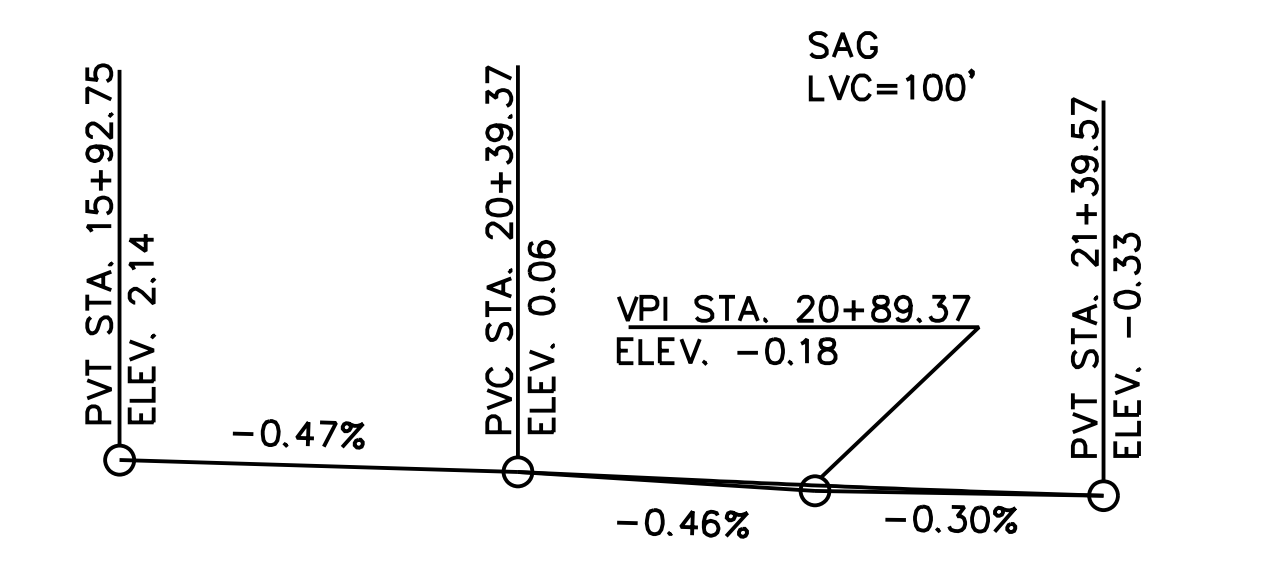
2 I-57 SB EX. PROFILE GRADE  
SCALE: N.T.S.



3 I-57 NB EX. PROFILE GRADE  
SCALE: N.T.S.



4 TRACK IN PROFILE GRADE  
SCALE: N.T.S.



5 TRACK IN PROFILE GRADE  
SCALE: N.T.S.

NOTE:  
1. EXISTING PROFILES ARE REPRESENTATIVE OF A BEST FIT FROM FIELD SURVEY BY SANCHEZ & ASSOCIATES LAND SURVEYING ON 05-21-2013. EXISTING PROFILE GRADE IS TO REMAIN UNCHANGED.



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**95th Terminal Improvement**  
15 West 95th Street (South Terminal)  
Chicago, IL 60628

IN CHARGE	S. KADERBEK
APPROVED BY	D.ZROKA
CHECKED BY	P.MAID
DESIGNED BY	L.SOMMER
DRAWN BY	T.SLEDGE
PROJECT NO.	2012-0021
FILE NAME	File Name

MARK	DATE	DESCRIPTION
	2/9/17	ISSUED FOR CONSTRUCTION

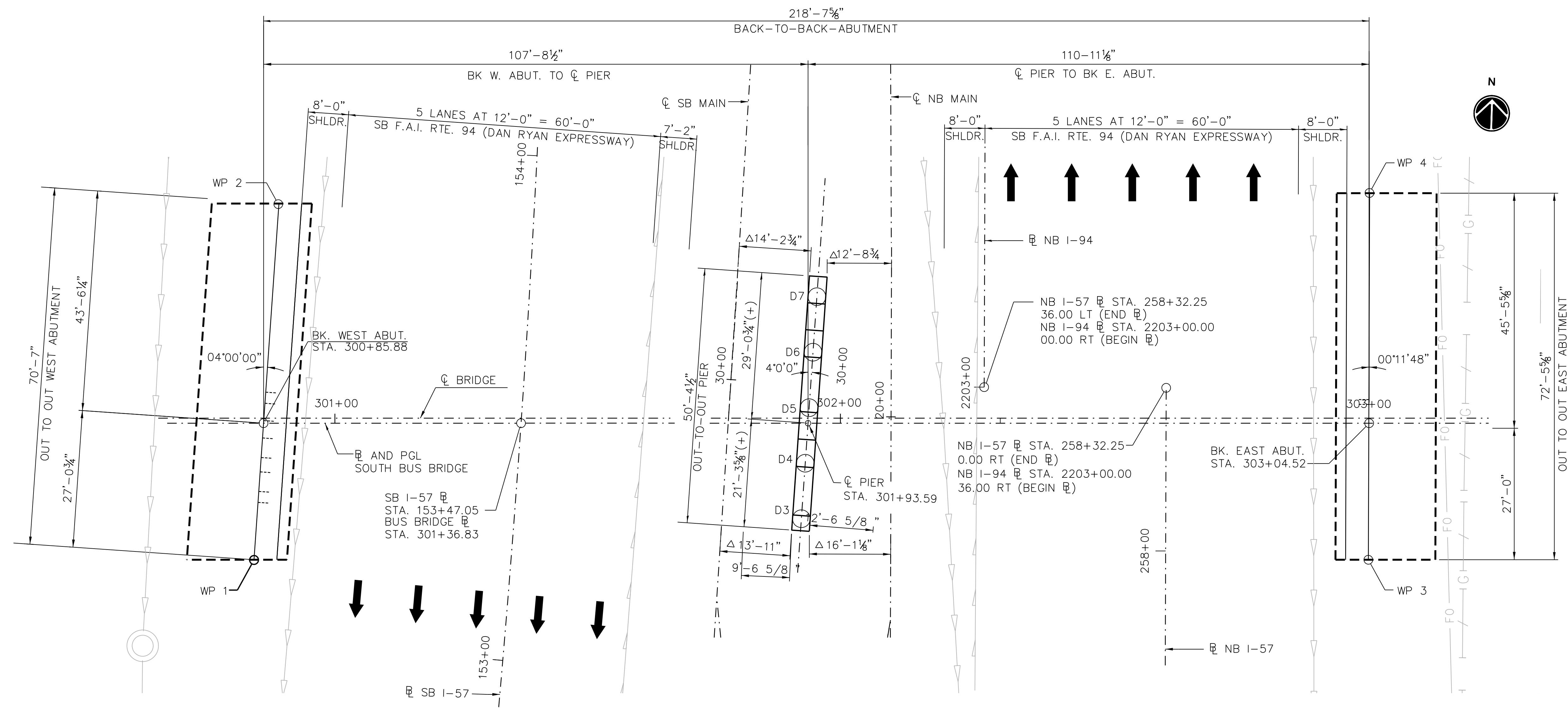
LOCATION IDENTIFIER: DR095

SOUTH BUS BRIDGE  
GENERAL DATA

**SBB-S-202**

FILE NAME = 070\_DR095\_S-202.dgn  
PLOT DATE = 8/31/2016

vscarpa



**1 SUBSTRUCTURE LAYOUT**  
SCALE: N.T.S.

△ FOR CLEARANCES DURING CONSTRUCTION, SEE TABLE

NOTES:  
1. FOR PIER DETAILS SEE PIER SHEETS SBB-S-233 AND SBB-S-235.

**1 WORK POINTS**

WP NO.	STATION	OFFSET	BASELINE
1	300+84.00	27	SOUTH BUS BRIDGE
2	300+88.92	-43.42	SOUTH BUS BRIDGE
3	303+04.43	27	SOUTH BUS BRIDGE
4	303+04.67	-45.47	SOUTH BUS BRIDGE

**2 DRILLED SHAFT**

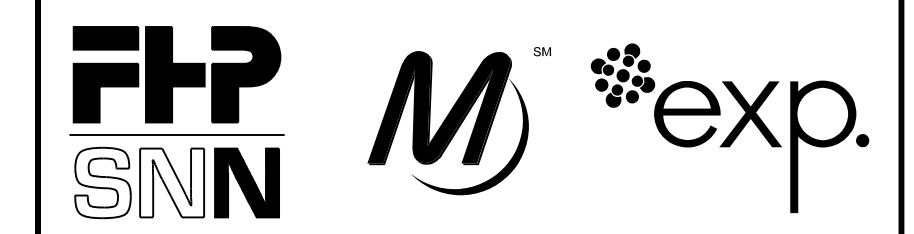
DRILLED SHAFT	STATION	OFFSET	BASELINE
D3	301+92.27	18.89 RT.	SOUTH BUS BRIDGE
D4	301+93.04	7.91 RT.	SOUTH BUS BRIDGE
D5	301+93.80	3.06 LT.	SOUTH BUS BRIDGE
D6	301+94.57	14.03 LT.	SOUTH BUS BRIDGE
D7	301+95.34	25.01 LT.	SOUTH BUS BRIDGE

**2 TEMPORARY CLEARANCE DURING CONSTRUCTION**

NORTHWEST PIER CORNER	10'-10 1/8"
NORTHEAST PIER CORNER	11'-9 3/8"
SOUTHWEST PIER CORNER	9'-6 5/8"
SOUTHEAST PIER CORNER	12'-6 5/8"

DISTANCES ARE TO CENTERLINE OF TRACK

FILE NAME = 070\_DR095\_S-203.DGN  
PLOT DATE = 10/13/2016



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**95th Terminal Improvement**  
14 West 95th Street (North Terminal)  
15 West 95th Street (South Terminal)  
Chicago, IL 60628

IN CHARGE	S. KADERBEK
APPROVED BY	D.ZROKA
CHECKED BY	P.MAID
DESIGNED BY	L.SOMMER
DRAWN BY	T.SLEDGE
PROJECT NO.	2012-0021
FILE NAME	File Name

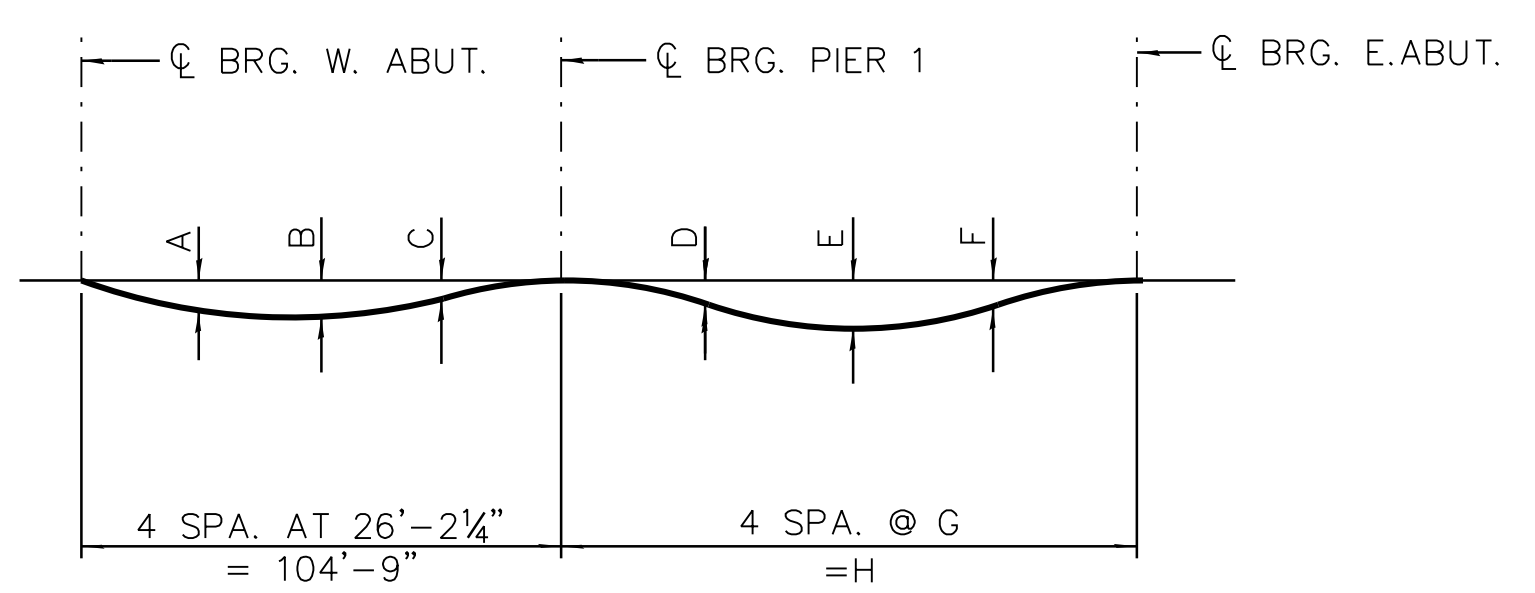
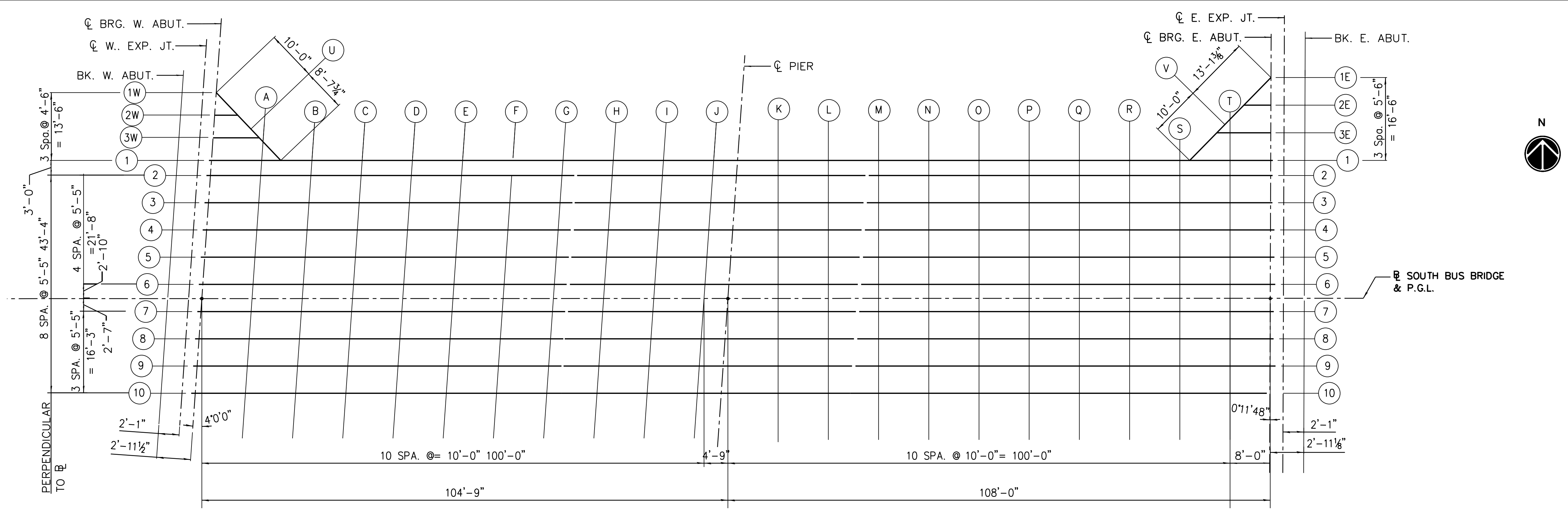
MARK	DATE	DESCRIPTION
△	10/13/16	REVISIONS 1
	10/06/16	ISSUED FOR CONSTRUCTION

LOCATION IDENTIFIER: DR095

SOUTH BUS BRIDGE  
FOOTING LAYOUT

**SBB-S-203**

vscarpa

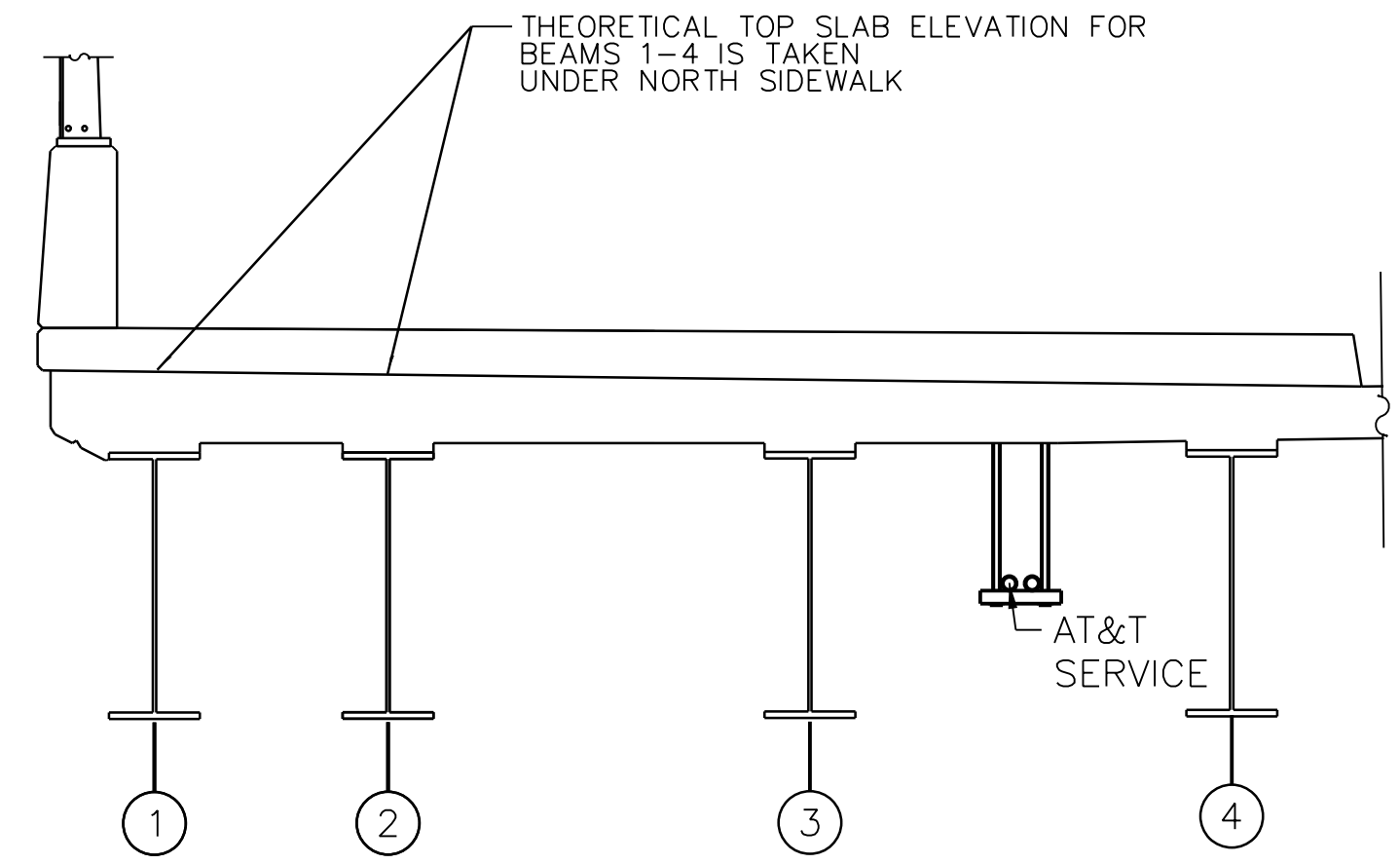


**DEAD LOAD DEFLECTION DIAGRAM**

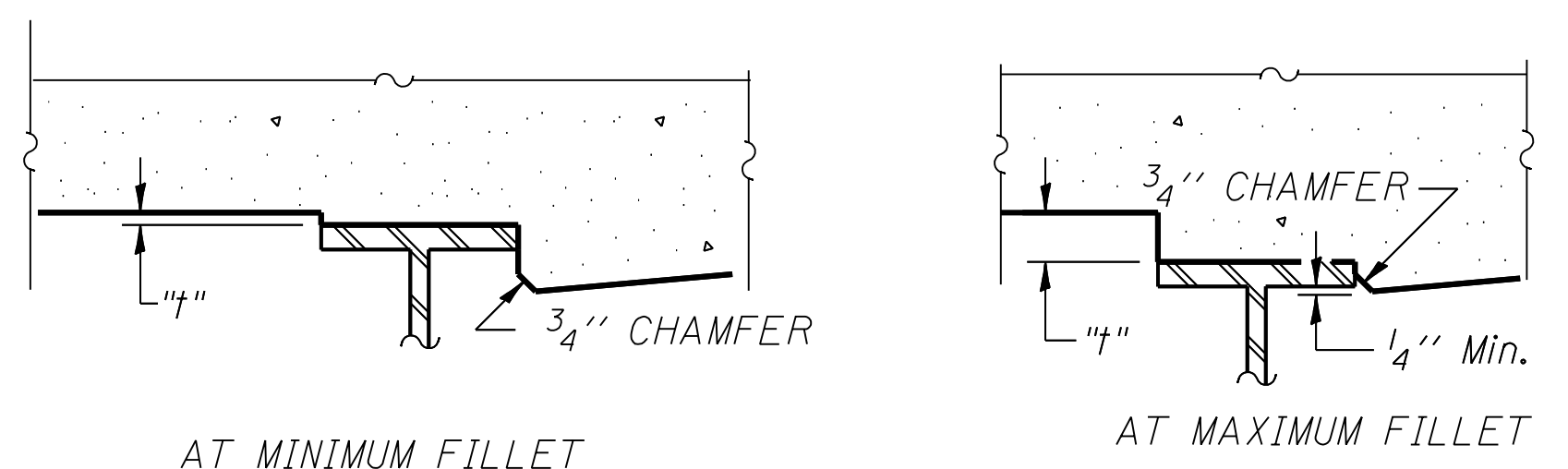
(INCLUDES WEIGHT OF CONCRETE, EXCLUDING BEAMS).

NOTE:  
THE ABOVE DEFLECTIONS ARE NOT FOR USE IN THE FIELD IF THE ENGINEER IS WORKING FROM THE "THEORETICAL GRADE ELEVATIONS ADJUSTED FOR DEAD LOAD DEFLECTION."

GIRDER NUMBER	DEAD LOAD DEFLECTIONS (INCHES)							
	SPAN 1				SPAN 2			
	A	B	C	D	E	F	G	H
1	1 1/4	1 3/8	5/8	3/4	1 5/8	1 1/2	26'-6 1/2"	106'-2 1/16"
2	1 3/4	2	3/4	7/8	2 1/4	2 1/8	26'-7 1/8"	106'-4 7/16"
3	1 3/4	2	3/4	7/8	2 1/4	2 1/8	26'-8 3/16"	106'-8 3/4"
4	1 1/2	1 1/4	1/2	3/4	1 5/8	1 3/8	26'-9 1/4"	107'-1 1/16"
5	1 1/2	1 1/4	1/2	3/4	1 5/8	1 3/8	26'-10 3/8"	107'-5 7/16"
6	1 1/2	1 1/4	1/2	3/4	1 5/8	1 3/8	26'-11 1/16"	107'-9 3/4"
7	1 1/2	1 1/4	1/2	3/4	1 5/8	1 3/8	27'-0 1/2"	108'-2 1/16"
8	1 1/2	1 1/4	1/2	3/4	1 5/8	1 3/8	27'-1 5/8"	108'-6 3/8"
9	1 1/2	1 1/4	1/2	3/4	1 5/8	1 3/8	27'-2 1/16"	108'-10 1/16"
10	1 1/2	1 1/4	1/2	3/4	1 5/8	1 3/8	27'-3 3/4"	109'-3"



**2 THEORETICAL TOP OF SLAB ELEVATIONS FOR BEAMS UNDER NORTH SIDEWALK**



TO DETERMINE "t": AFTER ALL STRUCTURAL STEEL HAS BEEN ERECTED, ELEVATIONS OF THE TOP FLANGES OF THE BEAMS SHALL BE TAKEN AT INTERVALS SHOWN ON SHEETS SBB S-205 & S-206. THESE ELEVATIONS SUBTRACTED FROM THE INDICATED "THEORETICAL GRADE ELEVATIONS ADJUSTED FOR DEAD LOAD DEFLECTION", MINUS SLAB THICKNESS, EQUALS THE FILLET HEIGHTS "t" ABOVE TOP FLANGE OF BEAMS.

**3 FILLET HEIGHTS**

SCALE: N.T.S.



**cta** CHICAGO TRANSIT AUTHORITY ENGINEERING

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**95th Terminal Improvement**  
15 West 95th Street (South Terminal)  
Chicago, IL 60628

IN CHARGE	S. KADERBEK
APPROVED BY	D.ZROKA
CHECKED BY	P.MAID
DESIGNED BY	L.SOMMER
DRAWN BY	T.SLEDGE
PROJECT NO.	2012-0021
FILE NAME	File Name

MARK	DATE	DESCRIPTION
	2/9/17	ISSUED FOR CONSTRUCTION

LOCATION IDENTIFIER: **DR095**

**SOUTH BUS BRIDGE  
TOP OF SLAB ELEVATIONS  
(SHEET 1 OF 4)**

**SBB-S-204**

FILE NAME = 070\_DR095\_S-204.dgn  
PLOT DATE = 8/31/2016



vscarp

GIRDER 1

Table with 5 columns: Location, Station, Offset, Theoretical Grade Elevations, Theoretical Grade Elevations Adjusted For Dead Load Deflection. Rows include Bk W Abut, CL. W. Exp. Jt., CL. Brg W Abut, A through T, Pier 1, and CL. Brg E Abut.

GIRDER 2

Table with 5 columns: Location, Station, Offset, Theoretical Grade Elevations, Theoretical Grade Elevations Adjusted For Dead Load Deflection. Rows include Bk W Abut, CL. W. Exp. Jt., CL. Brg W Abut, A through T, Pier 1, and CL. Brg E Abut.

GIRDER 3

Table with 5 columns: Location, Station, Offset, Theoretical Grade Elevations, Theoretical Grade Elevations Adjusted For Dead Load Deflection. Rows include Bk W Abut, CL. W. Exp. Jt., CL. Brg W Abut, A through T, Pier 1, and CL. Brg E Abut.

GIRDER 4

Table with 5 columns: Location, Station, Offset, Theoretical Grade Elevations, Theoretical Grade Elevations Adjusted For Dead Load Deflection. Rows include Bk W Abut, CL. W. Exp. Jt., CL. Brg W Abut, A through T, Pier 1, and CL. Brg E Abut.

GIRDER 5

Table with 5 columns: Location, Station, Offset, Theoretical Grade Elevations, Theoretical Grade Elevations Adjusted For Dead Load Deflection. Rows include Bk W Abut, CL. W. Exp. Jt., CL. Brg W Abut, A through T, Pier 1, and CL. Brg E Abut.

GIRDER 6

Table with 5 columns: Location, Station, Offset, Theoretical Grade Elevations, Theoretical Grade Elevations Adjusted For Dead Load Deflection. Rows include Bk W Abut, CL. W. Exp. Jt., CL. Brg W Abut, A through T, Pier 1, and CL. Brg E Abut.



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95th Terminal Improvement
15 West 95th Street (South Terminal)
Chicago, IL 60628

Table with 2 columns: Field Name, Value. Includes IN CHARGE (S. KADERBEK), APPROVED BY (D.ZROKA), CHECKED BY (P.MAID), DESIGNED BY (L.SOMMER), DRAWN BY (T.SLEDGE), PROJECT NO. (2012-0021), and a MARK table with DATE (2/9/17) and DESCRIPTION (ISSUED FOR CONSTRUCTION).

LOCATION IDENTIFIER: DR095

SOUTH BUS BRIDGE
TOP OF SLAB ELEVATIONS
(SHEET 2 OF 4)

SBB-S-205

vscarpa

PROFILE GRADE

Table with 5 columns: Location, Station, Offset, Theoretical Grade Elevations, Theoretical Grade Elevations Adjusted For Dead Load Deflection. Rows include Bk W Abut, CL. W. Exp. Jt., CL Brg W Abut, A through T, Pier 1, and Bk E Abut.

GIRDER 7

Table with 5 columns: Location, Station, Offset, Theoretical Grade Elevations, Theoretical Grade Elevations Adjusted For Dead Load Deflection. Rows include Bk W Abut, CL. W. Exp. Jt., CL Brg W Abut, A through T, Pier 1, and Bk E Abut.

GIRDER 8

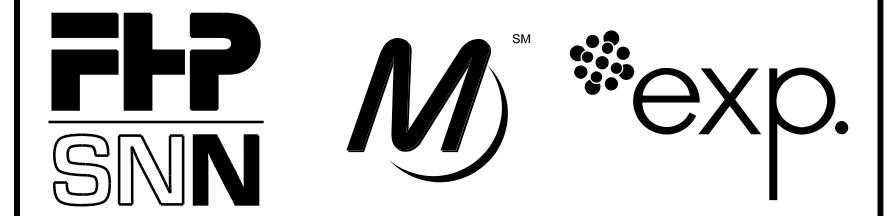
Table with 5 columns: Location, Station, Offset, Theoretical Grade Elevations, Theoretical Grade Elevations Adjusted For Dead Load Deflection. Rows include Bk W Abut, CL. W. Exp. Jt., CL Brg W Abut, A through T, Pier 1, and Bk E Abut.

GIRDER 9

Table with 5 columns: Location, Station, Offset, Theoretical Grade Elevations, Theoretical Grade Elevations Adjusted For Dead Load Deflection. Rows include Bk W Abut, CL. W. Exp. Jt., CL Brg W Abut, A through T, Pier 1, and Bk E Abut.

GIRDER 10

Table with 5 columns: Location, Station, Offset, Theoretical Grade Elevations, Theoretical Grade Elevations Adjusted For Dead Load Deflection. Rows include Bk W Abut, CL. W. Exp. Jt., CL Brg W Abut, A through T, Pier 1, and Bk E Abut.



SENSITIVE SECURITY INFORMATION

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95th Terminal Improvement
15 West 95th Street (South Terminal)
Chicago, IL 60628

Table with 2 columns: Field Name, Value. Includes IN CHARGE (S. KADERBEK), APPROVED BY (D.ZROKA), CHECKED BY (P.MAID), DESIGNED BY (L.SOMMER), DRAWN BY (T.SLEDGE), PROJECT NO. (2012-0021), FILE NAME (File Name).

Table with 3 columns: MARK, DATE, DESCRIPTION. Includes entry for 2/9/17 ISSUED FOR CONSTRUCTION.

LOCATION IDENTIFIER: DR095

SOUTH BUS BRIDGE
TOP OF SLAB ELEVATIONS
(SHEET 3 OF 4)

SBB-S-206

FILE NAME = 070\_DR095\_S-206.dgn
PLOT DATE = 8/31/2016

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**BEAM 1W**

Location	Station	Offset	Theoretical Grade Elevations	Theoretical Grade Elevations Adjusted For Dead Load Deflection
Bk W Abut	300+88.94	43.90	17.78	17.78
CL. W. Exp. Jt.	300+90.88	41.87	17.85	17.85
CL Brg W Abut	300+91.71	41.00	17.89	17.89
U	300+98.60	33.76	18.16	18.20
At Beam 1	301+04.57	27.50	18.40	18.47

**BEAM 2W**

Location	Station	Offset	Theoretical Grade Elevations	Theoretical Grade Elevations Adjusted For Dead Load Deflection
Bk W Abut	300+88.44	36.50	17.76	17.76
CL. W. Exp. Jt.	300+90.53	36.50	17.84	17.84
CL Brg W Abut	300+91.39	36.50	17.87	17.87
At Beam 1W	300+95.99	36.50	18.06	18.08

**BEAM 3W**

Location	Station	Offset	Theoretical Grade Elevations	Theoretical Grade Elevations Adjusted For Dead Load Deflection
Bk W Abut	300+88.12	32.00	17.74	17.74
CL. W. Exp. Jt.	300+90.21	32.00	17.83	17.83
CL Brg W Abut	300+91.08	32.00	17.86	17.86
At Beam 1W	301+00.28	32.00	18.23	18.28

**BEAM 1E**

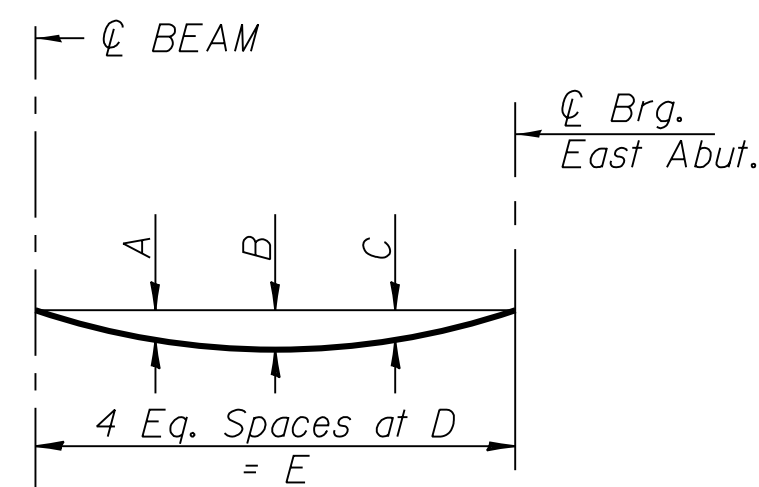
Location	Station	Offset	Theoretical Grade Elevations	Theoretical Grade Elevations Adjusted For Dead Load Deflection
At Beam 1	302+85.55	27.50	21.19	21.28
V	302+92.55	34.64	21.21	21.27
CL Brg E Abut	303+01.74	44.00	21.20	21.20
CL. E. Exp. Jt.	303+02.60	44.87	21.19	21.19
Bk E Abut	303+04.68	46.99	21.18	21.18

**BEAM 2E**

Location	Station	Offset	Theoretical Grade Elevations	Theoretical Grade Elevations Adjusted For Dead Load Deflection
At Beam 1E	302+96.34	38.50	21.22	21.28
CL Brg E Abut	303+01.74	38.50	21.20	21.20
CL. E. Exp. Jt.	303+02.64	38.50	21.19	21.19
Bk E Abut	303+04.65	38.50	21.18	21.18

**BEAM 3E**

Location	Station	Offset	Theoretical Grade Elevations	Theoretical Grade Elevations Adjusted For Dead Load Deflection
At Beam 1E	302+90.95	33.00	21.21	21.25
CL Brg E Abut	303+01.70	33.00	21.20	21.20
CL. E. Exp. Jt.	303+02.55	33.00	21.19	21.19
Bk E Abut	303+04.63	33.00	21.18	21.18



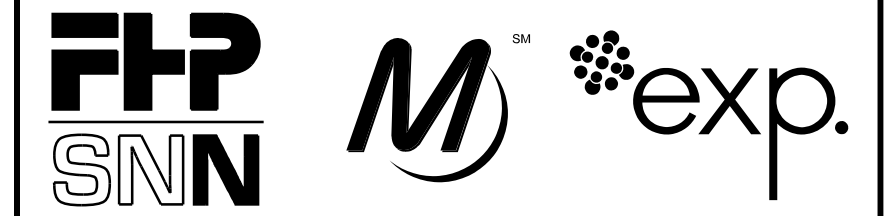
**DEAD LOAD DEFLECTIONS**

(Includes weight of concrete, excluding beams).

Beam	A	B	C	D	E
1E	0	1/8"	0	5'-9 3/8"	23'-1 3/8"

Note: Beams 1W, 2W, 3W, 2E & 3E have no measurable dead load deflection.

Note: All Offsets with respect to Bridge & P.G.L.



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**95th Terminal Improvement**  
 15 West 95th Street (South Terminal)  
 Chicago, IL 60628

IN CHARGE	S. KADERBEK
APPROVED BY	D.ZROKA
CHECKED BY	P.MAID
DESIGNED BY	L.SOMMER
DRAWN BY	T.SLEDGE
PROJECT NO.	2012-0021
FILE NAME	File Name

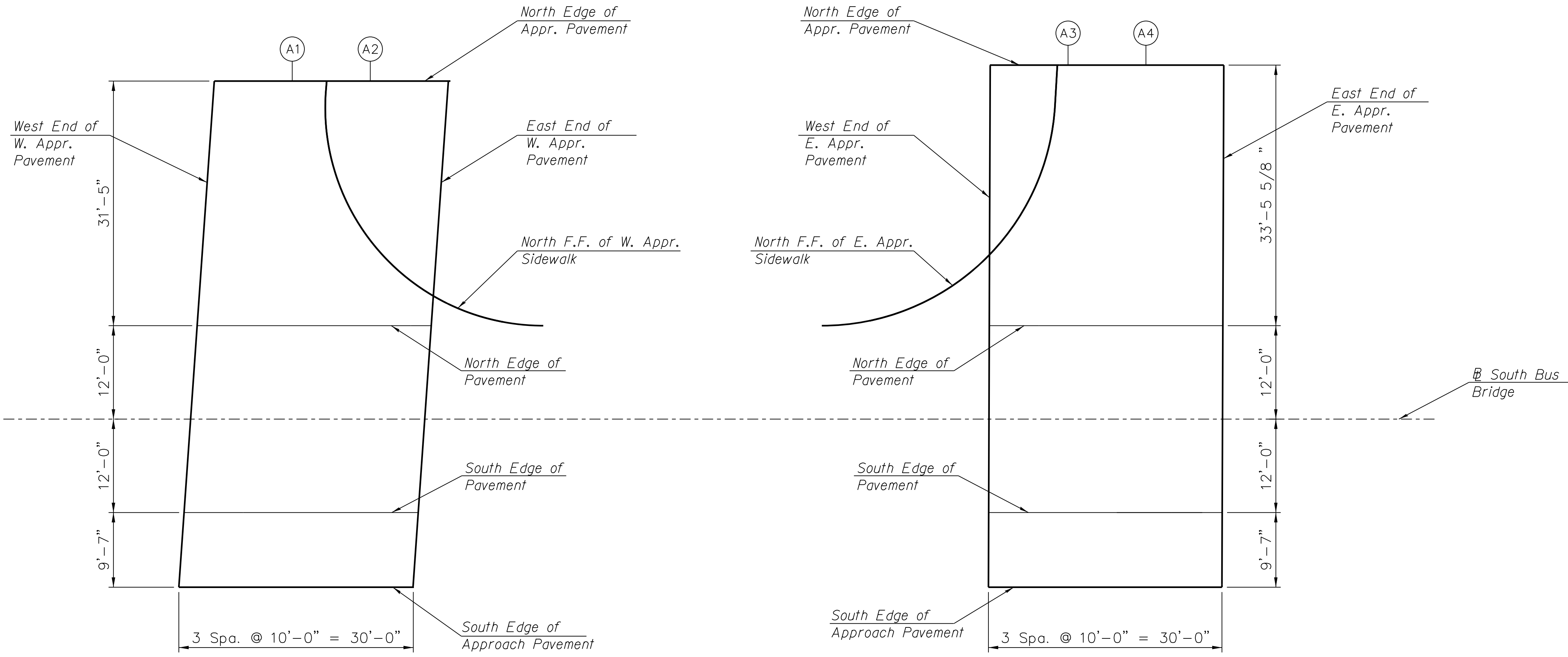
MARK	DATE	DESCRIPTION
	2/9/17	ISSUED FOR CONSTRUCTION

LOCATION IDENTIFIER: **DR095**

SOUTH BUS BRIDGE  
 TOP OF SLAB ELEVATIONS  
 (SHEET 4 OF 4)

**SBB-S-206A**

FILE NAME = 070\_DR095\_S-206A.dgn  
 PLOT DATE = 8/31/2016



PLAN

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FILE NAME = 070\_DR095\_S-207.DGN  
PLOT DATE = 8/31/2016



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FILE NAME	File Name

MARK	DATE	DESCRIPTION
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LOCATION IDENTIFIER: **DR095**  
SOUTH BUS BRIDGE  
TOP OF APPROACH SLAB ELEVATIONS  
(SHEET 1 OF 2)

**SBB-S-207**

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**NORTH EDGE OF APPROACH**

Location	Station	Offset	Theoretical Grade Elevations
West End W. Appr. Pav't.	300+59.34	+43.42	17.65
A1	300+69.37	+43.42	17.91
North F.F. W. Appr. Sidewalk	300+73.79	+43.42	18.68
A2	300+79.39	+43.42	18.78
East End W. Appr. Pav't.	300+89.42	+43.42	18.95
West End E. Appr. Pav't.	303+04.17	+45.47	21.71
North F.F. E. Appr. Sidewalk	303+12.80	+45.47	21.58
A3	303+14.17	+45.47	20.89
A4	303+24.17	+45.47	20.74
East End E. Appr. Pav't.	303+34.17	+45.47	20.57

**NORTH EDGE OF PAVEMENT**

Location	Station	Offset	Theoretical Grade Elevations
West End W. Appr. Pav't.	300+57.15	+12.00	16.58
A1	300+67.17	+12.00	16.91
A2	300+77.20	+12.00	17.31
East End W. Appr. Pav't.	300+87.22	+12.00	17.71
West End E. Appr. Pav't.	303+04.05	+12.00	21.18
A3	303+14.05	+12.00	21.06
A4	303+24.05	+12.00	20.88
East End E. Appr. Pav't.	303+34.05	+12.00	20.68

**TOP OF SOUTH BUS BRIDGE**

Location	Station	Offset	Theoretical Grade Elevations
West End W. Appr. Pav't.	300+56.31	0.00	16.65
A1	300+66.33	0.00	17.05
A2	300+76.36	0.00	17.45
East End W. Appr. Pav't.	300+86.38	0.00	17.85
West End E. Appr. Pav't.	303+04.01	0.00	21.36
A3	303+14.01	0.00	21.24
A4	303+24.01	0.00	21.06
East End E. Appr. Pav't.	303+34.01	0.00	20.86

**SOUTH EDGE OF PAVEMENT**

Location	Station	Offset	Theoretical Grade Elevations
West End W. Appr. Pav't.	300+55.47	-12.00	16.44
A1	300+65.49	-12.00	16.84
A2	300+75.52	-12.00	17.24
East End W. Appr. Pav't.	300+85.54	-12.00	17.64
West End E. Appr. Pav't.	303+03.97	-12.00	21.18
A3	303+13.97	-12.00	21.06
A4	303+23.97	-12.00	20.88
East End E. Appr. Pav't.	303+33.97	-12.00	20.68

**SOUTH EDGE OF APPROACH**

Location	Station	Offset	Theoretical Grade Elevations
West End W. Appr. Pav't.	300+54.80	-21.58	16.24
A1	300+64.82	-21.58	16.62
A2	300+74.85	-21.58	17.02
East End W. Appr. Pav't.	300+84.87	-21.58	17.42
West End E. Appr. Pav't.	303+03.94	-21.58	20.99
A3	303+13.94	-21.58	20.87
A4	303+23.94	-21.58	20.69
East End E. Appr. Pav't.	303+33.94	-21.58	20.49



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 15 West 95th Street (South Terminal)  
 Chicago, IL 60628

IN CHARGE	S. KADERBEK
APPROVED BY	D.ZROKA
CHECKED BY	P.MAID
DESIGNED BY	L.SOMMER
DRAWN BY	T.SLEDGE
PROJECT NO.	2012-0021
FILE NAME	File Name

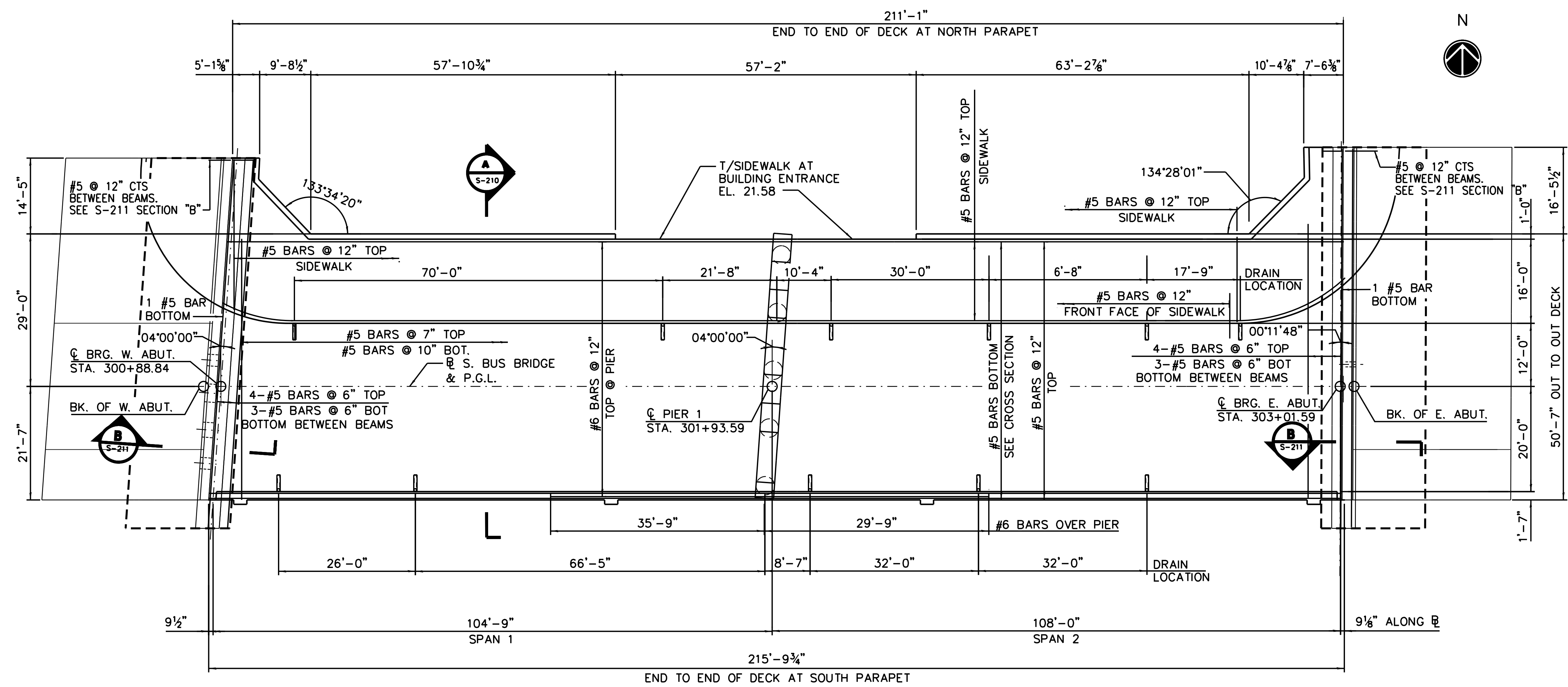
MARK	DATE	DESCRIPTION
	2/9/17	ISSUED FOR CONSTRUCTION

LOCATION IDENTIFIER: **DR095**  
 SOUTH BUS BRIDGE  
 TOP OF APPROACH SLAB ELEVATIONS  
 (SHEET 2 OF 2)

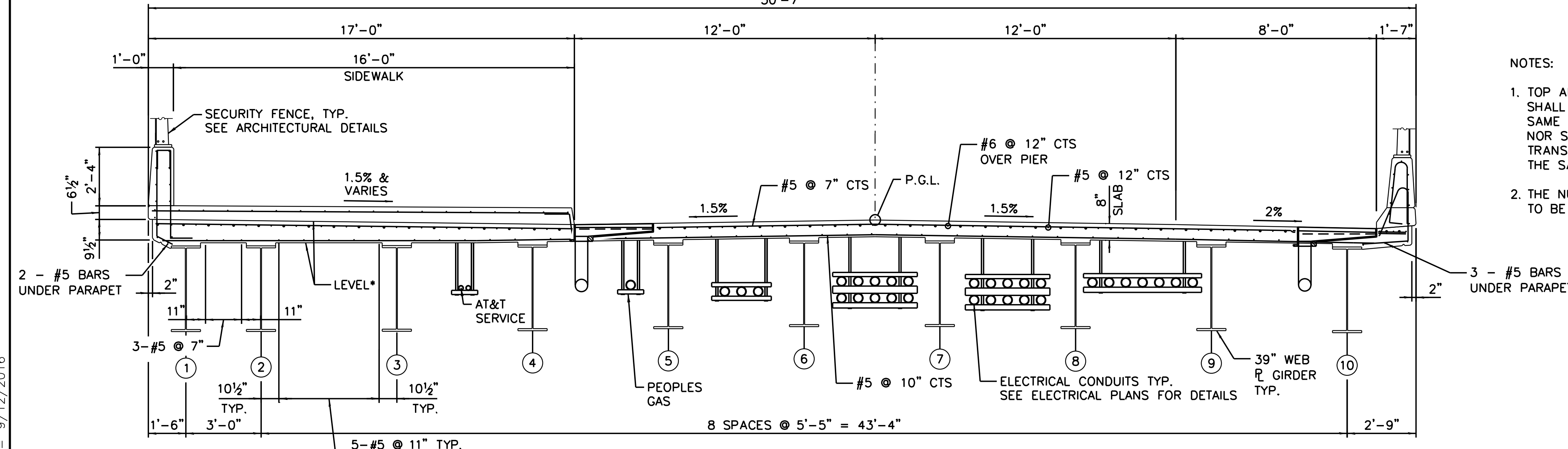
**SBB-S-208**

FILE NAME = 070\_DR095\_S-208.dgn  
 PLOT DATE = 8/31/2016

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1 PLAN



A CROSS SECTION

\* BOTTOM OF SLAB LEVEL UNDER SIDEWALK

- NOTES:
- TOP AND LONGITUDINAL BARS SHALL NOT BE LAPPED AT THE SAME LOCATIONS IN THE DECK, NOR SHOULD THE TOP AND BOTTOM TRANSVERSE BARS BE LAPPED AT THE SAME LOCATION.
  - THE NUMBER OF UTILITIES AND LOCATION TO BE VERIFIED IN THE FIELD.



CHICAGO TRANSIT AUTHORITY ENGINEERING

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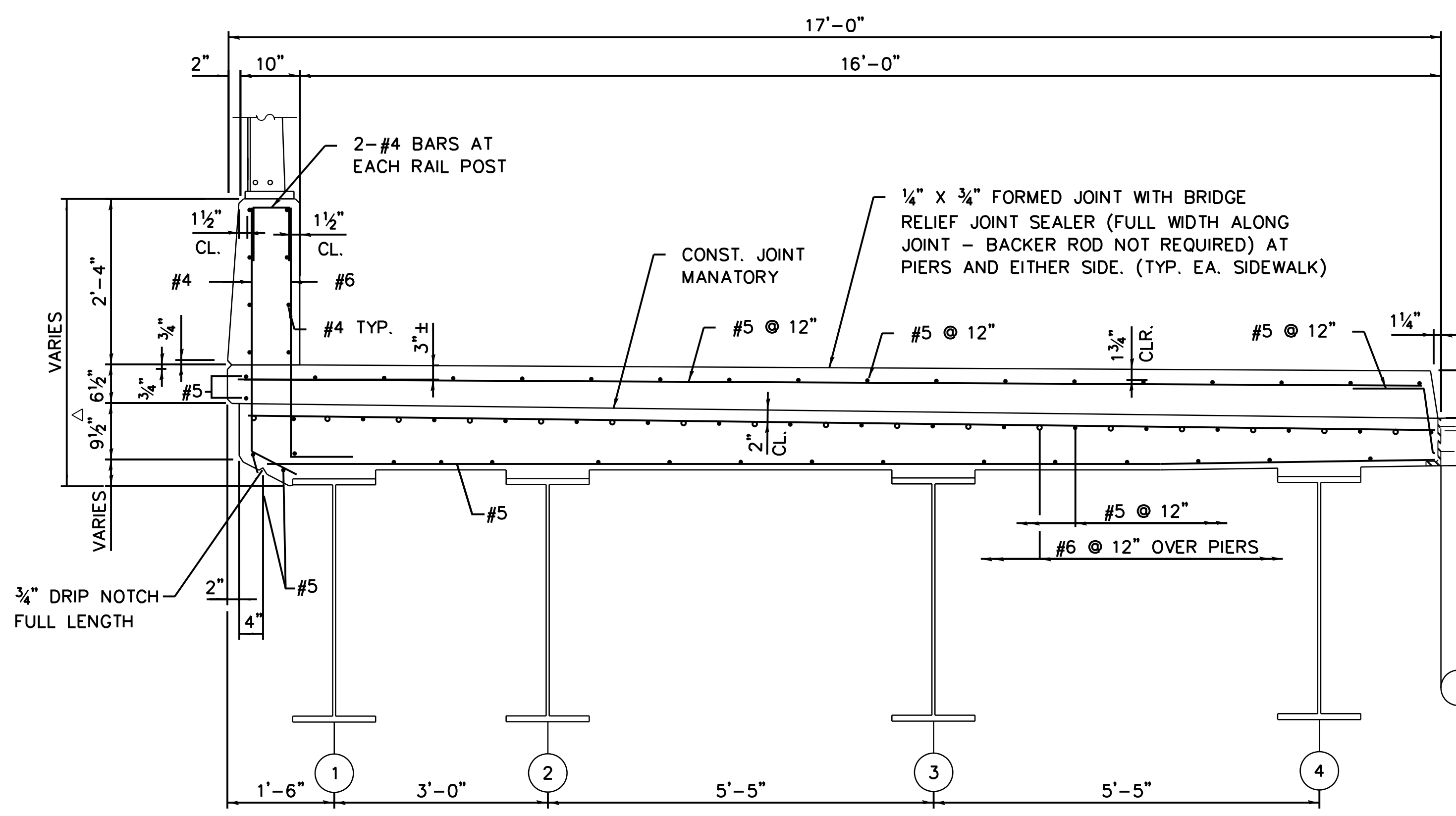
LOCATION IDENTIFIER: DR095

SOUTH BUS BRIDGE SUPERSTRUCTURE

**SBB-S-210**

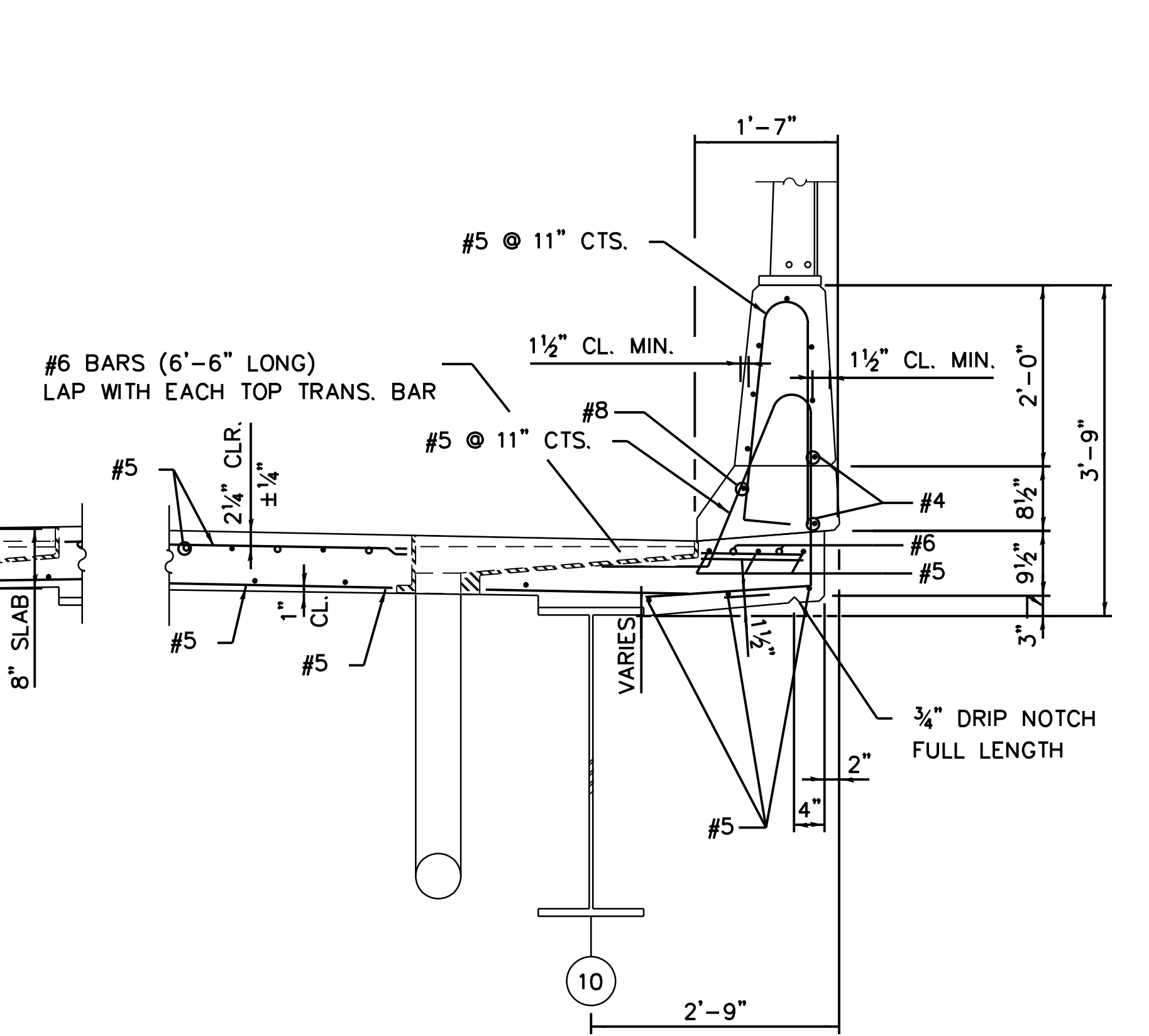
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 PLOT DATE = 9/12/2016

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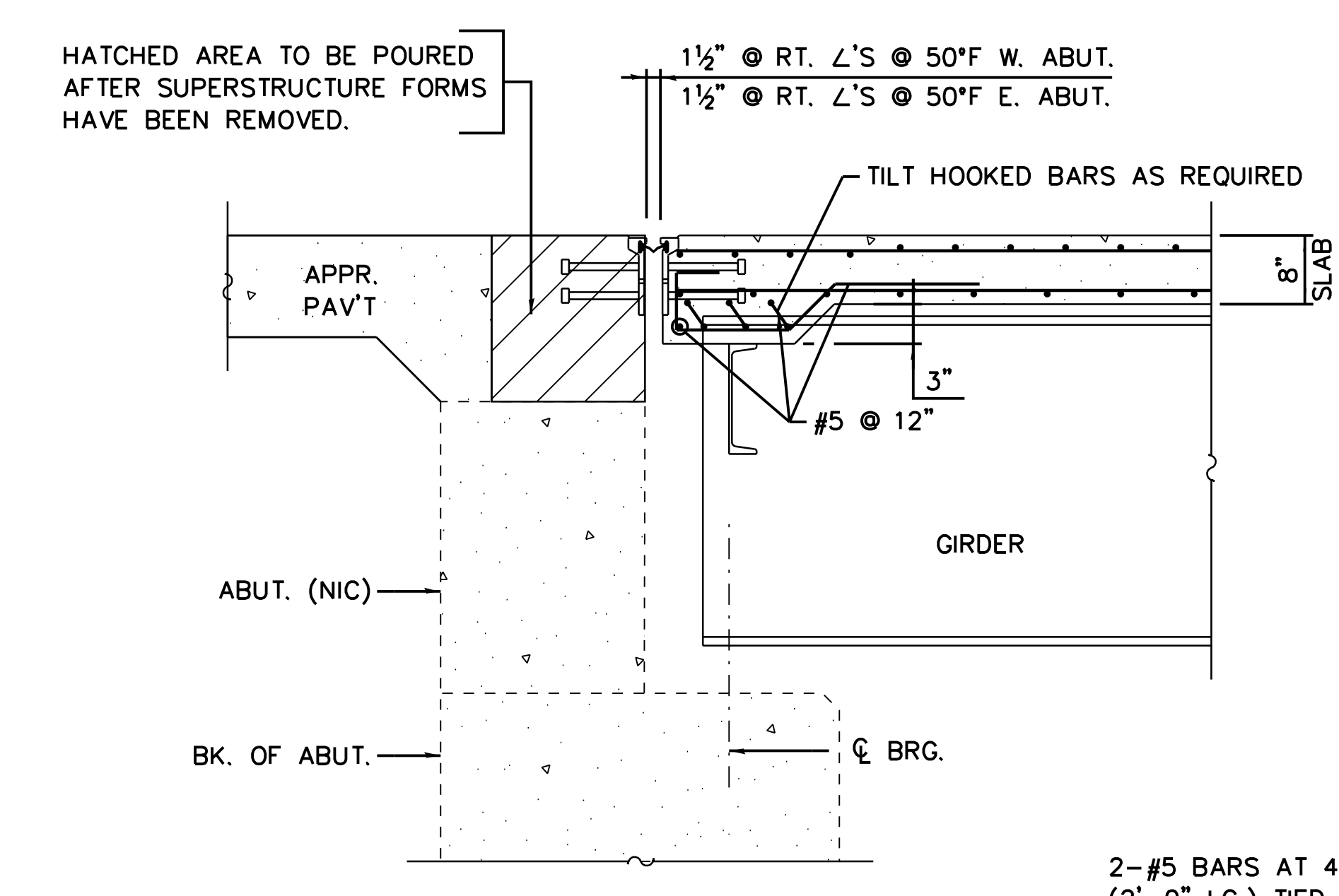


1 SECTION THRU NORTH SIDEWALK  
SCALE: N.T.S.

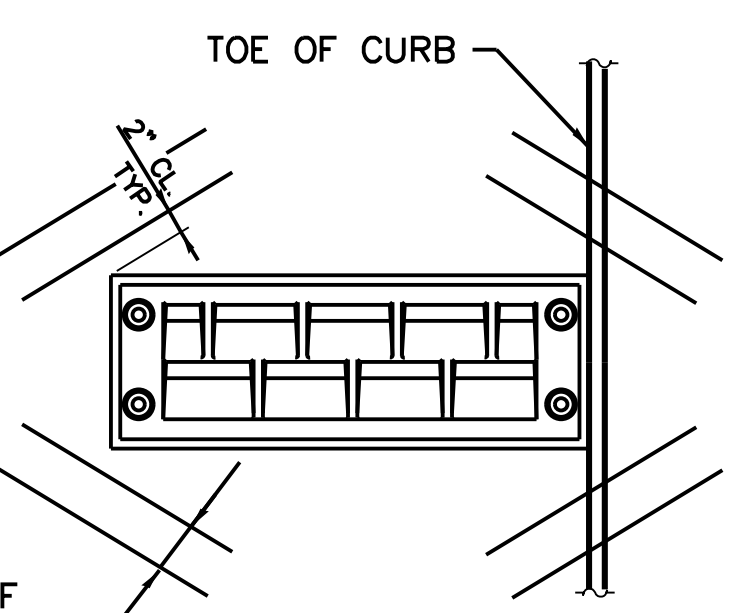
△ Thickness of sidewalk varies at concourse entrance



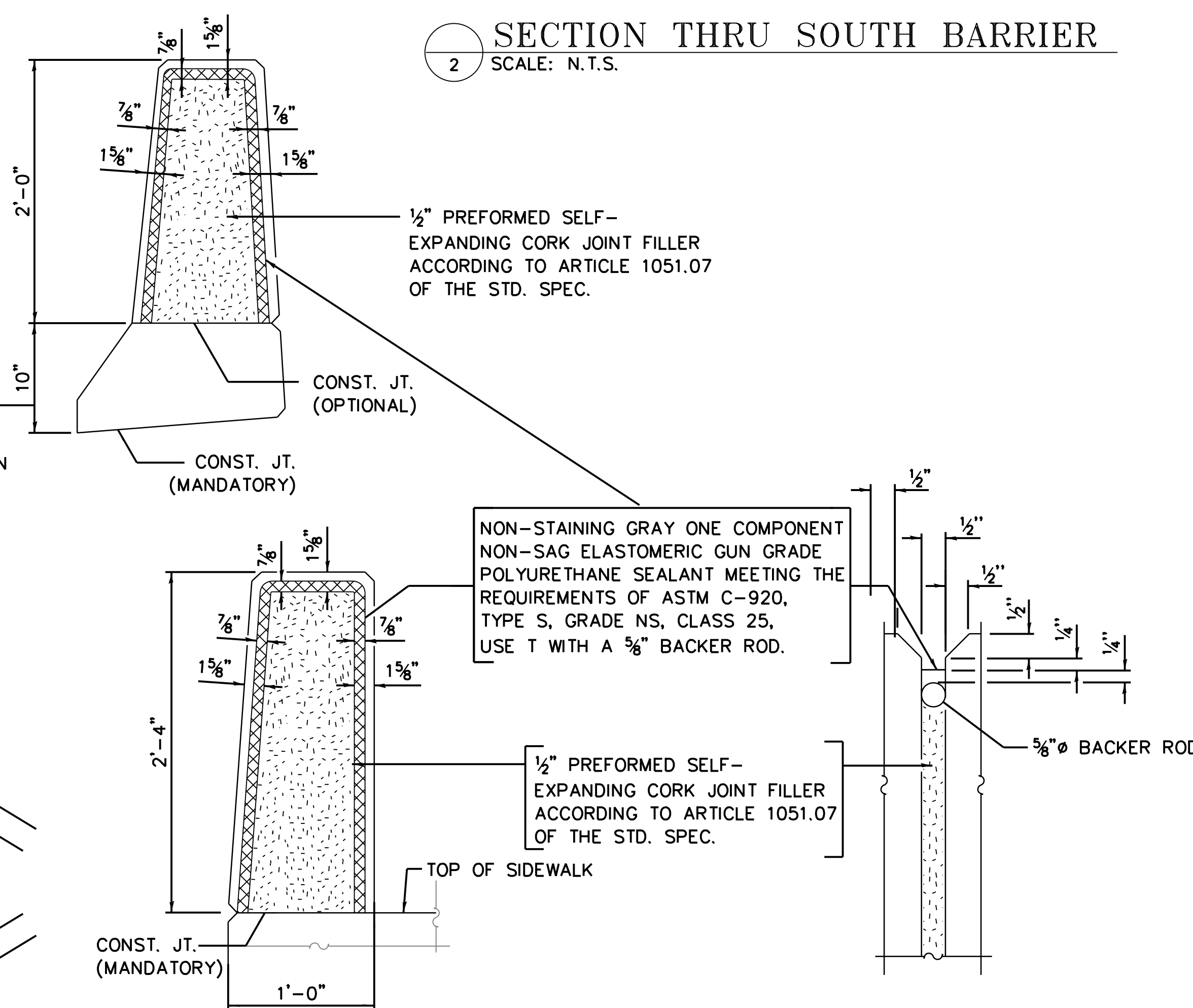
2 SECTION THRU SOUTH BARRIER  
SCALE: N.T.S.



B SECTION  
5-210 SCALE: N.T.S.



SCUPPER DETAIL



3 PARAPET JOINT DETAILS  
SCALE: N.T.S.



**cta** CHICAGO TRANSIT AUTHORITY ENGINEERING

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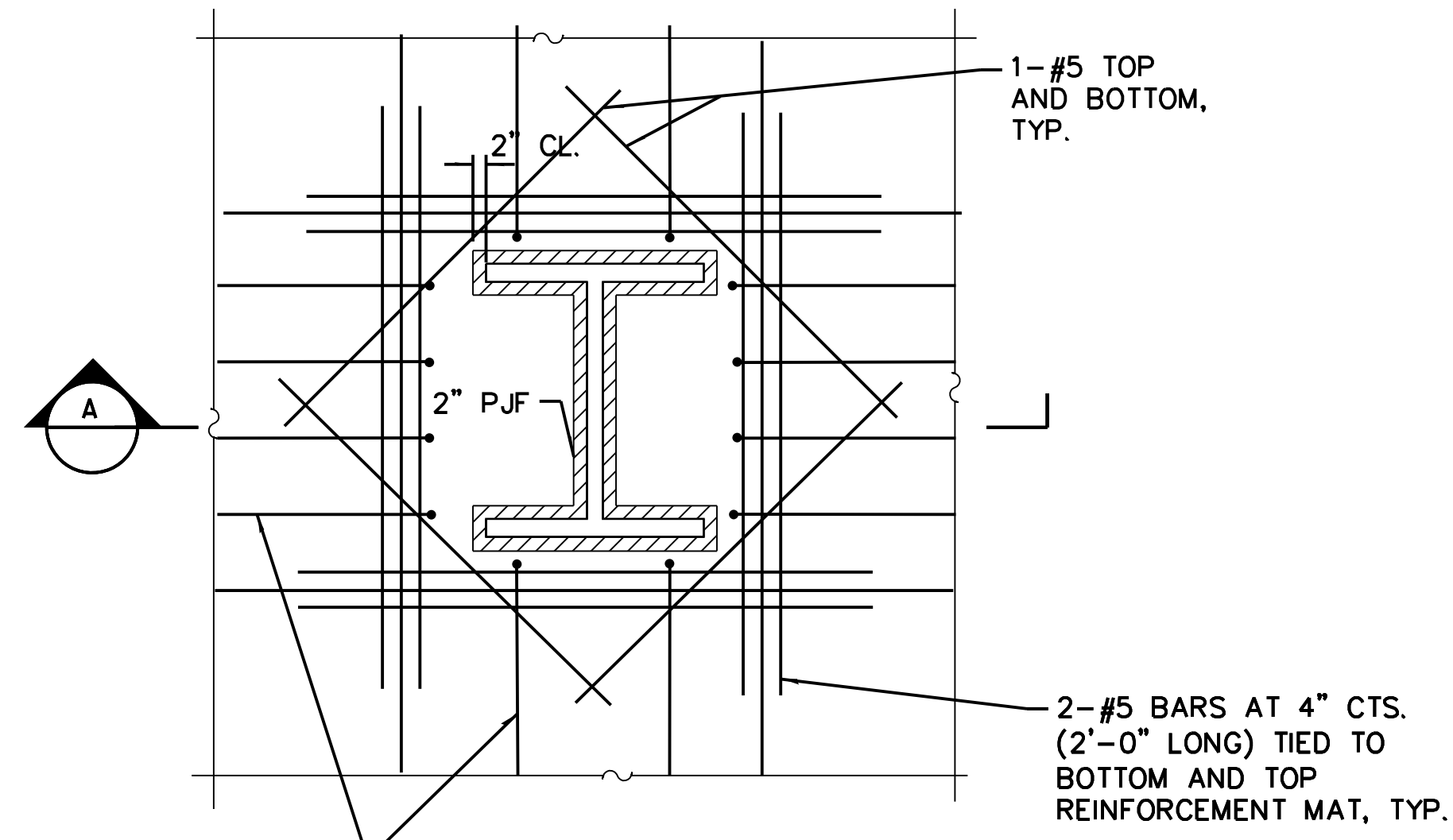
MARK	DATE	DESCRIPTION
	2/9/17	ISSUED FOR CONSTRUCTION

LOCATION IDENTIFIER: **DR095**  
SOUTH BUS BRIDGE  
SUPERSTRUCTURE DETAILS  
(SHEET 1 OF 2)

**SBB-S-211**

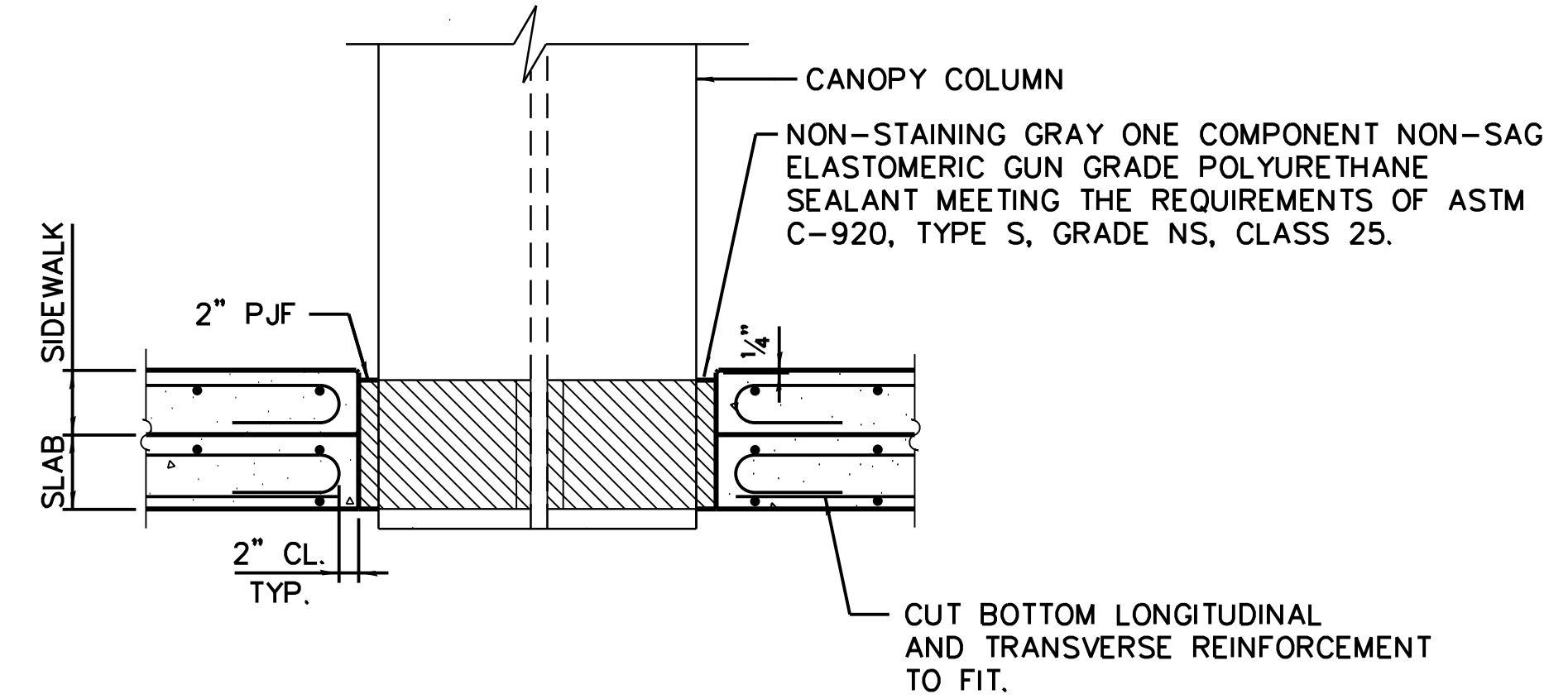
FILE NAME = 070\_DR095\_S-211.dgn  
PLOT DATE = 8/31/2016

vscarpqa



BEND TOP LONGITUDINAL AND TRANSVERSE REINFORCEMENT WITH STANDARD 180° HOOK

**PLAN**  
SCALE: N.T.S.



**SECTION A-A**  
STEEL COLUMN CONNECTION NOT SHOWN FOR CLARITY.



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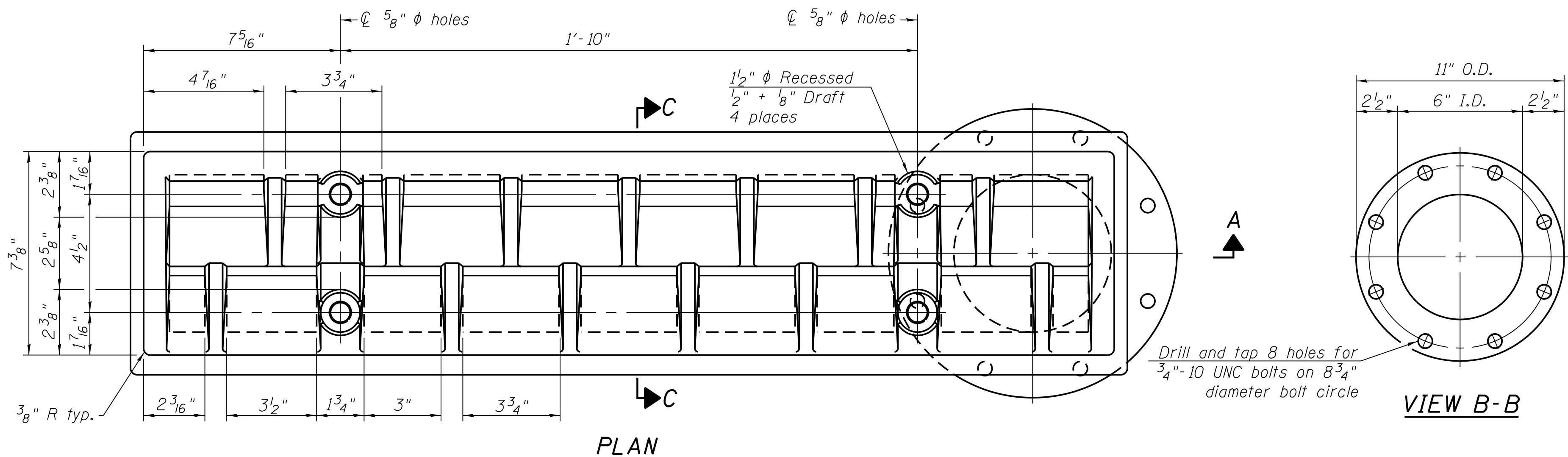
LOCATION IDENTIFIER: **DR095**  
 SOUTH BUS BRIDGE  
 SUPERSTRUCTURE DETAILS  
 (SHEET 2 OF 2)

**SBB-S-212**

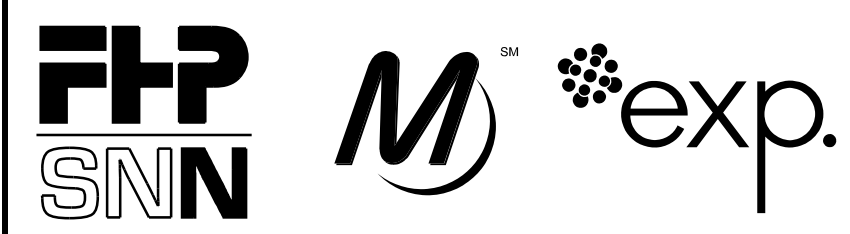
FILE NAME = 070\_DR095\_S-212.dgn  
 PLOT DATE = 10/21/2016



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**Notes:**  
 Refer to Drainage system specification, section 33 41 10.  
 All cast iron parts shall be gray iron conforming to the requirements of AASHTO M 105, Class 35B.  
 Bolts, anchor studs, washers and nuts shall conform to the requirements of ASTM A 307 and shall be galvanized according to AASHTO M 232.  
 As an alternate, bolts, anchor studs, washers and nuts may be stainless steel according to Article 1006.29(d) of the IDOT Standard Specifications.  
 Structural steel weldments of equal sections and of the same configuration may be substituted for the cast iron scupper frame. Fillet or full penetration welds shall be used for the weldments. Details shall be submitted to the Engineer for approval. Structural steel weldments shall not be substituted for the cast iron scupper grate. Structural steel frames and downspouts shall be galvanized according to AASHTO M111.  
 The Contractor shall take appropriate measures to assure that Protective Coat is not applied to the scupper.  
 Alternate fiberglass downspout conforming to ASTM D 2996 with a short-time rupture strength hoop tensile stress of 30,000 psi min. may be used in lieu of the cast iron or steel equivalent.



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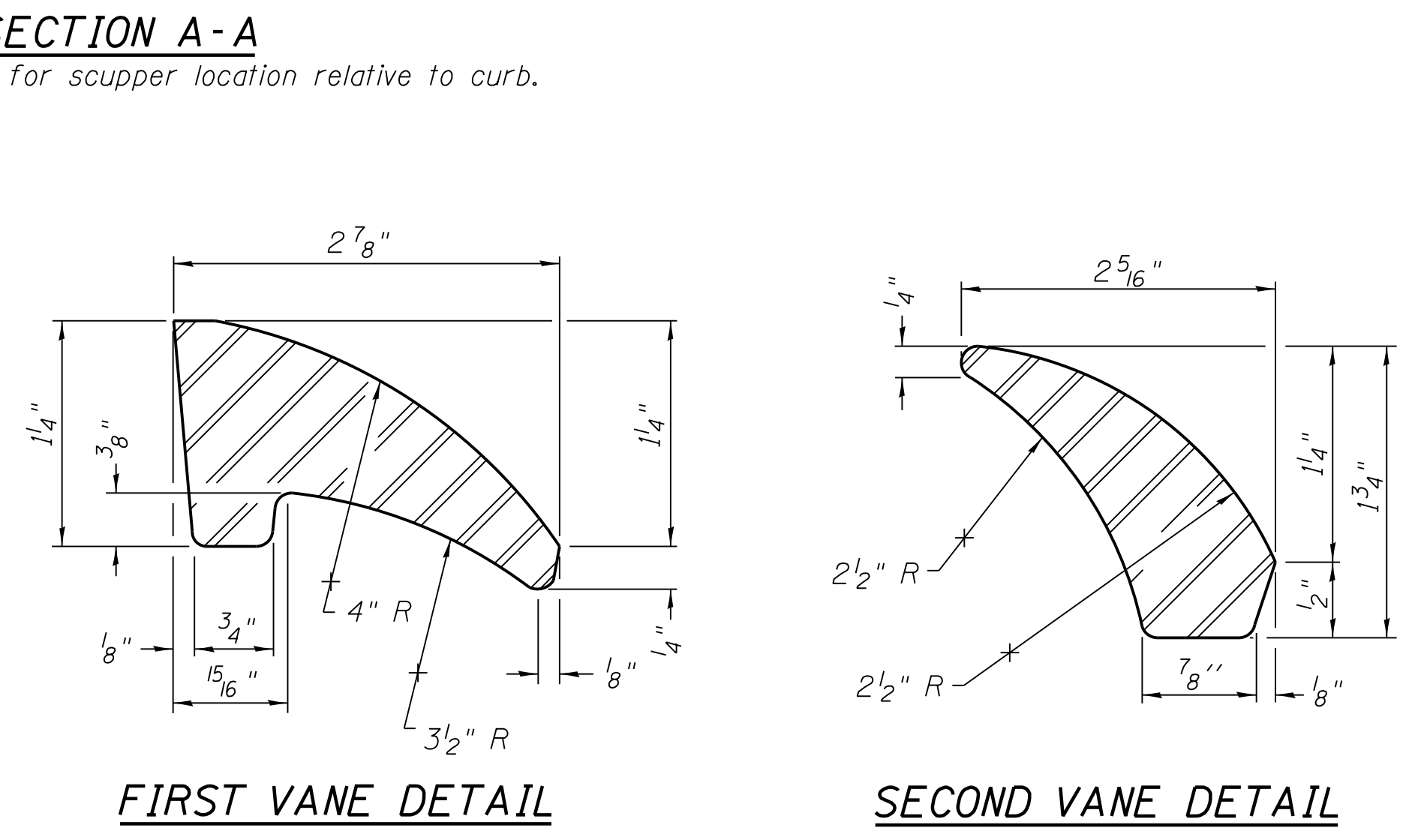
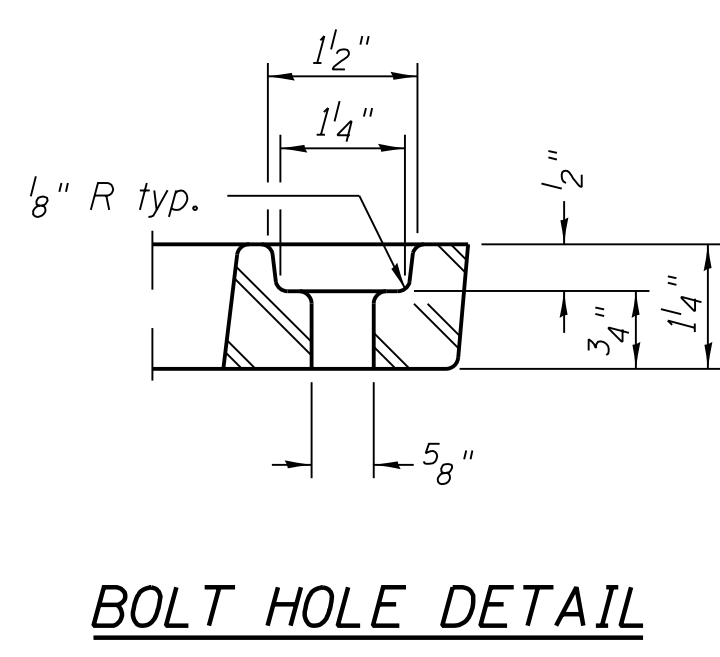
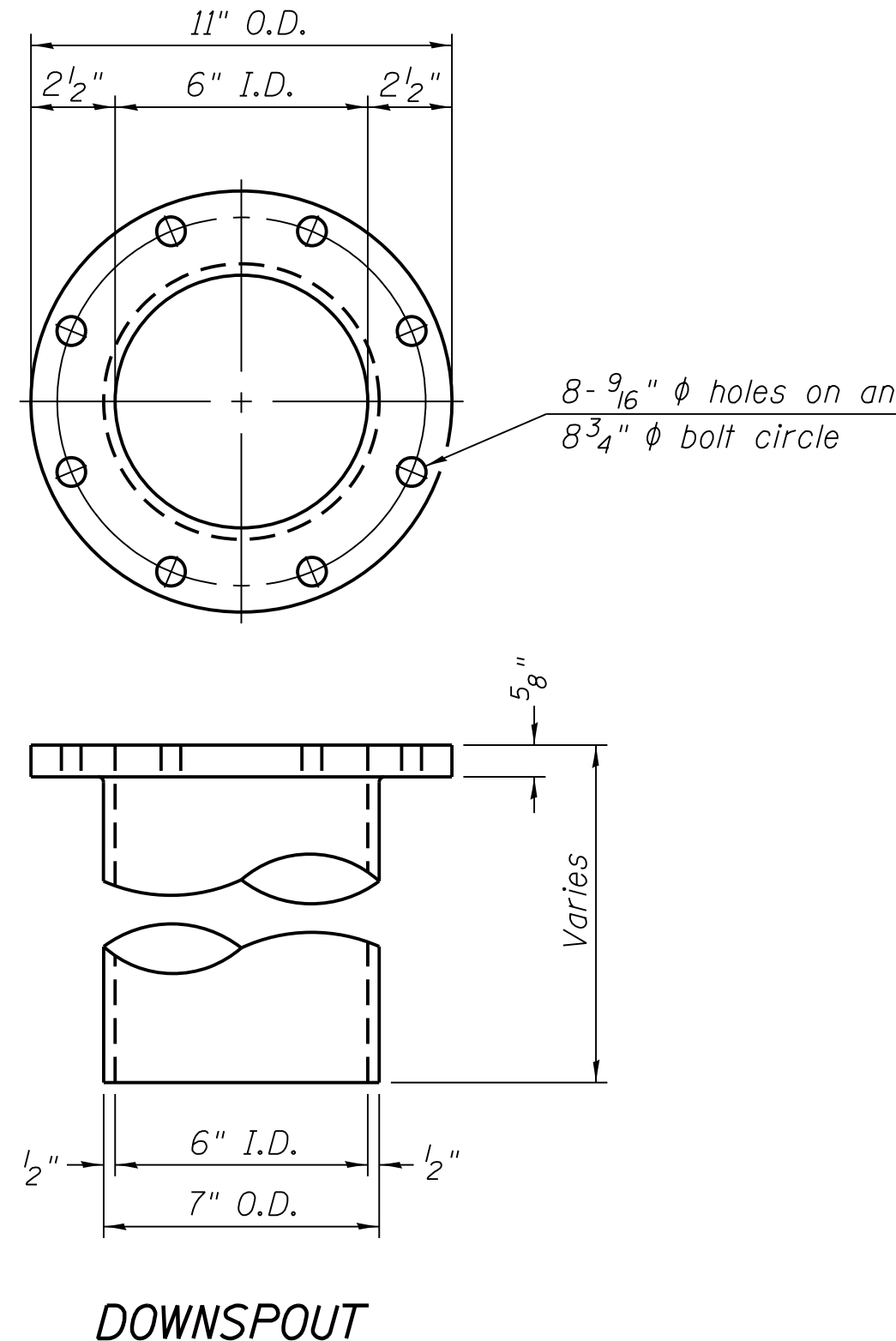
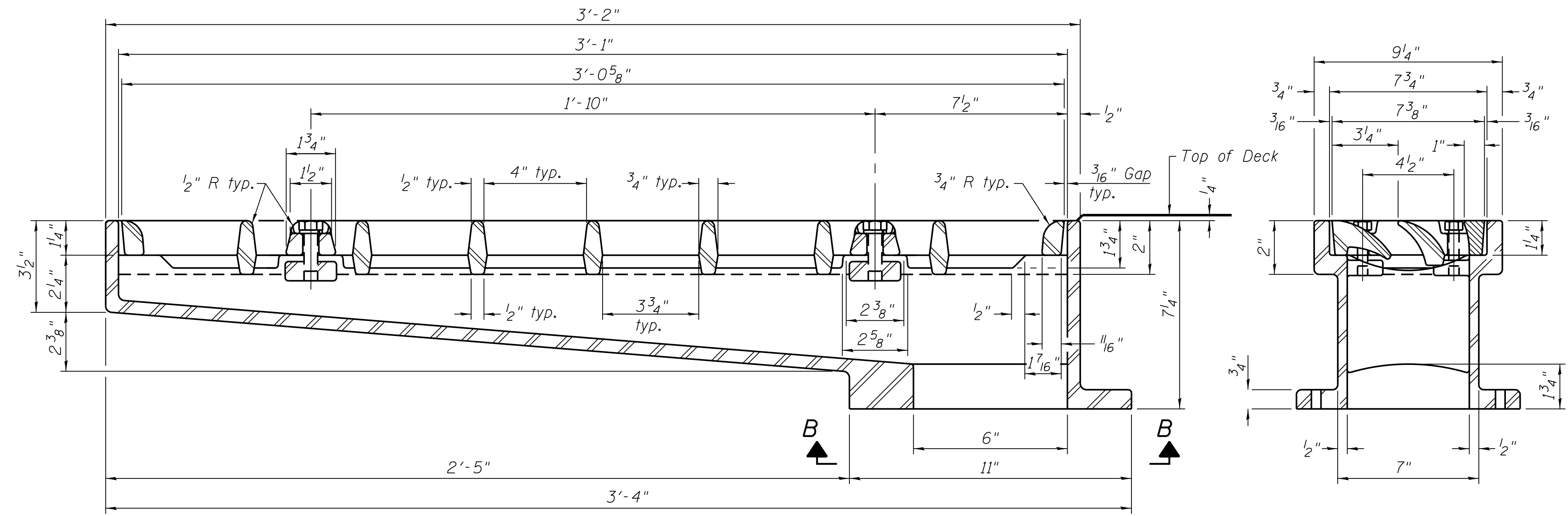
IN CHARGE	S. KADERBEK
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DRAWN BY	T.SLEDGE
PROJECT NO.	2012-0021
FILE NAME	File Name

2/9/17	ISSUED FOR CONSTRUCTION	
MARK	DATE	DESCRIPTION

LOCATION IDENTIFIER: **DR095**

**SOUTH BUS BRIDGE DRAINAGE SCUPPER, DS-33**

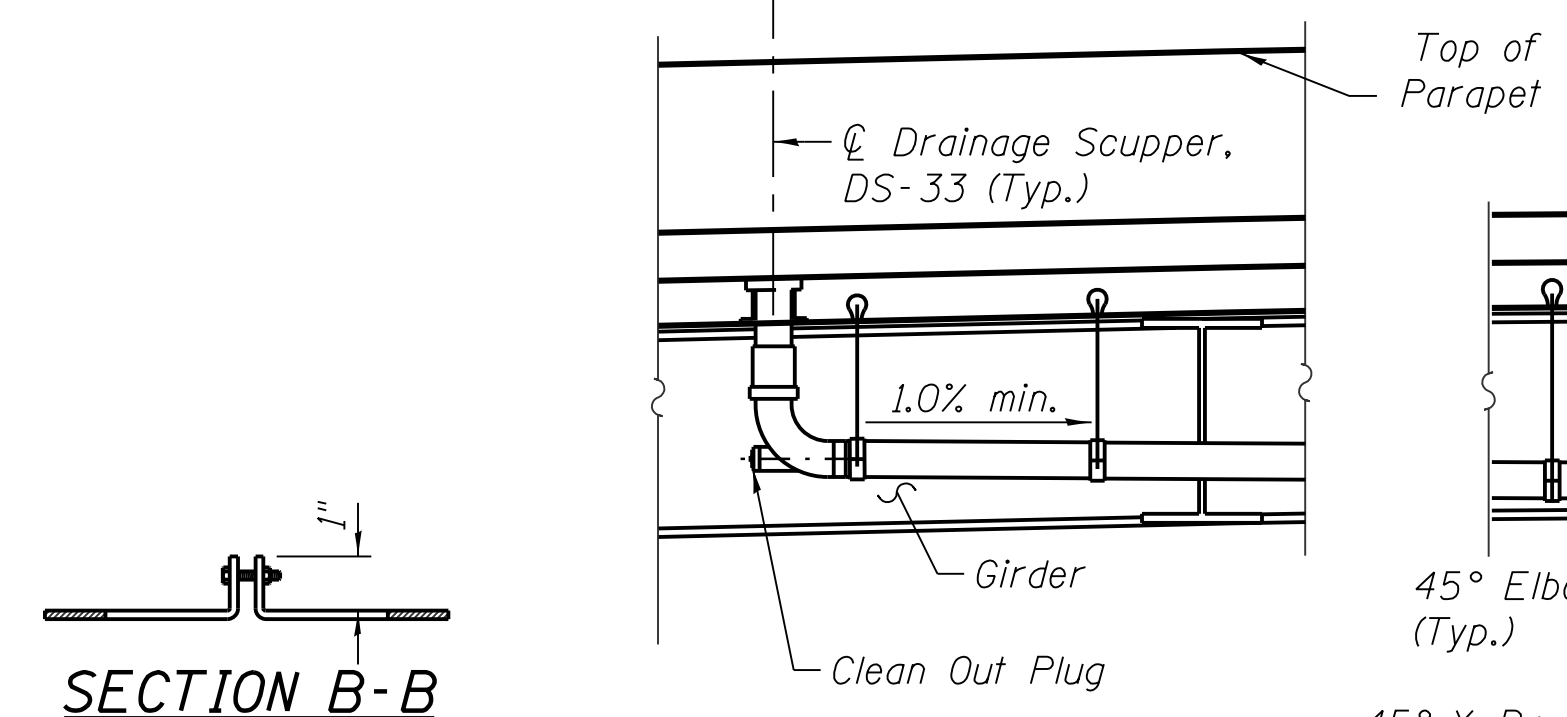
**SBB-S-212A**



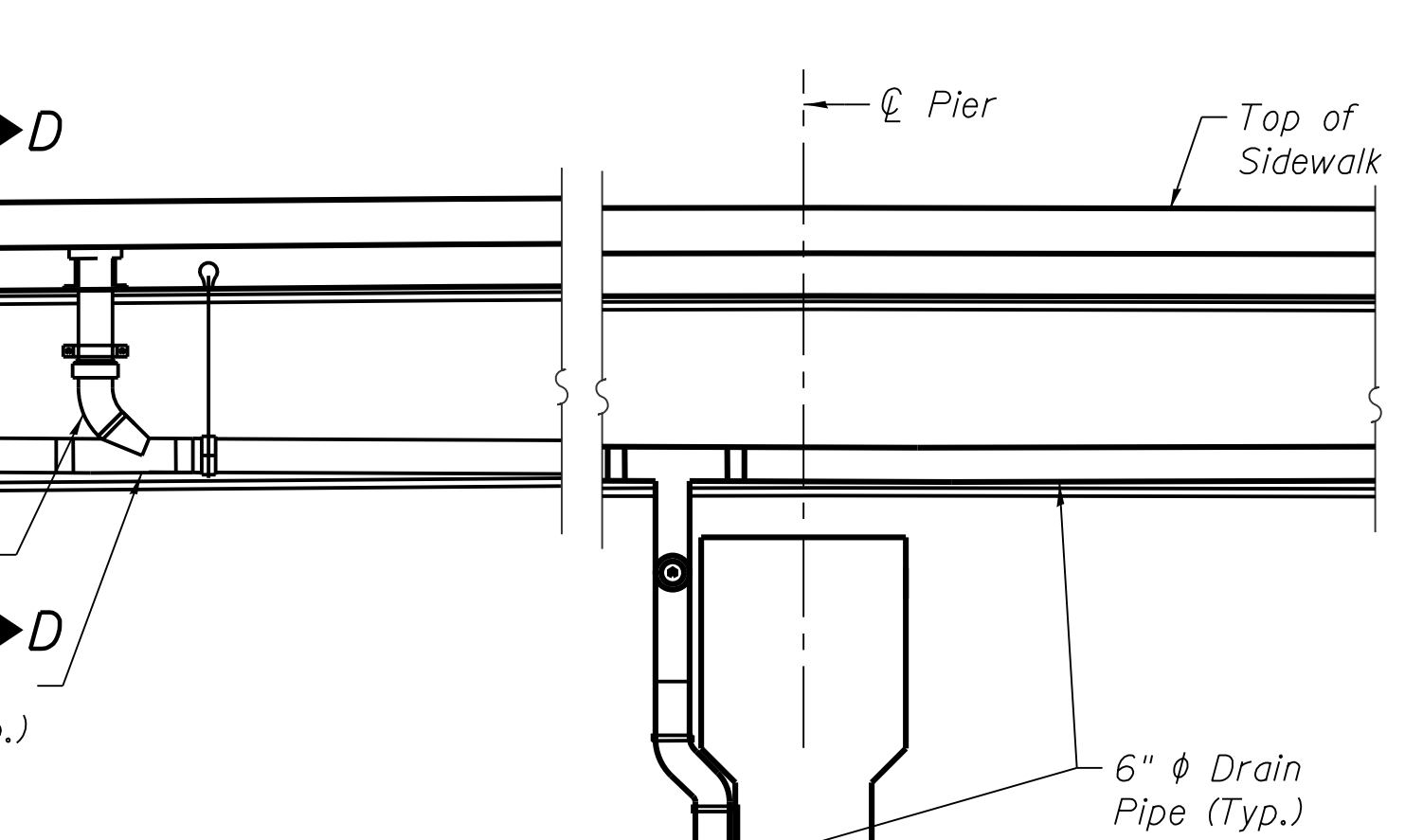
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 PLOT DATE = 8/31/2016

vscarpa

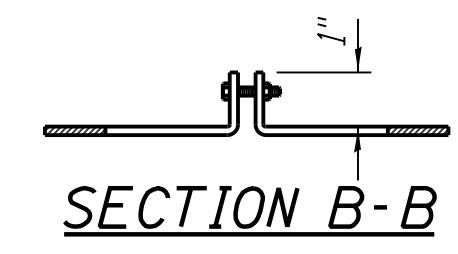
See Sheet SBB-S-210 for Spacing



**SPAN 1 ELEVATION**  
(Looking North)



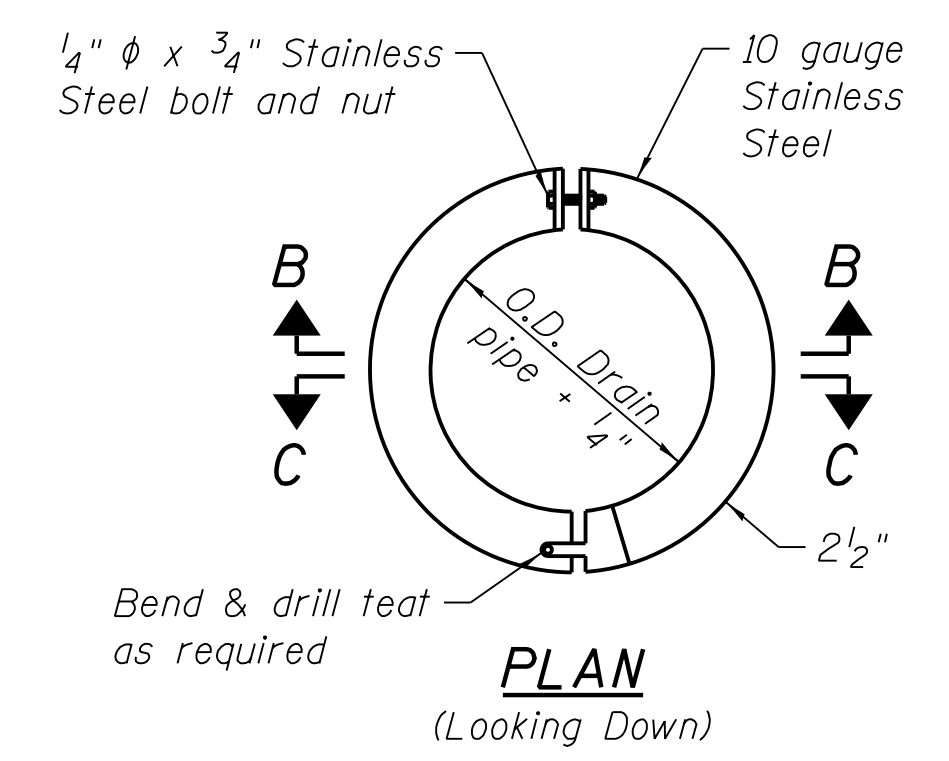
**SPAN 2 ELEVATION**  
(Looking North)



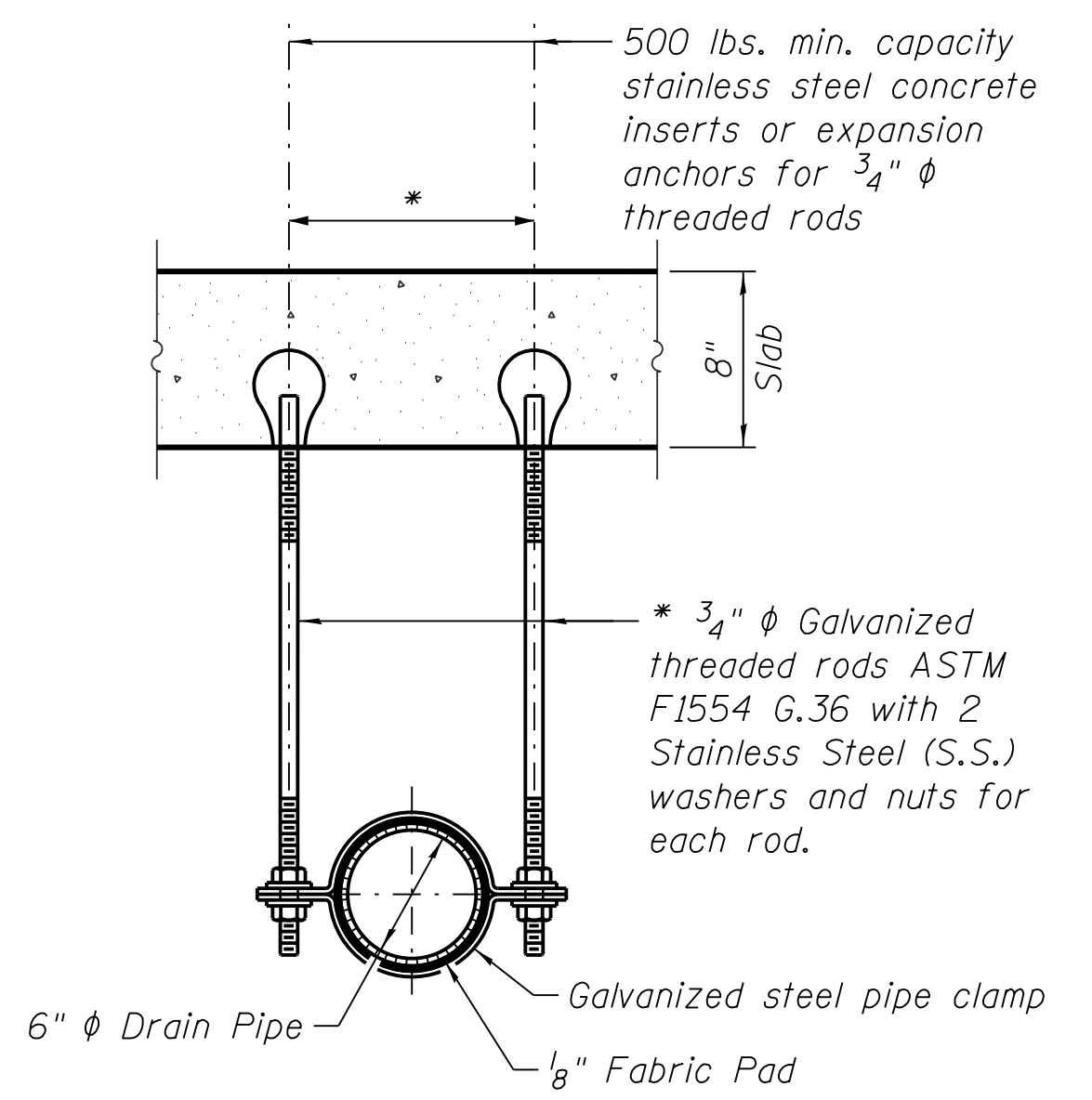
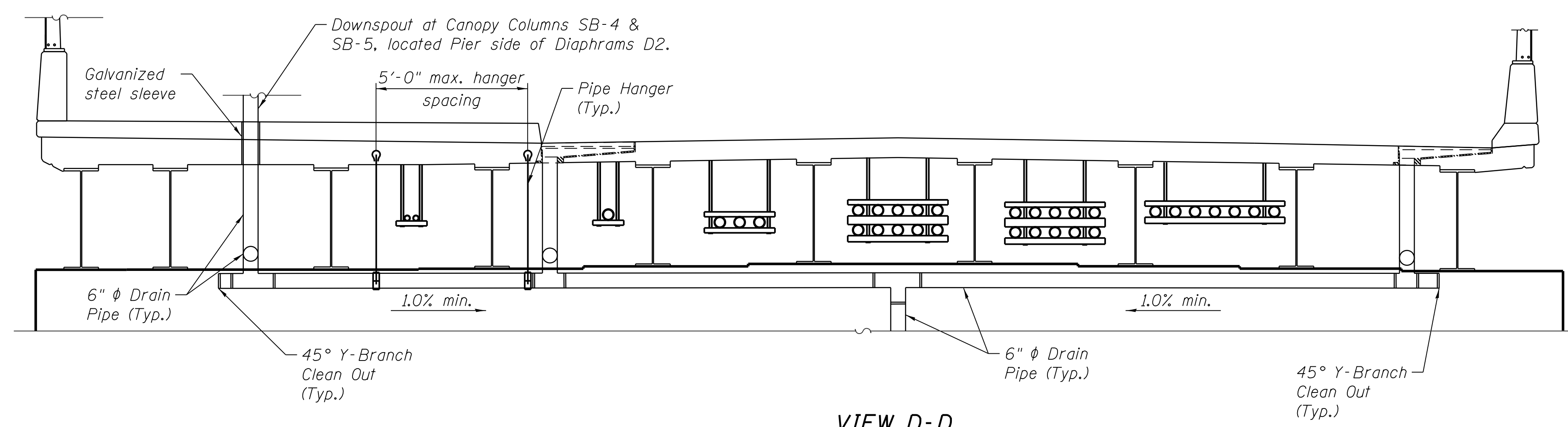
Stainless Steel 5/32" Ø Teat (bend as rivet (round head loose fit))

**SECTION C-C**

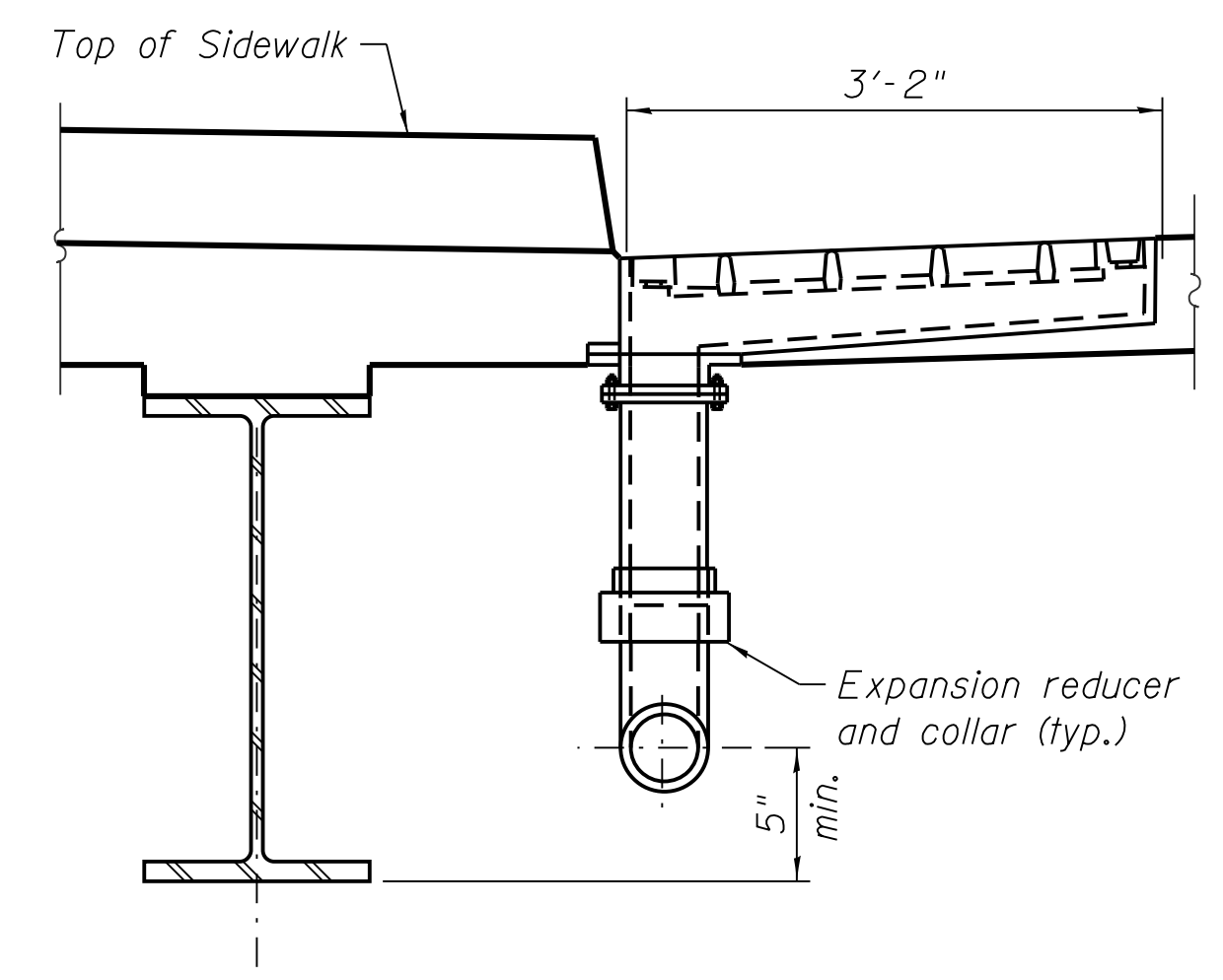
See Drainage Plans for outlet along C.T.A. tracks.



**EXPANSION COLLAR DETAILS**



\* Dimension as required by pipe clamp



**SECTION THRU SIDEWALK CURB**  
(North sidewalk curb shown, south curb with downspout on opposite side)

Notes:

- See sheet SBB-S-212A for drainage scupper details.
- Scuppers & bridge drainage system shall be located clear of all diaphragms.
- Fiberglass pipe shall conform to ASTM D 2996, RTRP short-time rupture strength hoop tensile stress of 30,000 p.s.i. minimum.
- Pipe supports shall be provided on all pipes at 5'-0" max. spacing.
- All pipe hangers, supports and hardware shall be hot-dipped galvanized in accordance with AASHTO M232 (ASTM A153).
- All bolts, nuts and washers shall be Stainless Steel, Type 304.
- All exposed surfaces of scuppers, hangers, brackets and piping shall be painted.
- Refer to Drainage system specification, section 33 41 10.



**CHICAGO TRANSIT AUTHORITY ENGINEERING**

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**95th Terminal Improvement**  
 15 West 95th Street (South Terminal)  
 Chicago, IL 60628

IN CHARGE	S. KADERBEK
APPROVED BY	D.ZROKA
CHECKED BY	P.MAID
DESIGNED BY	L.SOMMER
DRAWN BY	T.SLEDGE
PROJECT NO.	2012-0021
FILE NAME	File Name

MARK	DATE	DESCRIPTION
	2/9/17	ISSUED FOR CONSTRUCTION

LOCATION IDENTIFIER: **DR095**

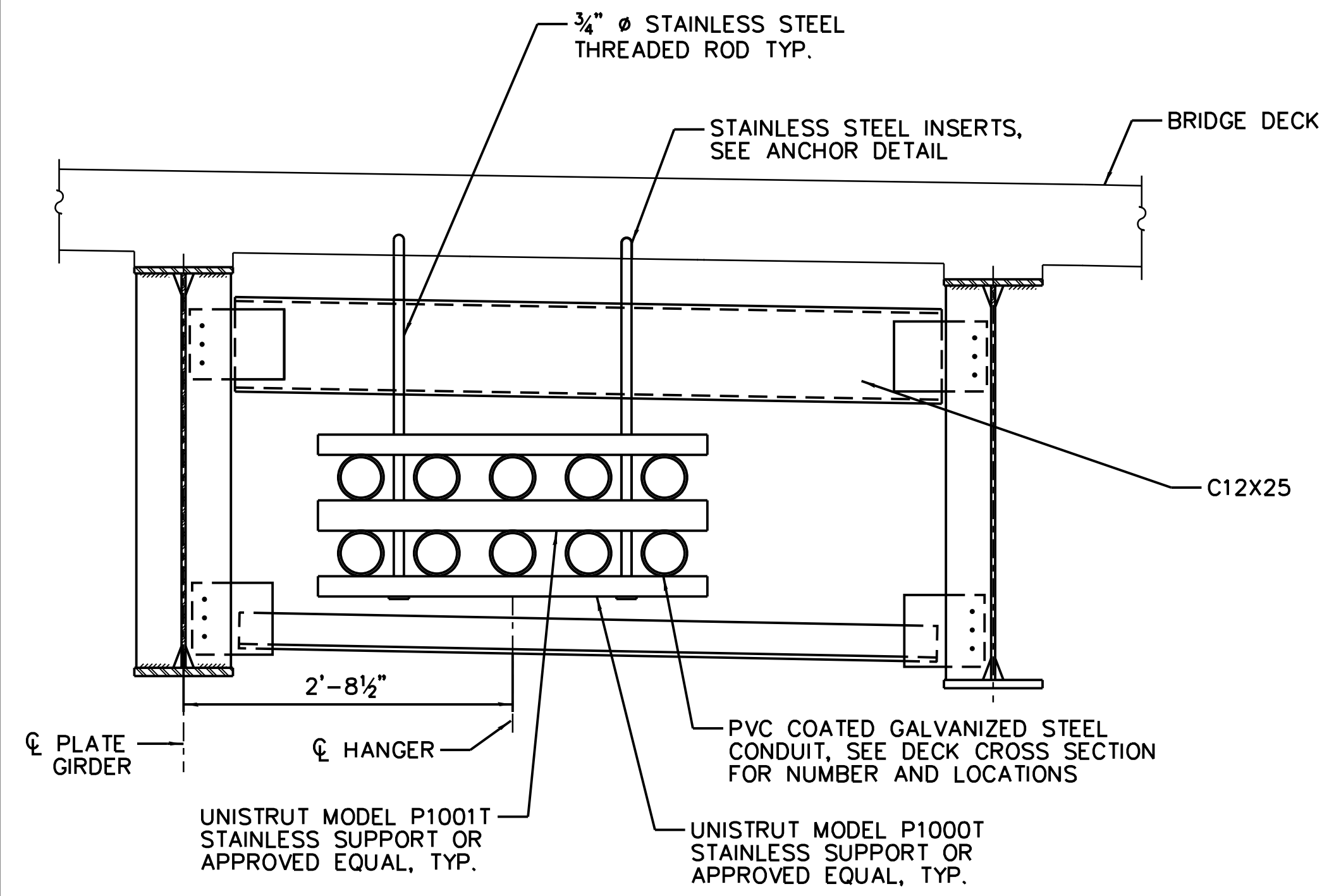
**SOUTH BUS BRIDGE  
 CLOSED DRAINAGE SYSTEM**

**SBB-S-212B**

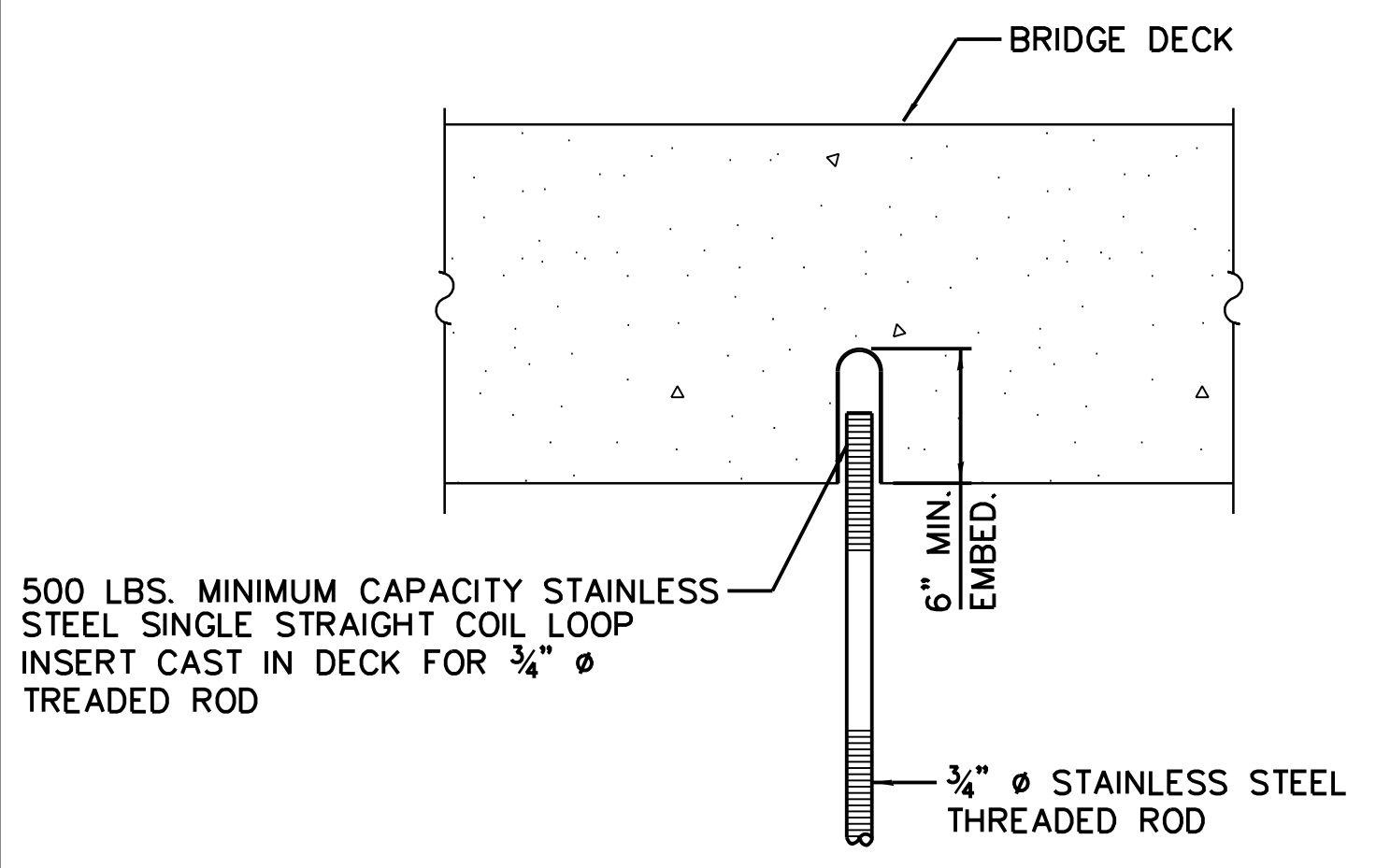
FILE NAME = 070\_DR095\_S-212B.dgn  
 PLOT DATE = 9/12/2016



vscorppa

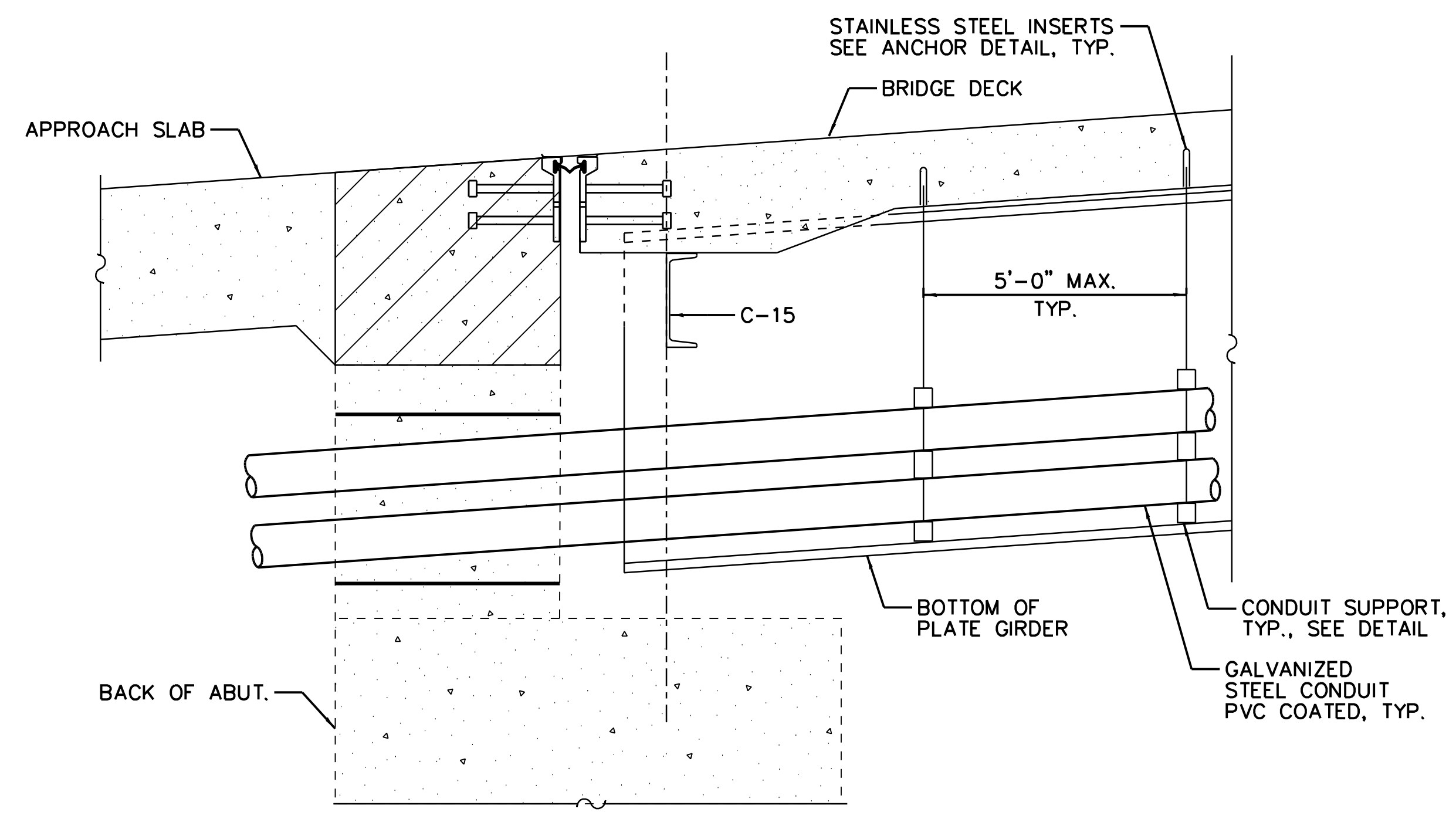


1 TYPICAL CONDUIT SUPPORT



3 THREADED ROD INSTALLATION ANCHOR DETAIL

500 LBS. MINIMUM CAPACITY STAINLESS STEEL SINGLE STRAIGHT COIL LOOP INSERT CAST IN DECK FOR 3/4\"/>



2 TYPICAL CONDUIT INSTALLATION

NOTES:

1. THE CONTRACTOR MUST USE APPROVED SINGLE STRAIGHT COIL LOOP INSERTS WHEN PENDANT MOUNTING THREADED RODS TO A NEW BRIDGE DECK. THE FLARED LOOP INSERTS MUST BE CAST INTO THE CONCRETE DECK.
2. THE BRIDGE CONTRACTOR SHALL COORDINATE THE CONCRETE INSERTS.
3. CONDUIT SHALL NOT COME INTO CONTACT WITH ANY BRACING OR OTHER STRUCTURAL MEMBERS.
4. CONDUIT SHALL BE SUPPORTED AT A MAXIMUM INTERVAL OF 5'. SUPPORTS SHALL BE LOCATED A MAXIMUM OF 3' AWAY FROM A CONNECTION.
5. ALL HARDWARE SHALL BE STAINLESS STEEL UNLESS NOTED OTHERWISE.



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 Chicago, IL 60628

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CHECKED BY	P.MAID
DESIGNED BY	L.SOMMER
DRAWN BY	T.SLEDGE
PROJECT NO.	2012-0021
FILE NAME	File Name

MARK	DATE	DESCRIPTION
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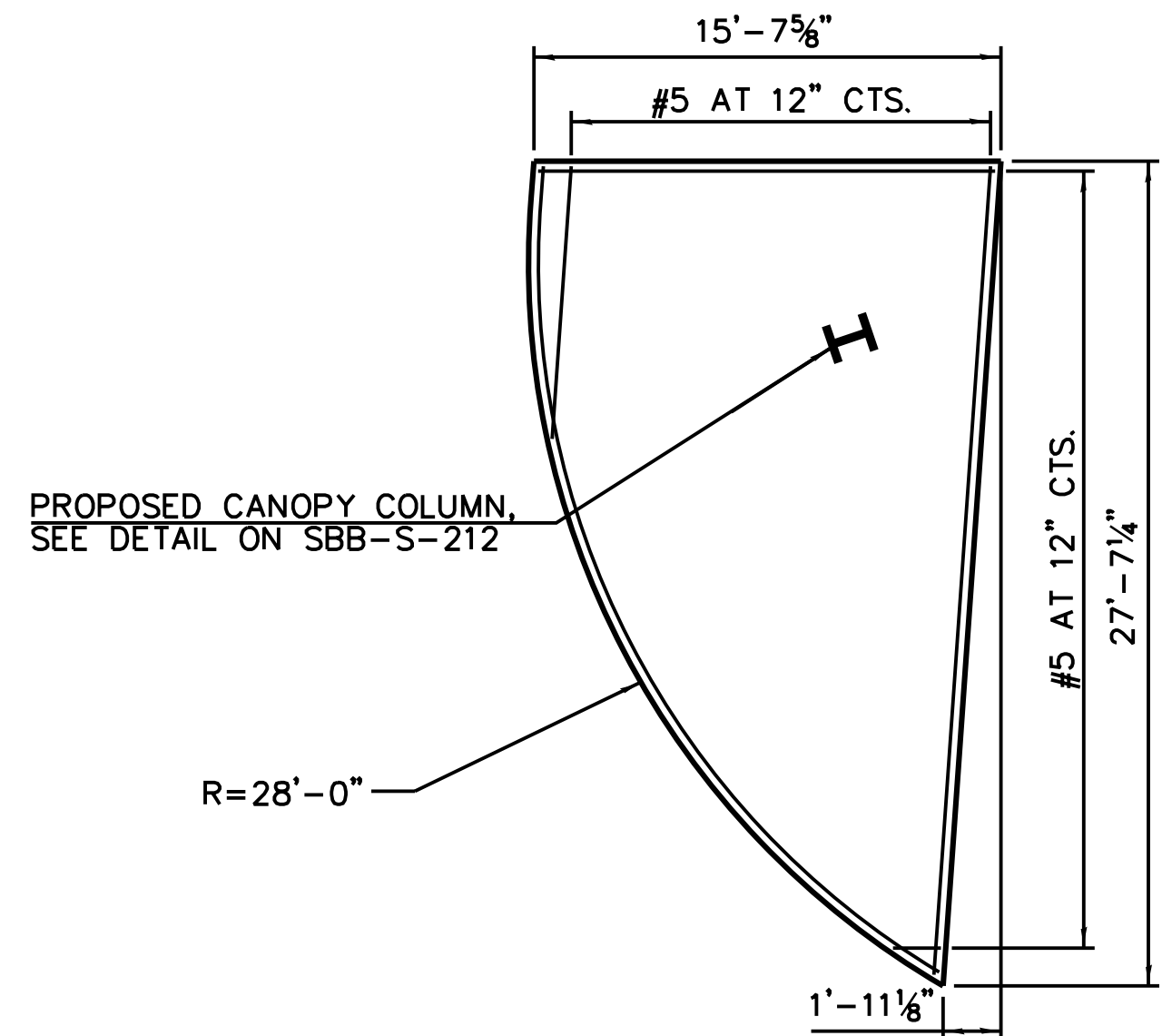
LOCATION IDENTIFIER: **DR095**

SOUTH BUS BRIDGE  
 CONDUIT HANGER DETAIL

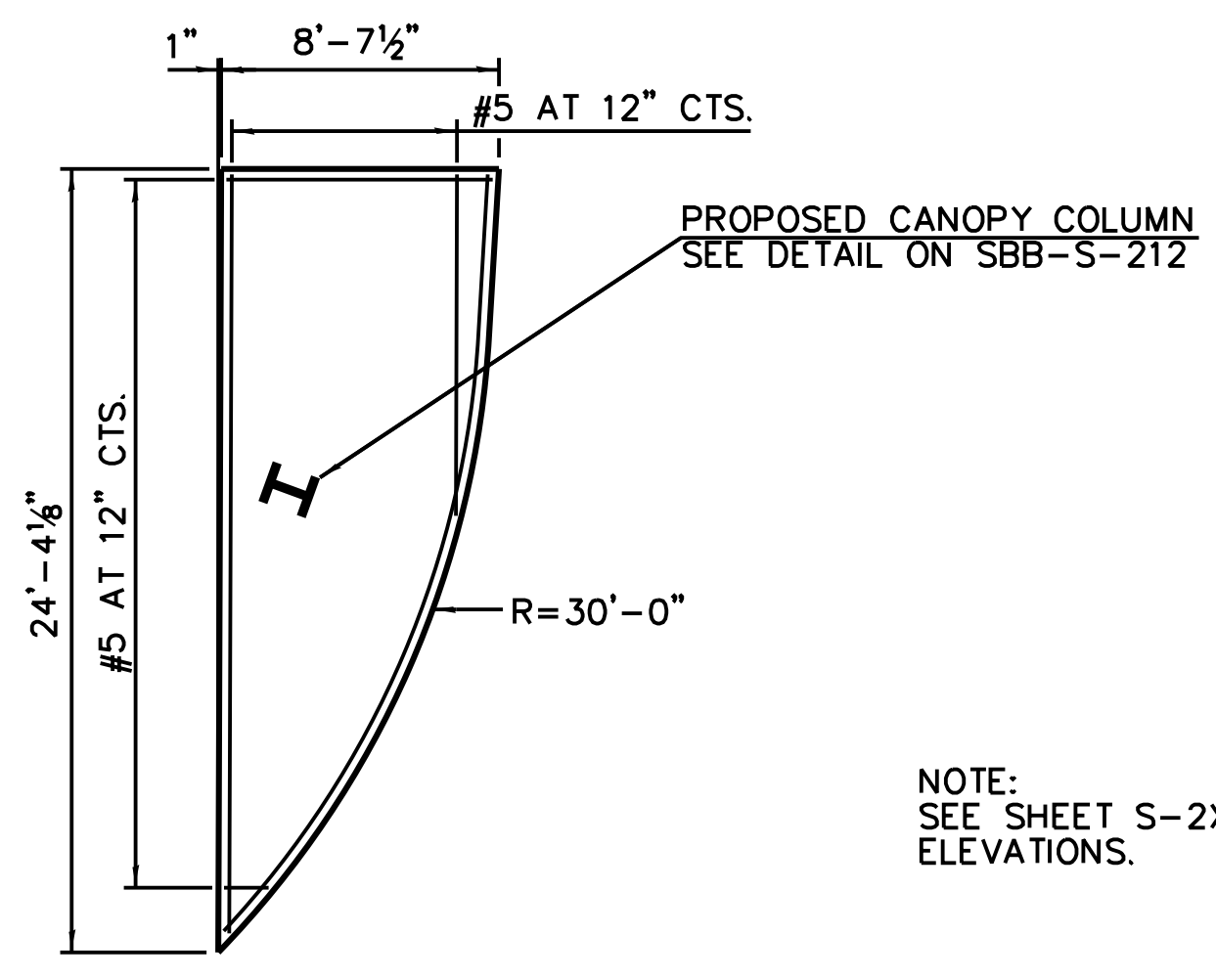
**SBB-S-213A**

FILE NAME = 070\_DR095\_S-213A.dgn  
 PLOT DATE = 8/31/2016

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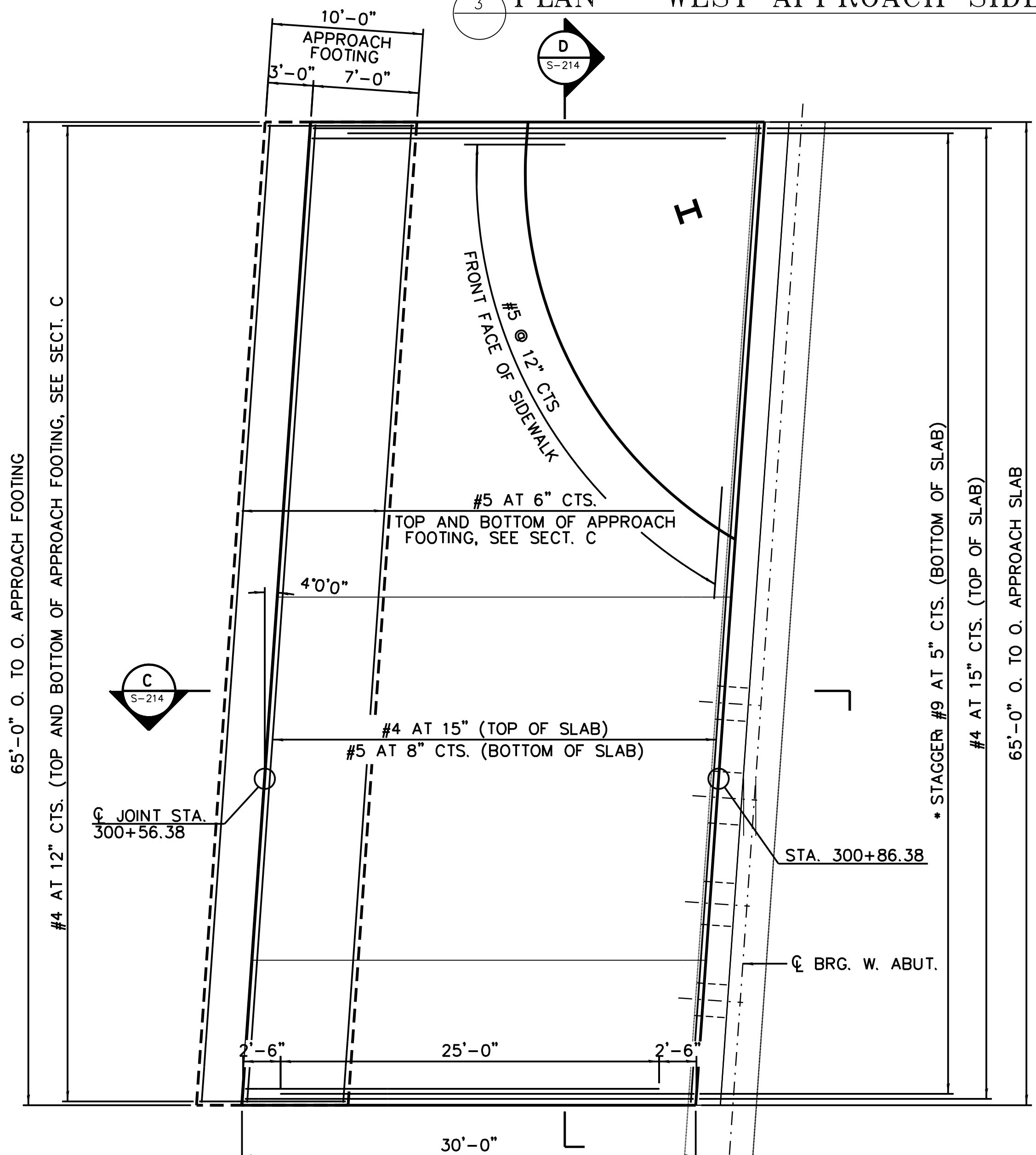


3 PLAN - WEST APPROACH SIDEWALK

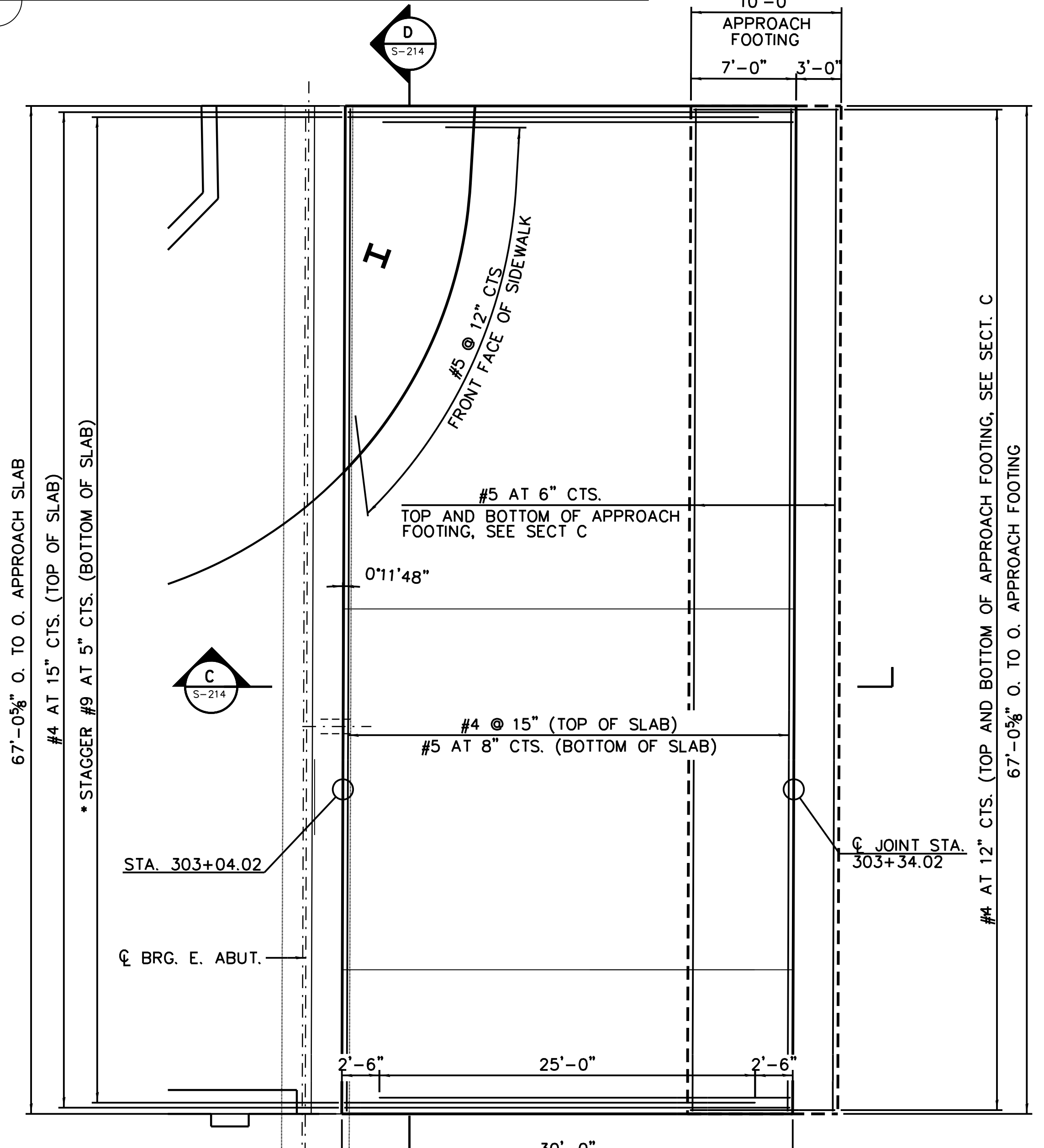


NOTE:  
SEE SHEET S-2XX FOR TOP OF APPROACH SLAB  
ELEVATIONS.

4 PLAN - EAST APPROACH SIDEWALK



1 PLAN - WEST APPROACH



2 PLAN - EAST APPROACH

\* TILT #9 BARS AS REQUIRED TO MAINTAIN CLEARANCE

FILE NAME = 070\_DR095\_S-213B.dgn  
PLOT DATE = 8/31/2016



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Chicago, IL 60628

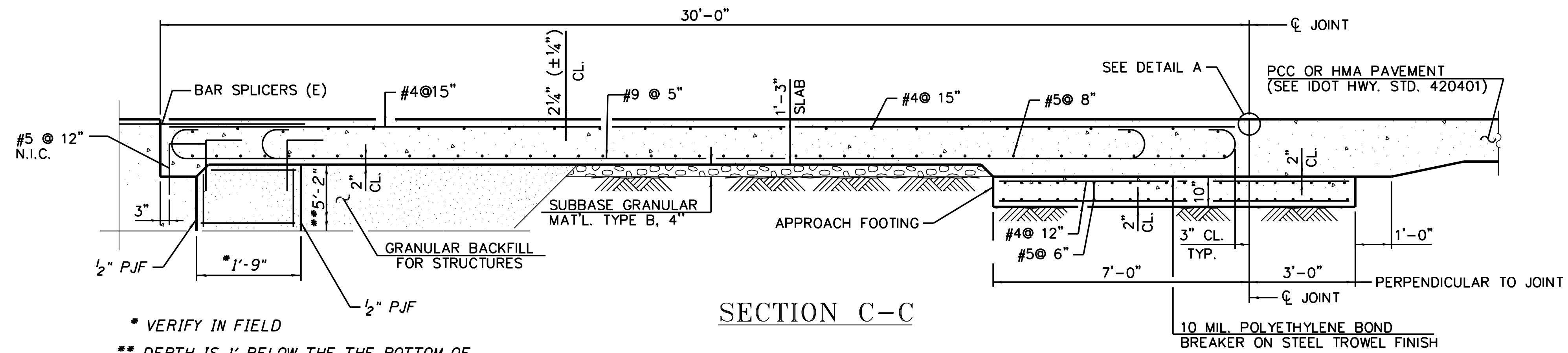
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APPROVED BY	D.ZROKA
CHECKED BY	P.MAID
DESIGNED BY	L.SOMMER
DRAWN BY	T.SLEDGE
PROJECT NO.	2012-0021
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MARK	DATE	DESCRIPTION
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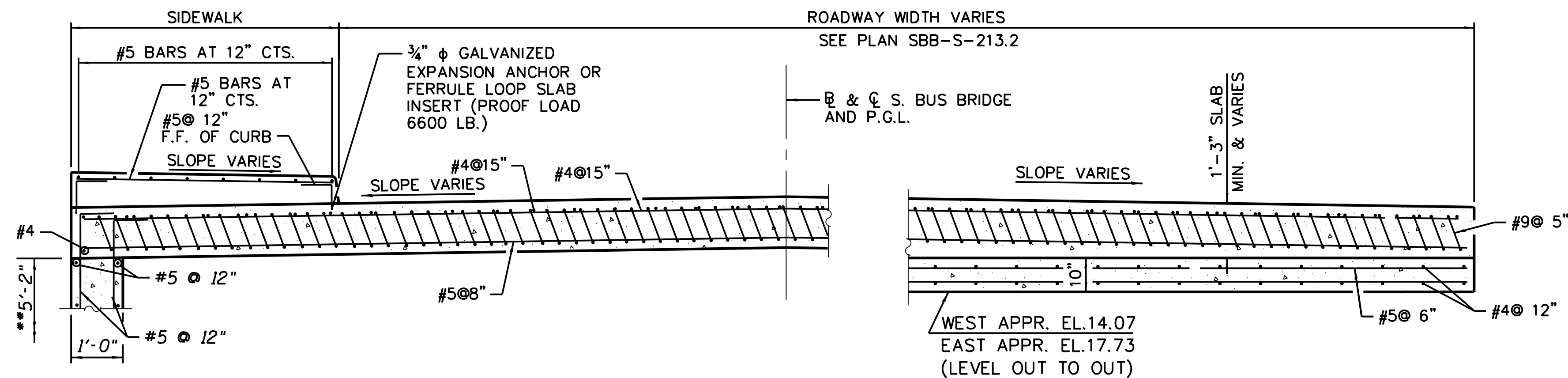
LOCATION IDENTIFIER: **DR095**  
SOUTH BUS BRIDGE  
BRIDGE APPROACH SLAB  
DETAILS (SHEET 1 OF 2)

**SBB-S-213B**

Rbrzezon



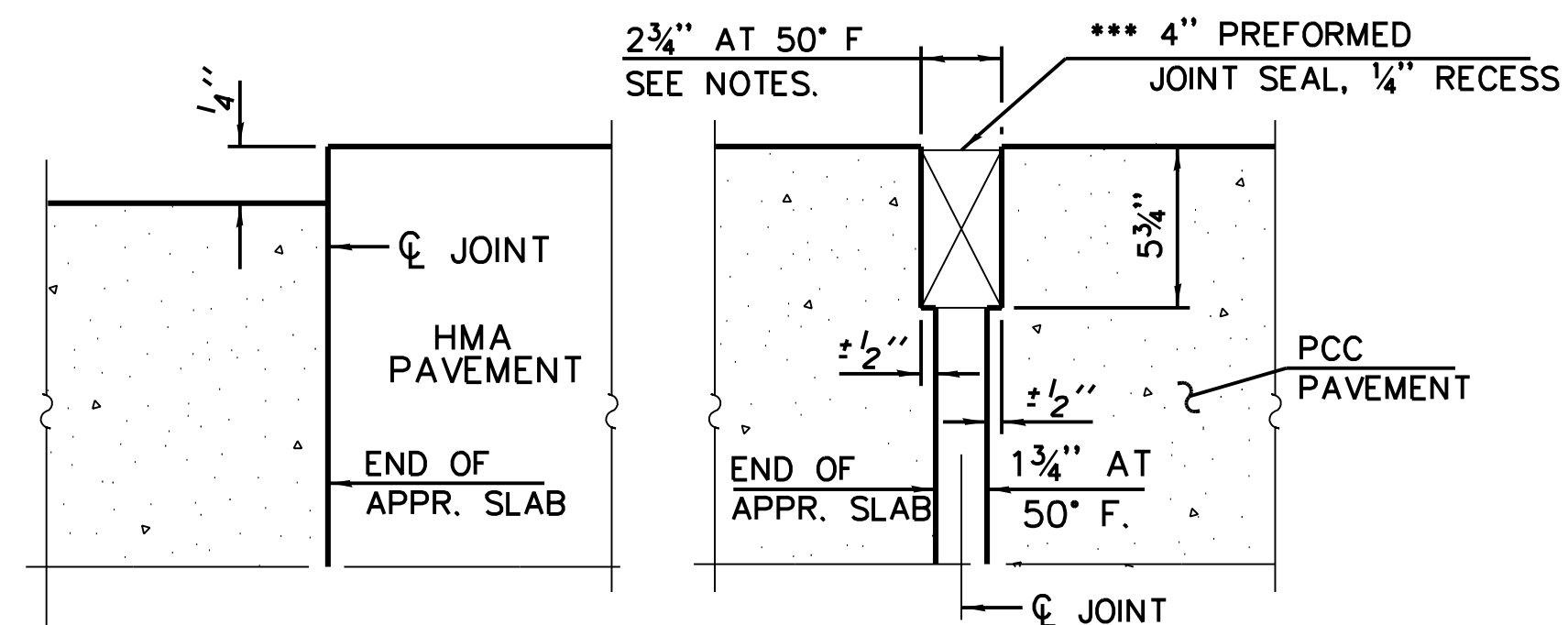
\* VERIFY IN FIELD  
 \*\* DEPTH IS 1' BELOW THE THE BOTTOM OF ADJACENT GRADE BEAM, VERIFY IN FIELD. THIS PIECE IS ON WEST SLAB ONLY.



NEAR ABUTMENT

SECTION D-D  
 (SEE PLAN FOR DIMENSIONS NOT SHOWN)

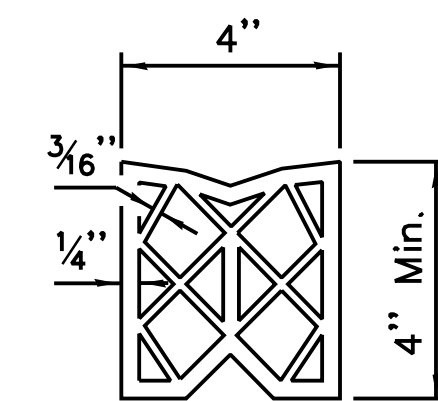
AT APPROACH FOOTING



FLEXIBLE PAVEMENT

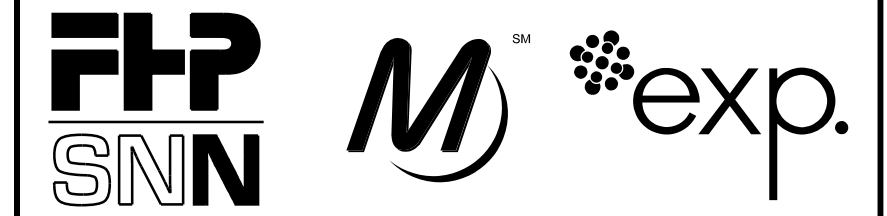
RIGID PAVEMENT

DETAIL A



PREFORMED JOINT SEAL

NOTE:  
 THE APPROACH FOOTING MAXIMUM APPLIED SERVICE BEARING PRESSURE (QMAX) = 2.0 KSF.  
 SEE PLAN FOR DIMENSIONS NOT SHOWN.  
 TILT #9 BARS AS REQUIRED TO MAINTAIN CLEARANCE.



**cta** CHICAGO TRANSIT AUTHORITY ENGINEERING

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CHECKED BY	P.MAID
DESIGNED BY	L.SOMMER
DRAWN BY	T.SLEDGE
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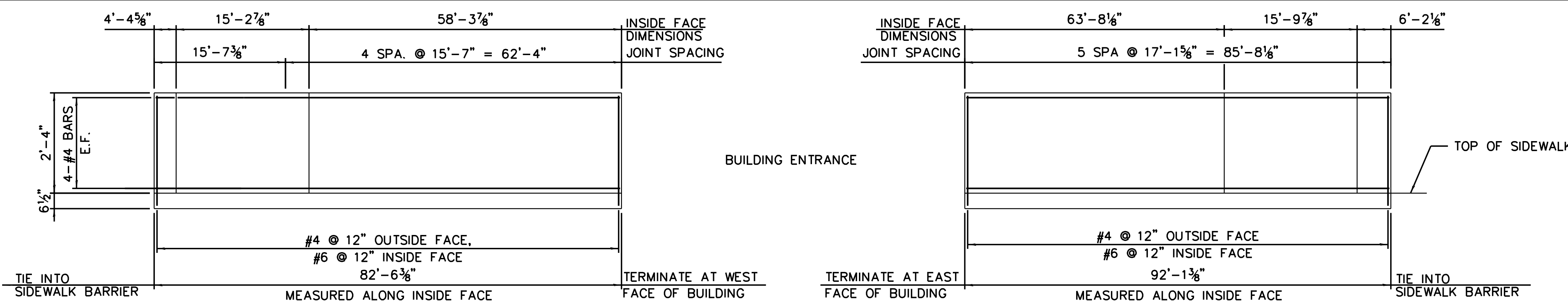
LOCATION IDENTIFIER: DR095

SOUTH BUS BRIDGE  
 BRIDGE APPROACH SLAB  
 DETAILS (SHEET 2 OF 2)

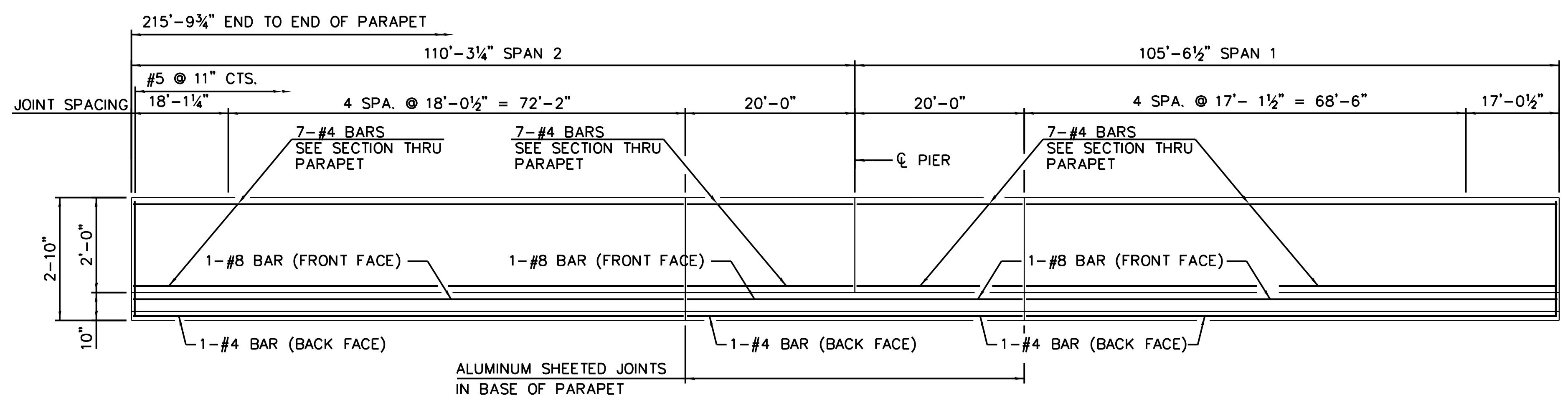
**SBB-S-214**

FILE NAME = 070\_DR095\_S-214.dgn  
 PLOT DATE = 11/28/2016

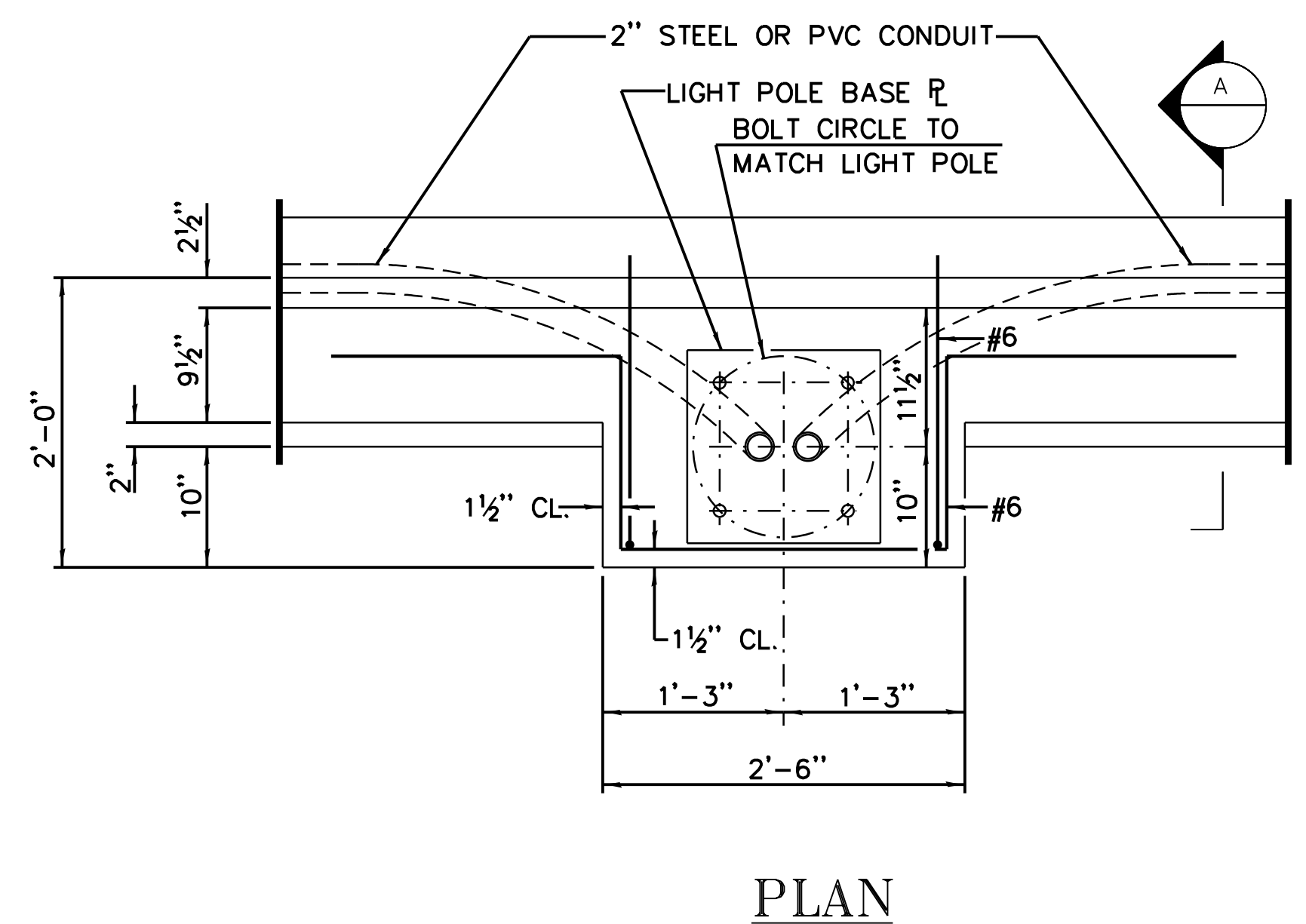
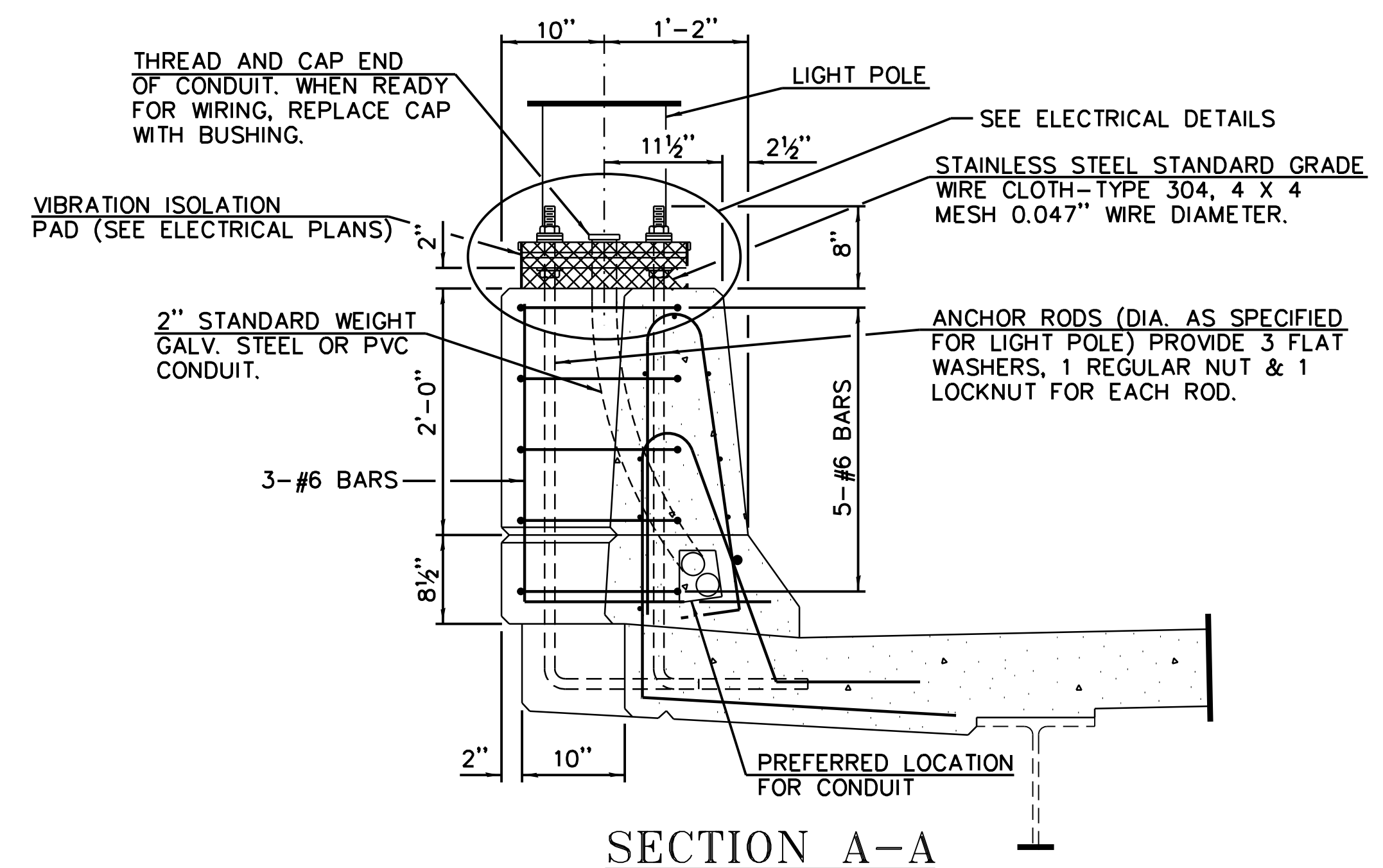
vscorppa



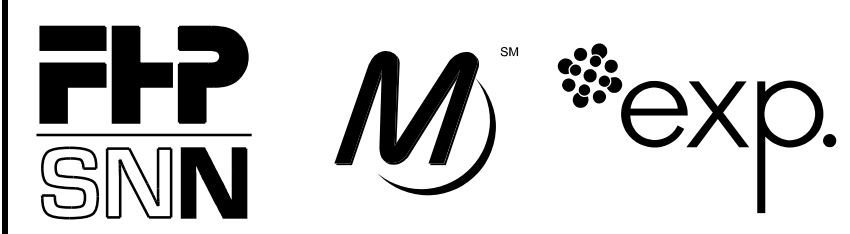
**1** INSIDE ELEVATION OF NORTH PARAPET  
SCALE: N.T.S.  
LOOKING NORTH



**2** INSIDE ELEVATION OF SOUTH PARAPET  
SCALE: N.T.S.  
LOOKING SOUTH



MINIMUM BAR LAPS	
BARS	LAPS
#4	2'-7"
#5	3'-3"
#6	3'-10"



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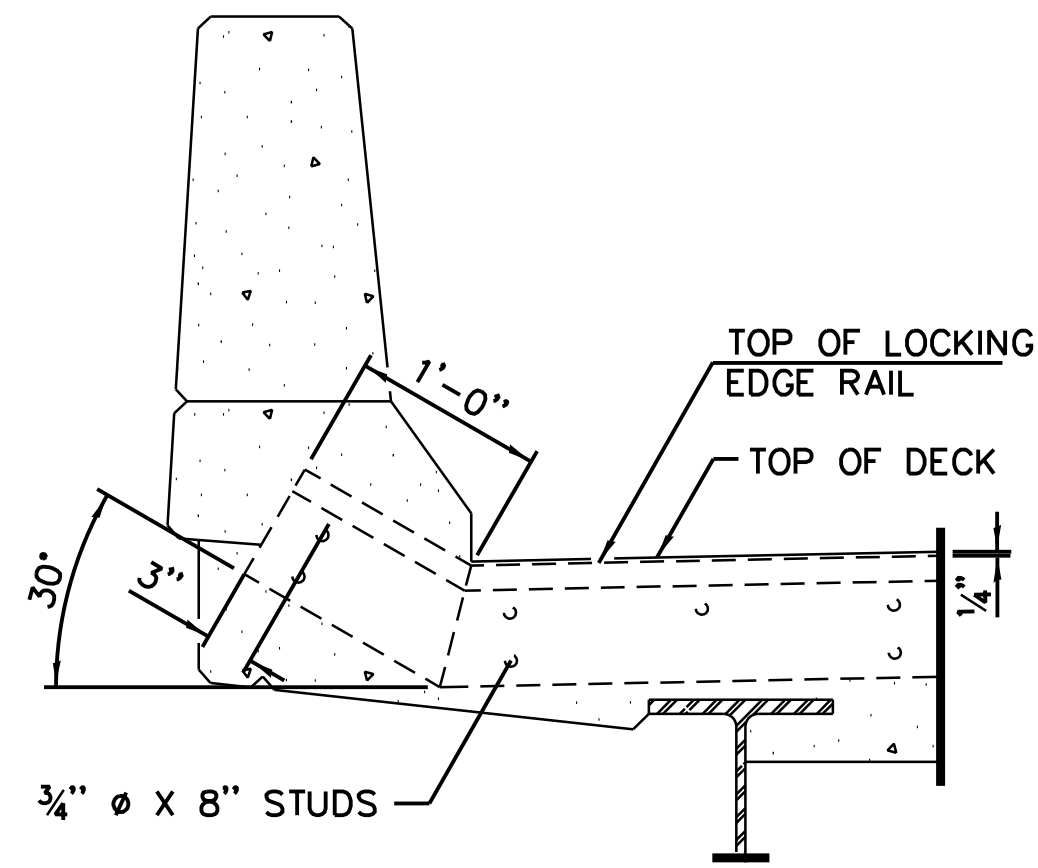
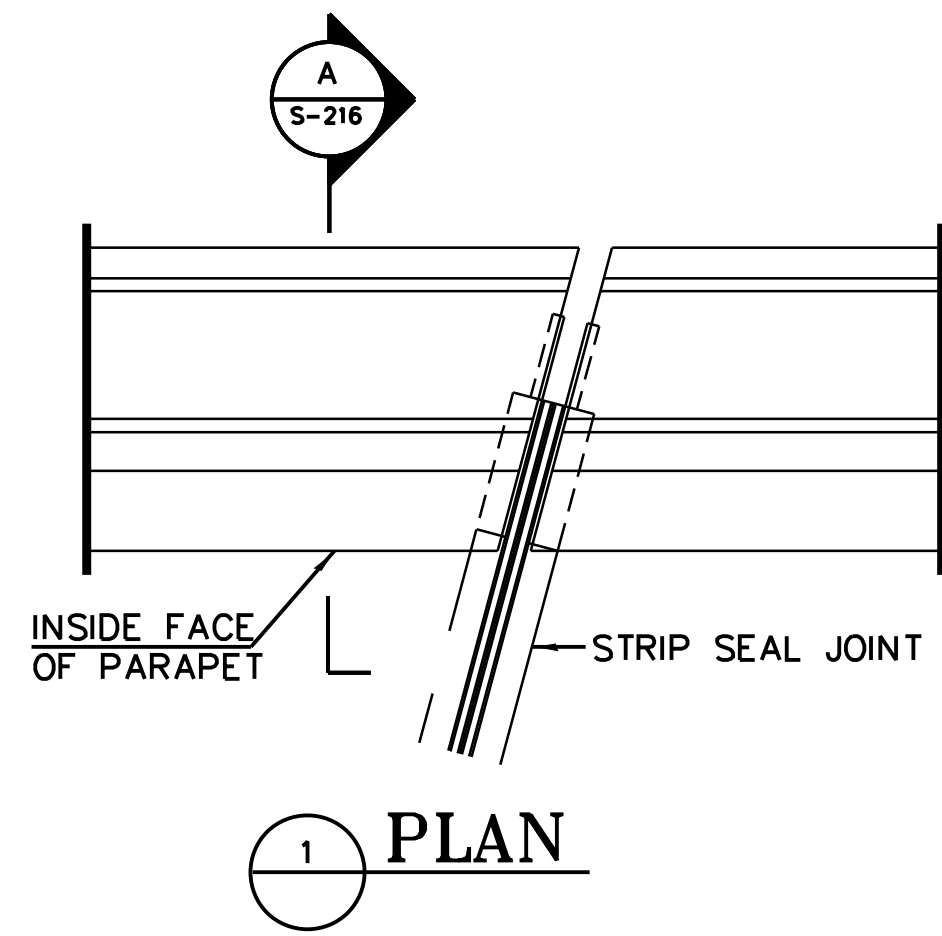
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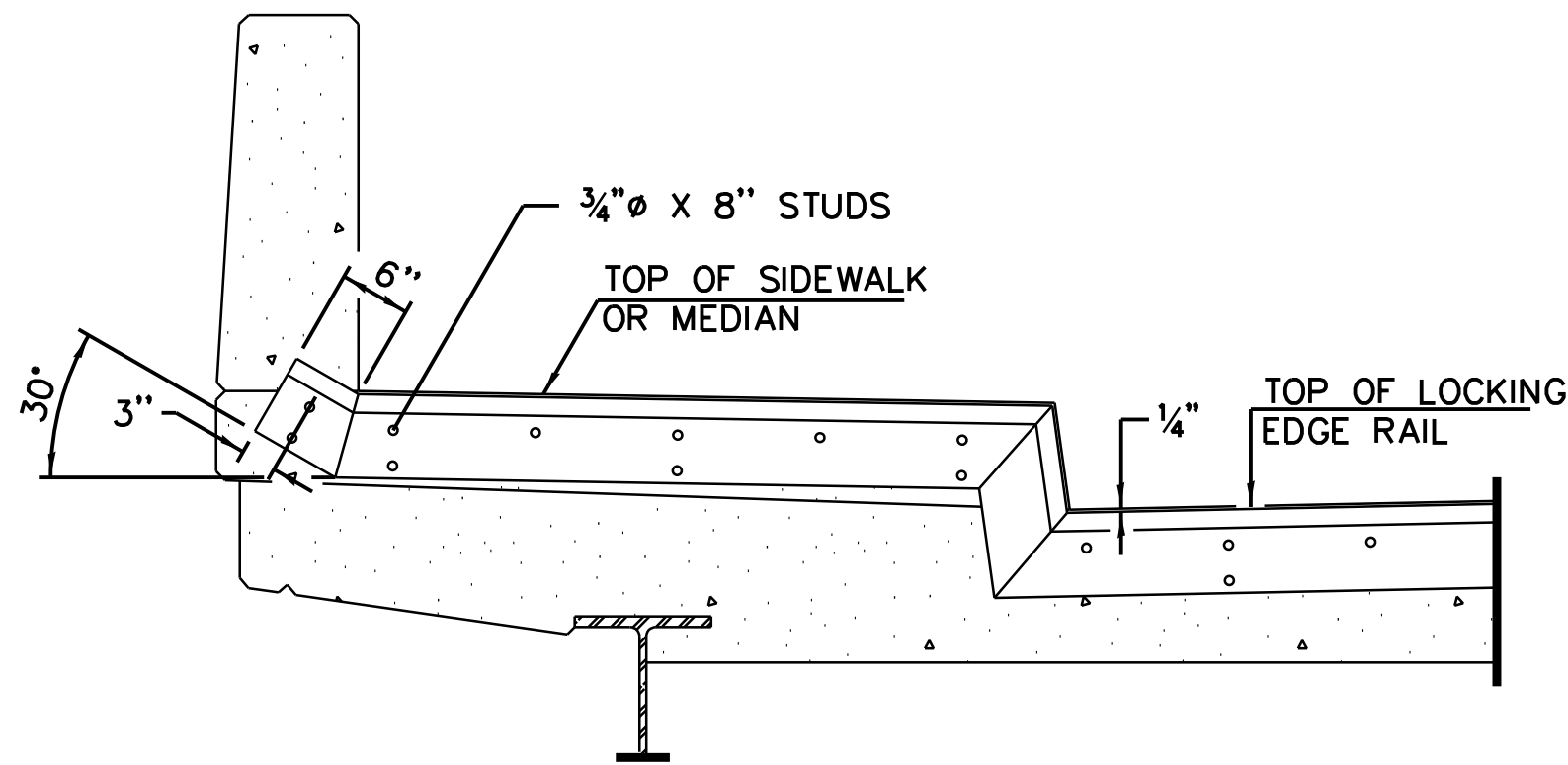
LOCATION IDENTIFIER: **DR095**  
SOUTH BUS BRIDGE  
BRIDGE RAILING  
DETAIL

**SBB-S-215**

FILE NAME = 070\_DR095\_S-215.dgn  
PLOT DATE = 8/31/2016

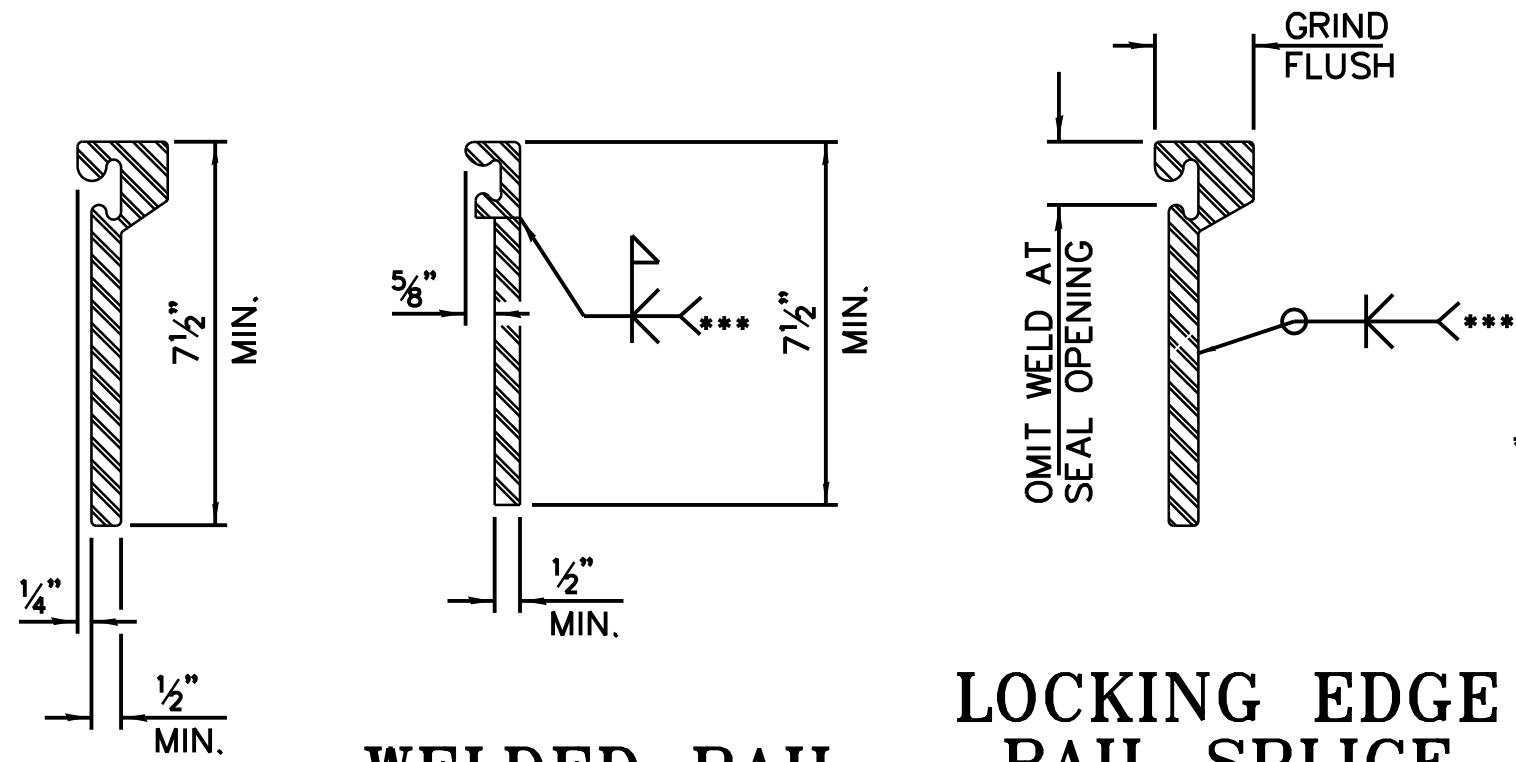


**A SECTION A-A**



**2 TYPICAL END TREATMENT AT SIDEWALK OR MEDIAN**

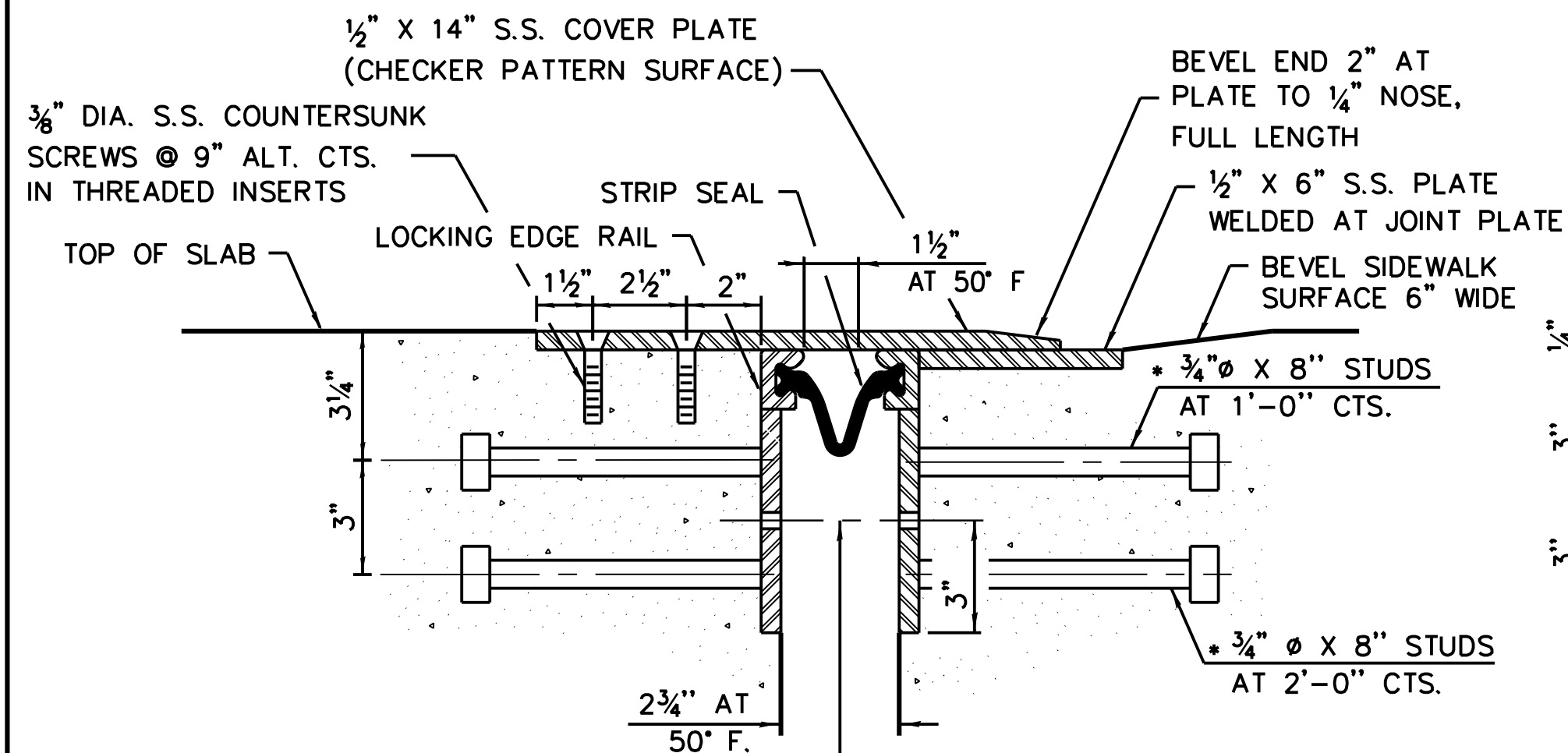
SHORTER PLATES WITH A SINGLE ROW OF STUDS AT 12" CTS. MAY BE NECESSARY ON MEDIANS WHICH ARE SHALLOWER THAN 9". SEE MANUFACTURER'S RECOMMENDATION.



**ROLLED EXTRUDED RAIL**

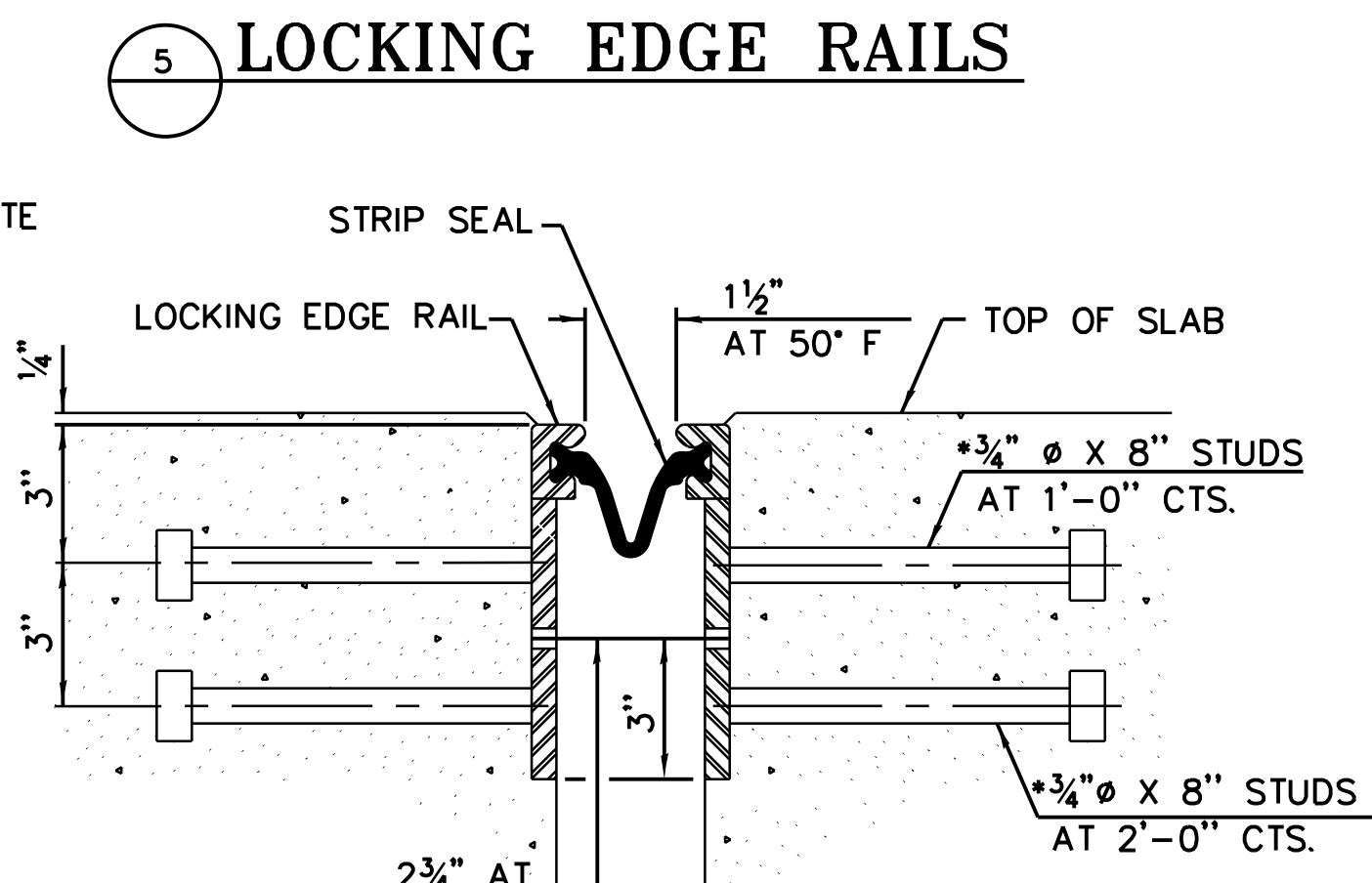
**LOCKING EDGE RAIL SPLICE**

THE INSIDE OF THE LOCKING EDGE RAIL GROOVE SHALL BE FREE OF WELD RESIDUE. ROLLED RAIL SHOWN, WELDED RAIL SIMILAR.



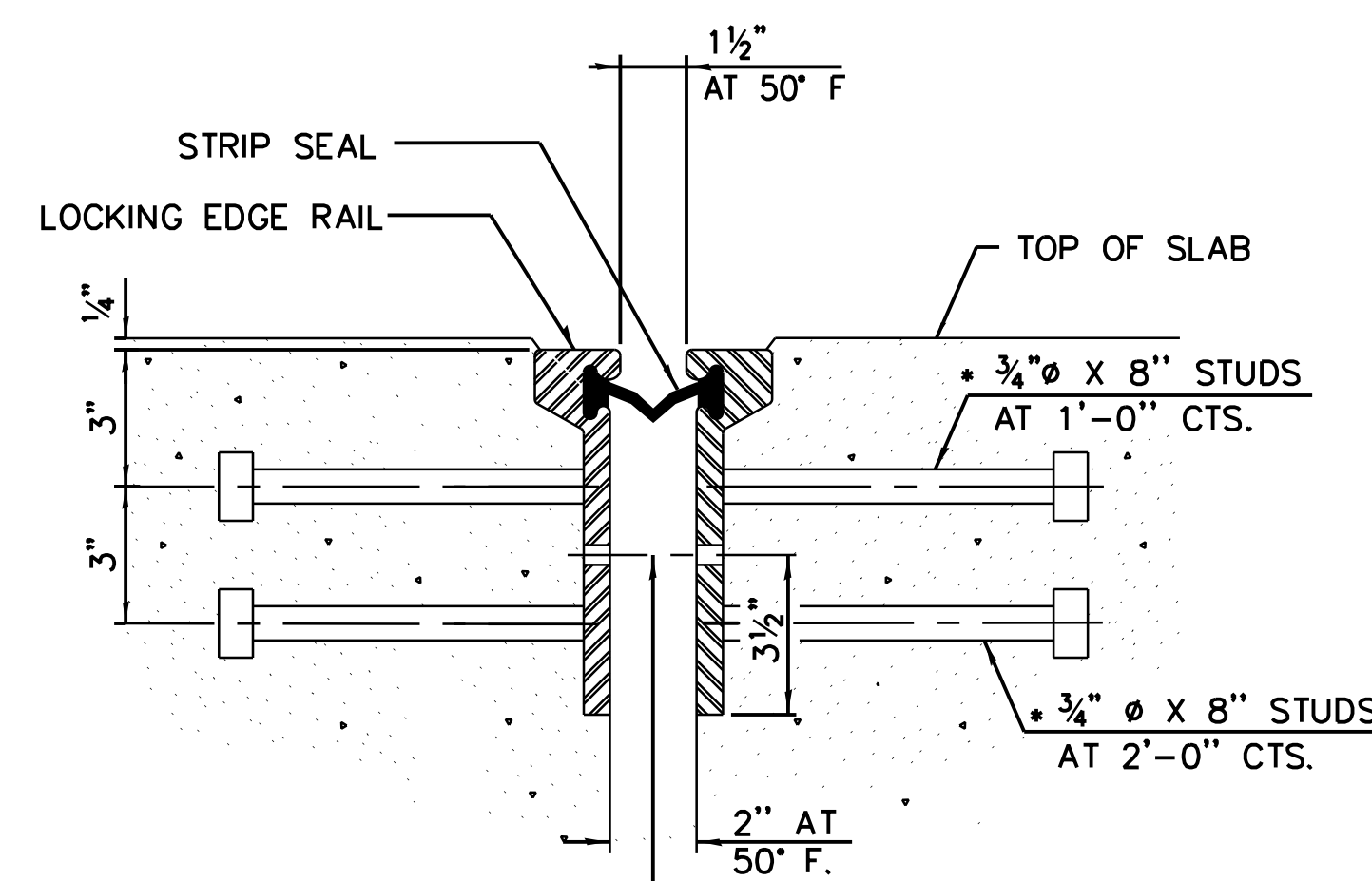
**3 SECTION THRU WELDED RAIL JOINT @ SIDEWALK**

\* GRANULAR OR SOLID FLUX FILLED HEADED STUDS CONFORMING TO ARTICLE 1006.32 OF THE IDOT STD. SPECS., AUTOMATICALLY END WELDED.



**4 SECTION THRU WELDED RAIL JOINT**

**5 LOCKING EDGE RAILS**



**6 SECTION THRU ROLLED RAIL JOINT**

**NOTES:**

THE STRIP SEAL SHALL BE MADE CONTINUOUS AND SHALL HAVE A MINIMUM THICKNESS OF 1/4". THE CONFIGURATION OF THE STRIP SEAL SHALL MATCH THE CONFIGURATION OF THE LOCKING EDGE RAILS. OPEN OR "WEBBED" STRIP SEAL GLAND CONFIGURATIONS ARE NOT PERMITTED. THE GLAND SHALL BE SIZED FOR A MAXIMUM RATED MOVEMENT OF 4 INCHES.

THE LOCKING EDGE RAILS DEPICTED ARE CONCEPTUAL ONLY, EXCEPT FOR THE MINIMUM DIMENSIONS SHOWN. THE ACTUAL CONFIGURATION OF THE LOCKING EDGE RAILS AND MATCHING STRIP SEAL MAY VARY FROM MANUFACTURER TO MANUFACTURER. FLANGED EDGE RAILS WILL NOT BE ALLOWED. LOCKING EDGE RAILS MAY BE SPLICED AT SLOPE DISCONTINUITIES.

THE MANUFACTURER'S RECOMMENDED INSTALLATION METHODS SHALL BE FOLLOWED.

THE JOINT OPENING AND DECK DIMENSIONS DETAILED ON THE SUPERSTRUCTURE ARE BASED ON A ROLLED RAIL EXPANSION JOINT. IF THE CONTRACTOR ELECTS TO USE THE WELDED RAIL EXPANSION JOINT, THE OPENING AND DECK DIMENSIONS SHALL BE MODIFIED ACCORDING TO THE DIMENSIONS DETAILED ON THIS SHEET.

ALL STEEL COMPONENTS SHALL BE GALVANIZED AFTER FABRICATION ACCORDING TO ARTICLE 520.03 OF THE IDOT STANDARD SPECIFICATIONS.

MAXIMUM SPACE BETWEEN RAIL SEGMENTS SHALL BE 3/16". SEALED WITH A SUITABLE SEALANT. JOINTS IN RAILS WITHIN 10 FT. OF CURBS SHALL BE WELDED.

ALL STAINLESS STEEL COVER PLATES TO BE STAINLESS DIAMOND FLOOR PLATE CONFORMING TO THE REQUIREMENTS OF ASTM A793, TYPE 304.



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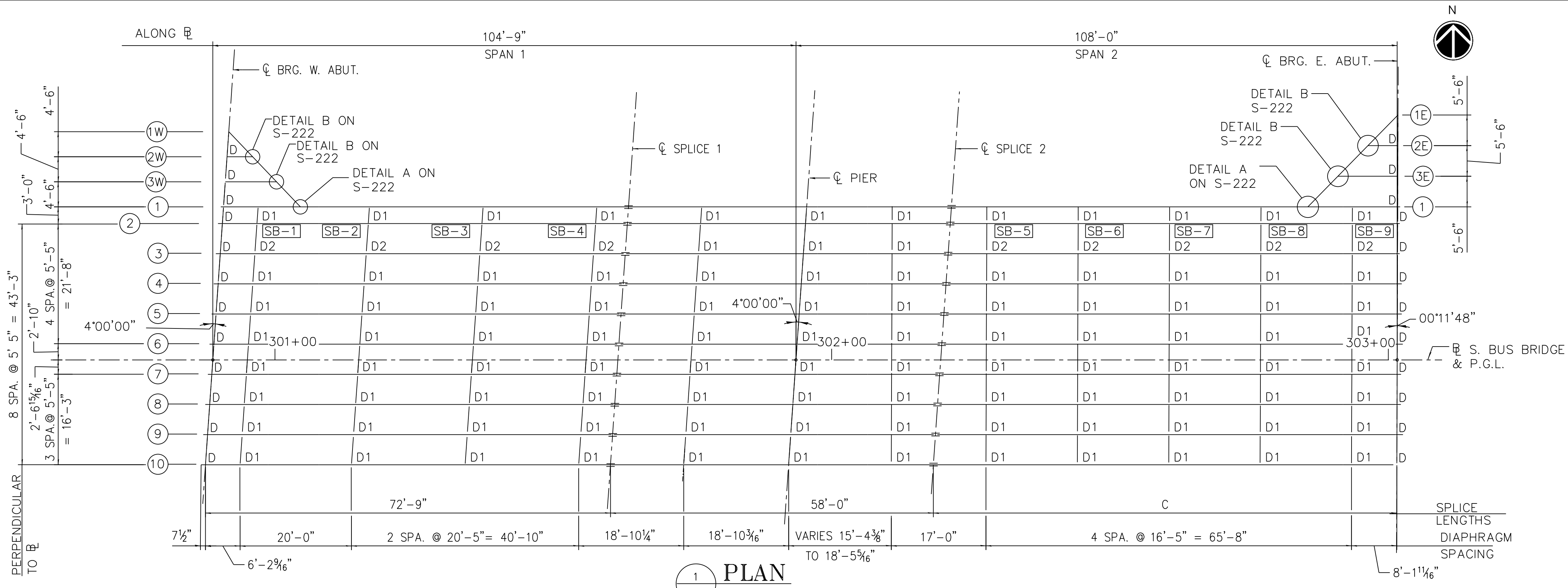
LOCATION IDENTIFIER: **DR095**

SOUTH BUS BRIDGE  
PREFORMED JOINT  
STRIP SEAL

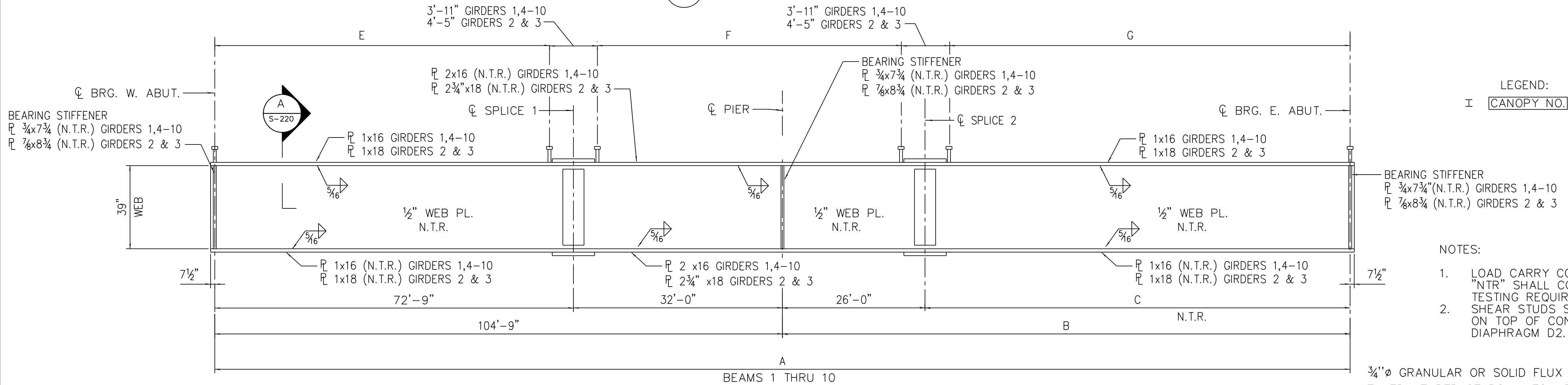
**SBB-S-216**



vscarpa



1 PLAN

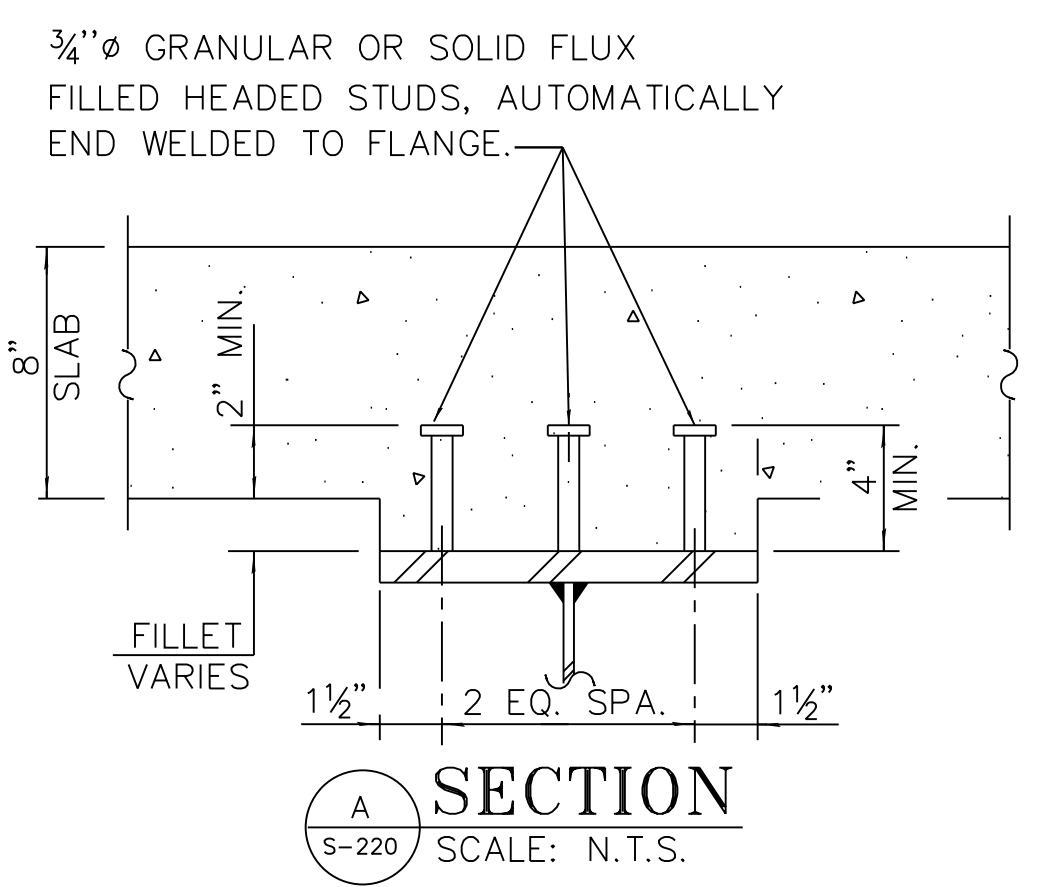


2 BEAM ELEVATION

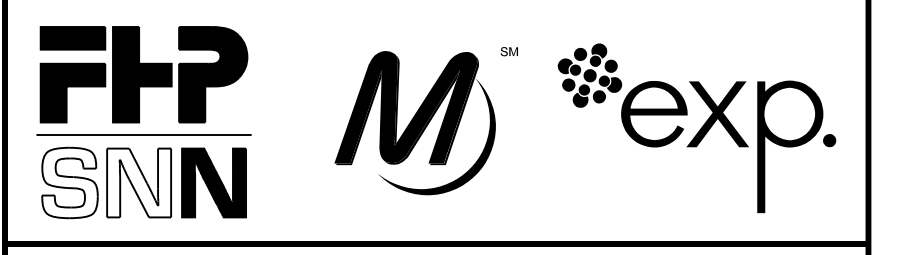
DIMENSIONAL TABLE

BEAM NO.	SHEAR STUD SPACING					
	A	B	C	E	F	G
1	210'-11 1/16"	106'-2 1/16"	80'-2 1/16"	114 SPA @ 7 1/2"(-)=70'-9 1/2"	87 SPA @ 7 1/2"(-)=54'-1"	126 SPA @ 7 1/2"(-)=78'-2 1/16"
2	211'-1 7/16"	106'-4 7/16"	80'-4 7/16"	113 SPA @ 7 1/2"(-)=70'-6 1/2"	86 SPA @ 7 1/2"(-)=53'-7"	126 SPA @ 7 1/2"(-)=78'-1 15/16"
3	211'-5 3/4"	106'-8 3/4"	80'-8 3/4"	113 SPA @ 7 1/2"(-)=70'-6 1/2"	86 SPA @ 7 1/2"(-)=53'-7"	126 SPA @ 7 1/2"(-)=78'-6 1/4"
4	211'-10 1/16"	107'-1 1/16"	81'-1 1/16"	114 SPA @ 7 1/2"(-)=70'-9 1/2"	87 SPA @ 7 1/2"(-)=54'-1"	127 SPA @ 7 1/2"(-)=79'-1 1/16"
5	212'-2 7/16"	107'-5 7/16"	81'-5 7/16"	114 SPA @ 7 1/2"(-)=70'-9 1/2"	87 SPA @ 7 1/2"(-)=54'-1"	128 SPA @ 7 1/2"(-)=79'-5 15/16"
6	212'-6 3/4"	107'-9 3/4"	81'-9 3/4"	114 SPA @ 7 1/2"(-)=70'-9 1/2"	87 SPA @ 7 1/2"(-)=54'-1"	128 SPA @ 7 1/2"(-)=79'-10 1/4"
7	212'-11 1/16"	108'-2 1/16"	82'-2 1/16"	114 SPA @ 7 1/2"(-)=70'-9 1/2"	87 SPA @ 7 1/2"(-)=54'-1"	129 SPA @ 7 1/2"(-)=80'-2 9/16"
8	213'-3 3/8"	108'-6 3/8"	82'-6 3/8"	114 SPA @ 7 1/2"(-)=70'-9 1/2"	87 SPA @ 7 1/2"(-)=54'-1"	129 SPA @ 7 1/2"(-)=80'-6 7/8"
9	213'-7 1/16"	108'-10 1/16"	82'-10 1/16"	114 SPA @ 7 1/2"(-)=70'-9 1/2"	87 SPA @ 7 1/2"(-)=54'-1"	130 SPA @ 7 1/2"(-)=80'-11 3/16"
10	214'-0"	109'-3"	83'-3"	114 SPA @ 7 1/2"(-)=70'-9 1/2"	87 SPA @ 7 1/2"(-)=54'-1"	131 SPA @ 7 1/2"(-)=81'-3 1/2"

CANOPY LOCATION	STATION	OFFSET	BASELINE
SB-1	300+96.59	-22.0797	℄ S. BUS BRIDGE
SB-2	301+16.59	-22.0797	℄ S. BUS BRIDGE
SB-3	301+37.01	-22.0797	℄ S. BUS BRIDGE
SB-4	301+57.43	-22.0797	℄ S. BUS BRIDGE
SB-5	302+27.86	-22.0797	℄ S. BUS BRIDGE
SB-6	302+44.27	-22.0797	℄ S. BUS BRIDGE
SB-7	302+60.67	-22.0797	℄ S. BUS BRIDGE
SB-8	302+77.11	-22.0797	℄ S. BUS BRIDGE
SB-9	302+93.52	-22.0797	℄ S. BUS BRIDGE



SECTION SCALE: N.T.S.



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**95th Terminal Improvement**  
 15 West 95th Street (South Terminal)  
 Chicago, IL 60628

IN CHARGE	S. KADERBEK
APPROVED BY	D.ZROKA
CHECKED BY	P.MAID
DESIGNED BY	L.SOMMER
DRAWN BY	T.SLEDGE
PROJECT NO.	2012-0021
FILE NAME	File Name

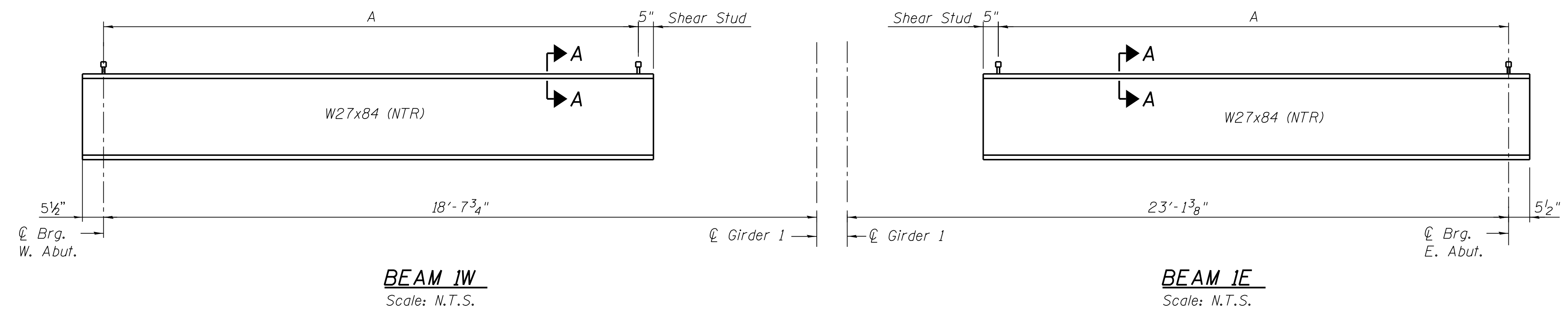
MARK	DATE	DESCRIPTION
	2/9/17	ISSUED FOR CONSTRUCTION

LOCATION IDENTIFIER: **DR095**

SOUTH BUS BRIDGE FRAMING PLAN  
**SBB-S-220**

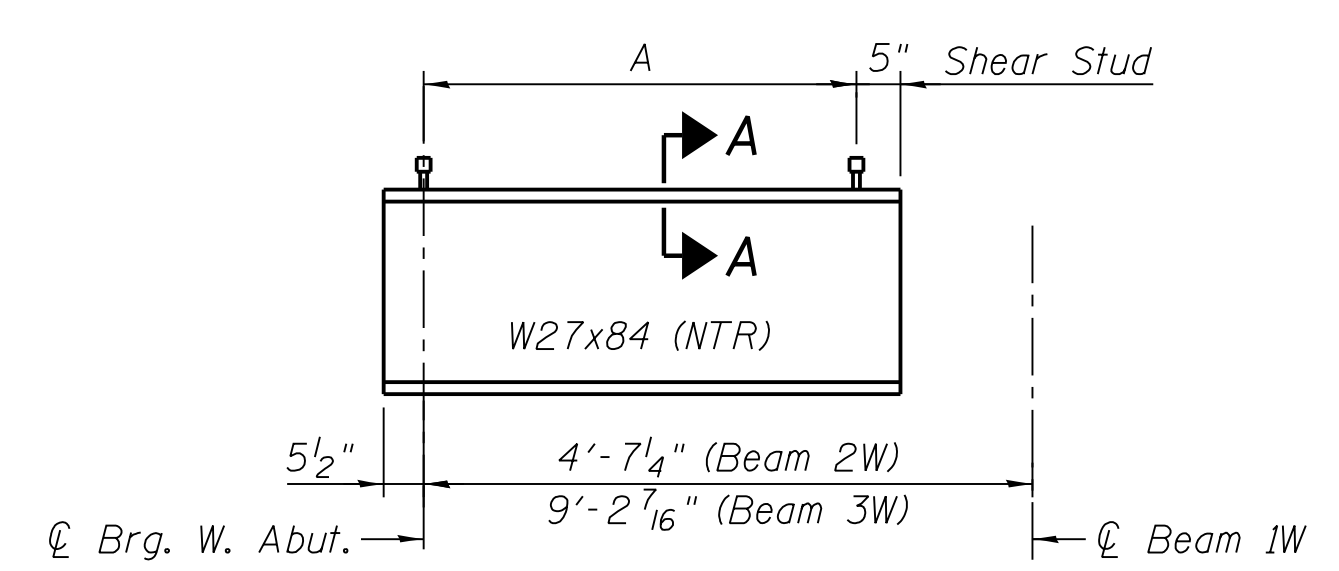
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 PLOT DATE = 8/31/2016

vscarpa

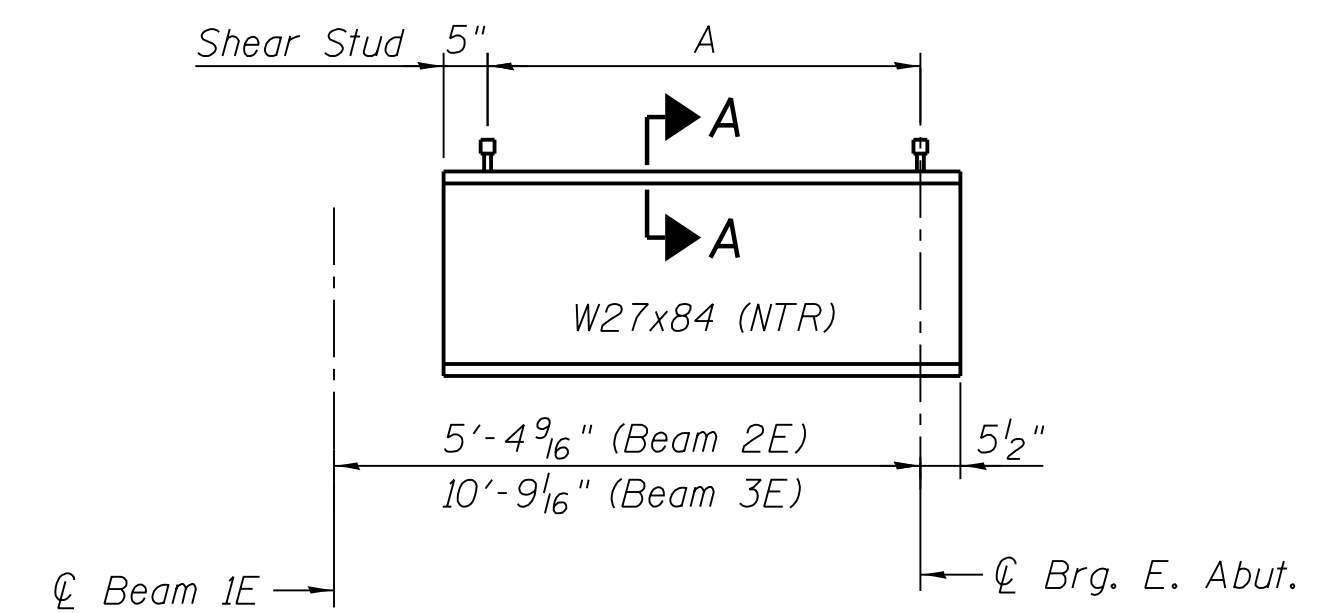


**BEAM 1W**  
Scale: N.T.S.

**BEAM 1E**  
Scale: N.T.S.

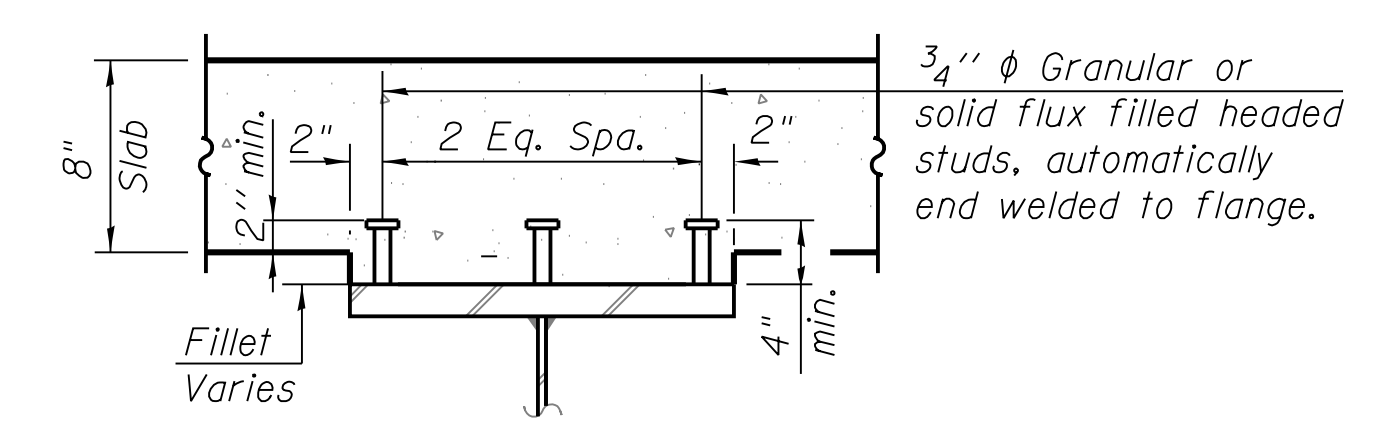


**BEAMS 2W & 3W**  
Scale: N.T.S.



**BEAMS 2E & 3E**  
Scale: N.T.S.

**BEAM ELEVATIONS**



**SECTION A-A**  
Scale: N.T.S.

**SHEAR STUD DIMENSIONS**

Beam	A
1W	22 Spa. @ 9" (-)=16'-7 3/4"
1E	28 Spa. @ 9" (-)=21'-2"
2W	3 Spa. @ 9" (-)=2'-7 1/4"
2E	4 Spa. @ 9" (-)=3'-4 1/2"
3W	9 Spa. @ 9" (-)=7'-2 7/16"
3E	11 Spa. @ 9" (-)=8'-9"

Note:  
1. Load carrying components designated "NTR" shall conform to the Impact Testing requirement, Zone 2.



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15 West 95th Street (South Terminal)  
Chicago, IL 60628

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APPROVED BY	D.ZROKA
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DESIGNED BY	L.SOMMER
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PROJECT NO.	2012-0021
FILE NAME	File Name

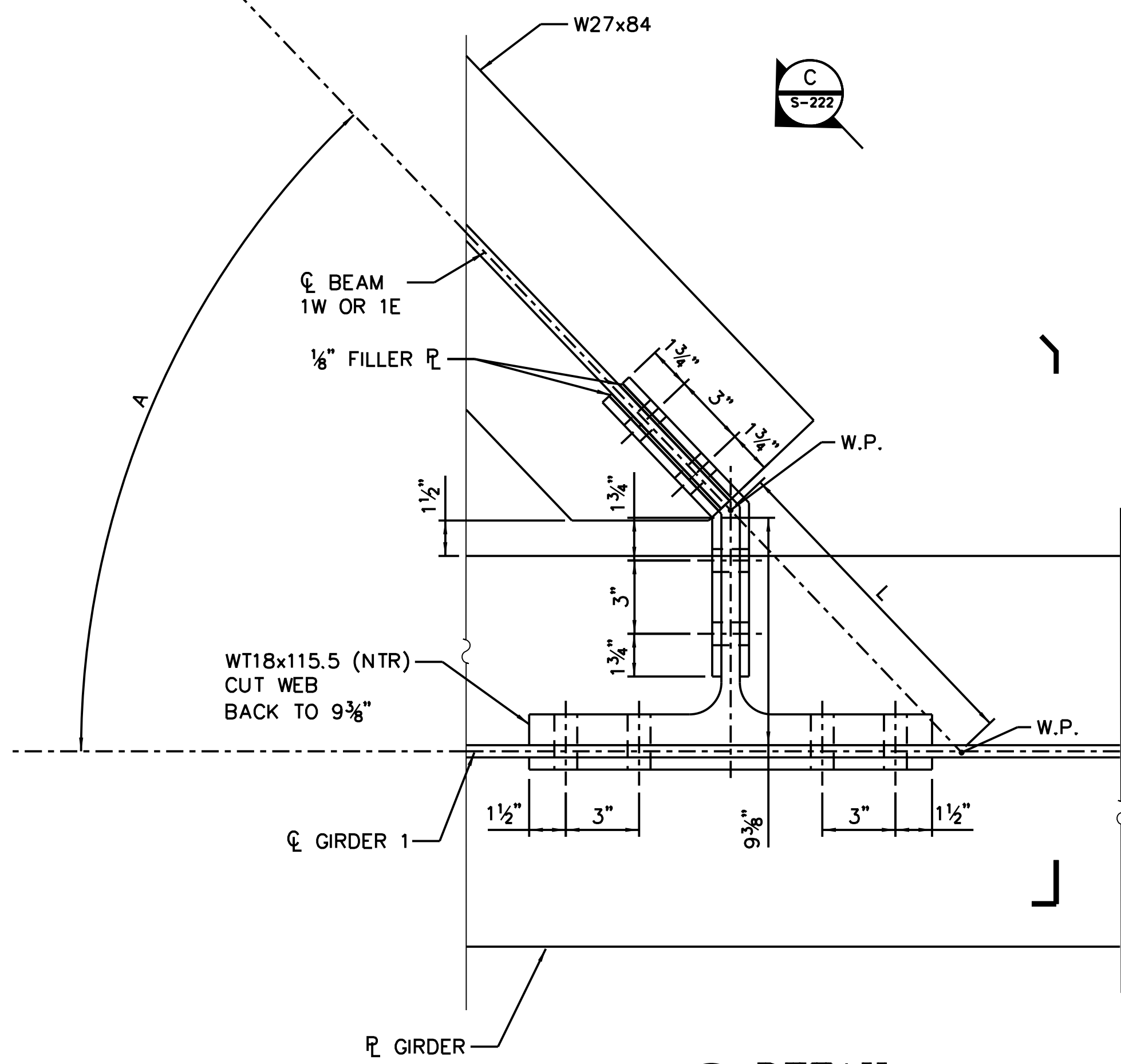
MARK	DATE	DESCRIPTION
	2/9/17	ISSUED FOR CONSTRUCTION

LOCATION IDENTIFIER: **DR095**  
**SOUTH BUS BRIDGE**  
**STRUCTURAL STEEL DETAILS**  
**(SHEET 1 OF 4)**

**SBB-S-221**

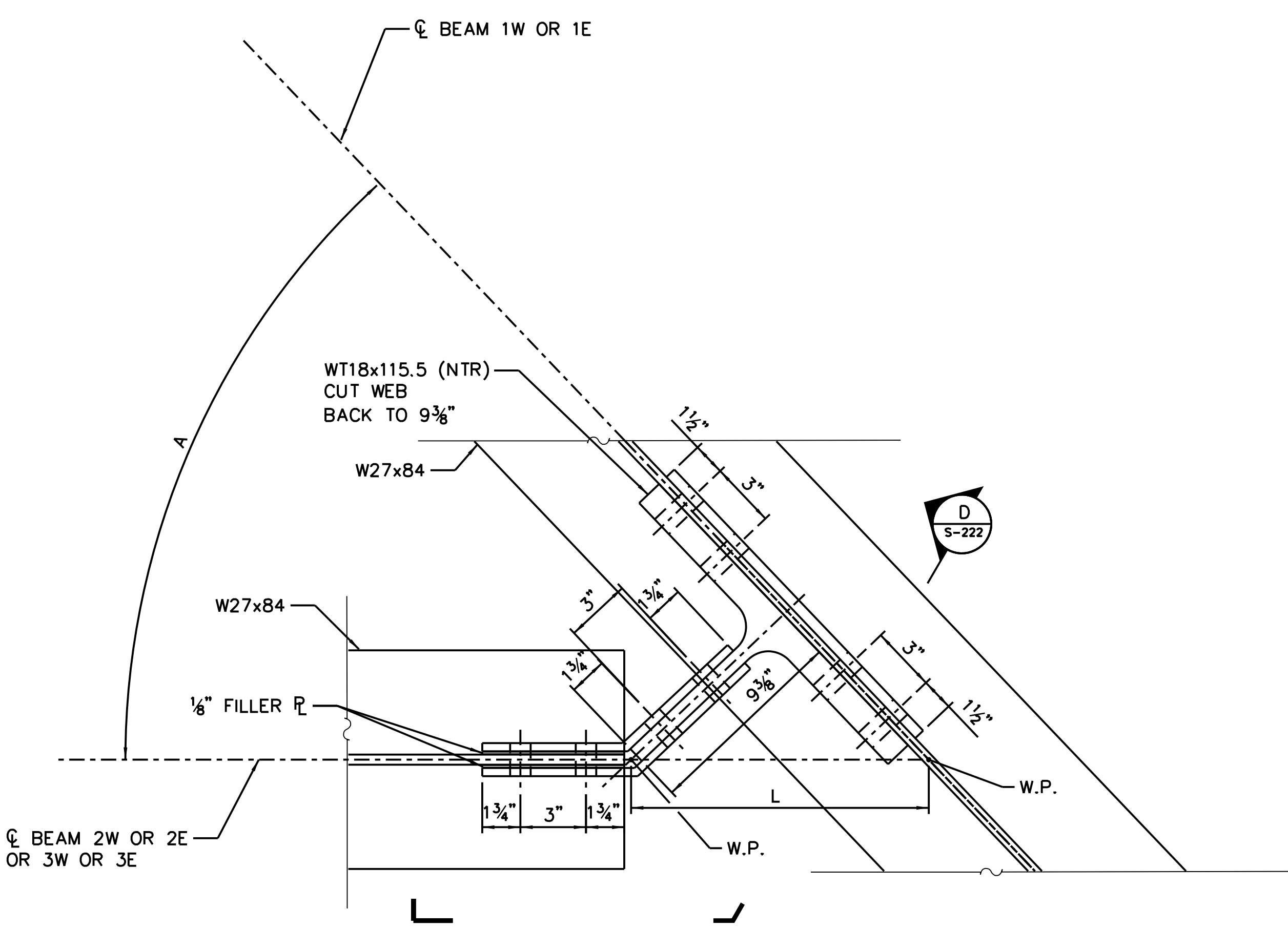
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PLOT DATE = 8/31/2016

vscarpa



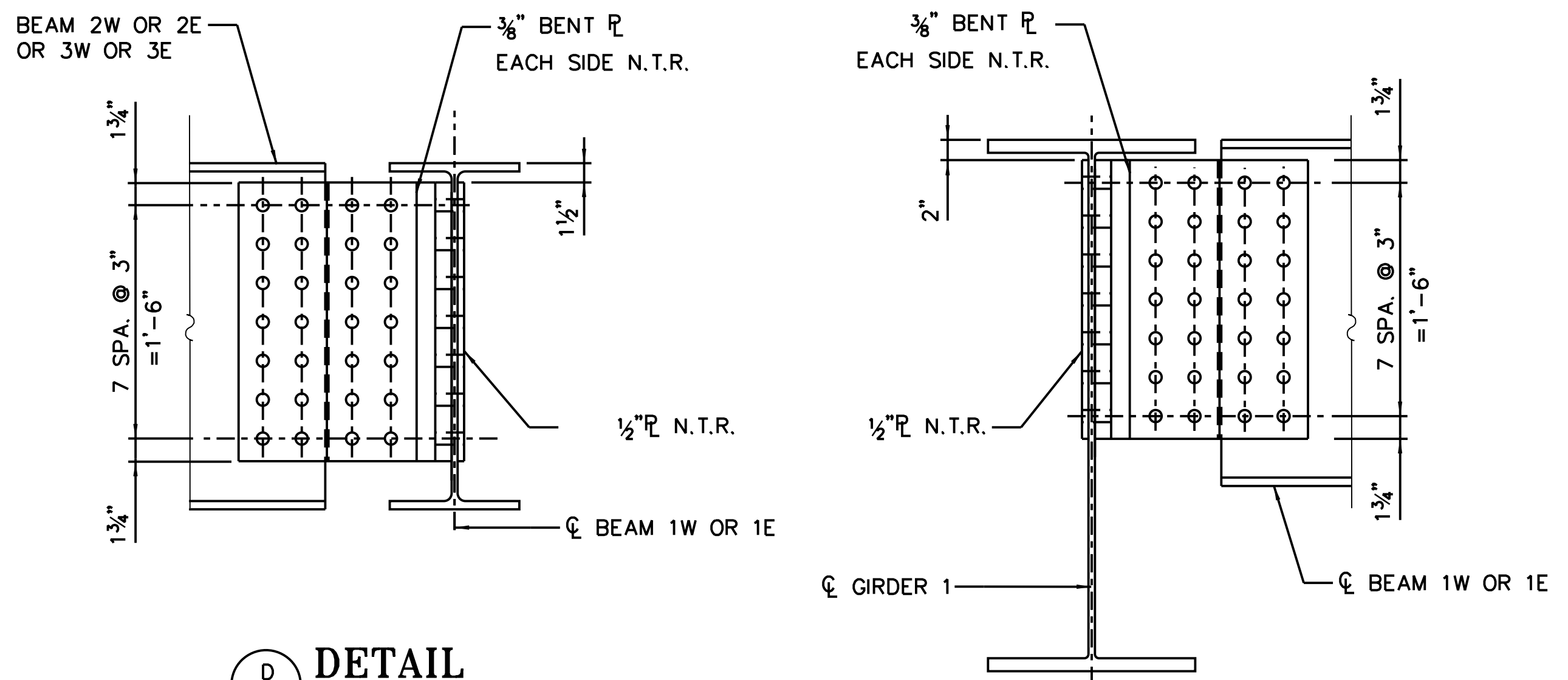
**A** DETAIL  
S-220 SCALE: N.T.S.

(CONNECTION AT BEAM 1W SHOWN,  
BEAM 1E SIMILAR BUT OPPOSITE)

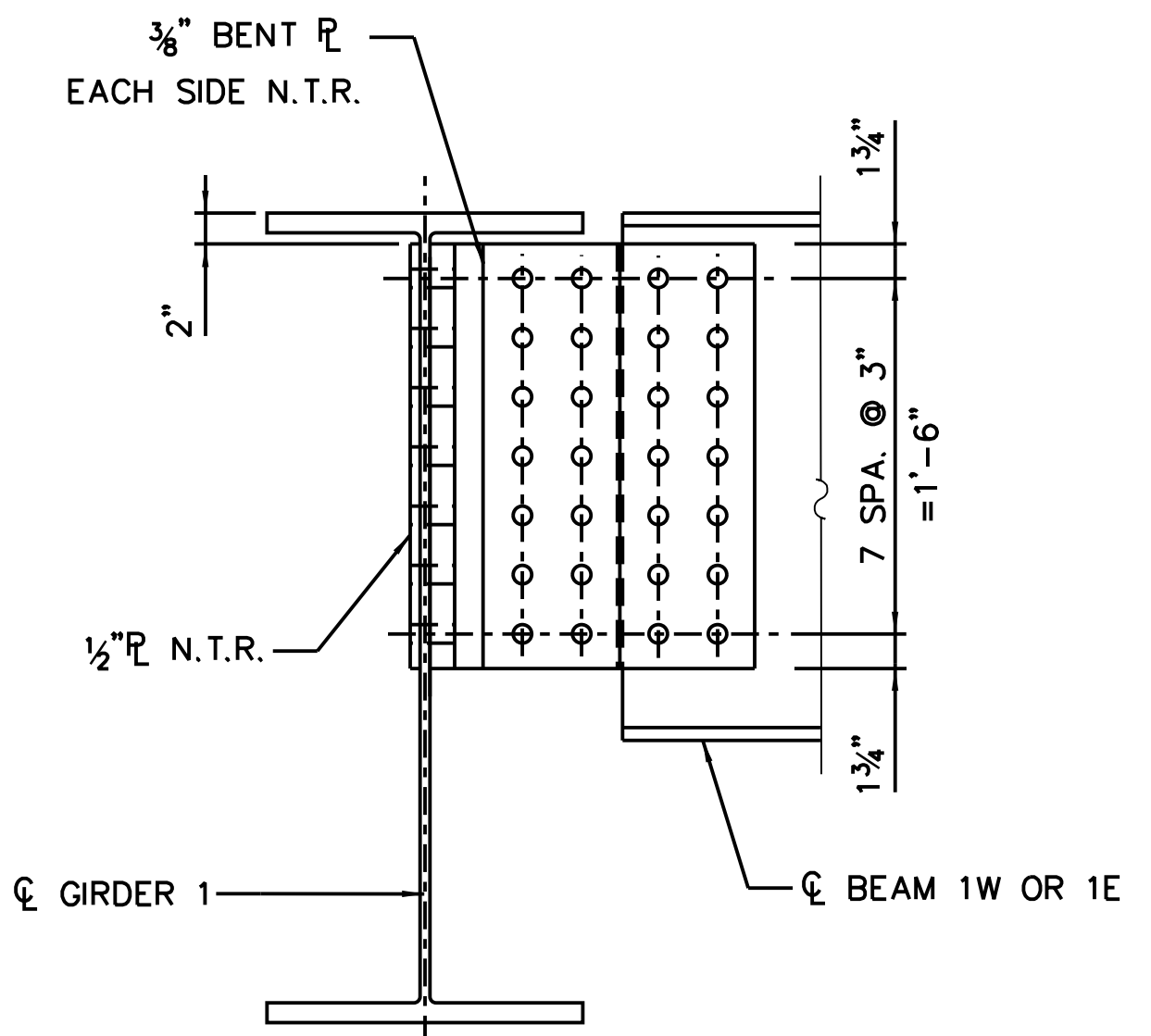


**B** DETAIL  
S-220 SCALE: N.T.S.

(CONNECTION AT BEAM 1W SHOWN,  
BEAM 1E SIMILAR BUT OPPOSITE)



**D** DETAIL  
S-222 SCALE: N.T.S.



**C** DETAIL  
S-222 SCALE: N.T.S.

GIRDER	BEAM	BEAM	ANGLE A (DEGREES)	L
G1	1W	-	46.3872	1'-1 5/16"
-	1W	2W	46.3872	1'-1 5/16"
-	1W	3W	46.3872	1'-1 5/16"
G1	1E	-	45.5422	1'-1 1/2"
-	1E	2E	45.5422	1'-1 1/2"
-	1E	3E	45.5422	1'-1 1/2"

FILE NAME = 070\_DR095\_S-222.dgn  
PLOT DATE = 8/31/2016



**CHICAGO TRANSIT AUTHORITY ENGINEERING**

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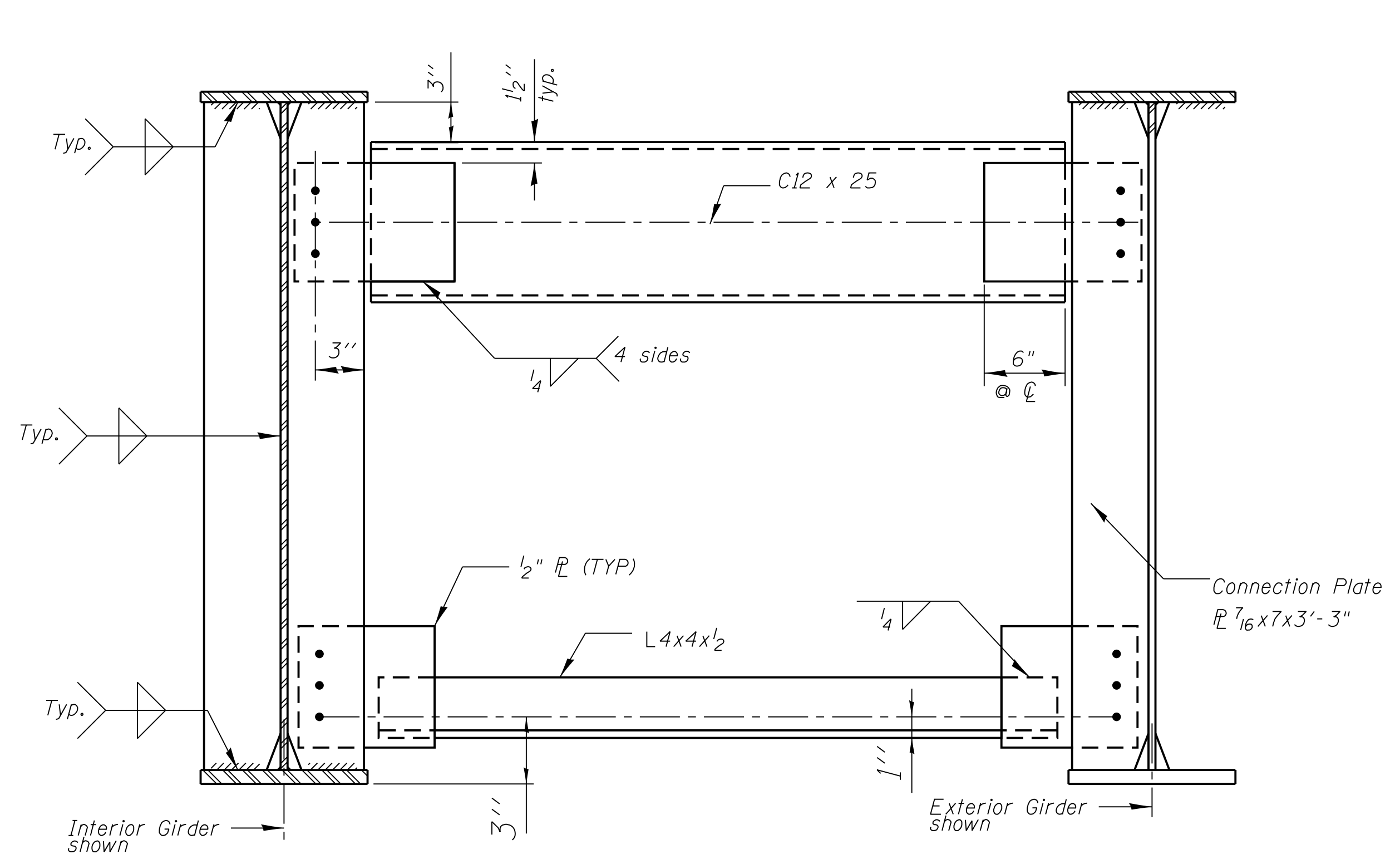
MARK	DATE	DESCRIPTION
	2/9/17	ISSUED FOR CONSTRUCTION

LOCATION IDENTIFIER: **DR095**

**SOUTH BUS BRIDGE  
STRUCTURAL STEEL DETAILS  
(SHEET 2 OF 4)**

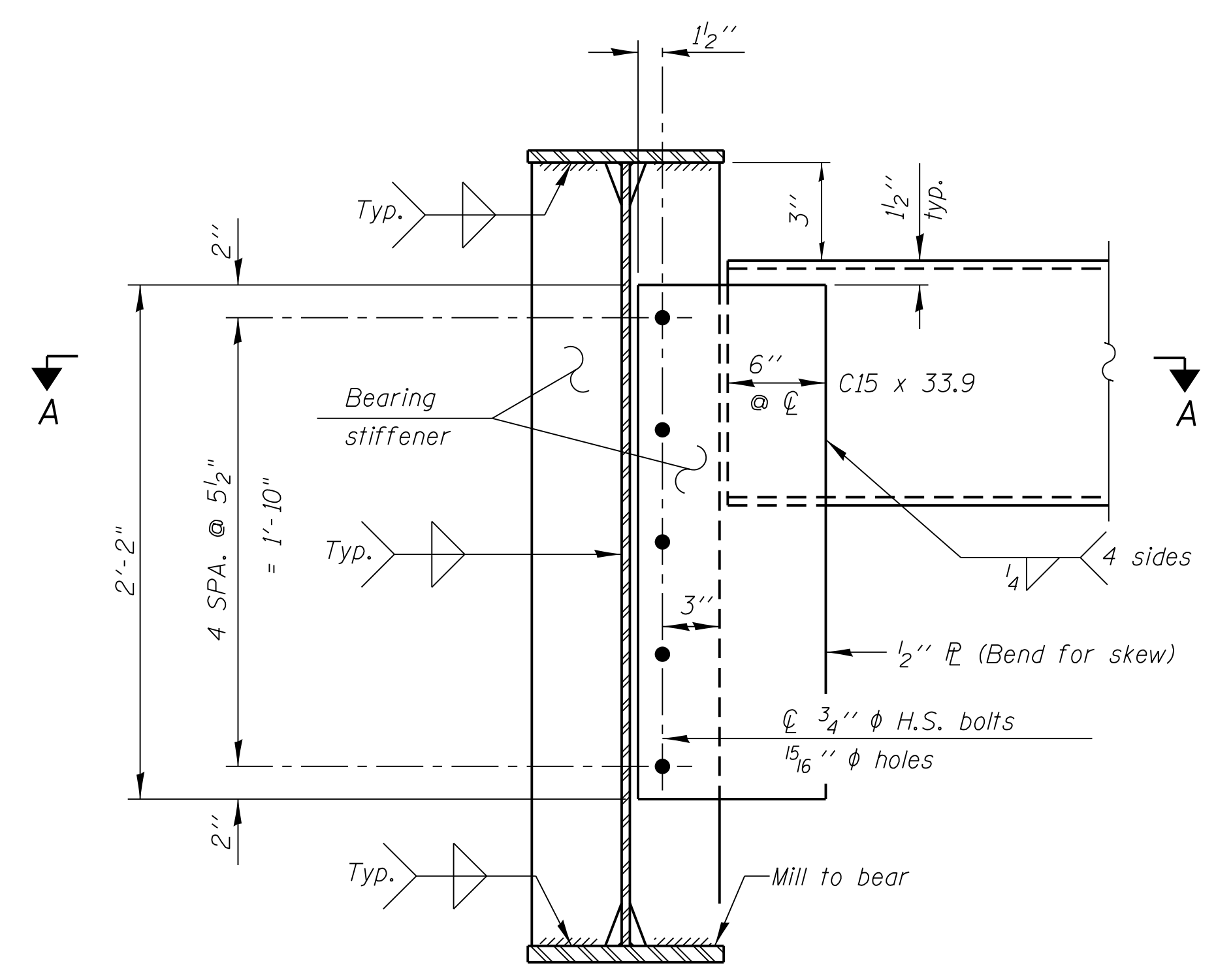
**SBB-S-222**

vscarpa

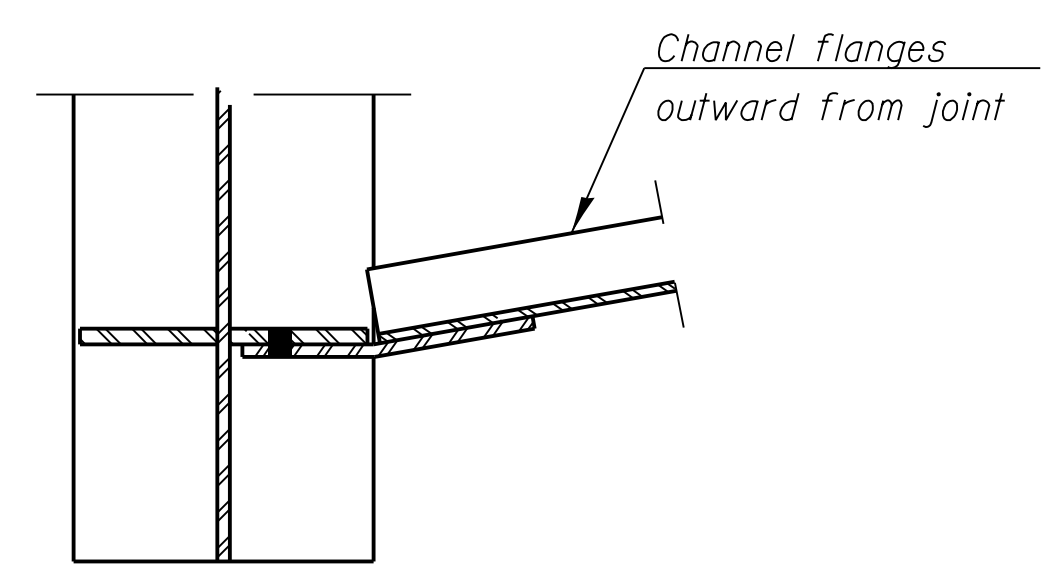


**INTERIOR DIAPHRAGM, D1**

Note:  
Detail 15/16"  $\phi$  holes for all 3/4"  $\phi$  bolts.  
Two hardened washers required for each set of oversized holes.



**END DIAPHRAGM, D**



**SECTION A - A**

Notes:  
1) For connection of canopy column to diaphragm D2, see architectural drawings.  
2) See weld termination detail on S-224.  
3) For diaphragm detail supporting canopy column, D2, see architectural drawings.



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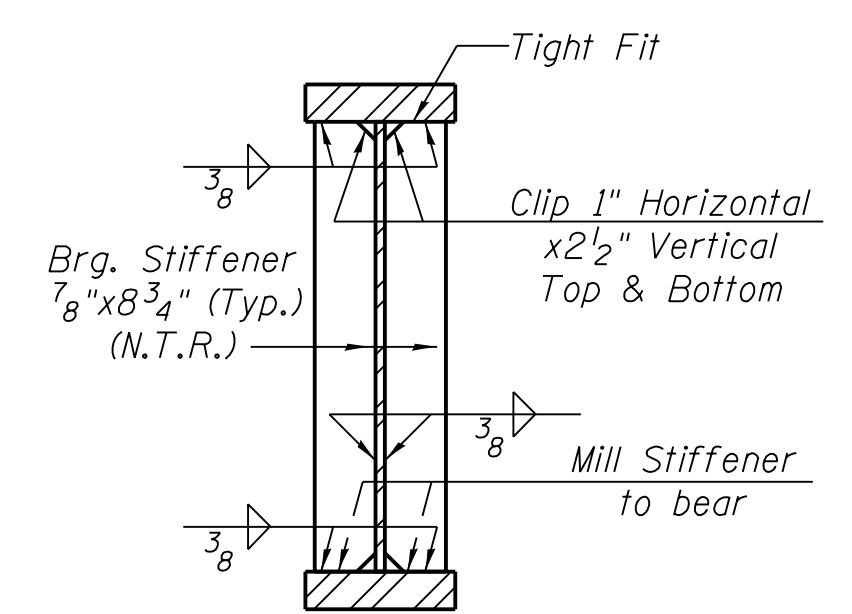
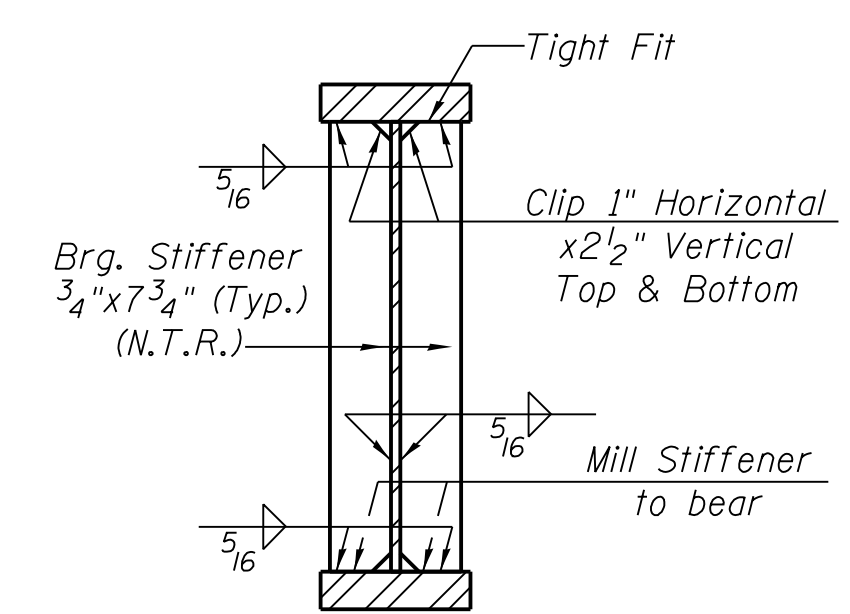
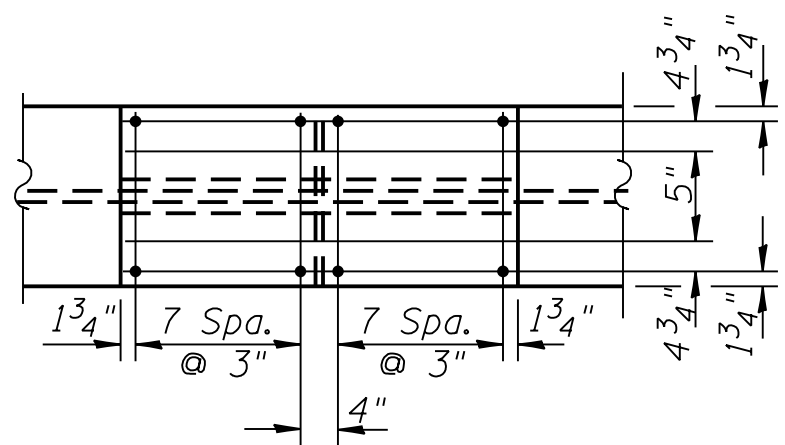
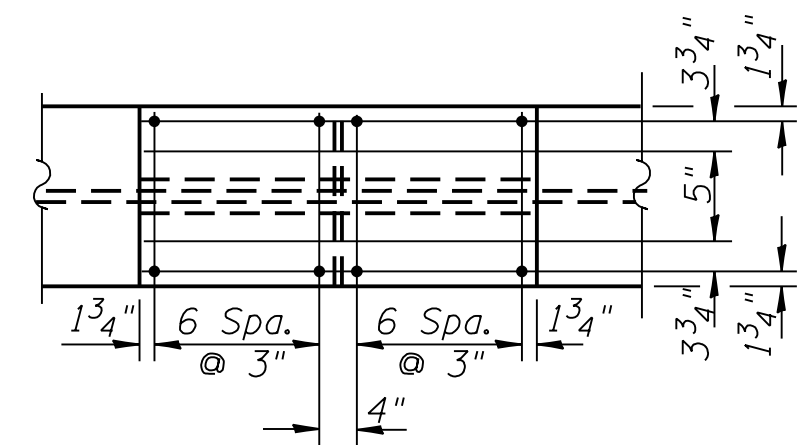
LOCATION IDENTIFIER: **DR095**

**SOUTH BUS BRIDGE**  
**STRUCTURAL STEEL DETAILS**  
(SHEET 3 OF 4)

**SBB-S-223**

FILE NAME = 070\_DR095\_S-223.dgn  
PLOT DATE = 8/31/2016

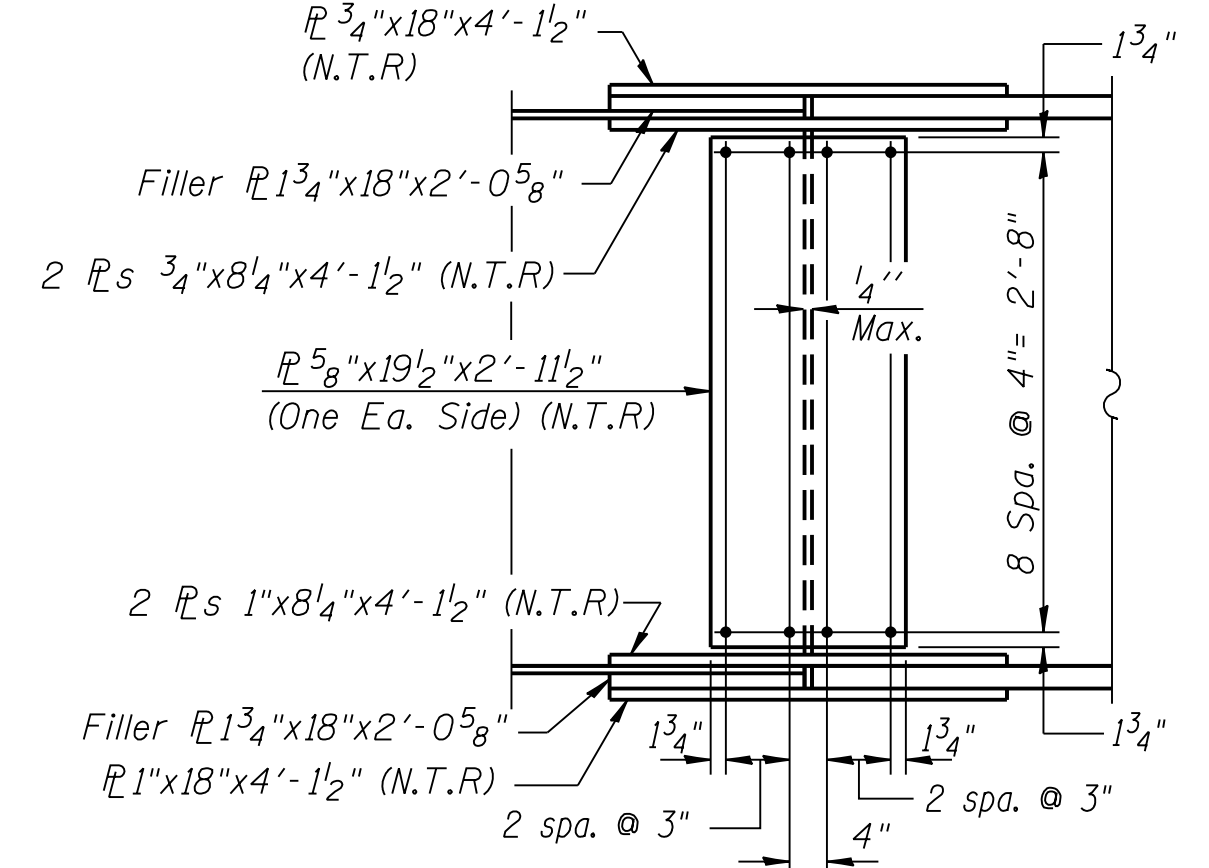
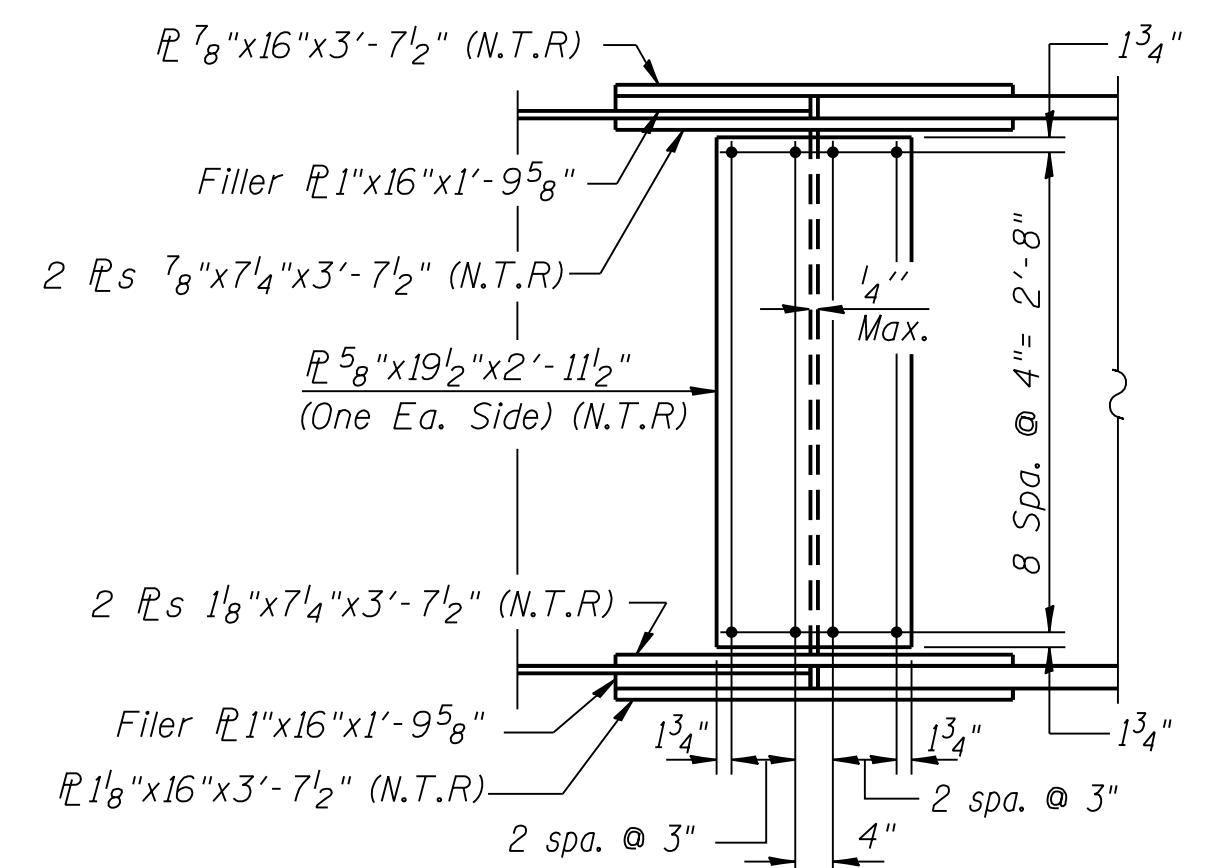
vscorpa



GIRDERS 1,4 Thru 10

GIRDERS 2 & 3

SECTION AT PIER & AT ABUTMENT

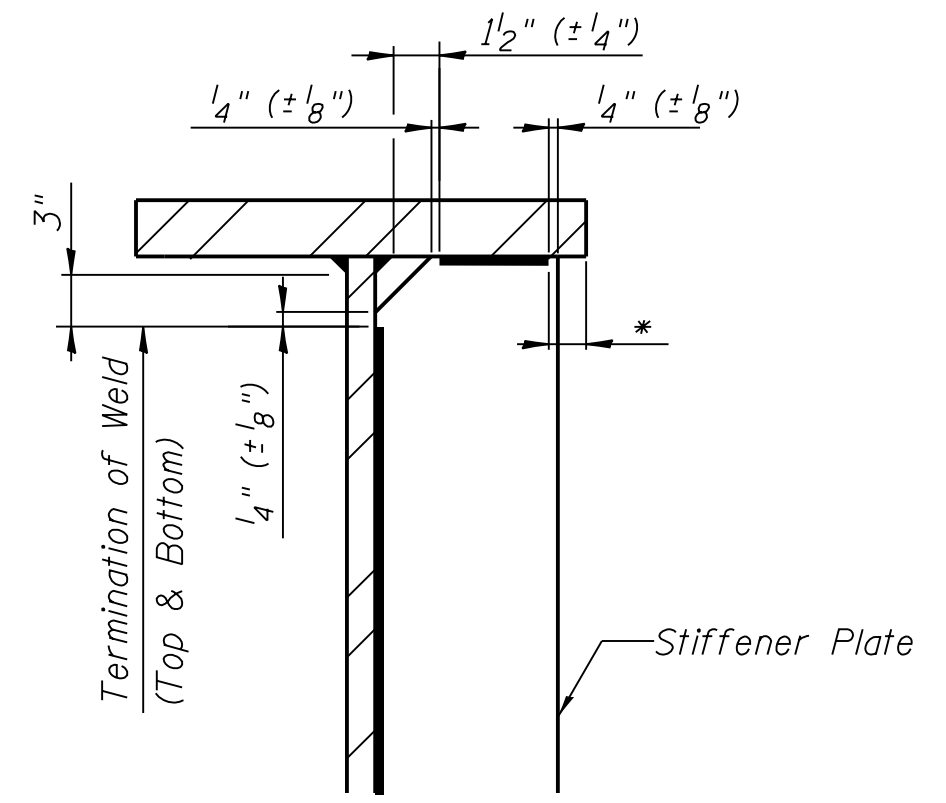


GIRDERS 1,4-10 AT SPLICES 1 & 2

GIRDERS 2 & 3 AT SPLICES 1 & 2

FIELD SPLICE DETAIL

NOTE:  
Notch toughness Required (N.T.R.) is required at all plates except at fillers



Weld Termination Detail

\* If stiffener plate extends beyond flange edge, the weld shall be terminated 1/4\"/>



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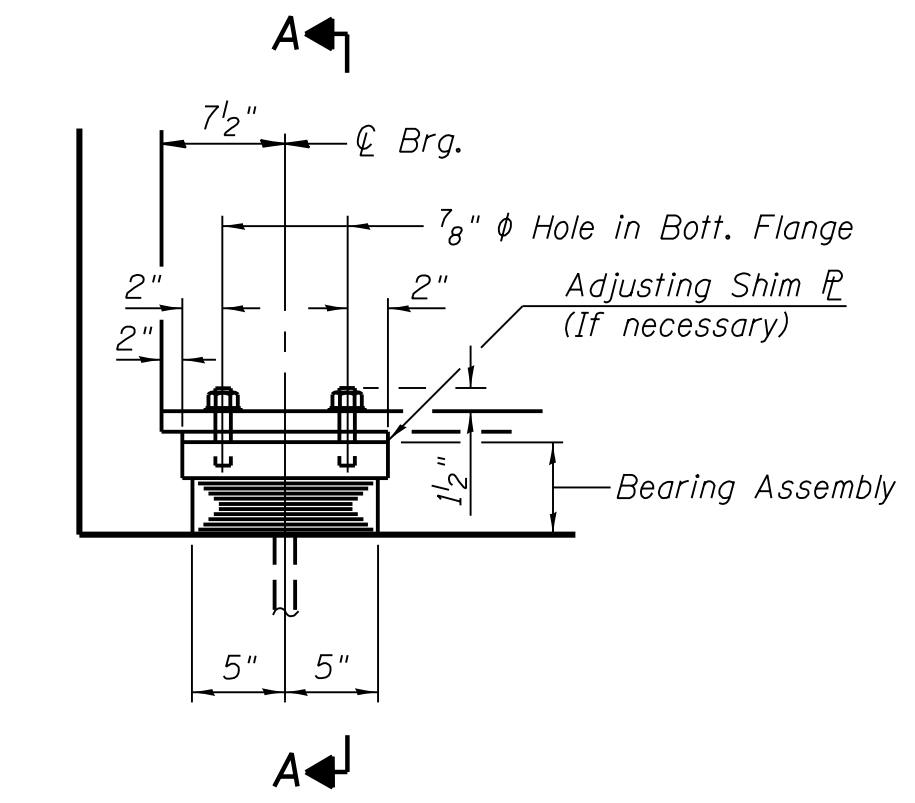
MARK	DATE	DESCRIPTION
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LOCATION IDENTIFIER: DR095  
SOUTH BUS BRIDGE  
STRUCTURAL STEEL DETAILS  
(SHEET 4 OF 4)

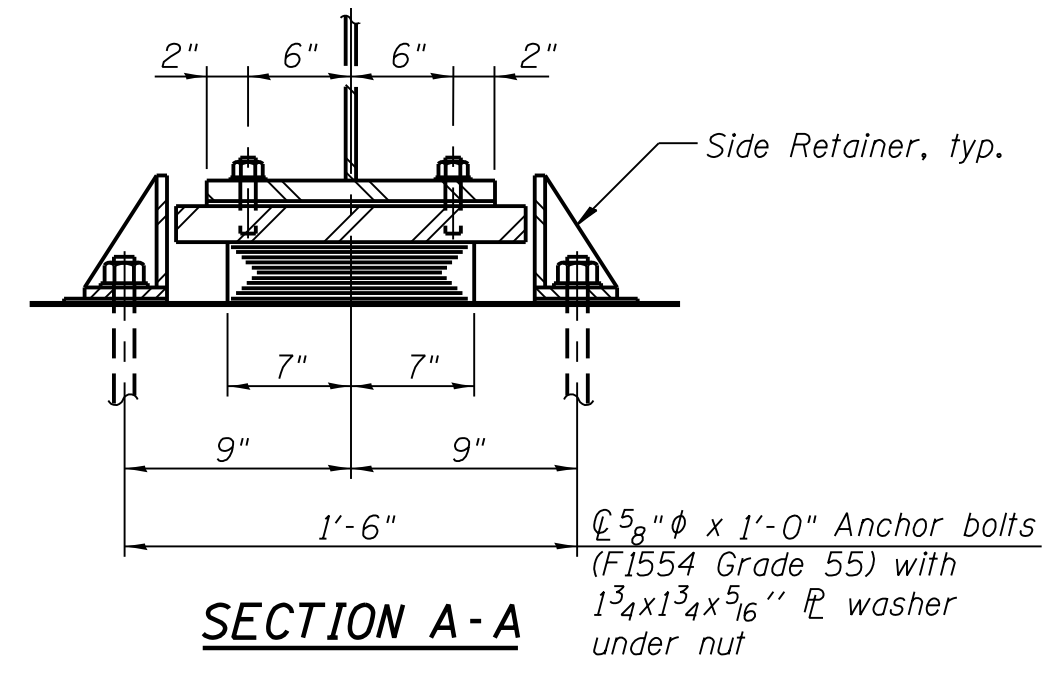
SBB-S-224

FILE NAME = 070\_DR095\_S-224.dgn  
PLOT DATE = 8/31/2016

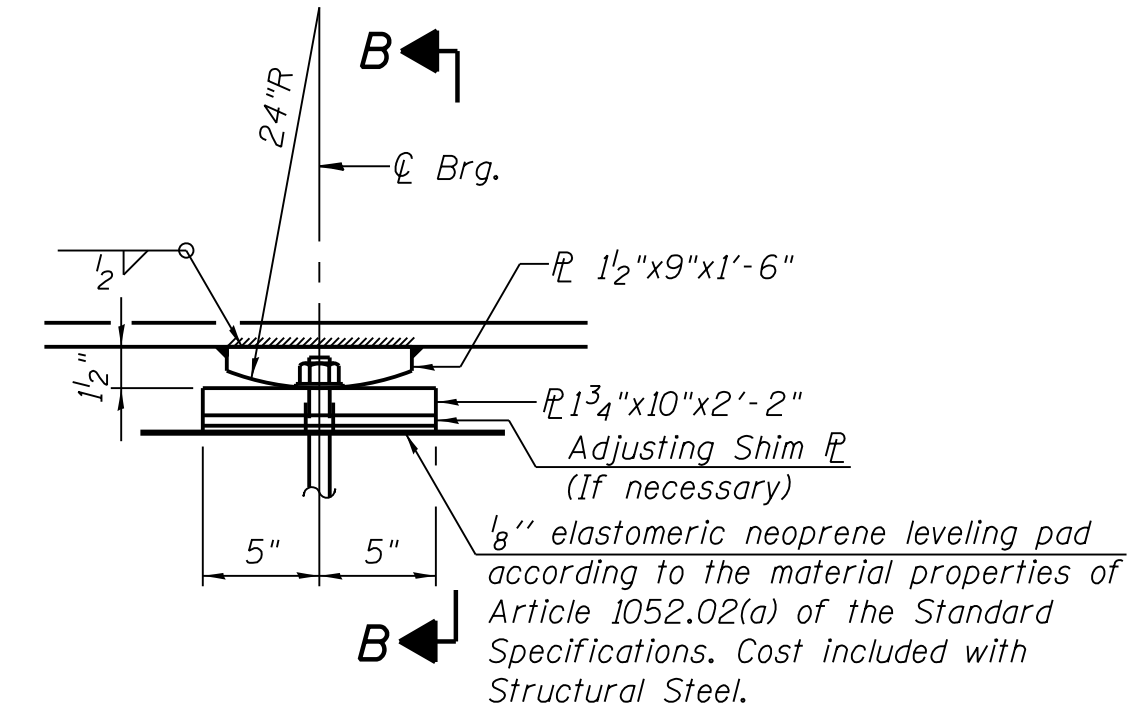
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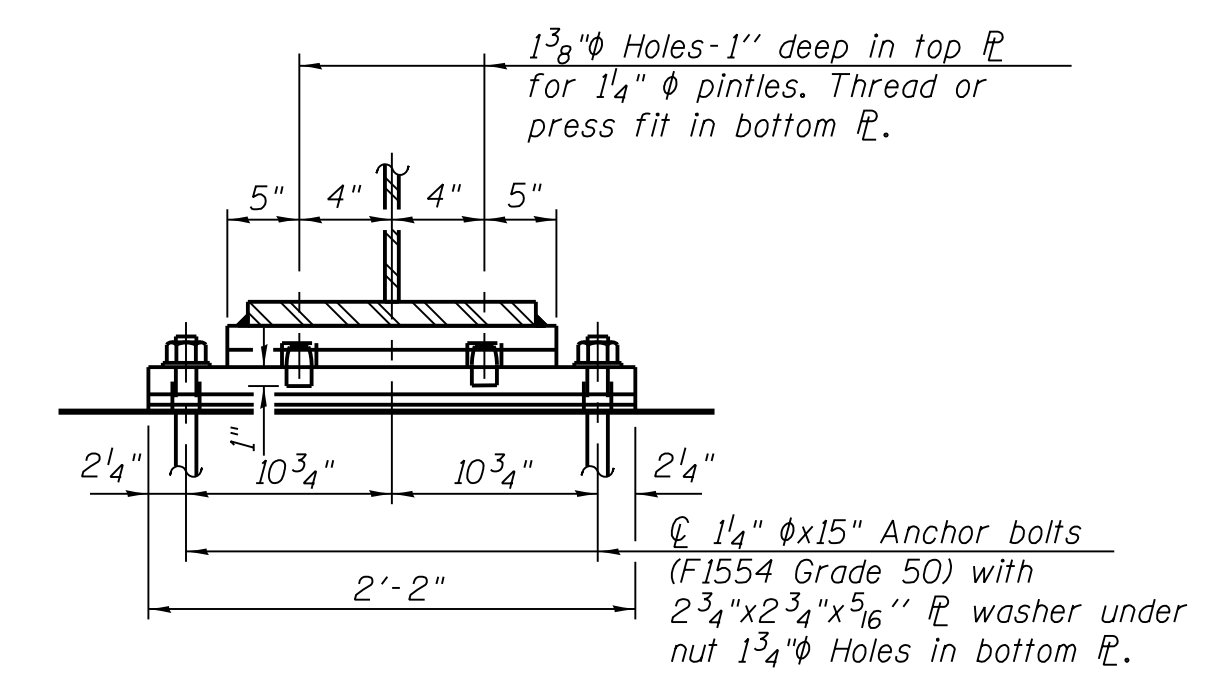
**ELEVATION AT ABUT.**  
Girders 1,4-10



**SECTION A-A**  
Anchor bolts (F1554 Grade 55) with 1 3/4 x 1 3/4 x 5/16 inch washer under nut



**ELEVATION AT PIER**  
Girders 1,4-10

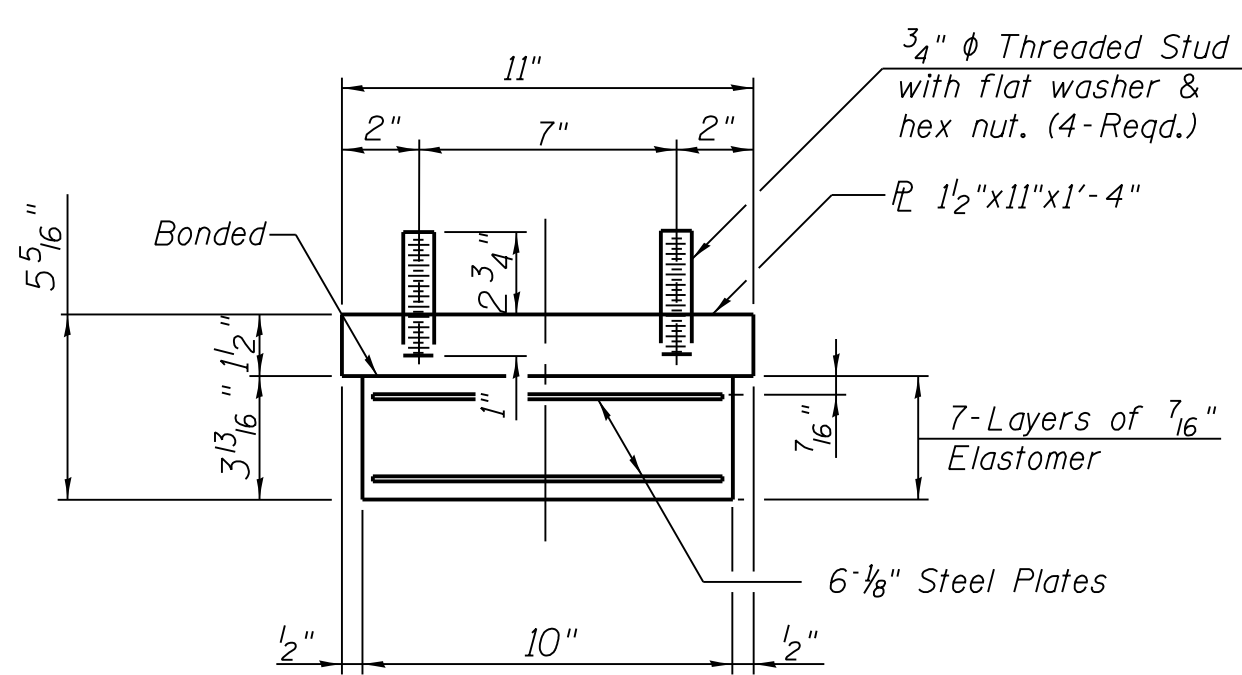


**SECTION B-B**

**FIXED BEARING**

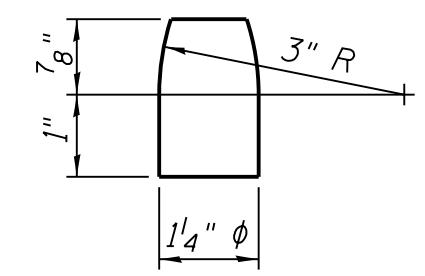
**TYPE I ELASTOMERIC EXP. BRG.**

- WEST ABUTMENT**  
Girder 1 add 1/2 x 11" x 1'-4" shim plate  
Girder 3 add 1/4 x 11" x 1'-4" shim plate  
Girder 5 add 1/2 x 11" x 1'-4" shim plate  
Girder 6 add 1/8 x 11" x 1'-4" shim plate
- EAST ABUTMENT**  
Girder 1 add 1/4 x 11" x 1'-4" shim plate  
Girder 4 add 1/4 x 11" x 1'-4" shim plate  
Girder 5 add 1/8 x 11" x 1'-4" shim plate



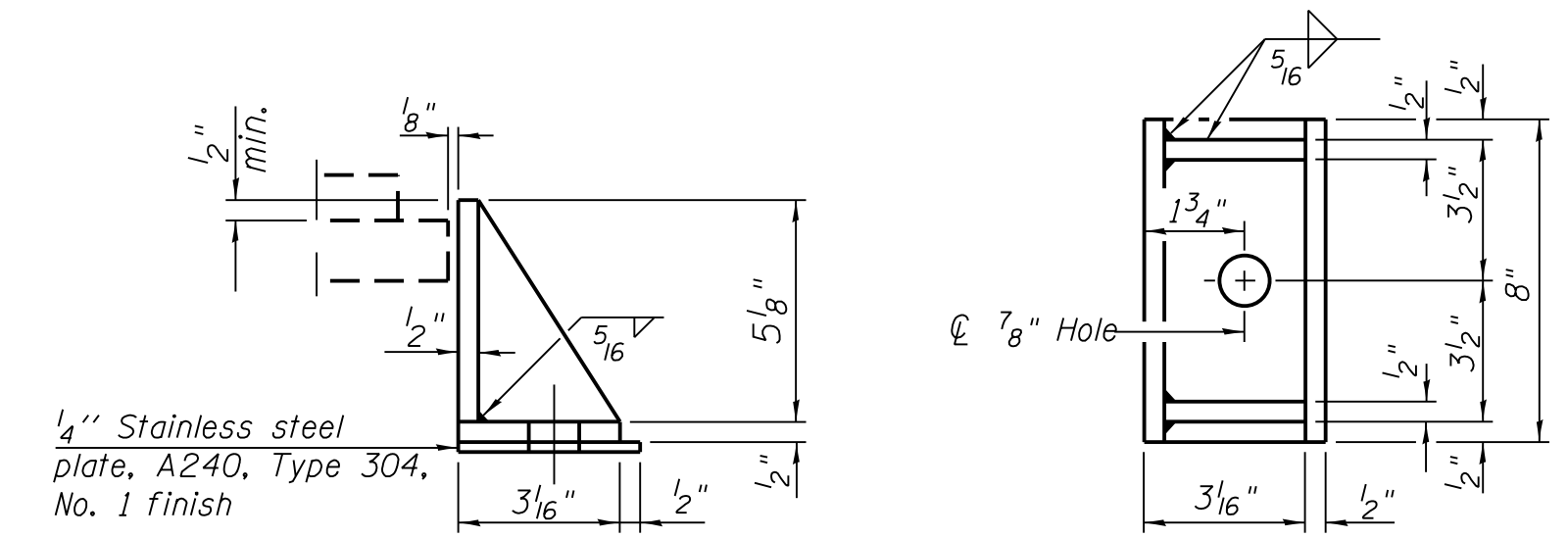
**BEARING ASSEMBLY**

Note:  
Shim plates shall not be placed under Bearing Assembly.



**PINTLE**

Notes:  
Anchor bolts shall be ASTM F1554 all-thread (or an Engineer-approved alternate material) of the grade(s) and diameter(s) specified. The corresponding specified grade of AASHTO M314 anchor bolts may be used in lieu of ASTM F1554.  
Side retainers and other steel members required for the elastomeric bearing assembly shall be included in the cost of Elastomeric Bearing Assembly, Type I.  
Beams shall be braced for stability during erection and remain braced until deck is poured and cured.  
Anchor bolts and side retainers at all supports shall be installed as each member is erected unless an equivalent temporary means of lateral restraint is used.



**SIDE RETAINER**  
Equivalent rolled angle with stiffeners will be allowed in lieu of welded plates.

FILE NAME = 070\_DR095\_S-225.dgn  
PLOT DATE = 8/31/2016



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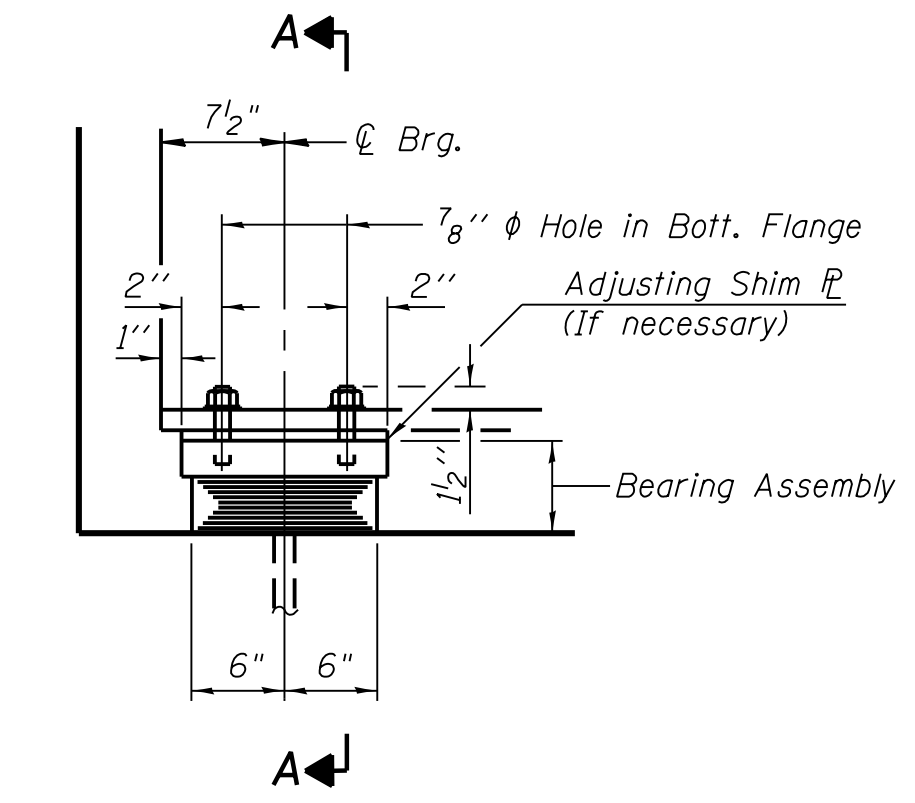
IN CHARGE	S. KADERBEK
APPROVED BY	D.ZROKA
CHECKED BY	P.MAID
DESIGNED BY	L.SOMMER
DRAWN BY	T.SLEDGE
PROJECT NO.	2012-0021
FILE NAME	File Name

MARK	DATE	DESCRIPTION
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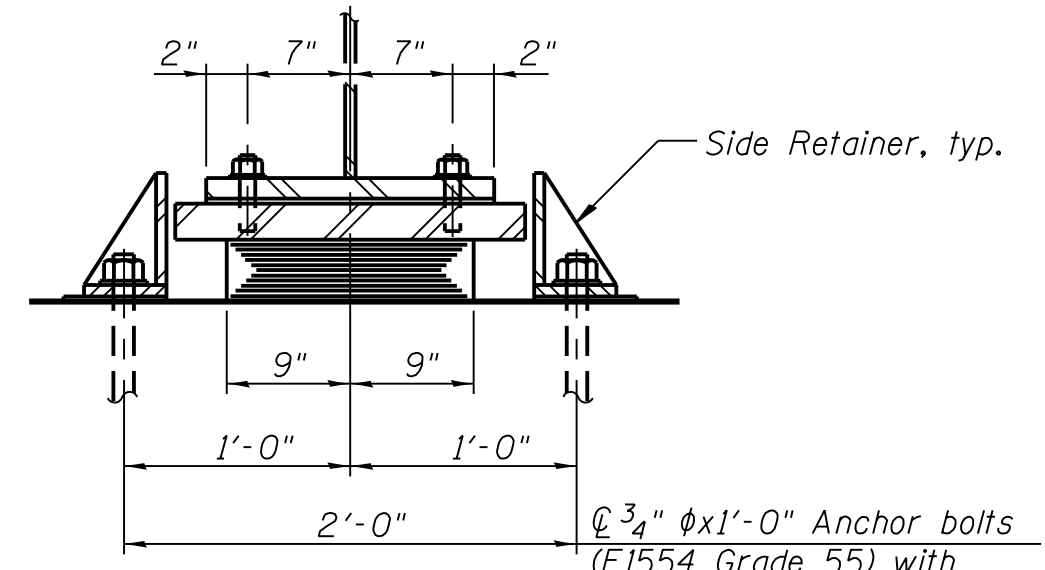
LOCATION IDENTIFIER: **DR095**  
SOUTH BUS BRIDGE  
BEARING DETAILS  
(SHEET 1 OF 4)

**SBB-S-225**

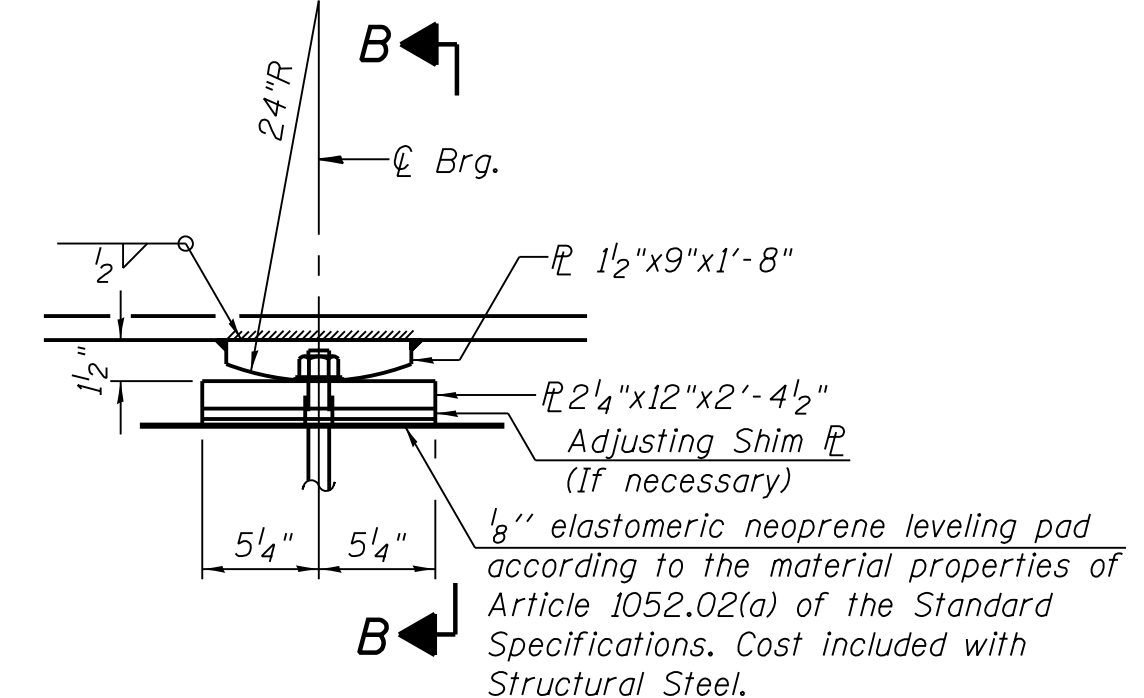
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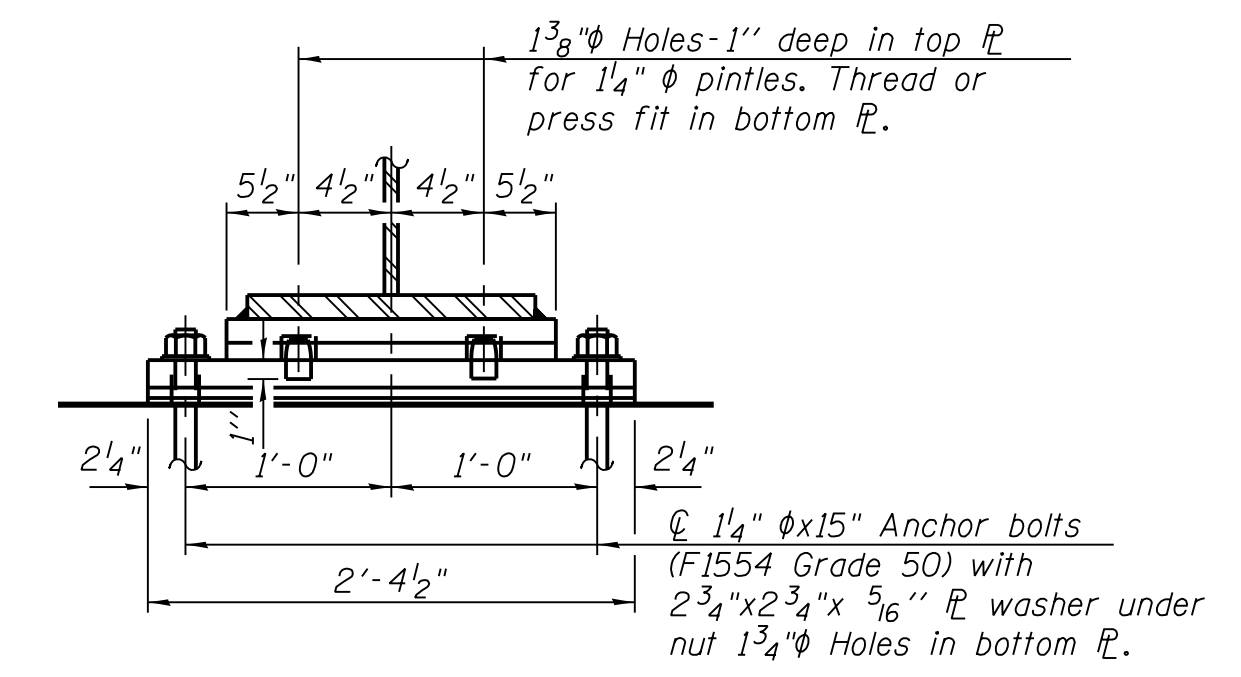
**ELEVATION AT ABUT.**



**SECTION A-A**



**ELEVATION AT PIER**

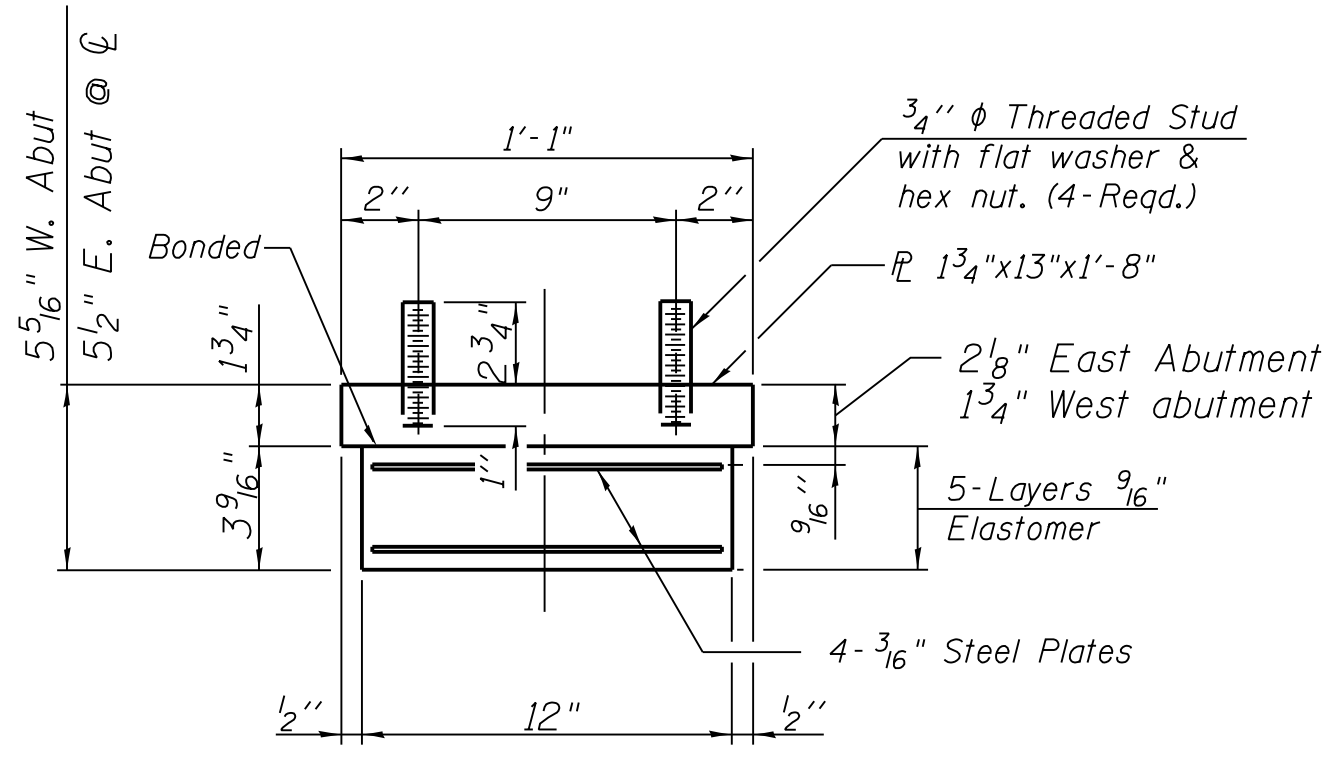


**SECTION B-B**

**TYPE I ELASTOMERIC EXP. BRG.**

(Girders 2 & 3)  
**WEST ABUTMENT**  
 Girder 2 add 1/2x1'-0"x1'-6" shim plate  
 Girder 3 add 3/8x1'-0"x1'-6" shim plate

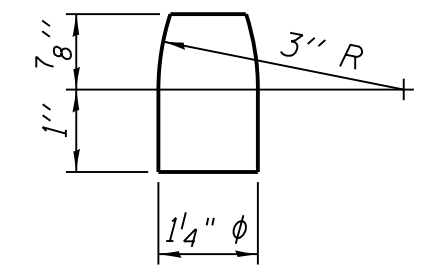
**FIXED BEARING**  
 Girders 2 & 3



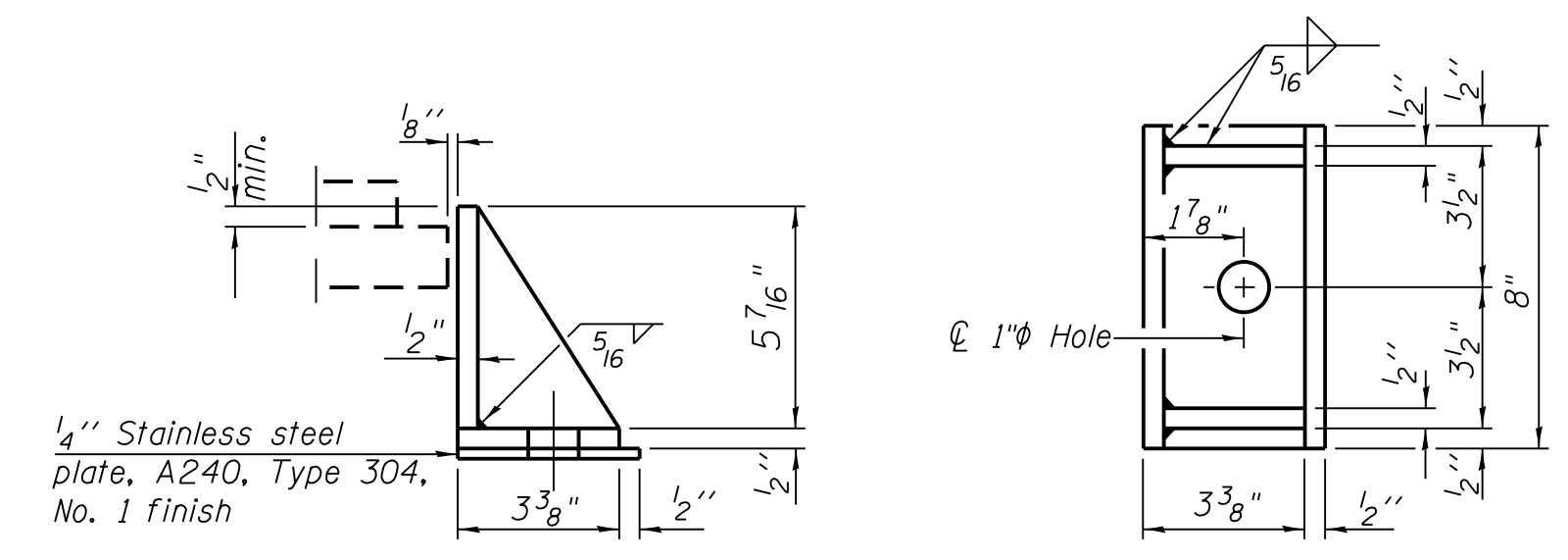
**BEARING ASSEMBLY**

Note:  
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Notes:  
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 Anchor bolts and side retainers at all supports shall be installed as each member is erected unless an equivalent temporary means of lateral restraint is used.



**PINTLE**



**SIDE RETAINER**

Equivalent rolled angle with stiffeners will be allowed in lieu of welded plates.

FILE NAME = 070\_DR095\_S-226.dgn  
 PLOT DATE = 8/31/2016



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**95th Terminal Improvement**  
 15 West 95th Street (South Terminal)  
 Chicago, IL 60628

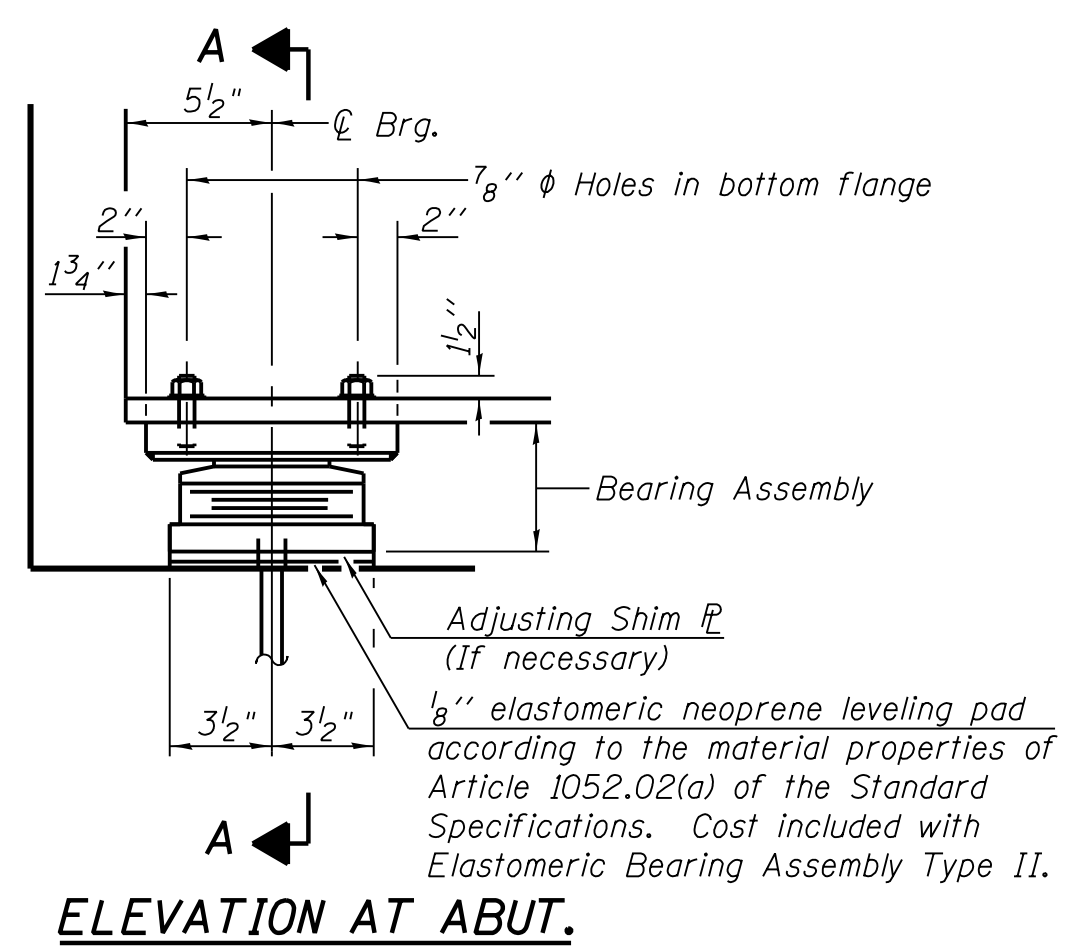
IN CHARGE	S. KADERBEK
APPROVED BY	D.ZROKA
CHECKED BY	P.MAID
DESIGNED BY	L.SOMMER
DRAWN BY	T.SLEDGE
PROJECT NO.	2012-0021
FILE NAME	File Name

MARK	DATE	DESCRIPTION
	2/9/17	ISSUED FOR CONSTRUCTION

LOCATION IDENTIFIER: **DR095**  
 SOUTH BUS BRIDGE  
 BEARING DETAILS  
 (SHEET 2 OF 4)

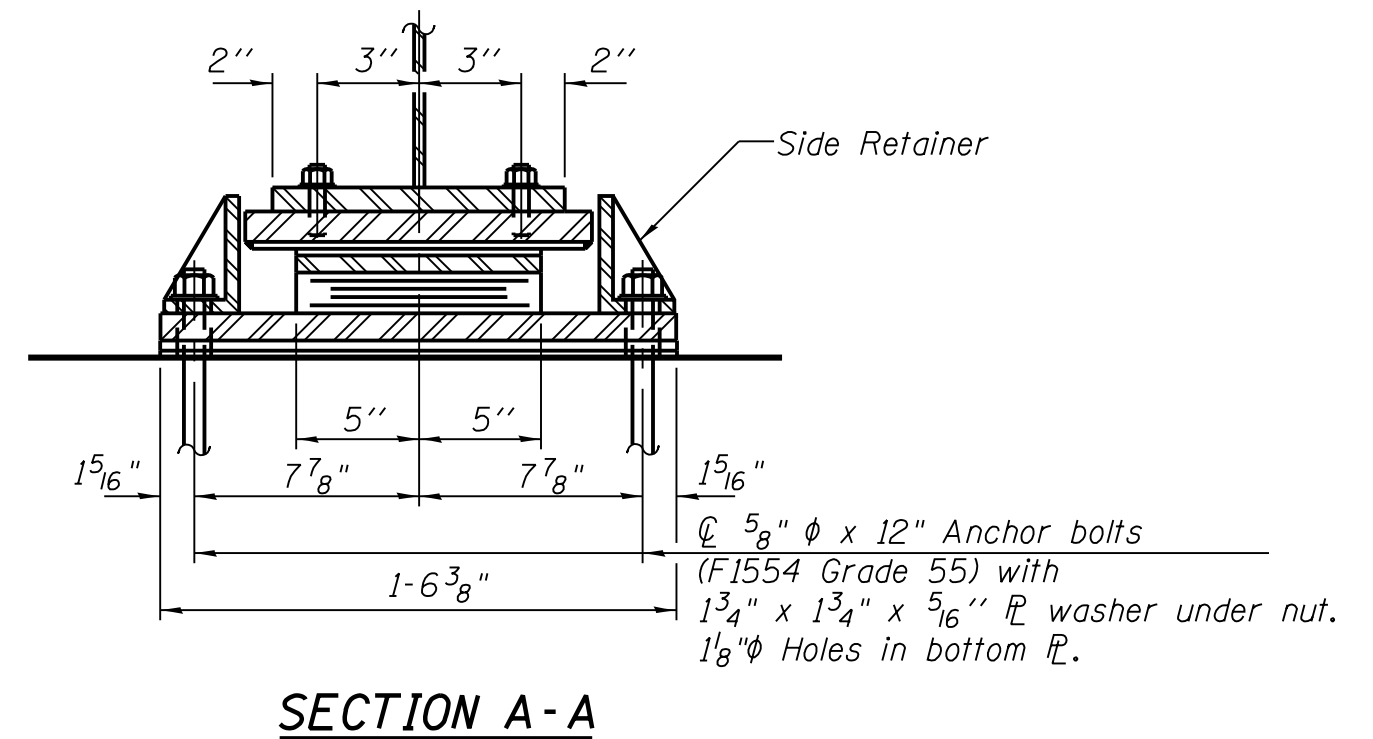
**SBB-S-226**

vscarpa

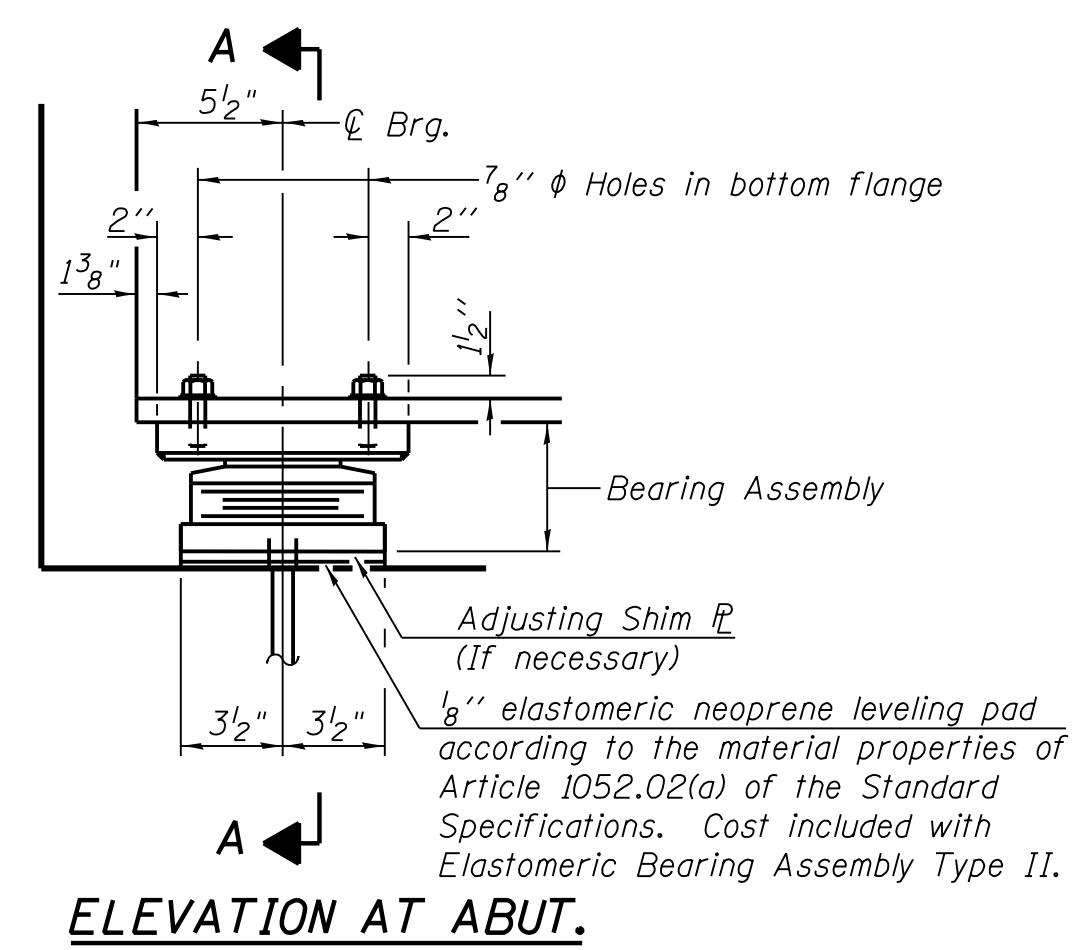


**ELEVATION AT ABUT.**

**TYPE II ELASTOMERIC EXP. BRG.**  
(Beams 2W, 2E, 3W, & 3E)

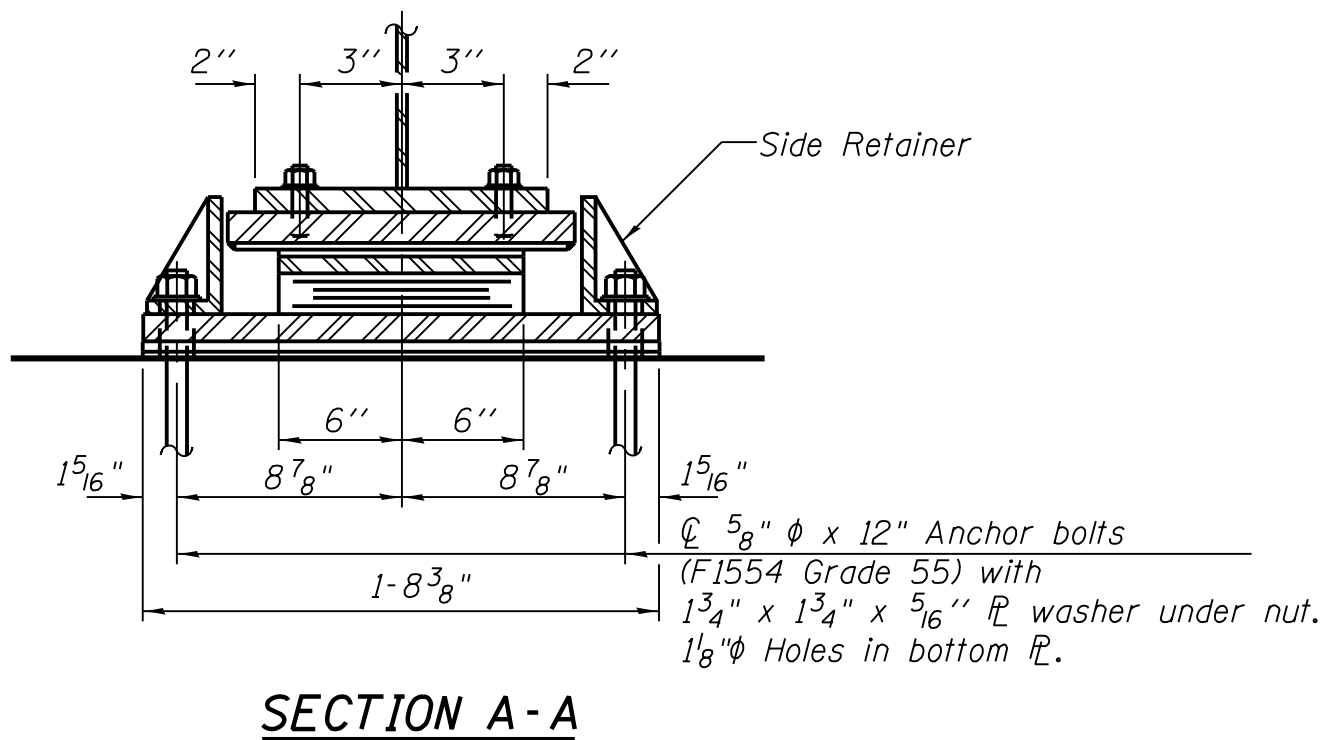


**SECTION A-A**

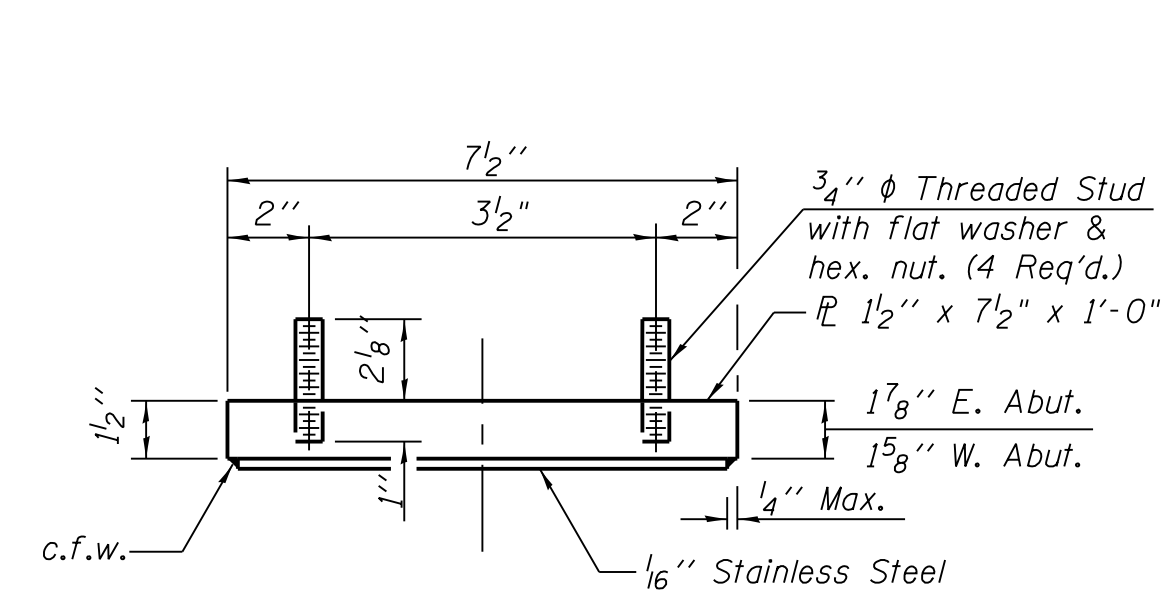


**ELEVATION AT ABUT.**

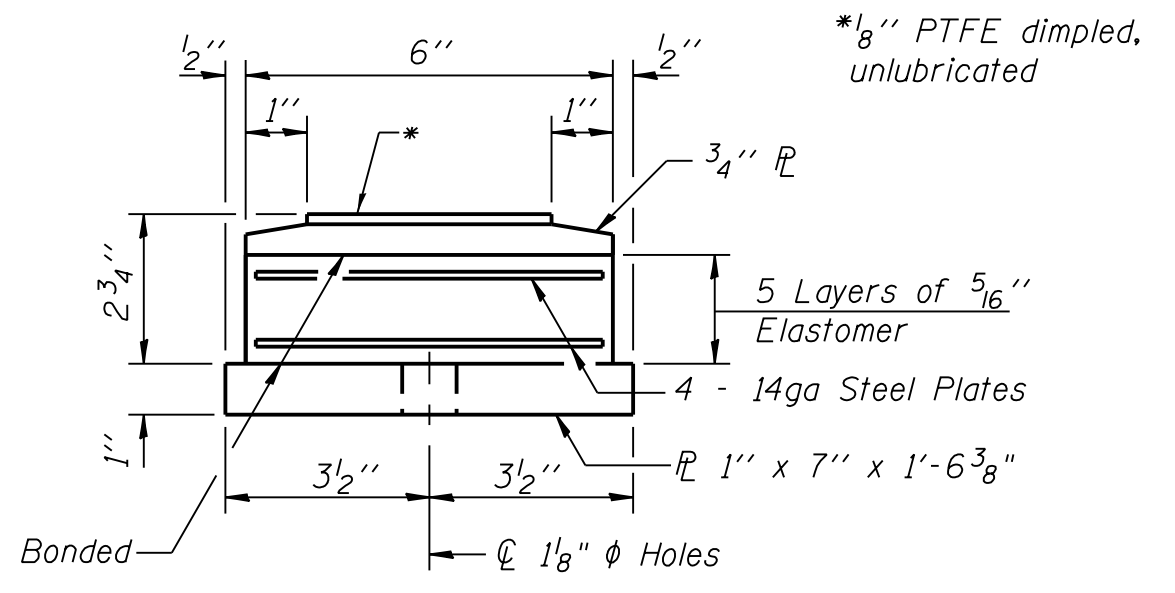
**TYPE II ELASTOMERIC EXP. BRG.**  
(Beams 1E & 1W)  
Girder 1E add 1/8"x8 1/4"x1'-2" shim plate



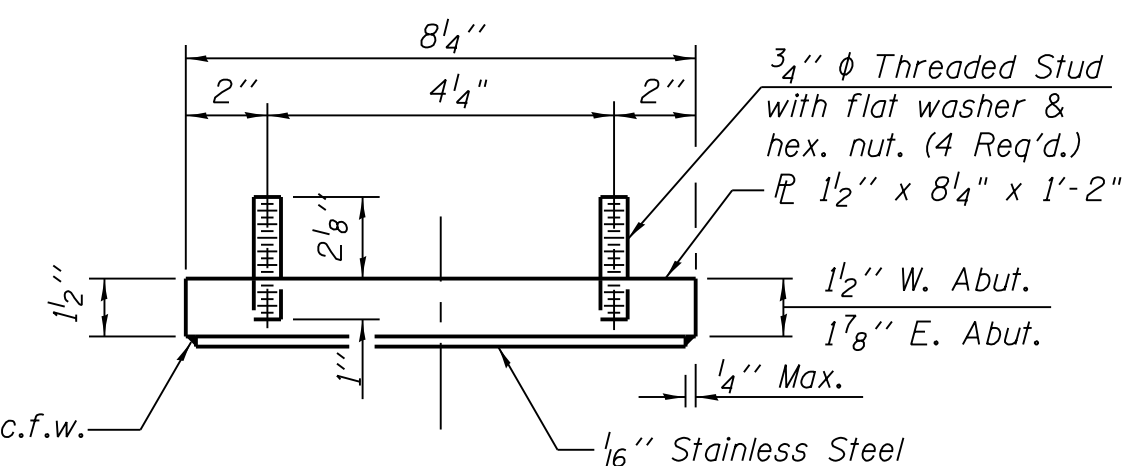
**SECTION A-A**



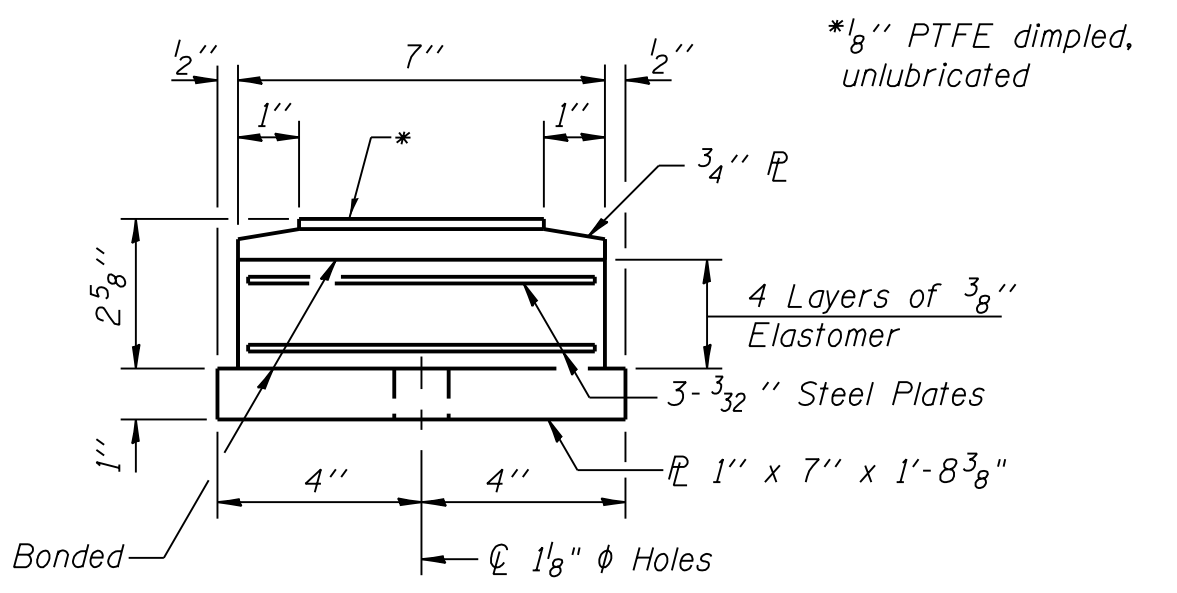
**TOP BEARING ASSEMBLY**  
(Beams 2W, 2E, 3W, & 3E)



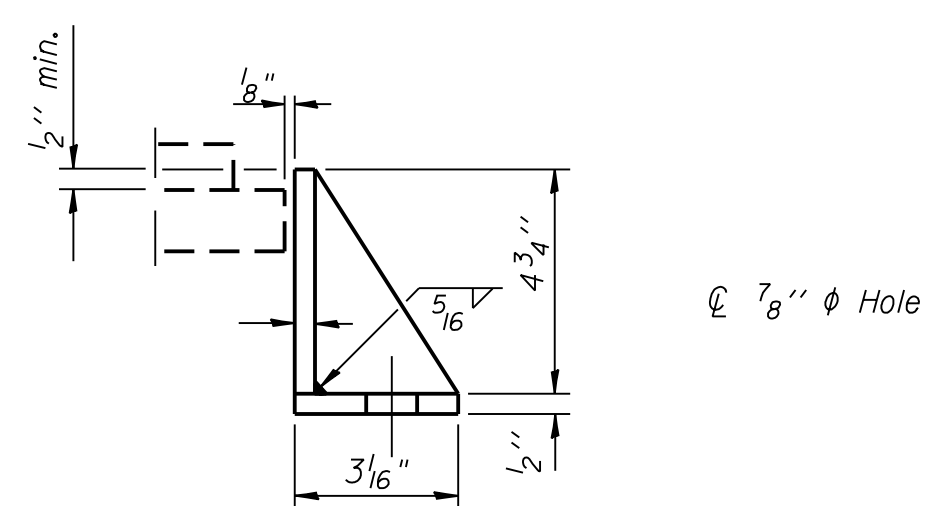
**BOTTOM BEARING ASSEMBLY**  
(Beams 2W, 2E, 3W, & 3E)



**TOP BEARING ASSEMBLY**  
(Beams 1E & 1W)

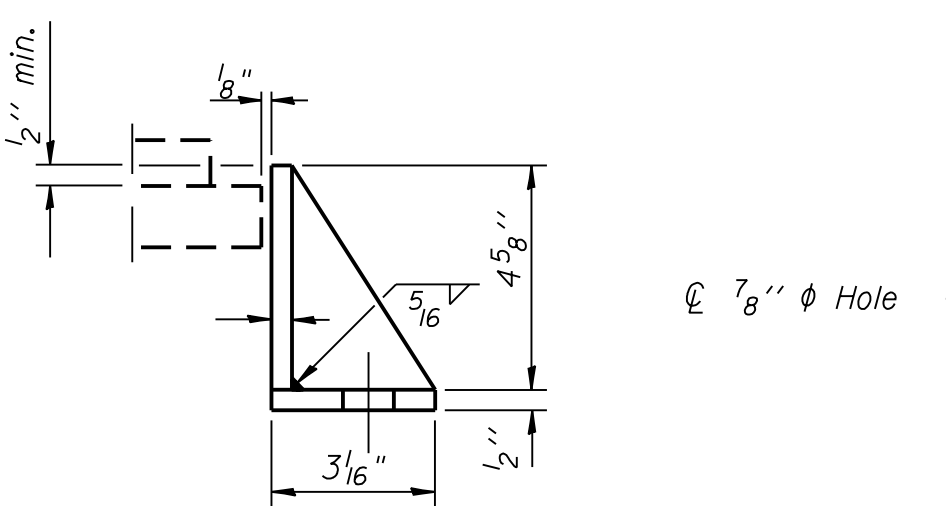


**BOTTOM BEARING ASSEMBLY**  
(Beams 1E & 1W)



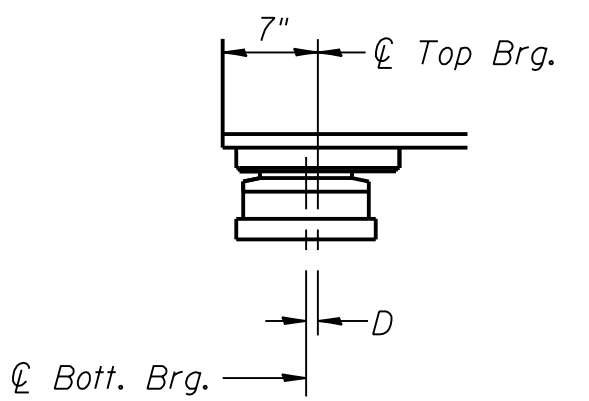
**SIDE RETAINER**

Equivalent rolled angle with stiffeners will be allowed in lieu of welded plates.  
(Beams 2W, 2E, 3W, & 3E)



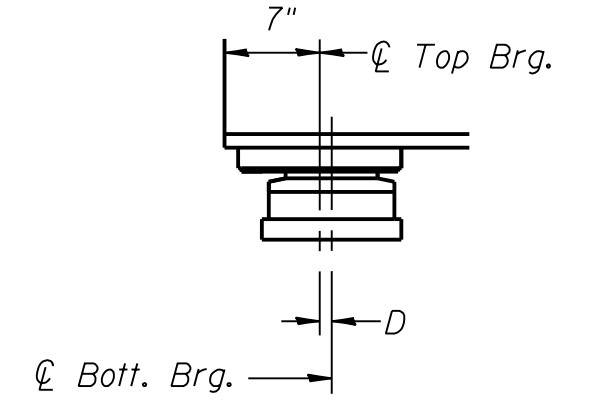
**SIDE RETAINER**

Equivalent rolled angle with stiffeners will be allowed in lieu of welded plates.  
(Beams 1E & 1W)



**BELOW 50°F.**

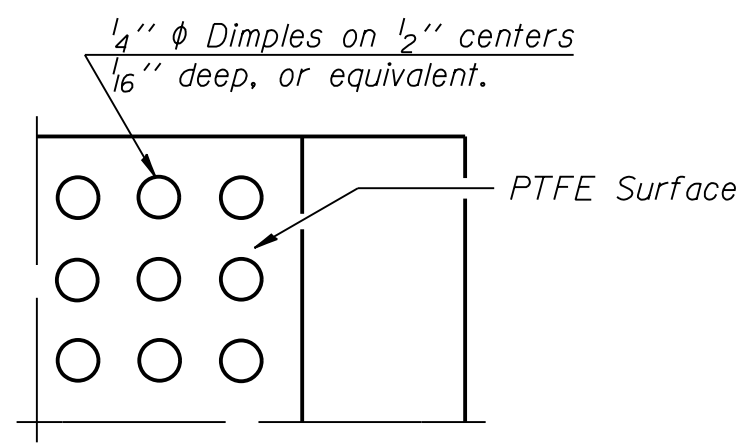
D = 1/8" per each 100' of expansion for every 15° temp. change from the normal temp. of 50°F.



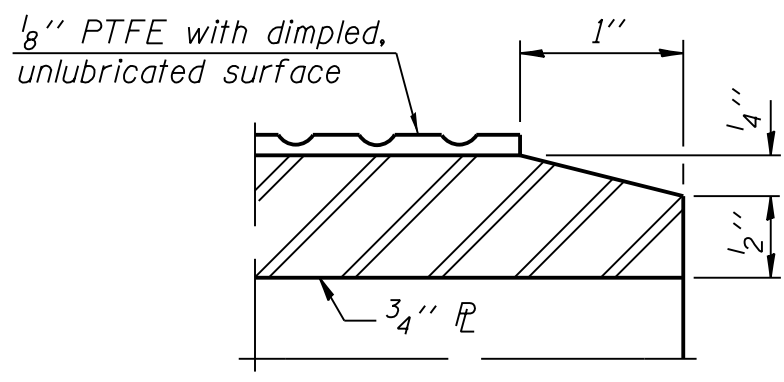
**ABOVE 50°F.**

**EXPANSION BEARING ORIENTATION**

The above diagrams are for informational purposes only to show the amount of expected offset "D" for the current temperature in the field.



**PLAN-PTFE SURFACE**



**SECTION THRU PTFE**

**Notes:**  
Anchor bolts shall be ASTM F1554 all-thread (or an Engineer-approved alternate material) of the grade(s) and diameter(s) specified. The corresponding specified grade of AASHTO M314 anchor bolts may be used in lieu of ASTM F1554.  
Side retainers and other steel members required for the elastomeric bearing assembly shall be included in the cost of Elastomeric Bearing Assembly, Type II.  
The 1/8" PTFE sheet shall be bonded directly to the top steel plate with a two-component, medium viscosity epoxy resin, conforming to the requirements of the Federal Specification MMM-A-134, Type I. The bond agent shall be applied on the full area of the contact surfaces.  
Bonding of 1/8" PTFE sheet during vulcanizing process will be permitted provided the process and method of adjusting assembly height is approved by the Engineer.  
Beams shall be braced for stability during erection and remain braced until deck is poured and cured.  
Anchor bolts and side retainers at all supports shall be installed as each member is erected unless an equivalent temporary means of lateral restraint is used.



**CHICAGO TRANSIT AUTHORITY ENGINEERING**

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**95th Terminal Improvement**  
15 West 95th Street (South Terminal)  
Chicago, IL 60628

<b>IN CHARGE</b>	S. KADERBEK
<b>APPROVED BY</b>	D.ZROKA
<b>CHECKED BY</b>	P.MAID
<b>DESIGNED BY</b>	L.SOMMER
<b>DRAWN BY</b>	T.SLEDGE
<b>PROJECT NO.</b>	2012-0021
<b>FILE NAME</b>	File Name

<b>MARK</b>	<b>DATE</b>	<b>DESCRIPTION</b>
	2/9/17	ISSUED FOR CONSTRUCTION

**LOCATION IDENTIFIER: DR095**

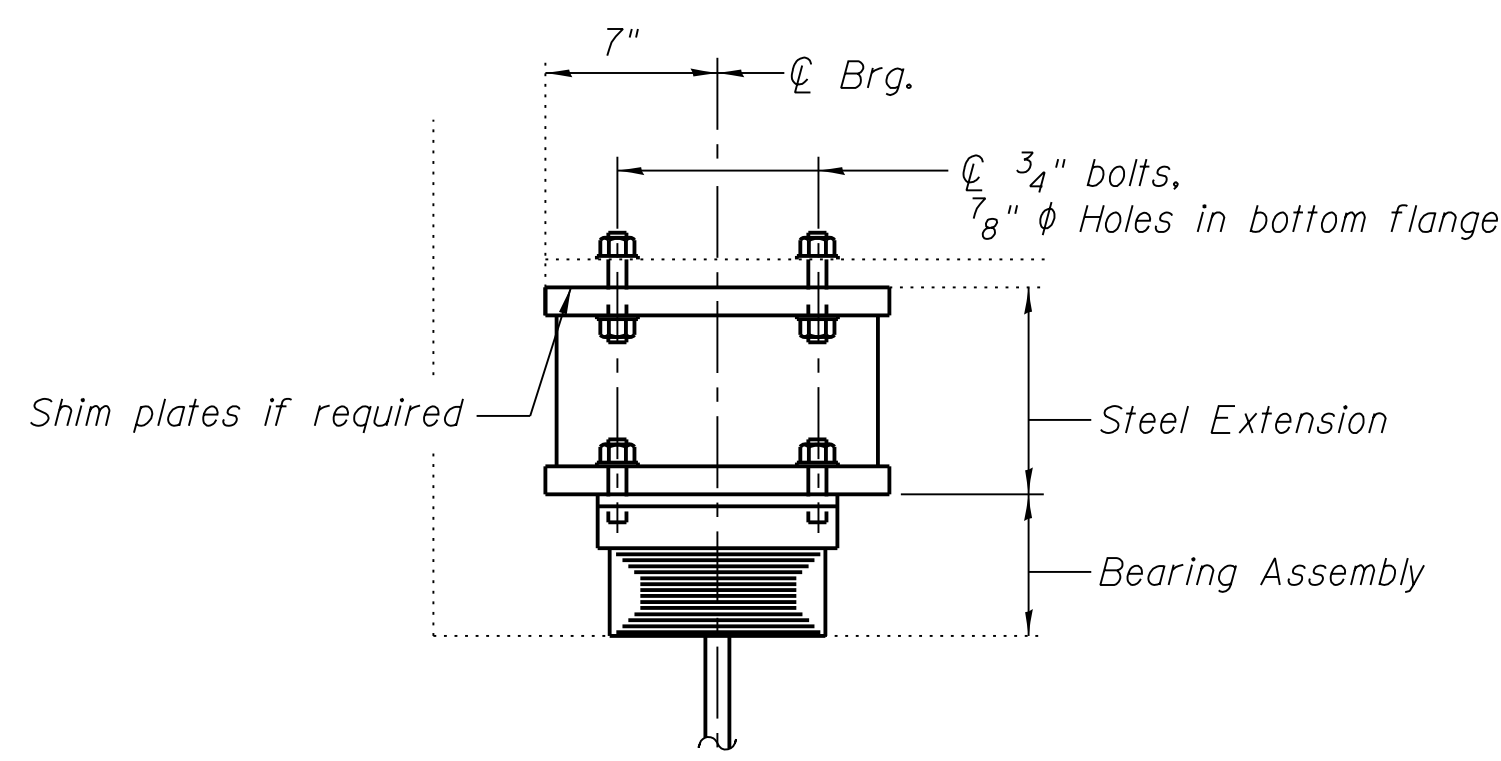
**SOUTH BUS BRIDGE BEARING DETAILS (SHEET 3 OF 4)**

**SBB-S-227**

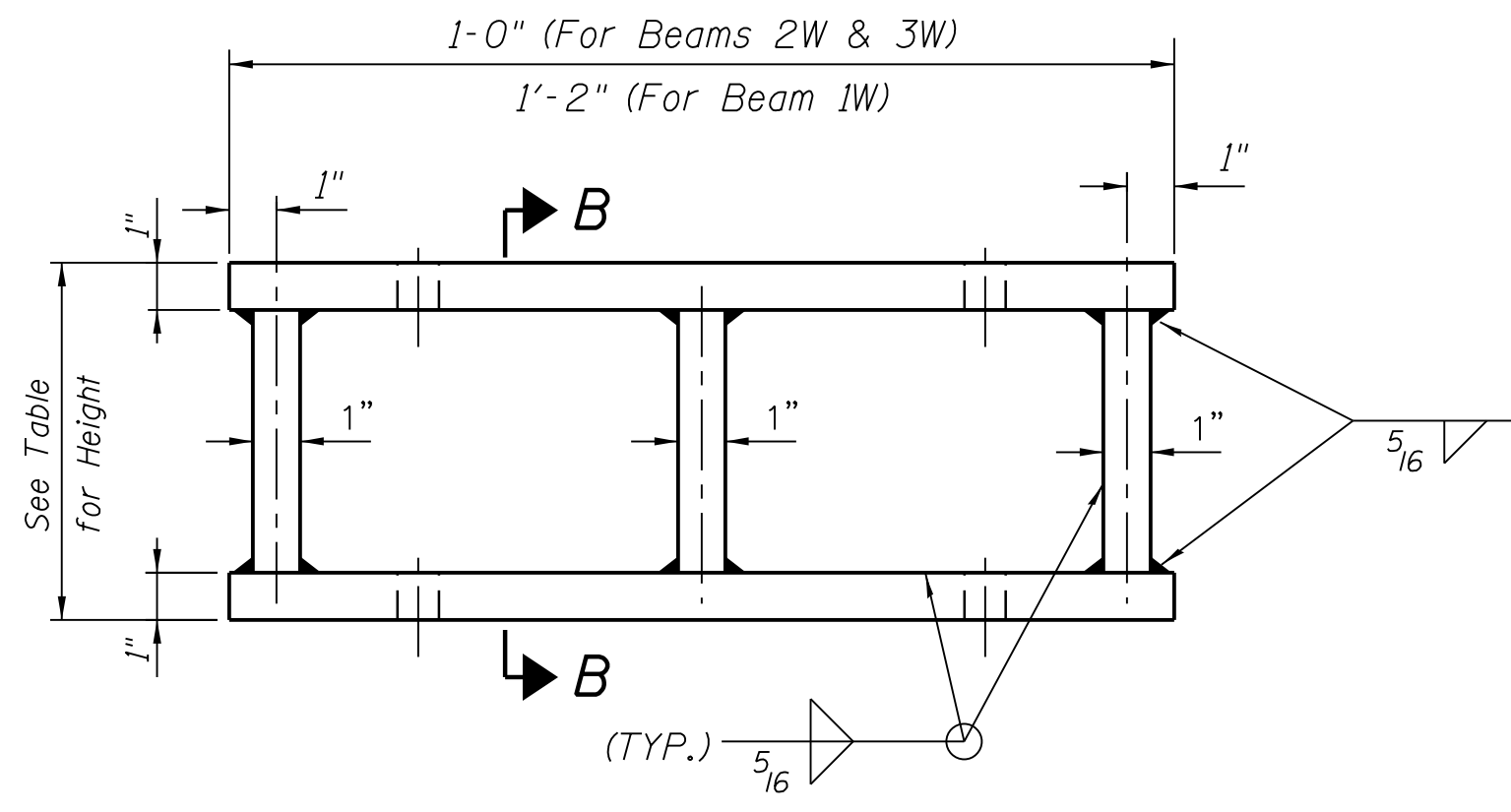
FILE NAME = 070\_DR095\_S-227.dgn  
PLOT DATE = 8/31/2016



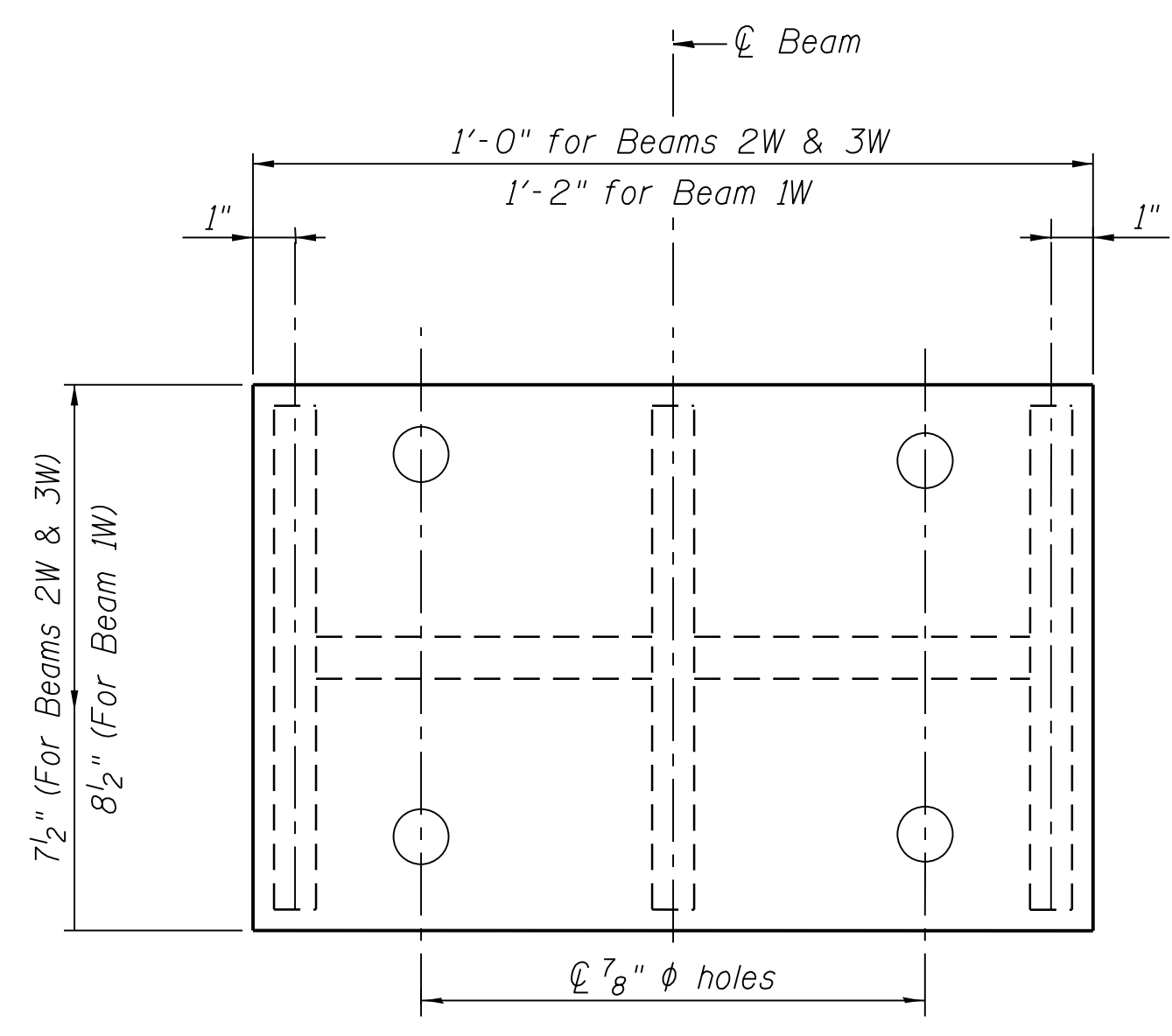
vscarpa



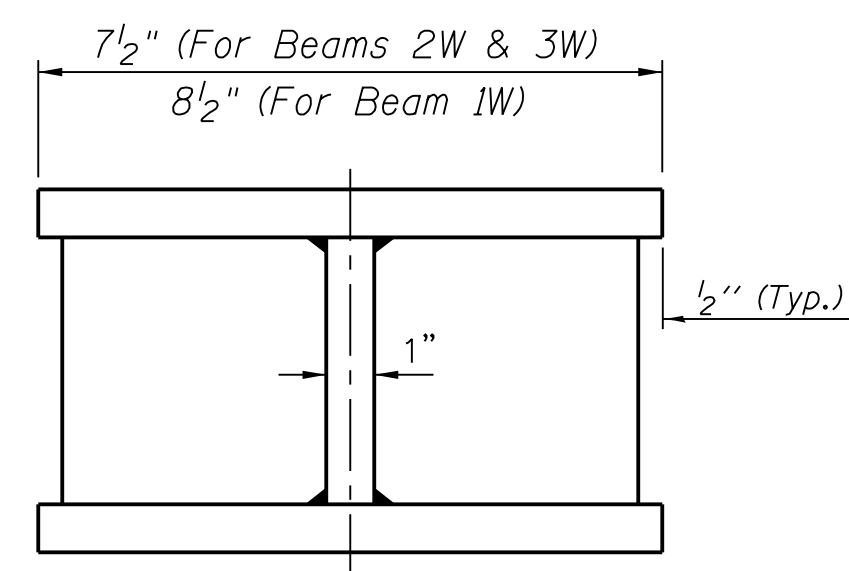
**ELEVATION AT FLARED BEAMS**



**ELEVATION TALL STEEL EXTENSION**



**PLAN TALL STEEL EXTENSION**



**SECTION B-B**

**TABLE OF TALL EXTENSION HEIGHTS**

Girder Number	Height
1W	6 <sup>5</sup> / <sub>16</sub> "
2W	6 <sup>7</sup> / <sub>16</sub> "
3W	6 <sup>9</sup> / <sub>16</sub> "

Notes:  
Prior to ordering any materials, the Contractor shall verify in the field all bearing height and shim thickness dimensions.

FILE NAME = 070\_DR095\_S-228.dgn  
PLOT DATE = 8/31/2016



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**95th Terminal Improvement**  
15 West 95th Street (South Terminal)  
Chicago, IL 60628

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CHECKED BY	P.MAID
DESIGNED BY	L.SOMMER
DRAWN BY	T.SLEDGE
PROJECT NO.	2012-0021
FILE NAME	File Name

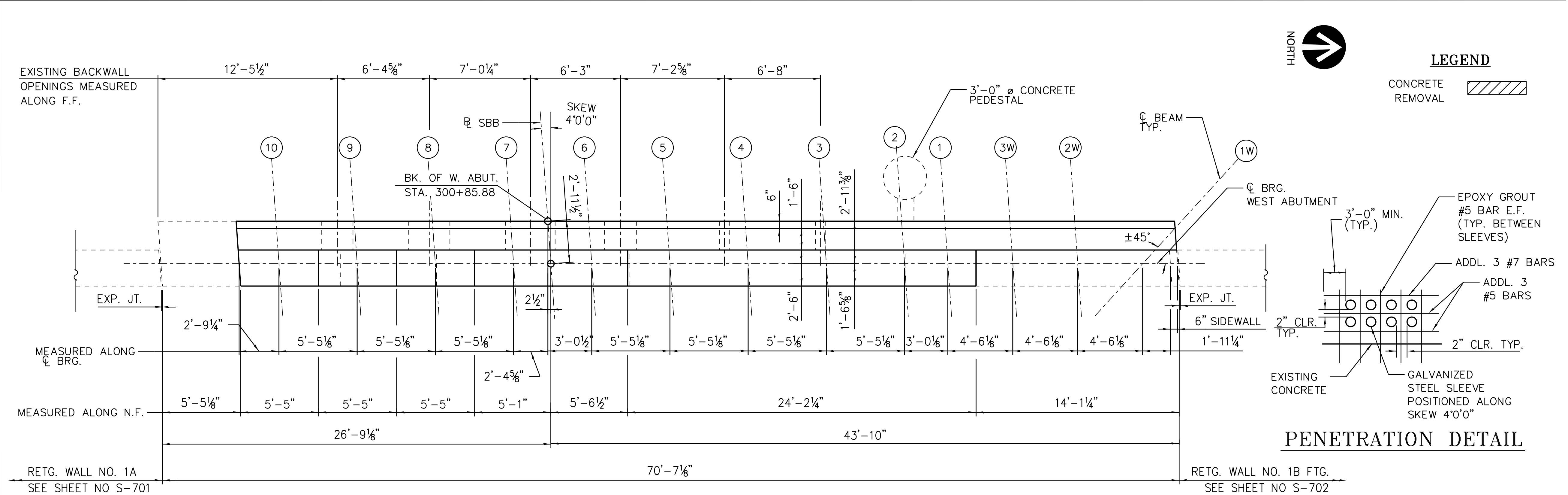
MARK	DATE	DESCRIPTION
	2/9/17	ISSUED FOR CONSTRUCTION

LOCATION IDENTIFIER: **DR095**

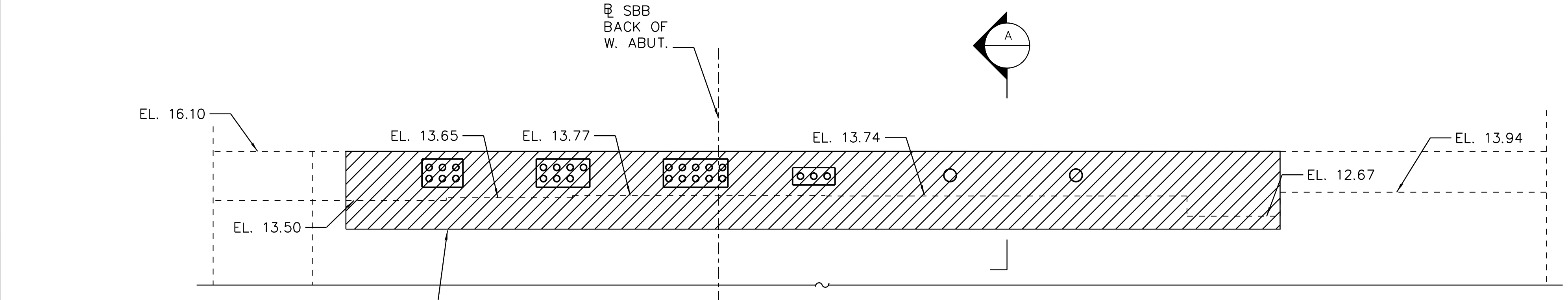
SOUTH BUS BRIDGE  
BEARING DETAILS  
(SHEET 4 OF 4)

**SBB-S-228**

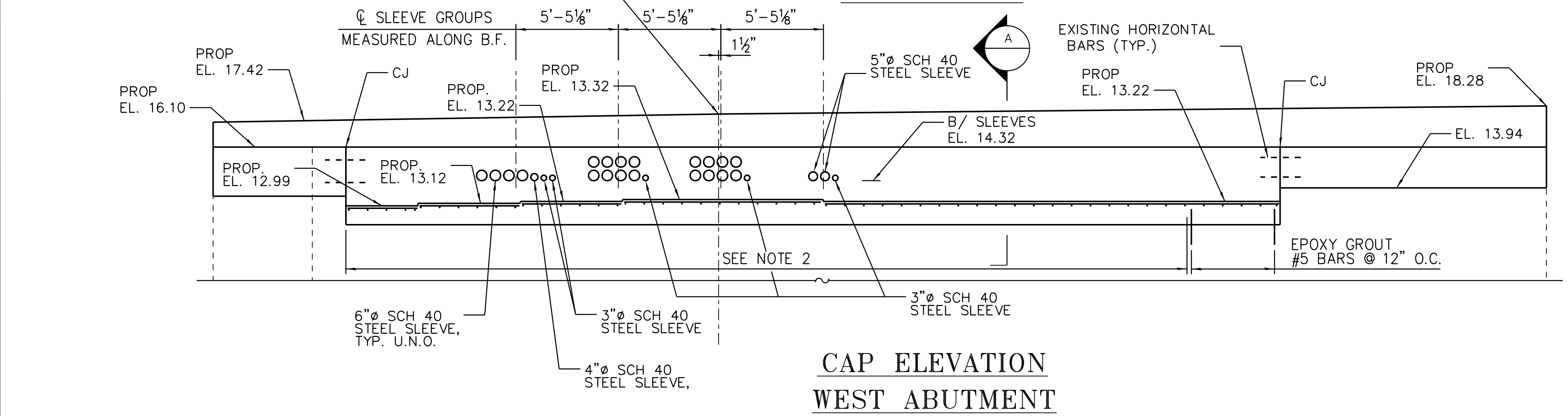
vscorpp



**CAP PLAN**



**CAP ELEVATION DEMOLITION**

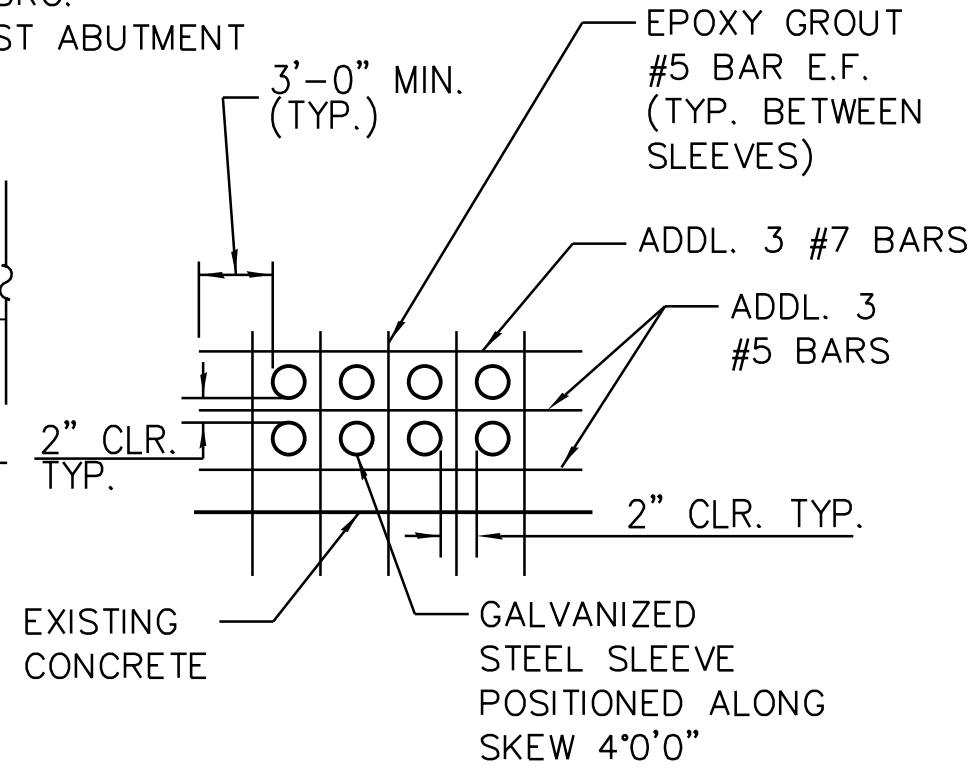


**CAP ELEVATION WEST ABUTMENT**

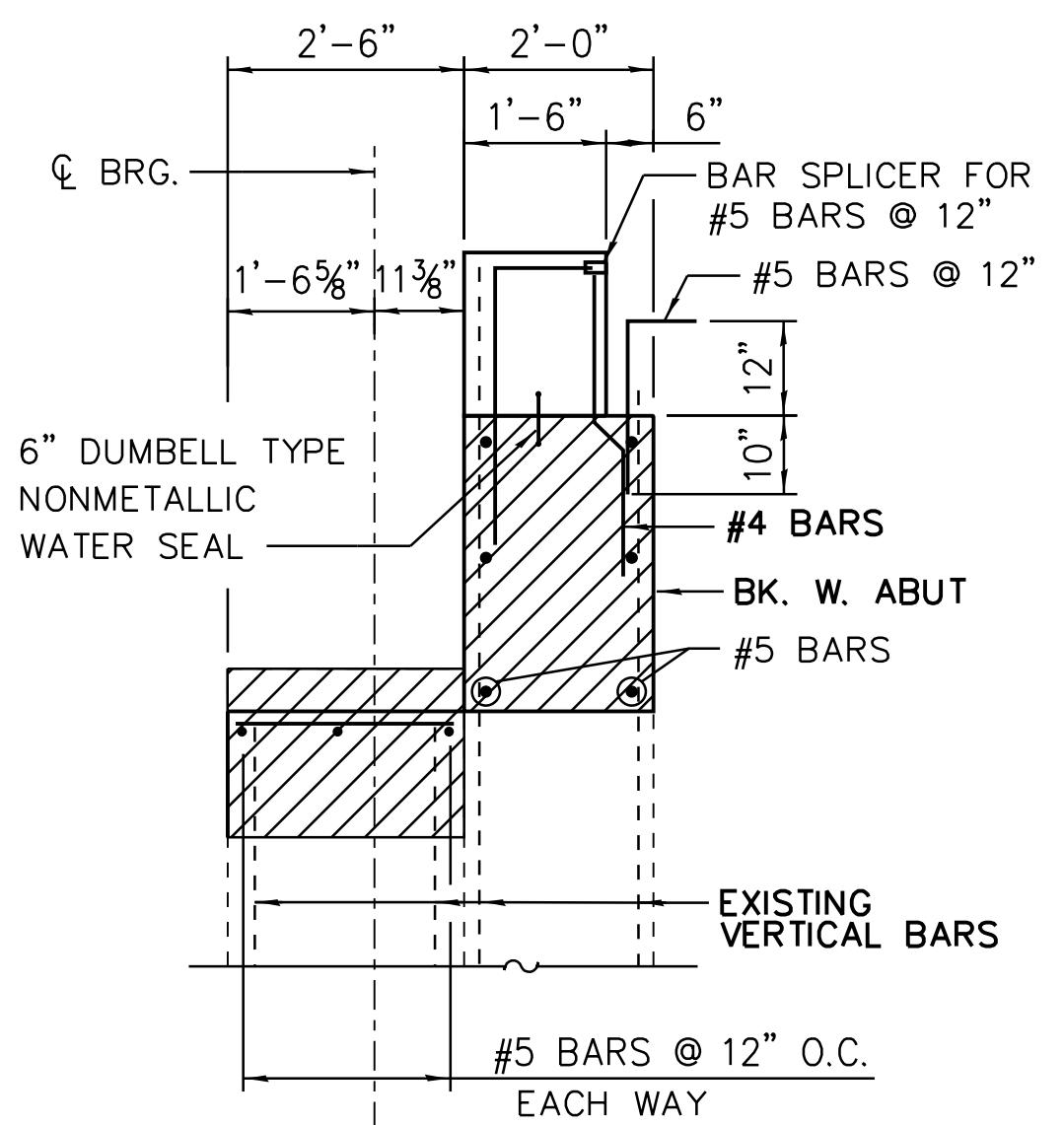


**LEGEND**

CONCRETE REMOVAL



**PENETRATION DETAIL**



**SECTION A**

- NOTES:
- EXISTING CONCRETE TO BE REMOVED TO CONCRETE REMOVAL ELEVATION.
  - EXISTING REINFORCEMENT SHALL BE CLEANED AND INCORPORATED INTO THE NEW CONSTRUCTION IN ACCORDANCE WITH IDOT STD. SPECIFICATION SECTION 501.05. COST INCLUDED WITH CONCRETE REMOVAL.
  - BEARING SEAT TO BE RECONSTRUCTED TO PROPOSED ELEVATION.
  - PROPOSED BACKWALL OPENINGS TO BE VERIFIED IN FIELD AND COORDINATED WITH ELECTRICAL AND UTILITY DRAWINGS.



**cta** CHICAGO TRANSIT AUTHORITY ENGINEERING

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**95th Terminal Improvement**  
 15 West 95th Street (South Terminal)  
 Chicago, IL 60628

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DESIGNED BY	L.SOMMER
DRAWN BY	T.SLEDGE
PROJECT NO.	2012-0021
FILE NAME	File Name

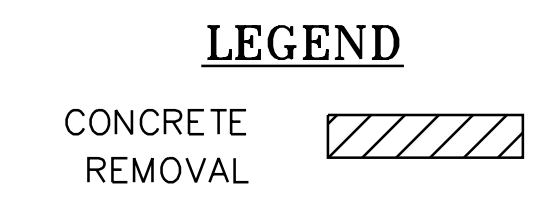
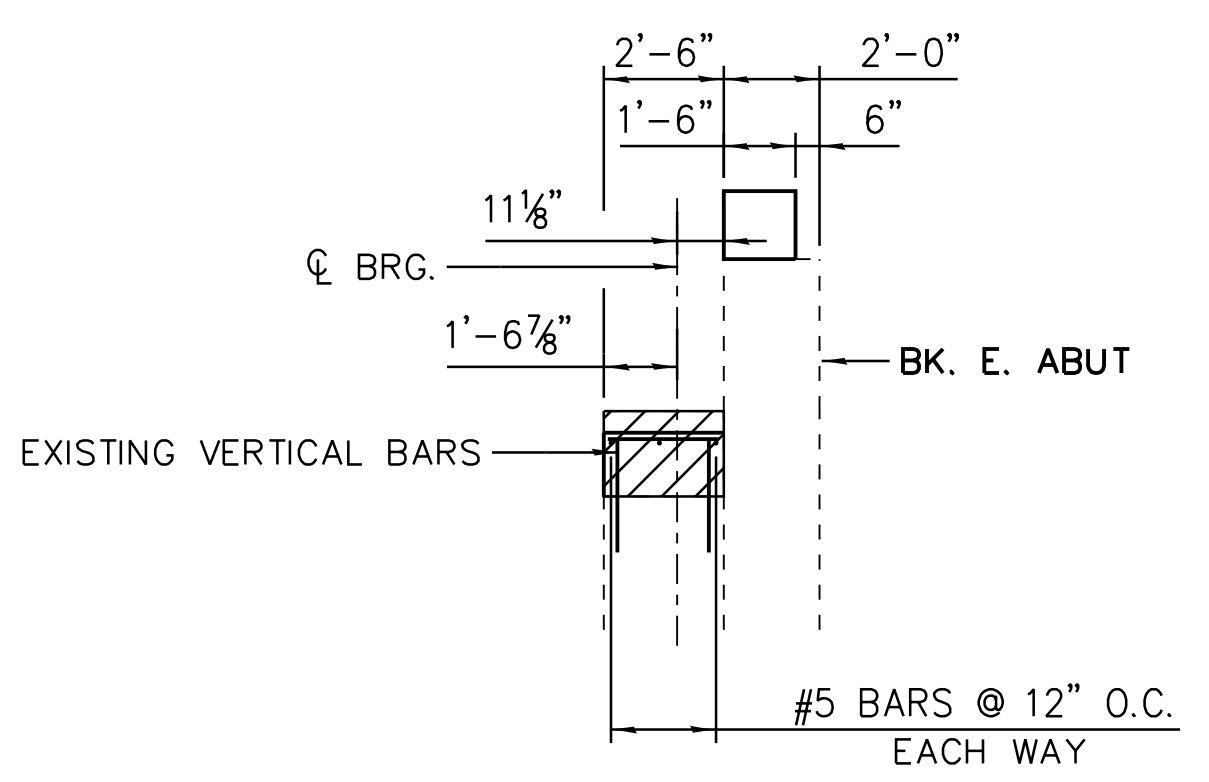
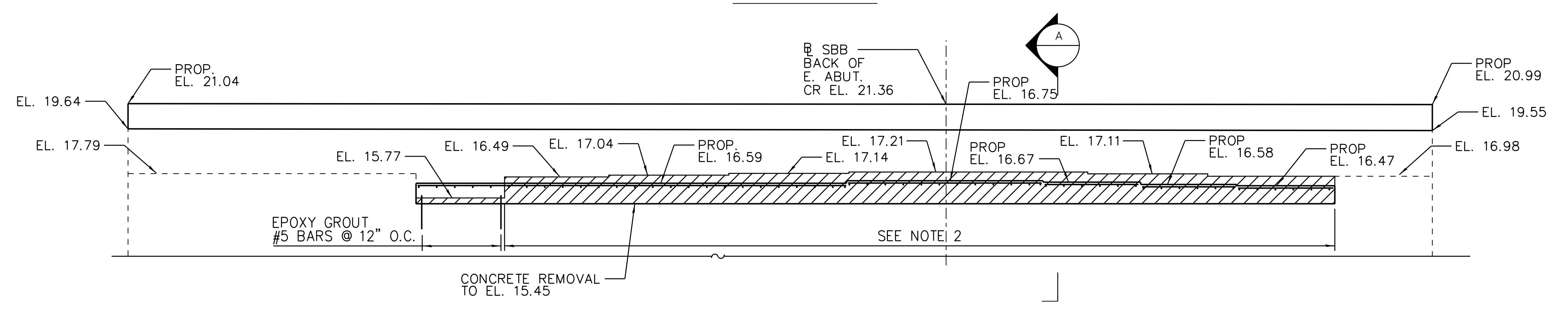
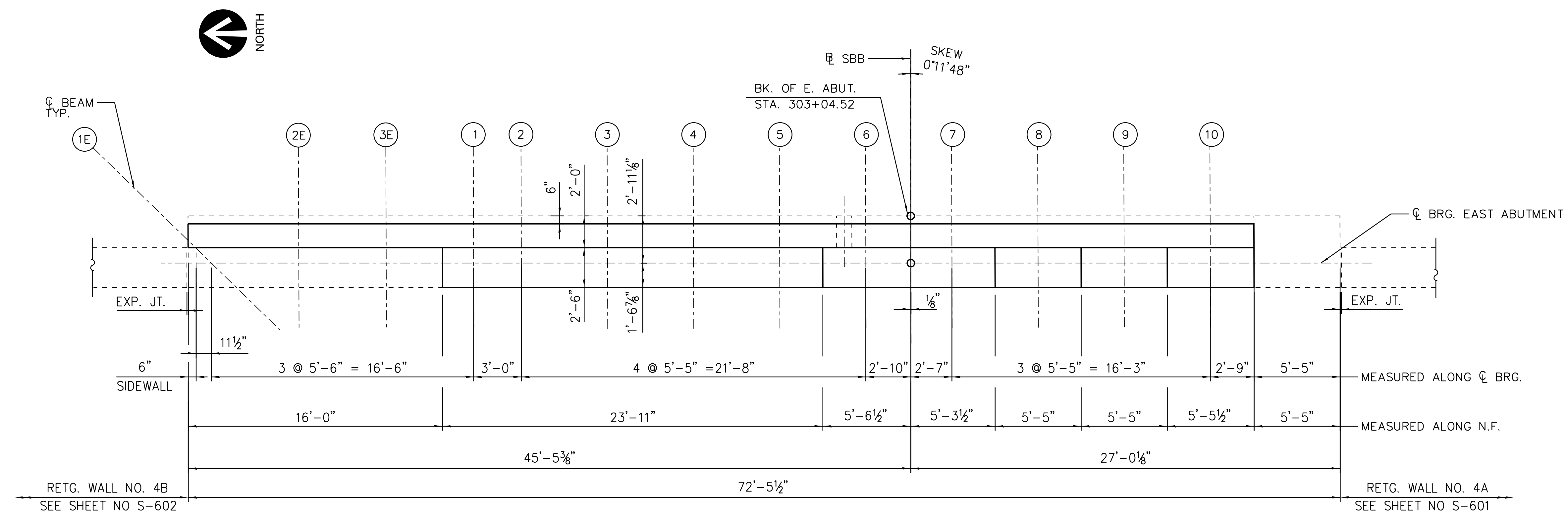
MARK	DATE	DESCRIPTION
	2/9/17	ISSUED FOR CONSTRUCTION

LOCATION IDENTIFIER: **DR095**  
 SOUTH BUS BRIDGE  
 WEST ABUTMENT  
 BEARING SEATS

**SBB-S-230A**

FILE NAME = 070\_DR095\_S-230A.dgn  
 PLOT DATE = 10/21/2016

vscorcpa



**NOTES:**

- 1-EXISTING CONCRETE TO BE REMOVED TO CONCRETE REMOVAL ELEVATION.
- 2-EXISTING REINFORCEMENT SHALL BE CLEANED AND INCORPORATED INTO THE NEW CONSTRUCTION IN ACCORDANCE WITH IDOT STD. SPECIFICATION SECTION 501.05. COST INCLUDED WITH CONCRETE REMOVAL.
- 3-BEARING SEAT TO BE RECONSTRUCTED TO PROPOSED ELEVATION.



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**95th Terminal Improvement**  
15 West 95th Street (South Terminal)  
Chicago, IL 60628

<b>IN CHARGE</b>	S. KADERBEK
<b>APPROVED BY</b>	D.ZROKA
<b>CHECKED BY</b>	P.MAID
<b>DESIGNED BY</b>	L.SOMMER
<b>DRAWN BY</b>	T.SLEDGE
<b>PROJECT NO.</b>	2012-0021
<b>FILE NAME</b>	File Name

MARK	DATE	DESCRIPTION
	2/9/17	ISSUED FOR CONSTRUCTION

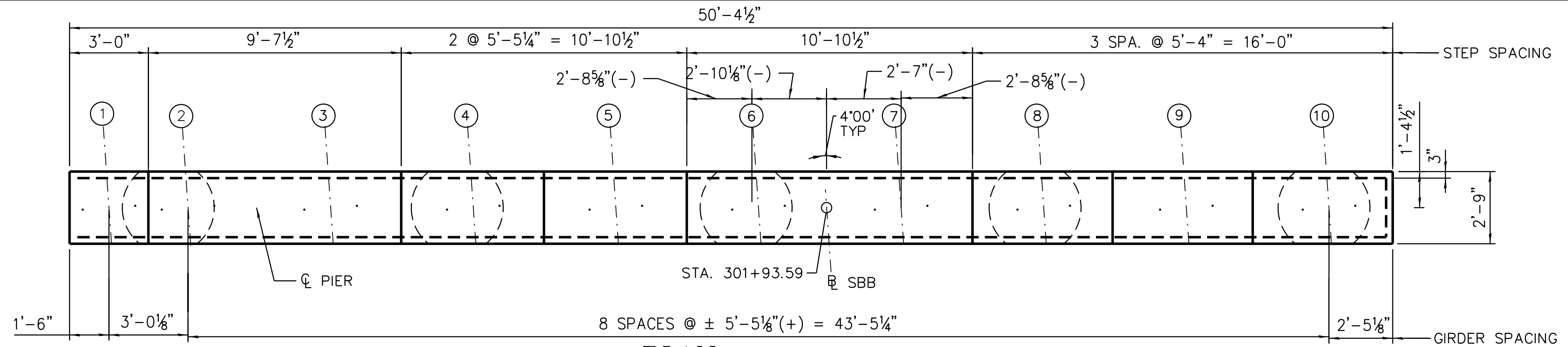
**LOCATION IDENTIFIER: DR095**

**SOUTH BUS BRIDGE EAST ABUTMENT BEARING SEATS**

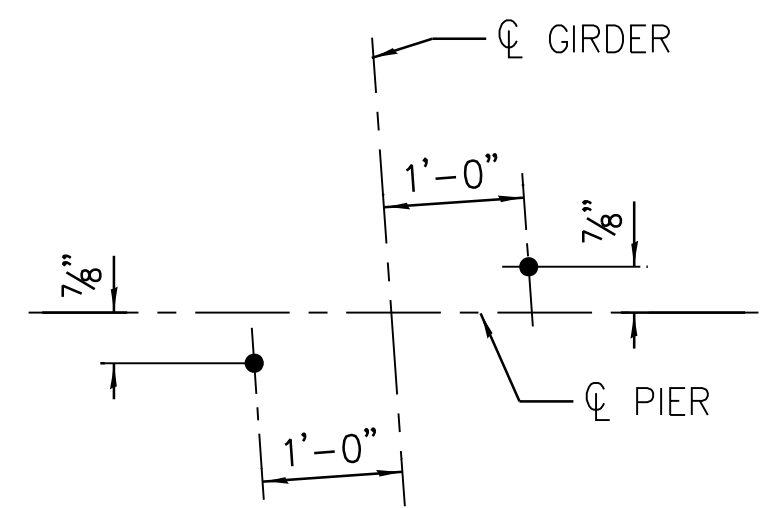
**SBB-S-231A**

FILE NAME = 070\_DR095\_S-231A.dgn  
PLOT DATE = 8/31/2016

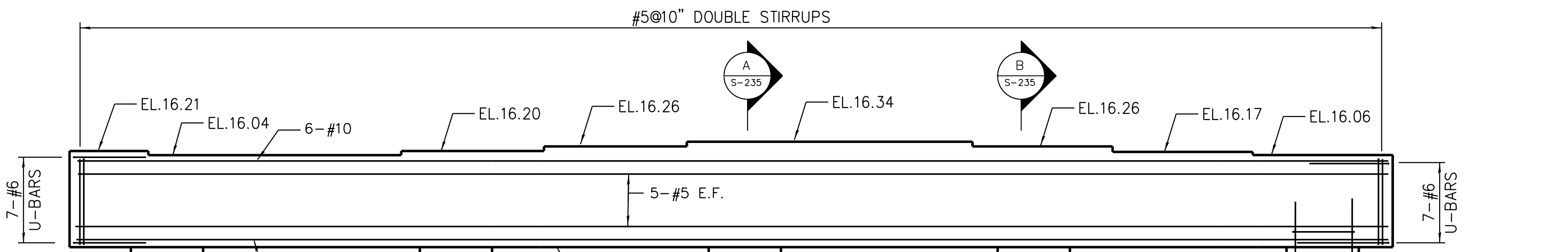
vscorp



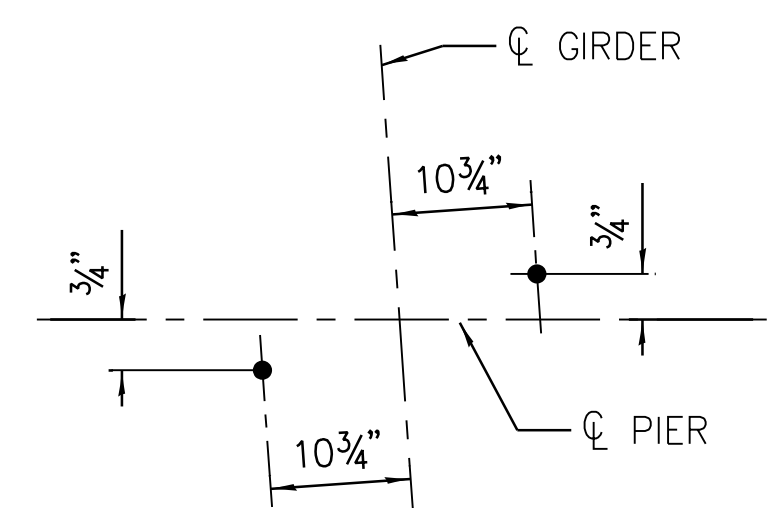
1 PLAN  
SCALE: N.T.S.



GIRDERS 2 & 3



2 ELEVATION

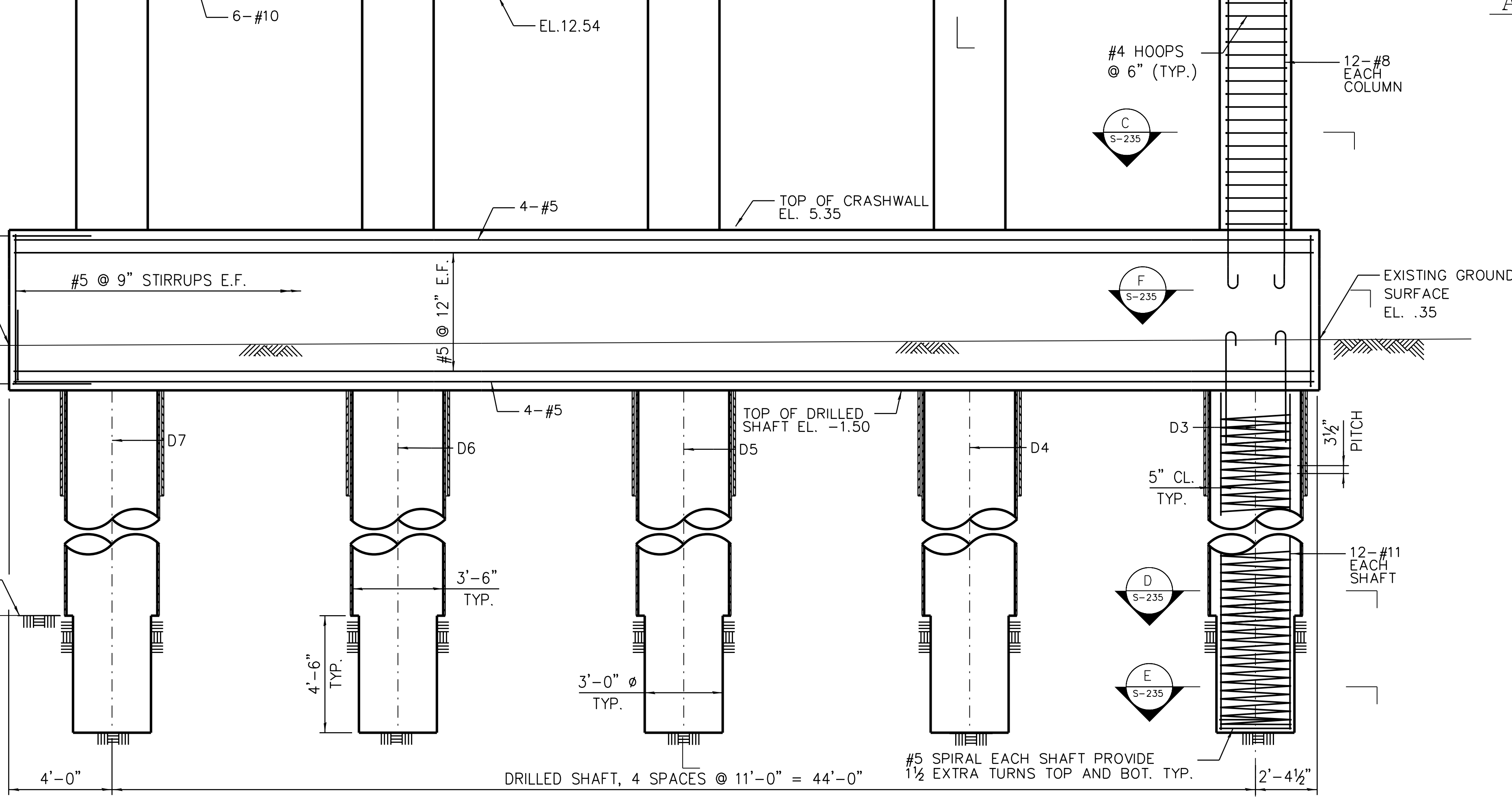


GIRDERS 1,4-10  
ANCHOR BOLT DETAIL

EXISTING GROUND SURFACE  
EL. 0.13

#5@12" U-BARS  
EACH END

EST. TOP OF COMPETENT  
ROCK EL. -39.00



DRILLED SHAFT, 4 SPACES @ 11'-0" = 44'-0"

#5 SPIRAL EACH SHAFT PROVIDE  
1 1/2 EXTRA TURNS TOP AND BOT. TYP.

CL DRILLED SHAFT (TYP.)

- NOTES:
- POUR STEPS MONOLITHICALLY WITH CAP
  - SPACE REINFORCEMENT IN CAP TO MISS ANCHOR BOLTS.

FACTORED RESISTANCE FOR DRILLED SHAFTS D3-D7				
SHAFT DIA (FT.)	MIN ROCK SOCKET LENGTH (FT.)	ROCK SOCKET DIA (FT)	FACTORED RESISTANCE (K)	ALLOWABLE BEARING PRESSURE (KSF)
3.50	4.50	3.00	2400	340



CHICAGO TRANSIT AUTHORITY ENGINEERING

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**95th Terminal Improvement**  
14 West 95th Street (North Terminal)  
15 West 95th Street (South Terminal)  
Chicago, IL 60628

IN CHARGE	S. KADERBEK
APPROVED BY	D.ZROKA
CHECKED BY	P.MAID
DESIGNED BY	L.SOMMER
DRAWN BY	T.SLEDGE
PROJECT NO.	2012-0021
FILE NAME	File Name

MARK	DATE	DESCRIPTION
	10/06/16	ISSUED FOR CONSTRUCTION

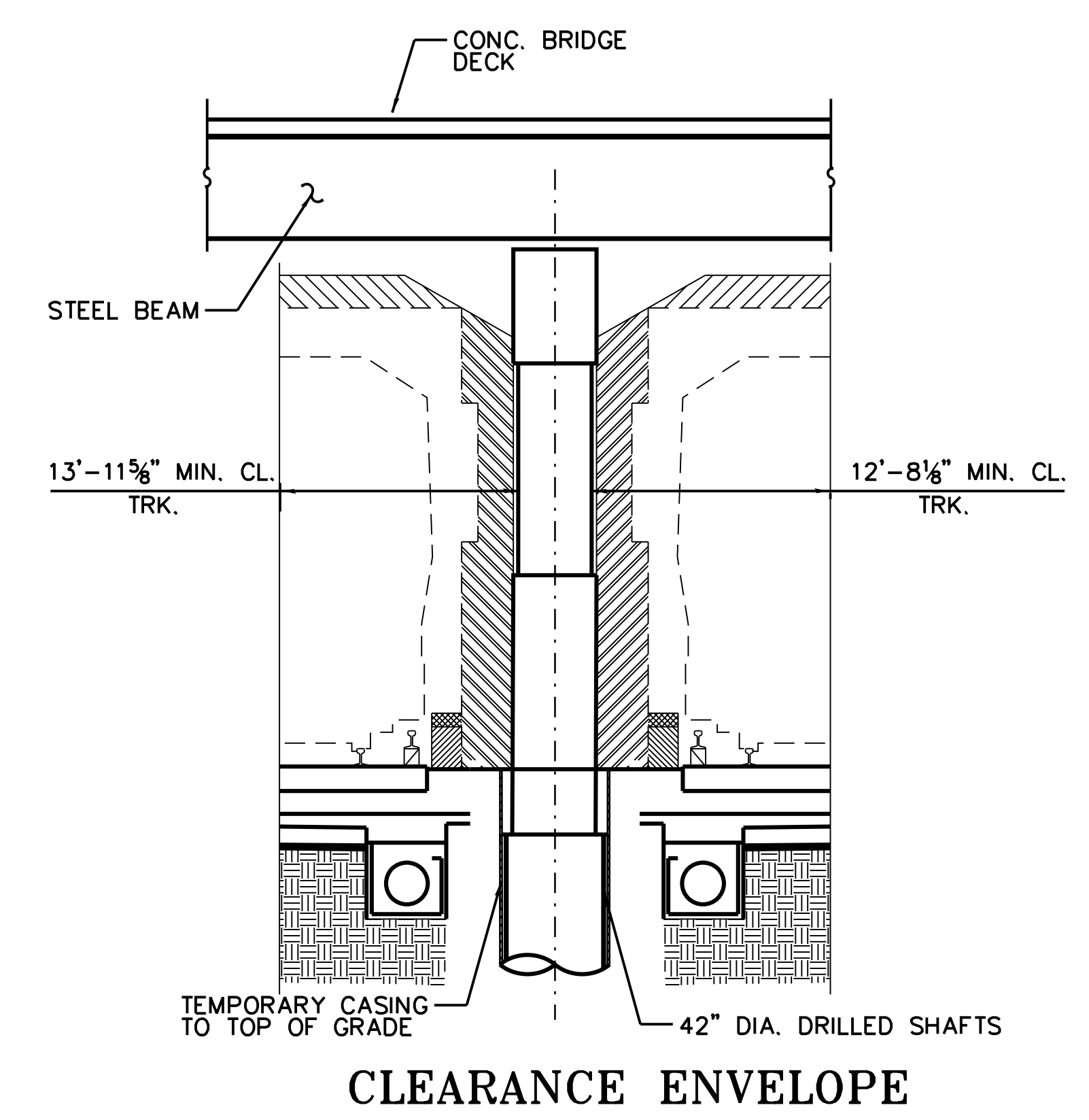
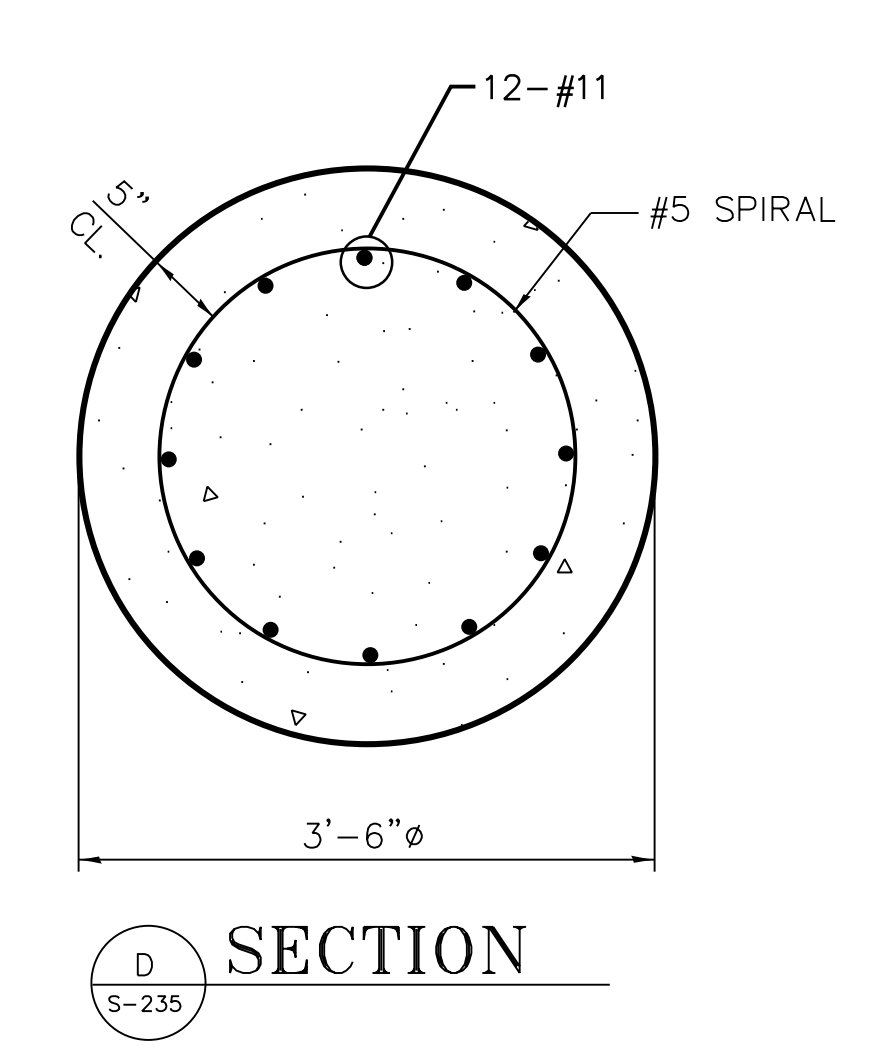
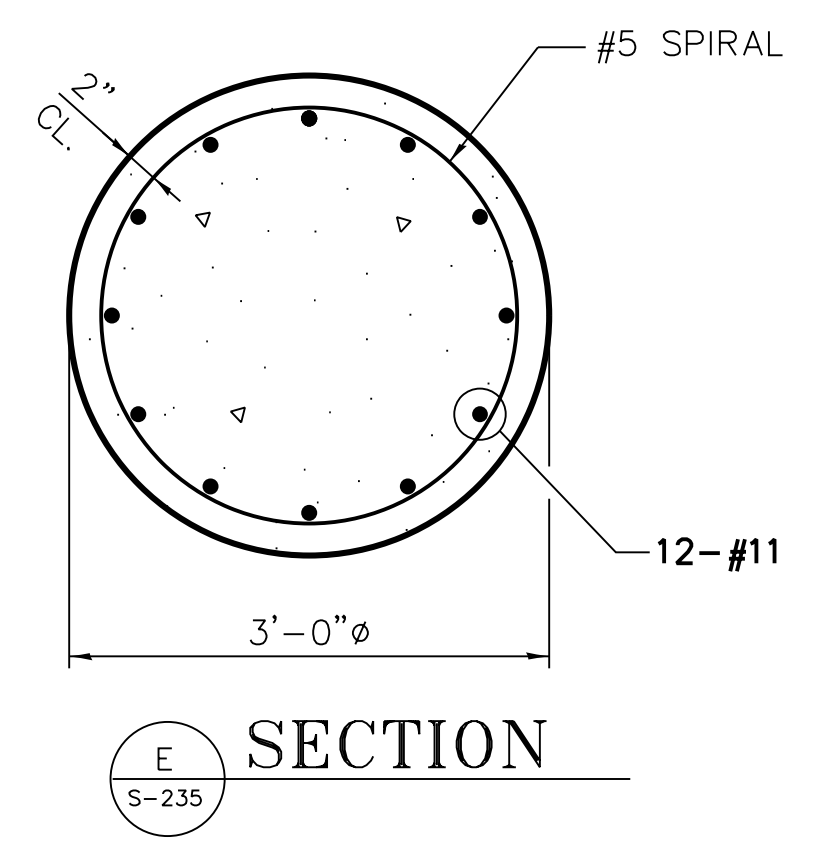
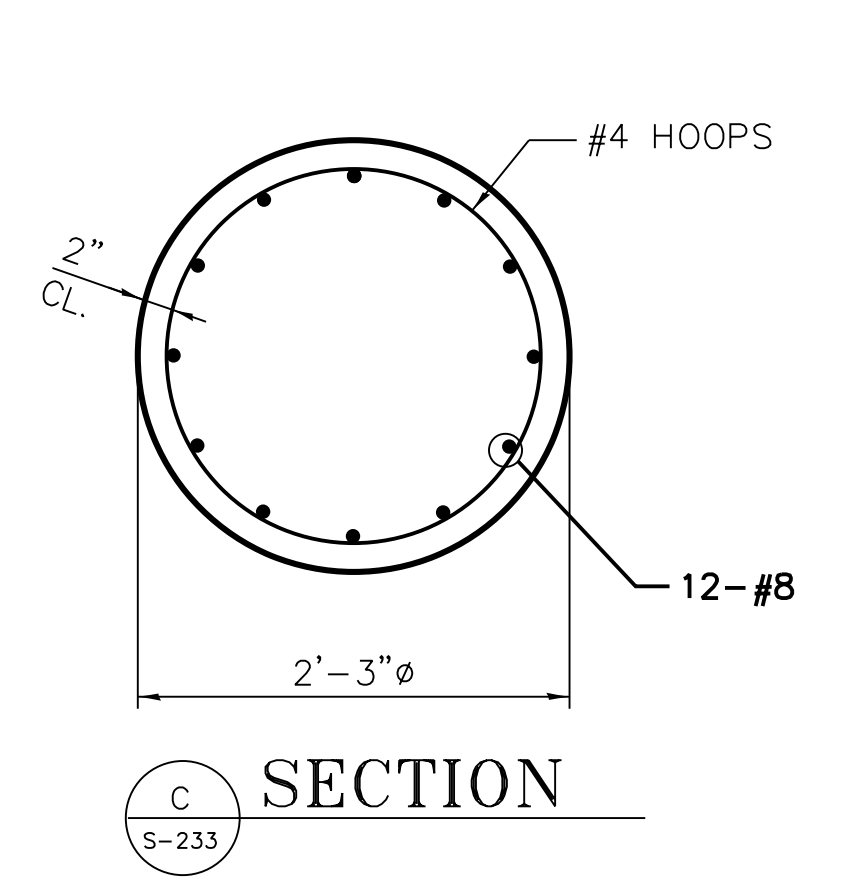
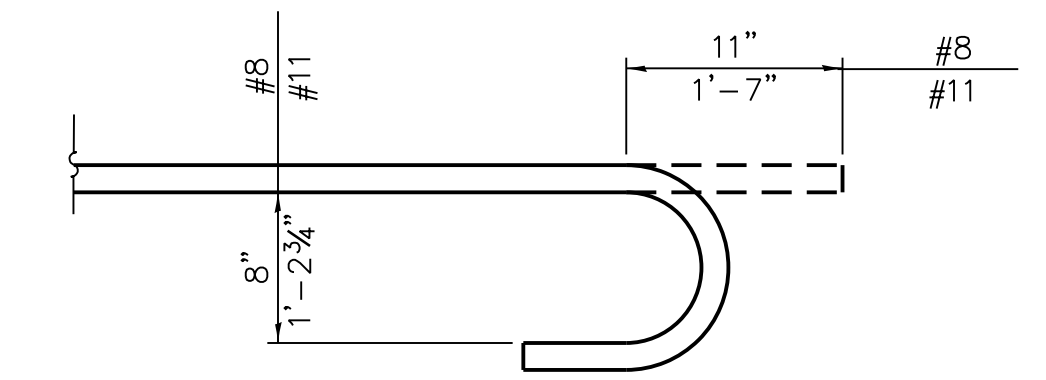
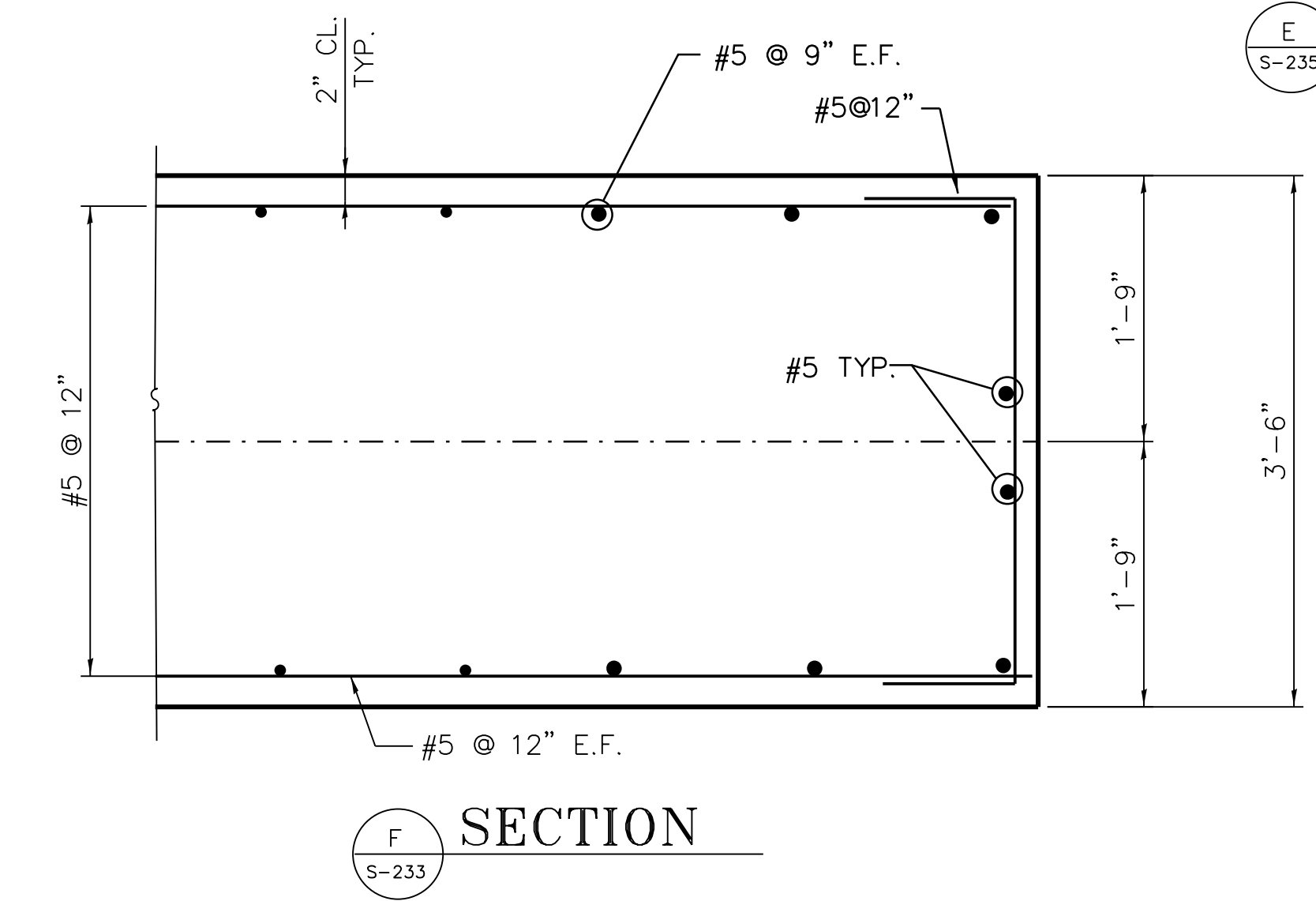
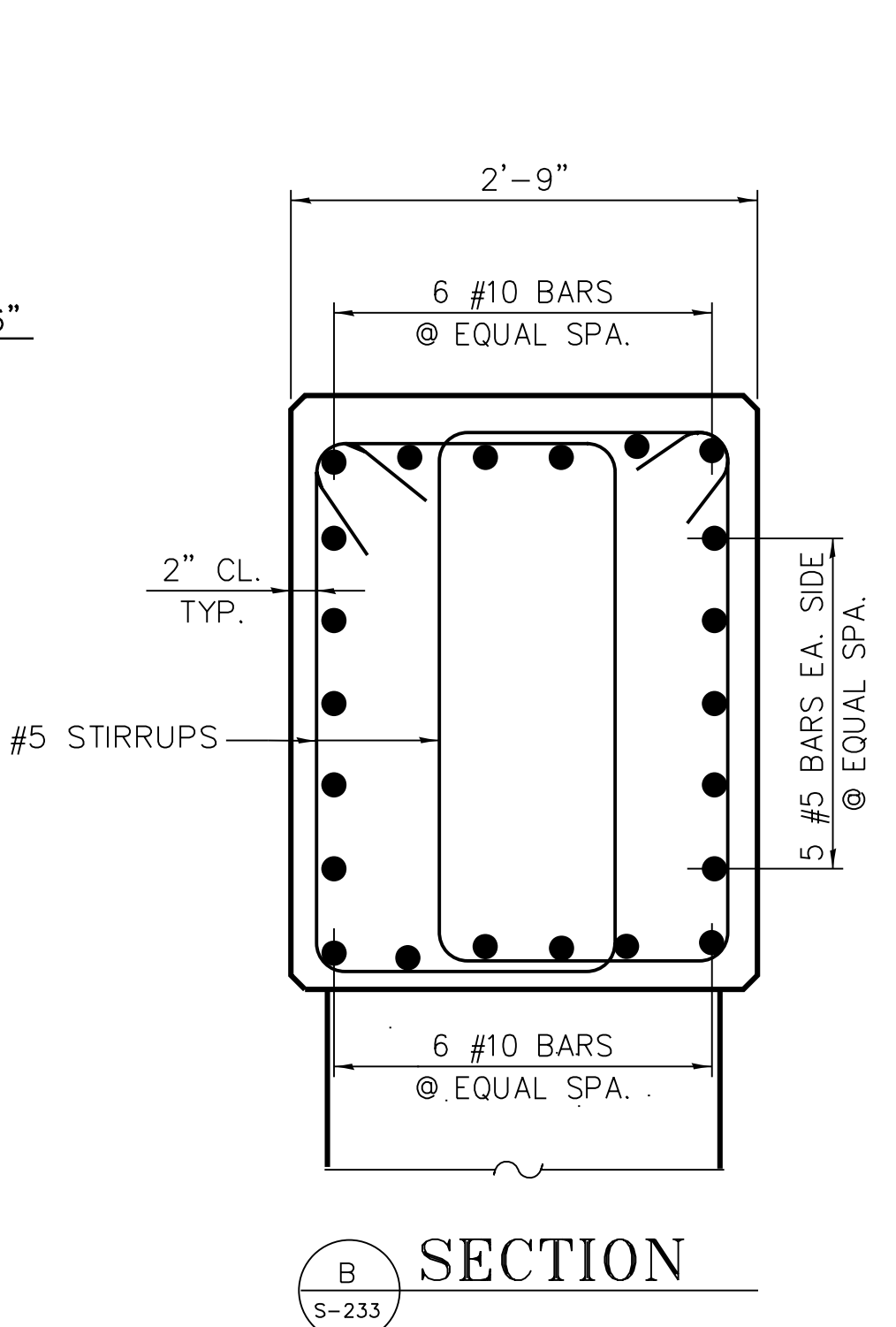
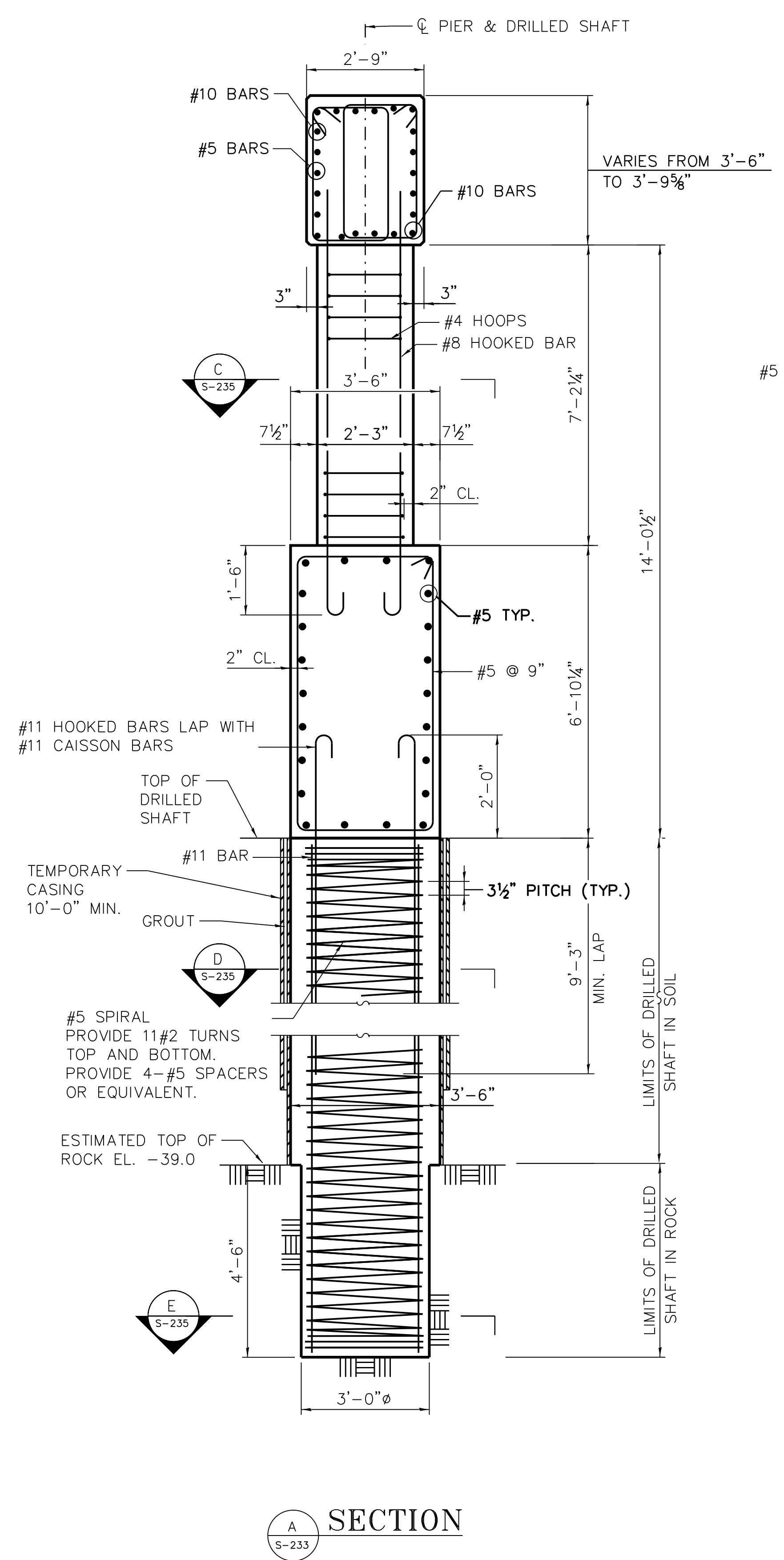
LOCATION IDENTIFIER: DR095  
SOUTH BUS BRIDGE  
PIER  
PLAN AND ELEVATION

SBB-S-233

FILE NAME = 070\_DR095\_S-233.dgn  
PLOT DATE = 10/7/2016

vscarpa

FILE NAME = 070\_DR095\_S-235.dgn  
PLOT DATE = 10/7/2016



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15 West 95th Street (South Terminal)  
Chicago, IL 60628

IN CHARGE	S. KADERBEK
APPROVED BY	D.ZROKA
CHECKED BY	P.MAID
DESIGNED BY	L.SOMMER
DRAWN BY	T.SLEDGE
PROJECT NO.	2012-0021
FILE NAME	File Name

MARK	DATE	DESCRIPTION
	10/06/16	ISSUED FOR CONSTRUCTION

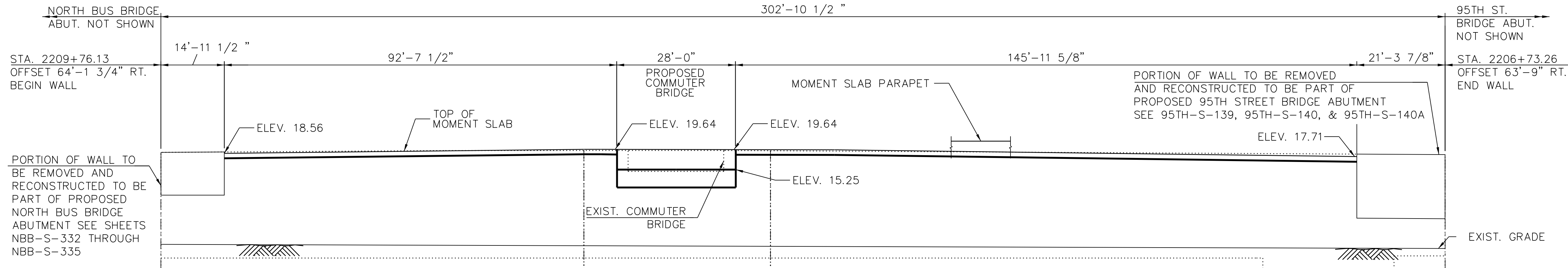
LOCATION IDENTIFIER: DR095

SOUTH BUS BRIDGE  
PIER DETAILS

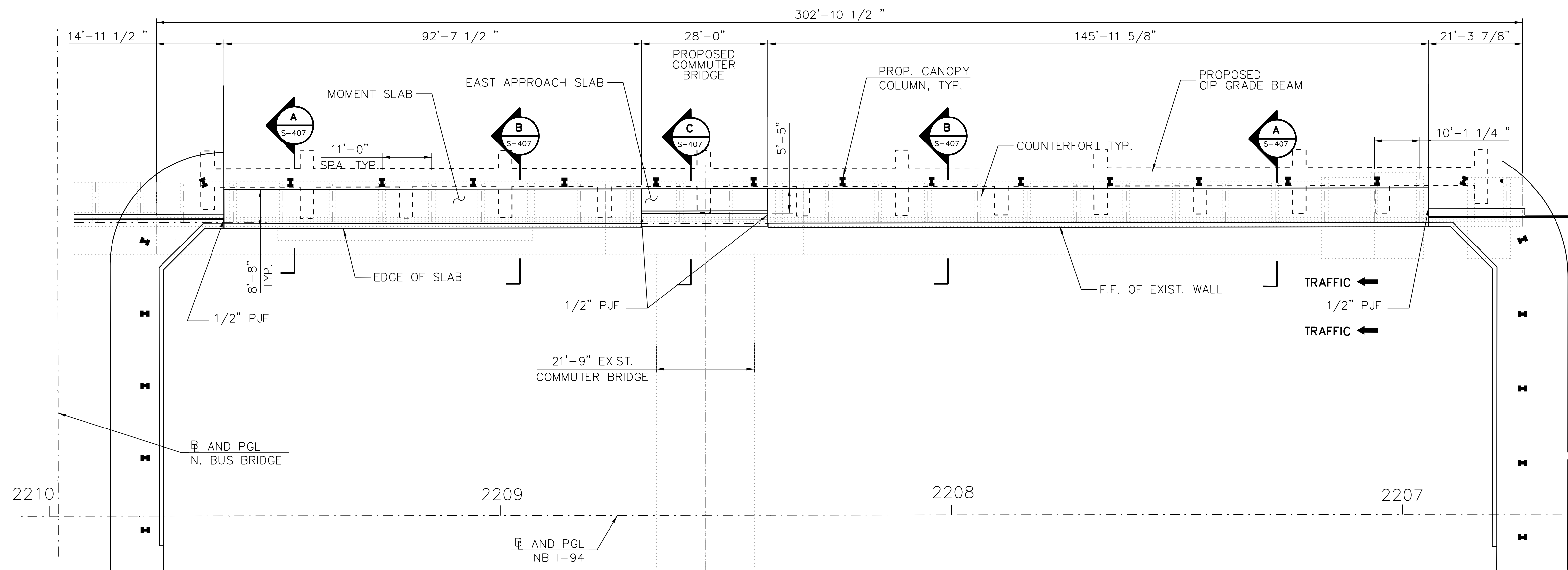
**SBB-S-235**

BENCH MARK: CITY OF CHICAGO BENCHMARK NO. 551. BENCH MONUMENT LOCATED ABOUT 50 FEET SOUTH OF THE SOUTH LINE OF W. 95TH ST., ABOUT 11 FEET EAST OF THE WEST LINE OF S. PERRY AVE. ELEVATION 16.092.

EXISTING STRUCTURE: THE EAST RETAINING WALL WAS ORIGINALLY CONSTRUCTED IN 1968 WITH A LENGTH OF 302'-10 1/2" AND EXTENDS FROM THE NORTH EDGE OF THE WEST ABUTMENT OF SOUTH 95TH ST. BRIDGE OVER F.A.I. 94 (DAN RYAN EXPRESSWAY) NORTH TO THE SOUTH EDGE OF CTA'S 95TH ST. STATION BUS BRIDGE EAST ABUTMENT. INCLUDED WITHIN THE LIMITS OF WALL IS THE EAST ABUTMENT FOR CTA'S 95TH ST. STATION COMMUTER BRIDGE. THE RETAINING WALL IS A REINFORCED CONCRETE "T" TYPE WALL ON SPREAD FOOTING WITH BASE KEY AND COUNTERFORTS. THE EXPOSED HEIGHT OF WALL VARIES FROM APPROXIMATELY 22'-2" AT THE SOUTH END TO 21'-8" AT THE NORTH END. THE STEM WIDTH AT THE VERY TOP OF WALL IS 2'-0" INCLUDING A 6" SHELF FOR SUPPORTING THE ADJACENT SIDEWALK. COUNTERFORTS ARE TYPICALLY SPACED AT 11'-0" CENTERS AND ARE 1'-6" THICK. PORTION OF THE EXISTING EAST RETAINING WALL WILL BE REMOVED AND RECONSTRUCTED TO SUPPORT THE WIDENED EAST ABUTMENT OF CTA'S 95TH ST. NORTH BUS BRIDGE, THE EAST ABUTMENT OF THE NEW NORTH COMMUTER BRIDGE AND NEW MOMENT SLABS.



**1 ELEVATION**  
SCALE: N.T.S.



**2 PLAN**  
SCALE: N.T.S.

**EXISTING WALL DESIGN SPECIFICATIONS**  
2002 AASHTO STANDARD SPECIFICATIONS FOR HIGHWAY BRIDGES, 17TH EDITION

**EXISTING WALL DESIGN STRESSES**  
FIELD UNITS  
F<sub>C</sub> = 3,500 PSI  
F<sub>Y</sub> = 60,000 PSI (REINFORCEMENT)

**ORIGINAL CONSTRUCTION (1968)**  
AMERICAN ASSOCIATION OF STATE HIGHWAY OFFICIALS STANDARD SPECIFICATIONS FOR HIGHWAY BRIDGES, 9 EDITION, DATED 1965

EXISTING UNITS  
F<sub>C</sub> = 4,000 PSI  
F<sub>Y</sub> = 40,000 PSI (REINFORCEMENT)  
F<sub>Y</sub> = 36,000 PSI (STRUCTURAL STEEL)

**SEISMIC DATA**  
SEISMIC PERFORMANCE CATEGORY (SPC)=A  
HORIZONTAL BEDROCK ACCELERATION COEFFICIENT (A)=0.04 G  
SITE COEFFICIENT = 1.0

- NOTES:**
- FOR PROPOSED CANOPY FOUNDATIONS AND GRADE BEAM, SEE S-1300 SERIES.
  - FOR PROFILE GRADE LINES, SEE SHEET NBB-S-302.
  - THE ENTIRE EXPOSED FACE OF WALLS SHALL HAVE WATERPROOF COATING APPLIED AS PER STRUCTURAL REPAIR OF CONCRETE SECTION 03 74 00.
  - FOR MOMENT SLAB DETAILS, SEE S-1403A.



**SENSITIVE SECURITY INFORMATION**  
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**DRAWING SCALE IS NOT GUARANTEED. CTA ASSUMES NO RISK OF LIABILITY FOR ERRORS CAUSED, DIRECTLY OR INDIRECTLY, BY SCALING OF THIS DRAWING.**

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CHECKED BY	P.MAID
DESIGNED BY	L.SOMMER
DRAWN BY	T.SLEDGE
PROJECT NO.	2012-0021
FILE NAME	File Name

MARK	DATE	DESCRIPTION
	2/9/17	ISSUED FOR CONSTRUCTION

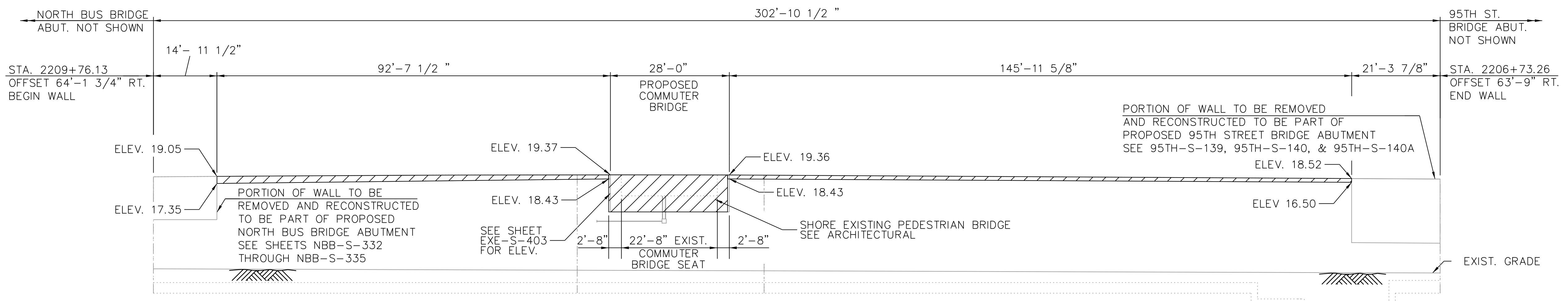
LOCATION IDENTIFIER: **DR095**

EXISTING EAST RETAINING WALL  
GENERAL PLAN AND ELEVATION

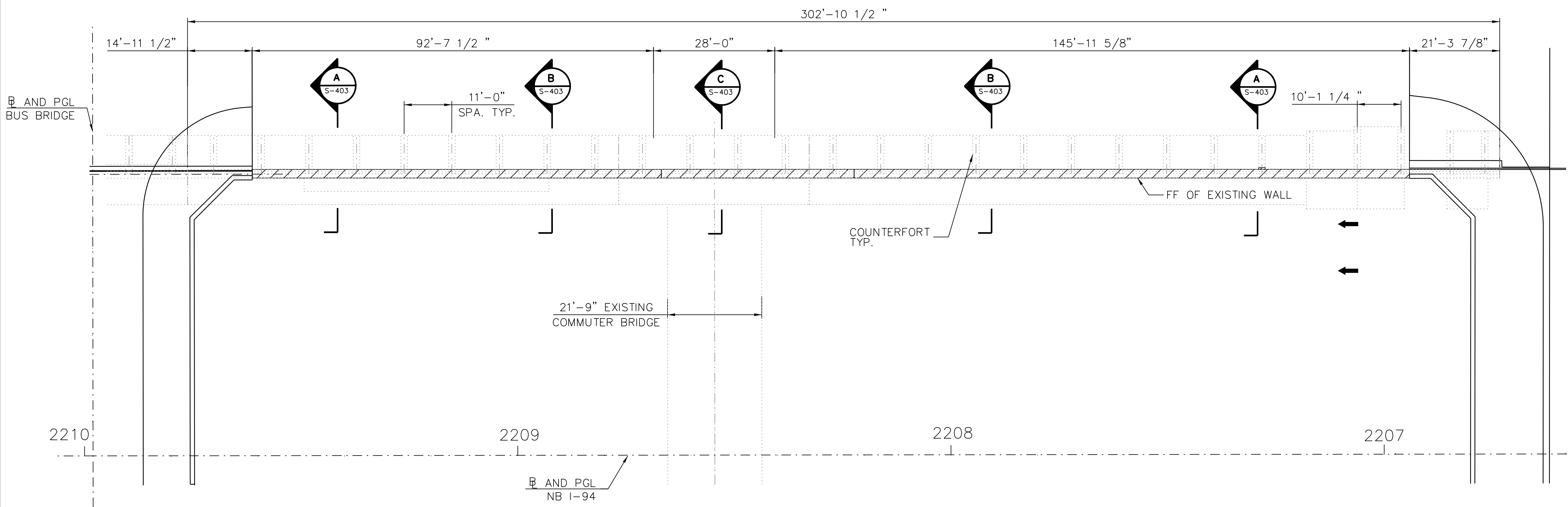
**EXE-S-401**

FILE NAME = 070\_DR095\_S-401.dgn  
PLOT DATE = 12/14/2016

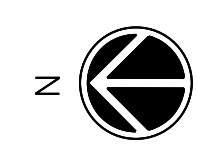
vscorppa



1 **ELEVATION**  
SCALE: N.T.S.



2 **PLAN**  
SCALE: N.T.S.



LEGEND:

CONCRETE REMOVAL

NOTE:

- FOR NOTES, SEE SHEET EXE-S-401.
- FOR SIDEWALK REMOVAL, SEE SHEET C-102.



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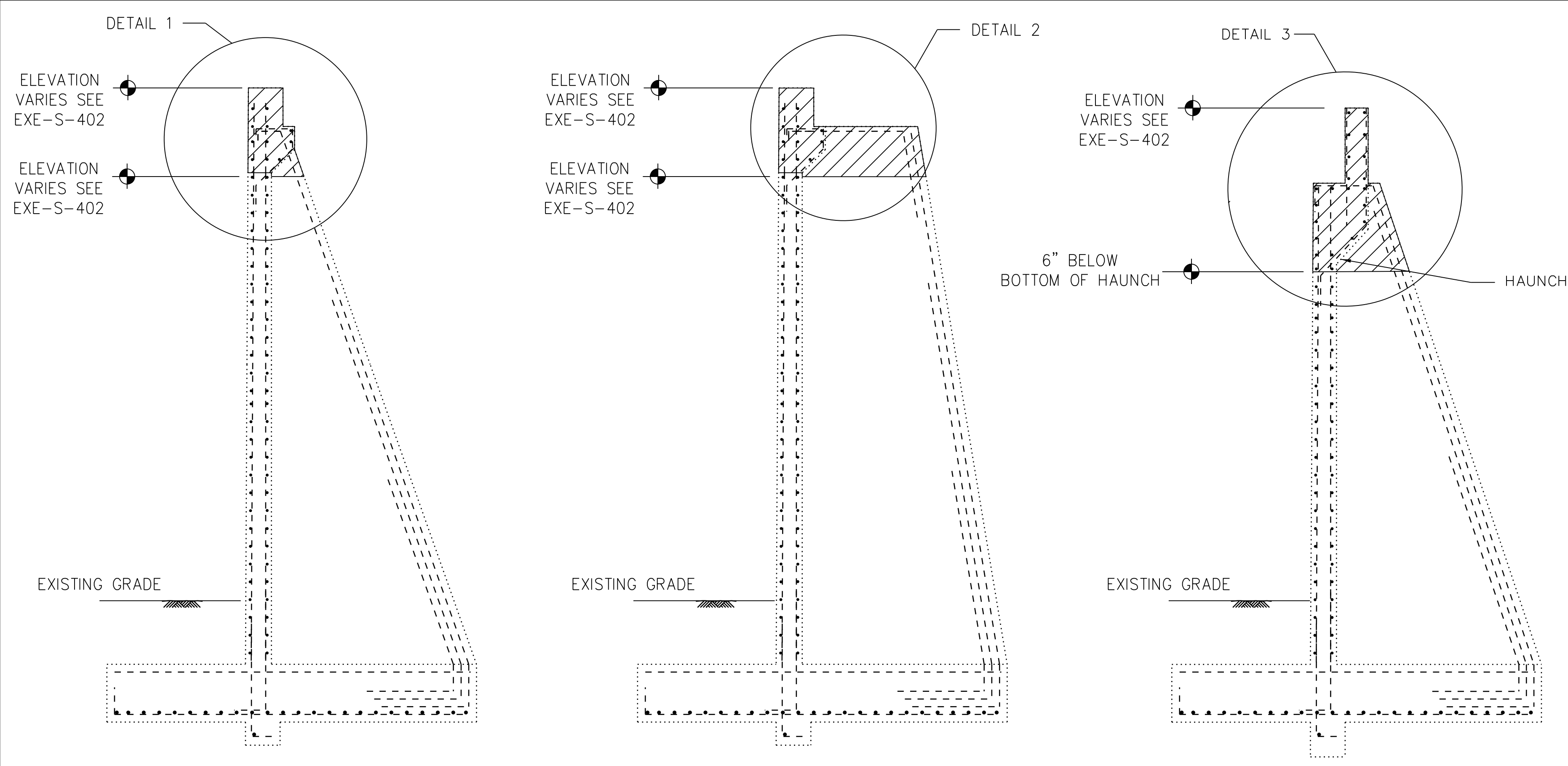
LOCATION IDENTIFIER: **DR095**

EXISTING EAST RETAINING WALL  
REMOVAL PLAN AND ELEVATION

**EXE-S-402**

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PLOT DATE = 12/14/2016

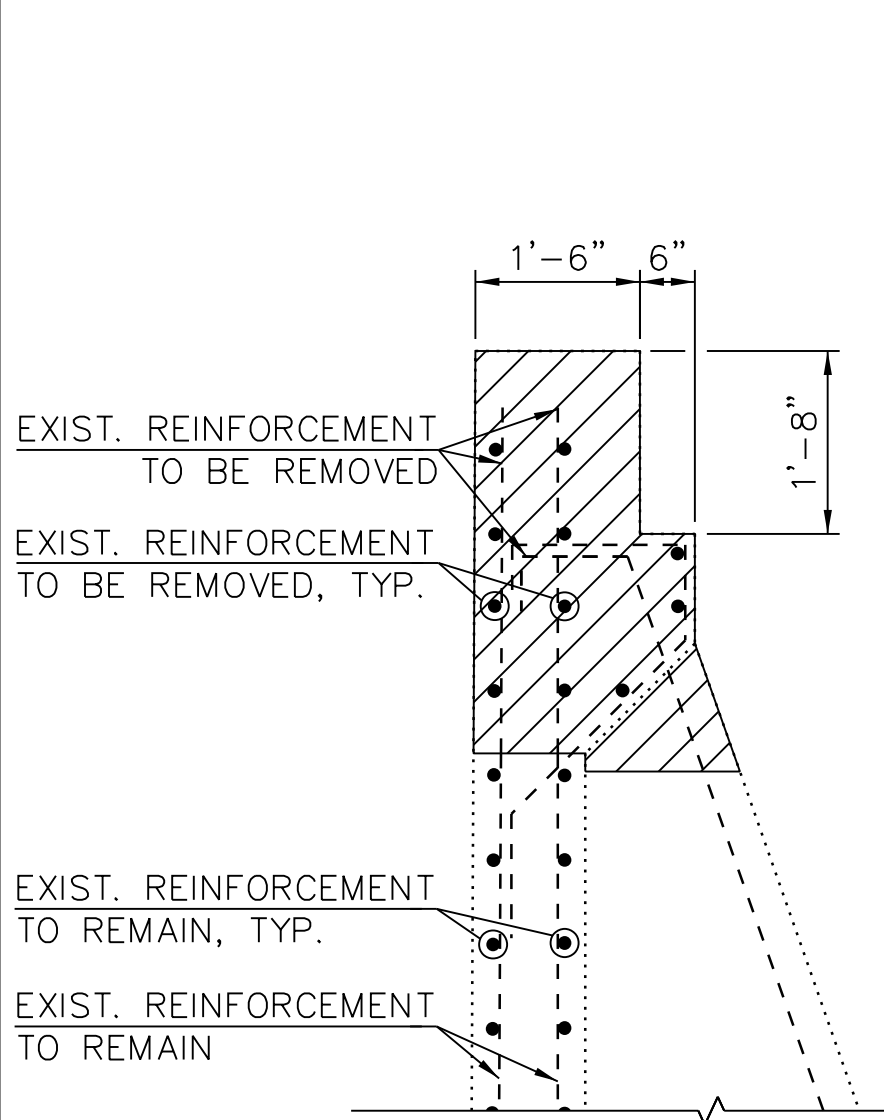
vscorppa



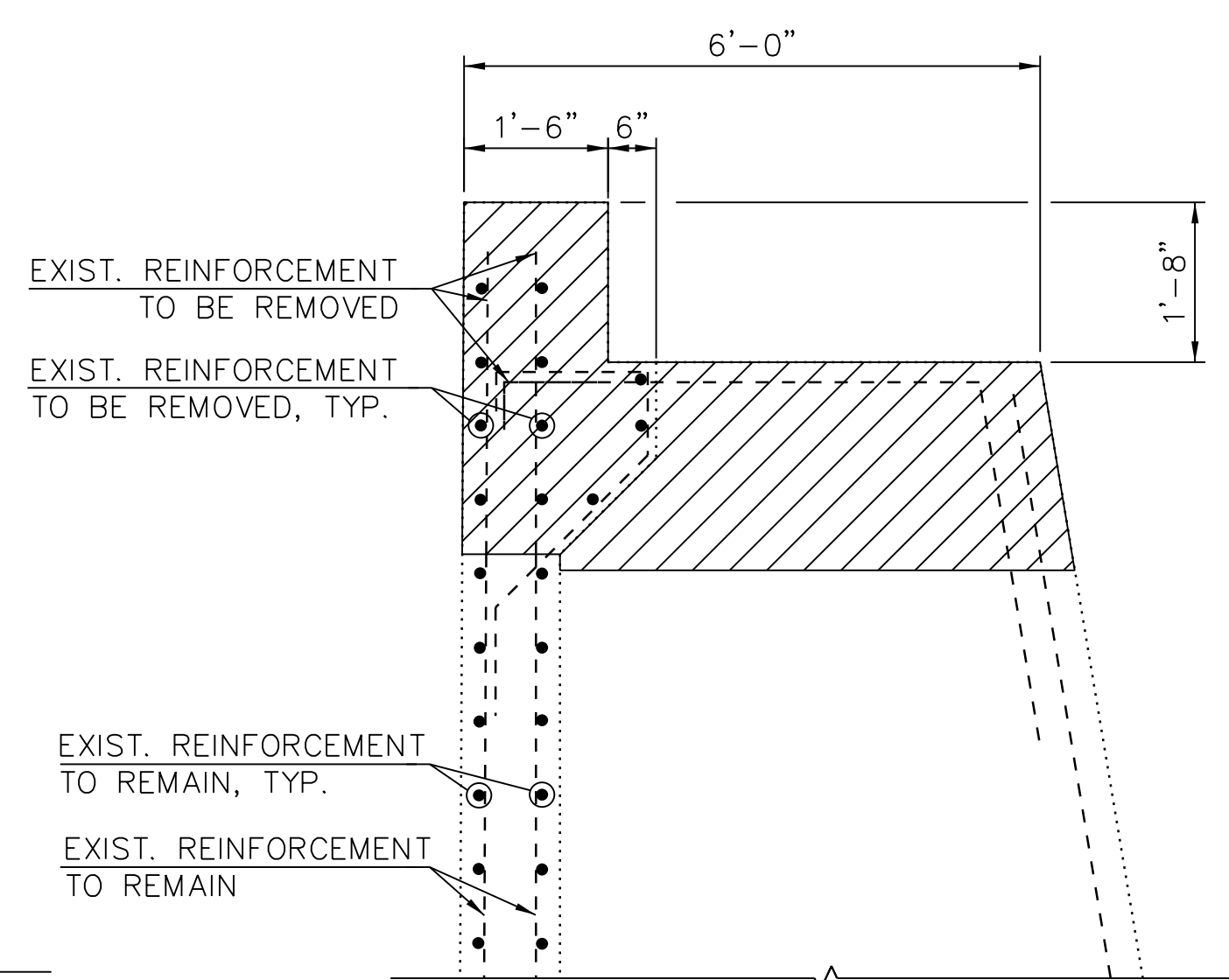
**A SECTION A**  
SCALE: N.T.S.

**B SECTION B**  
SCALE: N.T.S.

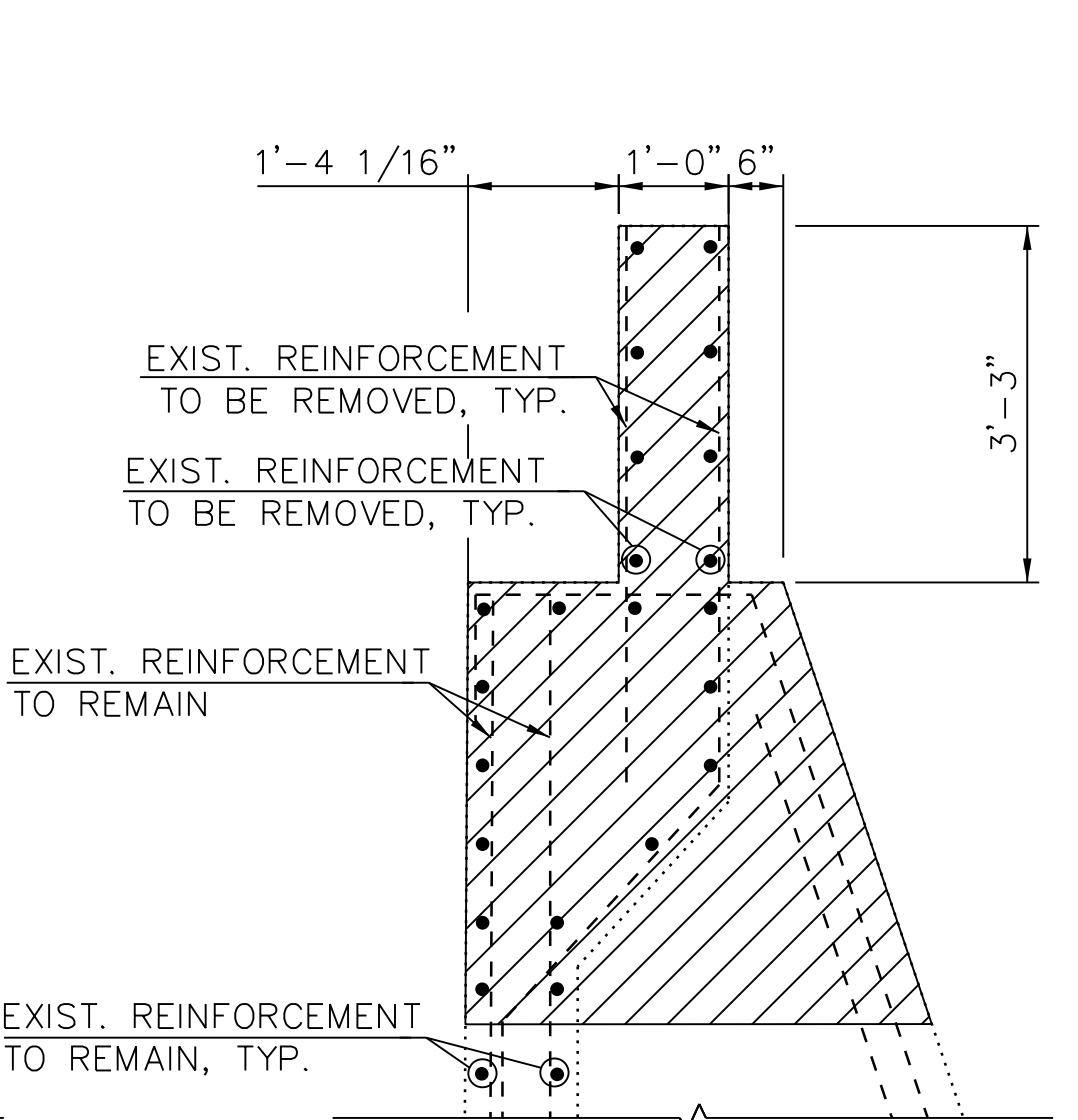
**C SECTION C**  
(AT COMMUTER BRIDGE)  
SCALE: N.T.S.



**1 DETAIL 1**  
SCALE: N.T.S.



**2 DETAIL 2**  
SCALE: N.T.S.



**3 DETAIL 3**  
SCALE: N.T.S.

**NOTES**

- EXISTING REINFORCEMENT EXTENDED INTO NEW CONCRETE SHALL BE CLEANED, STRAIGHTENED, AND INCORPORATED INTO THE NEW CONSTRUCTION.
- ANY REINFORCEMENT BARS THAT ARE DAMAGED DURING CONCRETE REMOVAL OPERATIONS SHALL BE REPAIRED OR REPLACED USING AN APPROVED BAR SPLICER OR ANCHORAGE SYSTEM.
- SAWCUT WALL TO PROVIDE CLEAN BEARING SURFACE FOR MOMENT SLABS. DAMAGED CONCRETE TO BE REPAIRED WITH NON-SHRINK GROUT AT NO ADDITIONAL COST.

**LEGEND:**



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<b>DRAWN BY</b>	T.SLEDGE
<b>PROJECT NO.</b>	2012-0021
<b>FILE NAME</b>	File Name

MARK	DATE	DESCRIPTION
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LOCATION IDENTIFIER: **DR095**

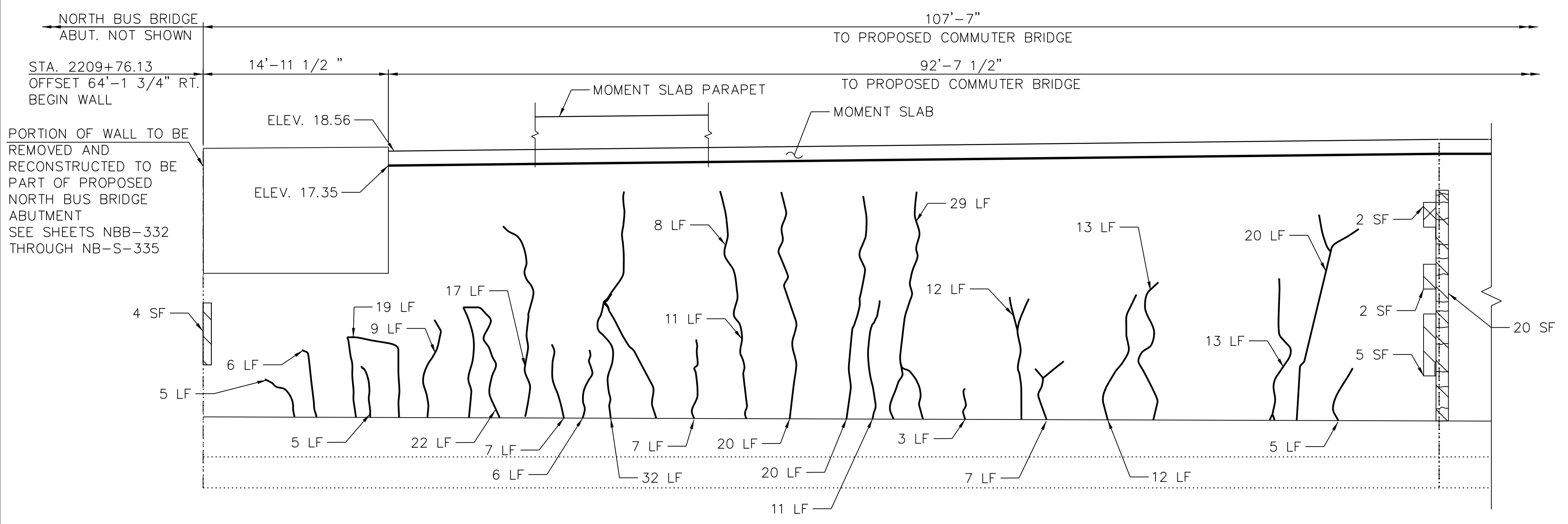
EXISTING EAST RETAINING WALL  
REMOVAL SECTIONS & DETAILS

**EXE-S-403**

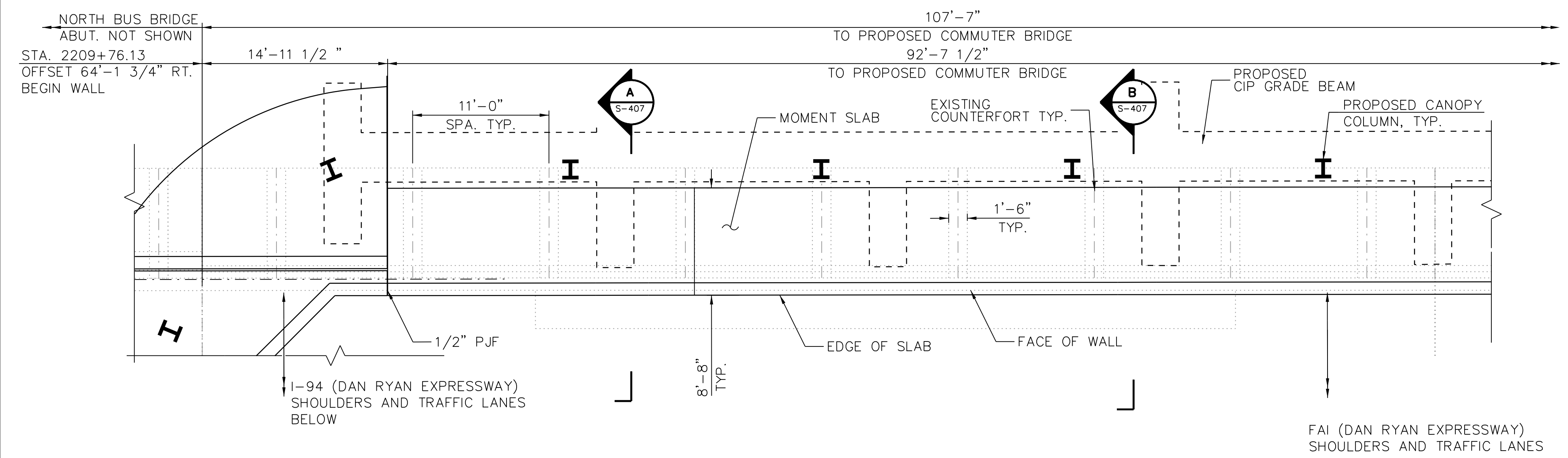
FILE NAME = 070\_DR095\_S-403.dgn  
PLOT DATE = 12/14/2016



vscarp



**1 EXISTING ELEVATION OF EAST RETAINING WALL**  
(LOOKING EAST)  
SCALE: N.T.S.



**2 EXISTING EAST RETAINING WALL PLAN**  
SCALE: N.T.S.

**NOTES:**

- FOR PORTION OF WALL TO BE REMOVED AND RECONSTRUCTED TO BE PART OF PROPOSED NORTH BUS BRIDGE ABUTMENT, SEE NORTH BUS BRIDGE PLANS.
- FOR PORTION OF WALL TO BE REMOVED AND RECONSTRUCTED TO BE PART OF PROPOSED COMMUTER BRIDGE ABUTMENT, SEE SHEETS EXE-S-405 AND EXE-S-407.
- FOR STRUCTURAL REPAIR OF CONCRETE DETAILS, SEE SHEET 95TH-S-150A
- FOR EXPANSION JOINT DETAILS, SEE SHEET 95TH-S-122.
- THE ENTIRE EXPOSED FACE OF WALLS SHALL HAVE WATERPROOF COATING APPLIED AS PER STRUCTURAL REPAIR OF CONCRETE SECTION 03 74 00.
- FOR PROPOSED CANOPY FOUNDATIONS AND GRADE BEAMS, SEE S-1300 SERIES.
- FOR MOMENT SLAB DETAILS, SEE S-1403A.

**LEGEND:**

- STRUCTURAL REPAIR OF CONCRETE (DEPTH GREATER THAN 5 INCHES)
- STRUCTURAL REPAIR OF CONCRETE (DEPTH EQUAL TO OR LESS THAN 5 INCHES)
- EPOXY SEAL (WIDTH >0.06")
- SF - SQUARE FOOT
- LF - LINEAR FOOT



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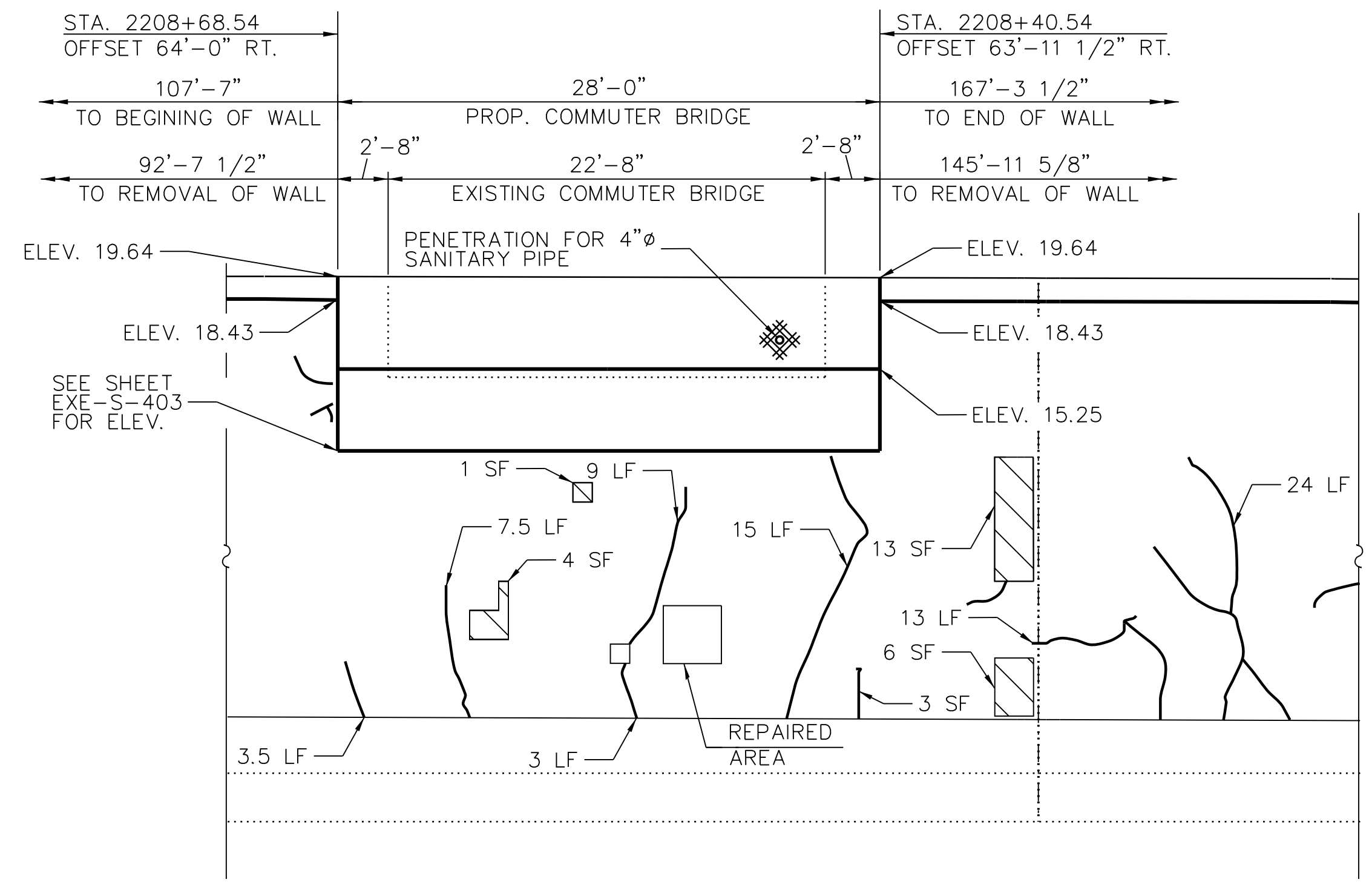
LOCATION IDENTIFIER: **DR095**

EXISTING EAST RETAINING WALL PLAN & ELEVATION (SHEET 1 OF 3)

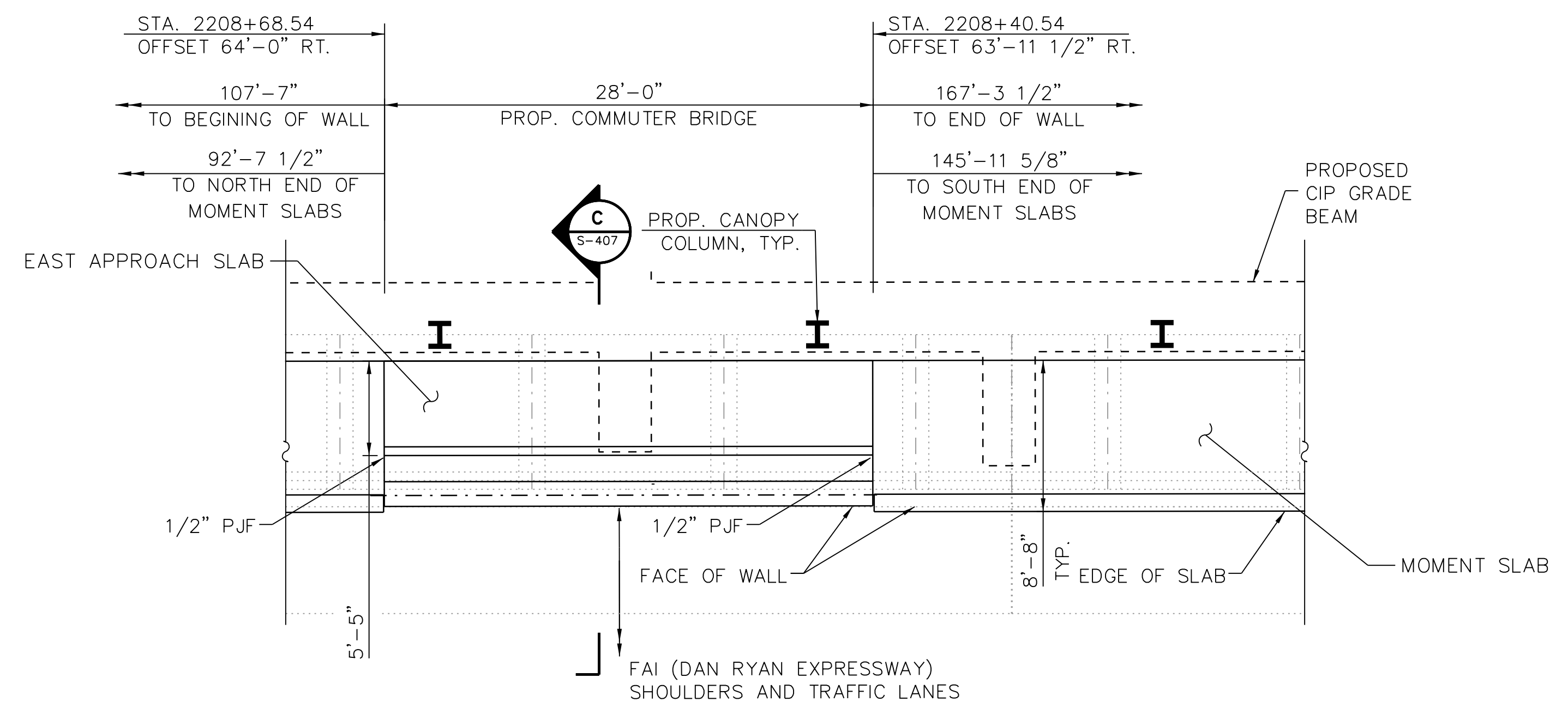
**EXE-S-404**

FILE NAME = 070\_DR095\_S-404.dgn  
PLOT DATE = 12/14/2016

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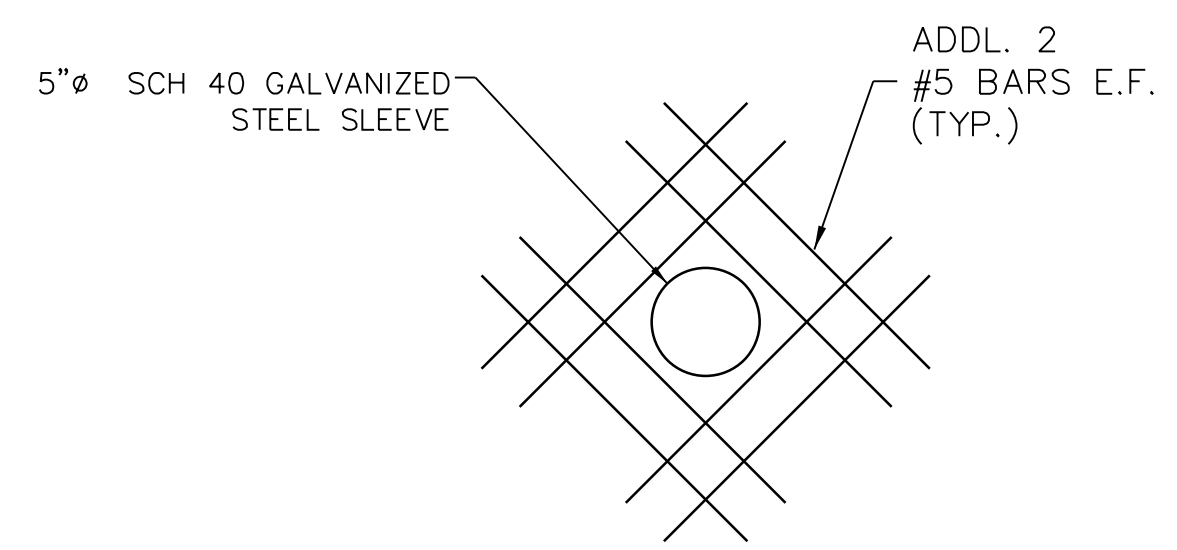


**1 EXISTING ELEVATION OF EAST RETAINING WALL**  
 (LOOKING EAST)  
 SCALE: N.T.S.



**2 EXISTING EAST RETAINING WALL PLAN**  
 SCALE: N.T.S.

- NOTES:**
- FOR PORTION OF WALL TO BE REMOVED AND RECONSTRUCTED TO BE PART OF PROPOSED NORTH BUS BRIDGE ABUTMENT, SEE NORTH BUS BRIDGE PLANS.
  - FOR STRUCTURAL REPAIR OF CONCRETE DETAILS, SEE SHEET 95TH-S-150A
  - THE ENTIRE EXPOSED FACE OF WALLS SHALL HAVE WATERPROOF COATING APPLIED AS PER STRUCTURAL REPAIR OF CONCRETE SECTION 03 74 00.
  - SEE SHEET C-200 TO C-203 FOR PENETRATION LOCATIONS.
  - FOR PROPOSED CANOPY FOUNDATIONS AND GRADE BEAMS, SEE S-1300 SERIES.
  - FOR MOMENT SLAB DETAILS, SEE S-1403A.



**3 PENETRATION DETAIL**  
 SCALE: N.T.S.

NOTE: GROUT BETWEEN PIPE AND SLEEVE AS REQUIRED.

- LEGEND:**
- STRUCTURAL REPAIR OF CONCRETE (DEPTH GREATER THAN 5 INCHES)
  - STRUCTURAL REPAIR OF CONCRETE (DEPTH EQUAL TO OR LESS THAN 5 INCHES)
  - EPOXY SEAL (WIDTH >0.06")
  - SF - SQUARE FOOT
  - LF - LINEAR FOOT



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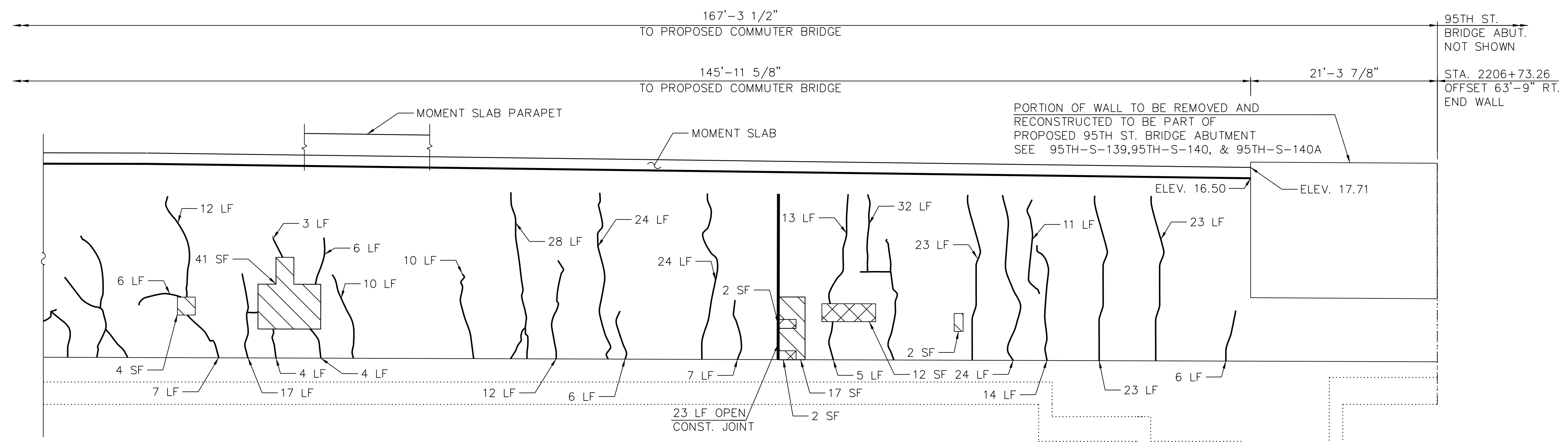
LOCATION IDENTIFIER: **DR095**

EXISTING EAST RETAINING WALL PLAN & ELEVATION (SHEET 2 OF 3)

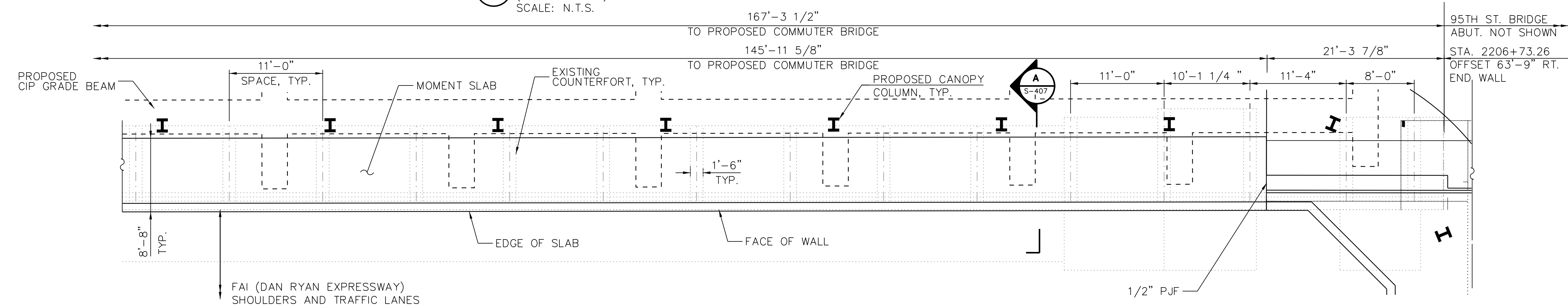
**EXE-S-405**

FILE NAME = 070\_DR095\_S-405.dgn  
 PLOT DATE = 11/11/2016

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**1 EXISTING ELEVATION OF EAST RETAINING WALL**  
 (LOOKING EAST)  
 SCALE: N.T.S.



**2 EXISTING EAST RETAINING WALL PLAN**  
 SCALE: N.T.S.

- NOTES:
- FOR PORTION OF WALL TO BE REMOVED AND RECONSTRUCTED TO BE PART OF PROPOSED COMMUTER BRIDGE ABUTMENT, SEE SHEETS EXE-S-405 AND EXE-S-407.
  - FOR STRUCTURAL REPAIR OF CONCRETE DETAILS, SEE SHEET 95TH-S-150A.
  - FOR EXPANSION JOINT DETAILS, SEE SHEET 95TH-S-122.
  - THE ENTIRE EXPOSED FACE OF WALLS SHALL HAVE WATERPROOF COATING APPLIED AS PER STRUCTURAL REPAIR OF CONCRETE SECTION 03 74 00.
  - FOR PROPOSED CANOPY FOUNDATIONS AND GRADE BEAMS, SEE S-1300 SERIES.
  - FOR MOMENT SLAB DETAILS, SEE S-1403A.

- LEGEND:
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  - STRUCTURAL REPAIR OF CONCRETE (DEPTH EQUAL TO OR LESS THAN 5 INCHES)
  - EPOXY SEAL (WIDTH >0.06")
  - SF - SQUARE FOOT
  - LF - LINEAR FOOT



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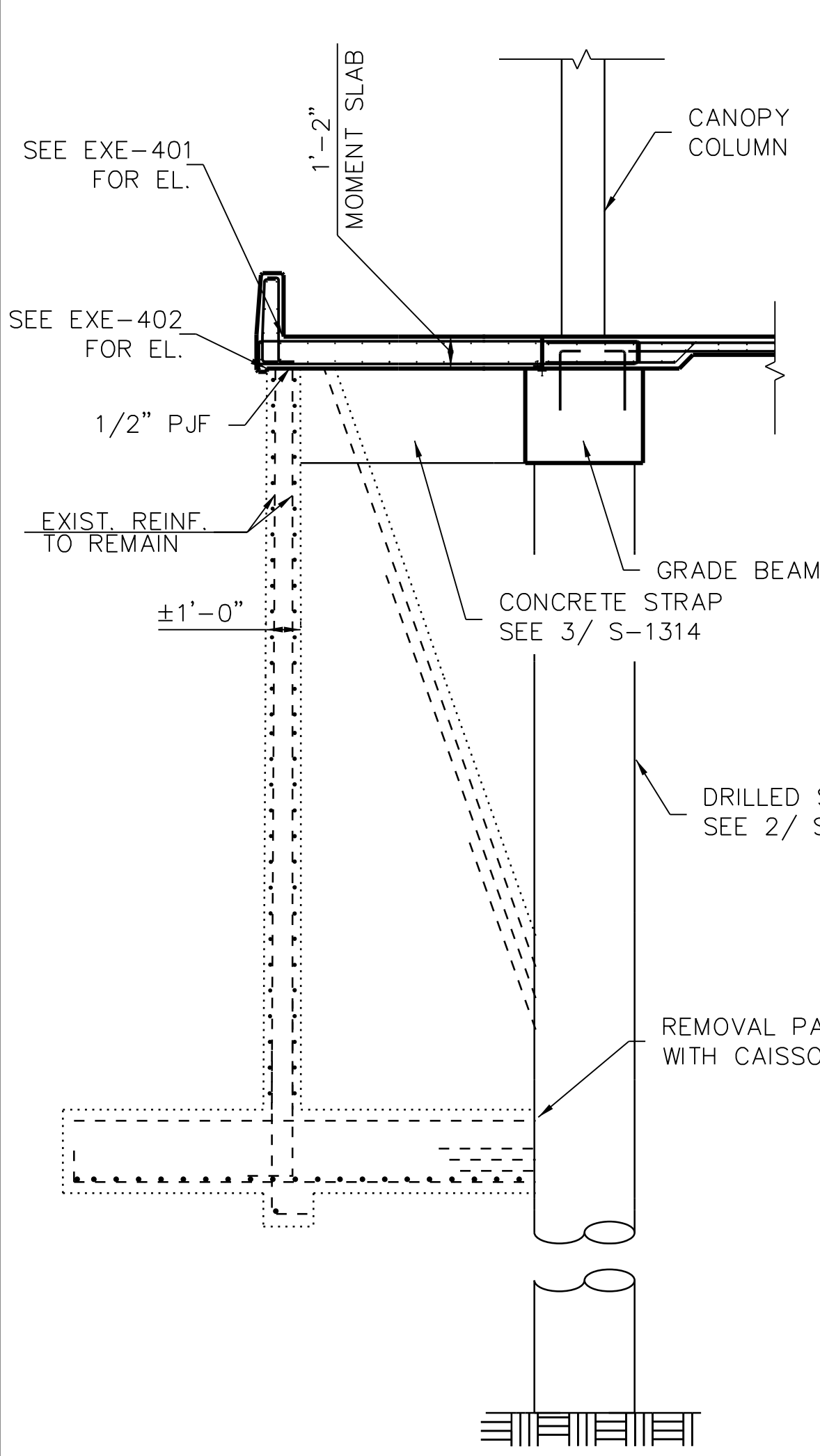
LOCATION IDENTIFIER: **DR095**

EXISTING EAST RETAINING WALL PLAN & ELEVATION (SHEET 3 OF 3)

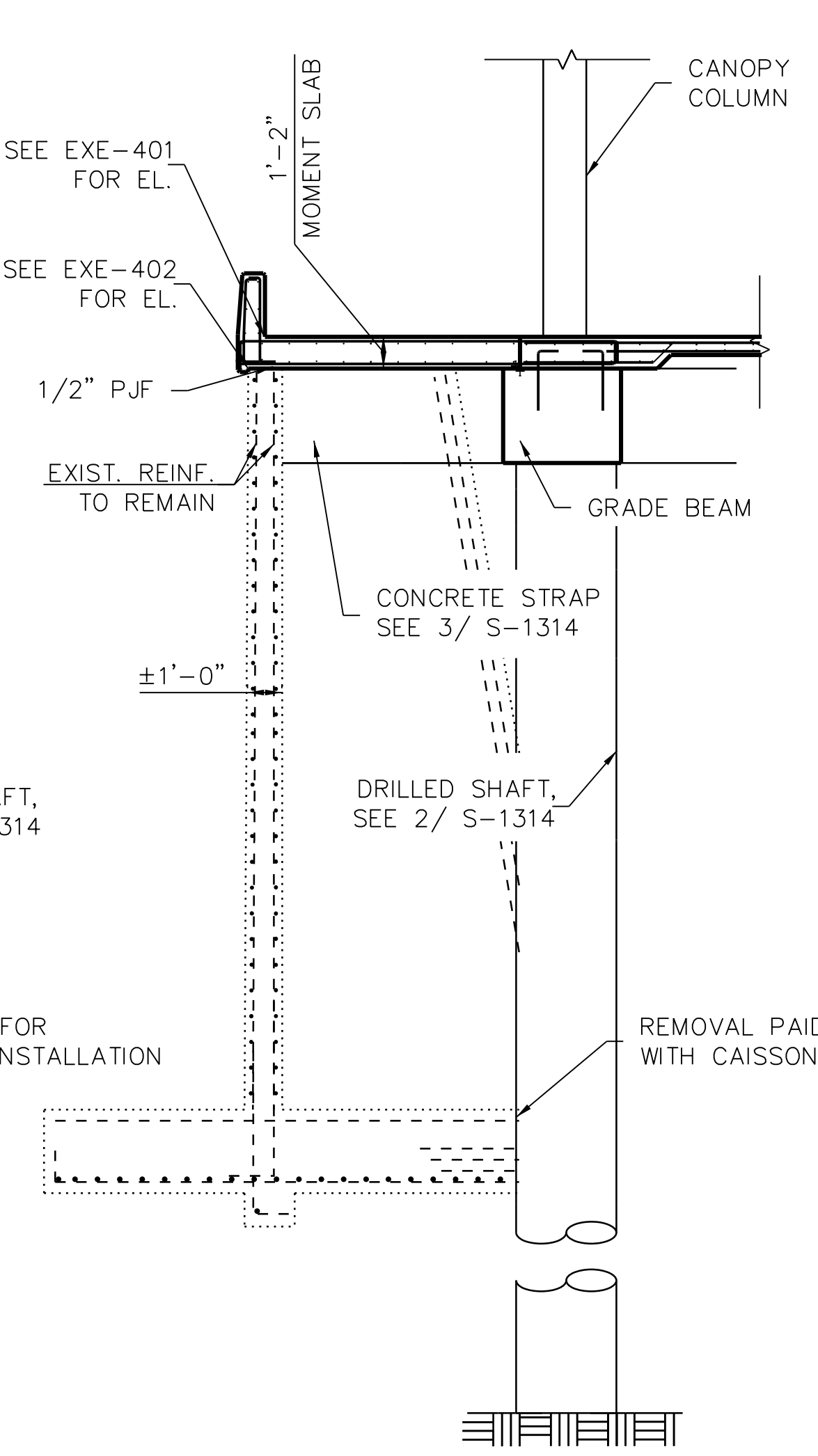
**EXE-S-406**

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 PLOT DATE = 12/14/2016

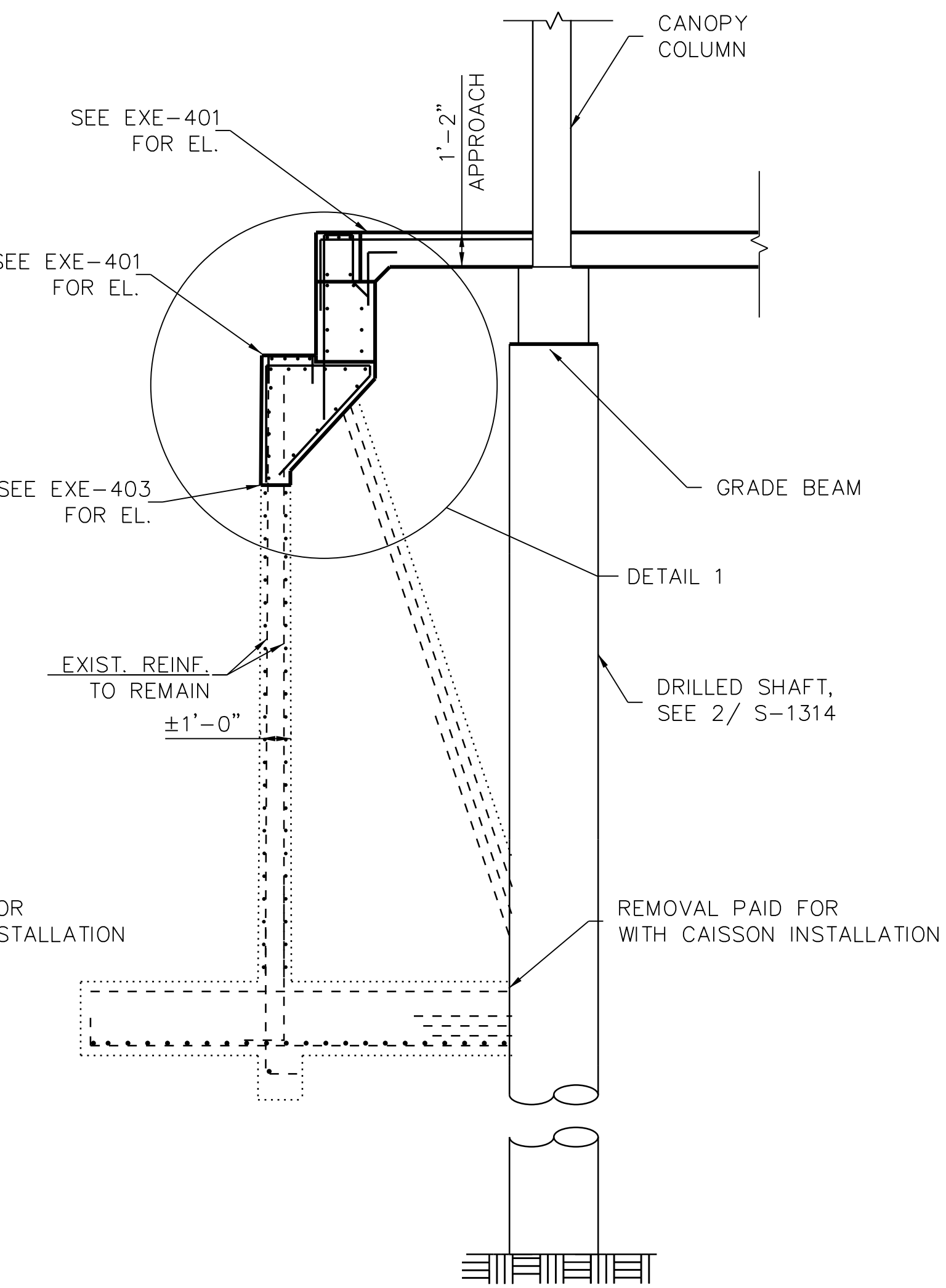
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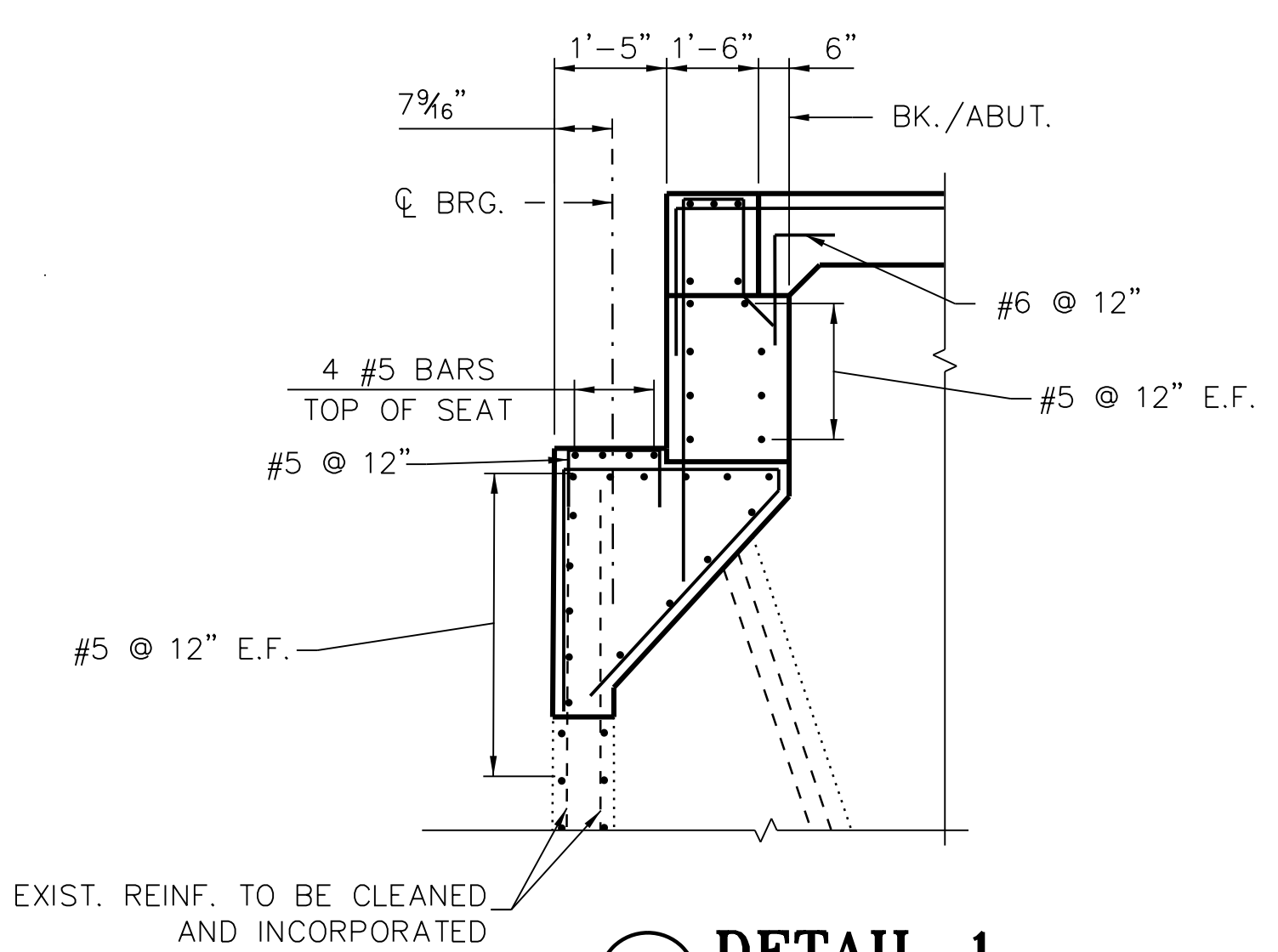
**A** SECTION A  
AT EXISTING RETAINING WALL  
SCALE: N.T.S.



**B** SECTION B  
AT EXISTING RETAINING WALL  
SCALE: N.T.S.



**C** SECTION C  
AT EXISTING COMMUTER BRIDGE  
SCALE: N.T.S.



**1** DETAIL 1  
SCALE: N.T.S.

**NOTE:**

1. FOR SIDEWALK, PARAPET AND FENCE, SEE CIVIL, BUILDING AND ARCHITECTURAL PLANS.
2. FOR COMMUTER BRIDGE AND RETAINING WALL EXCAVATION AND CELLULAR CONCRETE FILL, SEE SHEETS EXE-S-408 AND EXE-S-409.
3. FOR MOMENT SLAB DETAILS, SEE S-1403A.
4. COORDINATE CAST IN ANCHORS WITH COMMUTER BRIDGE DRAWINGS.



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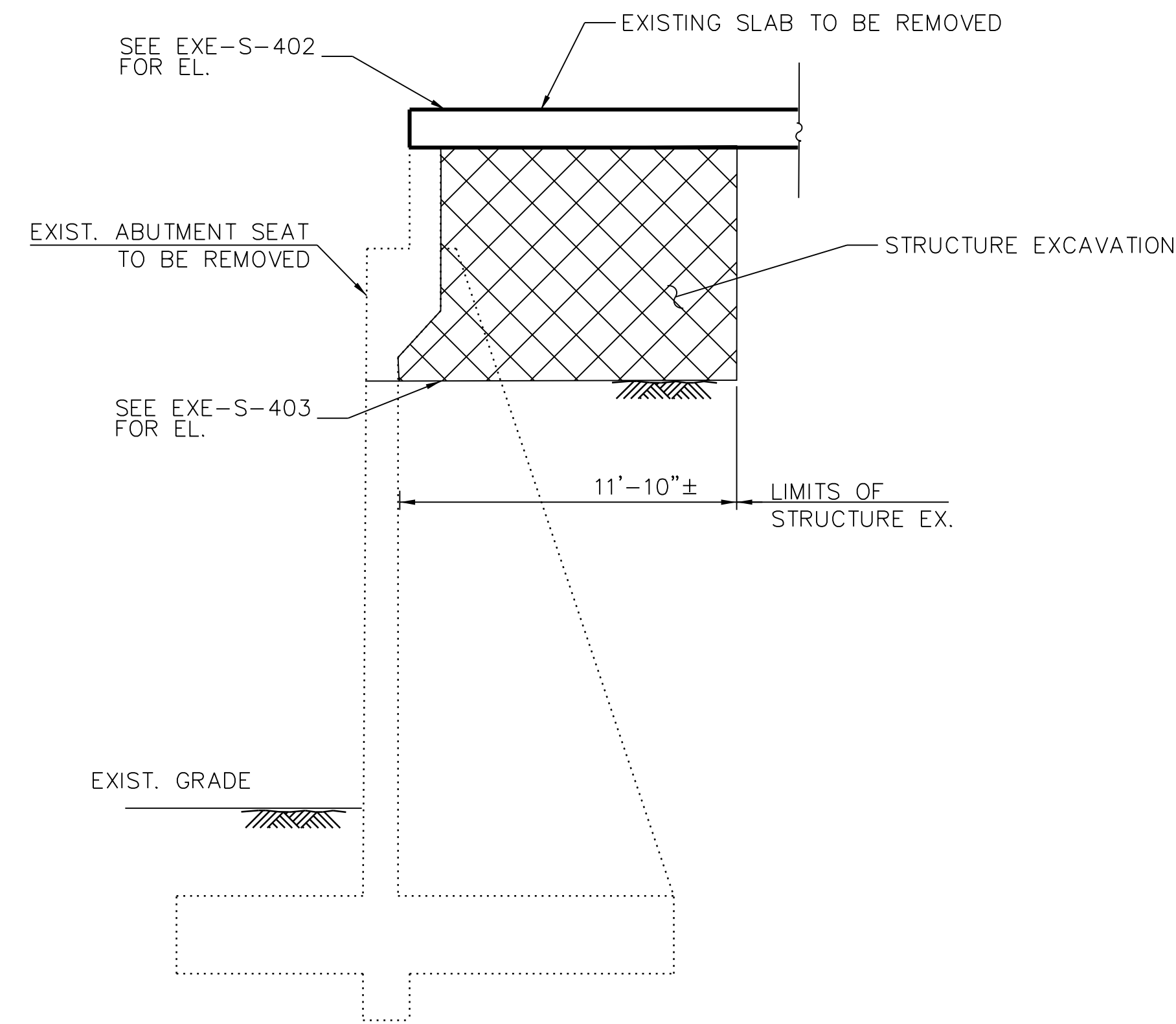
LOCATION IDENTIFIER: **DR095**

EXISTING EAST RETAINING WALL  
SECTIONS & DETAILS

**EXE-S-407**

FILE NAME = 070\_DR095\_S-407.dgn  
PLOT DATE = 12/14/2016

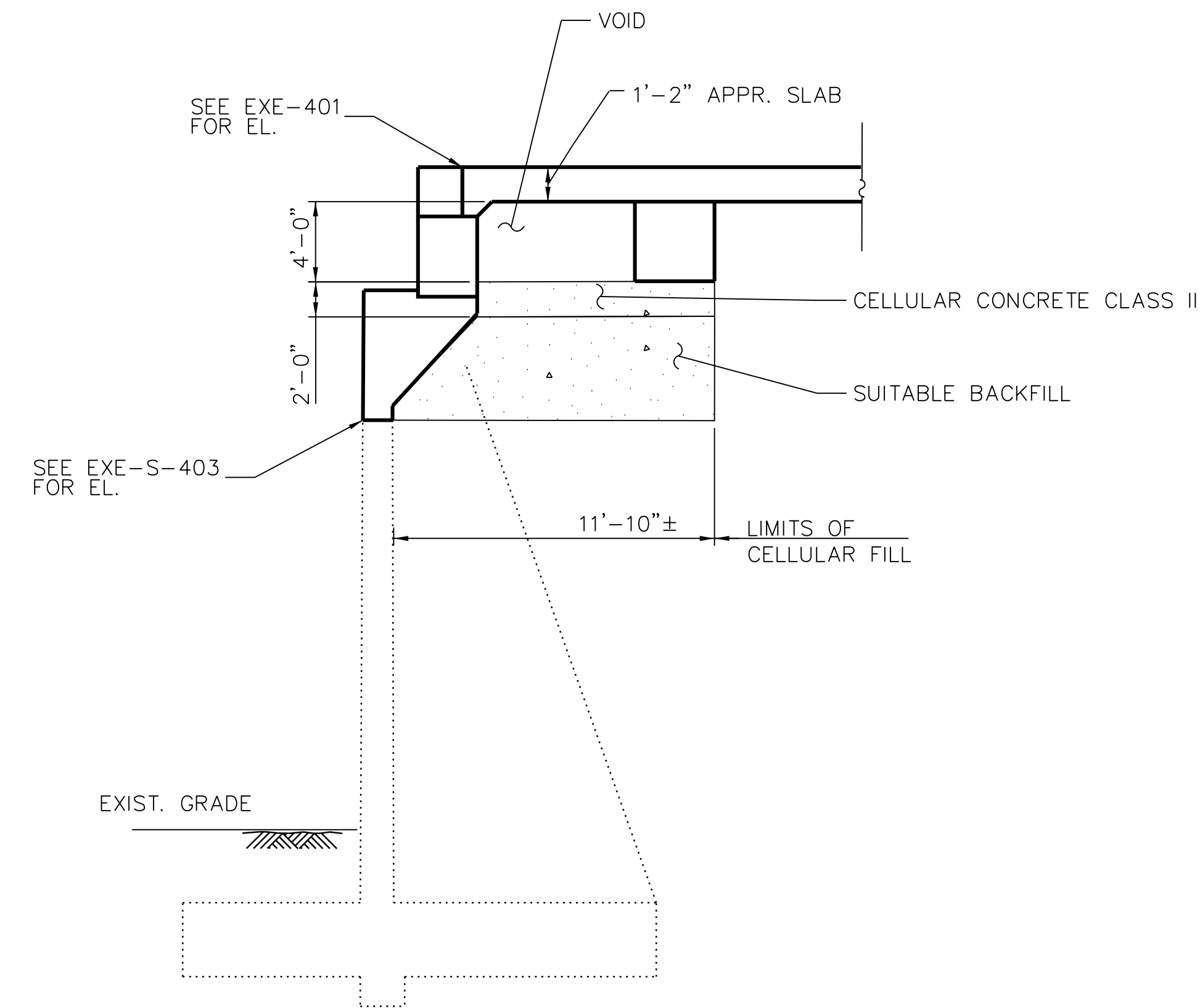
vscorppa



**SECTION C - EXCAVATION**  
 AT PROPOSED COMMUTER BRIDGE  
 SCALE: N.T.S.

**CONSTRUCTION SEQUENCE:**

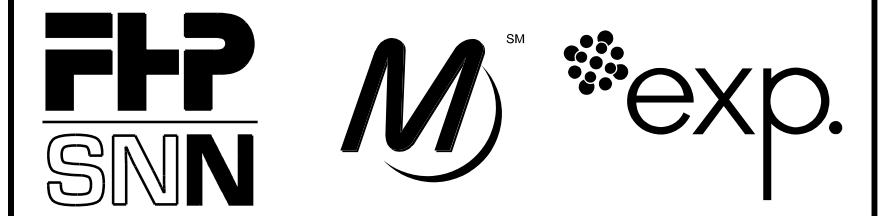
1. REMOVE EXISTING PAVEMENT (SEE CIVIL PLANS)
2. EXCAVATE BEHIND EXISTING RETAINING WALL TO THE LIMITS SHOWN ON THE PLANS.
3. REMOVE PORTION OF RETAINING WALL AS SHOWN ON SHEET EXE-S-403.
4. RECONSTRUCT BACKWALL AS SHOWN ON SHEETS EXE-S-407
5. BACKFILL WITH APPROVED LIGHTWEIGHT CELLULAR CONCRETE IN LIFTS NOT EXCEEDING FOUR (4) FEET.
6. CONSTRUCT APPROACH SLAB, CLOSURE POUR AND SIDEWALK.



**SECTION C - LIGHTWEIGHT CELLULAR CONCRETE**  
 AT PROPOSED COMMUTER BRIDGE  
 SCALE: N.T.S.

**LEGEND:**

- STRUCTURE EXCAVATION
- CELLULAR CONCRETE



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**95th Terminal Improvement**  
 15 West 95th Street (South Terminal)  
 Chicago, IL 60628

**IN CHARGE** S. KADERBEK

**APPROVED BY** D.ZROKA

**CHECKED BY** P.MAID

**DESIGNED BY** L.SOMMER

**DRAWN BY** T.SLEDGE

**PROJECT NO.** 2012-0021

**FILE NAME** File Name

MARK	DATE	DESCRIPTION
	2/9/17	ISSUED FOR CONSTRUCTION

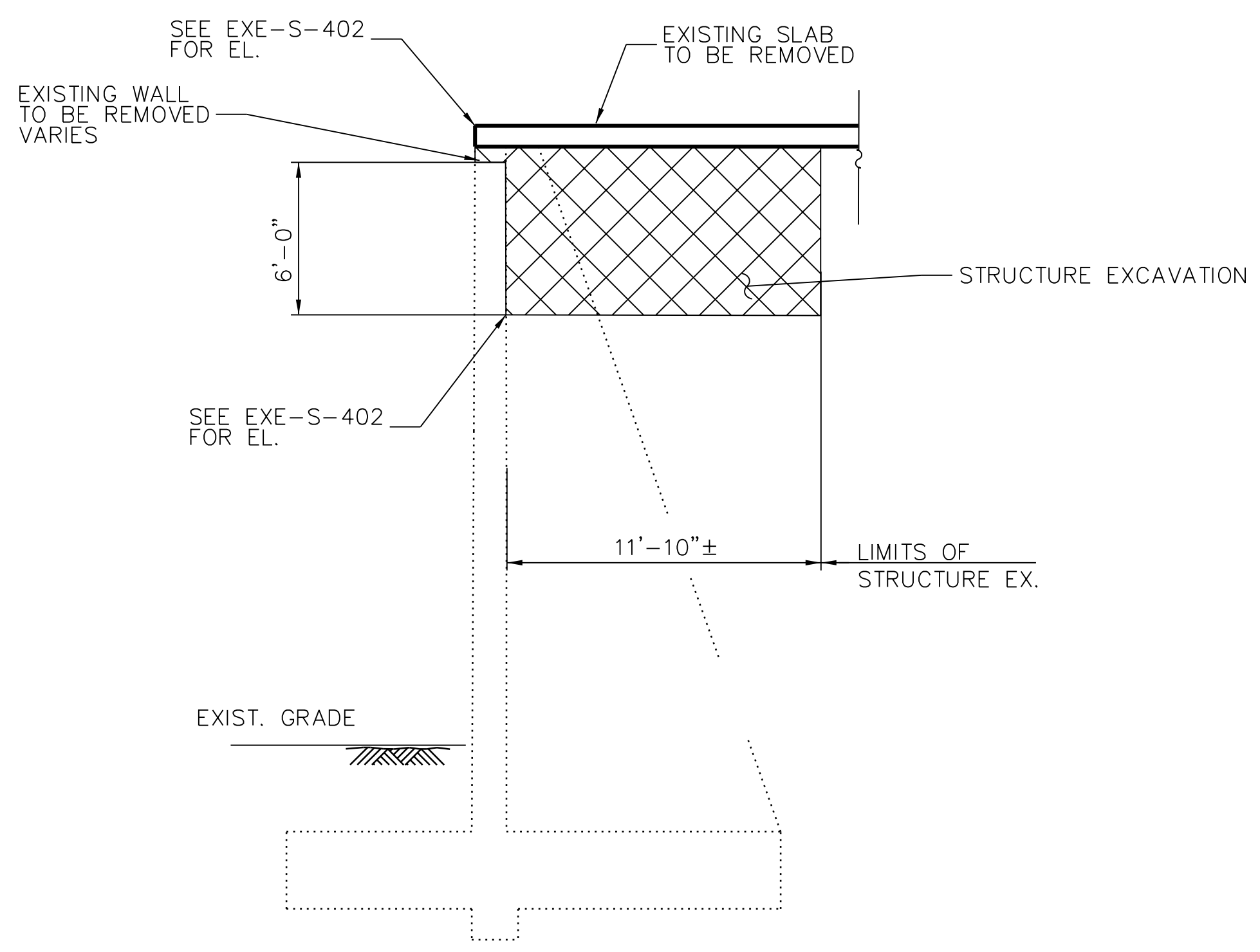
**LOCATION IDENTIFIER:** DR095

**COMMUTER BRIDGE  
 EXCAVATION & CELLULAR CONCRETE**

**EXE-S-408**

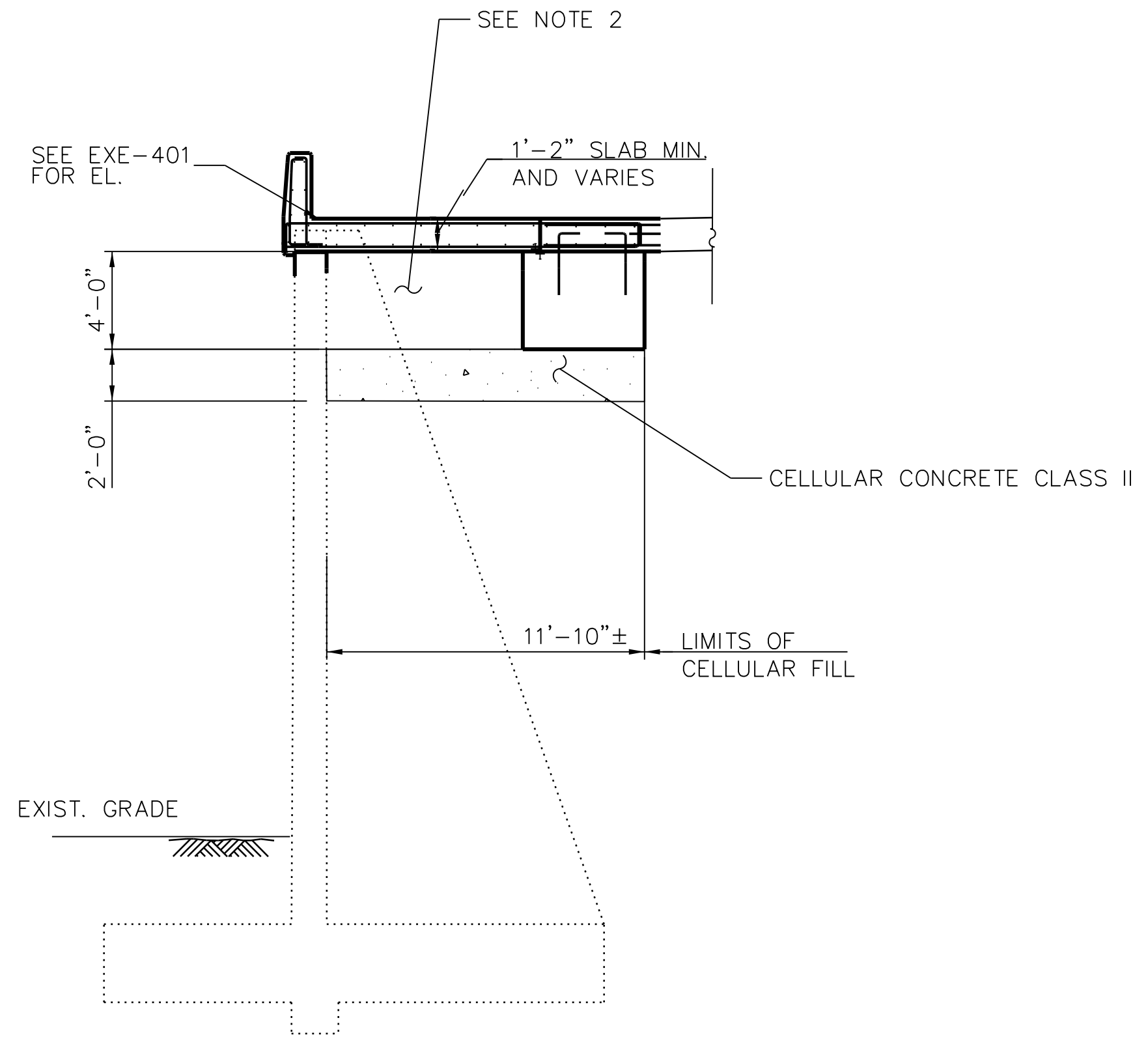
FILE NAME = 070\_DR095\_S-408.dgn  
 PLOT DATE = 1/23/2017

vscorppa



**SECTION A - EXCAVATION**  
 AT EXISTING RETAINING WALL  
 SCALE: N.T.S.

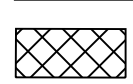
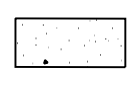
- CONSTRUCTION SEQUENCE:
1. REMOVE EXISTING PAVEMENT (SEE CIVIL PLANS)
  2. EXCAVATE BEHIND EXISTING RETAINING WALL TO THE LIMITS SHOWN ON THE PLANS.
  3. REMOVE PORTION OF RETAINING WALL AS SHOWN ON SHEET EXE-S-403.
  4. RECONSTRUCT BACKWALL AS SHOWN ON SHEETS EXE-S-407
  5. BACKFILL WITH APPROVED CELLULAR FILL IN LIFTS NOT EXCEEDING THREE (3) FEET.
  6. CONSTRUCT MOMENT SLABS.



**SECTION A - LIGHTWEIGHT CELLULAR CONCRETE**  
 AT EXISTING RETAINING WALL  
 SCALE: N.T.S.

- NOTES:
1. NO FILL EXCEPT WHERE NEEDED TO MATCH 95TH STREET BRIDGE ABUTMENT GRADE.

LEGEND:

	STRUCTURE EXCAVATION
	CELLULAR FILL



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APPROVED BY	D.ZROKA
CHECKED BY	P.MAID
DESIGNED BY	L.SOMMER
DRAWN BY	T.SLEDGE
PROJECT NO.	2012-0021
FILE NAME	File Name

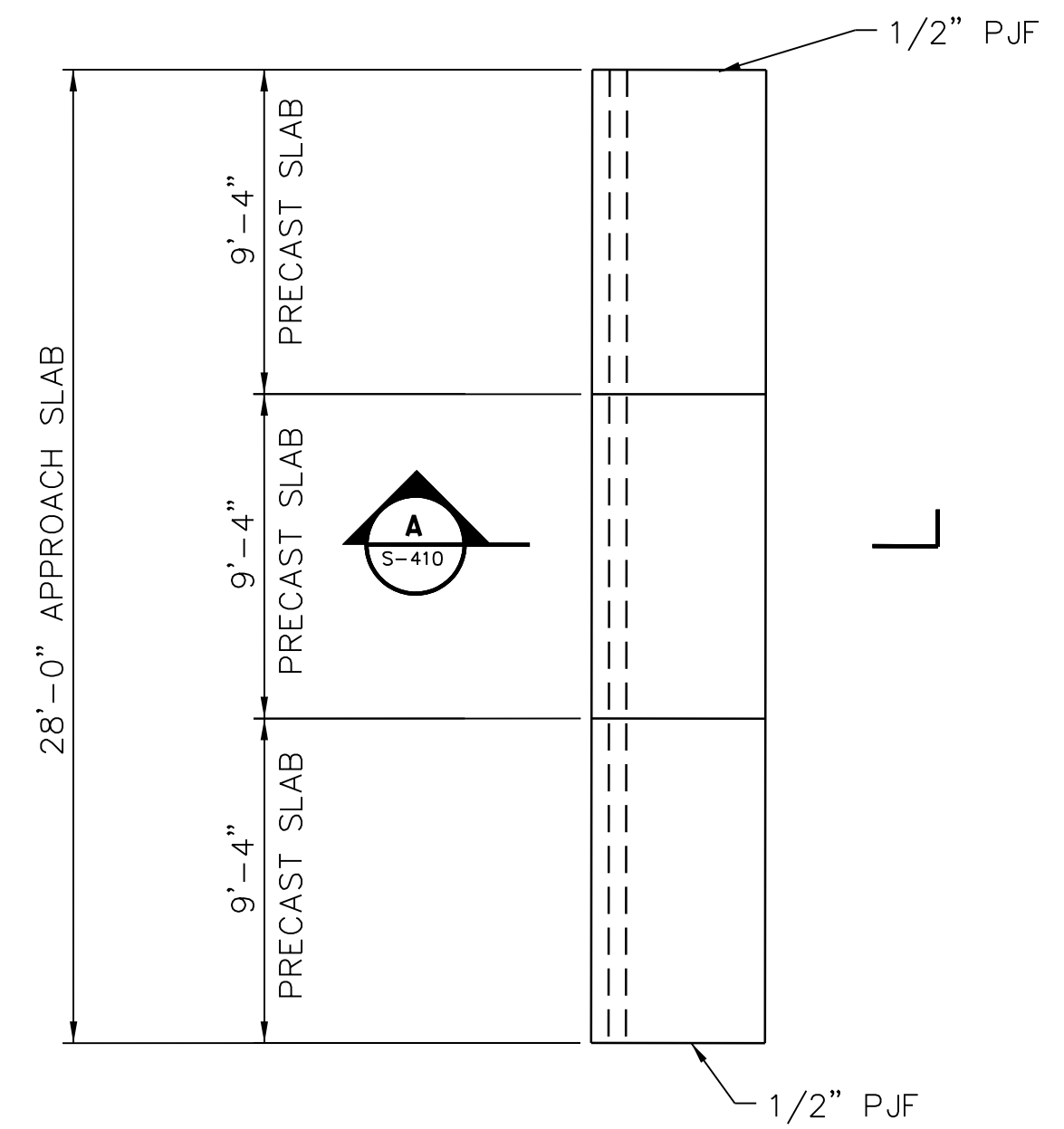
MARK	DATE	DESCRIPTION
	2/9/17	ISSUED FOR CONSTRUCTION

LOCATION IDENTIFIER: **DR095**  
 EXISTING RETAINING WALL  
 EXCAVATION AND CELLULAR  
 CONCRETE

**EXE-S-409**

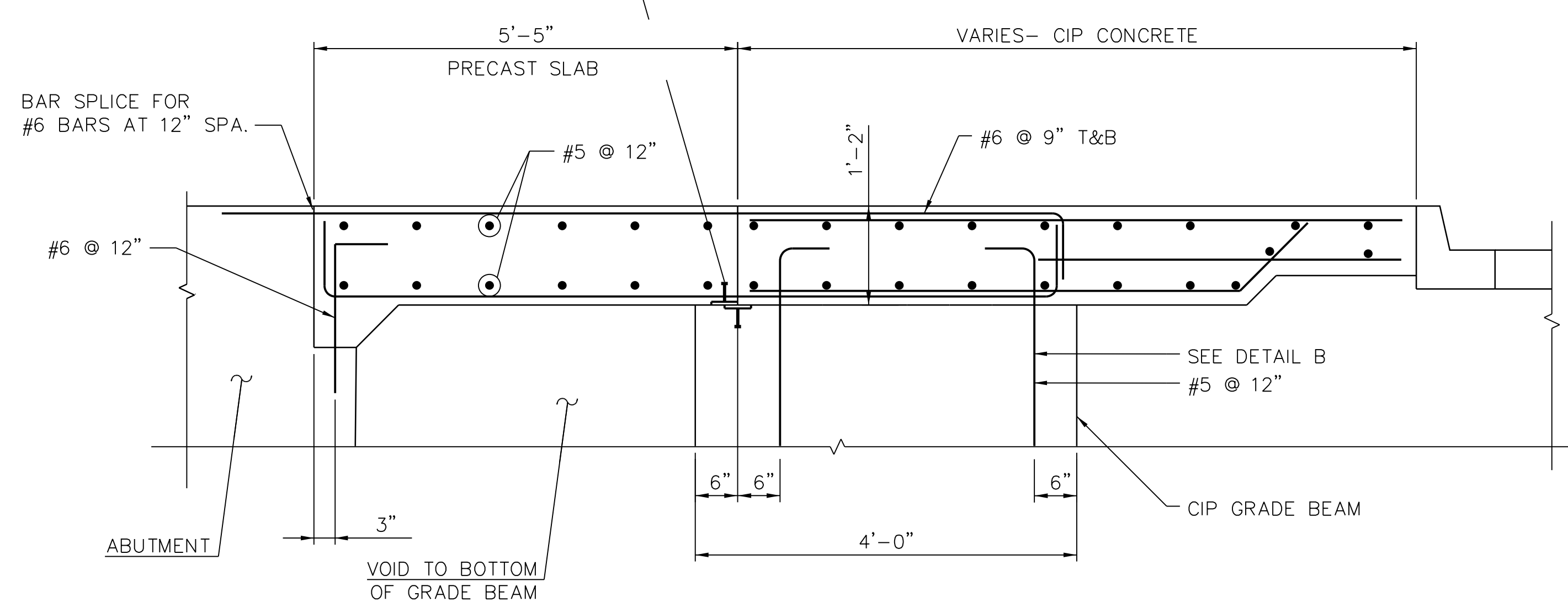
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 PLOT DATE = 12/14/2016

vscarpa

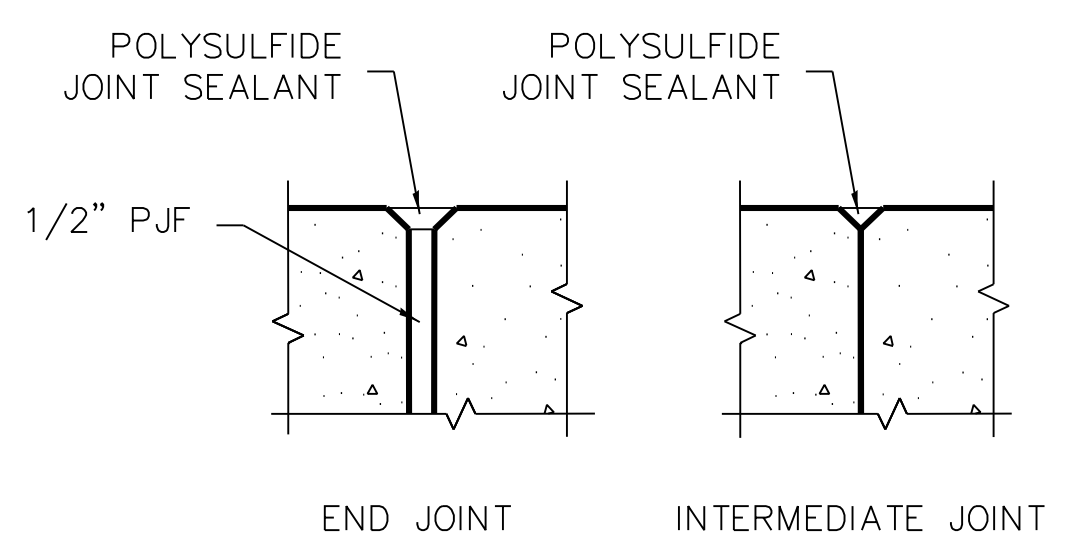


1 PLAN

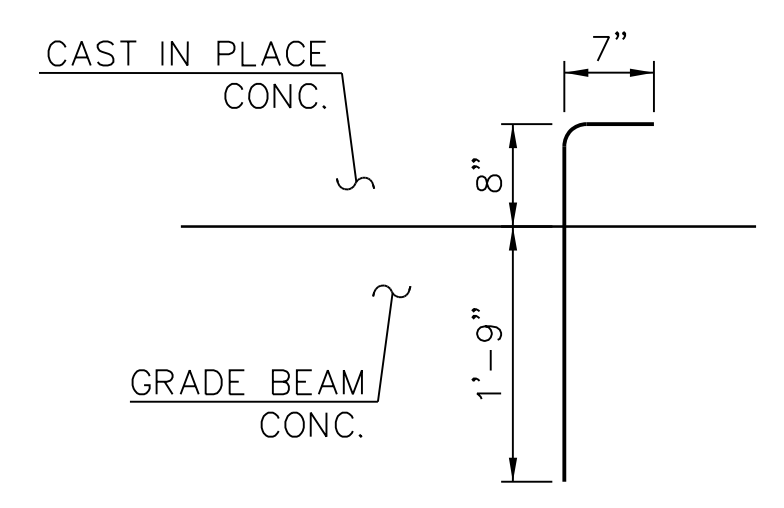
PL 1/2"x4"x0'-6" WITH TWO 1/2" DIA X 3" SHEAR STUDS. SHIM AS REQUIRED AND FIELD WELD WITH 5/16" X 4" WELD TO HOLD MOMENT SLAB IN PLACE



A APPROACH SLAB SECTION SCALE: N.T.S.



C JOINTING DETAIL



B DETAIL B SCALE: N.T.S.



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**95th Terminal Improvement**  
 15 West 95th Street (South Terminal)  
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IN CHARGE	S. KADERBEK
APPROVED BY	D.ZROKA
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PROJECT NO.	2012-0021
FILE NAME	File Name

MARK	DATE	DESCRIPTION
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LOCATION IDENTIFIER: **DR095**

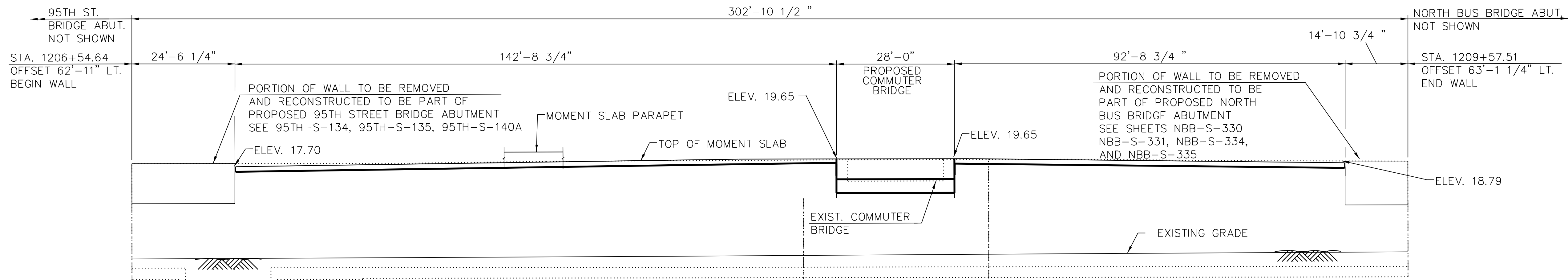
COMMUTER BRIDGE  
 EAST APPROACH SLAB

**EXE-S-410**

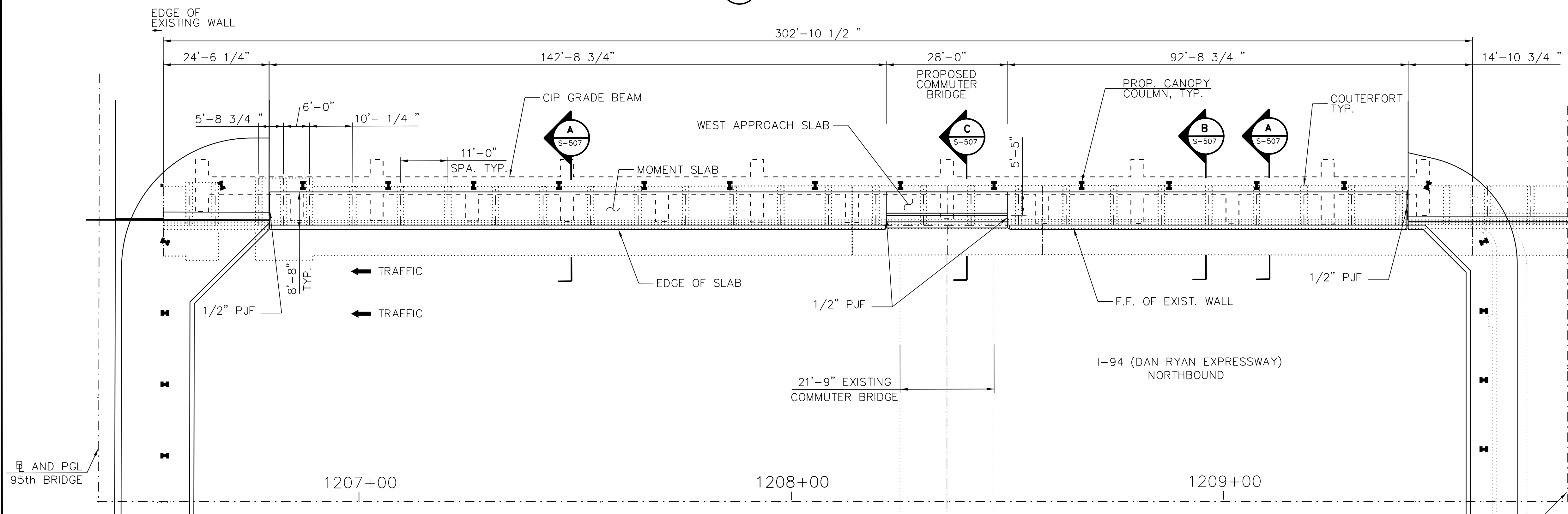
FILE NAME = 070\_DR095\_S-410.dgn  
 PLOT DATE = 12/14/2016

BENCH MARK: CITY OF CHICAGO BENCHMARK NO. 551. BENCH MONUMENT LOCATED ABOUT 50 FEET SOUTH OF THE SOUTH LINE OF W. 95TH ST., ABOUT 11 FEET EAST OF THE WEST LINE OF S. PERRY AVE. ELEVATION 16.092.

EXISTING STRUCTURE: THE WEST RETAINING WALL WAS ORIGINALLY CONSTRUCTED IN 1968 WITH A LENGTH OF 302'-10 1/2" AND EXTENDS FROM THE NORTH EDGE OF THE WEST ABUTMENT OF SOUTH 95TH ST. BRIDGE OVER F.A.I. 94 (DAN RYAN EXPRESSWAY) NORTH TO THE SOUTH EDGE OF CTA'S 95TH ST. STATION BUS BRIDGE WEST ABUTMENT. INCLUDED WITHIN THE LIMITS OF WALL IS THE WEST ABUTMENT FOR CTA'S 95TH ST. STATION COMMUTER BRIDGE. THE RETAINING WALL IS A REINFORCED CONCRETE "T" TYPE WALL ON SPREAD FOOTING WITH BASE KEY AND COUNTERFORTS. THE EXPOSED HEIGHT OF WALL VARIES FROM APPROXIMATELY 22'-7" AT THE SOUTH END TO 21'-6" AT THE NORTH END (MEASURED FROM GRADE AT FRONT FACE OF WALL). THE STEM WIDTH AT THE VERY TOP OF WALL IS 2'-0" INCLUDING A 6" SHELF FOR SUPPORTING THE ADJACENT SIDEWALK AND HANDRAIL. PORTIONS OF THE EXISTING WEST RETAINING WALL WILL BE REMOVED AND RECONSTRUCTED TO SUPPORT THE WIDENED WEST ABUTMENT OF CTA'S 95TH ST. STATION NORTH BUS BRIDGE, THE WEST ABUTMENT OF THE NEW NORTH COMMUTER BRIDGE AND NEW MOMENT SLABS.



**1 ELEVATION**  
SCALE: N.T.S.



**2 PLAN**  
SCALE: N.T.S.

**EXISTING WALL DESIGN SPECIFICATIONS**  
2002 AASHTO STANDARD SPECIFICATIONS FOR HIGHWAY BRIDGES, 17TH EDITION  
ORIGINAL CONSTRUCTION (1968)  
AMERICAN ASSOCIATION OF STATE HIGHWAY OFFICIALS STANDARD SPECIFICATIONS FOR HIGHWAY BRIDGES, 9TH EDITION, DATED 1965

**EXISTING WALL DESIGN STRESSES**  
FIELD UNITS  
F<sub>C</sub> = 3,500 PSI  
F<sub>Y</sub> = 60,000 PSI (REINFORCEMENT)

**EXISTING UNITS**  
F<sub>C</sub> = 4,000 PSI  
F<sub>Y</sub> = 40,000 PSI (REINFORCEMENT)  
F<sub>Y</sub> = 36,000 PSI (STRUCTURAL STEEL)

**SEISMIC DATA**  
SEISMIC PERFORMANCE CATEGORY (SPC)=A  
HORIZONTAL BEDROCK ACCELERATION COEFFICIENT (A)=0.04 G  
SITE COEFFICIENT = 1.0

- NOTES:
1. FOR PROPOSED CANOPY FOUNDATIONS, SEE S-1300 SERIES.
  2. FOR PROFILE GRADE LINES, SEE SHEET NBB-S-302.
  3. THE ENTIRE EXPOSED FACE OF WALLS SHALL HAVE WATERPROOF COATING APPLIED AS PER STRUCTURAL REPAIR OF CONCRETE SECTION 03 74 00.
  4. FOR MOMENT SLAB DETAILS, SEE S-1403A.



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APPROVED BY	D.ZROKA
CHECKED BY	P.MAID
DESIGNED BY	L.SOMMER
DRAWN BY	T.SLEDGE
PROJECT NO.	2012-0021
FILE NAME	File Name

MARK	DATE	DESCRIPTION
	2/9/17	ISSUED FOR CONSTRUCTION

LOCATION IDENTIFIER: **DR095**

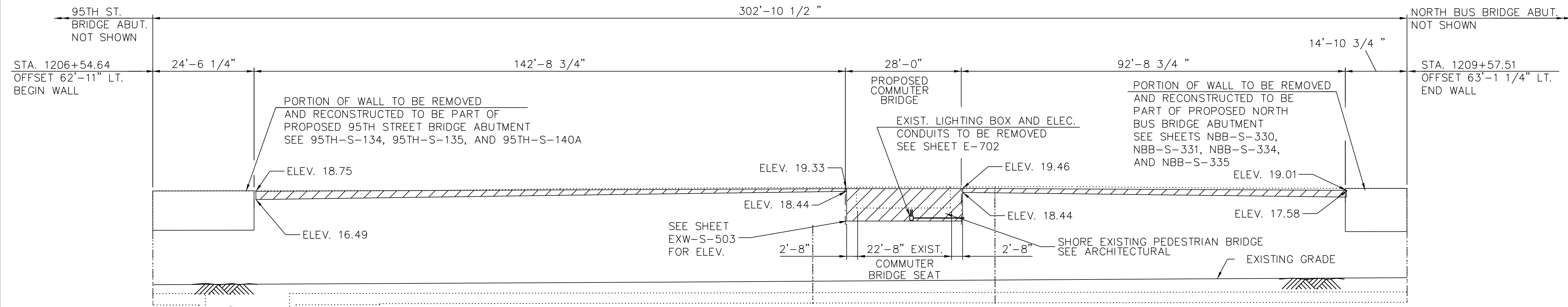
EXISTING WEST RETAINING WALL  
GENERAL PLAN AND ELEVATION

**EXW-S-501**

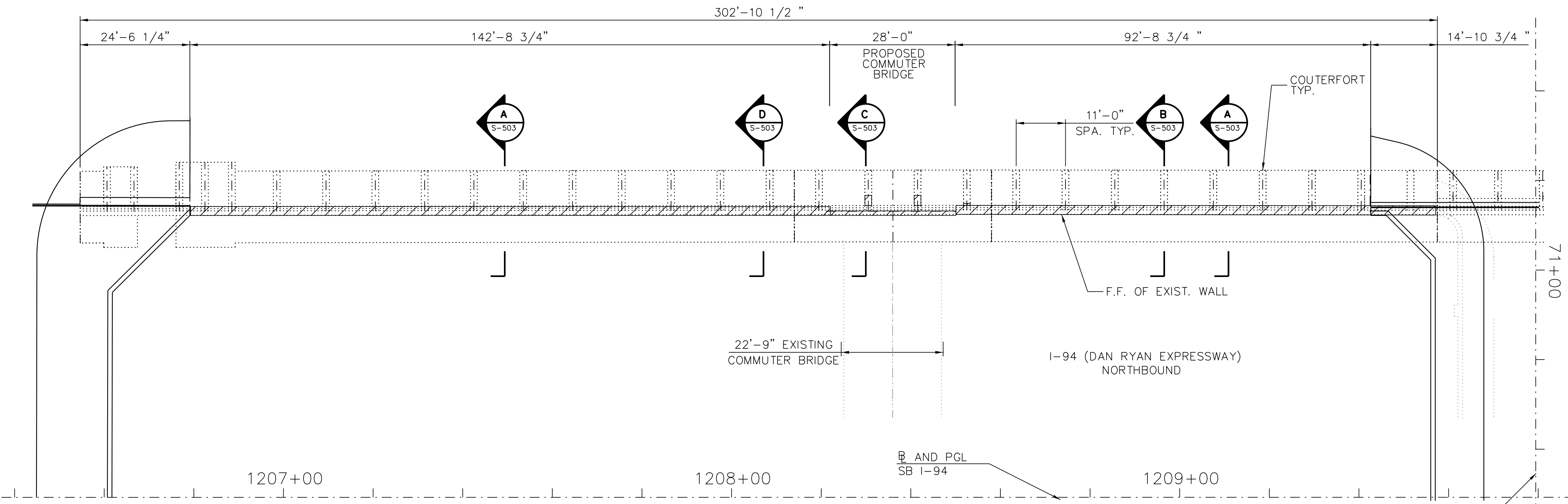
FILE NAME = 070\_DR095\_S-501.dgn  
PLOT DATE = 12/14/2016



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**1 ELEVATION**  
SCALE: N.T.S.



**2 PLAN**  
SCALE: N.T.S.

LEGEND:  
 CONCRETE REMOVAL

NOTE:  
 1. FOR NOTES, SEE SHEET EXW-S-501.  
 2. FOR SIDEWALK REMOVAL, SEE SHEET C-102.



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PROJECT NO.	2012-0021
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MARK	DATE	DESCRIPTION
	2/9/17	ISSUED FOR CONSTRUCTION

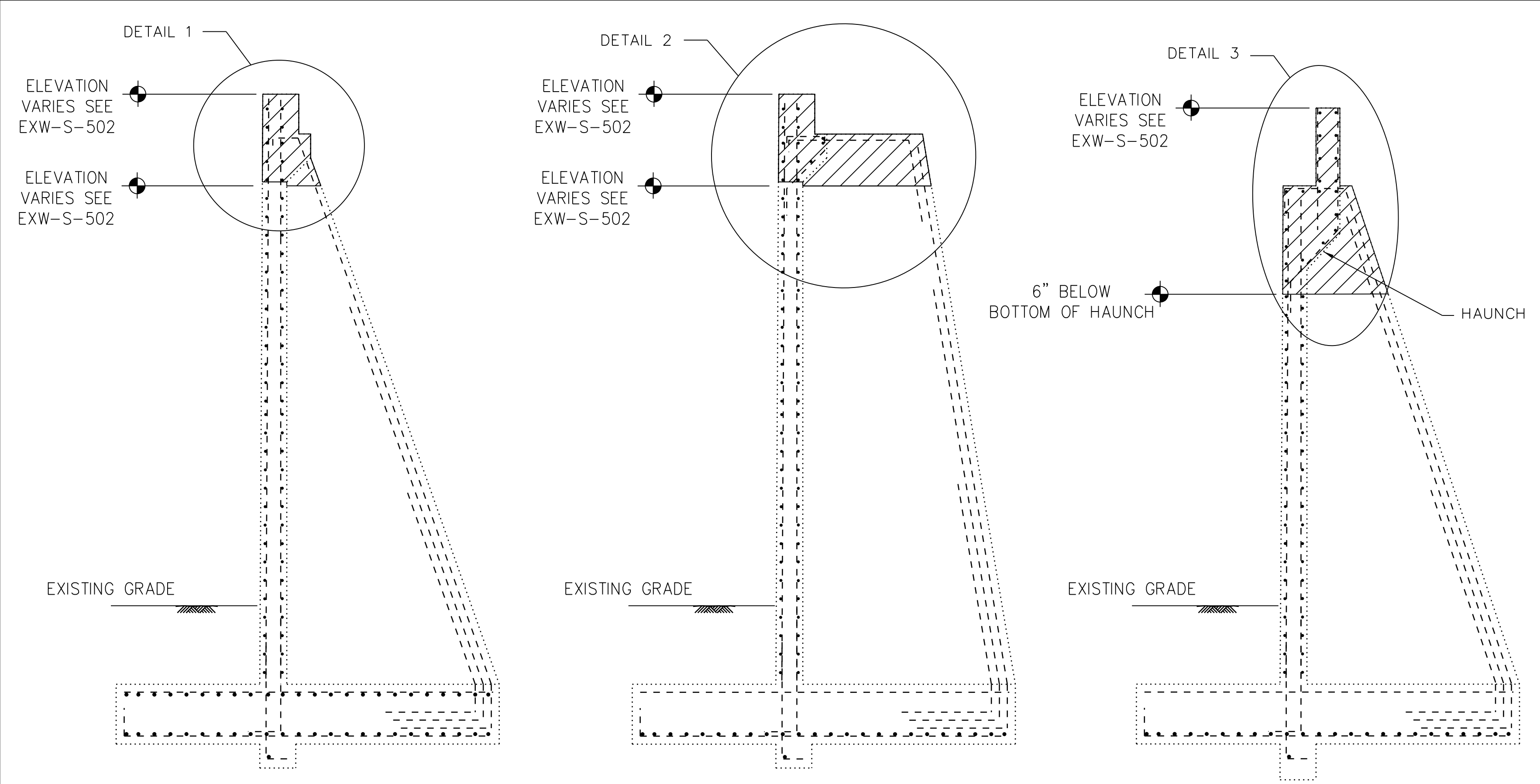
LOCATION IDENTIFIER: **DR095**

EXISTING WEST RETAINING WALL  
 REMOVAL PLAN AND ELEVATION

**EXW-S-502**

FILE NAME = 070\_DR095\_S-502.dgn  
 PLOT DATE = 12/14/2016

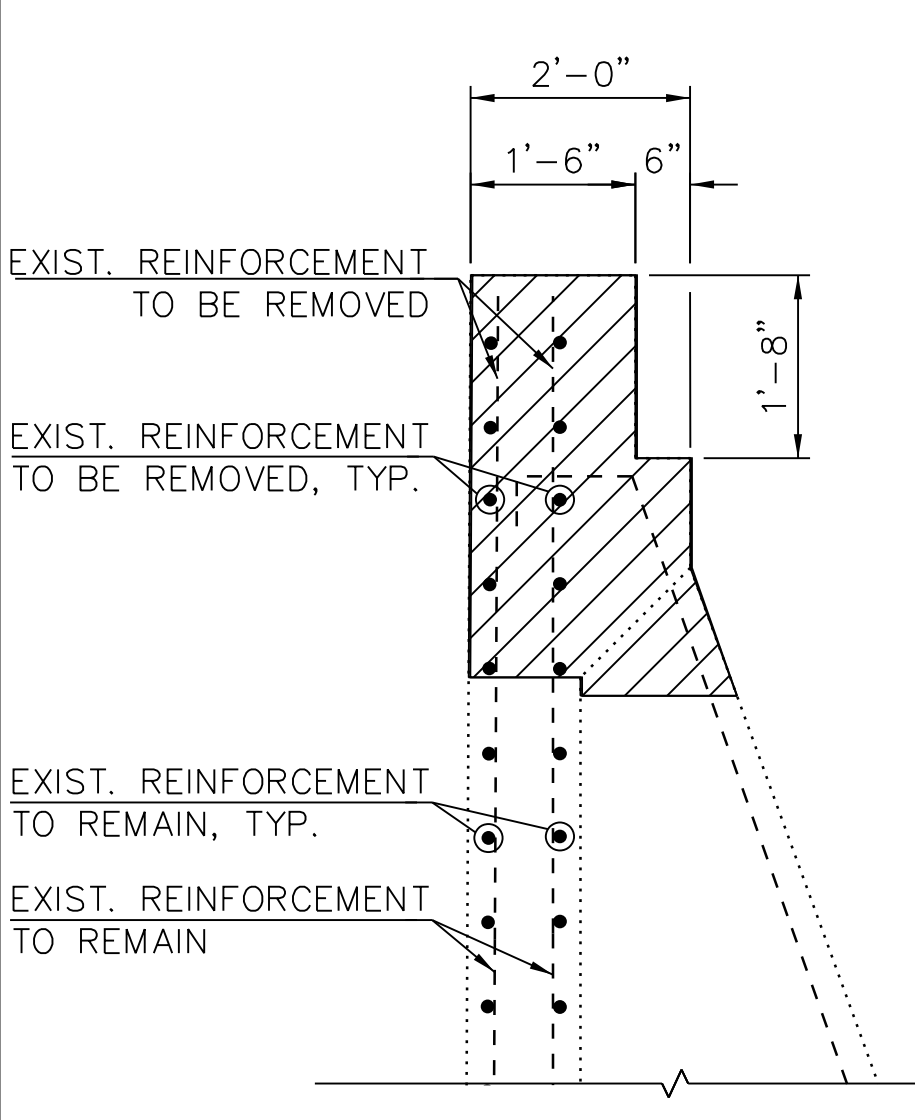
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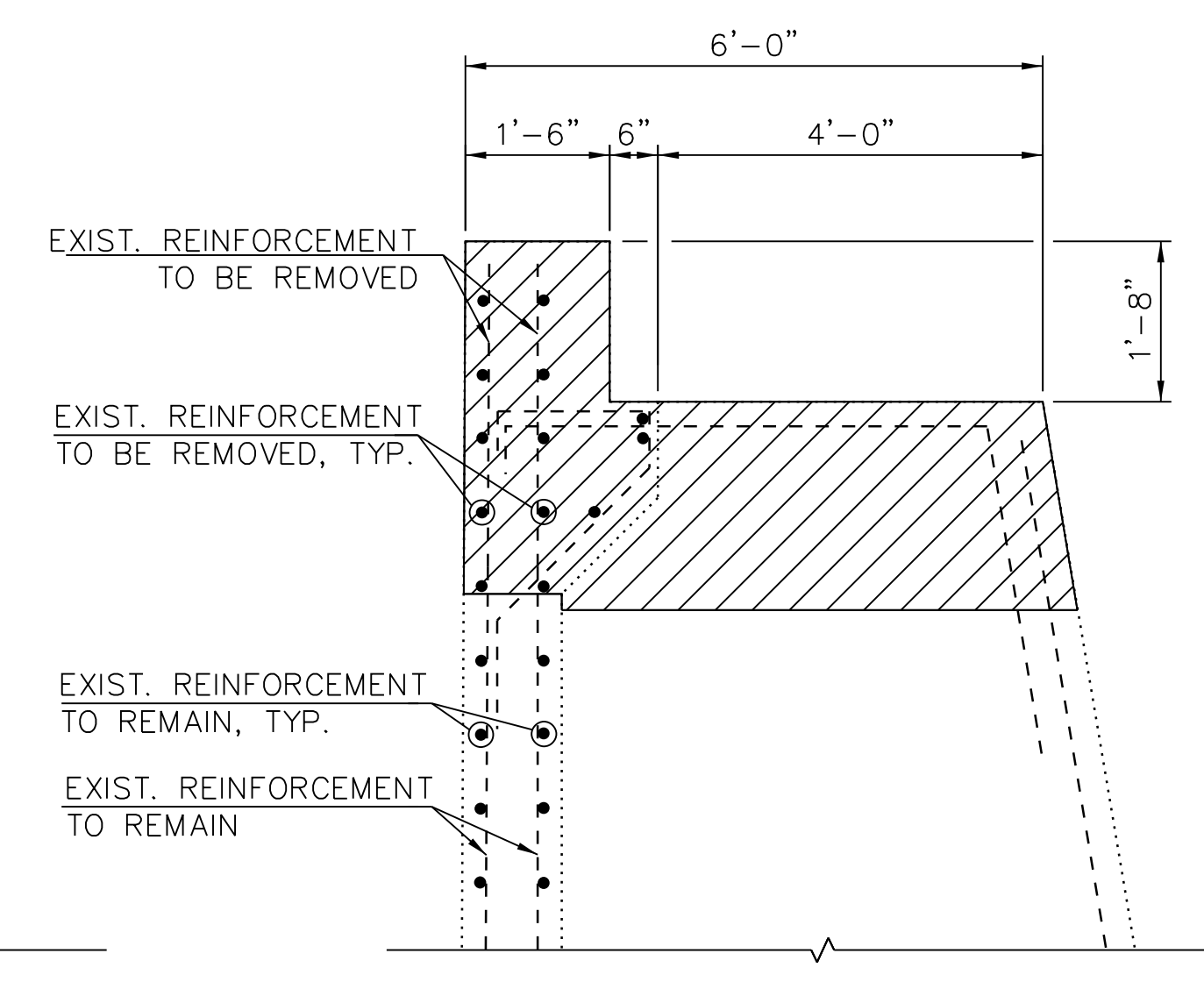
**A SECTION A**  
S-502 (TYPICAL WALL SECTION)  
SCALE: N.T.S.

**B SECTION B**  
S-502 SCALE: N.T.S.

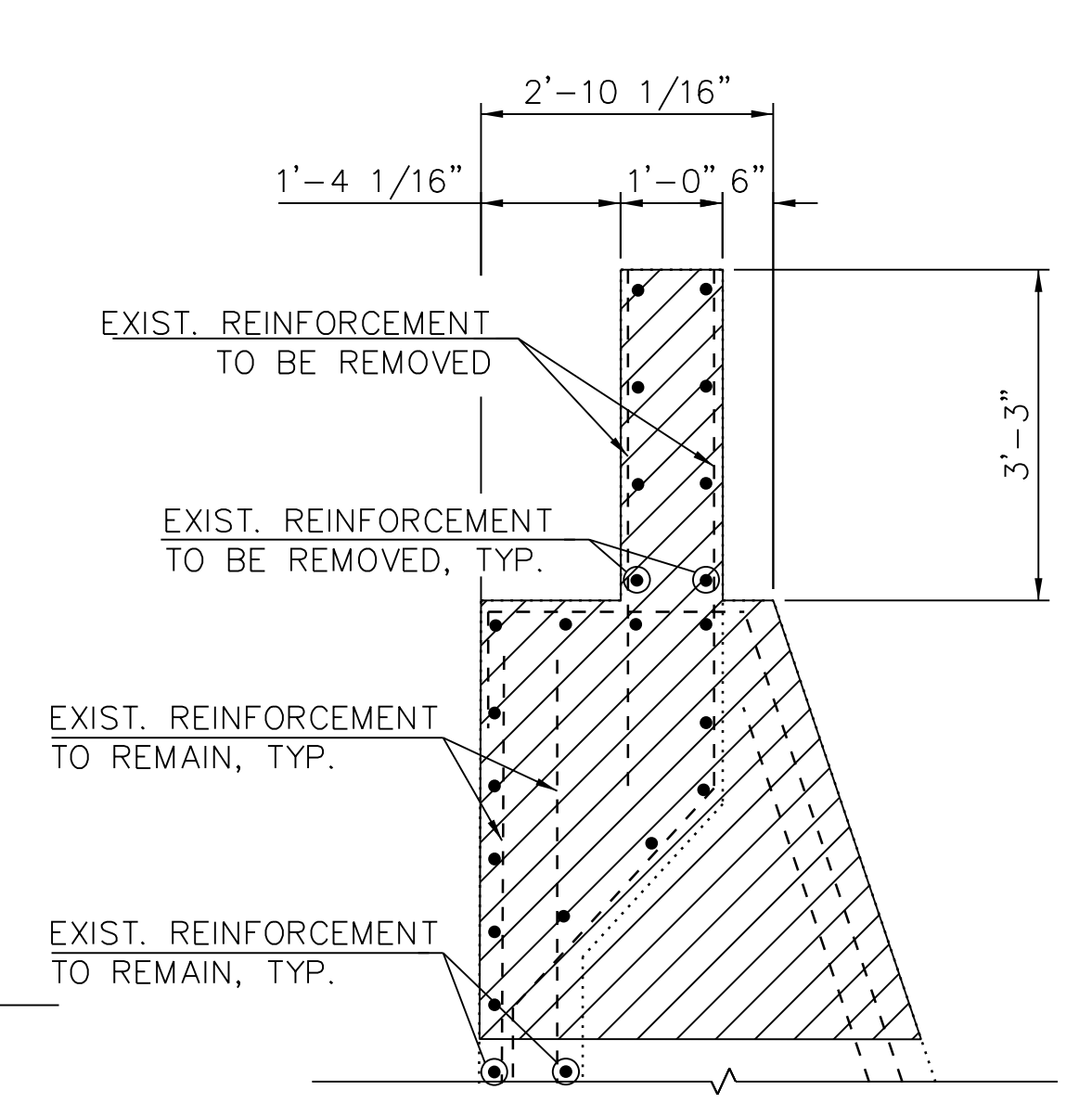
**C SECTION C**  
S-502 (AT COMMUTER BRIDGE)  
SCALE: N.T.S.



**1 DETAIL 1**  
SCALE: N.T.S.



**2 DETAIL 2**  
SCALE: N.T.S.

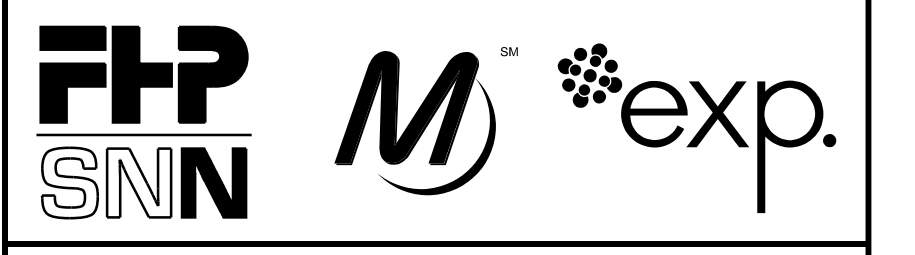


**3 DETAIL 3**  
SCALE: N.T.S.

**NOTES**

- EXISTING REINFORCEMENT EXTENDED INTO NEW CONCRETE SHALL BE CLEANED, STRAIGHTENED, AND INCORPORATED INTO THE NEW CONSTRUCTION.
- ANY REINFORCEMENT BARS THAT ARE DAMAGED DURING CONCRETE REMOVAL OPERATIONS SHALL BE REPAIRED OR REPLACED USING AN APPROVED BAR SPlicer OR ANCHORAGE SYSTEM.
- SAWCUT WALL TO PROVIDE CLEAN BEARING SURFACE FOR MOMENT SLABS. DAMAGED CONCRETE TO BE REPAIRED WITH NON-SHRINK GROUT AT NO ADDITIONAL COST.

LEGEND:  
 CONCRETE REMOVAL



**CHICAGO TRANSIT AUTHORITY ENGINEERING**

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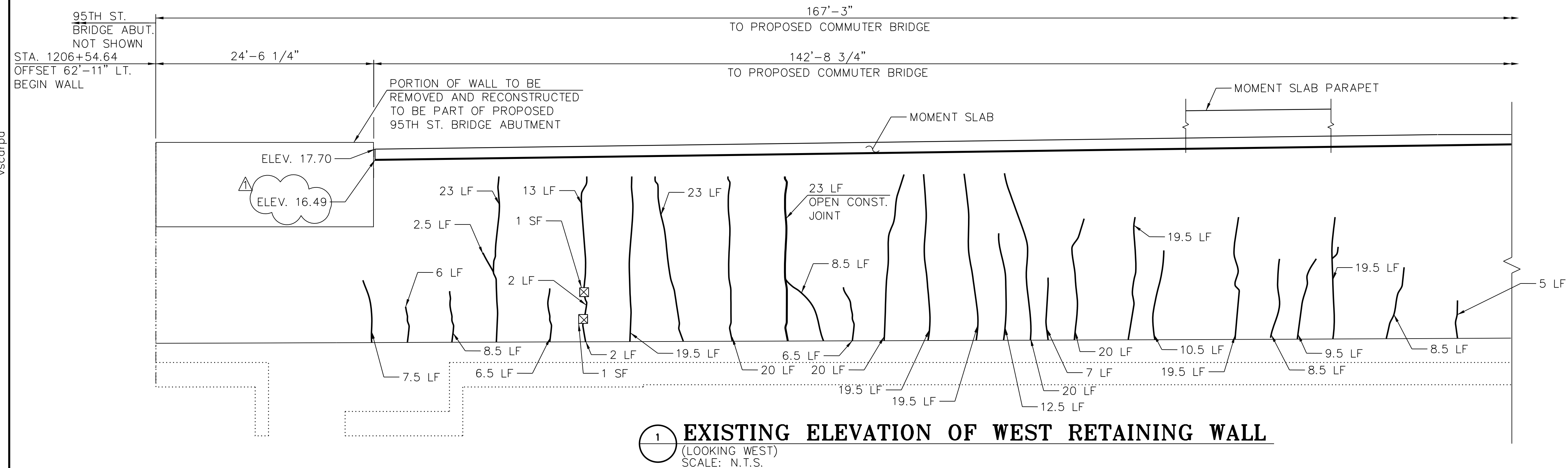
LOCATION IDENTIFIER: **DR095**

EXISTING WEST RETAINING WALL  
 REMOVAL SECTIONS & DETAILS

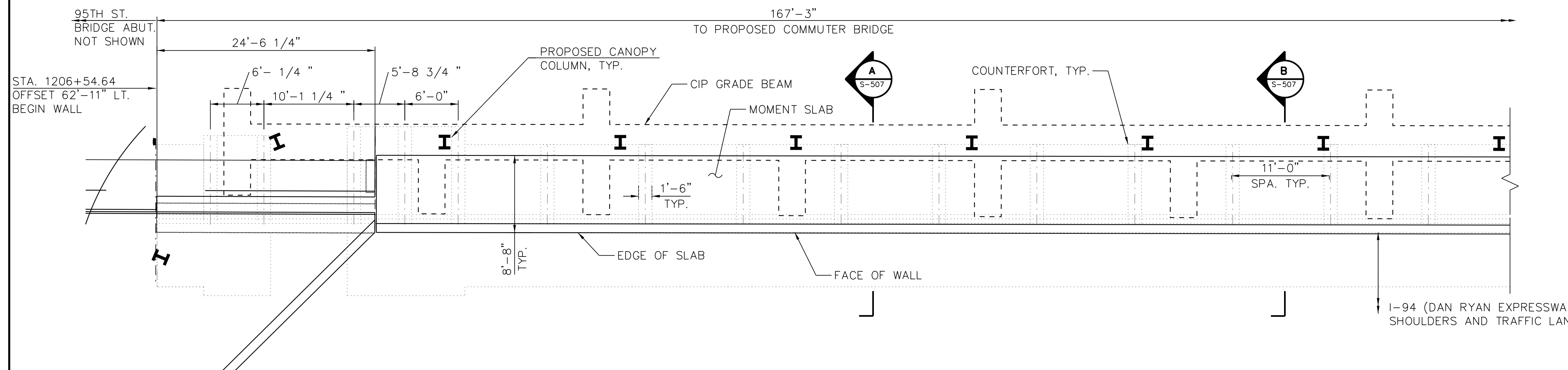
**EXW-S-503**

FILE NAME = 070\_DR095\_S-503.dgn  
 PLOT DATE = 12/14/2016

vscorppa



**1 EXISTING ELEVATION OF WEST RETAINING WALL**  
(LOOKING WEST)  
SCALE: N.T.S.



**2 EXISTING WEST RETAINING WALL PLAN**  
SCALE: N.T.S.

NOTES:

- FOR SECTIONS A AND B, SEE SHEET EXW-S-507.
- FOR PORTION OF WALL TO BE REMOVED AND RECONSTRUCTED TO BE PART OF PROPOSED COMMUTER BRIDGE ABUTMENT, SEE SHEETS EXW-S-505 AND EXW-S-507.
- FOR STRUCTURAL REPAIR OF CONCRETE DETAILS, SEE SHEET 95TH-S-150A.
- FOR EXPANSION JOINT DETAILS, SEE SHEET 95TH-S-122.
- THE ENTIRE EXPOSED FACE OF WALLS SHALL HAVE WATERPROOF COATING APPLIED AS PER STRUCTURAL REPAIR OF CONCRETE SECTION 03 74 00.
- FOR PROPOSED CANOPY FOUNDATIONS AND GRADE BEAMS, SEE S-1300 SERIES.
- FOR MOMENT SLAB DETAILS, SEE S-1403A.

- LEGEND:
- STRUCTURAL REPAIR OF CONCRETE (DEPTH EQUAL TO OR LESS THAN 5 INCHES)
  - STRUCTURAL REPAIR OF CONCRETE (DEPTH GREATER THAN 5 INCHES)
  - EPOXY SEAL (WIDTH >0.06")
  - SF - SQUARE FOOT
  - LF - LINEAR FOOT



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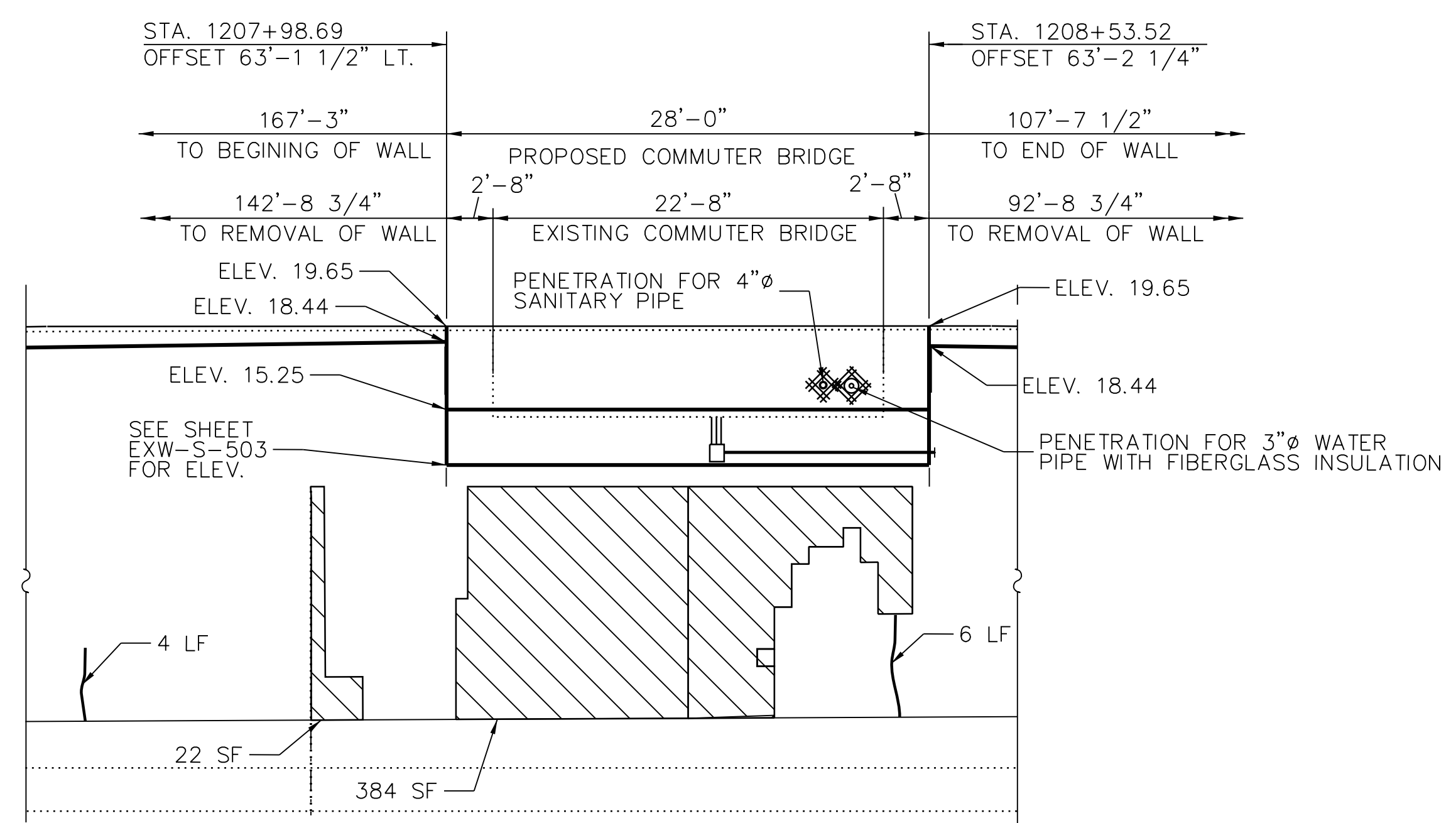
LOCATION IDENTIFIER: **DR095**

EXISTING WEST RETAINING WALL PLAN & ELEVATION (SHEET 1 OF 3)

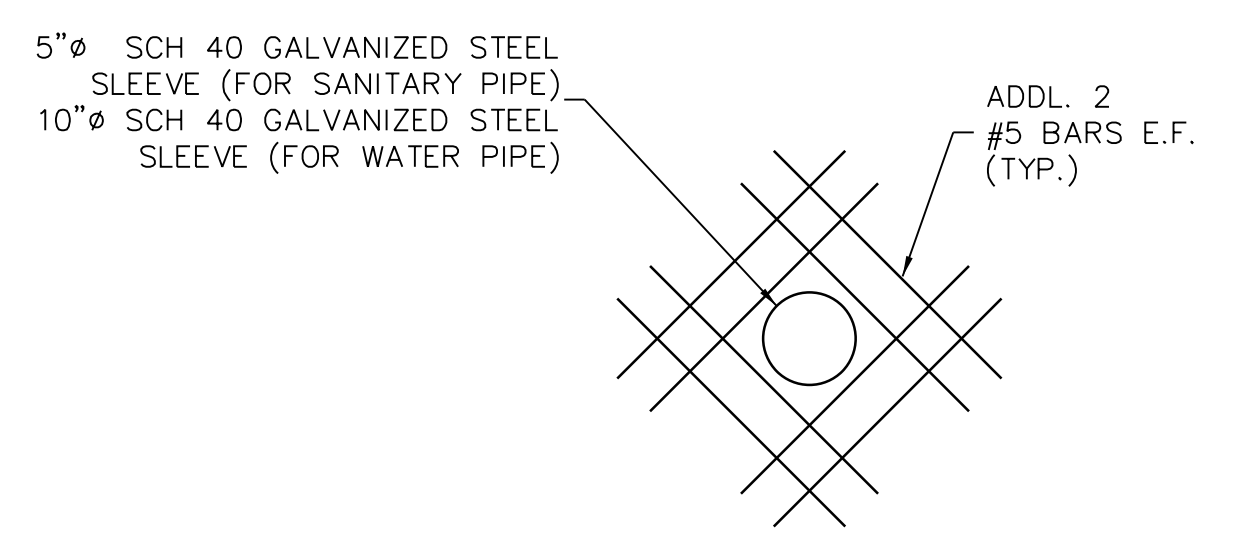
**EXW-S-504**

FILE NAME = 070\_DR095\_S-504.dgn  
PLOT DATE = 12/14/2016

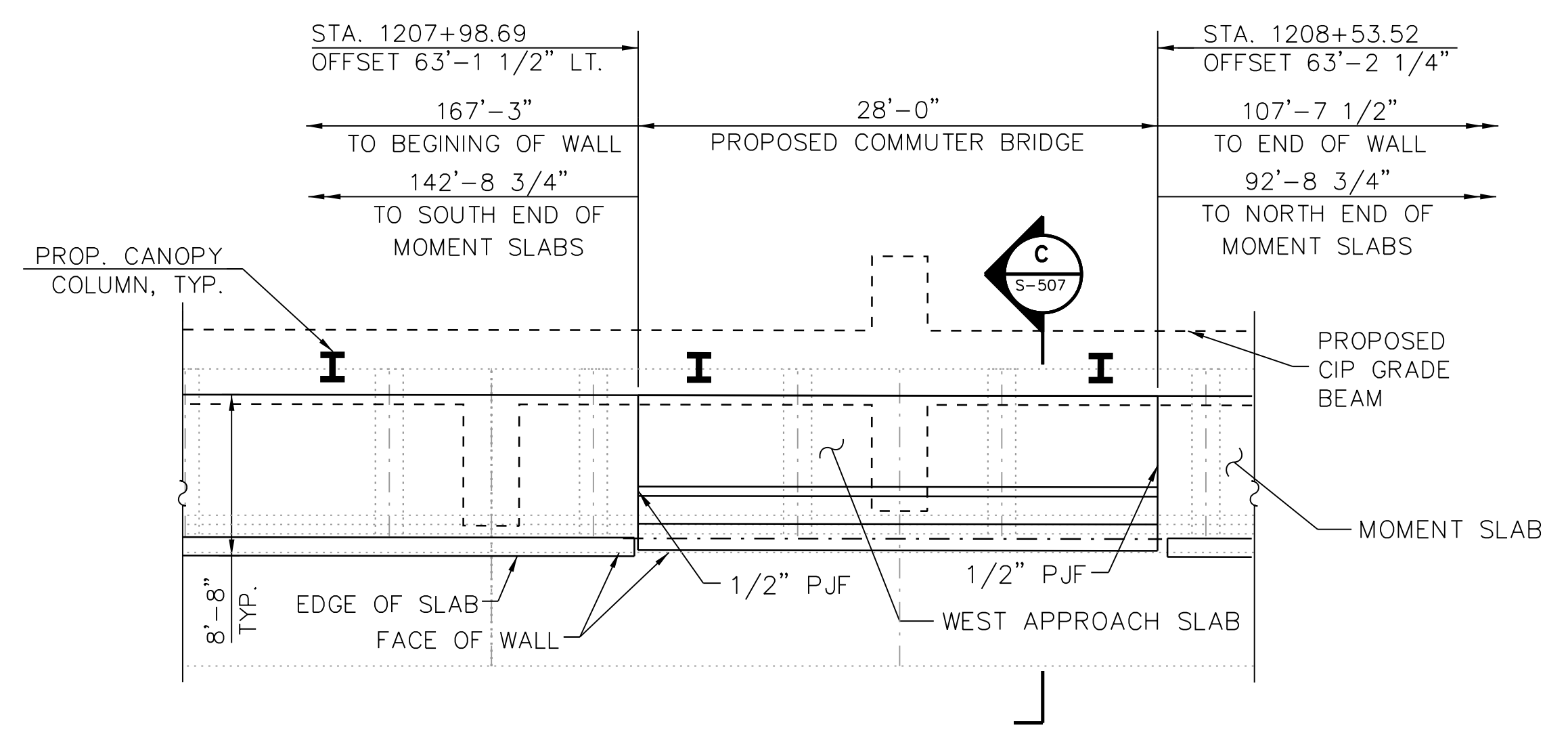
vscorppa



**1 EXISTING ELEVATION OF WEST RETAINING WALL**  
(LOOKING WEST)  
SCALE: N.T.S.



**3 PENETRATION DETAIL**  
SCALE: N.T.S.  
NOTE: GROUT BETWEEN PIPE AND SLEEVE AS REQUIRED.



**2 EXISTING WEST RETAINING WALL PLAN**  
SCALE: N.T.S.

- NOTES:
- FOR STRUCTURAL REPAIR OF CONCRETE DETAILS, SEE SHEET 95TH-S-150A.
  - THE ENTIRE EXPOSED FACE OF WALLS SHALL HAVE WATERPROOF COATING APPLIED AS PER STRUCTURAL REPAIR OF CONCRETE SECTION 03 74 00.
  - SEE SHEET C-200 TO C-203 FOR PENETRATION LOCATIONS.
  - FOR PROPOSED CANOPY FOUNDATIONS AND GRADE BEAMS, SEE S-1300 SERIES.
  - FOR MOMENT SLAB DETAILS, SEE S-1403A.

- LEGEND:
- STRUCTURAL REPAIR OF CONCRETE (DEPTH EQUAL TO OR LESS THAN 5 INCHES)
  - STRUCTURAL REPAIR OF CONCRETE (DEPTH GREATER THAN 5 INCHES)
  - EPOXY SEAL (WIDTH >0.06")
  - SF - SQUARE FOOT
  - LF - LINEAR FOOT



**SENSITIVE SECURITY INFORMATION**  
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DRAWING SCALE IS NOT GUARANTEED. CTA ASSUMES NO RISK OF LIABILITY FOR ERRORS CAUSED, DIRECTLY OR INDIRECTLY, BY SCALING OF THIS DRAWING.

**95th Terminal Improvement**  
15 West 95th Street (South Terminal)  
Chicago, IL 60628

IN CHARGE	S. KADERBEK
APPROVED BY	D.ZROKA
CHECKED BY	P.MAID
DESIGNED BY	L.SOMMER
DRAWN BY	T.SLEDGE
PROJECT NO.	2012-0021
FILE NAME	File Name

MARK	DATE	DESCRIPTION
	2/9/17	ISSUED FOR CONSTRUCTION

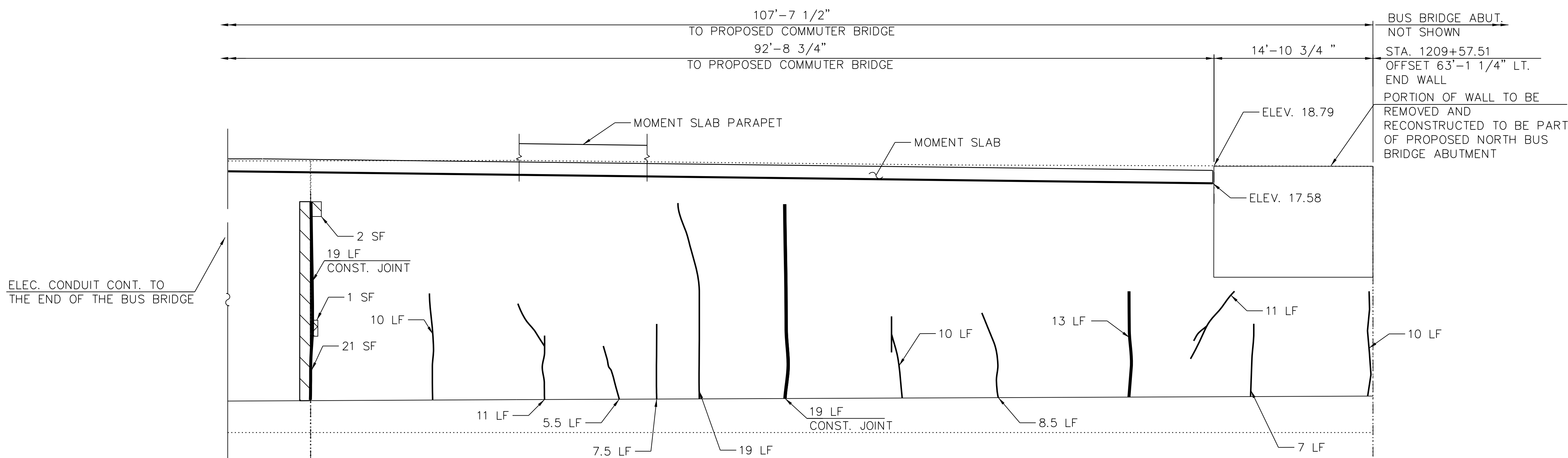
LOCATION IDENTIFIER: **DR095**

EXISTING WEST RETAINING WALL PLAN & ELEVATION (SHEET 2 OF 3)

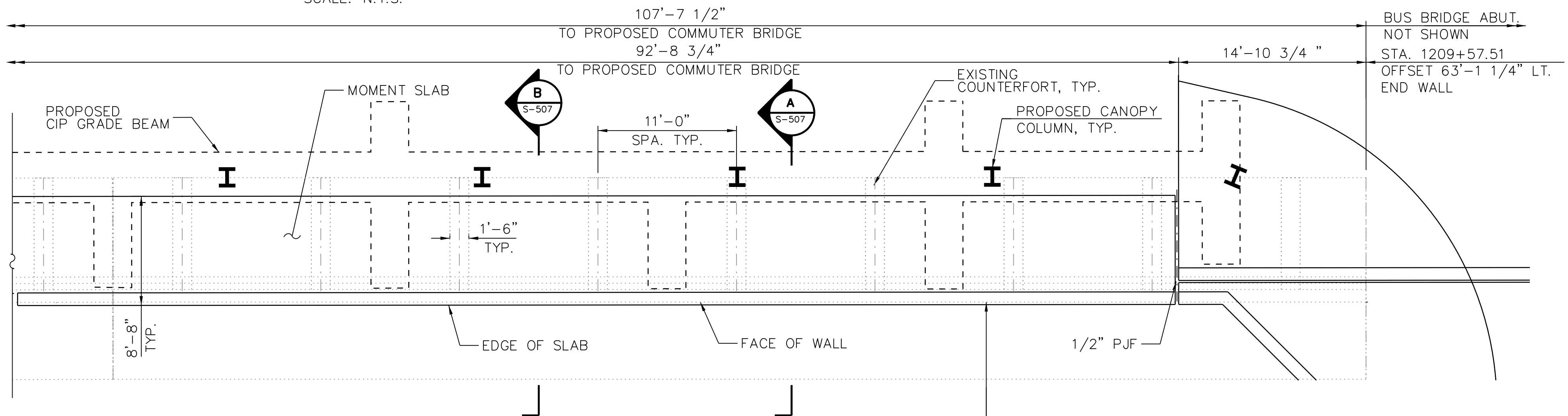
**EXW-S-505**

FILE NAME = 070\_DR095\_S-505.dgn  
PLOT DATE = 11/11/2016

vscorppa



**1 EXISTING ELEVATION OF WEST RETAINING WALL**  
(LOOKING WEST)  
SCALE: N.T.S.



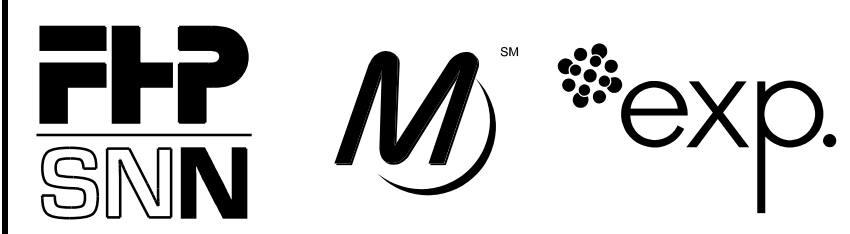
**2 EXISTING WEST RETAINING WALL PLAN**  
SCALE: N.T.S.

**NOTES:**

1. FOR PORTION OF WALL TO BE REMOVED AND RECONSTRUCTED TO BE PART OF PROPOSED NORTH BUS BRIDGE ABUTMENT, SEE NORTH BUS BRIDGE PLANS.
2. FOR PORTION OF WALL TO BE REMOVED AND RECONSTRUCTED TO BE PART OF PROPOSED COMMUTER BRIDGE ABUTMENT, SEE SHEETS EXW-S-505 AND EXW-S-507.
3. FOR STRUCTURAL REPAIR OF CONCRETE DETAILS, SEE SHEET 95TH-S-150A.
4. FOR EXPANSION JOINT DETAILS, SEE SHEET 95TH-S-122.
5. THE ENTIRE EXPOSED FACE OF WALLS SHALL HAVE WATERPROOF COATING APPLIED AS PER STRUCTURAL REPAIR OF CONCRETE SECTION 03 74 00.
6. FOR PROPOSED CANOPY FOUNDATIONS AND GRADE BEAMS, SEE S-1300 SERIES.
7. FOR MOMENT SLAB DETAILS, SEE S-1403A.

**LEGEND:**

- STRUCTURAL REPAIR OF CONCRETE (DEPTH GREATER THAN 5 INCHES)
- STRUCTURAL REPAIR OF CONCRETE (DEPTH EQUAL TO OR LESS THAN 5 INCHES)
- EPOXY SEAL (WIDTH >0.06")
- SF - SQUARE FOOT
- LF - LINEAR FOOT



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PROJECT NO.	2012-0021
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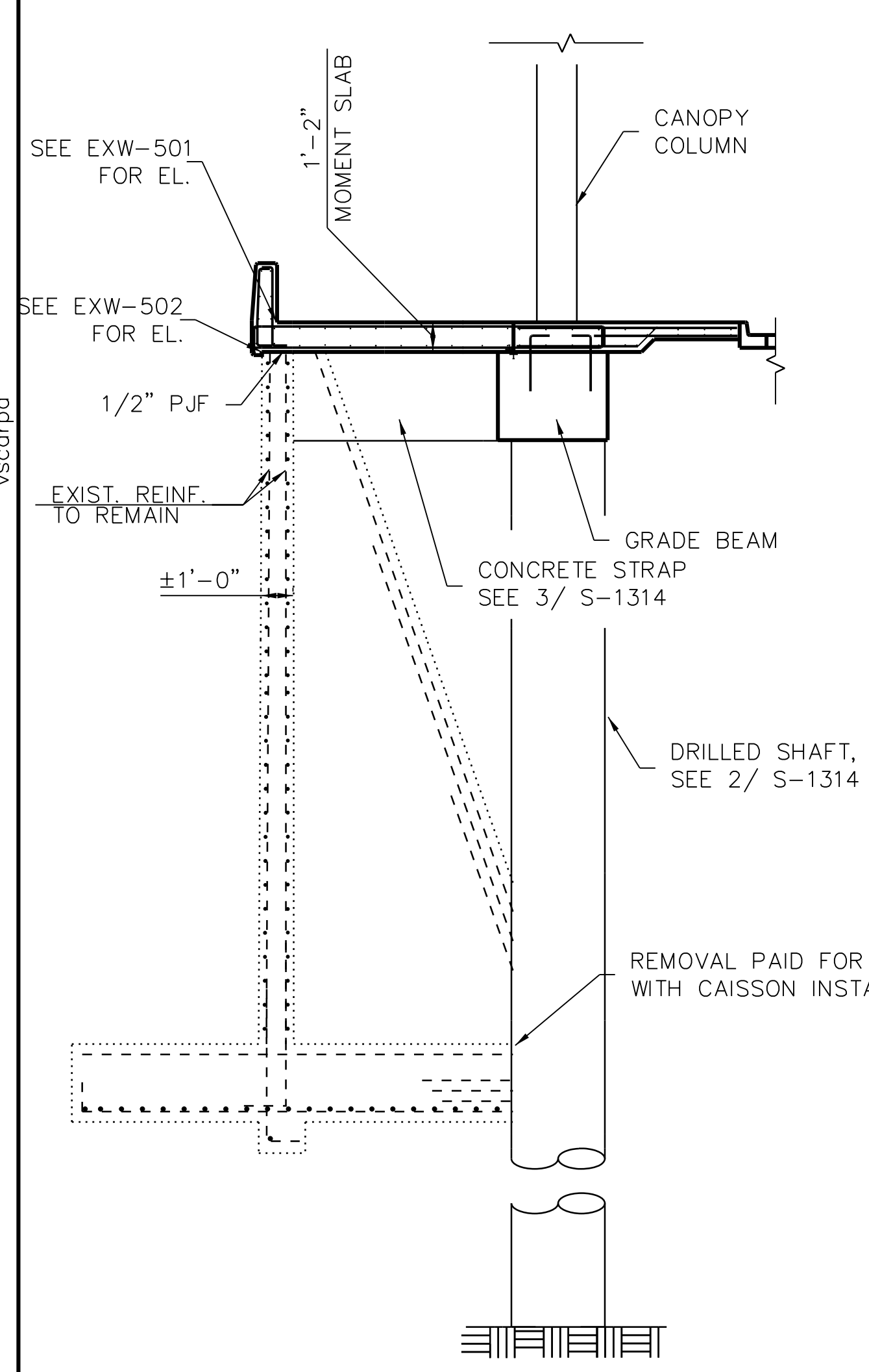
LOCATION IDENTIFIER: **DR095**

EXISTING WEST RETAINING WALL PLAN & ELEVATION (SHEET 3 OF 3)

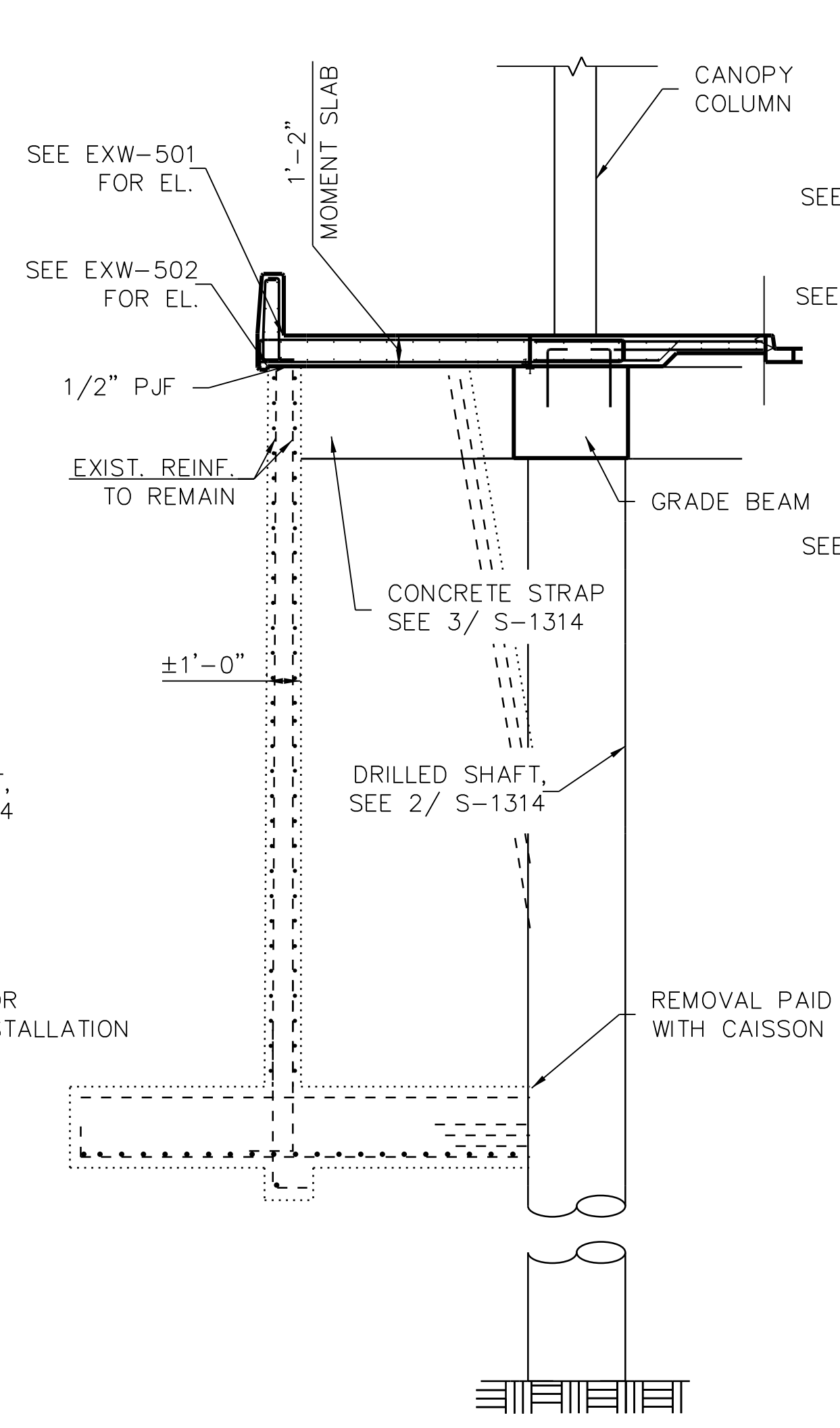
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PLOT DATE = 12/14/2016

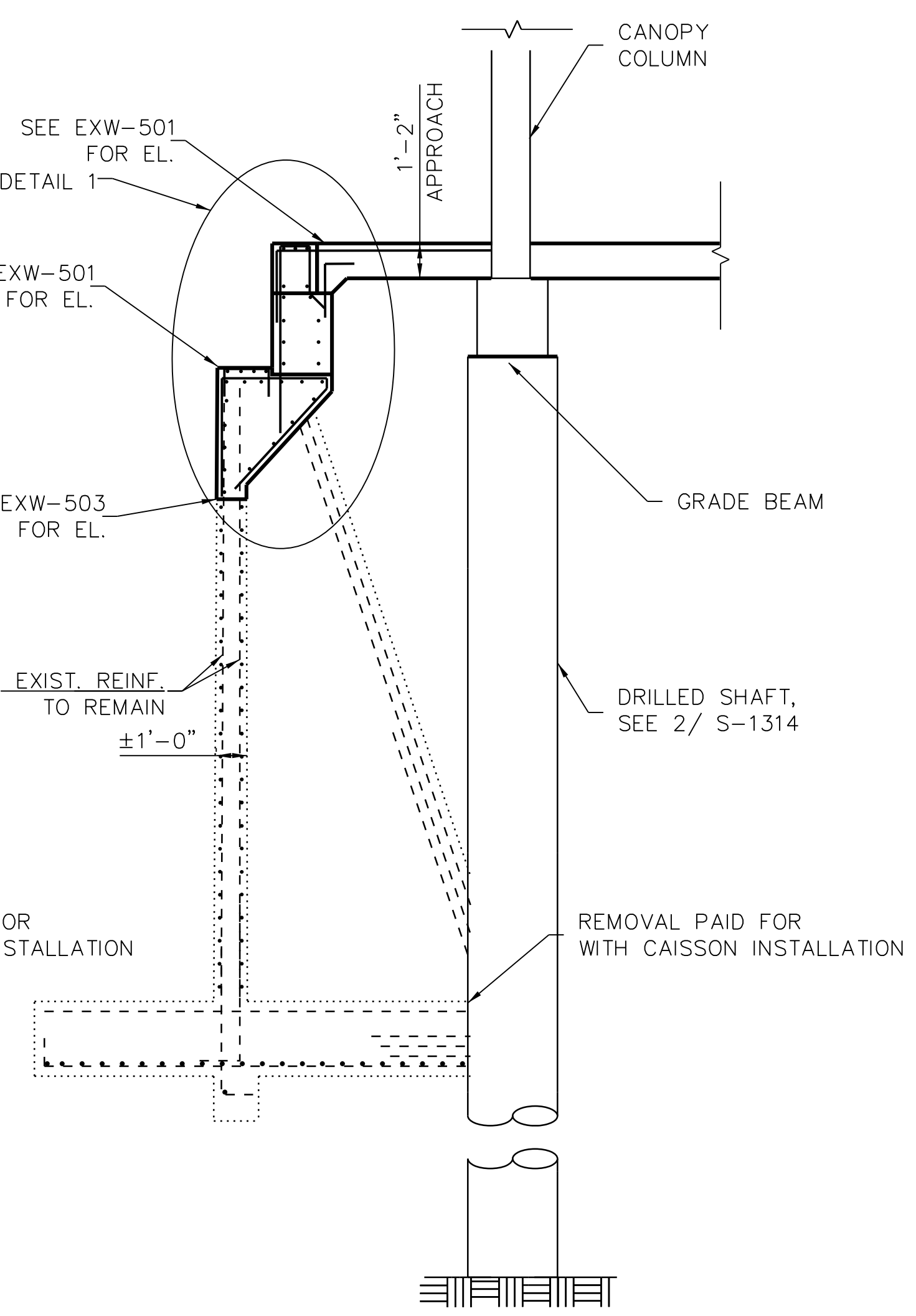
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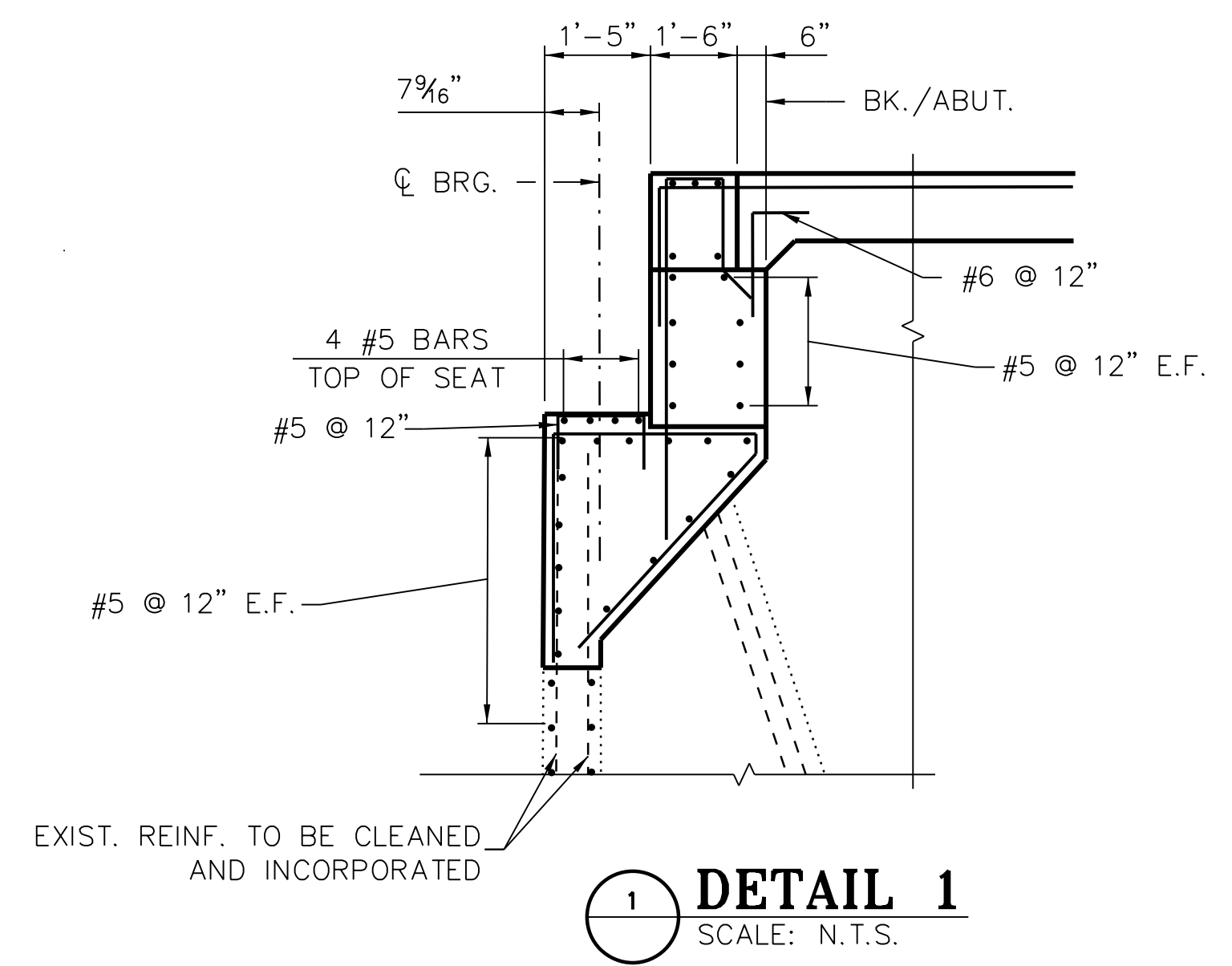
**A** SECTION A  
 AT EXISTING RETAINING WALL  
 SCALE: N.T.S.



**B** SECTION B  
 AT EXISTING RETAINING WALL  
 SCALE: N.T.S.



**C** SECTION C  
 AT EXISTING COMMUTER BRIDGE  
 SCALE: N.T.S.



**1** DETAIL 1  
 SCALE: N.T.S.

**NOTE:**

1. FOR SIDEWALK, PARAPET AND FENCE, SEE CIVIL, BUILDING AND ARCHITECTURAL PLANS.
2. FOR COMMUTER BRIDGE AND RETAINING WALL EXCAVATION AND CELLULAR CONCRETE FILL, SEE SHEETS EXE-S-408 AND EXE-S-409.
3. FOR MOMENT SLAB DETAILS, SEE S-1403A.
4. COORDINATE CAST IN ANCHORS WITH COMMUTER BRIDGE DRAWINGS.



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DESIGNED BY	L.SOMMER
DRAWN BY	T.SLEDGE
PROJECT NO.	2012-0021
FILE NAME	File Name

MARK	DATE	DESCRIPTION
	2/9/17	ISSUED FOR CONSTRUCTION

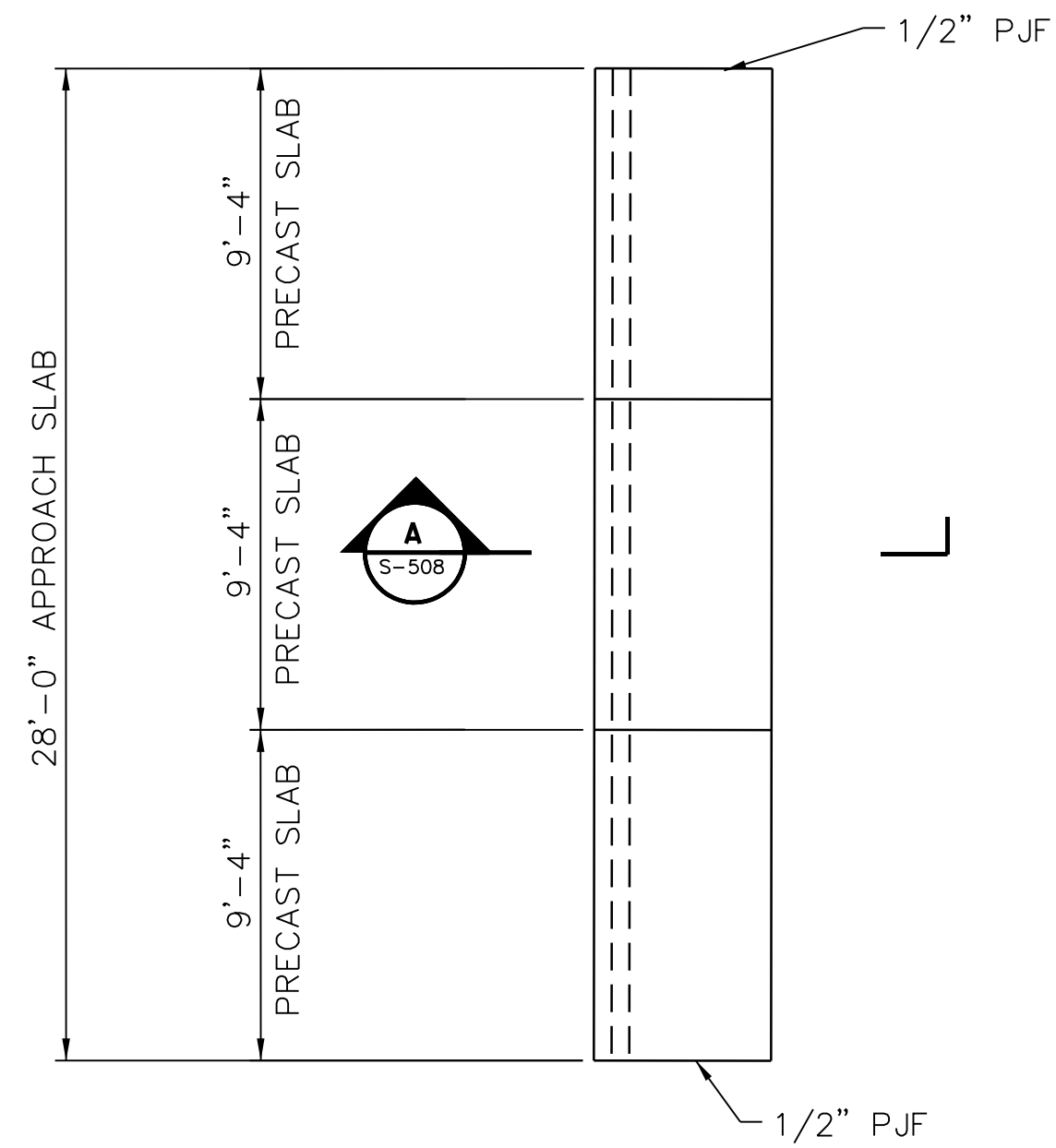
LOCATION IDENTIFIER: **DR095**

EXISTING WEST RETAINING WALL  
 SECTIONS & DETAILS

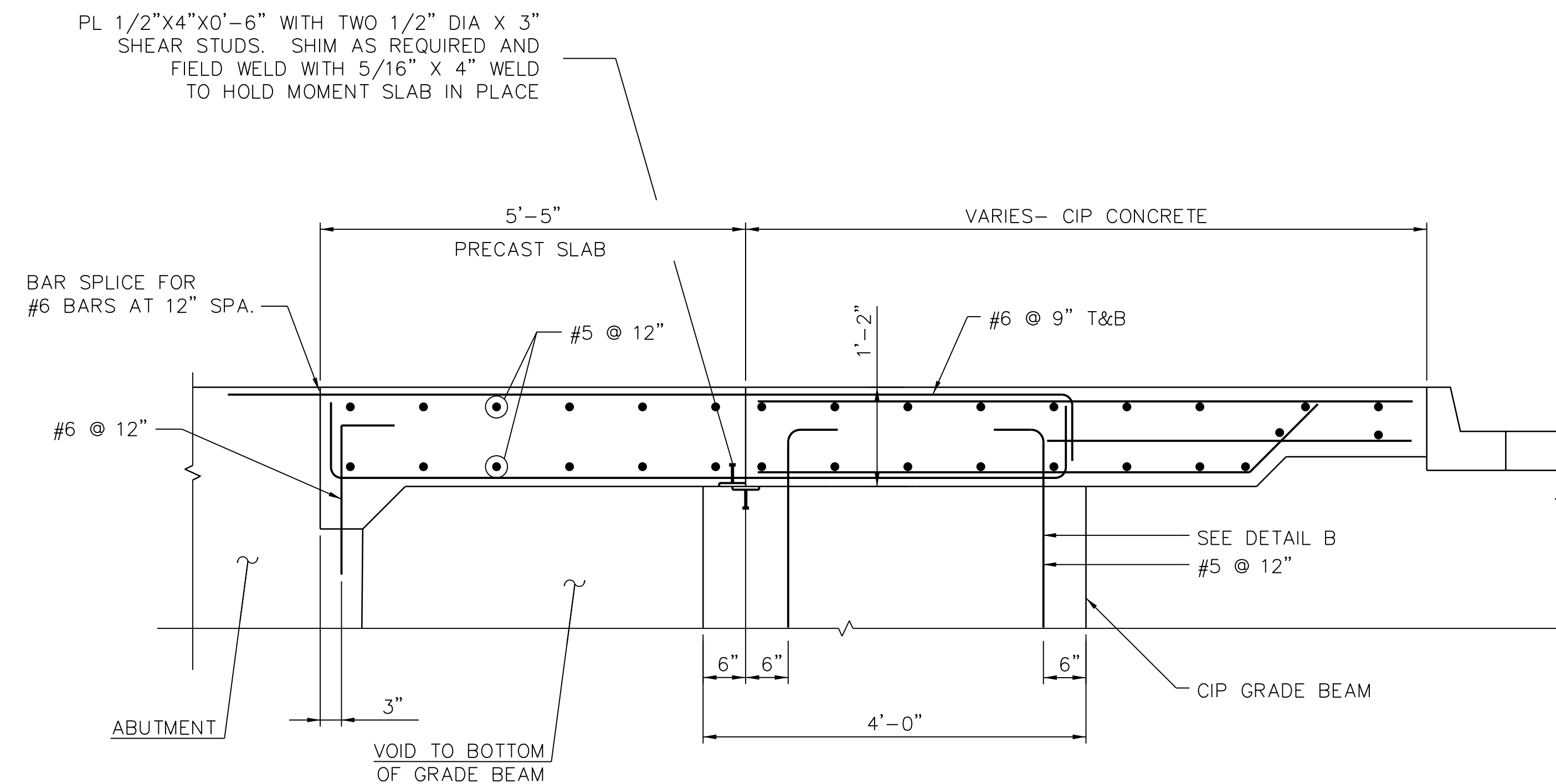
**EXW-S-507**

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 PLOT DATE = 12/14/2016

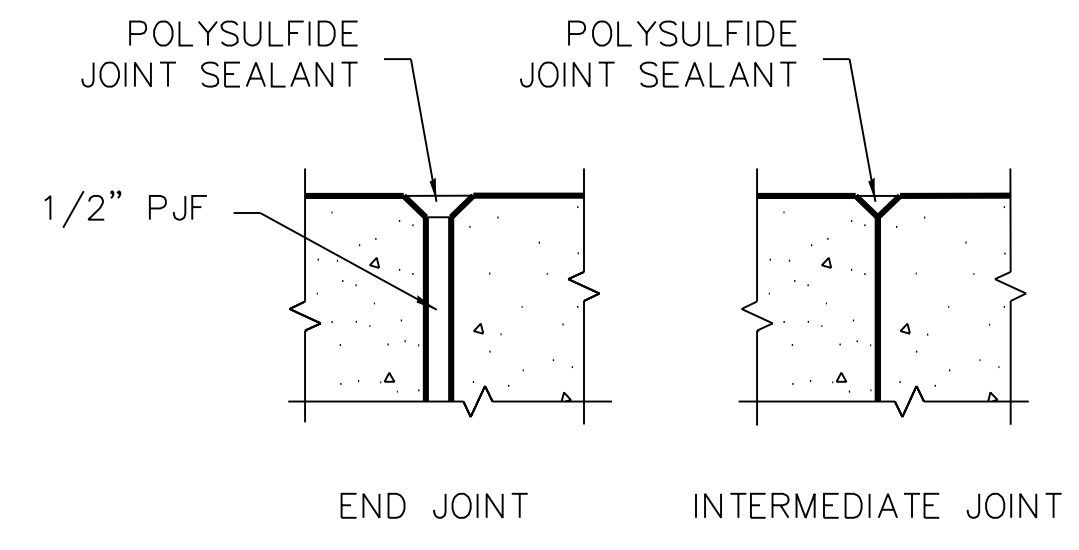
vscorppa



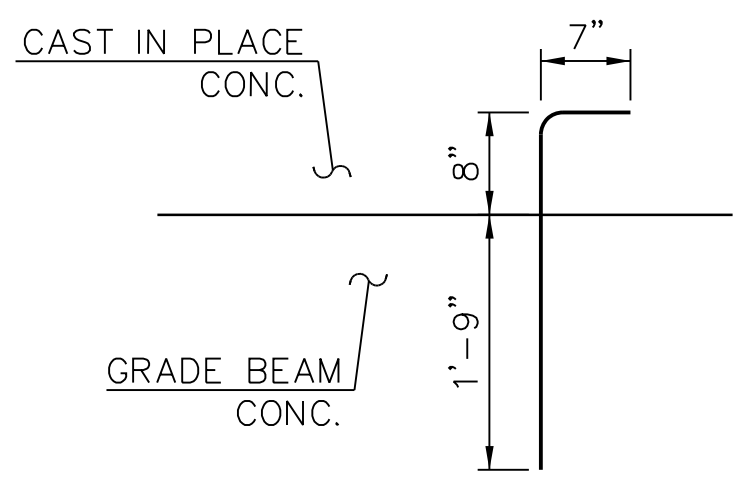
1 PLAN



A APPROACH SLAB SECTION  
SCALE: N.T.S.



C JOINTING DETAIL



B DETAIL B  
SCALE: N.T.S.



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PROJECT NO.	2012-0021
FILE NAME	File Name

MARK	DATE	DESCRIPTION
	2/9/17	ISSUED FOR CONSTRUCTION

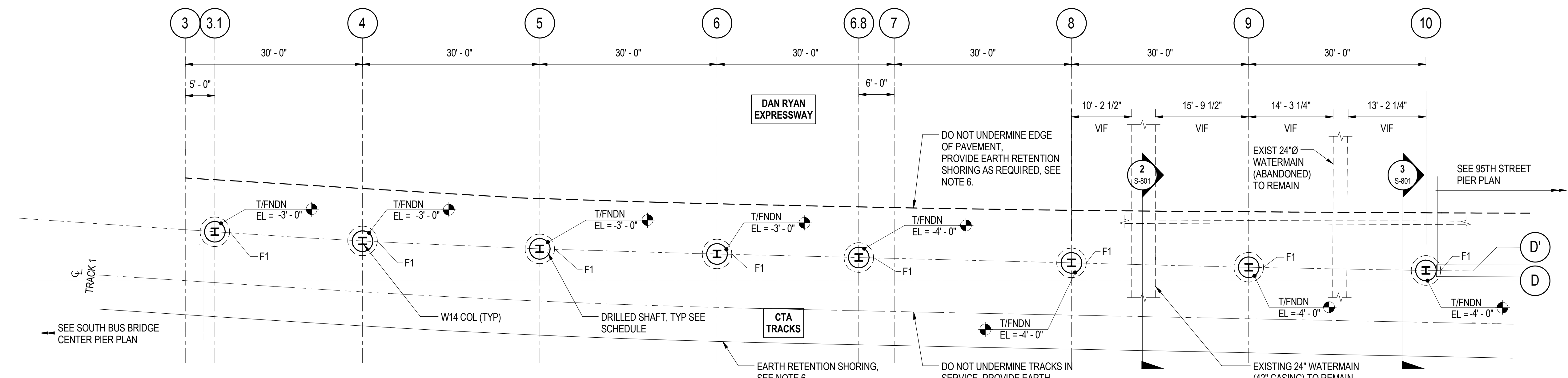
LOCATION IDENTIFIER: **DR095**

COMMUTER BRIDGE  
 WEST APPROACH SLAB

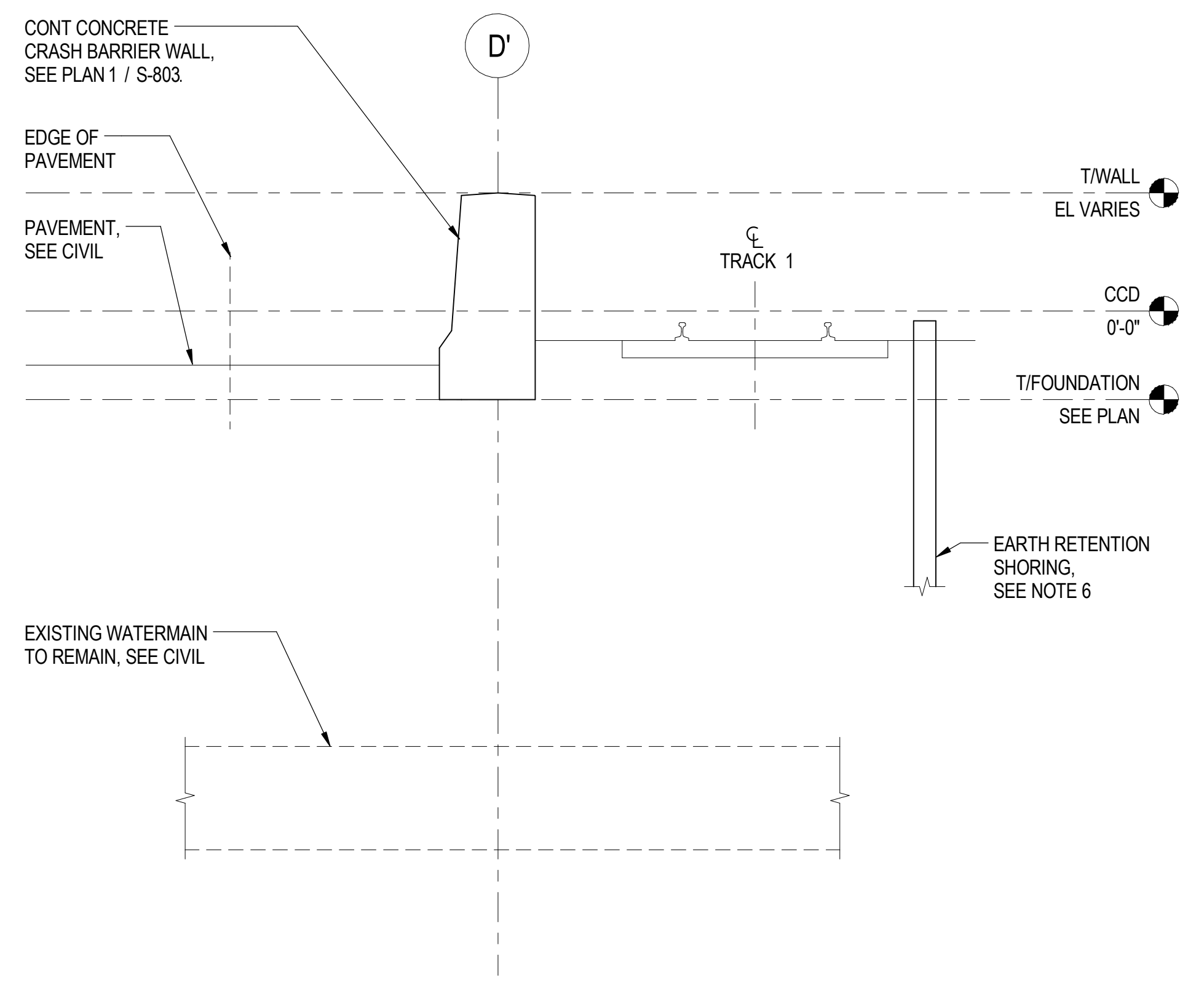
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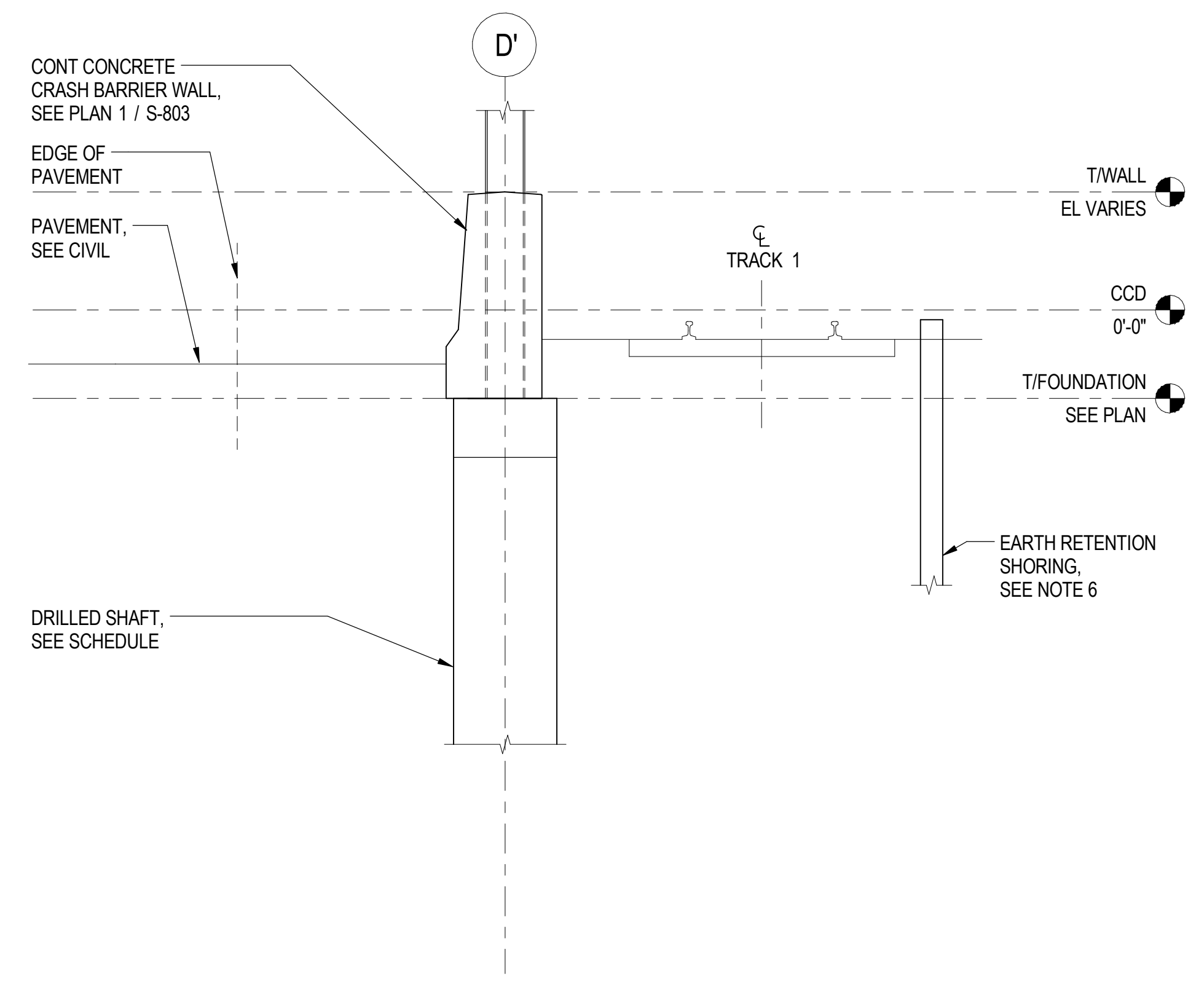
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**1 FOUNDATION PLAN**  
S-801 SCALE: 1" = 10'-0"  
N

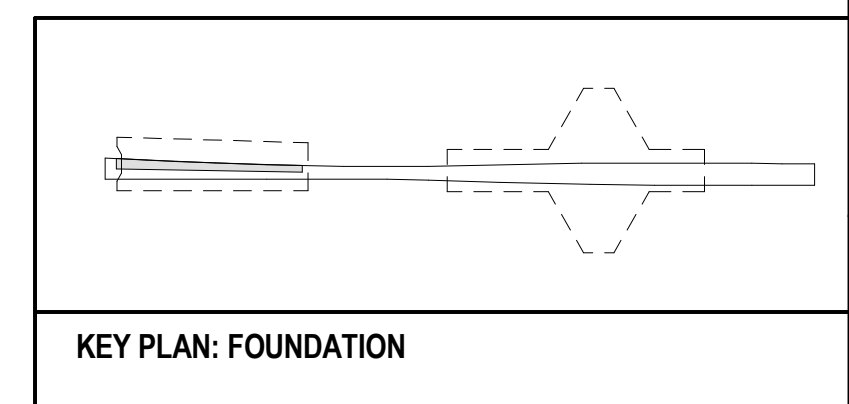


**2 FOUNDATION SECTION**  
S-801 SCALE: 1/4" = 1'-0"



**3 FOUNDATION SECTION**  
S-801 SCALE: 1/4" = 1'-0"

- SHEET NOTES:**
- FOR GENERAL PROJECT NOTES, SEE S-000 SERIES.
  - FOR TYPICAL DETAILS, SEE S-1100 SERIES.
  - COORDINATE ALL EXISTING UTILITIES SHOWN WITH CIVIL DRAWINGS. CONTRACTOR TO FIELD SURVEY ALL UTILITIES NOTED FOR RELOCATION PRIOR TO PROCEEDING WITH DRILLED SHAFT INSTALLATION. CONTRACTOR TO REPORT ANY DISCREPANCIES TO THE DOR FOR REVIEW. REFER TO CIVIL DRAWINGS FOR ALL UTILITIES NOT SHOWN.
  - REFER TO S-1103 FOR DRILLED SHAFT AND GRADE BEAM SIZE, ELEVATION AND REINFORCEMENT INFORMATION.
  - SECTIONS SHOWN ON THIS PLAN ARE DIAGRAMMATICAL ONLY FOR UTILITY AND CONSTRUCTION PHASING COORDINATION. TOP OF WALL ELEVATION VARIES. REFER TO S-1104 FOR ALL TOP OF WALL ELEVATIONS NOT INDICATED.
  - THE SUPPORT OF EXCAVATION SYSTEM IS THE SOLE RESPONSIBILITY OF THE CONTRACTOR. CONTRACTOR SHALL COORDINATE ALL WORK WITH THE EXISTING UNDERGROUND UTILITIES TO REMAIN. INTERRUPTION OF THE SHEET PILING MAY BE REQUIRED.
  - "Fx" INDICATES CAISSON MARK. SEE SCHEDULE ON S-1103.
  - ELEVATIONS ARE BASED ON THE CHICAGO CITY DATUM (CCD).



**SENSITIVE SECURITY INFORMATION**  
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**95th Terminal Improvement**  
15 West 95th Street (South Terminal)  
Chicago, IL 60628

<b>IN CHARGE</b>	J. JAKALSKI
<b>APPROVED BY</b>	J. JAKALSKI
<b>CHECKED BY</b>	R.S. NAIR
<b>DESIGNED BY</b>	G. FORKAN
<b>DRAWN BY</b>	M. DONAHUE
<b>PROJECT NO.</b>	2012-0021.02
<b>FILE NAME</b>	DR095_S-StationFoundation

MARK	DATE	DESCRIPTION
	10/6/16	ISSUED FOR CONSTRUCTION

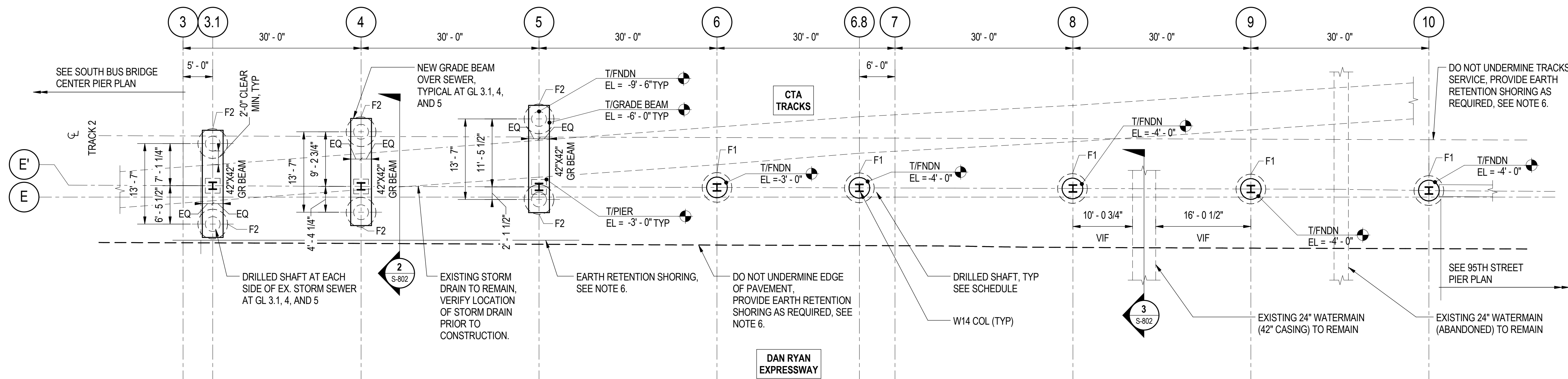
LOCATION IDENTIFIER: **DR095**  
**SOUTH TERMINAL FOUNDATION PLAN - AREA A & B**

**S-801**

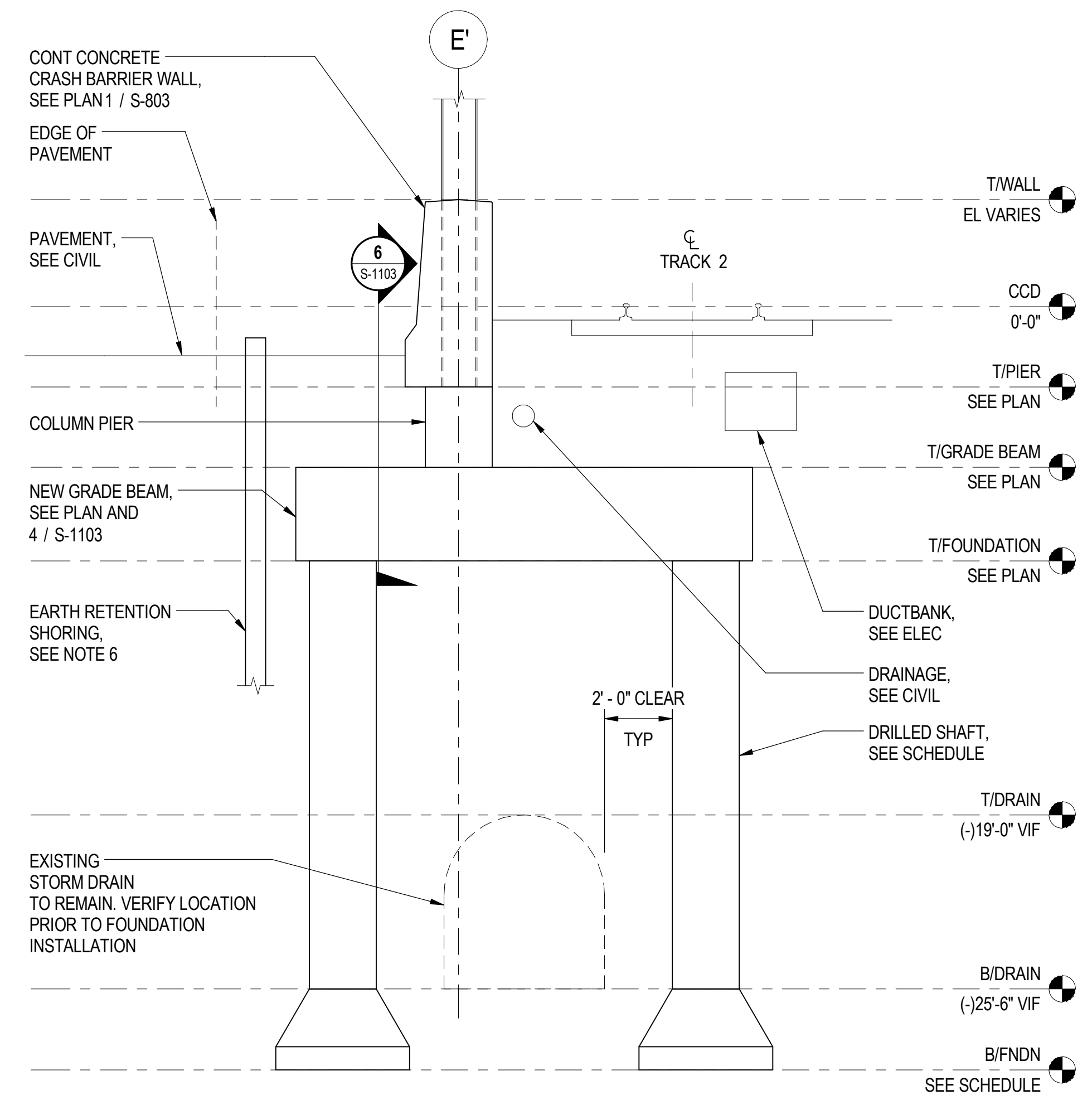


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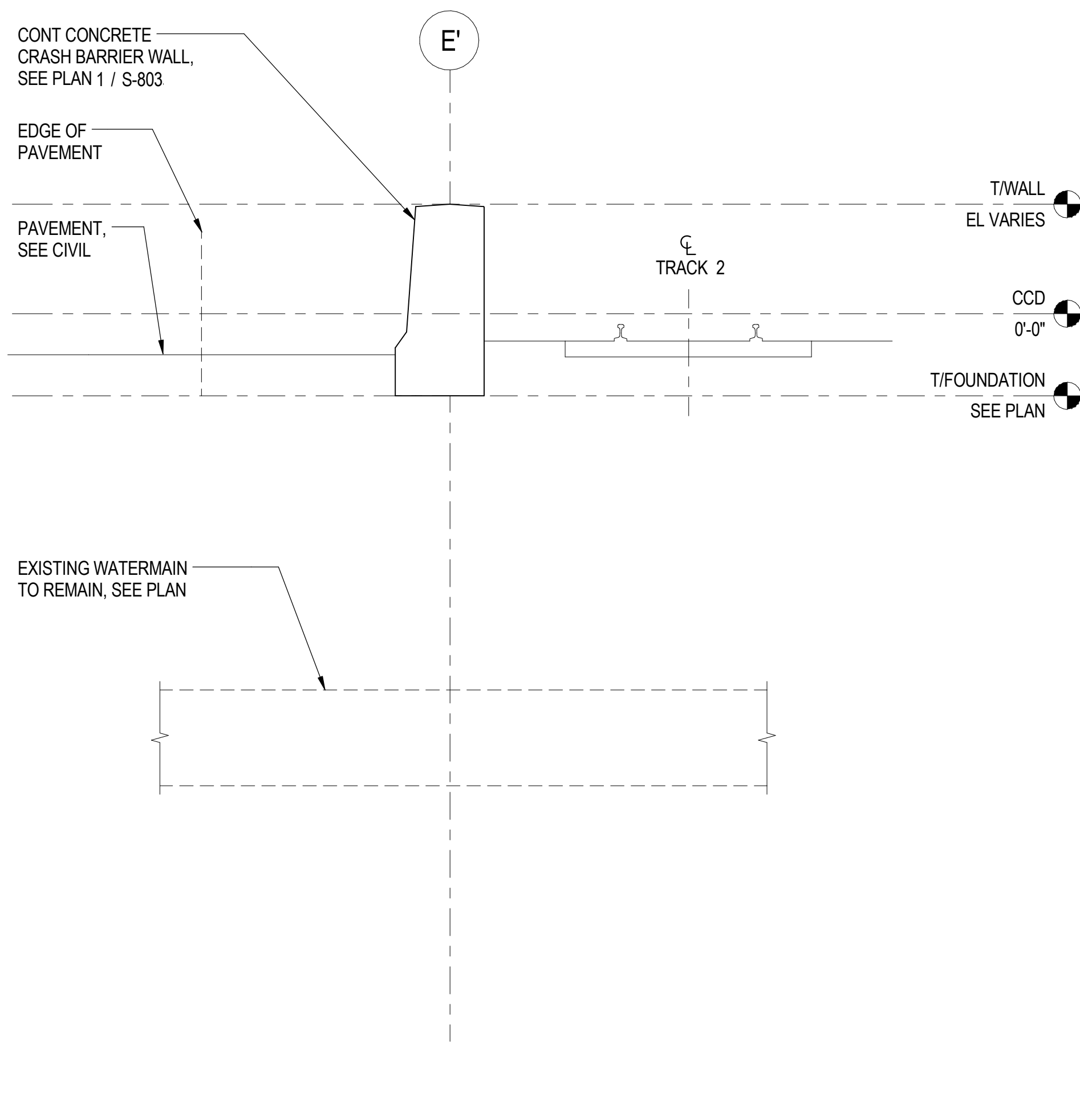
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**1 FOUNDATION PLAN**  
S-802 SCALE: 1" = 10'-0"



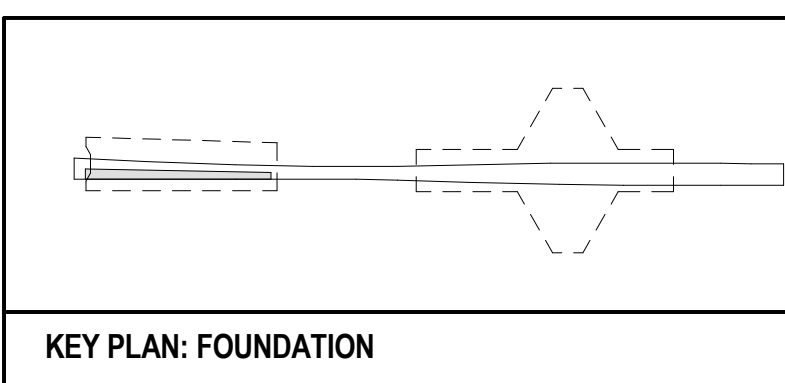
**2 FOUNDATION SECTION**  
S-802 SCALE: 1/4" = 1'-0"



**3 FOUNDATION SECTION**  
S-802 SCALE: 1/4" = 1'-0"

**SHEET NOTES:**

- FOR GENERAL PROJECT NOTES, SEE S-000 SERIES.
- FOR TYPICAL DETAILS, SEE S-1100 SERIES.
- COORDINATE ALL EXISTING UTILITIES SHOWN WITH CIVIL DRAWINGS. CONTRACTOR TO FIELD SURVEY ALL UTILITIES NOTED FOR RELOCATION PRIOR TO PROCEEDING WITH DRILLED SHAFT INSTALLATION. CONTRACTOR TO REPORT ANY DISCREPANCIES TO THE DOR FOR REVIEW. REFER TO CIVIL DRAWINGS FOR ALL UTILITIES NOT SHOWN.
- REFER TO S-1103 FOR DRILLED SHAFT AND GRADE BEAM SIZE, ELEVATION AND REINFORCEMENT INFORMATION.
- SECTIONS SHOWN ON THIS PLAN ARE DIAGRAMMATICAL ONLY FOR UTILITY AND CONSTRUCTION PHASING COORDINATION. TOP OF WALL ELEVATION VARIES, REFER TO S-1104 FOR ALL TOP OF WALL ELEVATIONS NOT INDICATED.
- THE SUPPORT OF EXCAVATION SYSTEM IS THE SOLE RESPONSIBILITY OF THE CONTRACTOR. CONTRACTOR SHALL COORDINATE ALL WORK WITH THE EXISTING UNDERGROUND UTILITIES TO REMAIN. INTERRUPTION OF THE SHEET PILING MAY BE REQUIRED.
- "Fx" INDICATES CAISSON MARK. SEE SCHEDULE ON S-1103.
- ELEVATIONS ARE BASED ON THE CHICAGO CITY DATUM (CCD).



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APPROVED BY	J. JAKALSKI
CHECKED BY	R.S. NAIR
DESIGNED BY	G. FORKAN
DRAWN BY	M. DONAHUE
PROJECT NO.	2012-0021.02
FILE NAME	DR095_S-StationFoundation

MARK	DATE	DESCRIPTION
	10/6/16	ISSUED FOR CONSTRUCTION

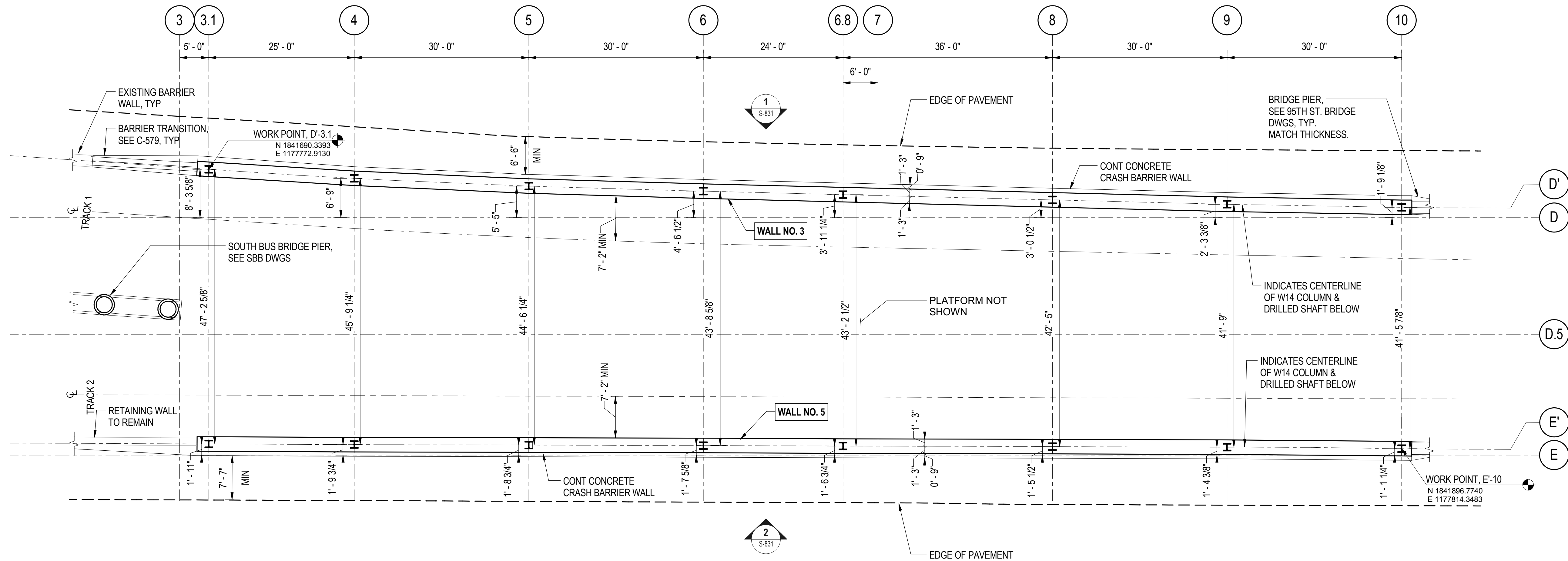
LOCATION IDENTIFIER: DR095

**SOUTH TERMINAL FOUNDATION PLAN - AREA A & B**

**S-802**

06/10/2016 4:02:00 PM

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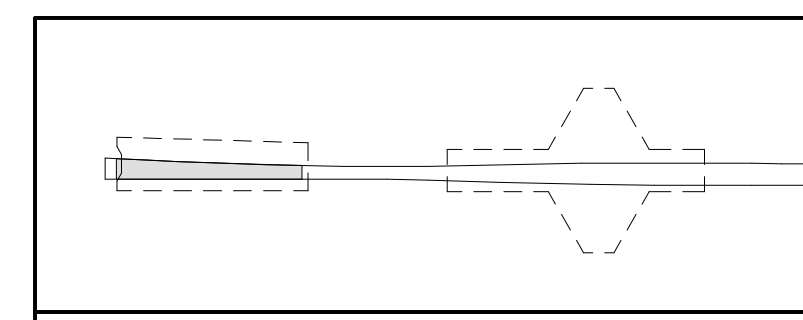


**1** CRASH BARRIER WALL AND COLUMN SCHEMATIC PLAN  
S-803 SCALE: 1" = 10'-0"

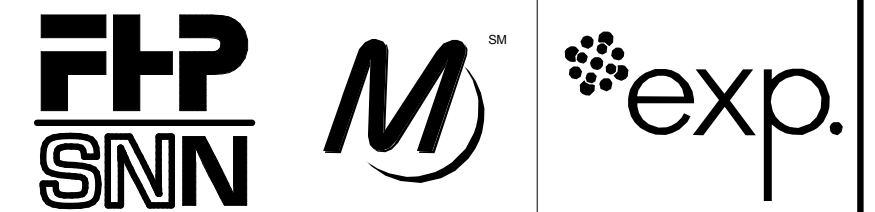


**SHEET NOTES:**

1. FOR GENERAL PROJECT NOTES, SEE S-000 SERIES.
2. FOR TYPICAL DETAILS, SEE S-1100 SERIES.
3. REFER TO S-1104 FOR WALL REINFORCEMENT AND DETAILS.
4. PROVIDE CONTROL JOINT AT 15'-0" O.C. MAX. TYP, SEE S-1112.
5. ALL EXPOSED EDGES SHALL BE CHAMFERED 3/4", INCLUDING THE TOP OF THE WALL AND AT THE INTERFACE WITH BRIDGE PIERS.



KEY PLAN: CRASH BARRIER WALLS



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DESIGNED BY	G. FORKAN
DRAWN BY	M. DONAHUE
PROJECT NO.	2012-0021.02
FILE NAME	DR095_S-StationFoundation

MARK	DATE	DESCRIPTION
	10/6/16	ISSUED FOR CONSTRUCTION

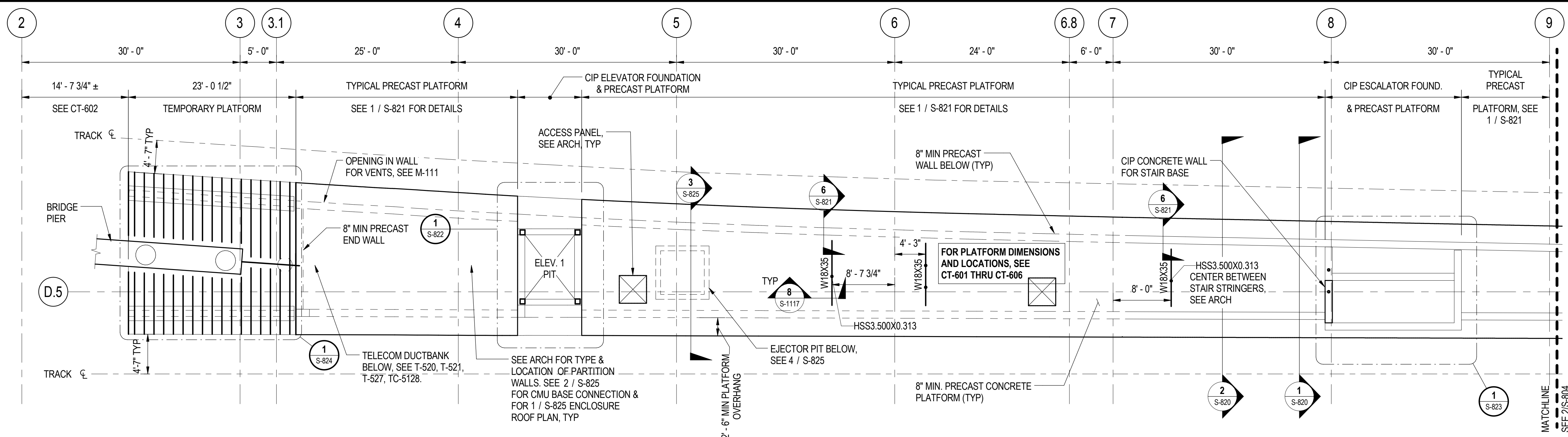
LOCATION IDENTIFIER: DR095

**SOUTH TERMINAL CRASH BARRIER WALL AND COLUMN LOCATION PLAN**

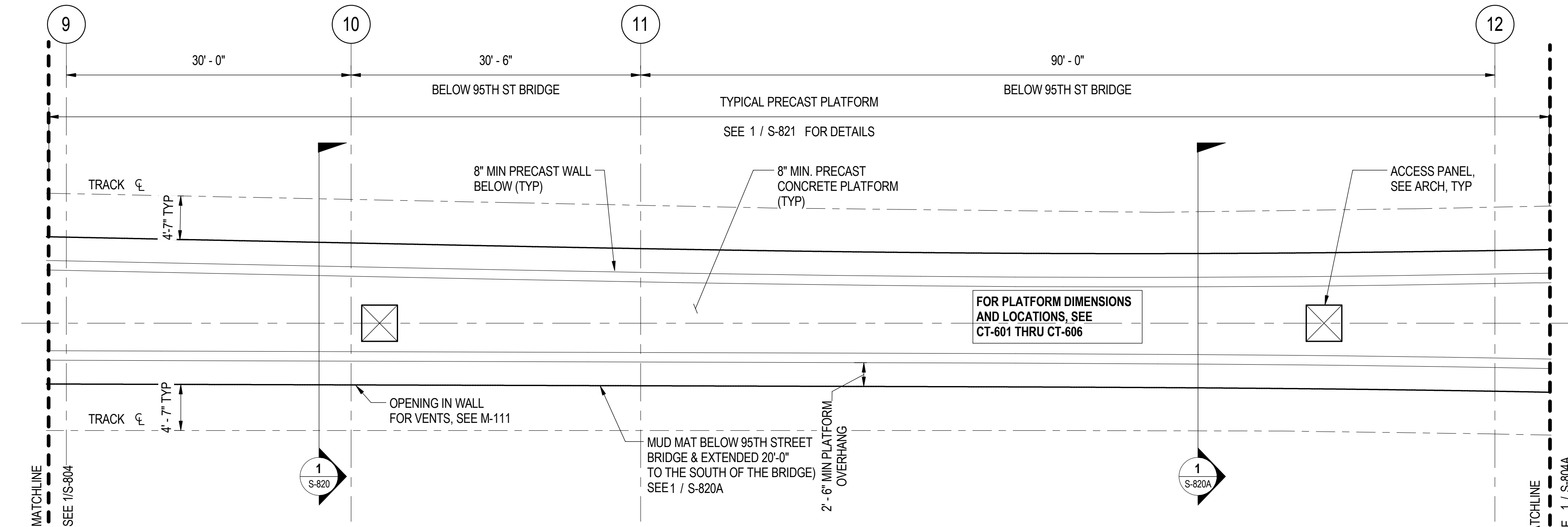
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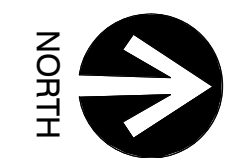
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**1** SOUTH TERMINAL PLATFORM PLAN - AREA A  
S-804 SCALE: 1/8" = 1'-0"



**2** SOUTH TERMINAL PLATFORM PLAN - AREA B  
S-804 SCALE: 1/8" = 1'-0"



**SHEET NOTES:**

- FOR GENERAL NOTES, SEE DWGS S-001 TO S-004.
- FOR TYPICAL PLATFORM DETAILS REFER TO S-821
- ALL MEP PENETRATIONS ARE NOT SHOWN. CONTRACTOR TO COORDINATE ALL PRECAST PLATFORM OPENINGS AND PENETRATIONS WITH ARCH AND MEP DRAWINGS PRIOR TO FABRICATION. ALL REQUIRED PENETRATIONS THROUGH THE PRECAST LARGER THAN 12" BY 12" SHALL BE CAST IN THE PLANT AND SUBMITTED TO THE DOR FOR REVIEW AND APPROVAL PRIOR TO FABRICATION.
- PRECAST PLATFORM AND BARREL SIZES SHOWN ARE MINIMUM SIZES. THE LAYOUT SHOWN ON THE PLANS AND DETAILS IS SUGGESTED ONLY, AS A MEANS TO CLARIFY THE TYPE OF ELEMENTS REQUIRED TO CONSTRUCT THE PLATFORM. REFER TO S-821 FOR TYPICAL CONNECTION DETAILS BETWEEN PRECAST COMPONENTS. THE FINAL DESIGN AND DETAILING FOR ALL PRECAST PLATFORM SECTIONS AND CONNECTIONS ARE THE RESPONSIBILITY OF THE PRECAST FABRICATOR AND THEIR STRUCTURAL ENGINEER FOR THE LOADS AND EXTENTS PROVIDED ON THE DRAWINGS AND IN THE SPECIFICATIONS. SHOP DRAWINGS SHALL BE SUBMITTED FOR FINAL REVIEW AND APPROVAL PRIOR TO PROCEEDING WITH ANY FABRICATION.
- REFER TO TRACK DWGS FOR CTA TRACK CLEARANCE REQUIREMENTS NOT SHOWN.
- PRECAST SYSTEM CONSISTS OF PRECAST BARREL SECTIONS THAT SHALL BE CAST IN PIECES TO LENGTHS REQUIRED BY THE PRECASTER FOR TRANSPORTATION AND INSTALLATION REQUIREMENTS THROUGHOUT THE LENGTH OF THE PLATFORM AS INDICATED ON THE PLANS. EACH BARREL WILL BE PLACED SO THAT IT IS A MINIMUM OF 2'-6" CLEAR FROM THE EDGE OF THE PLATFORM SLAB. THE DISTANCE BETWEEN THE INTERIOR WALLS OF THE BARRELS WILL VARY ALONG THE LENGTH OF THE PLATFORM.
- PLATFORM DESIGN LOADS ARE AS FOLLOWS:
  - LIVE LOAD: 100 PSF EXCEPT AS NOTED ON 7 / S-821
  - CONSTRUCTION LOAD: 30 PSF COMBINED WITH 3 KIP POINT LOAD
  - HORIZ. GLANCING BLOW (P): 20 KIP (4000 PLF OVER 5' PLATFORM EDGE)
  - PARTITION WALL LOAD: 950 PLF
  - STAIR COLUMN LOAD: 25 KIP
  - STAIR BASE LOAD: 10 KIP
  - BARRIER LOAD: ±0.50 KIP-FT / FT OVERTURNING



**SENSITIVE SECURITY INFORMATION**  
WARNING: THIS RECORD CONTAINS SENSITIVE SECURITY INFORMATION THAT IS CONTROLLED UNDER 49 CFR PARTS 15 AND 1520. NO PART OF THIS RECORD MAY BE DISCLOSED TO PERSONS WITHOUT A "NEED TO KNOW" AS DEFINED IN CFR PARTS 15 AND 1520. EXCEPT WITH THE WRITTEN PERMISSION OF THE ADMINISTRATOR OF THE TRANSPORTATION SECURITY ADMINISTRATION OR THE SECRETARY OF TRANSPORTATION. UNAUTHORIZED RELEASE MAY RESULT IN CIVIL PENALTY OR OTHER ACTION. FOR U.S. GOVERNMENT AGENCIES, PUBLIC DISCLOSURE IS GOVERNED BY 5 U.S.C. 552 AND 49 CFR PARTS 15 AND 1520.

**DRAWING SCALE IS NOT GUARANTEED. CTA ASSUMES NO RISK OF LIABILITY FOR ERRORS CAUSED, DIRECTLY OR INDIRECTLY, BY SCALING OF THIS DRAWING.**

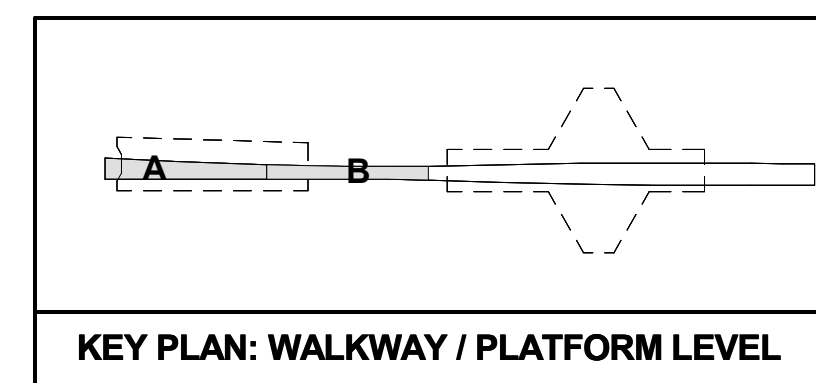
**95th Terminal Improvement**  
15 West 95th Street (South Terminal)  
Chicago, IL 60628

IN CHARGE	J. JAKALSKI
APPROVED BY	J. JAKALSKI
CHECKED BY	R.S. NAIR
DESIGNED BY	G. FORKAN
DRAWN BY	M. DONAHUE
PROJECT NO.	2012-0021.02
FILE NAME	DR095_S-Terminal

MARK	DATE	DESCRIPTION
	2/9/17	ISSUED FOR CONSTRUCTION

LOCATION IDENTIFIER: **DR095**

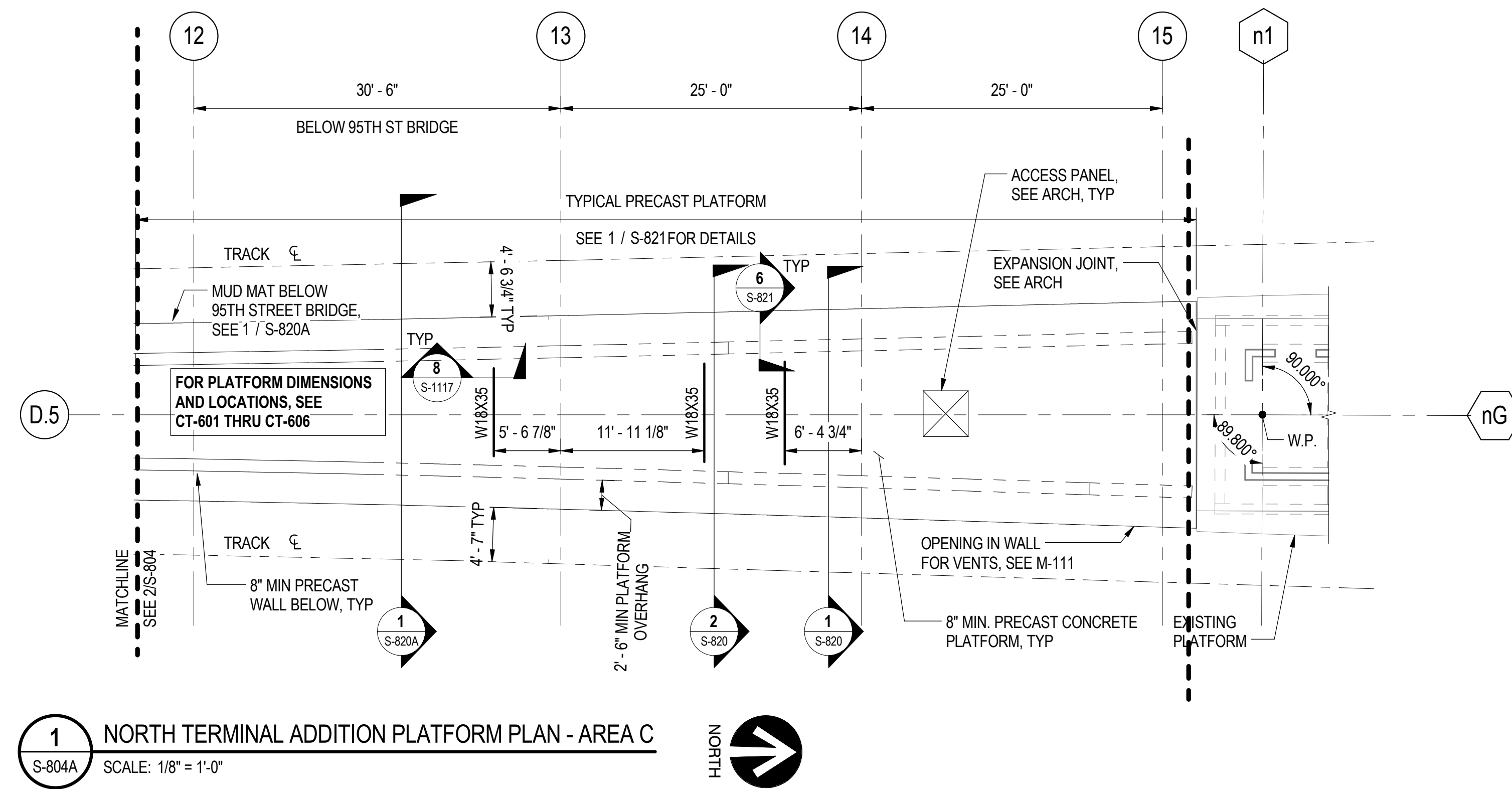
**SOUTH TERMINAL PLATFORM PLAN - AREA A & B**



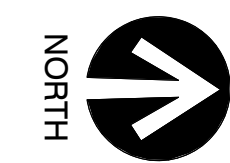
**S-804**

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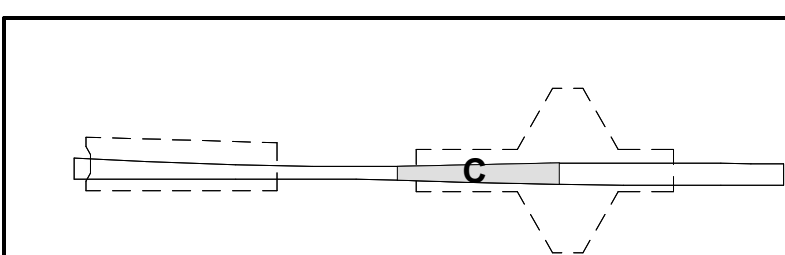


**1** NORTH TERMINAL ADDITION PLATFORM PLAN - AREA C  
S-804A SCALE: 1/8" = 1'-0"



**SHEET NOTES:**

- FOR GENERAL NOTES, SEE DWGS S-001 TO S-004.
- FOR TYPICAL PLATFORM DETAILS REFER TO S-821
- ALL MEP PENETRATIONS ARE NOT SHOWN. CONTRACTOR TO COORDINATE ALL PRECAST PLATFORM OPENINGS AND PENETRATIONS WITH ARCH AND MEP DRAWINGS PRIOR TO FABRICATION. ALL REQUIRED PENETRATIONS THROUGH THE PRECAST LARGER THAN 12" BY 12" SHALL BE CAST IN THE PLANT AND SUBMITTED TO THE DOR FOR REVIEW AND APPROVAL PRIOR TO FABRICATION.
- PRECAST PLATFORM AND BARREL SIZES SHOWN ARE MINIMUM SIZES. THE LAYOUT SHOWN ON THE PLANS AND DETAILS IS SUGGESTED ONLY, AS A MEANS TO CLARIFY THE TYPE OF ELEMENTS REQUIRED TO CONSTRUCT THE PLATFORM. REFER TO S-821 FOR TYPICAL CONNECTION DETAILS BETWEEN PRECAST COMPONENTS. THE FINAL DESIGN AND DETAILING FOR ALL PRECAST PLATFORM SECTIONS AND CONNECTIONS ARE THE RESPONSIBILITY OF THE PRECAST FABRICATOR AND THEIR STRUCTURAL ENGINEER FOR THE LOADS AND EXTENTS PROVIDED ON THE DRAWINGS AND IN THE SPECIFICATIONS. SHOP DRAWINGS SHALL BE SUBMITTED FOR FINAL REVIEW AND APPROVAL PRIOR TO PROCEEDING WITH ANY FABRICATION.
- REFER TO TRACK DWGS FOR CTA TRACK CLEARANCE REQUIREMENTS NOT SHOWN.
- PRECAST SYSTEM CONSISTS OF PRECAST BARREL SECTIONS THAT SHALL BE CAST IN PIECES TO LENGTHS REQUIRED BY THE PRECASTER FOR TRANSPORTATION AND INSTALLATION REQUIREMENTS THROUGHOUT THE LENGTH OF THE PLATFORM AS INDICATED ON THE PLANS. EACH BARREL WILL BE PLACED SO THAT IT IS A MINIMUM OF 2'-6" CLEAR FROM THE EDGE OF THE PLATFORM SLAB. THE DISTANCE BETWEEN THE INTERIOR WALLS OF THE BARRELS WILL VARY ALONG THE LENGTH OF THE PLATFORM.
- PLATFORM DESIGN LOADS ARE AS FOLLOWS:
  - LIVE LOAD: 100 PSF EXCEPT AS NOTED ON 7 / S-821
  - CONSTRUCTION LOAD: 30 PSF COMBINED WITH 3 KIP POINT LOAD
  - HORIZ. GLANCING BLOW (P): 20 KIP (4000 PLF OVER 5' PLATFORM EDGE)
  - PARTITION WALL LOAD: 950 PLF
  - STAIR COLUMN LOAD: 25 KIP
  - STAIR BASE LOAD: 10 KIP
  - BARRIER LOAD: ±0.50 KIP-FT / FT OVERTURNING



KEY PLAN: WALKWAY / PLATFORM LEVEL



**SENSITIVE SECURITY INFORMATION**  
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**DRAWING SCALE IS NOT GUARANTEED. CTA ASSUMES NO RISK OF LIABILITY FOR ERRORS CAUSED, DIRECTLY OR INDIRECTLY, BY SCALING OF THIS DRAWING.**

**95th Terminal Improvement**  
15 West 95th Street (South Terminal)  
Chicago, IL 60628

IN CHARGE	J. JAKALSKI
APPROVED BY	J. JAKALSKI
CHECKED BY	R.S. NAIR
DESIGNED BY	G. FORKAN
DRAWN BY	C. BOVE
PROJECT NO.	2012-0021.02
FILE NAME	DR095_S-Terminal

MARK	DATE	DESCRIPTION
	2/9/17	ISSUED FOR CONSTRUCTION

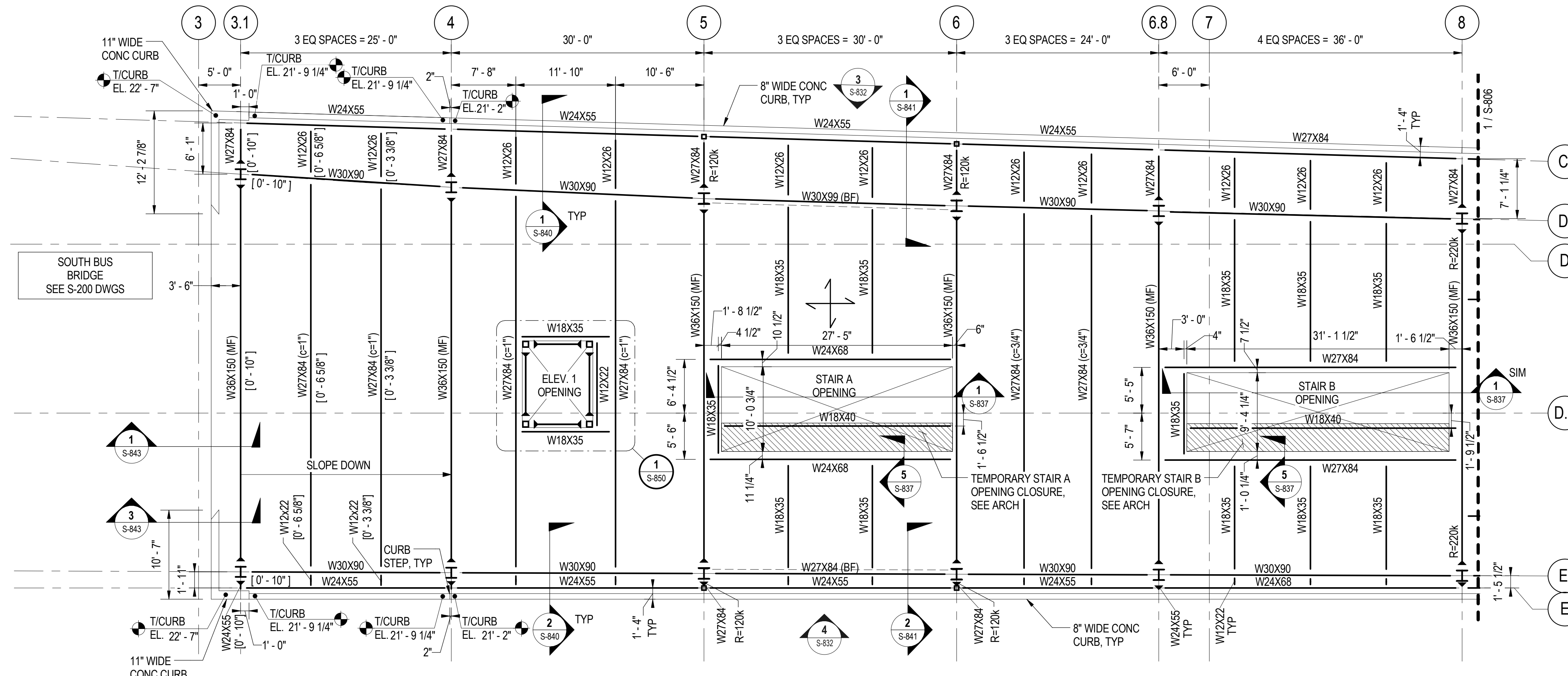
LOCATION IDENTIFIER: **DR095**

**NORTH TERMINAL ADDITION PLATFORM PLAN - AREA C**

**S-804A**

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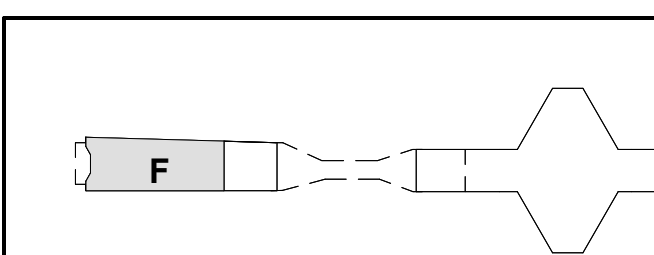
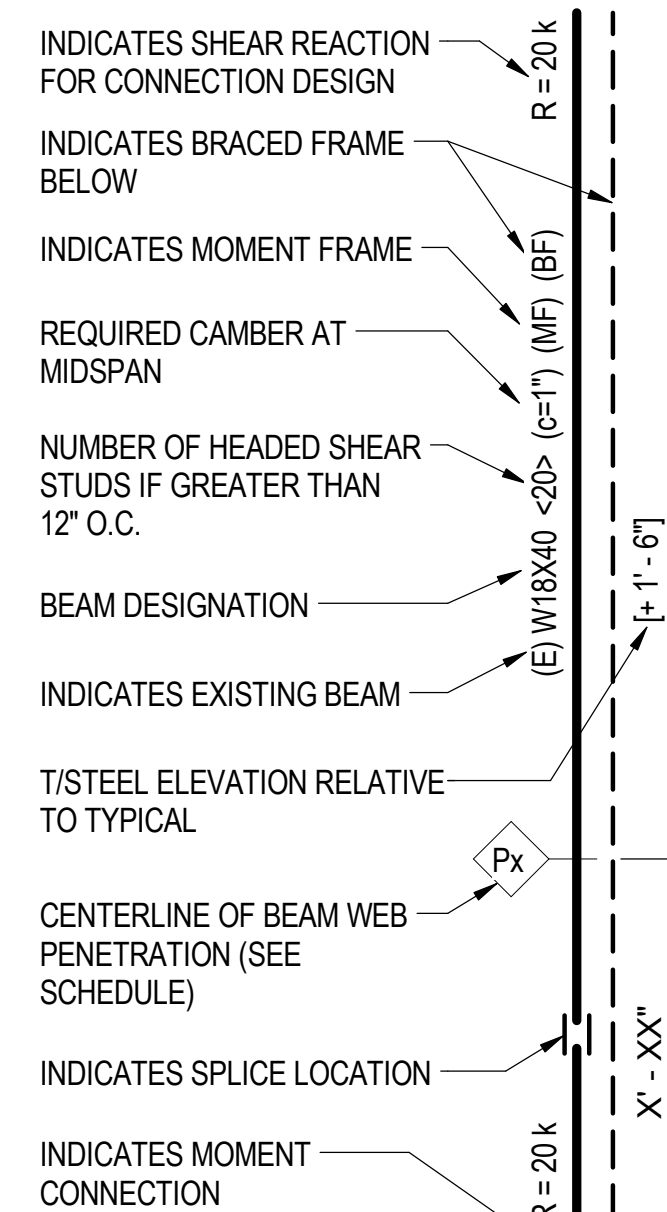
**1 SOUTH CONCOURSE FRAMING PLAN - AREA F**  
S-805 SCALE: 1/8" = 1'-0"



**SHEET NOTES:**

- FOR GENERAL NOTES, SEE DWGS S-001 TO S-004.
- FOR TYPICAL DETAILS REFER TO S-1112 TO S-1120.
- TYPICAL FLOOR CONSTRUCTION: 8" NORMAL WEIGHT CONCRETE FORMED SLAB W/ #4 BARS T&B EA. WAY @ 12" O.C. U.N.O.
- PROVIDE A MINIMUM OF 1 SHEAR STUD PER FOOT OF BEAM LENGTH FOR ALL BEAMS WHERE STUDS ARE NOT INDICATED ON THE PLAN.
- HEADED SHEAR STUDS ARE 3/4" DIA X 6" LONG, STUD WELDED TO CENTER OF BEAM TOP FLANGE AT 12" O.C. MIN.
- FOR TOP OF FINISHED FLOOR ELEVATIONS AND INFORMATION REGARDING FLOOR PENETRATIONS FOR STAIRS, ESCALATORS, AND ELEVATORS, SEE ARCH DWGS.
- FOR EXACT LOCATION OF ALL STEEL AROUND VERTICAL CIRCULATION OPENINGS, COORDINATE WITH VERTICAL CIRCULATION MANUFACTURER REQUIREMENTS.
- TOP OF STEEL IS 19" - 4 1/2" CCD TYPICAL UNO.
- SEE S-833 & S-834 FOR MOMENT FRAME ELEVATIONS.
- SEE MEP DRAWINGS FOR LOCATIONS OF FLOOR PENETRATIONS, SEE 5 / S-1115 & 2 / S-1113
- ALL CONCOURSE LEVEL STEEL FRAMING TO BE GALVANIZED
- STEEL BEAMS SHALL BE Laterally BRACED DURING SLAB PLACEMENT.

**STEEL BEAM LEGEND**



KEY PLAN: CONCOURSE LEVEL



**SENSITIVE SECURITY INFORMATION**  
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**95th Terminal Improvement**  
15 West 95th Street (South Terminal)  
Chicago, IL 60628

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<b>APPROVED BY</b>	J. JAKALSKI
<b>CHECKED BY</b>	R.S. NAIR
<b>DESIGNED BY</b>	G. FORKAN
<b>DRAWN BY</b>	M. DONAHUE
<b>PROJECT NO.</b>	2012-0021.02
<b>FILE NAME</b>	DR095_S-Terminal

MARK	DATE	DESCRIPTION
	2/9/17	ISSUED FOR CONSTRUCTION

LOCATION IDENTIFIER: DR095

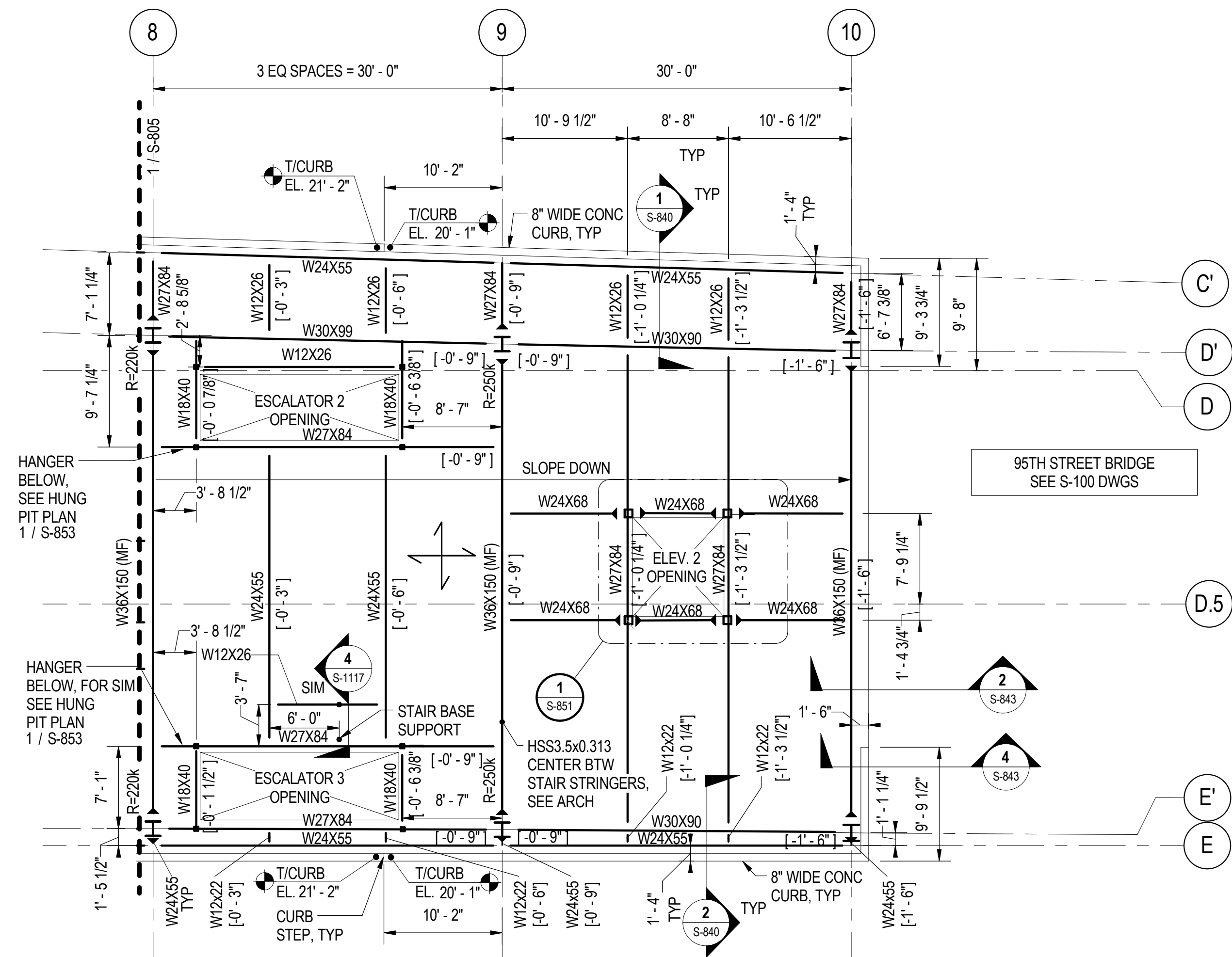
**SOUTH TERMINAL CONCOURSE FRAMING PLAN - AREA F**

**S-805**

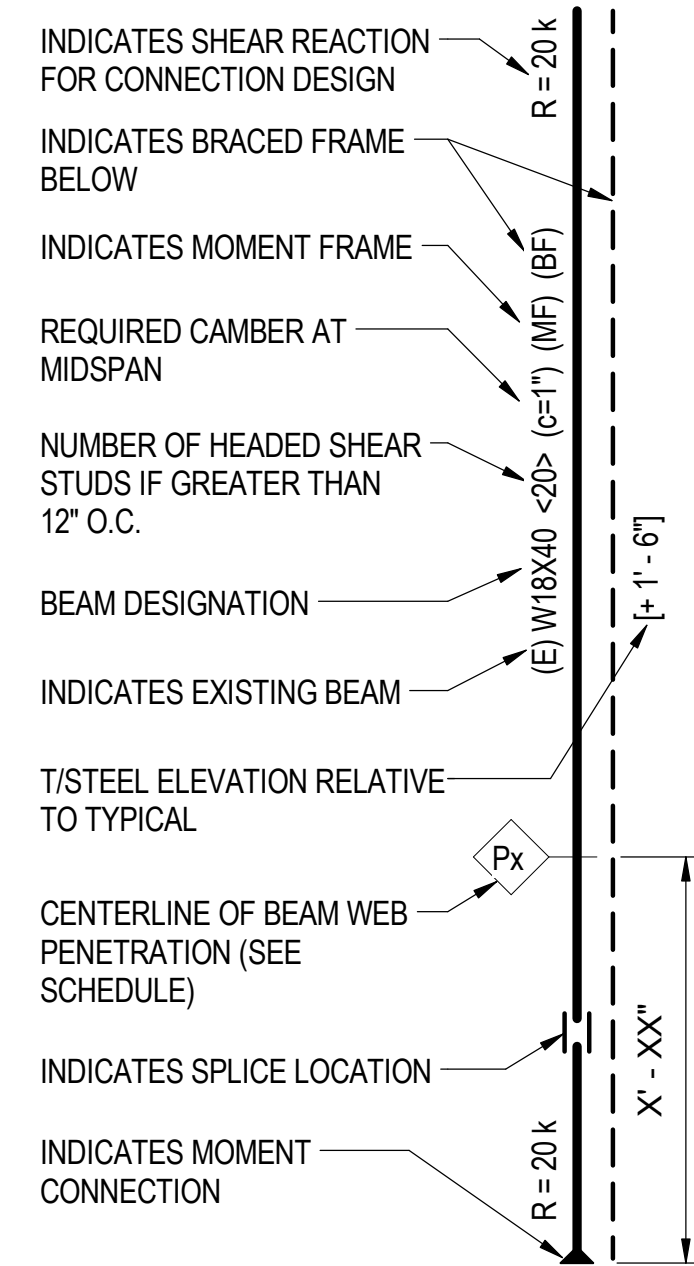
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**1 SOUTH CONCOURSE FRAMING PLAN - AREA G**  
S-806 SCALE: 1/8" = 1'-0"

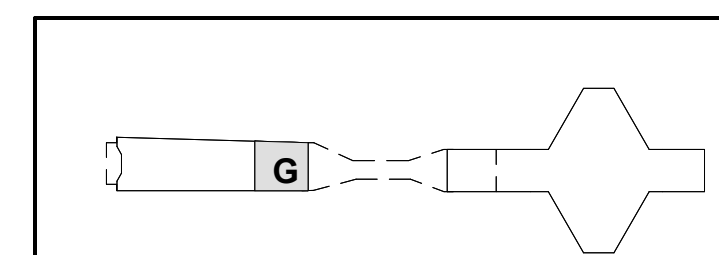


**STEEL BEAM LEGEND**



**SHEET NOTES:**

- FOR GENERAL NOTES, SEE DWGS S-001 TO S-004.
- FOR TYPICAL DETAILS REFER TO S-1112 TO S-1120.
- TYPICAL FLOOR CONSTRUCTION: 8" NORMAL WEIGHT CONCRETE FORMED SLAB W/ #4 BARS T&B EA. WAY @ 12" O.C. U.N.O.
- PROVIDE A MINIMUM OF 1 SHEAR STUD PER FOOT OF BEAM LENGTH FOR ALL BEAMS WHERE STUDS ARE NOT INDICATED ON THE PLAN.
- HEADED SHEAR STUDS ARE 3/4" DIA X 6" LONG, STUD WELDED TO CENTER OF BEAM TOP FLANGE AT 12" O.C. MIN.
- FOR TOP OF FINISHED FLOOR ELEVATIONS AND INFORMATION REGARDING FLOOR PENETRATIONS FOR STAIRS, ESCALATORS, AND ELEVATORS, SEE ARCH DWGS.
- FOR EXACT LOCATION OF ALL STEEL AROUND VERTICAL CIRCULATION OPENINGS, COORDINATE WITH VERTICAL CIRCULATION MANUFACTURER REQUIREMENTS.
- TOP OF STEEL IS 19' - 4 1/2" CCD TYPICAL UNO.
- SEE S-833 & S-834 FOR MOMENT FRAME ELEVATIONS.
- SEE MEP DRAWINGS FOR LOCATIONS OF FLOOR PENETRATIONS, SEE 5 / S-1115 & 2 / S-1113
- ALL CONCOURSE LEVEL STEEL FRAMING TO BE GALVANIZED
- STEEL BEAMS SHALL BE Laterally BRACED DURING SLAB PLACEMENT.



KEY PLAN: CONCOURSE LEVEL



**SENSITIVE SECURITY INFORMATION**  
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**95th Terminal Improvement**  
15 West 95th Street (South Terminal)  
Chicago, IL 60628

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<b>APPROVED BY</b>	J. JAKALSKI
<b>CHECKED BY</b>	R.S. NAIR
<b>DESIGNED BY</b>	G. FORKAN
<b>DRAWN BY</b>	M. DONAHUE
<b>PROJECT NO.</b>	2012-0021.02
<b>FILE NAME</b>	DR095_S-Terminal

MARK	DATE	DESCRIPTION
	2/9/17	ISSUED FOR CONSTRUCTION

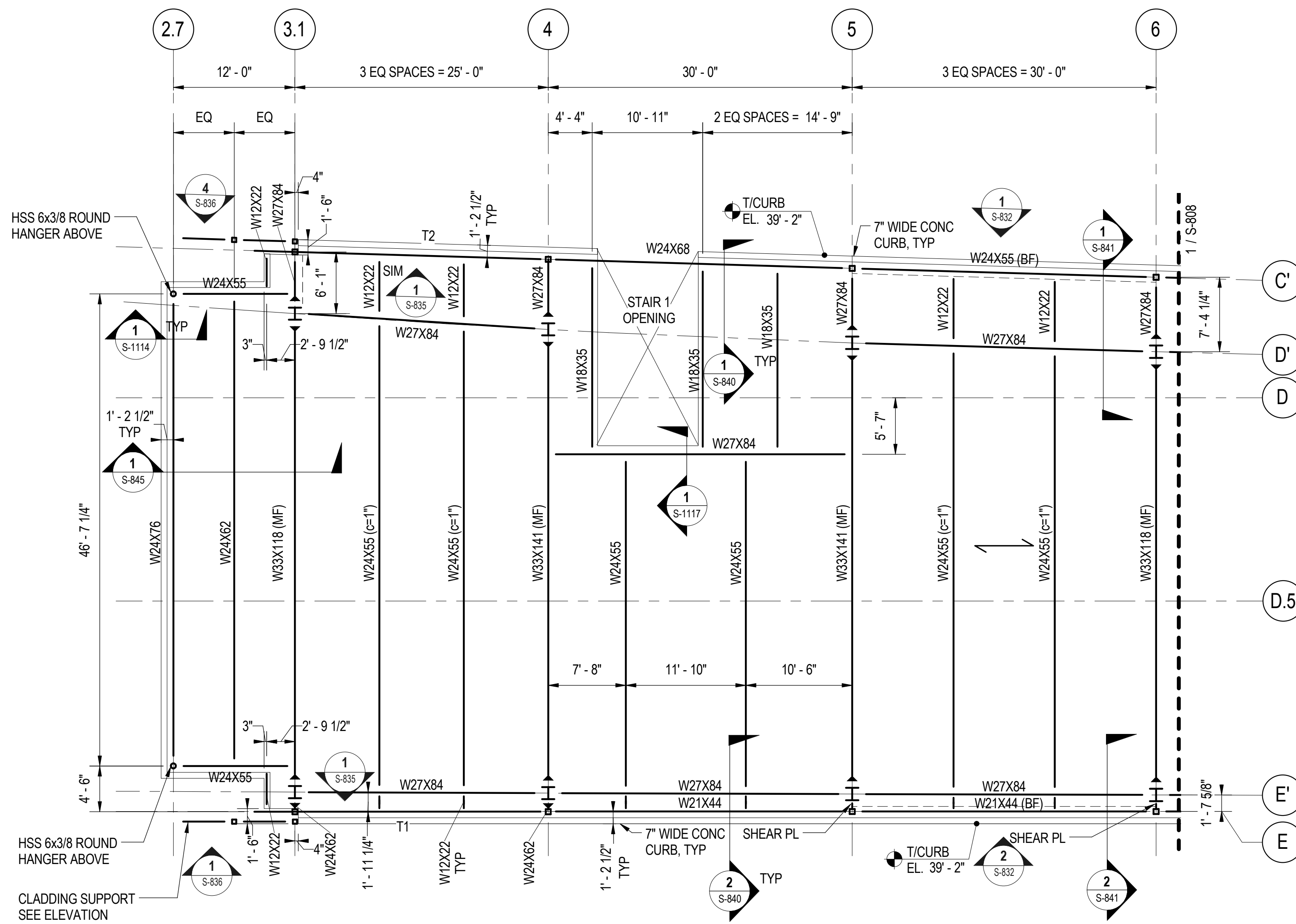
LOCATION IDENTIFIER: DR095

**SOUTH TERMINAL CONCOURSE FRAMING PLAN - AREA G**

**S-806**

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**1 SOUTH TERMINAL LEVEL 2 FRAMING PLAN - AREA K**  
 S-807 SCALE: 1/8" = 1'-0"



**STEEL BEAM LEGEND**

- INDICATES SHEAR REACTION FOR CONNECTION DESIGN
- INDICATES BRACED FRAME BELOW
- INDICATES MOMENT FRAME
- REQUIRED CAMBER AT MIDSPAN
- NUMBER OF HEADED SHEAR STUDS IF GREATER THAN 12" O.C.
- BEAM DESIGNATION
- INDICATES EXISTING BEAM
- T/STEEL ELEVATION RELATIVE TO TYPICAL
- CENTERLINE OF BEAM WEB PENETRATION (SEE SCHEDULE)
- INDICATES SPLICE LOCATION
- INDICATES MOMENT CONNECTION

**SHEET NOTES:**

1. FOR GENERAL NOTES, SEE DWGS S-001 TO S-004.
2. FOR TYPICAL DETAILS REFER TO S-1112 TO S-1120.
3. TYPICAL FLOOR CONSTRUCTION: 3" 18 GAGE GALVANIZED COMPOSITE STEEL DECK W/ 3-1/2" MIN. LWC TOPPING (TOTAL THICKNESS = 6 1/2" MIN. REINF. W/ 6X6 W2.6XW2.6 WWF. FASTEN DECK AT STEEL SUPPORTS W/ 3/4" PUDDLE WELDS AT 12" O.C. & PROVIDE 2 WELDED SIDELAPS.
4. PROVIDE A MINIMUM OF 1 SHEAR STUD PER FOOT OF BEAM LENGTH FOR ALL BEAMS WHERE STUDS ARE NOT INDICATED ON THE PLAN.
5. HEADED SHEAR STUDS ARE 3/4" DIA X 5 1/4" LONG, ATTACHED TO THE BEAM TOP FLANGE PER THE TYPICAL COMPOSITE SLAB DETAIL.
6. FOR EXACT LOCATION OF ALL STEEL AROUND VERTICAL CIRCULATION OPENINGS, COORDINATE WITH VERTICAL CIRCULATION MANUFACTURER REQUIREMENTS.
7. TOP OF STEEL IS 38' - 1 1/2" CCD TYPICAL UNO.
8. Tx INDICATES STEEL TRUSS. SEE 1 / S-835 FOR SIZE AND INFORMATION.
9. SEE S-833 & S-834 FOR MOMENT FRAME ELEVATIONS.
10. FOR TOP OF FINISHED FLOOR ELEVATIONS AND INFORMATION REGARDING FLOOR PENETRATIONS FOR STAIRS, ESCALATORS, AND ELEVATORS, SEE ARCH DWGS.
11. SEE MEP DRAWINGS FOR LOCATIONS OF FLOOR PENETRATIONS, SEE 5 / S-1115.



**SENSITIVE SECURITY INFORMATION**  
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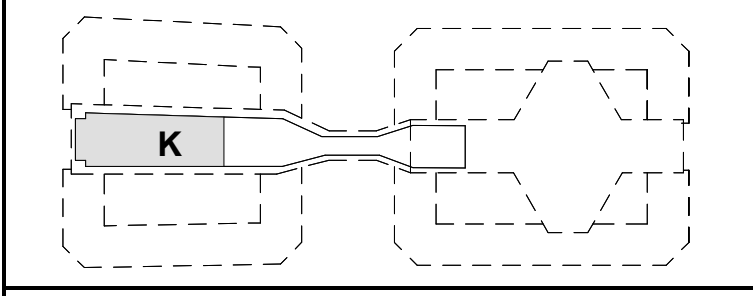
**DRAWING SCALE IS NOT GUARANTEED. CTA ASSUMES NO RISK OF LIABILITY FOR ERRORS CAUSED, DIRECTLY OR INDIRECTLY, BY SCALING OF THIS DRAWING.**

**95th Terminal Improvement**  
 15 West 95th Street (South Terminal)  
 Chicago, IL 60628

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APPROVED BY	J. JAKALSKI
CHECKED BY	R.S. NAIR
DESIGNED BY	G. FORKAN
DRAWN BY	M. DONAHUE
PROJECT NO.	2012-0021.02
FILE NAME	DR095_S-Terminal

MARK	2/9/17	ISSUED FOR CONSTRUCTION
DATE		DESCRIPTION

LOCATION IDENTIFIER: **DR095**



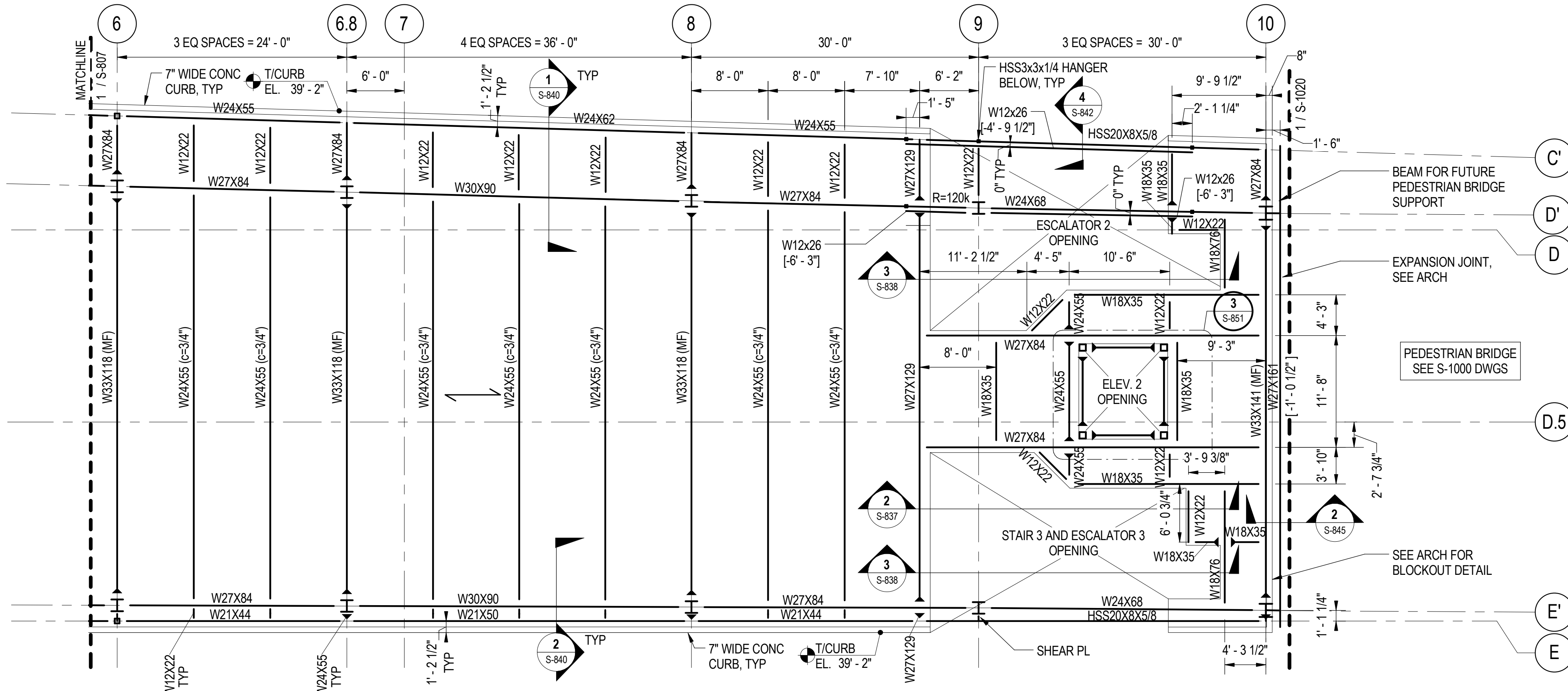
**KEY PLAN: SECOND FLOOR LEVEL**

**SOUTH TERMINAL SECOND FLOOR FRAMING PLAN - AREA K**

**S-807**

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**STEEL BEAM LEGEND**

- INDICATES SHEAR REACTION FOR CONNECTION DESIGN
- INDICATES BRACED FRAME BELOW
- INDICATES MOMENT FRAME
- REQUIRED CAMBER AT MIDSPAN
- NUMBER OF HEADED SHEAR STUDS IF GREATER THAN 12" O.C.
- BEAM DESIGNATION
- INDICATES EXISTING BEAM
- T/STEEL ELEVATION RELATIVE TO TYPICAL
- CENTERLINE OF BEAM WEB PENETRATION (SEE SCHEDULE)
- INDICATES SPLICE LOCATION
- INDICATES MOMENT CONNECTION

**1 SOUTH TERMINAL LEVEL 2 FRAMING PLAN - AREA L**  
S-808 SCALE: 1/8" = 1'-0"



**SHEET NOTES:**

1. FOR GENERAL NOTES, SEE DWGS S-001 TO S-004.
2. FOR TYPICAL DETAILS REFER TO S-1112 TO S-1120.
3. TYPICAL FLOOR CONSTRUCTION: 3" 18 GAGE GALVANIZED COMPOSITE STEEL DECK W/ 3-1/2" MIN. LWC TOPPING (TOTAL THICKNESS = 6 1/2" MIN. REINF. W/ 6X6 W2.6XW2.6 WWF. FASTEN DECK AT STEEL SUPPORTS w/ 3/4" PUDDLE WELDS AT 12" O.C. & PROVIDE 2 WELDED SIDELAPS.
4. PROVIDE A MINIMUM OF 1 SHEAR STUD PER FOOT OF BEAM LENGTH FOR ALL BEAMS WHERE STUDS ARE NOT INDICATED ON THE PLAN.
5. HEADED SHEAR STUDS ARE 3/4" DIA X 5 1/4" LONG, ATTACHED TO THE BEAM TOP FLANGE PER THE TYPICAL COMPOSITE SLAB DETAIL.
6. FOR EXACT LOCATION OF ALL STEEL AROUND VERTICAL CIRCULATION OPENINGS, COORDINATE WITH VERTICAL CIRCULATION MANUFACTURER REQUIREMENTS.
7. TOP OF STEEL IS 38" - 1 1/2" CCD TYPICAL UNO.
8. SEE S-833 & S-834 FOR MOMENT FRAME ELEVATIONS.
9. FOR INFORMATION REGARDING FLOOR PENETRATIONS FOR STAIRS, ESCALATORS, AND ELEVATORS, SEE ARCH DWGS.
10. SEE MEP DRAWINGS FOR LOCATIONS OF FLOOR PENETRATIONS, SEE5 / S-1115.



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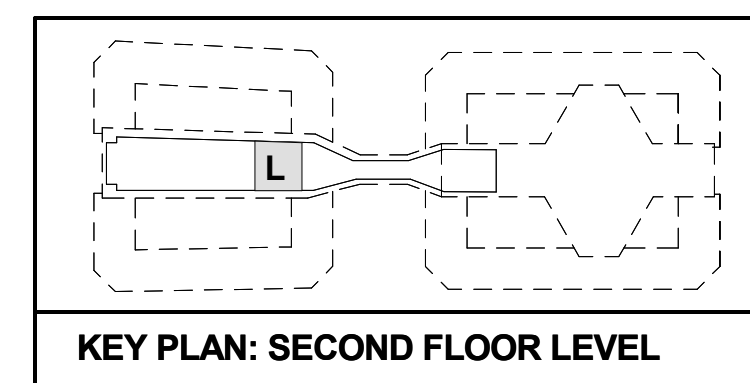
**95th Terminal Improvement**  
15 West 95th Street (South Terminal)  
Chicago, IL 60628

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<b>APPROVED BY</b>	J. JAKALSKI
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<b>DRAWN BY</b>	C. BOVE
<b>PROJECT NO.</b>	2012-0021.02
<b>FILE NAME</b>	DR095_S-Terminal

MARK	DATE	DESCRIPTION
	2/9/17	ISSUED FOR CONSTRUCTION

LOCATION IDENTIFIER: DR095

**SOUTH TERMINAL SECOND FLOOR FRAMING PLAN - AREA L**

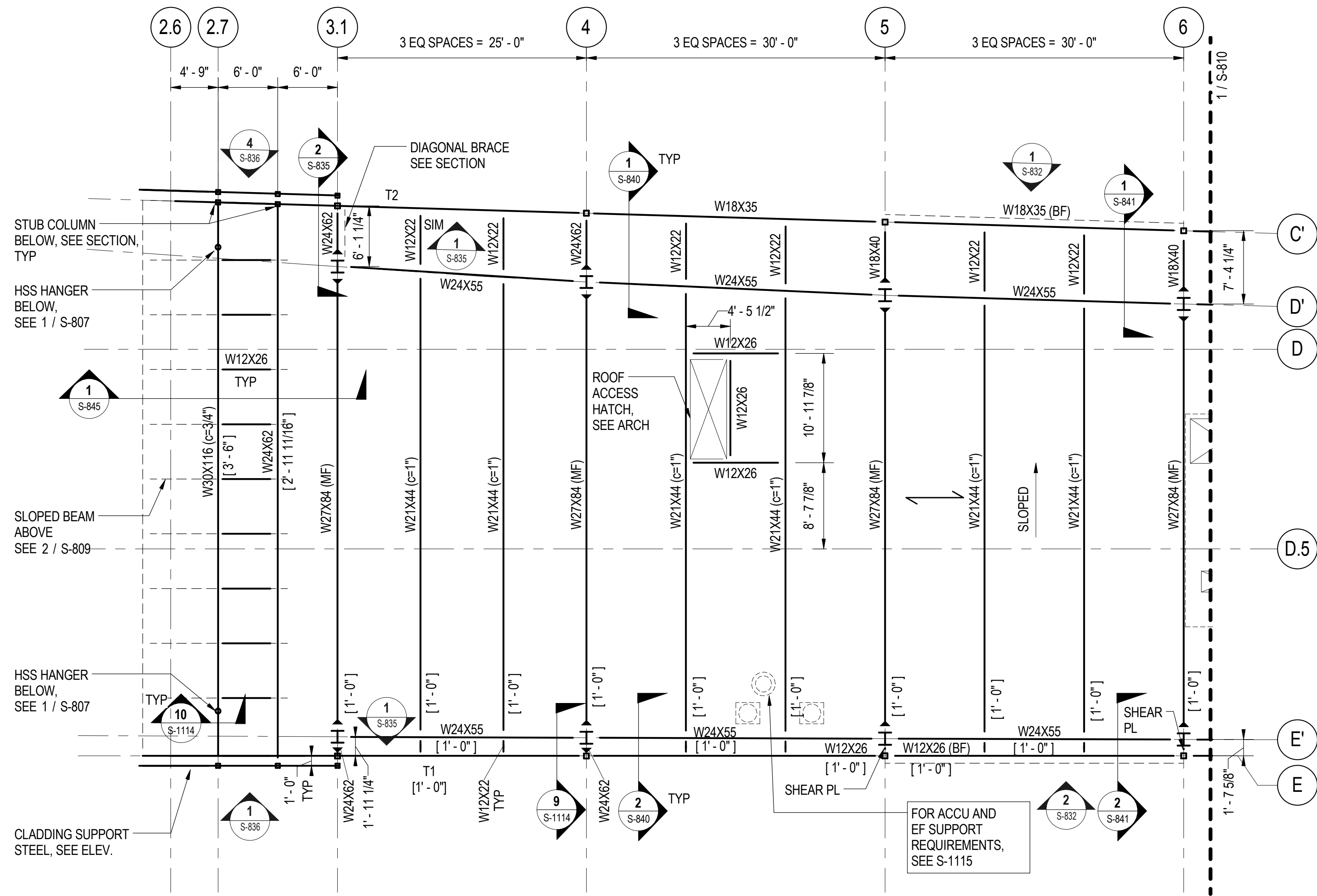


**S-808**

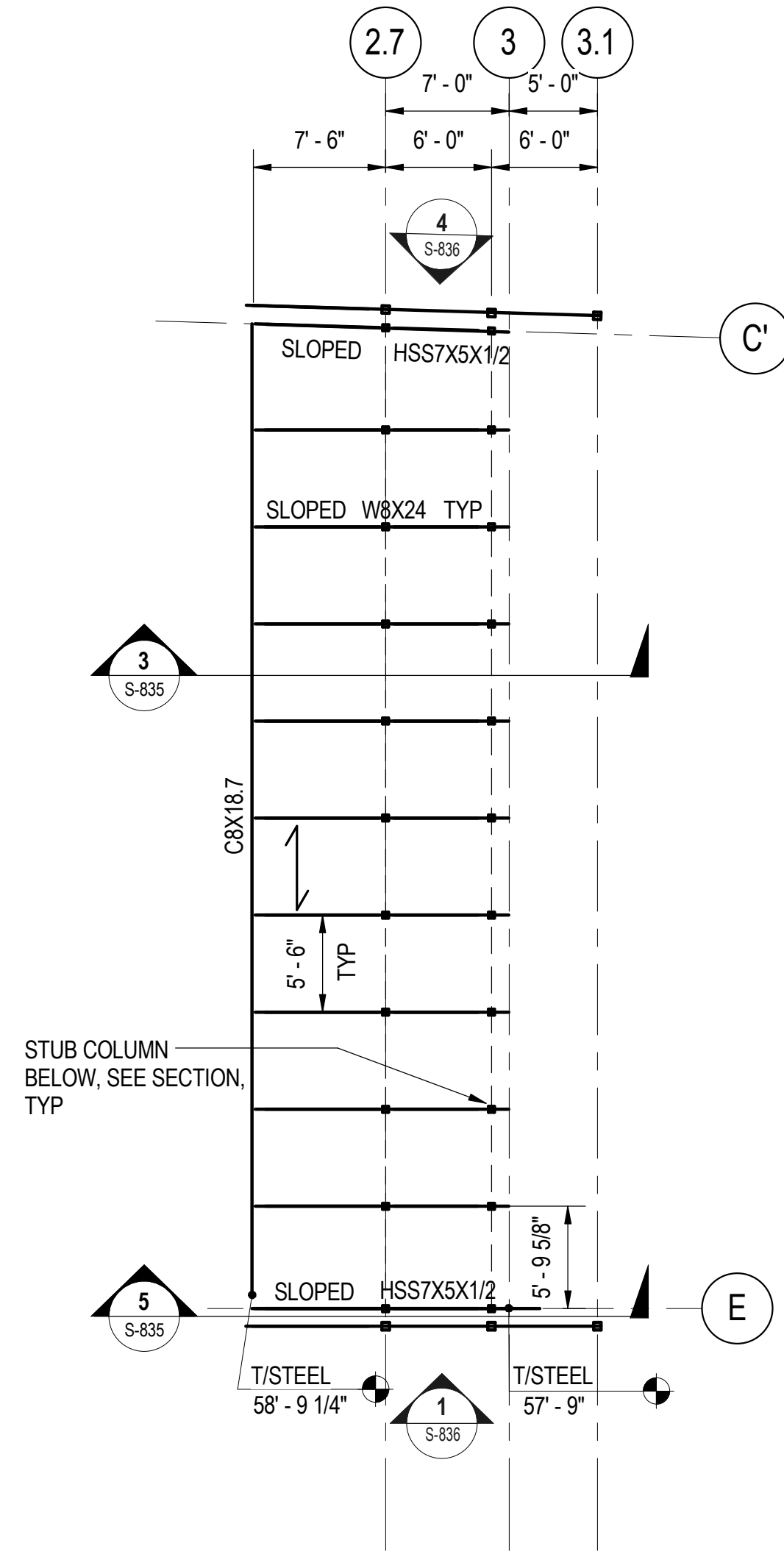


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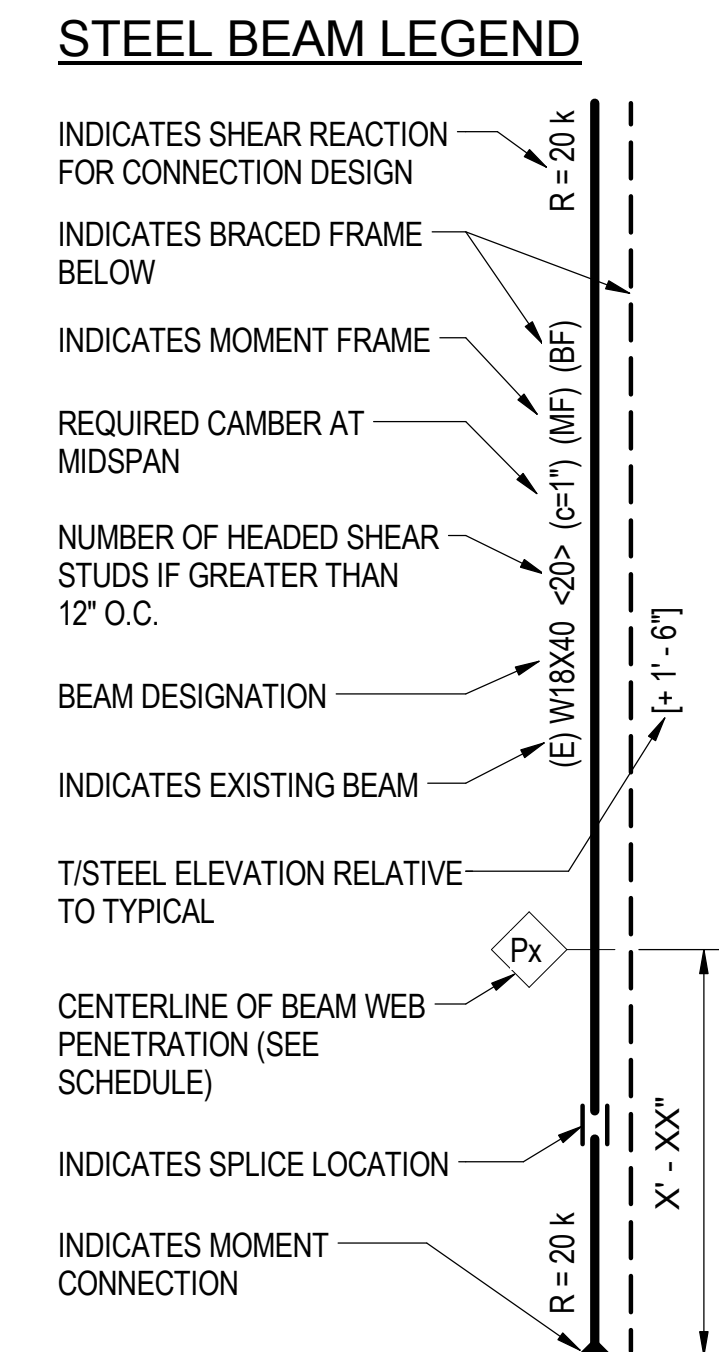
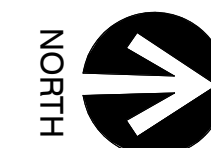
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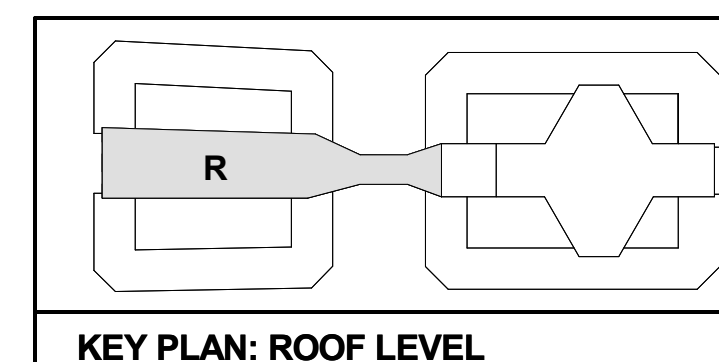
**1** SOUTH TERMINAL ROOF FRAMING PLAN - AREA R.1  
S-809 SCALE: 1/8" = 1'-0"



**2** SOUTH TERMINAL SOUTH PROW ROOF FRAMING PLAN  
S-809 SCALE: 1/8" = 1'-0"



- SHEET NOTES:**
- FOR GENERAL NOTES, SEE DWGS S-001 TO S-004.
  - FOR TYPICAL DETAILS REFER TO S-1112 TO S-1120.
  - TYPICAL ROOF CONSTRUCTION: 3" 18 GAGE GALVANIZED STEEL ROOF DECK. FASTEN AT STEEL SUPPORTS w/ 5/8" PUDDLE WELDS AT 8" O.C. & PROVIDE (3) #10 SCREW SIDELAP FASTENERS.
  - FOR EXACT LOCATION OF ALL STEEL AROUND VERTICAL CIRCULATION OPENINGS, COORDINATE WITH VERTICAL CIRCULATION MANUFACTURER REQUIREMENTS.
  - TOP OF STEEL IS 54' - 0" CCD TYPICAL UNO.
  - Tx INDICATES STEEL TRUSS. SEE 1 / S-835 FOR SIZE AND INFORMATION.
  - SEE S-833 & S-834 FOR MOMENT FRAME ELEVATIONS.
  - FOR INFORMATION REGARDING ROOF PENETRATIONS FOR STAIRS, ESCALATORS, AND ELEVATORS, SEE ARCH DWGS.
  - SEE MEP DRAWINGS FOR LOCATIONS OF ROOF PENETRATIONS, SEE 2 / S-1115



**cta** CHICAGO TRANSIT AUTHORITY ENGINEERING

**SENSITIVE SECURITY INFORMATION**  
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**95th Terminal Improvement**  
15 West 95th Street (South Terminal)  
Chicago, IL 60628

IN CHARGE	J. JAKALSKI
APPROVED BY	J. JAKALSKI
CHECKED BY	R.S. NAIR
DESIGNED BY	G. FORKAN
DRAWN BY	M. DONAHUE
PROJECT NO.	2012-0021.02
FILE NAME	DR095_S-Terminal

MARK	DATE	DESCRIPTION
	2/9/17	ISSUED FOR CONSTRUCTION

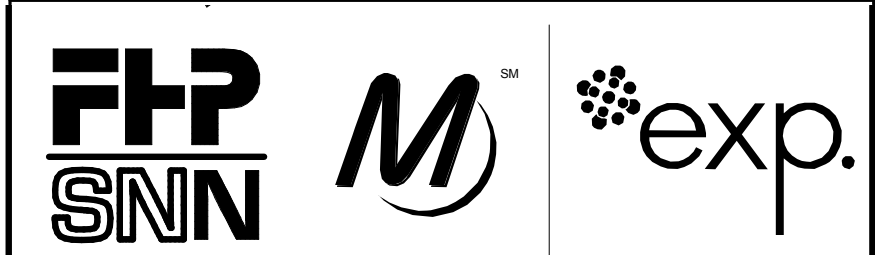
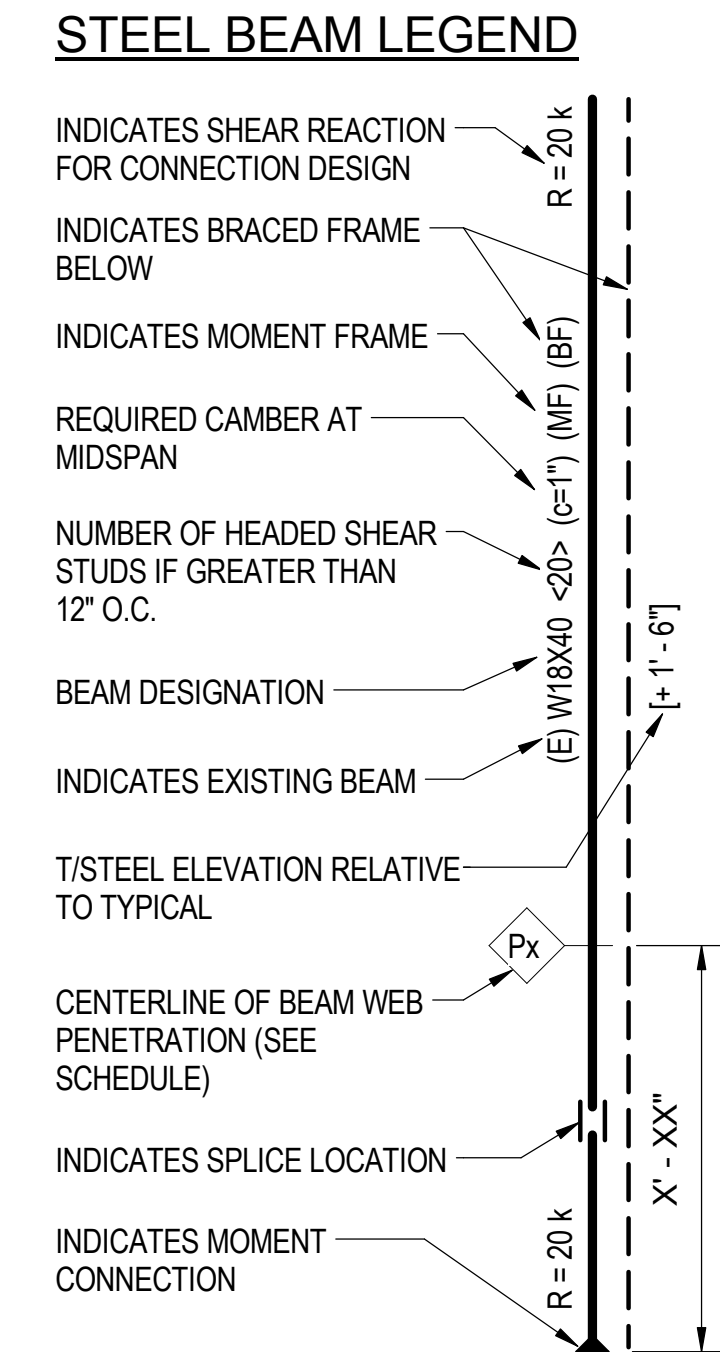
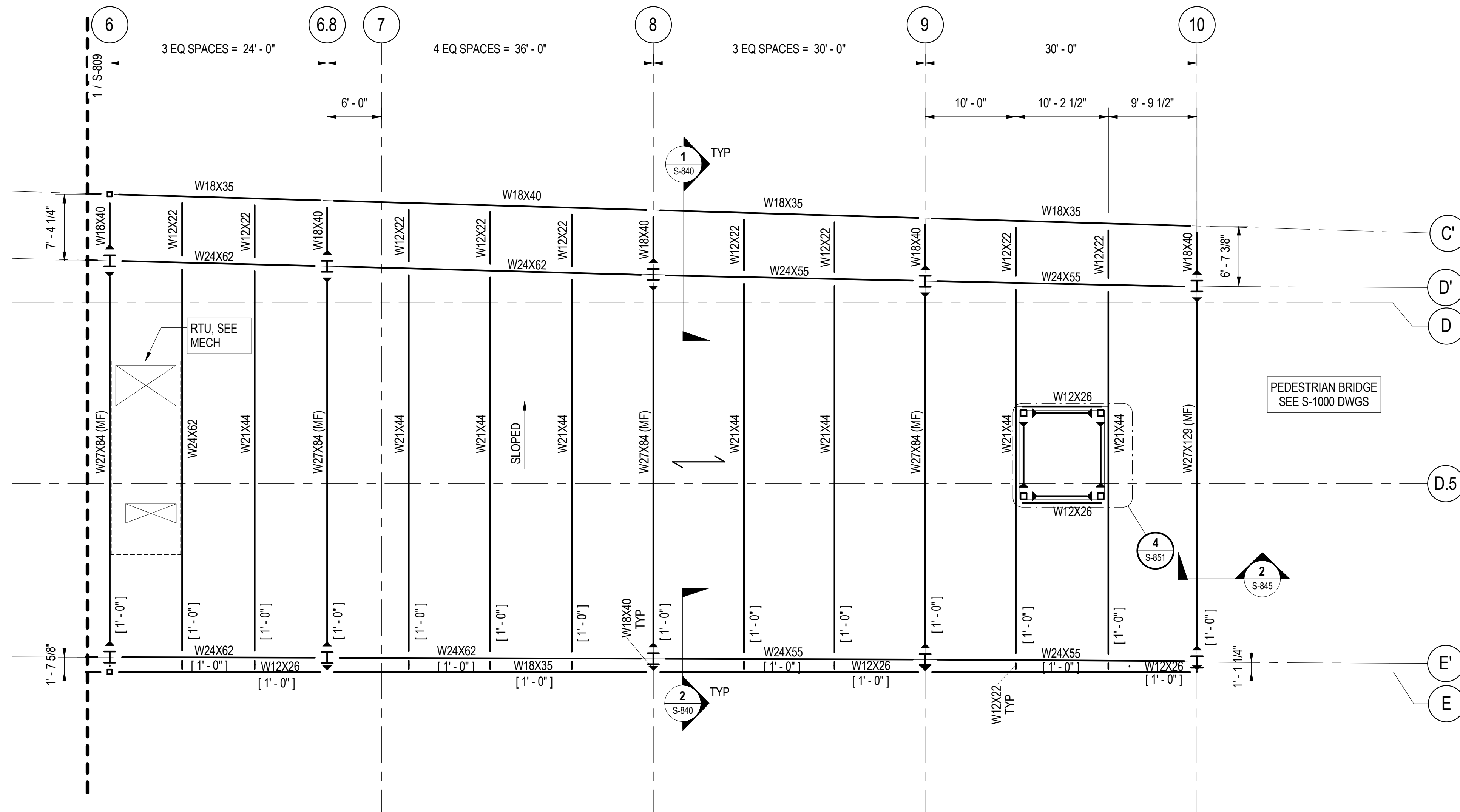
LOCATION IDENTIFIER: **DR095**

**SOUTH TERMINAL ROOF FRAMING PLAN - AREA R.1**

**S-809**

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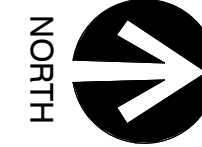
MARK	DATE	DESCRIPTION
	2/9/17	ISSUED FOR CONSTRUCTION

LOCATION IDENTIFIER: DR095

**SOUTH TERMINAL ROOF FRAMING PLAN - AREA R.2**

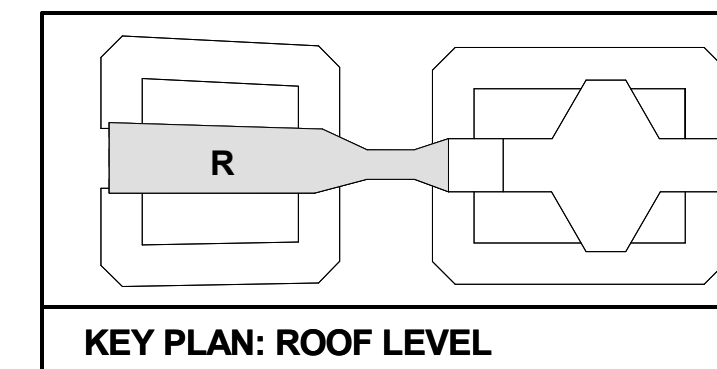
**S-810**

**1 SOUTH TERMINAL ROOF FRAMING PLAN - AREA R.2**  
 S-810 SCALE: 1/8" = 1'-0"



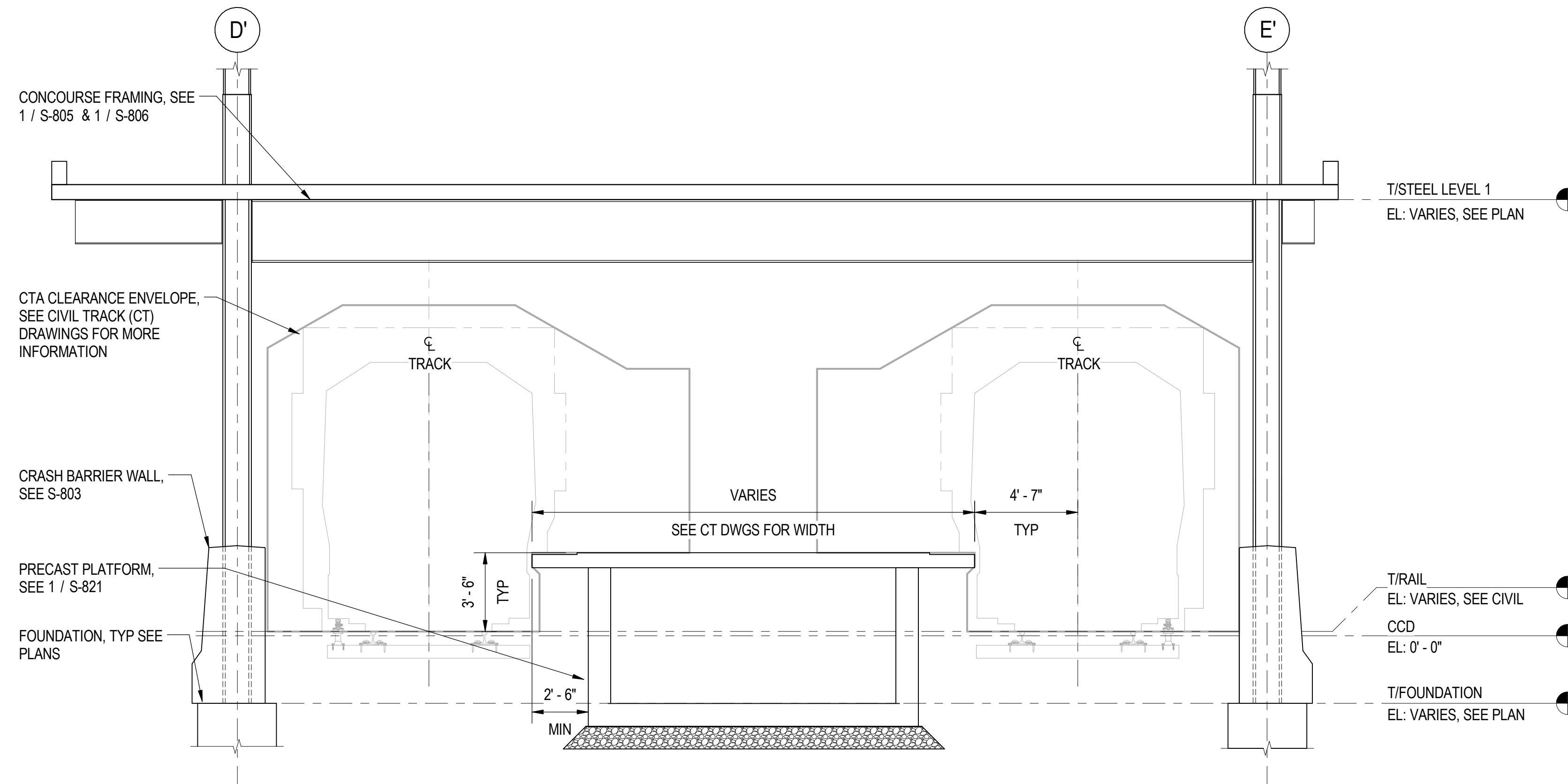
**SHEET NOTES:**

- FOR GENERAL NOTES, SEE DWGS S-001 TO S-004.
- FOR TYPICAL DETAILS REFER TO S-1112 TO S-1120.
- TYPICAL ROOF CONSTRUCTION: 3" 18 GAGE GALVANIZED STEEL ROOF DECK. FASTEN AT STEEL SUPPORTS w/ 5/8" PUDDLE WELDS AT 8" O.C. & PROVIDE (3) #10 SCREW SIDELAP FASTENERS.
- FOR EXACT LOCATION OF ALL STEEL AROUND VERTICAL CIRCULATION OPENINGS, COORDINATE WITH VERTICAL CIRCULATION MANUFACTURER REQUIREMENTS.
- TOP OF STEEL IS 54' - 0" CCD TYPICAL UNO.
- SEE S-833 & S-834 FOR MOMENT FRAME ELEVATIONS.
- FOR INFORMATION REGARDING ROOF PENETRATIONS FOR STAIRS, ESCALATORS, AND ELEVATORS, SEE ARCH DWGS.
- SEE MEP DRAWINGS FOR LOCATIONS OF ROOF PENETRATIONS, SEE 2 / S-1115.

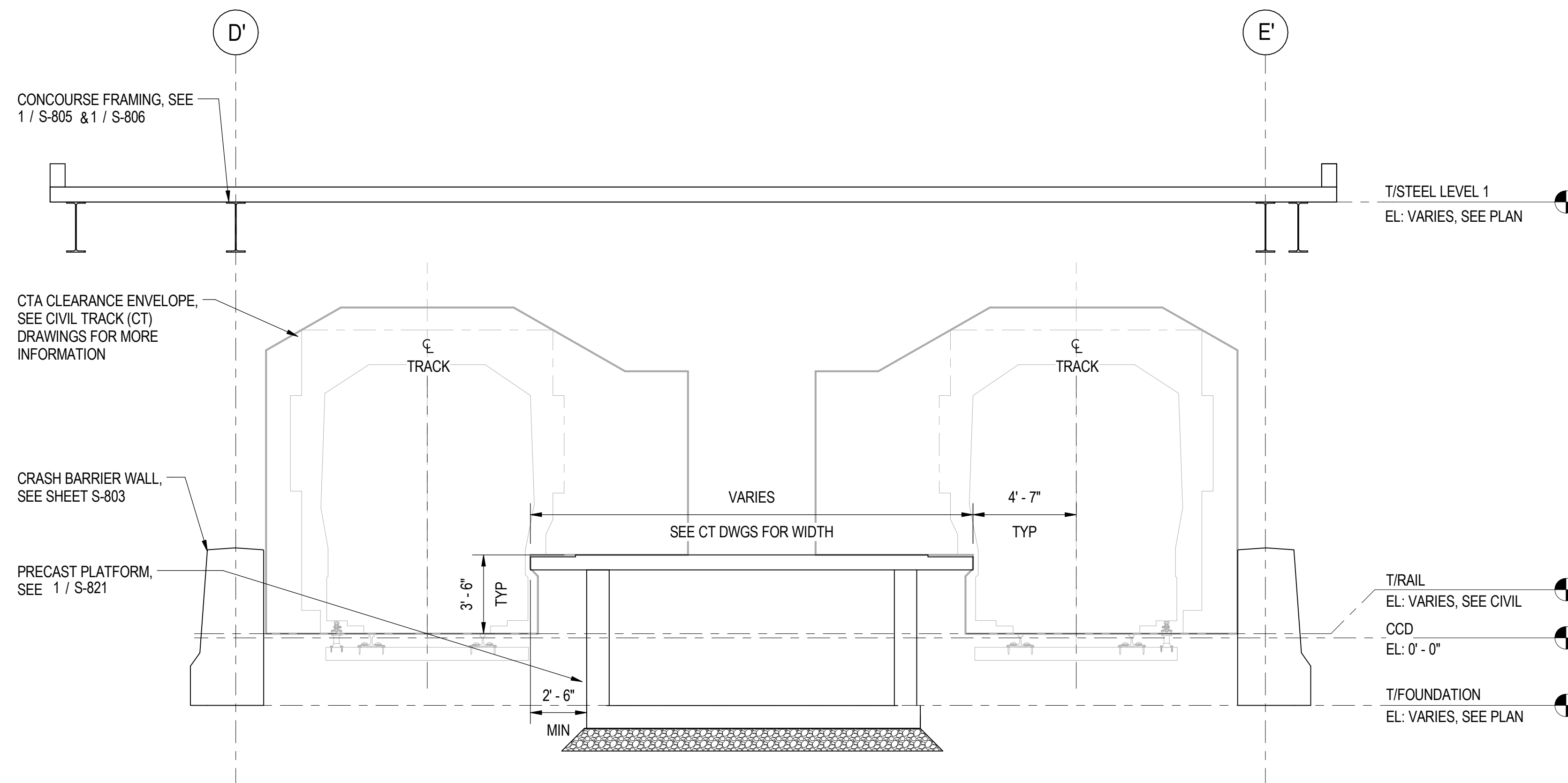


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**1** TYPICAL SOUTH PLATFORM SECTION AT COLUMN  
S-820 SCALE: 1/4" = 1'-0"



**2** TYPICAL SOUTH PLATFORM SECTION BETWEEN COLUMNS  
S-820 SCALE: 1/4" = 1'-0"



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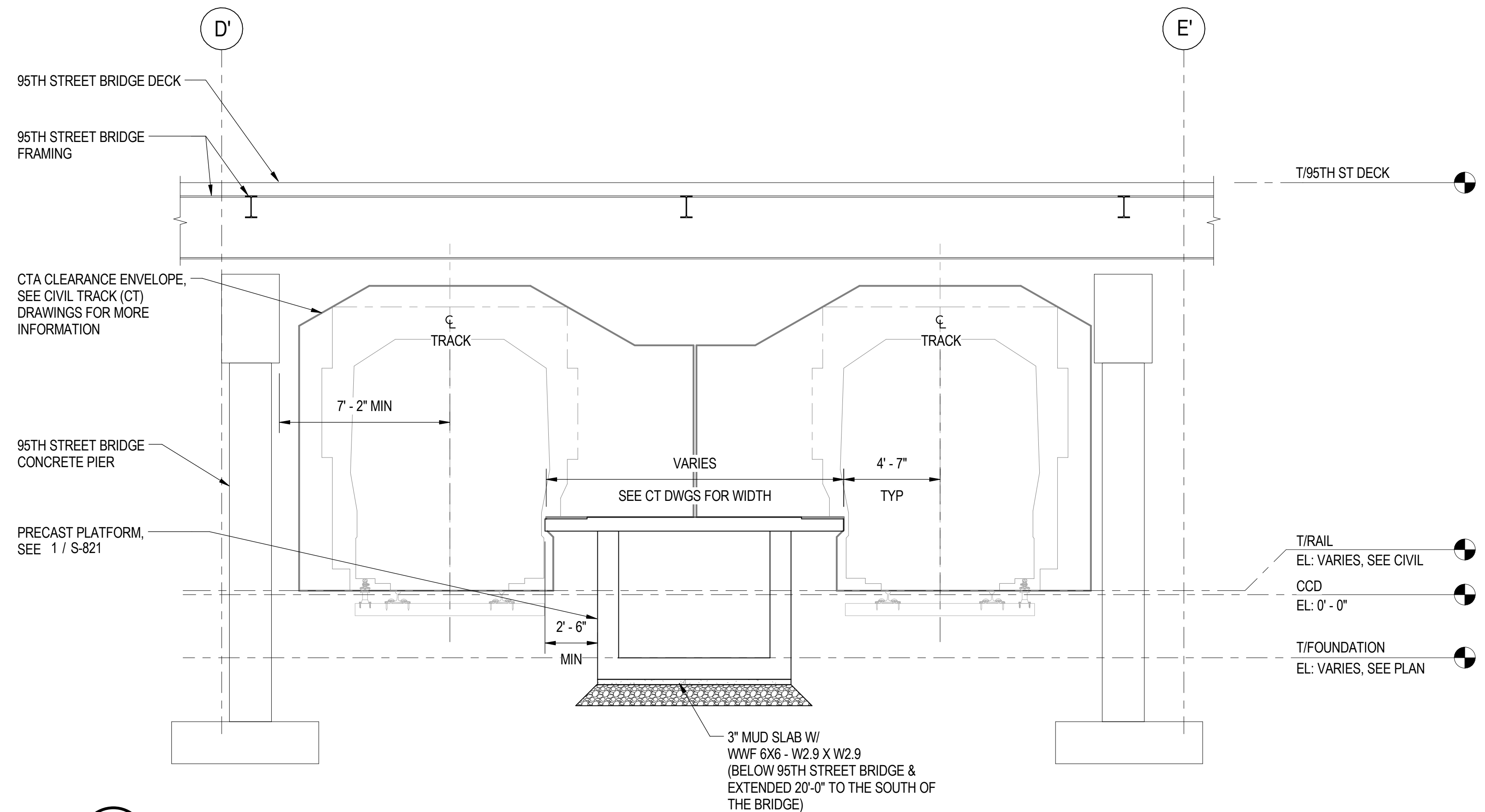
IN CHARGE	J. JAKALSKI
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DRAWN BY	J. PEARSON
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FILE NAME	DR095_S-Terminal

MARK	DATE	DESCRIPTION
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LOCATION IDENTIFIER: DR095

**SOUTH TERMINAL FOUNDATION TRANSVERSE BUILDING SECTION**

S-820



**1** TYPICAL SOUTH TERMINAL PLATFORM SECTION AT 95TH STREET  
 S-820A SCALE: 1/4" = 1'-0"

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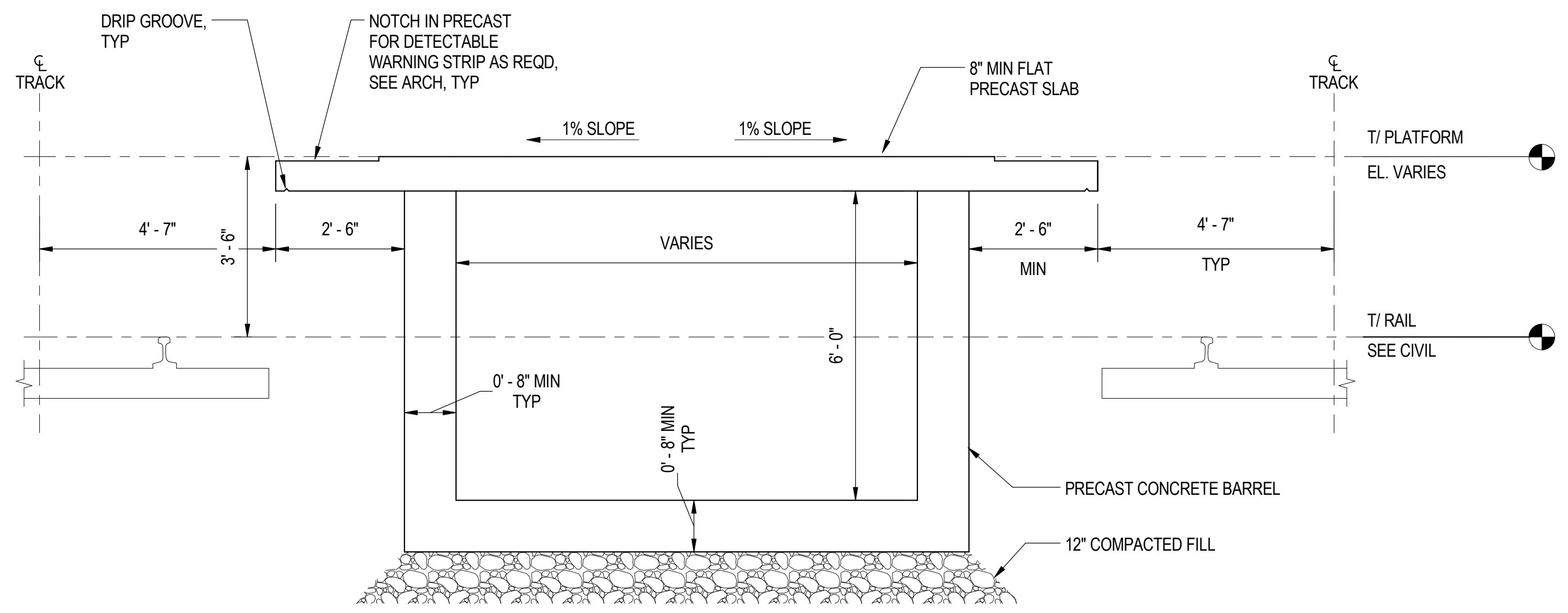
MARK	DATE	DESCRIPTION
	2/9/17	ISSUED FOR CONSTRUCTION

LOCATION IDENTIFIER: **DR095**  
**SOUTH TERMINAL FOUNDATION  
 TRANSVERSE BUILDING SECTION  
 AT 95TH STREET**

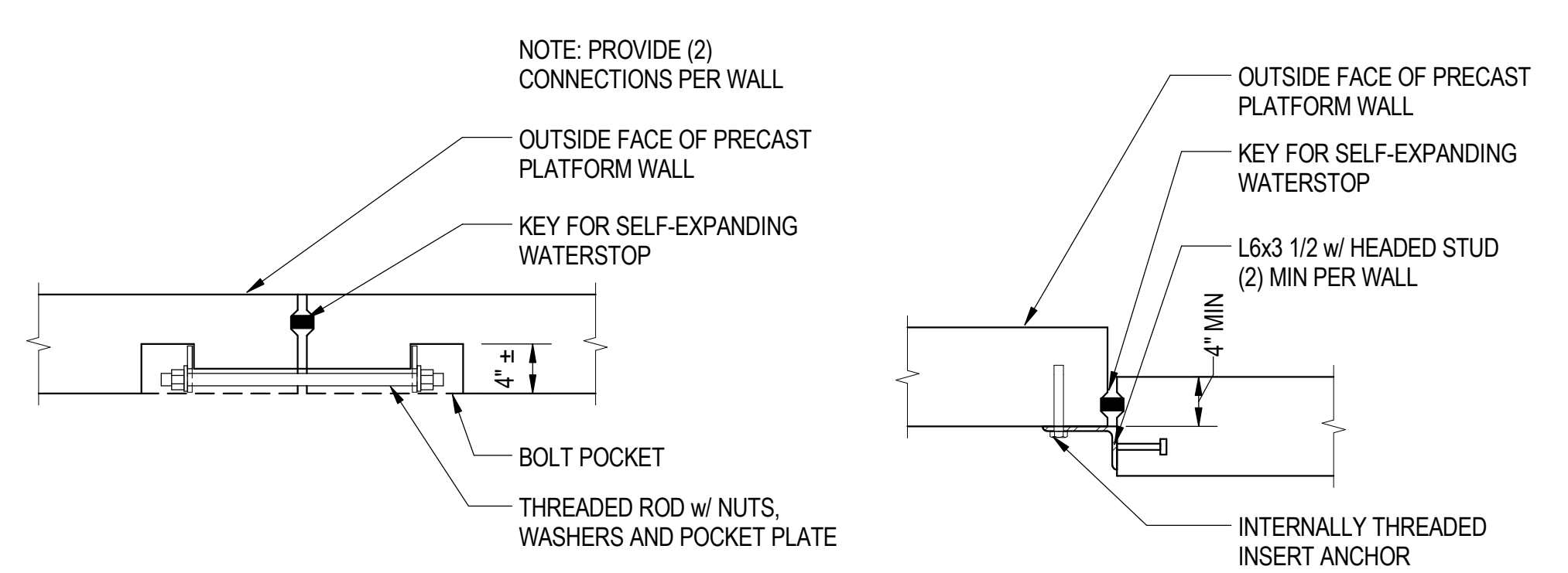
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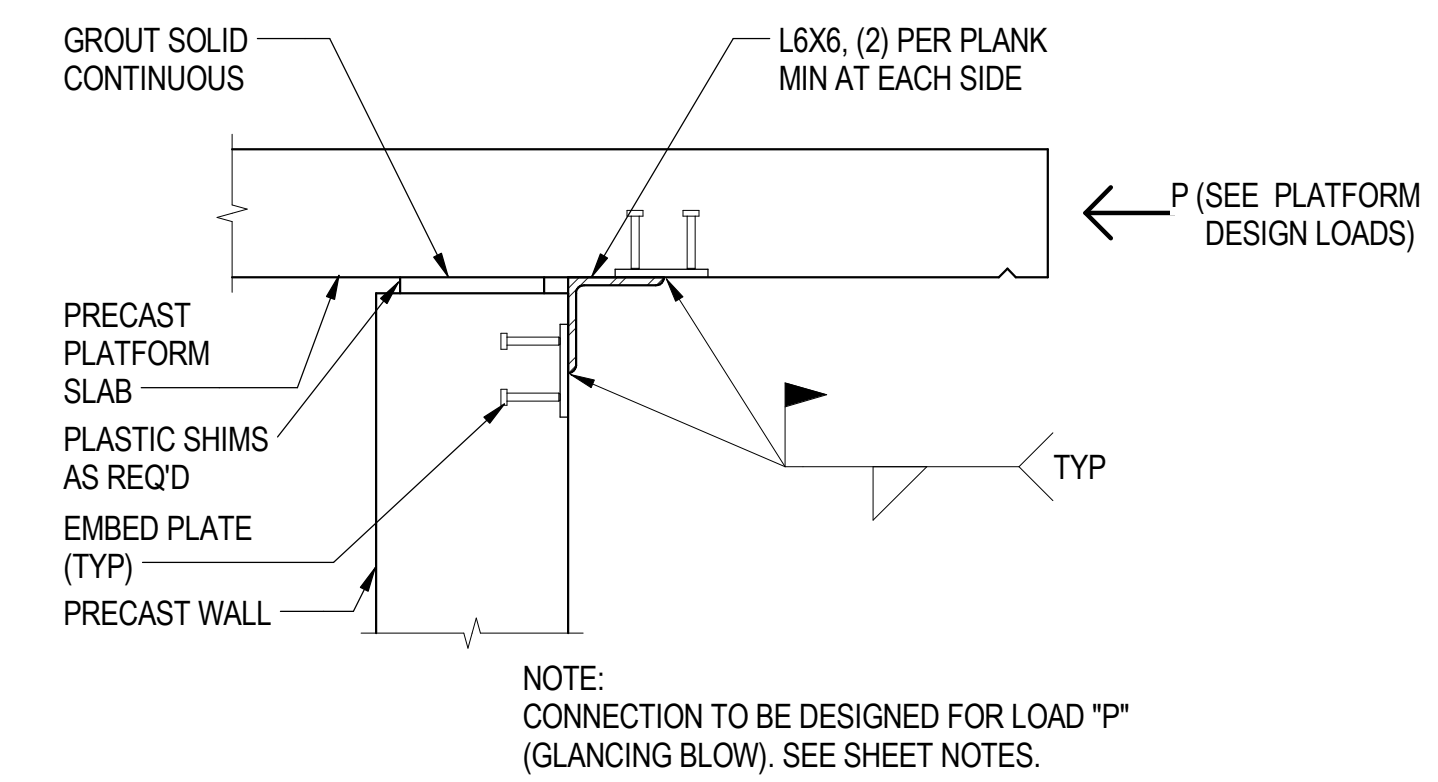
**1** TYPICAL PLATFORM SECTION  
S-821 SCALE: 1/2" = 1'-0"



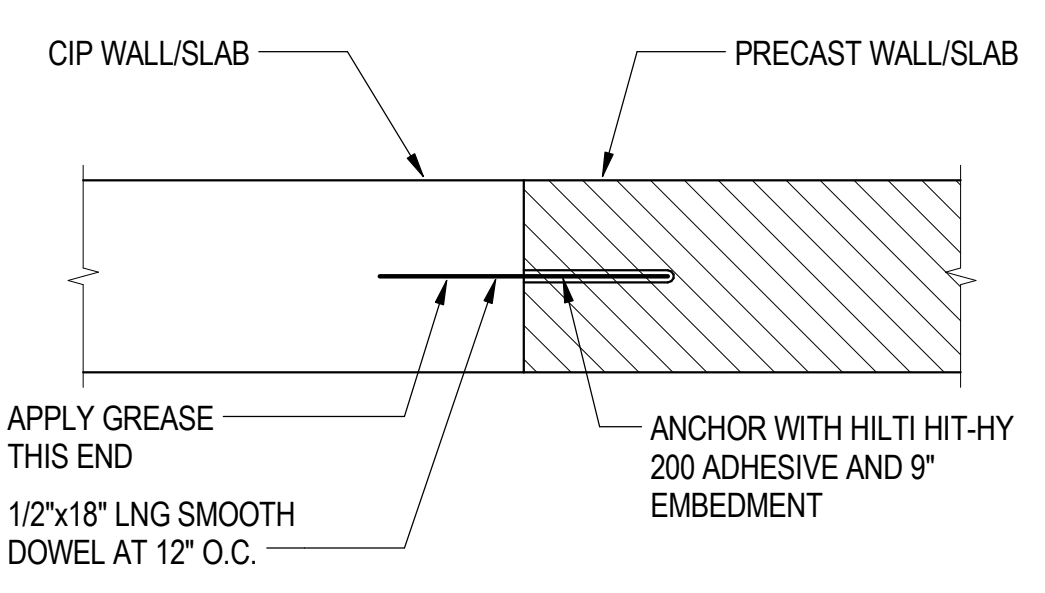
**2** TYPICAL PRECAST WALL TO WALL CONNECTION - PLAN  
S-821 SCALE: 1" = 1'-0"

**SHEET NOTES:**

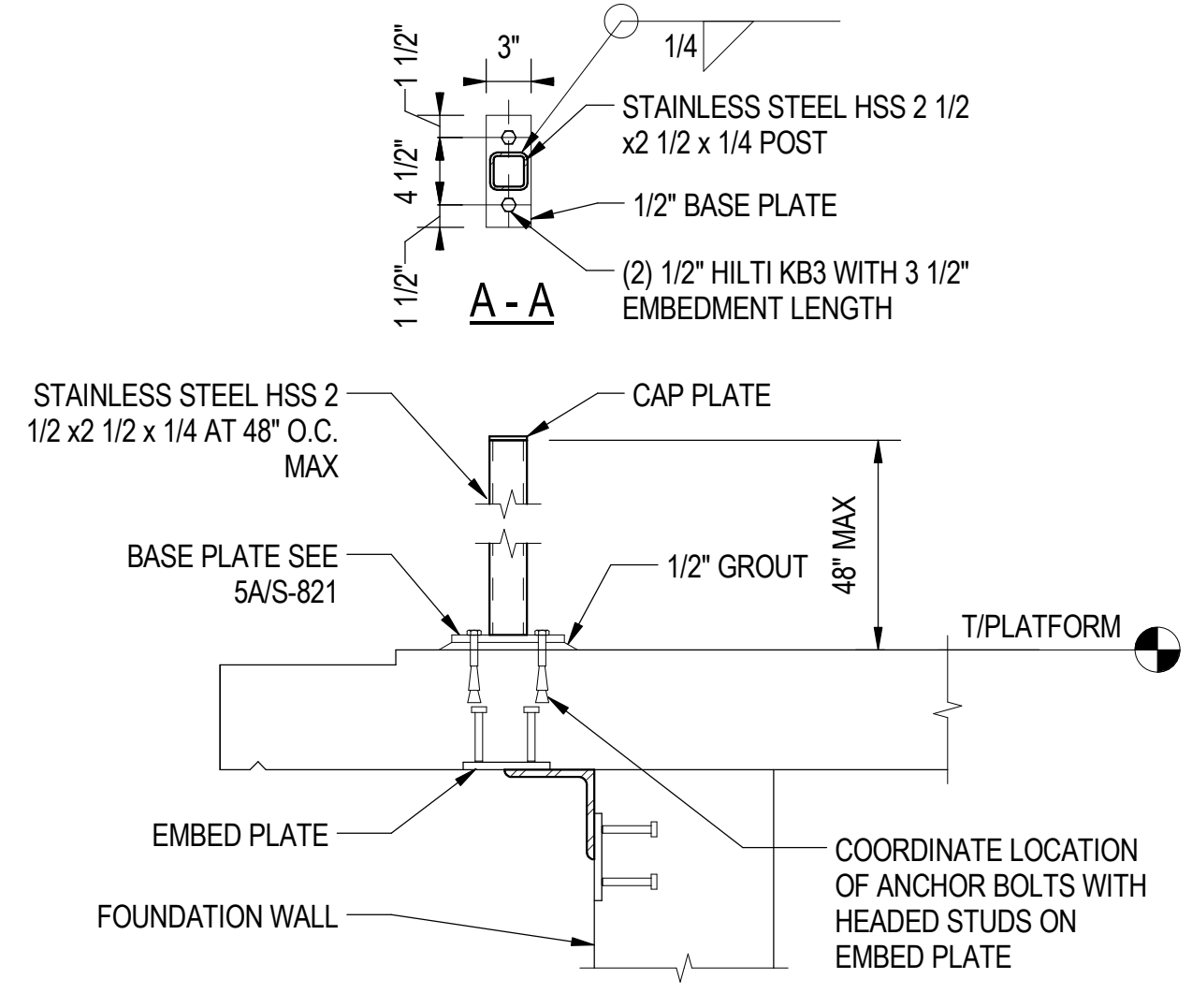
- FOR GENERAL NOTES, SEE DWGS S-001 TO S-004.
- FOR TYPICAL PLATFORM DETAILS REFER TO S-821
- ALL MEP PENETRATIONS ARE NOT SHOWN. CONTRACTOR TO COORDINATE ALL PRECAST PLATFORM OPENINGS AND PENETRATIONS WITH ARCH AND MEP DRAWINGS PRIOR TO FABRICATION. ALL REQUIRED PENETRATIONS THROUGH THE PRECAST LARGER THAN 12" BY 12" SHALL BE CAST IN THE PLANT AND SUBMITTED TO THE DOR FOR REVIEW AND APPROVAL PRIOR TO FABRICATION.
- PRECAST PLATFORM AND BARREL SIZES SHOWN ARE MINIMUM SIZES. THE LAYOUT SHOWN ON THE PLANS AND DETAILS IS SUGGESTED ONLY, AS A MEANS TO CLARIFY THE TYPE OF ELEMENTS REQUIRED TO CONSTRUCT THE PLATFORM. REFER TO S-821 FOR TYPICAL CONNECTION DETAILS BETWEEN PRECAST COMPONENTS. THE FINAL DESIGN AND DETAILING FOR ALL PRECAST PLATFORM SECTIONS AND CONNECTIONS ARE THE RESPONSIBILITY OF THE PRECAST FABRICATOR AND THEIR STRUCTURAL ENGINEER FOR THE LOADS AND EXTENTS PROVIDED ON THE DRAWINGS AND IN THE SPECIFICATIONS. SHOP DRAWINGS SHALL BE SUBMITTED FOR FINAL REVIEW AND APPROVAL PRIOR TO PROCEEDING WITH ANY FABRICATION.
- REFER TO TRACK DWGS FOR CTA TRACK CLEARANCE REQUIREMENTS NOT SHOWN.
- PRECAST SYSTEM CONSISTS OF PRECAST BARREL SECTIONS THAT SHALL BE CAST IN PIECES TO LENGTHS REQUIRED BY THE PRECASTER FOR TRANSPORTATION AND INSTALLATION REQUIREMENTS THROUGHOUT THE LENGTH OF THE PLATFORM AS INDICATED ON THE PLANS. EACH BARREL WILL BE PLACED SO THAT IT IS A MINIMUM OF 2'-6" CLEAR FROM THE EDGE OF THE PLATFORM SLAB. THE DISTANCE BETWEEN THE INTERIOR WALLS OF THE BARRELS WILL VARY ALONG THE LENGTH OF THE PLATFORM.
- PLATFORM DESIGN LOADS ARE AS FOLLOWS:
  - LIVE LOAD: 100 PSF EXCEPT AS NOTED ON 7 / S-821
  - CONSTRUCTION LOAD: 30 PSF COMBINED WITH 3 KIP POINT LOAD
  - HORIZ. GLANCING BLOW (P): 20 KIP (4000 PLF OVER 5' PLATFORM EDGE)
  - PARTITION WALL LOAD: 950 PLF
  - STAIR COLUMN LOAD: 25 KIP
  - STAIR BASE LOAD: 10 KIP
  - BARRIER LOAD: ±0.50 KIP-FT / FT OVERTURNING



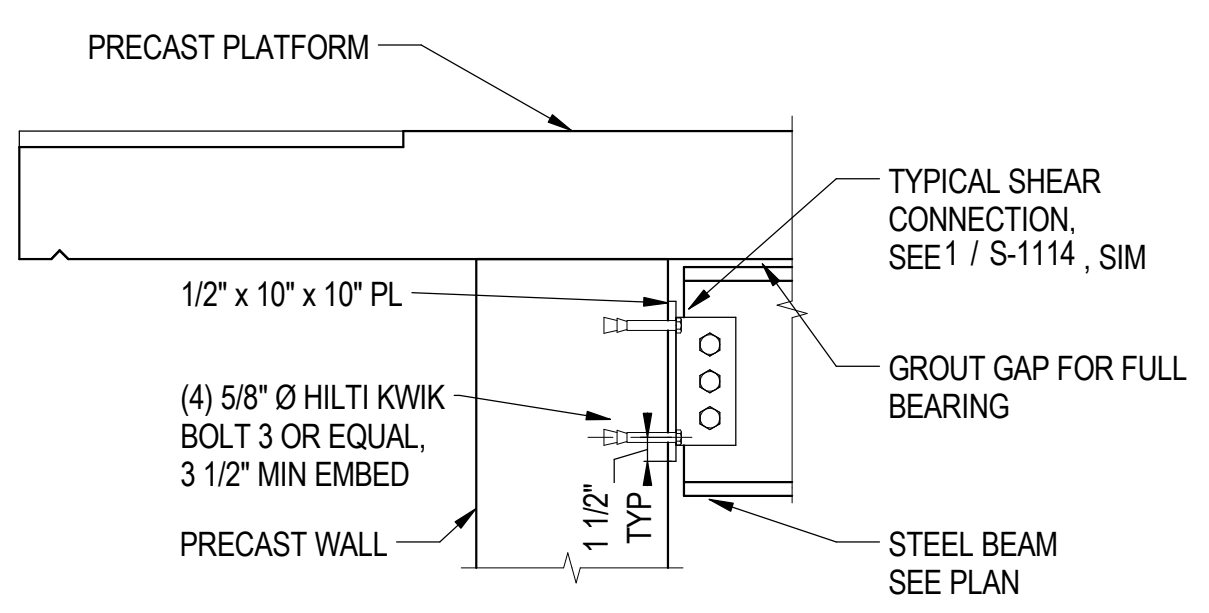
**3** TYPICAL PRECAST WALL TO PLATFORM CONNECTION  
S-821 SCALE: 1" = 1'-0"



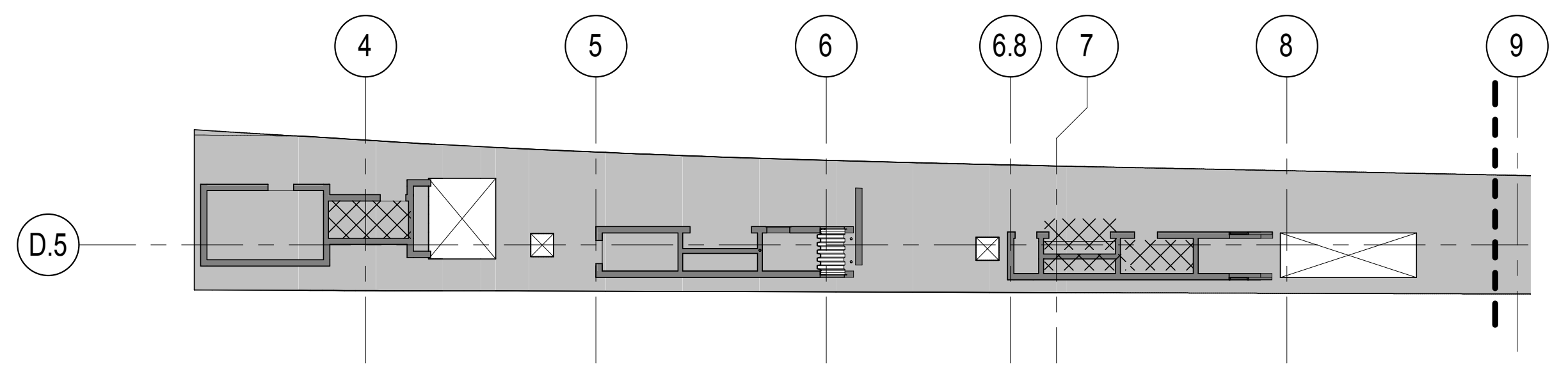
**4** TYPICAL PRECAST TO CIP DOWEL JOINT  
S-821 SCALE: 1" = 1'-0"



**5** BARRIER POST CONNECTION  
S-821 SCALE: 1" = 1'-0"



**6** STEEL BEAM CONNECTION TO PRECAST  
S-821 SCALE: 1" = 1'-0"



**7** SOUTH TERMINAL PLATFORM LOADING DIAGRAM  
S-821 SCALE: 1/16" = 1'-0"

**LIVE LOADING DIAGRAM LEGEND**

- 950 PLF LINE LOAD
- 200 PSF
- 100 PSF

- FOR LOCATION OF STAIRS, SEE PLATFORM PLANS  
- FOR LOCATION OF BARRIER POSTS & PARTITION WALLS, SEE ARCH



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DRAWN BY	C. BOVE
PROJECT NO.	2012-0021.02
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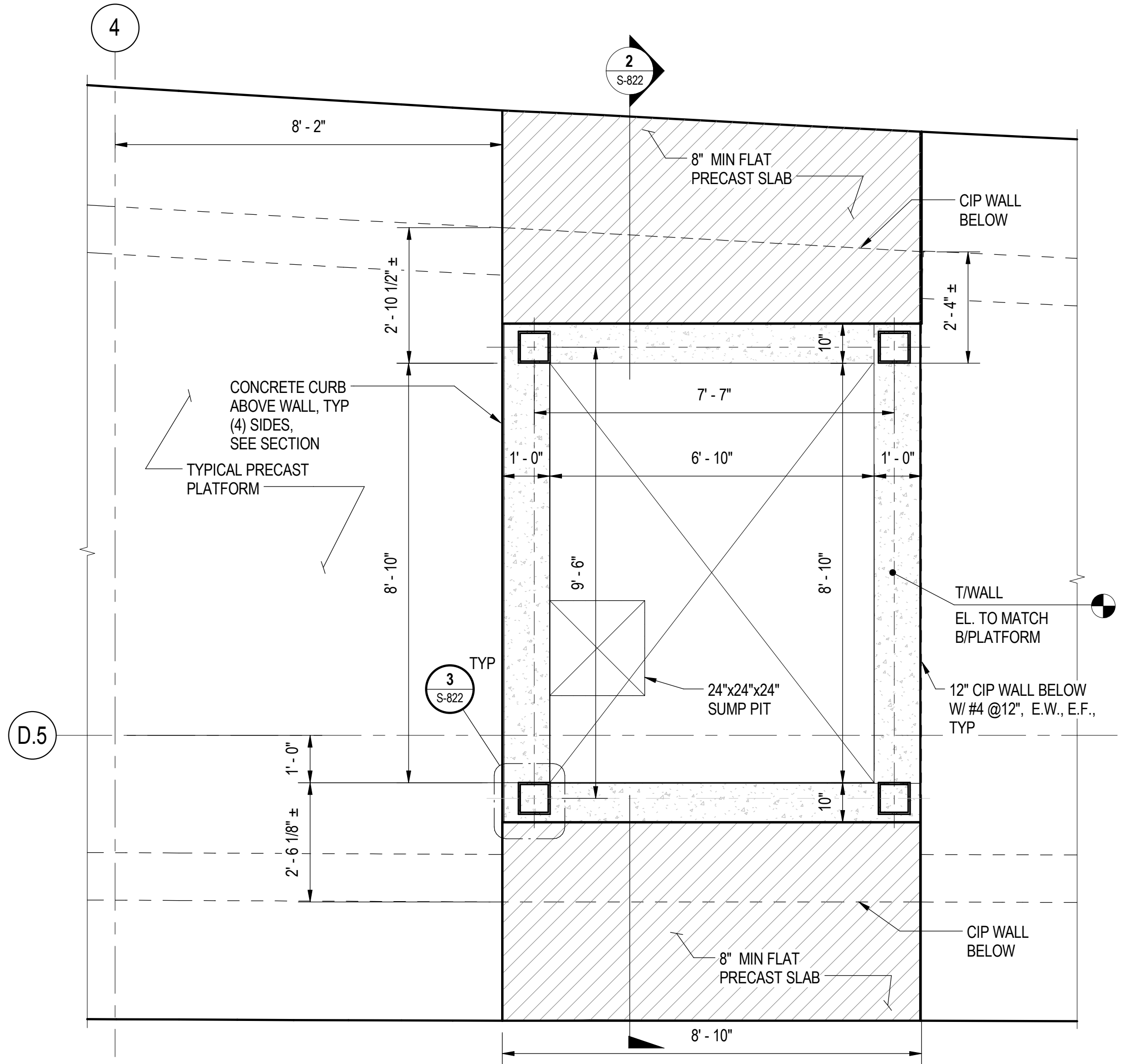
LOCATION IDENTIFIER: **DR095**

**SOUTH TERMINAL TYPICAL PLATFORM SECTIONS & DETAILS**

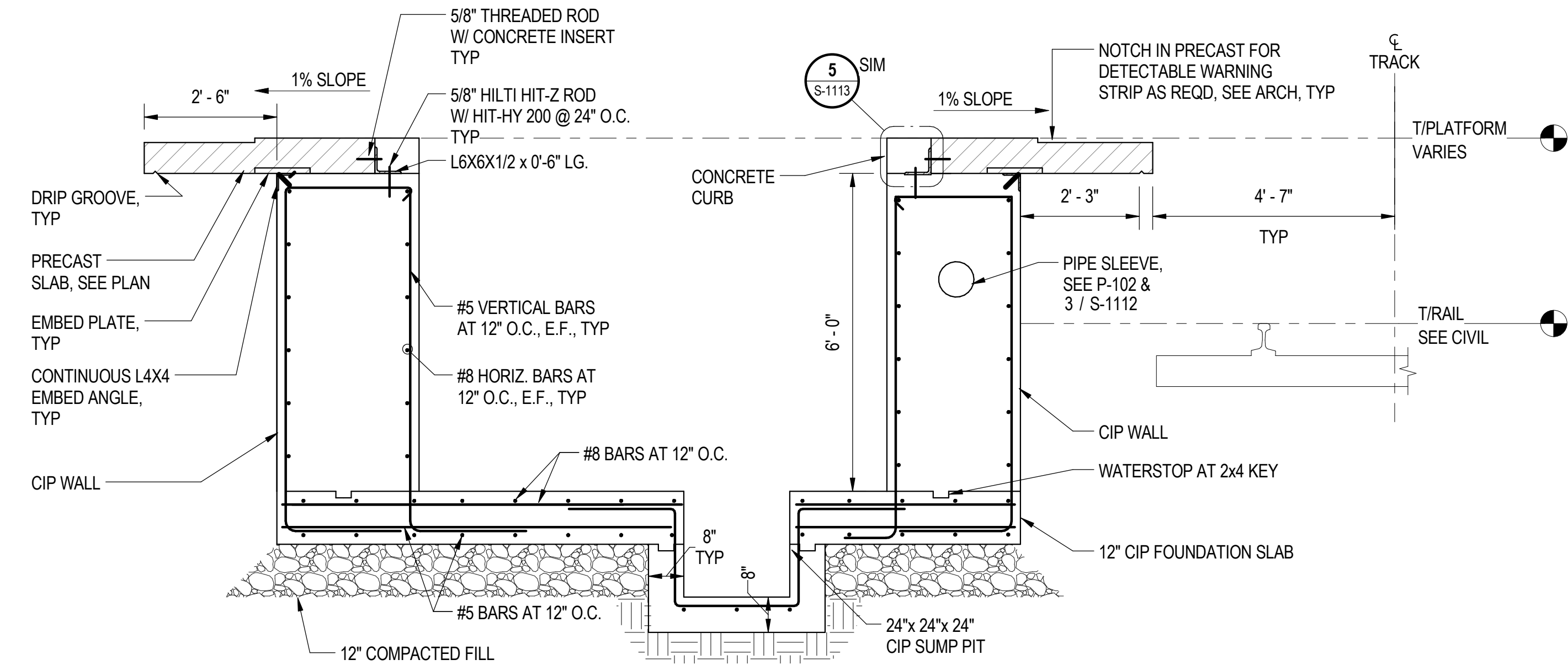
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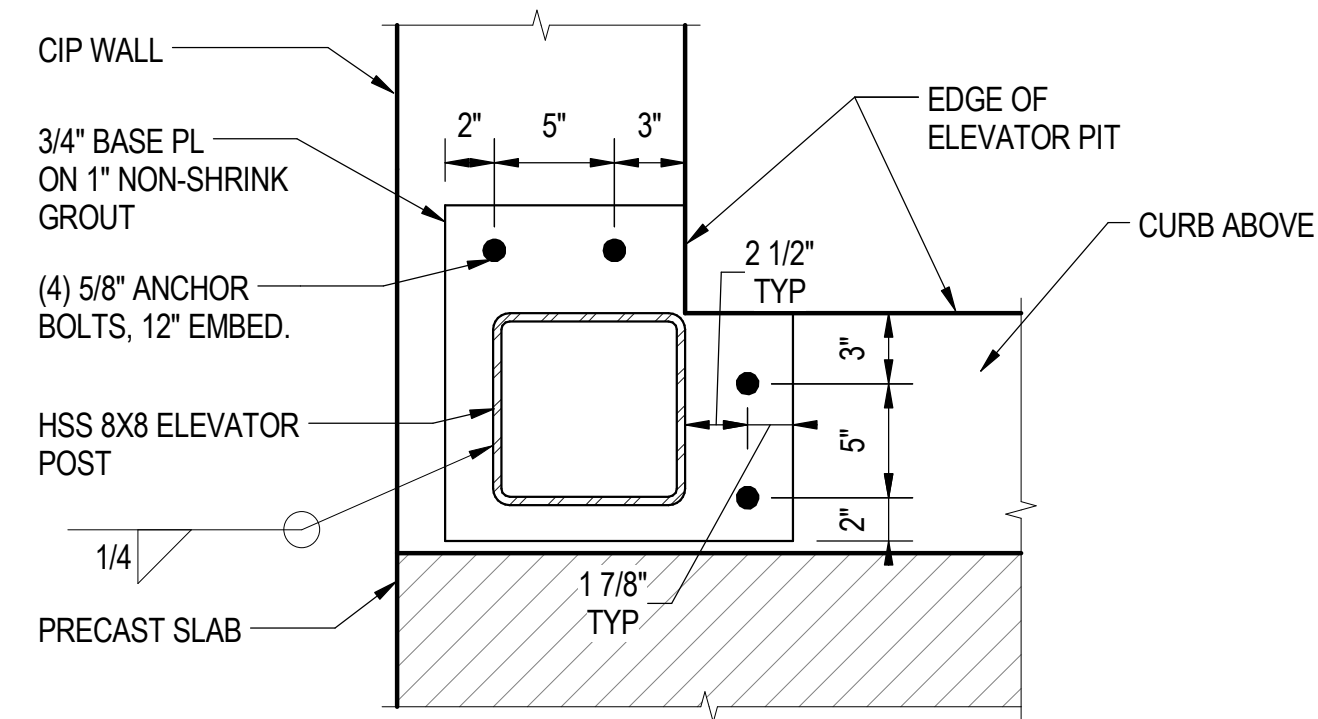
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**1** PLATFORM PLAN AT ELEVATOR #1  
S-822 SCALE: 1/2" = 1'-0"



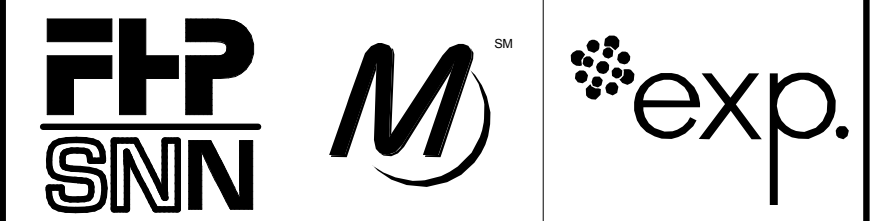
**2** ELEVATOR #1 FOUNDATION SECTION  
S-822 SCALE: 1/2" = 1'-0"



**3** ELEVATOR POST BASE  
S-822 SCALE: 1 1/2" = 1'-0"

**SHEET NOTES:**

- FOR GENERAL NOTES, SEE DWGS S-001 TO S-004.
- FOR TYPICAL DETAILS REFER TO S-1112 TO S-1120.
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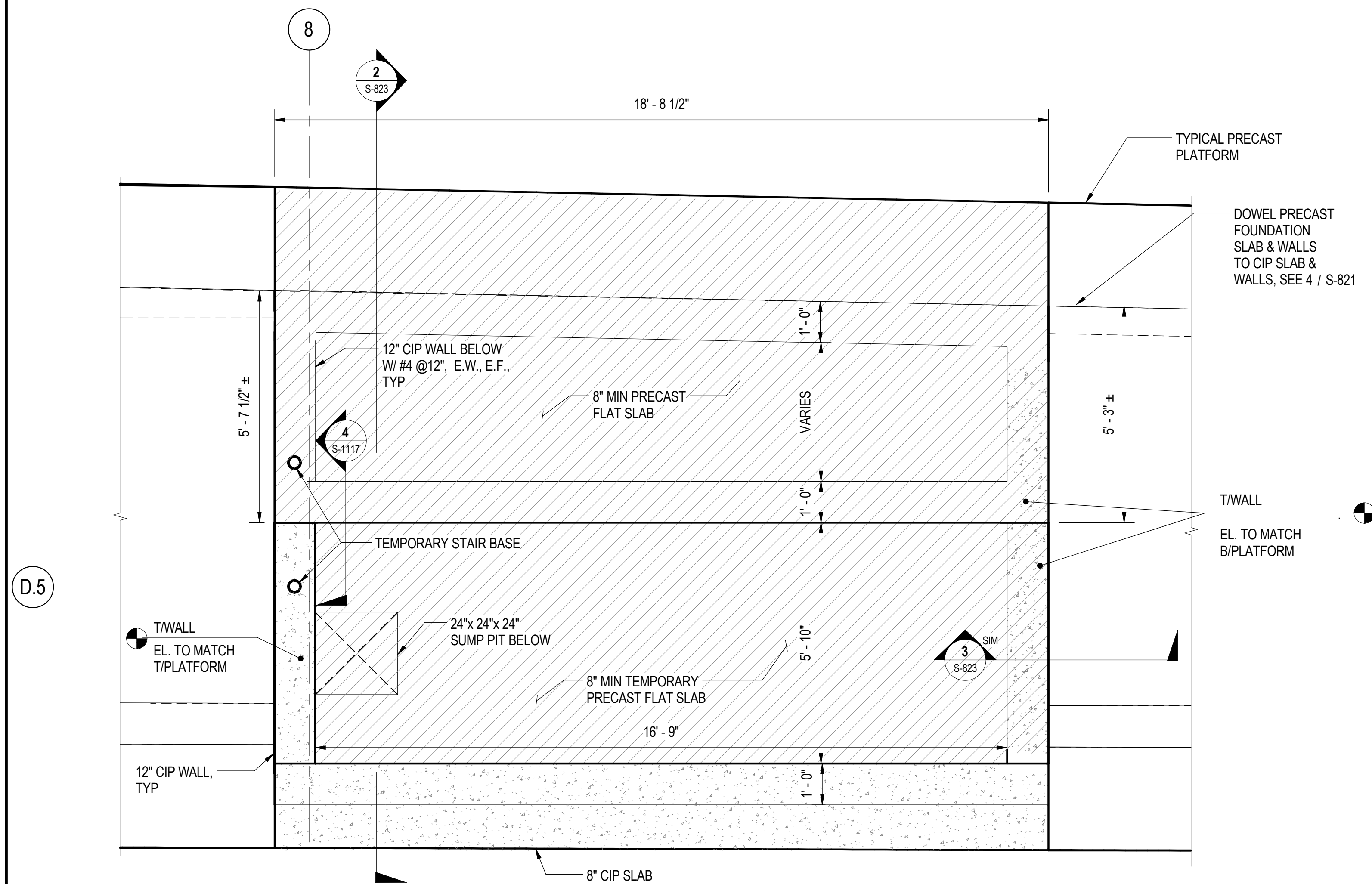
LOCATION IDENTIFIER: **DR095**

**SOUTH TERMINAL ELEVATOR FOUNDATION**

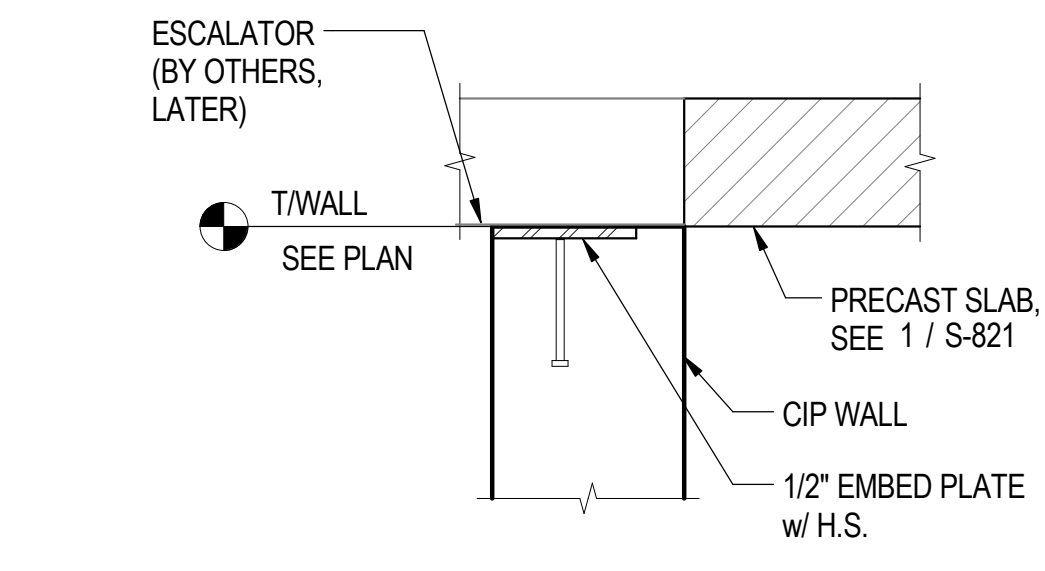
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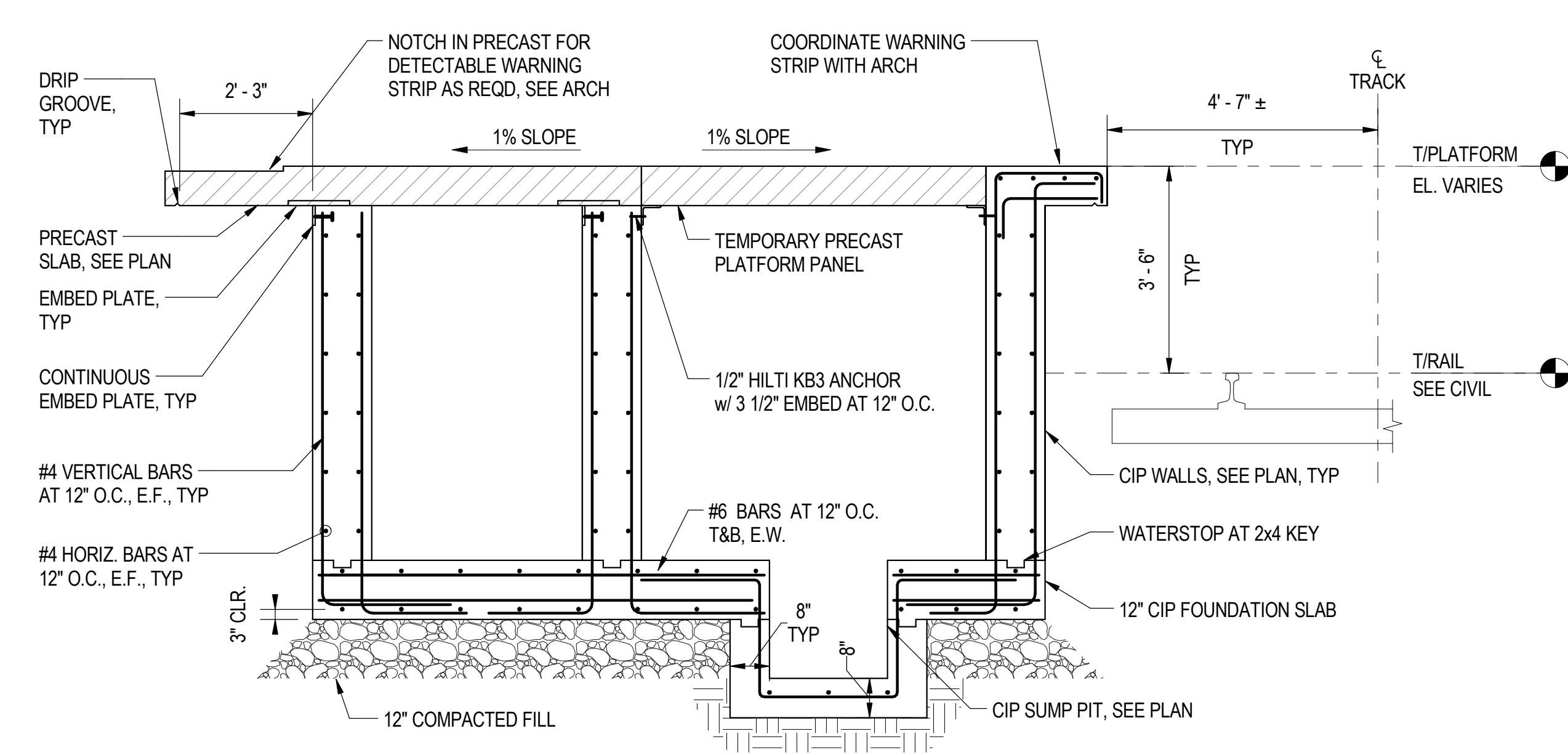
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**1** PLATFORM PLAN AT ESCALATOR #1  
S-823 SCALE: 1/2" = 1'-0"

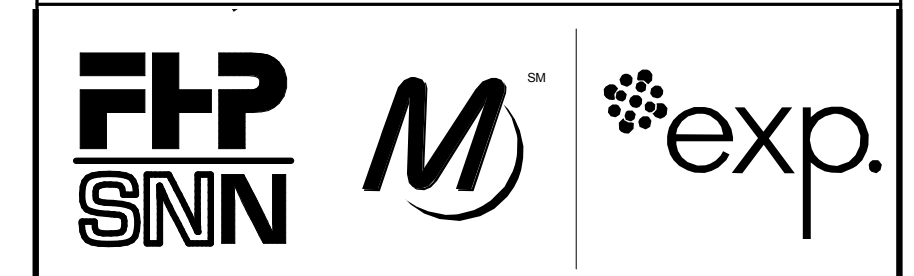


**3** ESCALATOR #1 BASE CONNECTION  
S-823 SCALE: 1" = 1'-0"



**2** ESCALATOR #1 FOUNDATION SECTION  
S-823 SCALE: 1/2" = 1'-0"

- SHEET NOTES:**
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  - FOR TYPICAL DETAILS REFER TO S-1112 TO S-1120.
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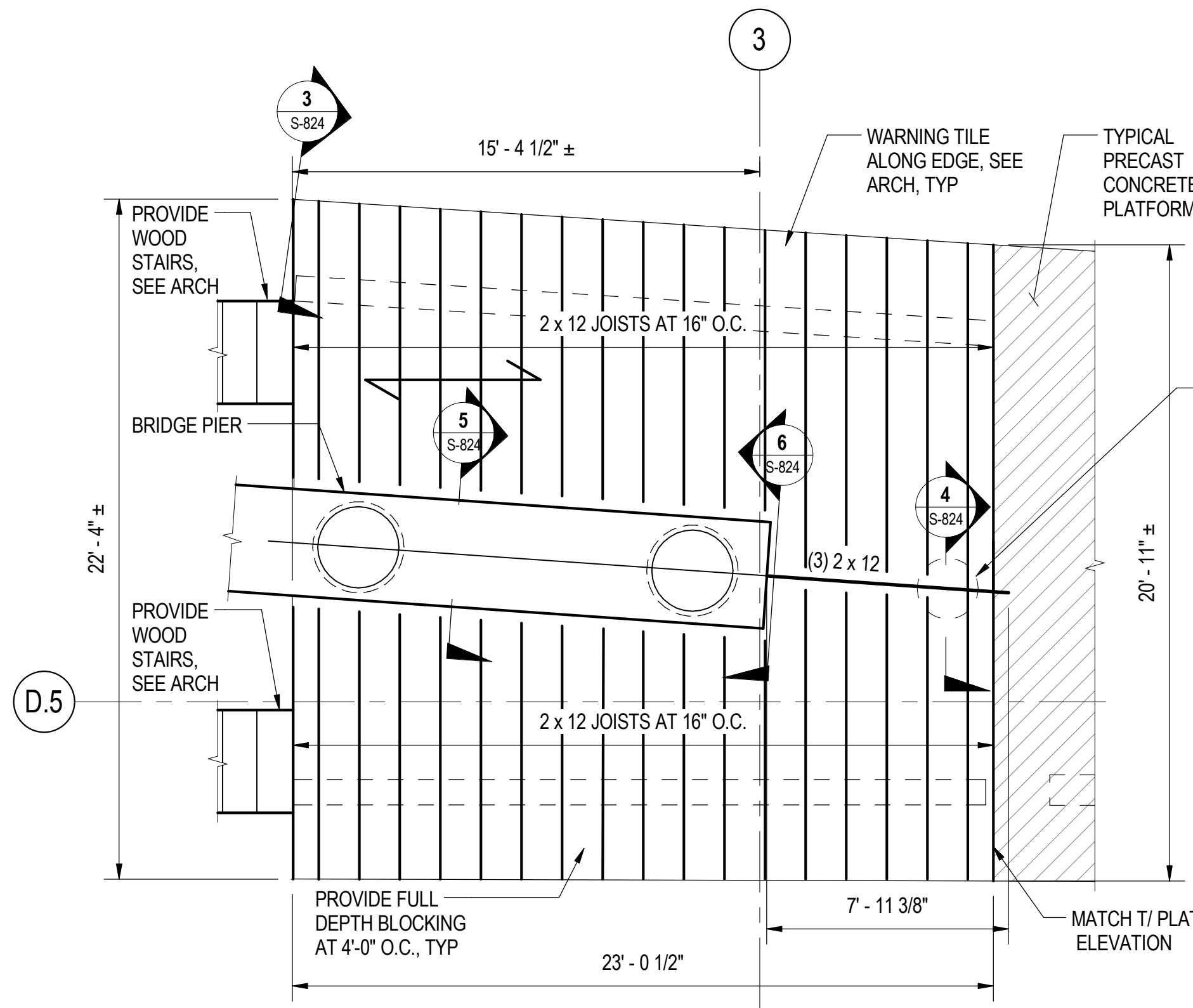
MARK	DATE	DESCRIPTION
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LOCATION IDENTIFIER: **DR095**  
**SOUTH TERMINAL ESCALATOR FOUNDATION**

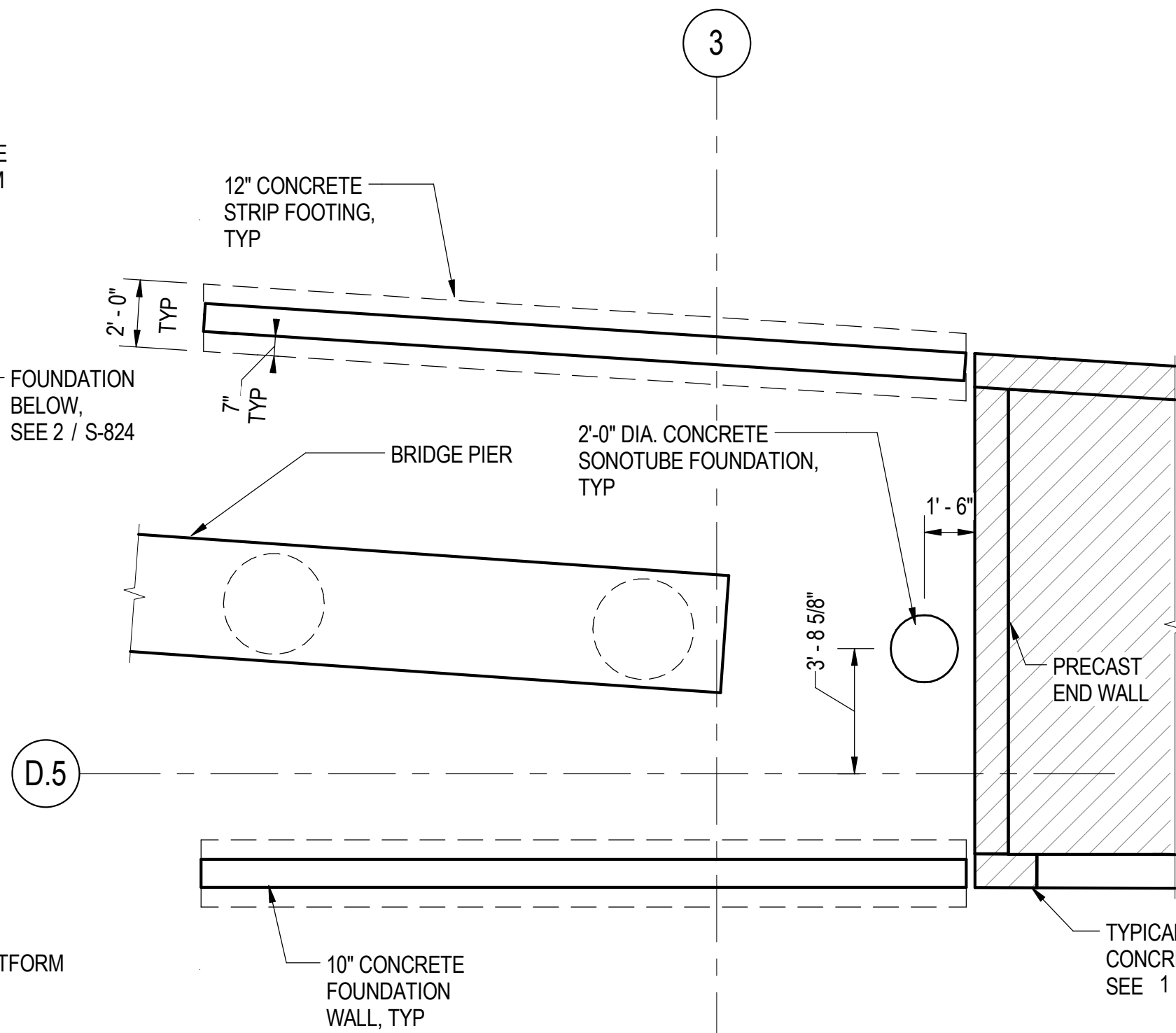
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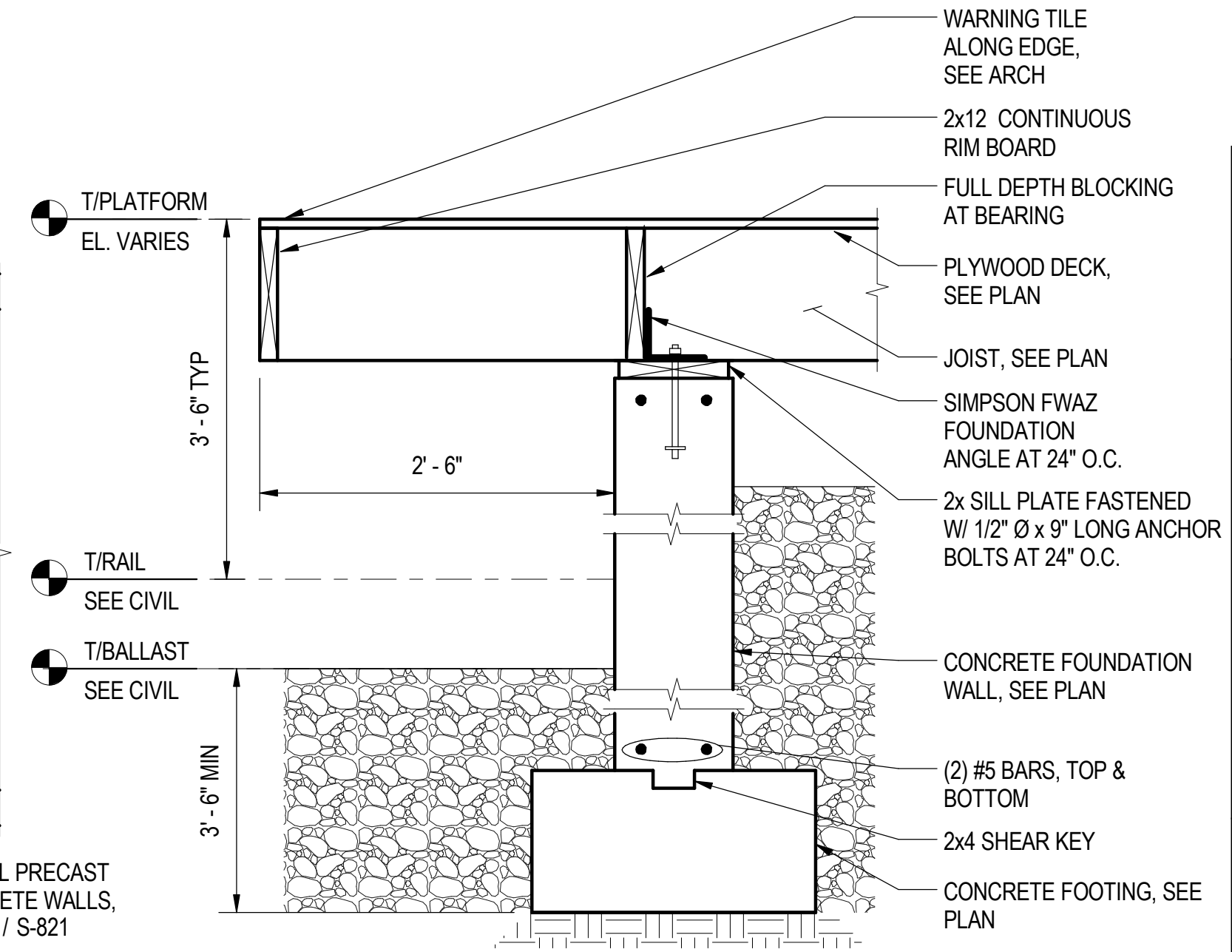
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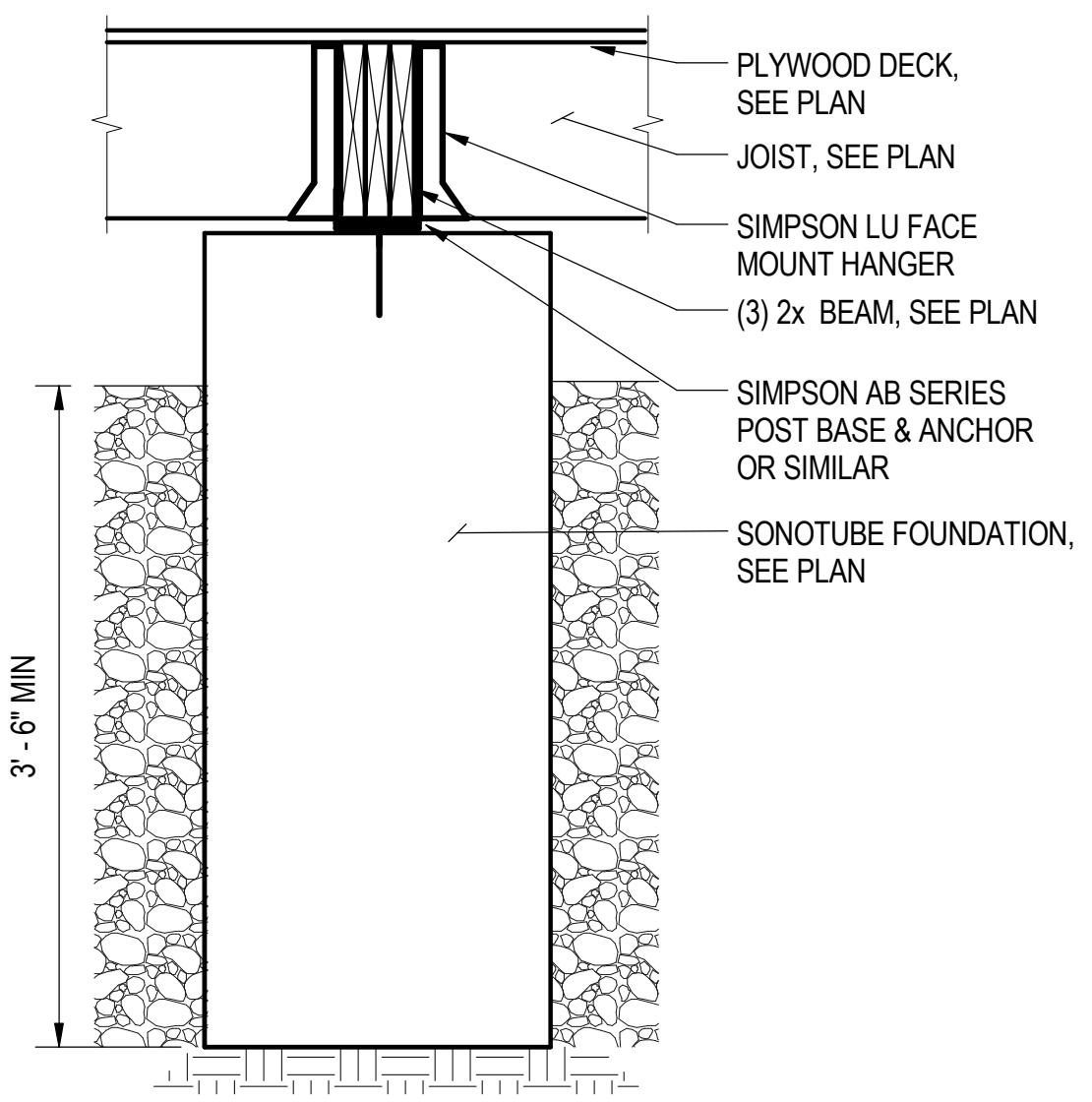
**1** SOUTH TERMINAL TEMPORARY PLATFORM PLAN  
S-824 SCALE: 1/4" = 1'-0"



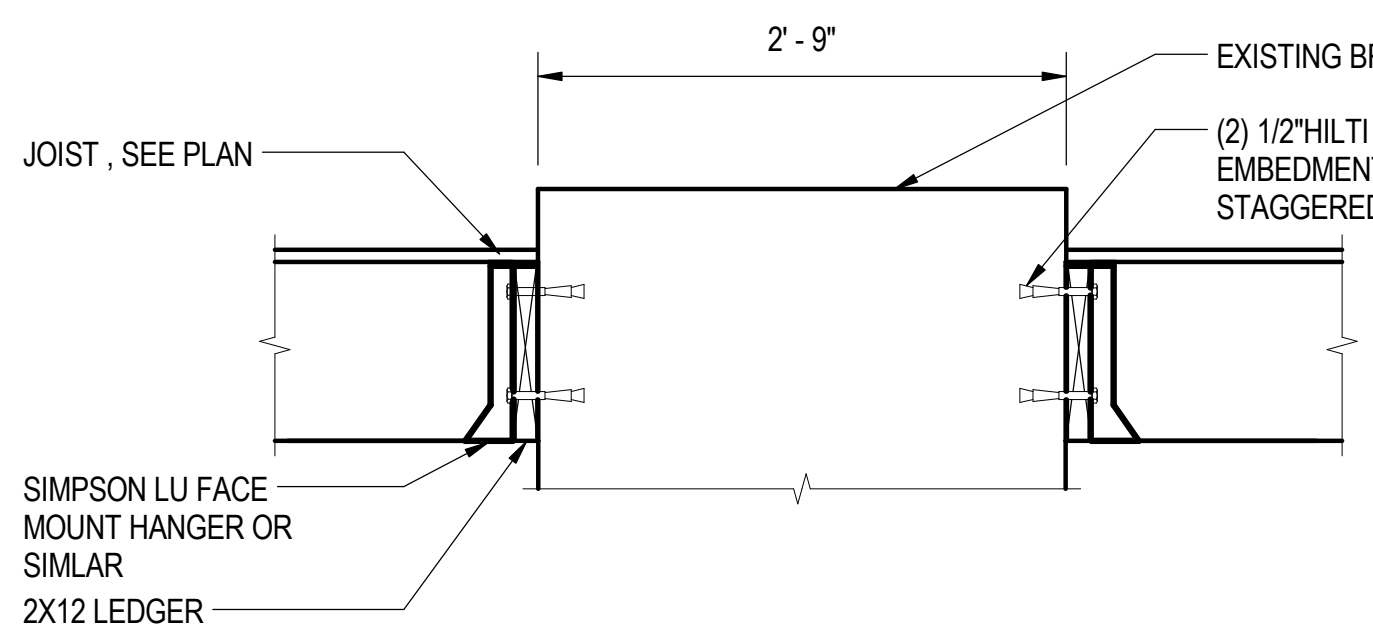
**2** SOUTH TERMINAL TEMPORARY PLATFORM FOUNDATION PLAN  
S-824 SCALE: 1/4" = 1'-0"



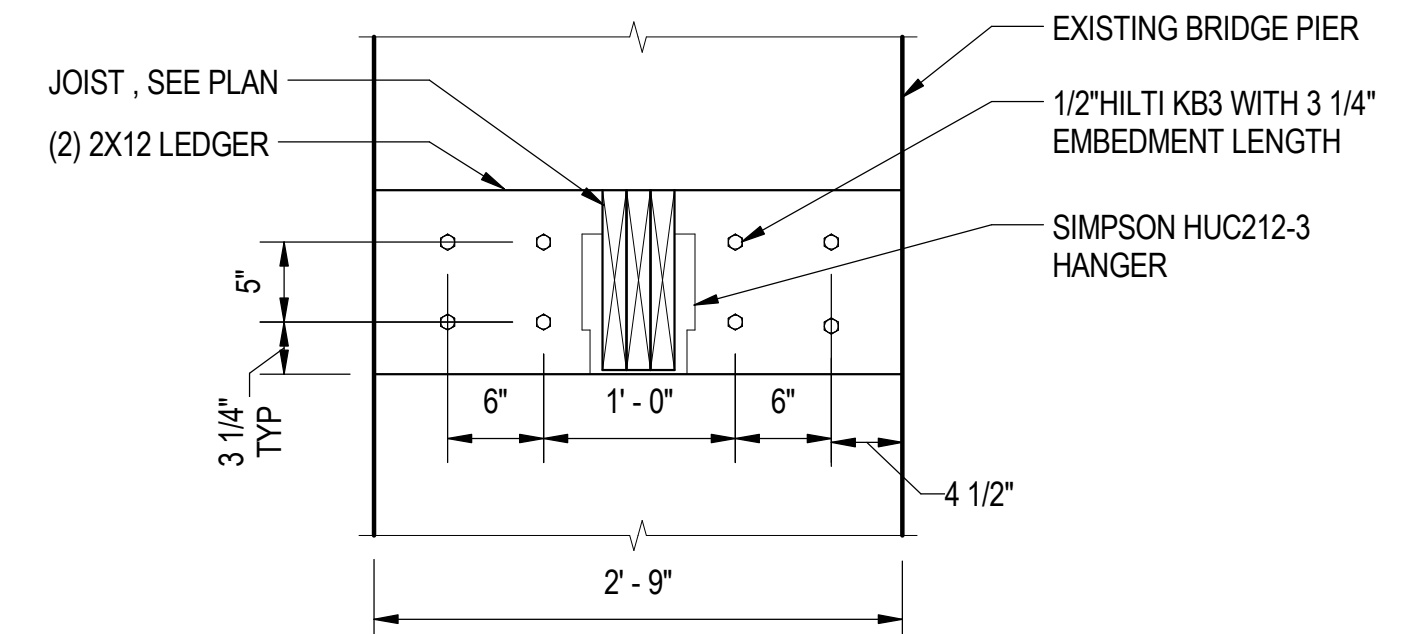
**3** TEMPORARY PLATFORM SECTION AT END  
S-824 SCALE: 1" = 1'-0"



**4** TEMPORARY PLATFORM SECTION AT SONOTUBE  
S-824 SCALE: 1" = 1'-0"



**5** TEMPORARY PLATFORM SECTION AT BRIDGE PIER  
S-824 SCALE: 1" = 1'-0"



**6** TEMPORARY PLATFORM CONNECTION AT BRIDGE PIER  
S-824 SCALE: 1" = 1'-0"

- SHEET NOTES:**
- FOR GENERAL NOTES, SEE DWGS S-001 TO S-004.
  - FOR TYPICAL DETAILS REFER TO S-1112 TO S-1120.
  - ↔ INDICATES 3/4" EXTERIOR GRADE PLYWOOD FASTENED W/ 10d NAILS AT 6" O.C. EDGE & 12" O.C. FIELD.
  - ALL WOOD TO BE PRESSURE TREATED U.N.O.



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**95th Terminal Improvement**  
15 West 95th Street (South Terminal)  
Chicago, IL 60628

IN CHARGE	J. JAKALSKI
APPROVED BY	J. JAKALSKI
CHECKED BY	R.S. NAIR
DESIGNED BY	G. FORKAN
DRAWN BY	C. BOVE
PROJECT NO.	2012-0021.02
FILE NAME	DR095_S-Terminal

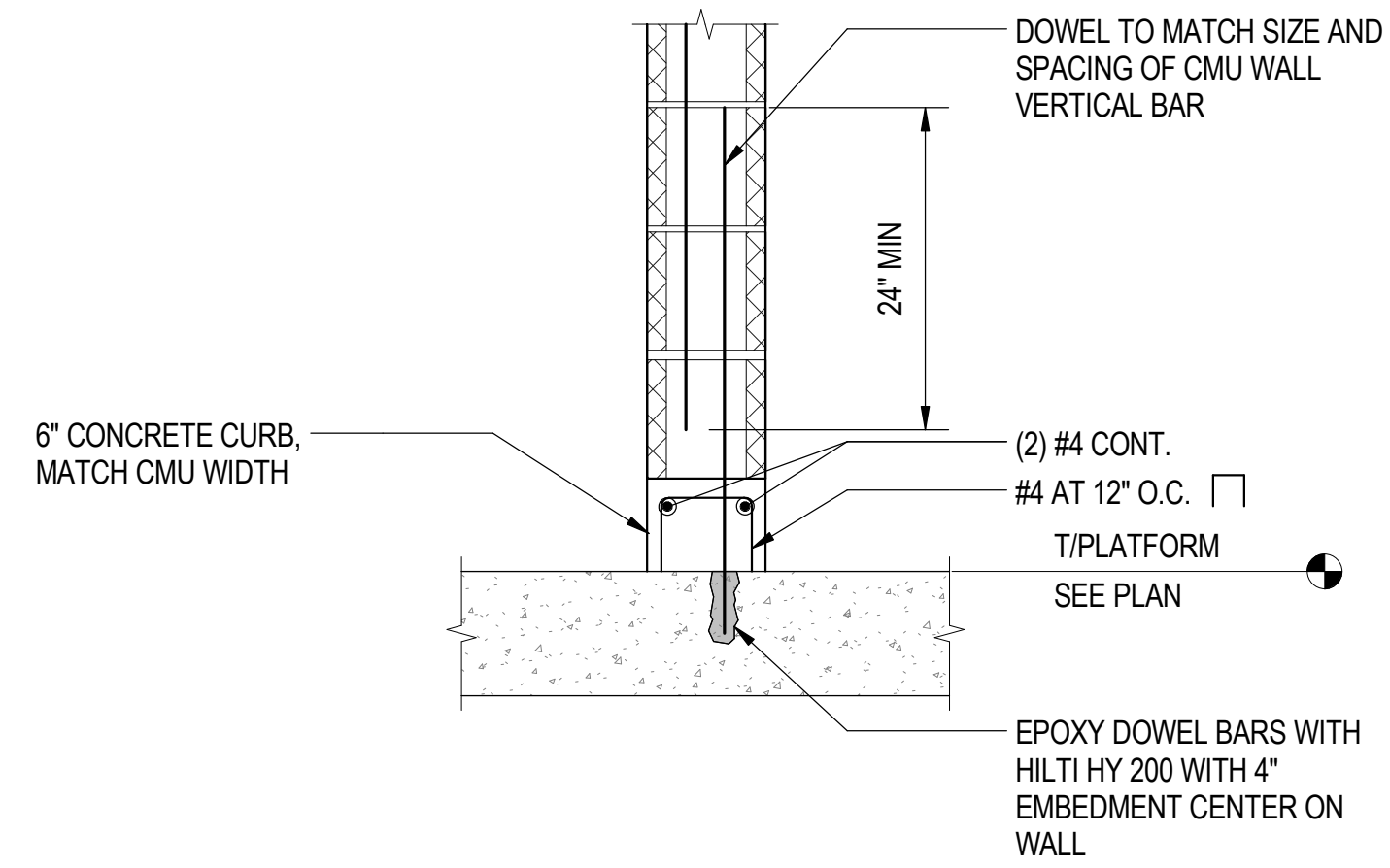
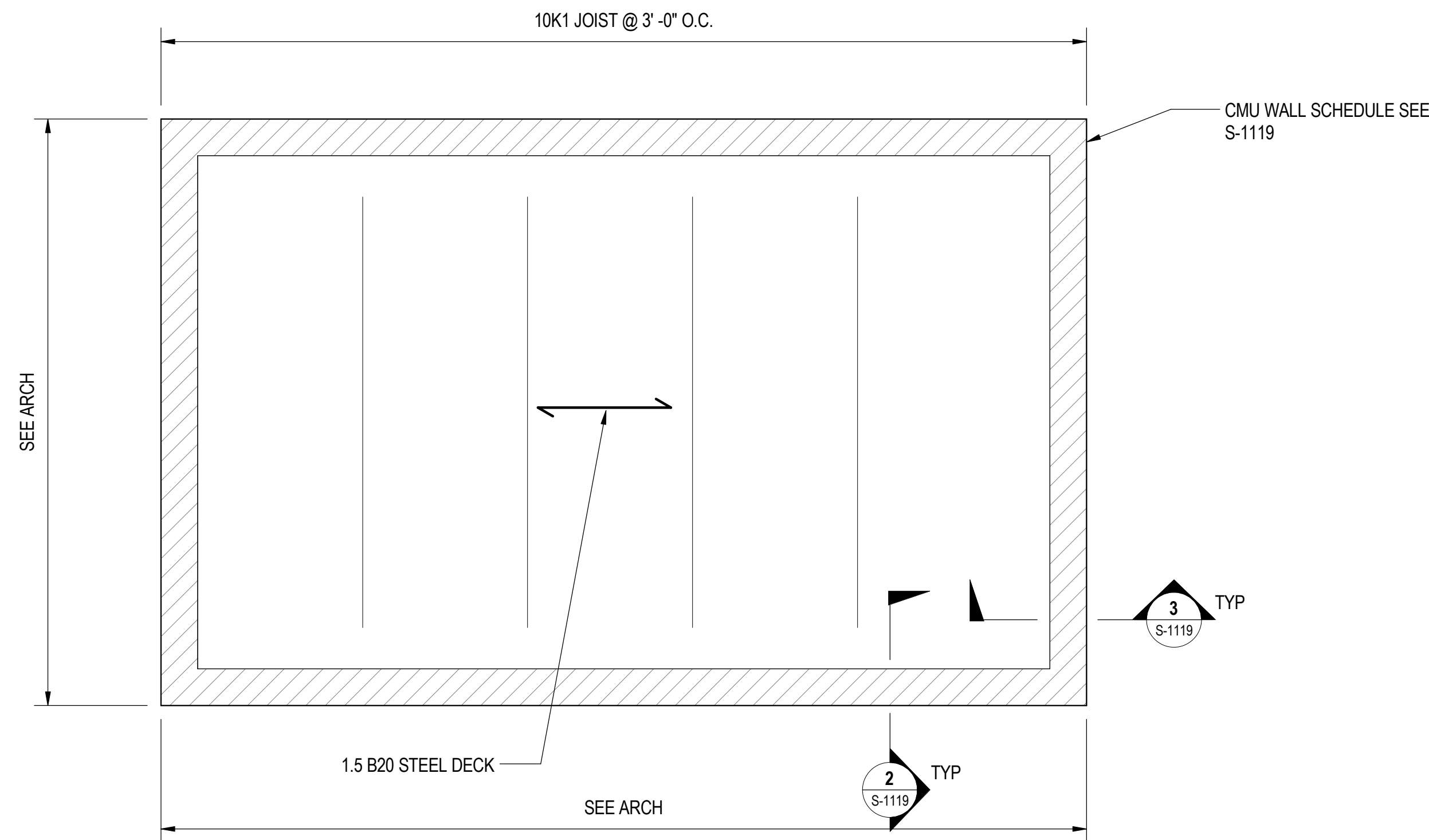
MARK	DATE	DESCRIPTION
	2/9/17	ISSUED FOR CONSTRUCTION

LOCATION IDENTIFIER: **DR095**

**SOUTH TERMINAL TEMPORARY PLATFORM**

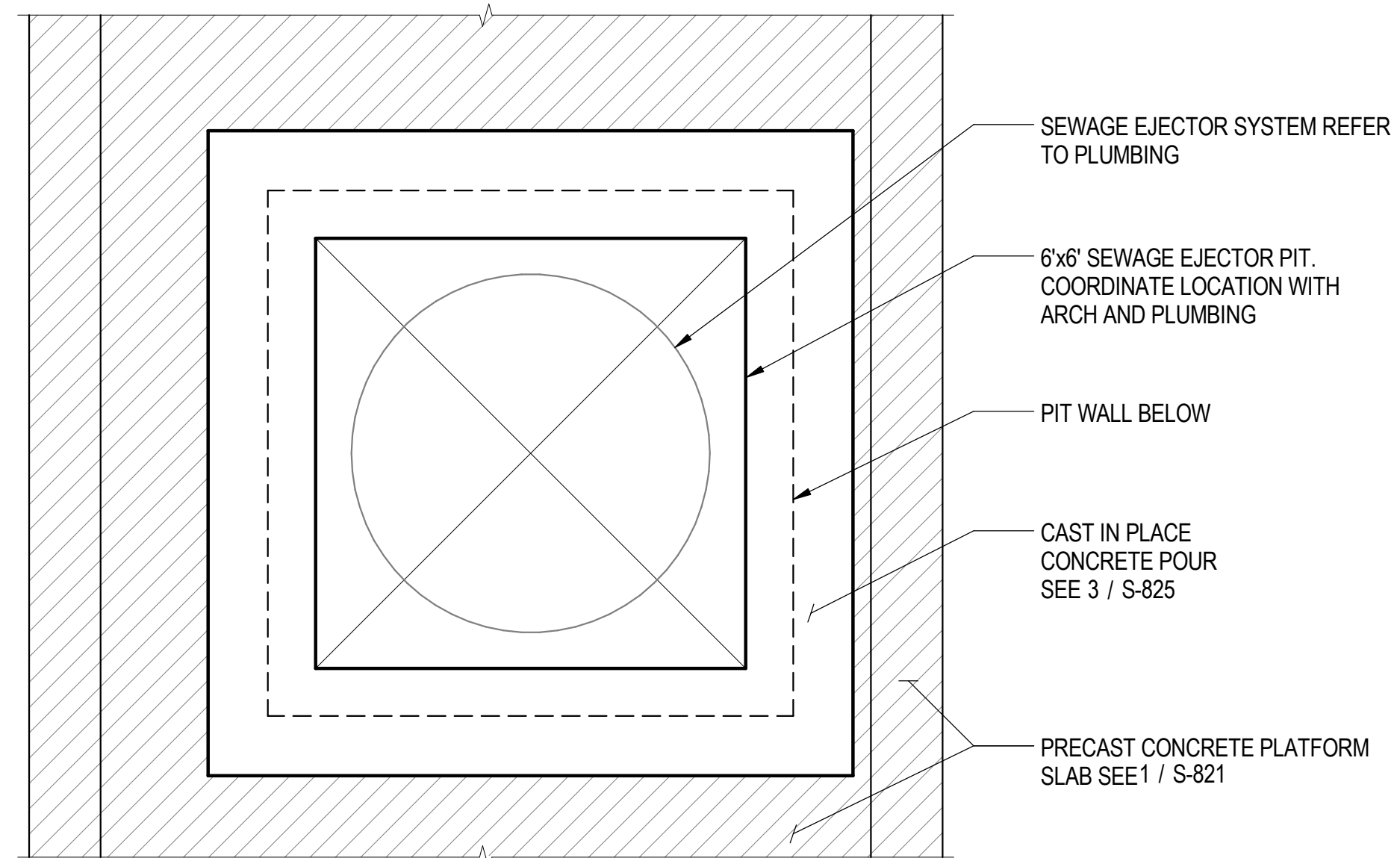
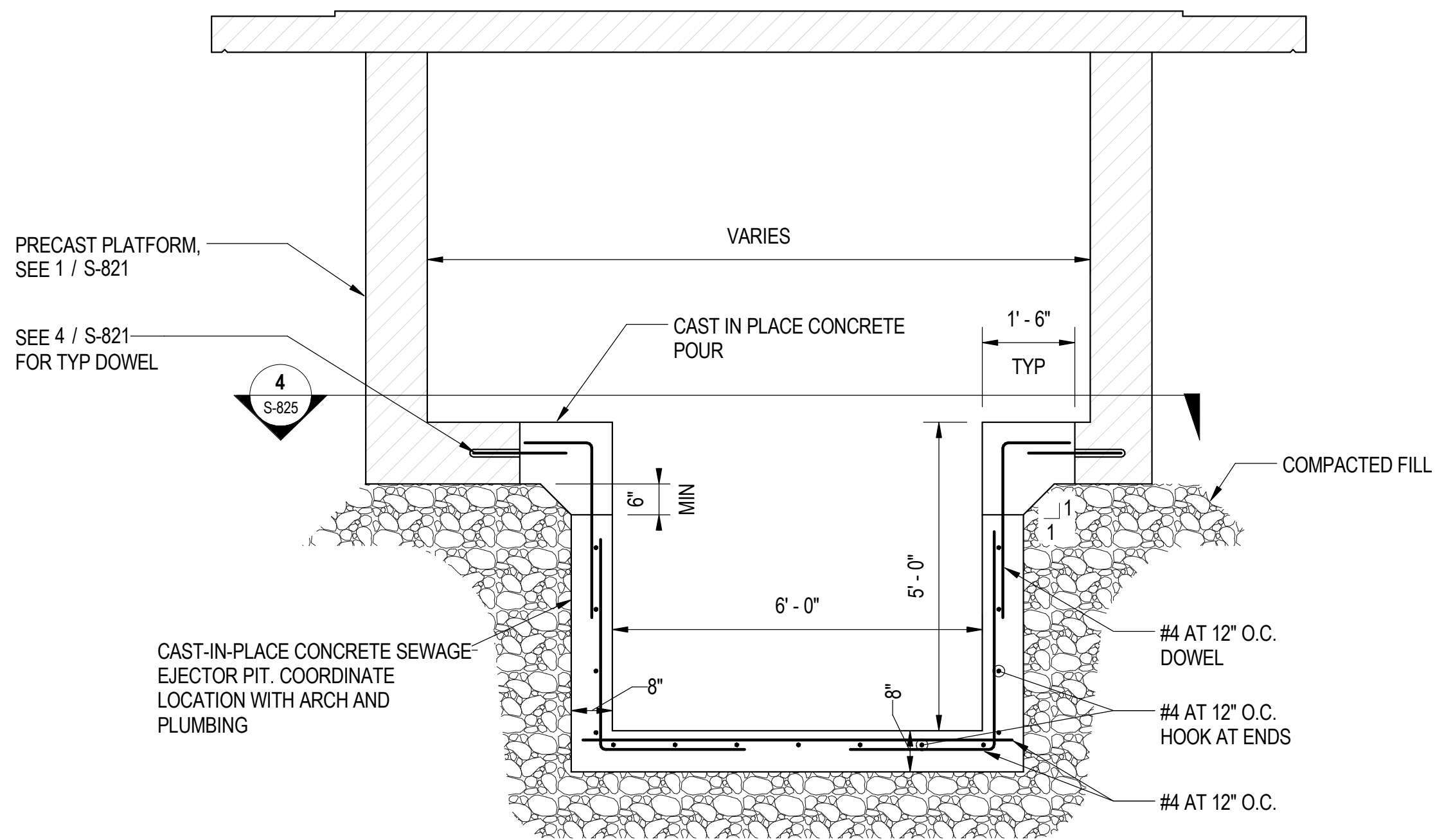
**S-824**





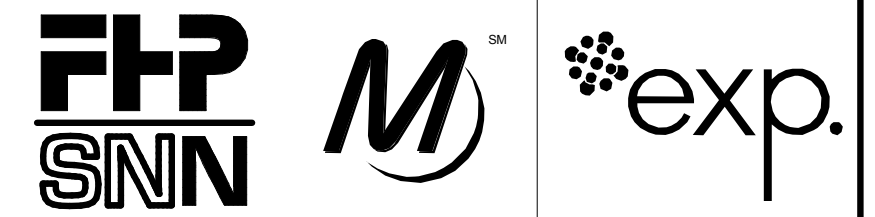
**1** PLATFORM LEVEL ENCLOSURE ROOF FRAMING PLAN  
S-825 SCALE: 1/2" = 1'-0"

**2** PLATFORM LEVEL CMU WALL TO PLATFORM SLAB  
S-825 SCALE: 1" = 1'-0"



**3** SEWAGE EJECTOR PIT DETAIL  
S-825 SCALE: 1/2" = 1'-0"

**4** SEWAGE EJECTOR PIT PLAN DETAIL  
S-825 SCALE: 1/2" = 1'-0"



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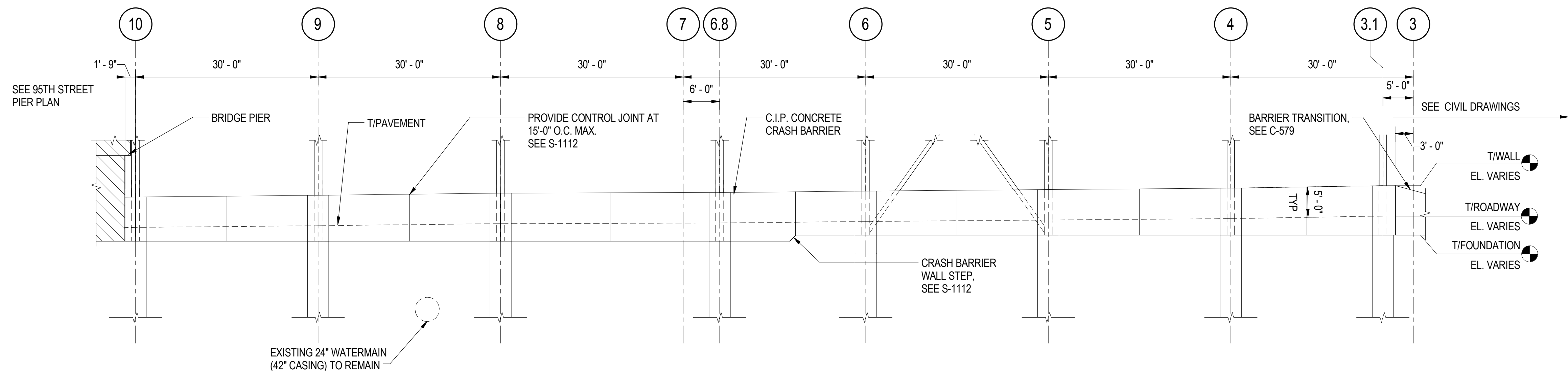
IN CHARGE	J. JAKALSKI
APPROVED BY	J. JAKALSKI
CHECKED BY	R.S. NAIR
DESIGNED BY	G. FORKAN
DRAWN BY	C. BOVE
PROJECT NO.	2012-0021.02
FILE NAME	DR095_S-Terminal

MARK	DATE	DESCRIPTION
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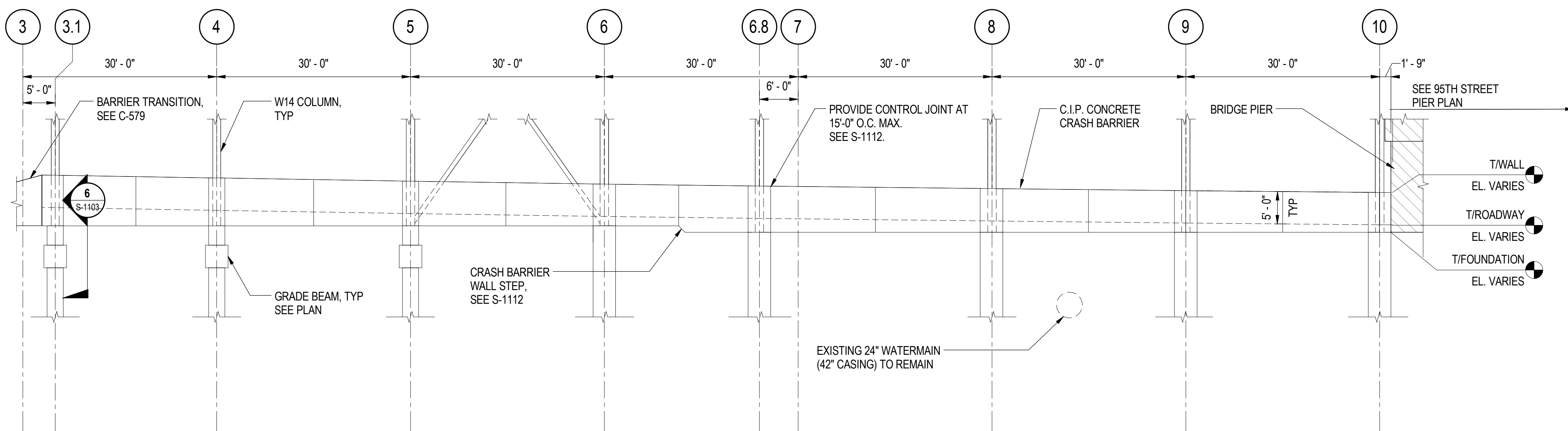
LOCATION IDENTIFIER: **DR095**

**SOUTH TERMINAL PLATFORM DETAILS**

**S-825**



**1** WALL NO. 3 - ELEVATION (LOOKING EAST)  
S-831 SCALE: 3/32" = 1'-0"



**2** WALL NO. 5 - ELEVATION (LOOKING WEST)  
S-831 SCALE: 3/32" = 1'-0"

- SHEET NOTES:**
- FOR GENERAL PROJECT NOTES, SEE S-000 SERIES.
  - FOR TYPICAL DETAILS, SEE S-1100 SERIES.
  - REFER TO S-1104 FOR WALL REINFORCEMENT AND DETAILS.
  - PROVIDE CONTROL JOINT AT 15'-0" O.C. MAX. TYP, SEE S-1112.
  - ALL EXPOSED EDGES SHALL BE CHAMFERED 3/4", INCLUDING THE TOP OF THE WALL AND AT THE INTERFACE WITH BRIDGE PIERS.



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PROJECT NO.	2012-0021.02
FILE NAME	DR095_S-StationFoundation

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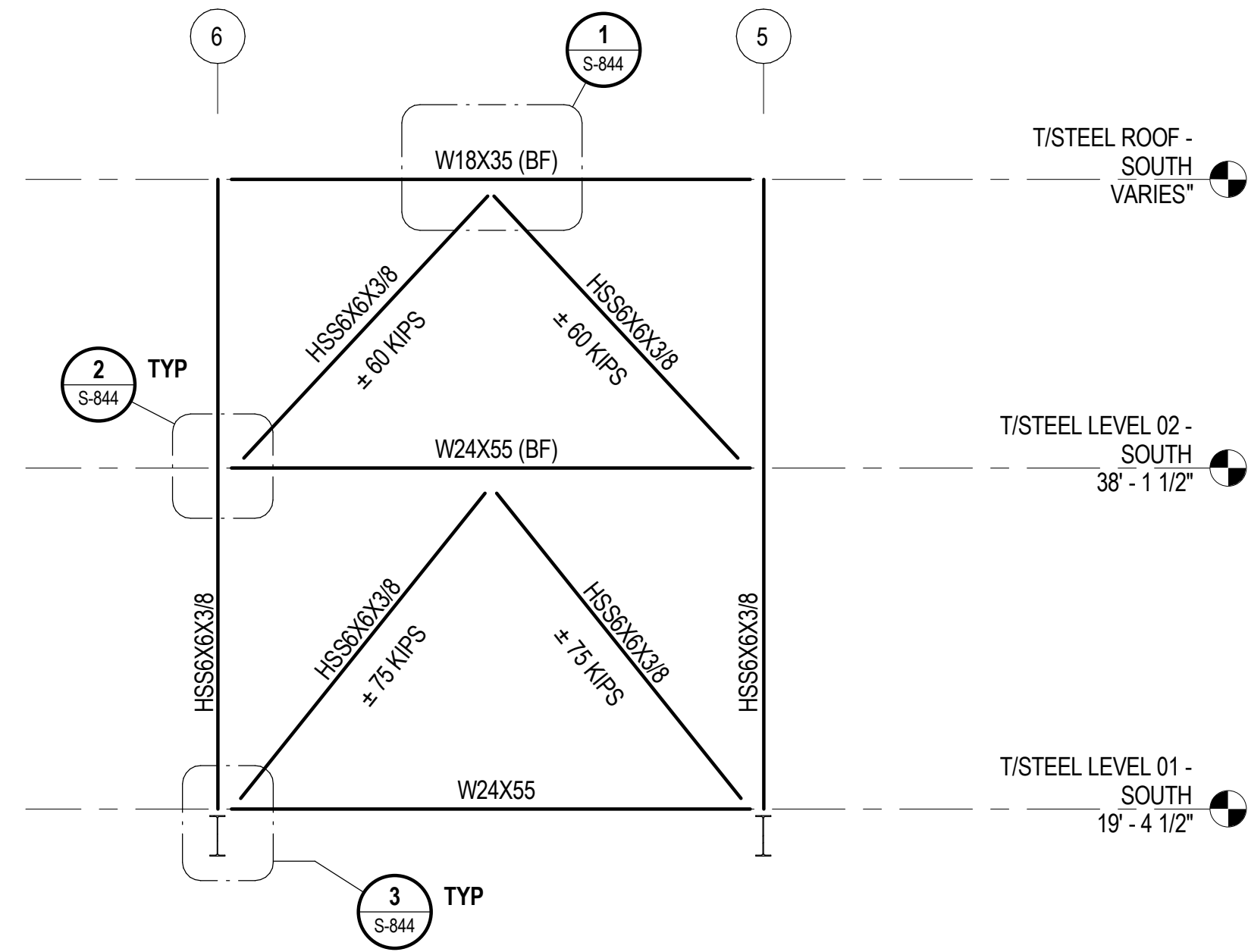
LOCATION IDENTIFIER: **DR095**

**SOUTH TERMINAL CRASH BARRIER WALL ELEVATIONS**

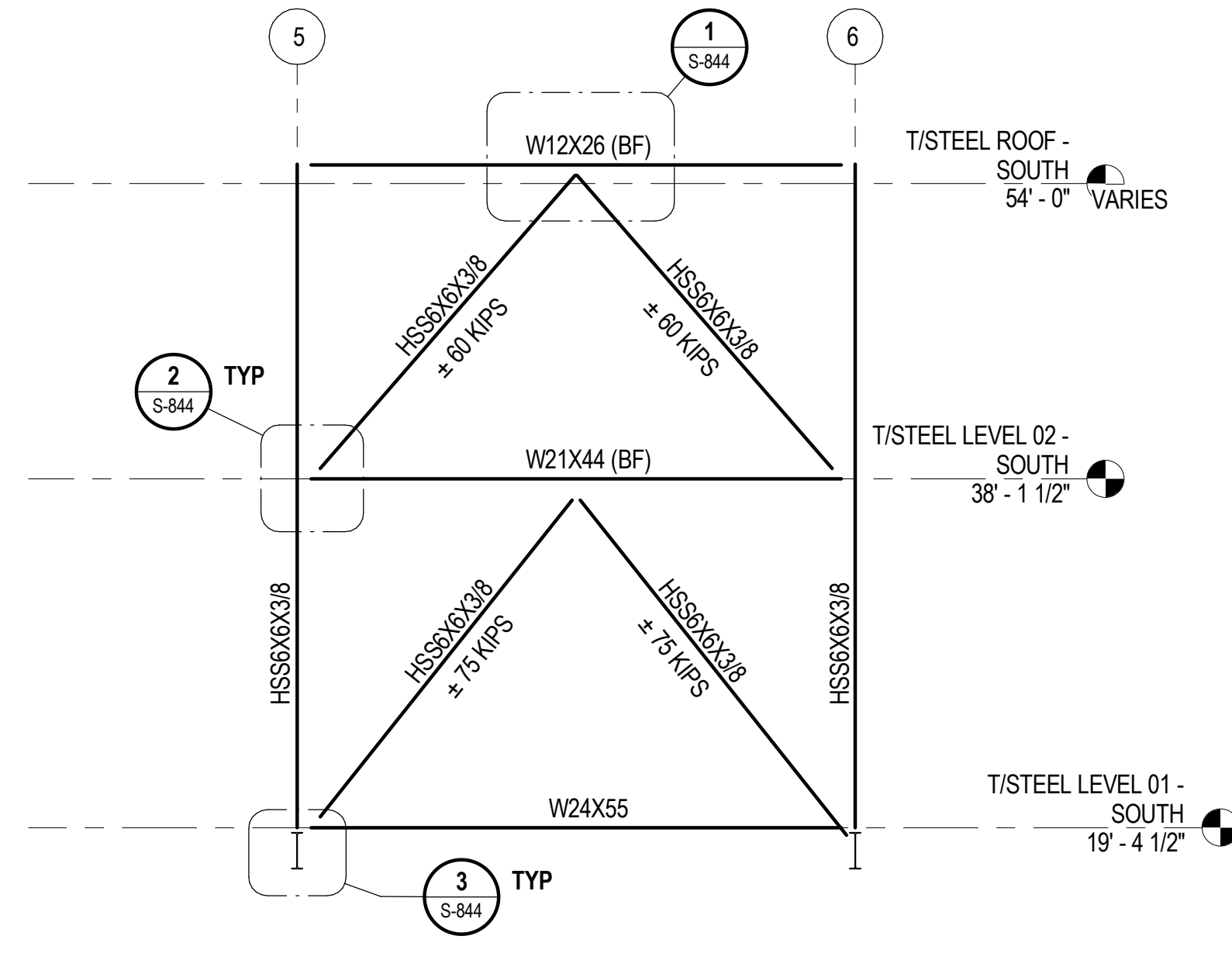
**S-831**

07/10/2016 2:17:00 PM

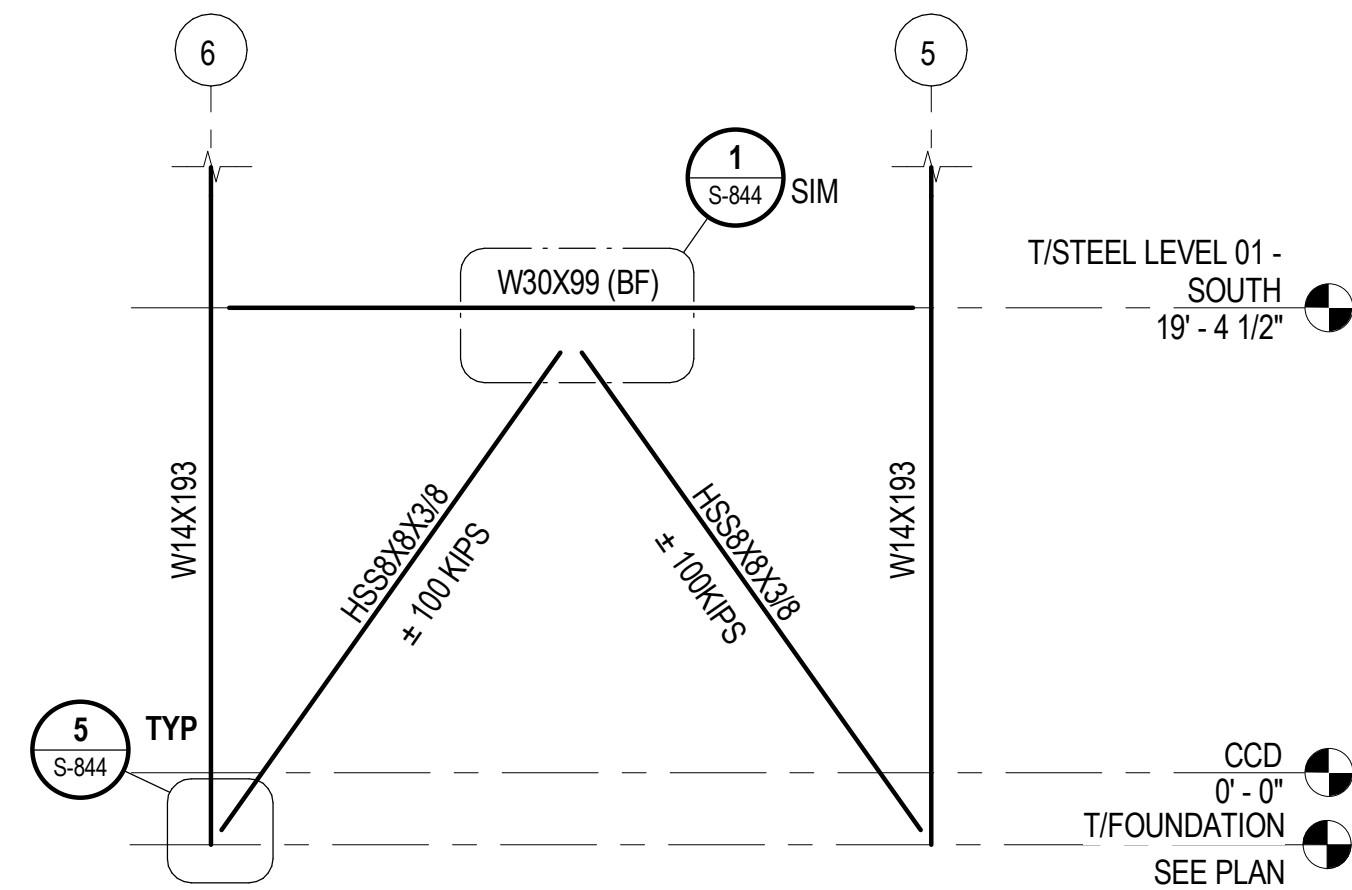
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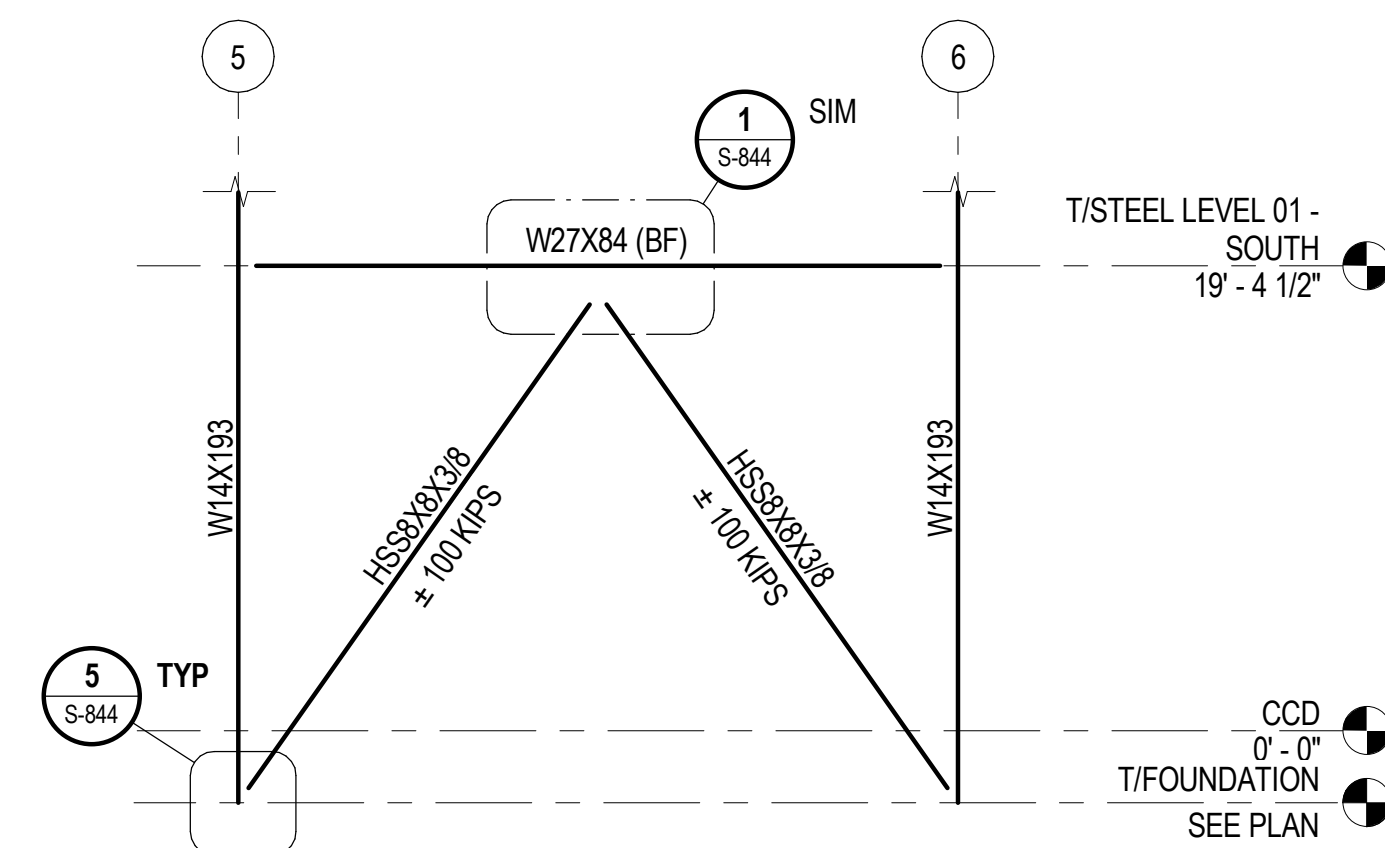
**1** BRACED FRAME ELEVATION ALONG GRID C'  
S-832 SCALE: 1/8" = 1'-0"



**2** BRACED FRAME ELEVATION ALONG GRID E  
S-832 SCALE: 1/8" = 1'-0"



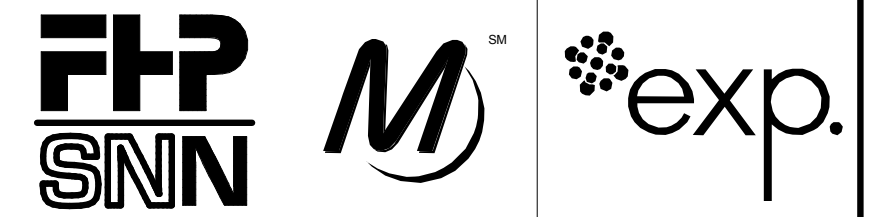
**3** BRACED FRAME ELEVATION ALONG GRID D'  
S-832 SCALE: 1/8" = 1'-0"



**4** BRACED FRAME ALONG GRID E'  
S-832 SCALE: 1/8" = 1'-0"

**SHEET NOTES:**

- FOR GENERAL NOTES, SEE DWGS S-001 TO S-004.
- BRACE CONNECTIONS TO BE DESIGNED BY LICENSED SE IN THE STATE OF IL TO DEVELOP 100% OF INDICATED BRACE FORCES.
- ALL INDICATED BRACE FORCES ARE FACTORED AXIAL LOADS. FORCES TO BE CONSIDERED EITHER TENSION OR COMPRESSION.
- BRACED FRAME JOINTS SHALL BE DESIGNED USING THE AISC UNIFORM FORCE METHOD TO AVOID MOMENTS AT CONNECTION INTERFACES OR EQUIVALENT UNLESS NOTED OTHERWISE.



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DRAWN BY	J. PEARSON
PROJECT NO.	2012-0021.02
FILE NAME	DR095_S-Terminal

MARK	DATE	DESCRIPTION
	2/9/17	ISSUED FOR CONSTRUCTION

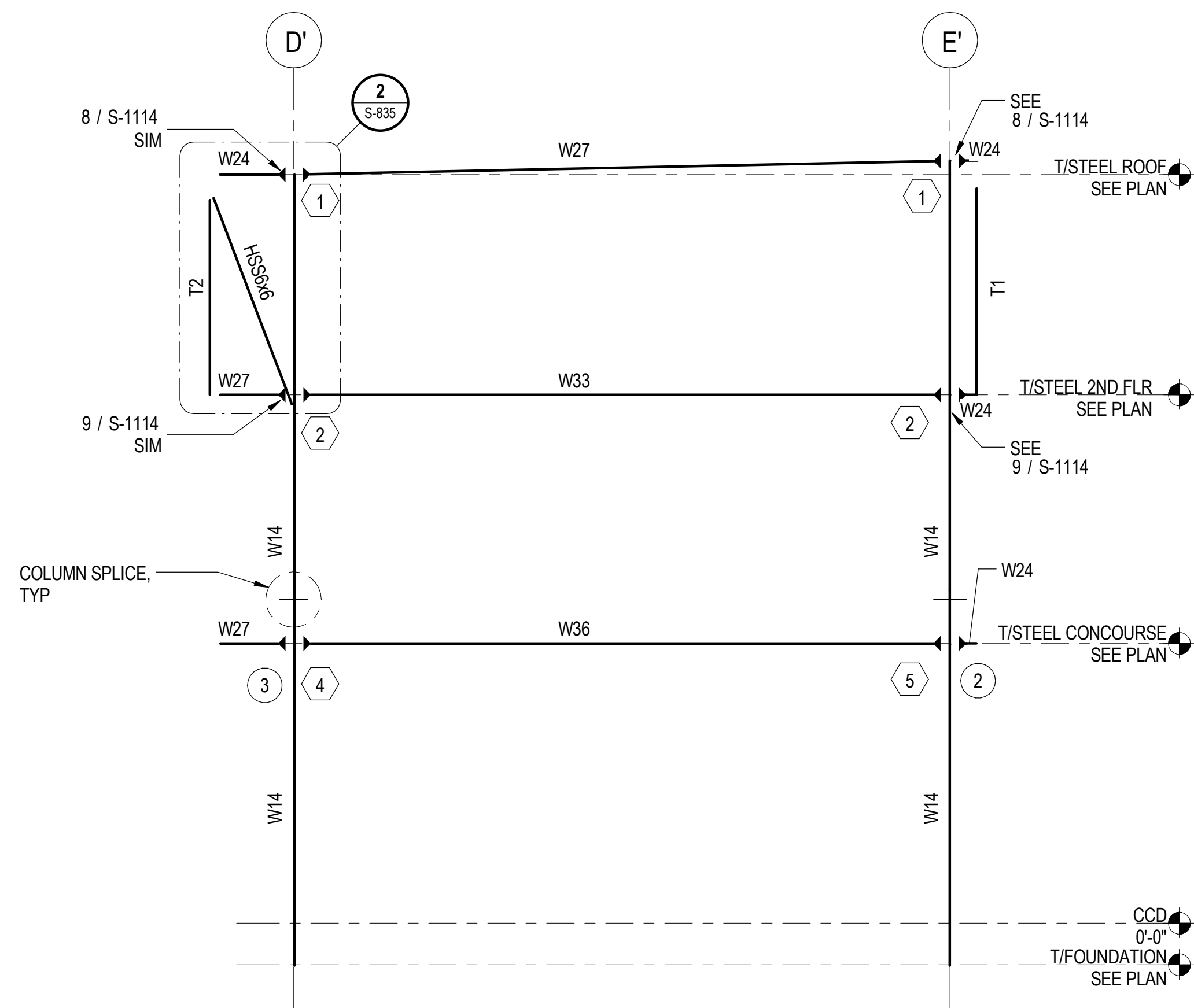
LOCATION IDENTIFIER: **DR095**

**SOUTH TERMINAL BRACED FRAME ELEVATIONS**

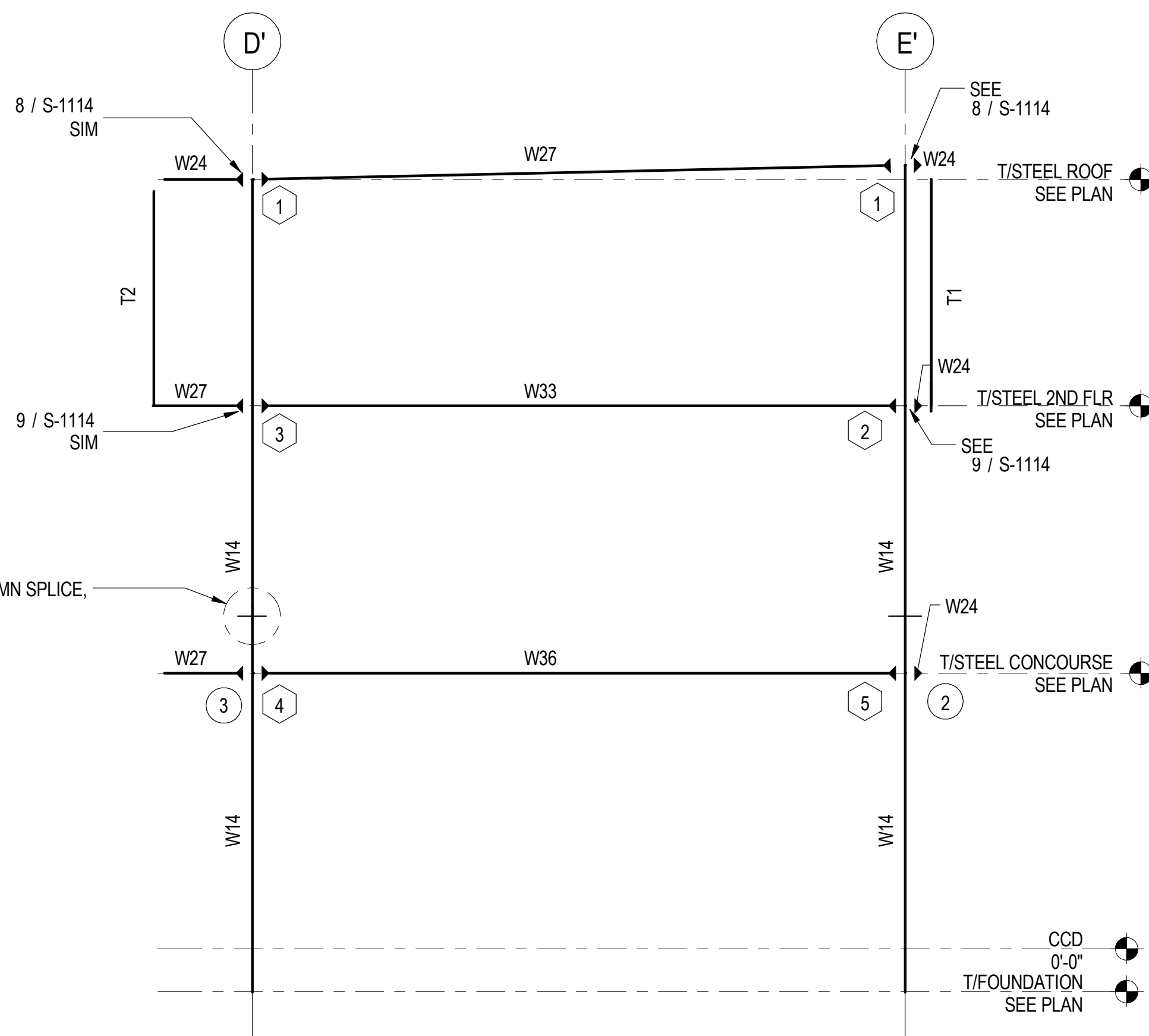
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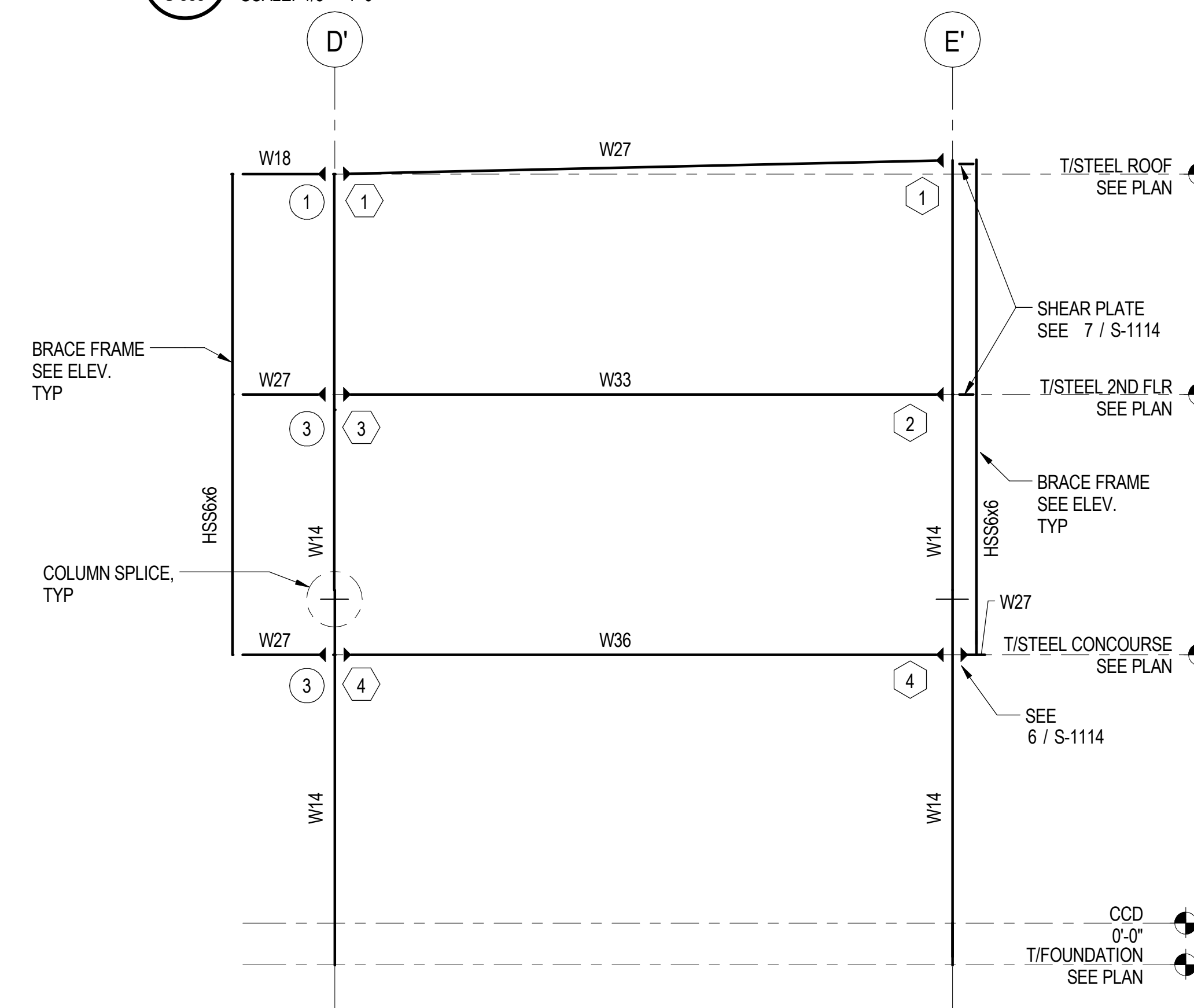
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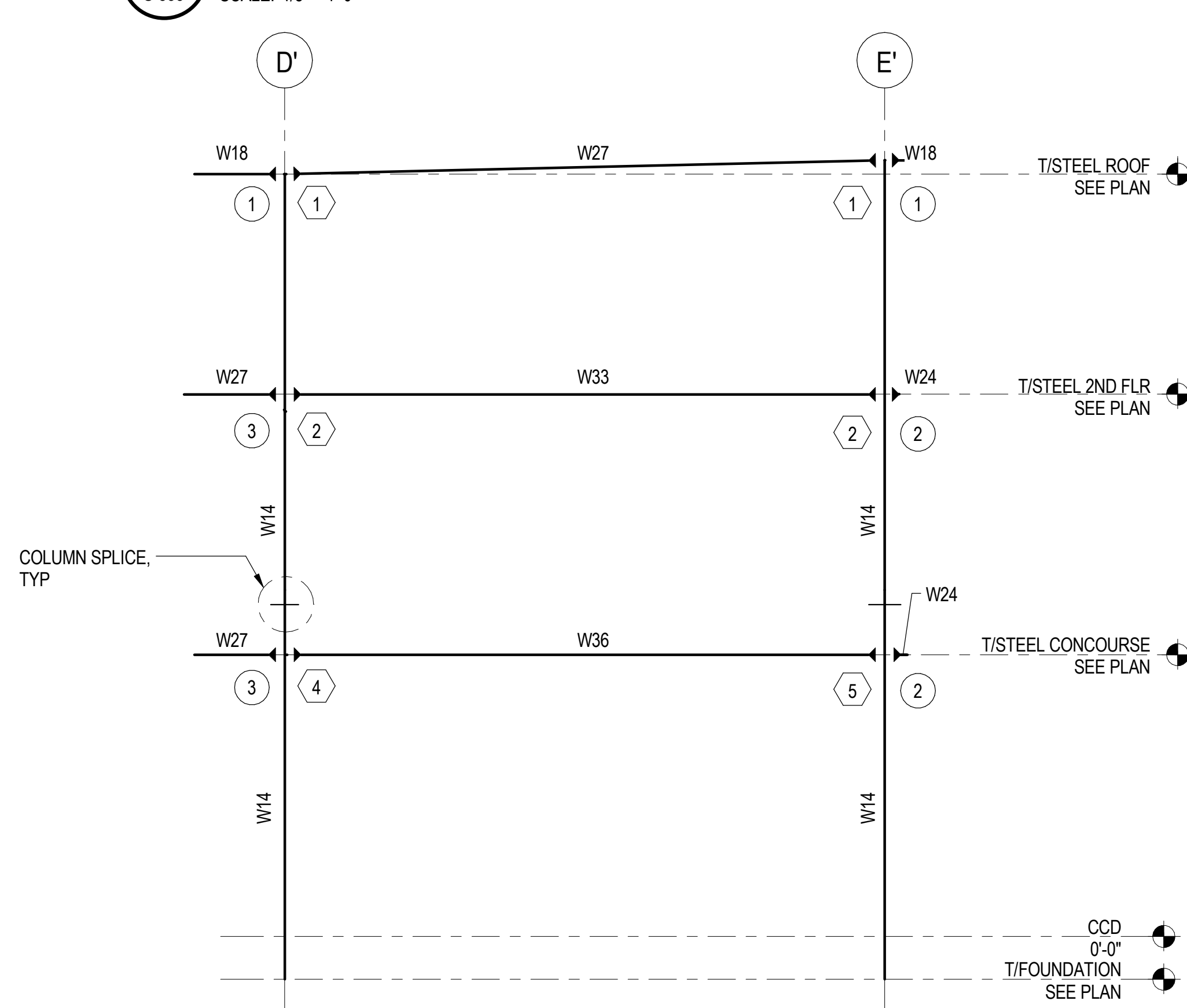
**1** MOMENT FRAME ELEVATION AT GRID 3.1  
S-833 SCALE: 1/8" = 1'-0"



**2** MOMENT FRAME ELEVATION AT GRID 4  
S-833 SCALE: 1/8" = 1'-0"



**3** MOMENT FRAME ELEVATION AT GRID 5 & 6  
S-833 SCALE: 1/8" = 1'-0"



**4** MOMENT FRAME ELEVATION AT GRID 6.8 & 8  
S-833 SCALE: 1/8" = 1'-0"

**STEEL BEAM LEGEND**

- INDICATES SHEAR REACTION FOR CONNECTION DESIGN
- INDICATES BRACED FRAME BELOW
- INDICATES MOMENT FRAME
- REQUIRED CAMBER AT MIDSPAN
- NUMBER OF HEADED SHEAR STUDS IF GREATER THAN 12" O.C.
- BEAM DESIGNATION
- INDICATES EXISTING BEAM
- T/STEEL ELEVATION RELATIVE TO TYPICAL
- CENTERLINE OF BEAM WEB PENETRATION (SEE SCHEDULE)
- INDICATES SPLICE LOCATION
- INDICATES MOMENT CONNECTION

**SHEET NOTES**

1. FOR GENERAL NOTES, SEE DWGS S-001 TO S-004.
2. FOR TYPICAL DETAILS REFER TO S-1112 TO S-1120.
3. ALL STEEL SHALL BE CONTINUOUS U.N.O. REFER TO TYPICAL DETAILS FOR GRAVITY COLUMN SPLICING REQUIREMENTS. ALL SPLICE LOCATIONS NOT SHOWN SHALL BE REVIEWED AND APPROVED BY THE DOR PRIOR TO PROCEEDING WITH FABRICATION.
4. (X) INDICATES MARK FOR MOMENT FRAME CONNECTION TYPE. SEE SCHEDULE ON SHEET S-1116.
5. (X) INDICATES MARK FOR OUTRIGGER CONNECTION TYPE. SEE SCHEDULE ON SHEET S-1114.



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15 West 95th Street (South Terminal)  
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PROJECT NO.	2012-0021.02
FILE NAME	DR095_S-Terminal

MARK	DATE	ISSUED FOR CONSTRUCTION	DESCRIPTION
	2/9/17		

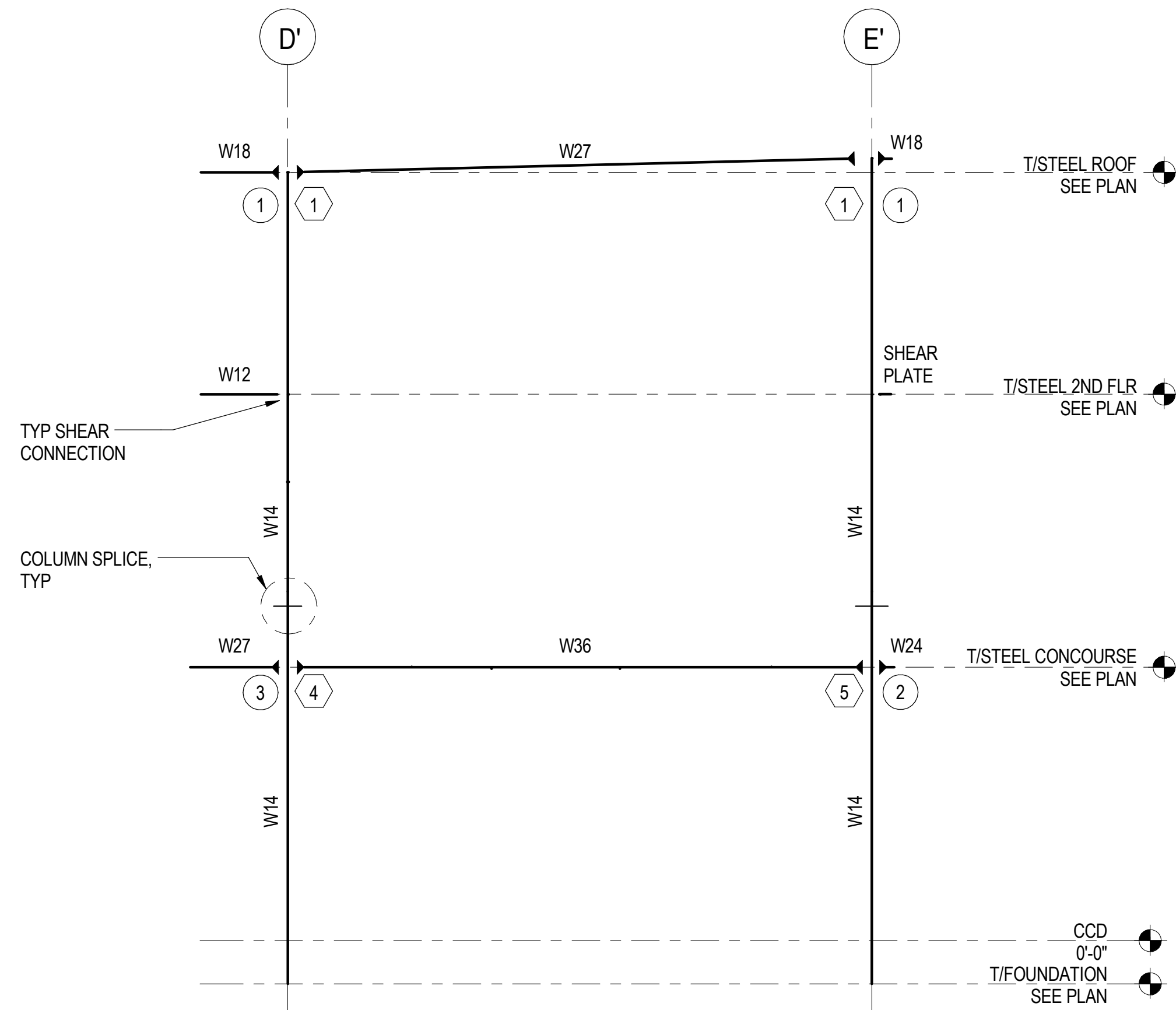
LOCATION IDENTIFIER: **DR095**

**SOUTH TERMINAL MOMENT FRAME ELEVATIONS - 1**

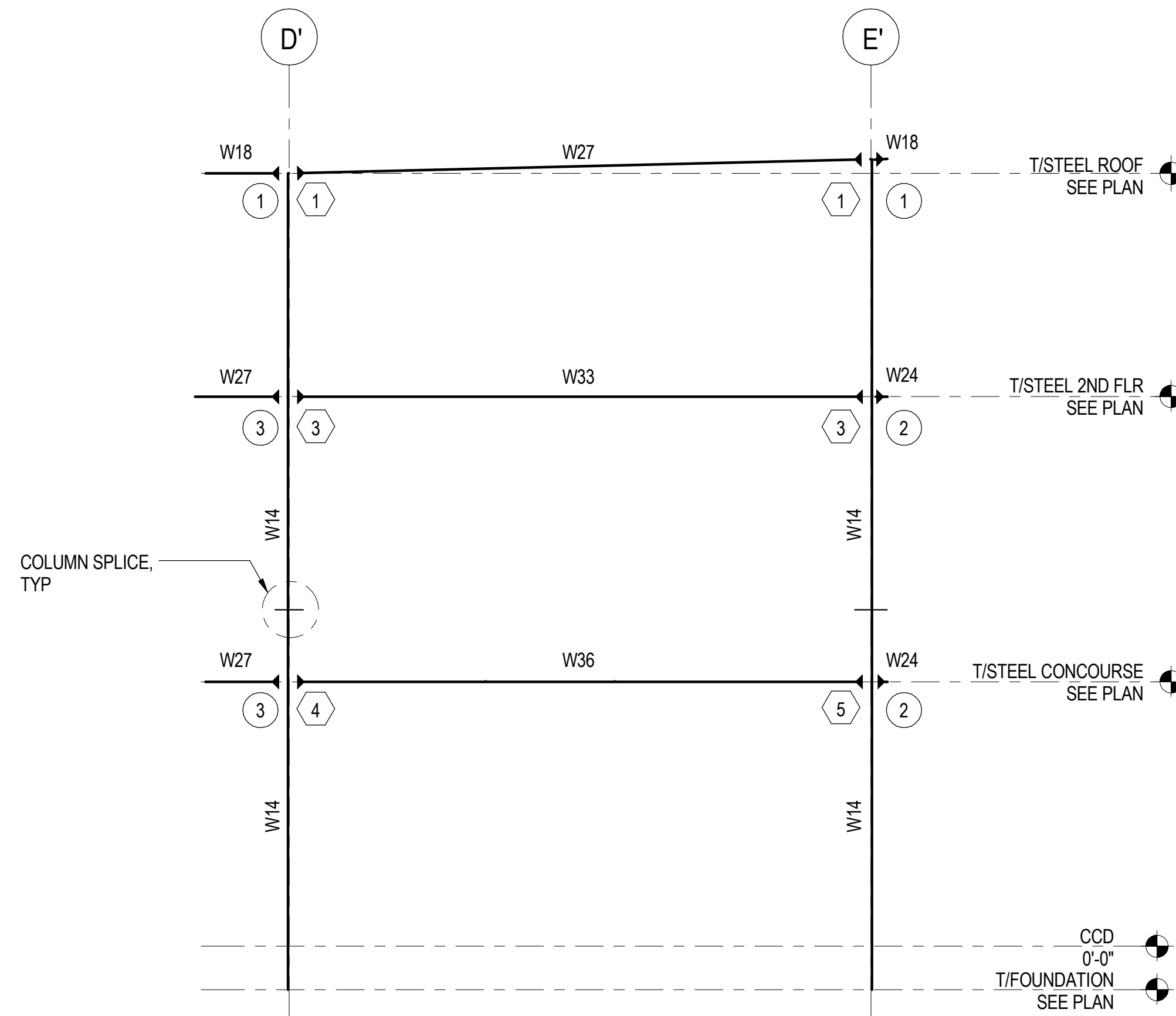
**S-833**

30/11/2016 7:05:56 PM

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**1** MOMENT FRAME ELEVATION AT GRID 9  
S-834 SCALE: 1/8" = 1'-0"



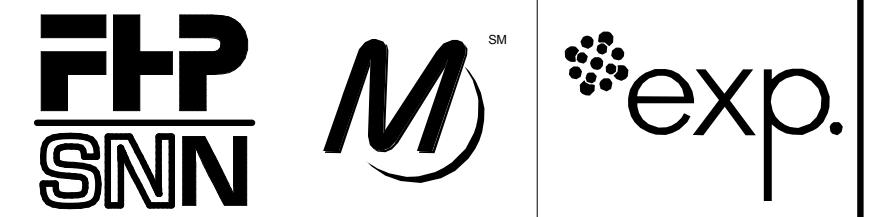
**2** MOMENT FRAME ELEVATION AT GRID 10  
S-834 SCALE: 1/8" = 1'-0"

**STEEL BEAM LEGEND**

- INDICATES SHEAR REACTION FOR CONNECTION DESIGN  $R = 20\text{ k}$
- INDICATES BRACED FRAME BELOW
- INDICATES MOMENT FRAME (MF) (BF)
- REQUIRED CAMBER AT MIDSPAN (MF) (BF)
- NUMBER OF HEADED SHEAR STUDS IF GREATER THAN 12" O.C.  $<2D_s>$  (MF) (BF)
- BEAM DESIGNATION (E) W18X40
- INDICATES EXISTING BEAM
- T/STEEL ELEVATION RELATIVE TO TYPICAL  $\pm 1'-6"$
- CENTERLINE OF BEAM WEB PENETRATION (SEE SCHEDULE)  $P_x$
- INDICATES SPLICE LOCATION  $X - XX"$
- INDICATES MOMENT CONNECTION  $R = 20\text{ k}$

**SHEET NOTES**

1. FOR GENERAL NOTES, SEE DWGS S-001 TO S-004.
2. FOR TYPICAL DETAILS REFER TO S-1112 TO S-1120.
3. ALL STEEL SHALL BE CONTINUOUS U.N.O. REFER TO TYPICAL DETAILS FOR GRAVITY COLUMN SPLICING REQUIREMENTS. ALL SPLICE LOCATIONS NOT SHOWN SHALL BE REVIEWED AND APPROVED BY THE DOR PRIOR TO PROCEEDING WITH FABRICATION.
4. (X) INDICATES MARK FOR MOMENT FRAME CONNECTION TYPE. SEE SCHEDULE ON SHEET S-1116.
5. (X) INDICATES MARK FOR OUTRIGGER CONNECTION TYPE. SEE SCHEDULE ON SHEET S-1114.



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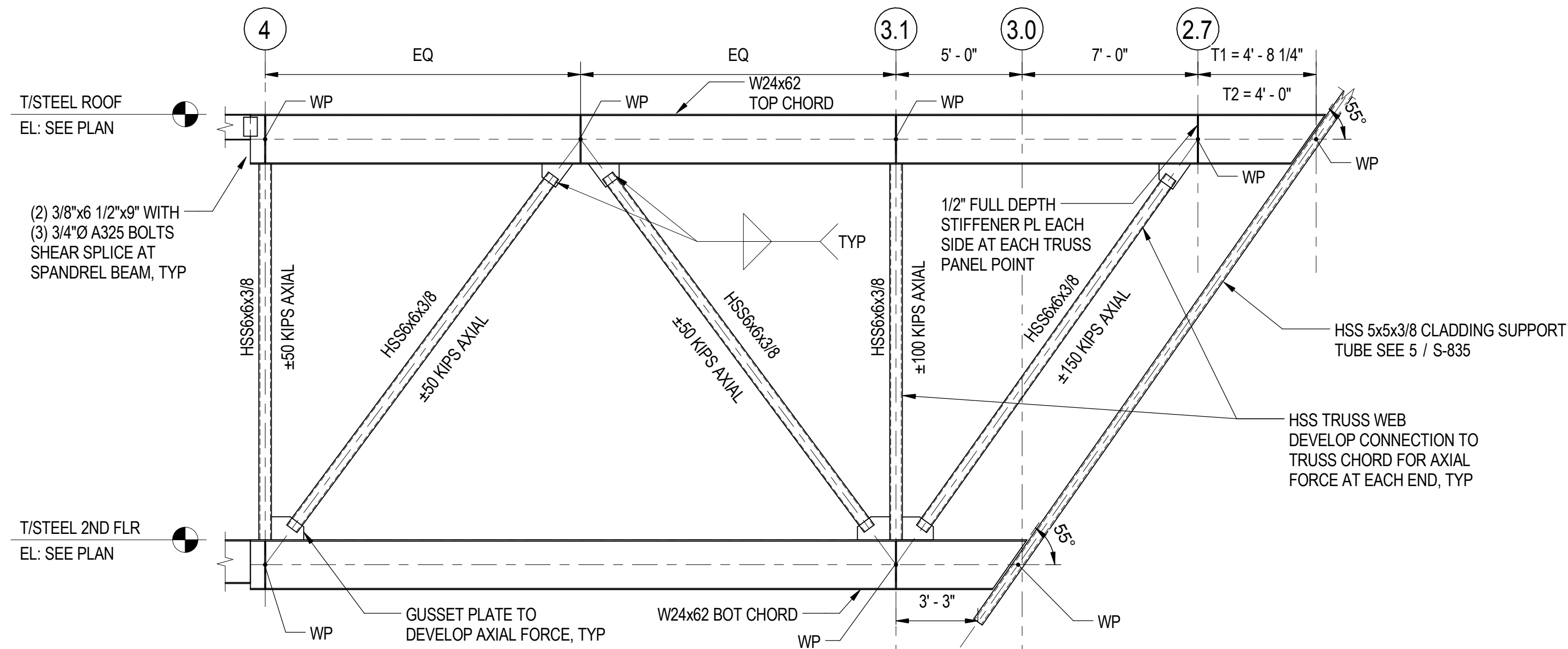
LOCATION IDENTIFIER: **DR095**

**SOUTH TERMINAL MOMENT FRAME ELEVATIONS - 2**

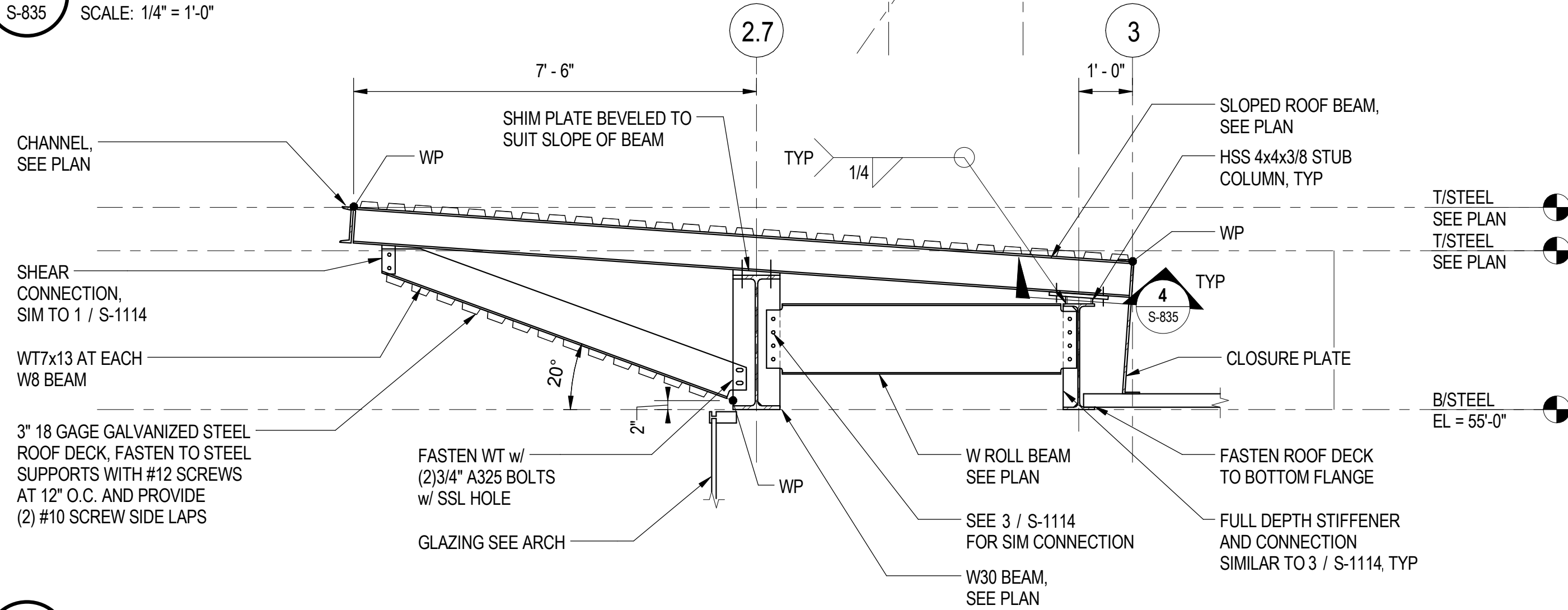
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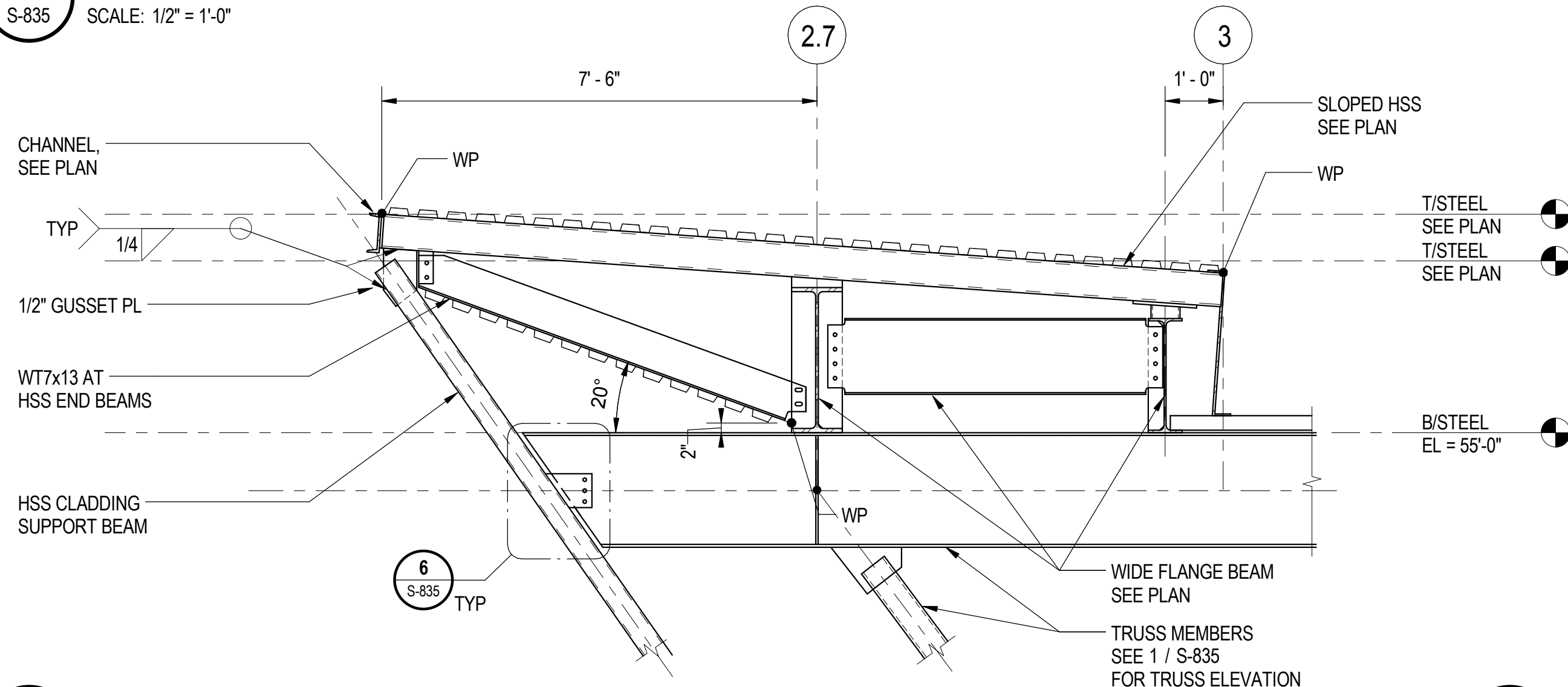
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**1 TRUSS ELEVATION**  
S-835 SCALE: 1/4" = 1'-0"

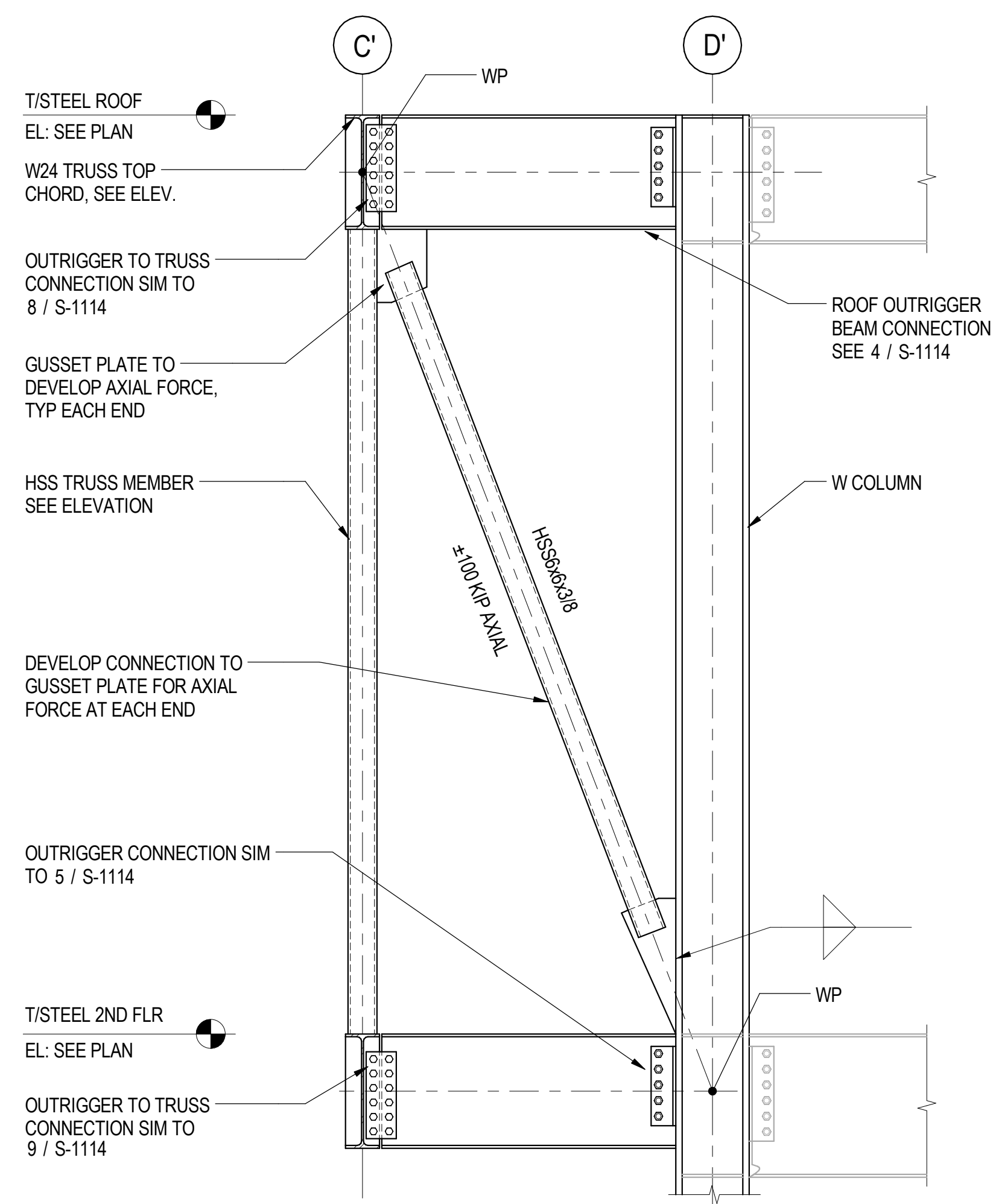


**3 SOUTH PROW CLADDING SUPPORT**  
S-835 SCALE: 1/2" = 1'-0"

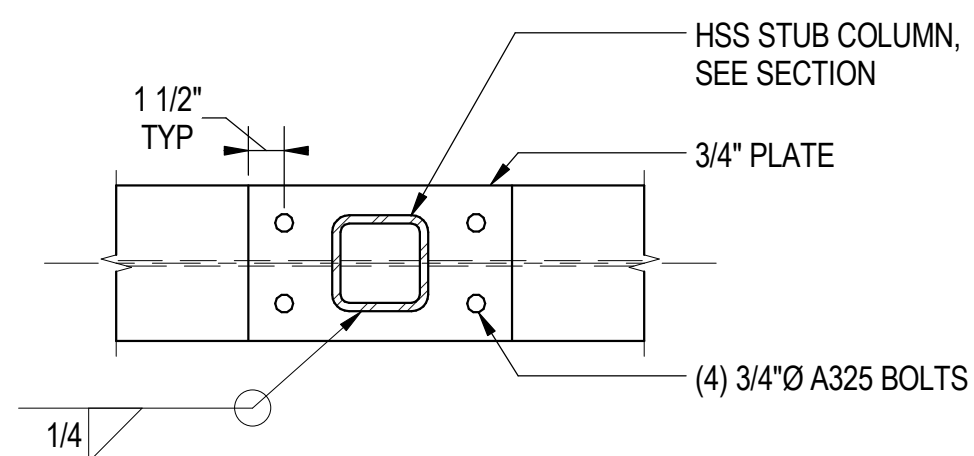


**5 SOUTH PROW CLADDING SUPPORT AT END**  
S-835 SCALE: 1/2" = 1'-0"

**6 CLADDING SUPPORT TUBE CONNECTION**  
S-835 SCALE: 1" = 1'-0"



**2 TRUSS OUTRIGGER BEAM GRID 3.1**  
S-835 SCALE: 1/2" = 1'-0"



**4 TYPICAL STUB COLUMN PLATE CONNECTION**  
S-835 SCALE: 1 1/2" = 1'-0"

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DATE		DESCRIPTION

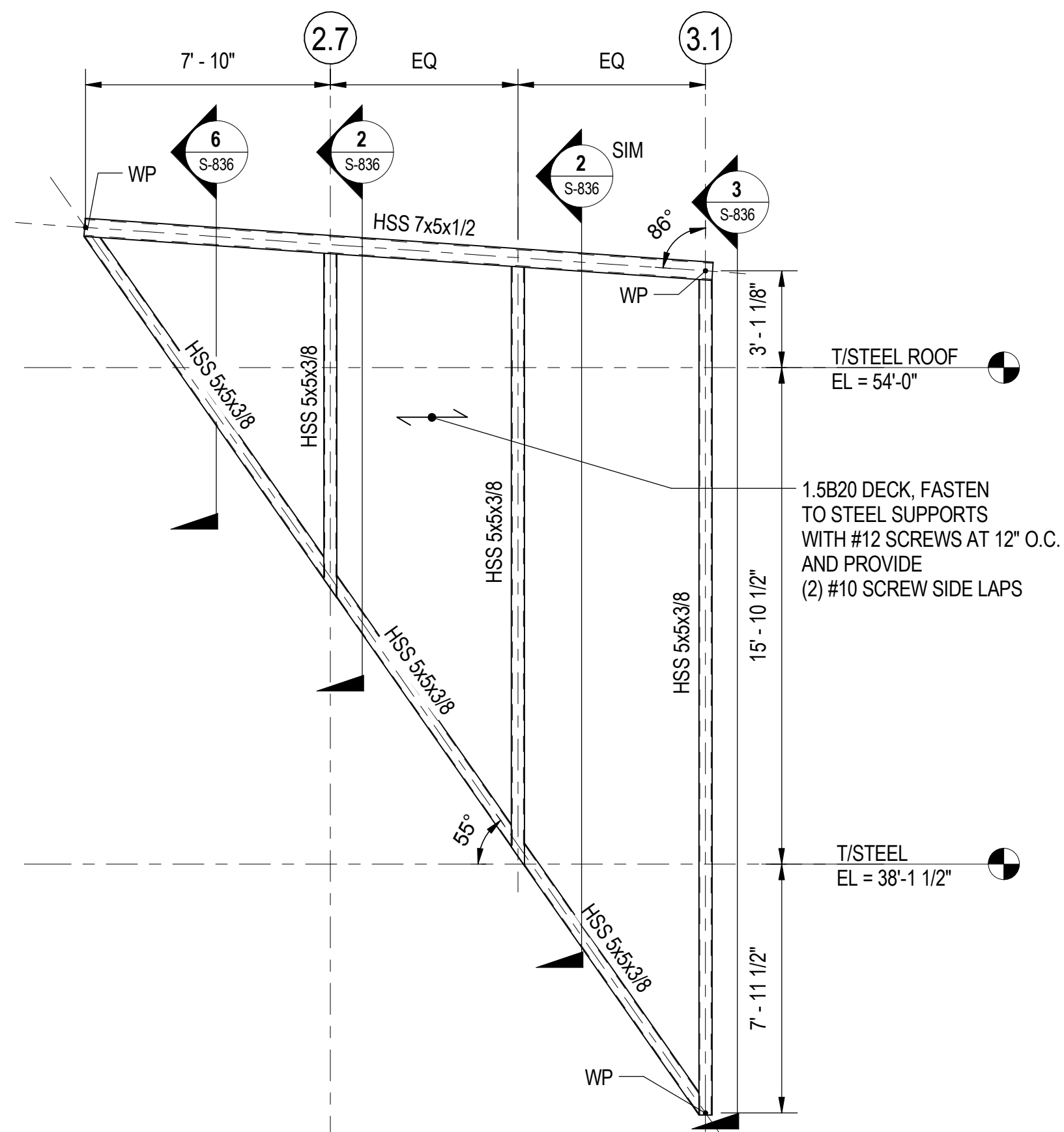
LOCATION IDENTIFIER: **DR095**

**SOUTH TERMINAL ELEVATIONS AND DETAILS**

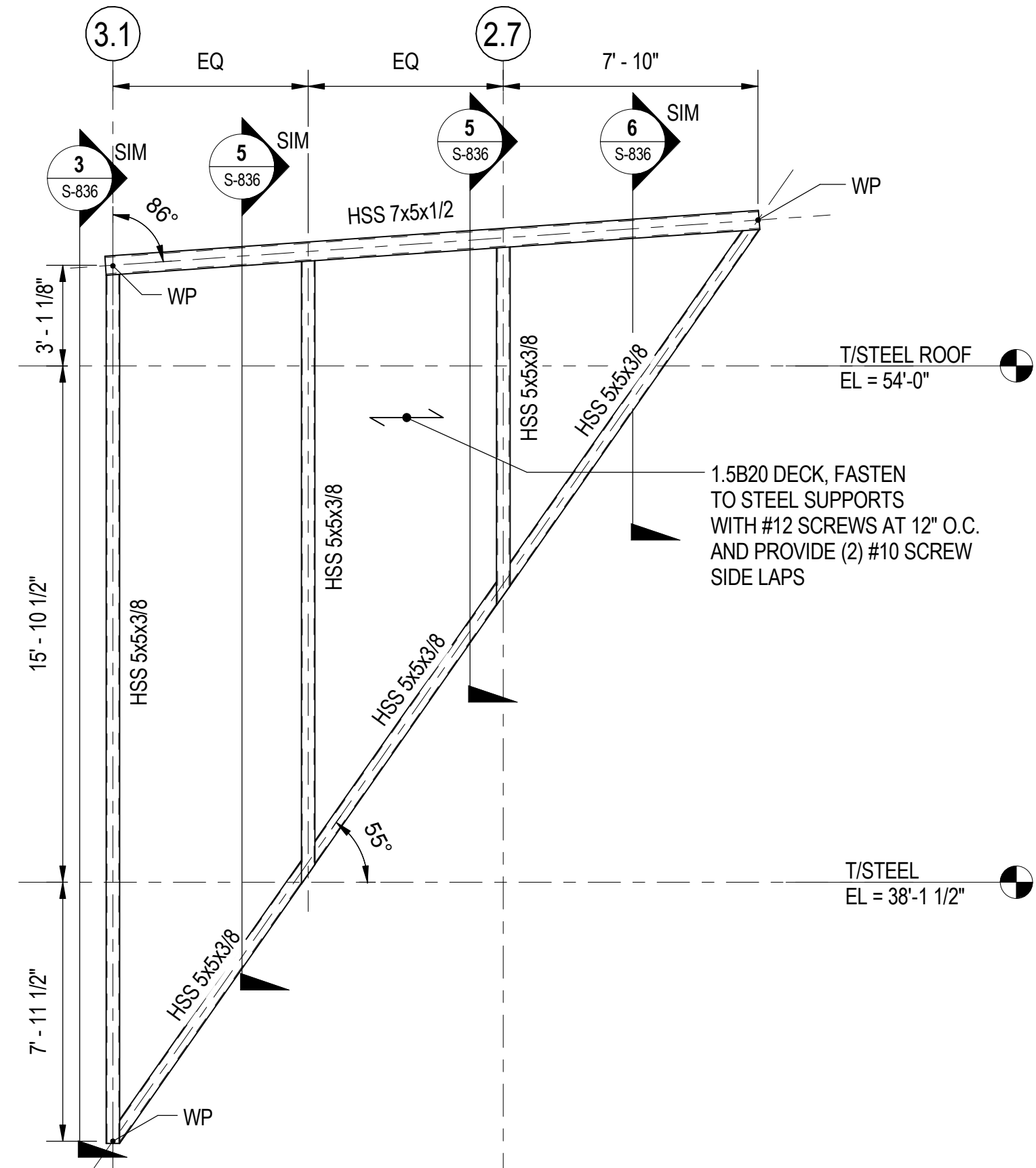
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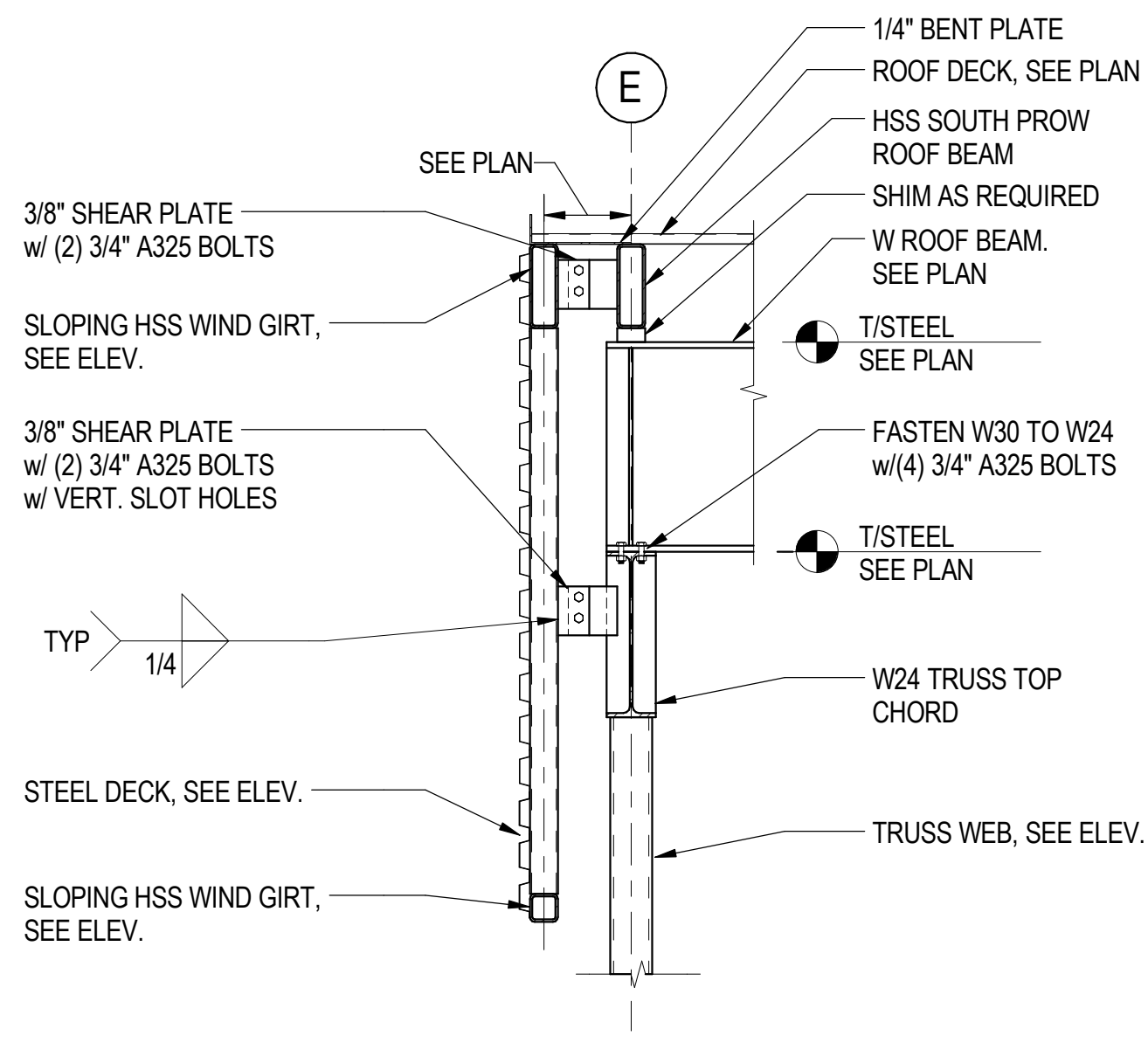
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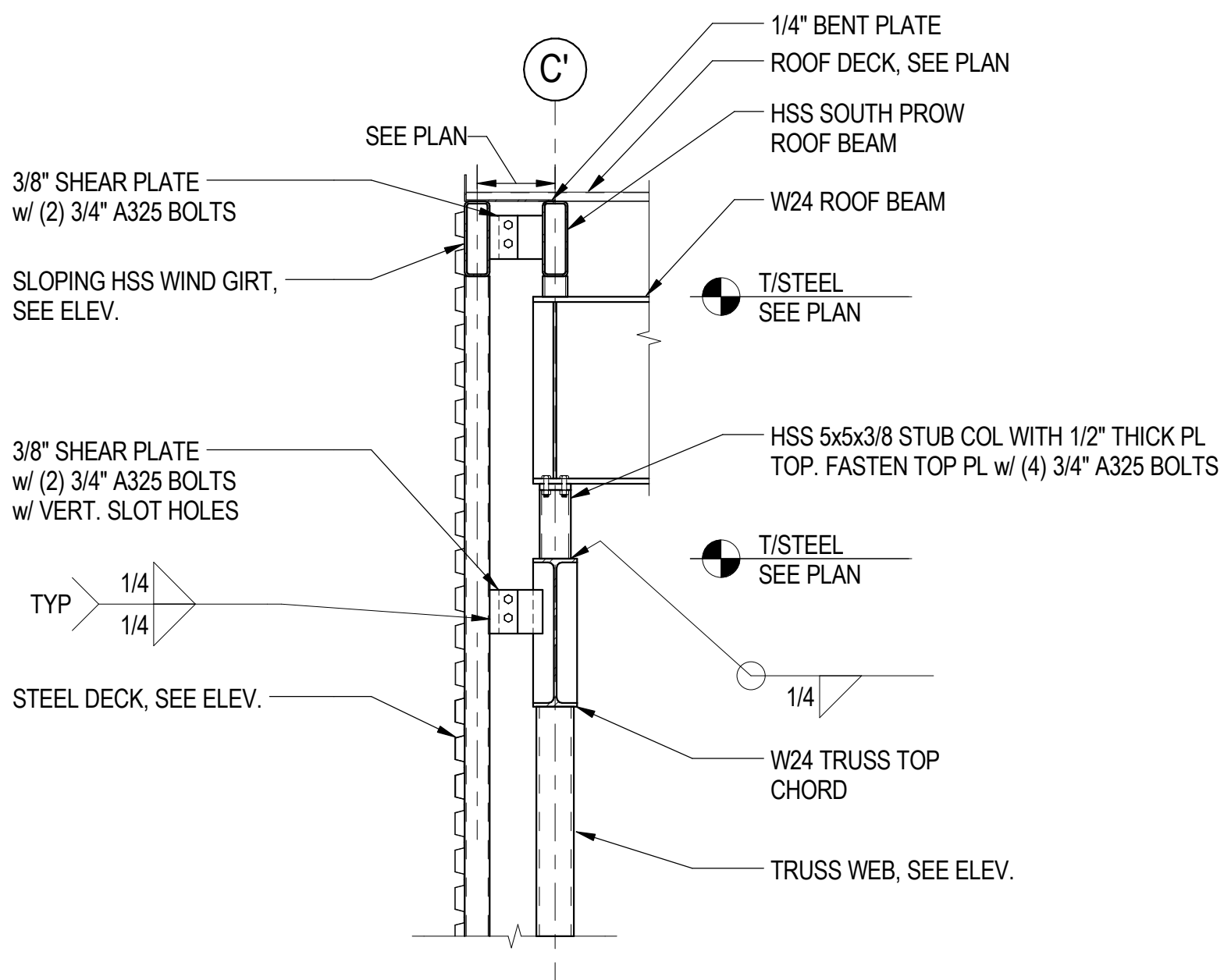
**1** CLADDING SUPPORT EAST ELEVATION  
 S-836 SCALE: 1/4" = 1'-0"



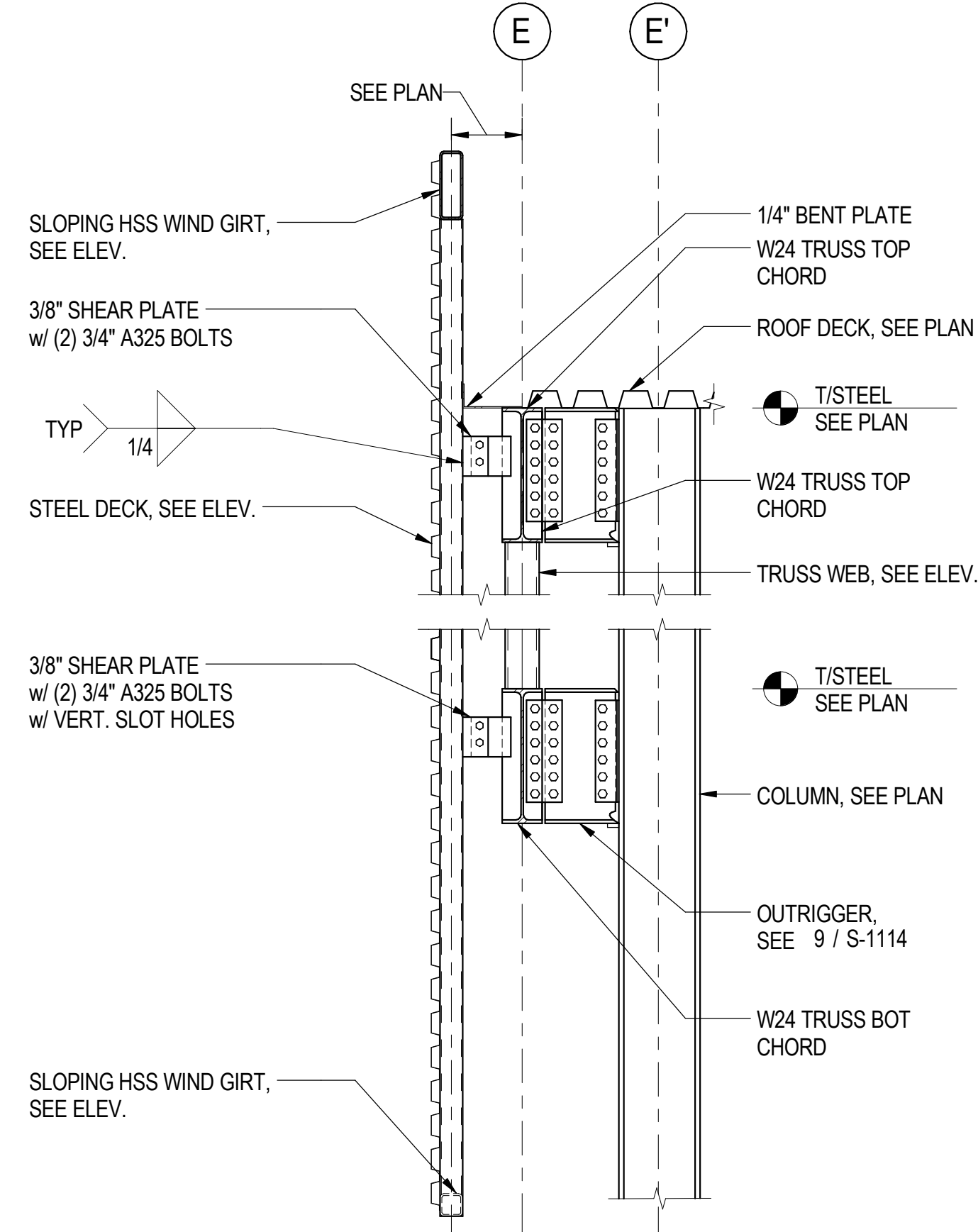
**4** CLADDING SUPPORT WEST ELEVATION  
 S-836 SCALE: 1/4" = 1'-0"



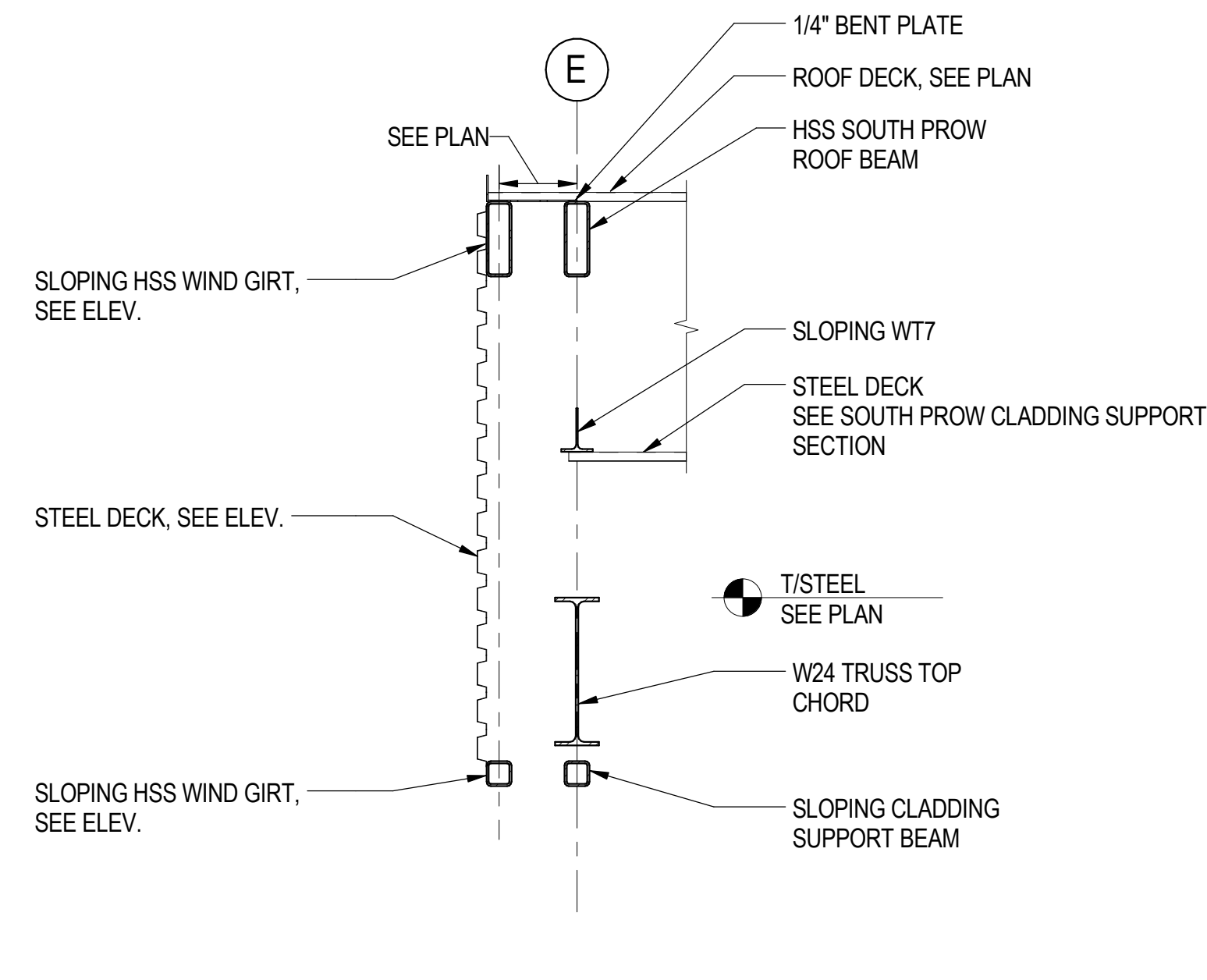
**2** CLADDING SUPPORT DETAIL AT 2.7  
 S-836 SCALE: 1/2" = 1'-0"



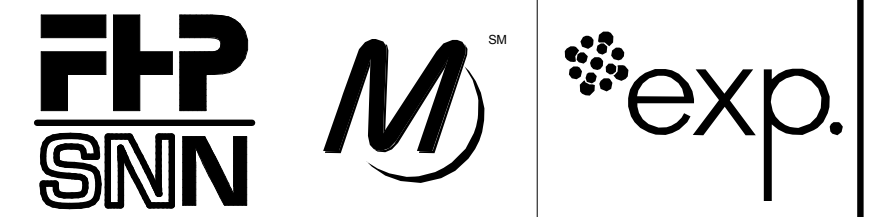
**5** CLADDING SUPPORT DETAIL AT 2.7  
 S-836 SCALE: 1/2" = 1'-0"



**3** CLADDING SUPPORT DETAIL AT GRID 3.1  
 S-836 SCALE: 1/2" = 1'-0"



**6** CLADDING SUPPORT SECTION  
 S-836 SCALE: 1/2" = 1'-0"



**SENSITIVE SECURITY INFORMATION**  
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DRAWING SCALE IS NOT GUARANTEED. CTA ASSUMES NO RISK OF LIABILITY FOR ERRORS CAUSED, DIRECTLY OR INDIRECTLY, BY SCALING OF THIS DRAWING.

**95th Terminal Improvement**  
 15 West 95th Street (South Terminal)  
 Chicago, IL 60628

IN CHARGE	J. JAKALSKI
APPROVED BY	J. JAKALSKI
CHECKED BY	R.S. NAIR
DESIGNED BY	G. FORKAN
DRAWN BY	J. PEARSON
PROJECT NO.	2012-0021.02
FILE NAME	DR095_S-Terminal

MARK	2/9/17	ISSUED FOR CONSTRUCTION
DATE		DESCRIPTION

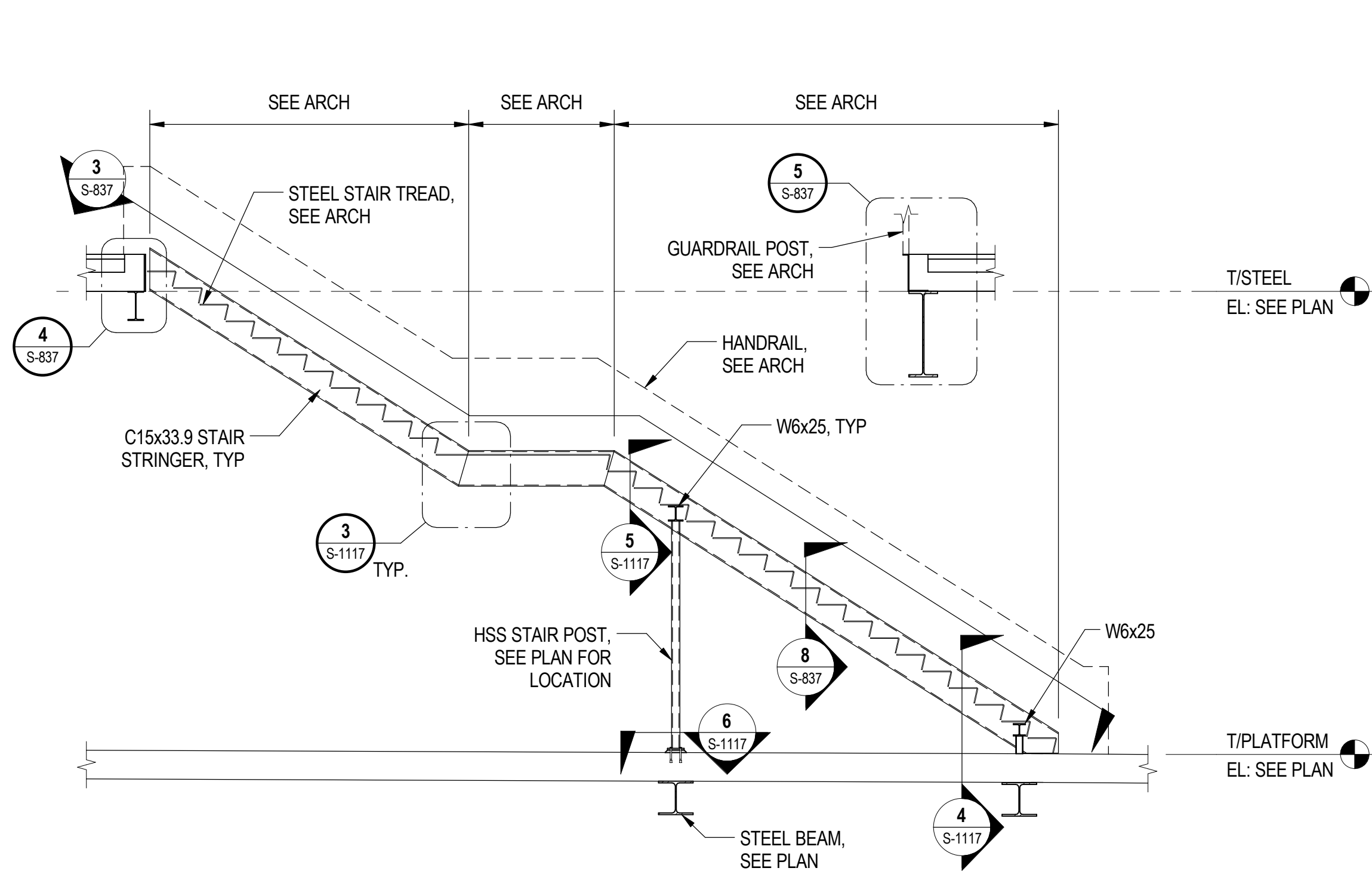
LOCATION IDENTIFIER: DR095

**SOUTH TERMINAL ELEVATIONS AND DETAILS**

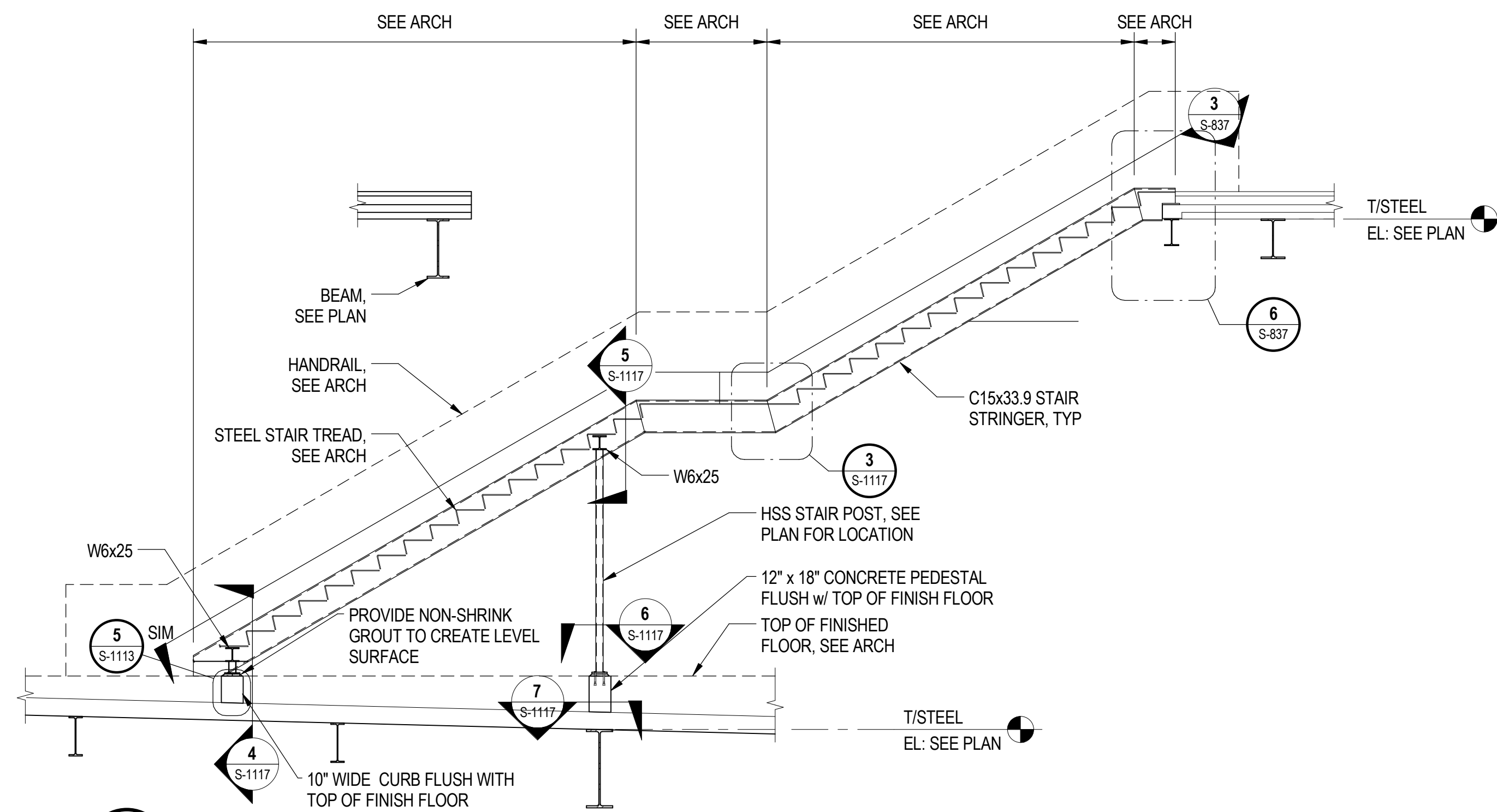
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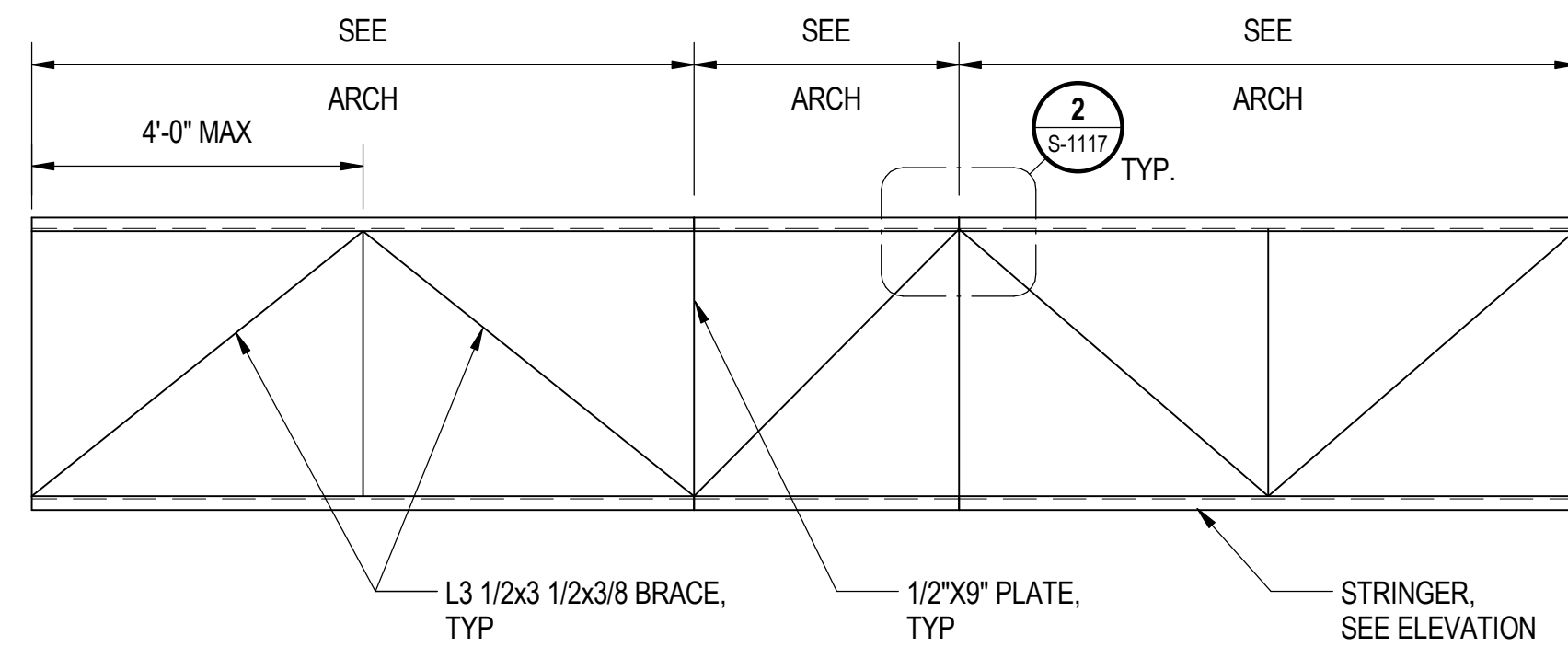
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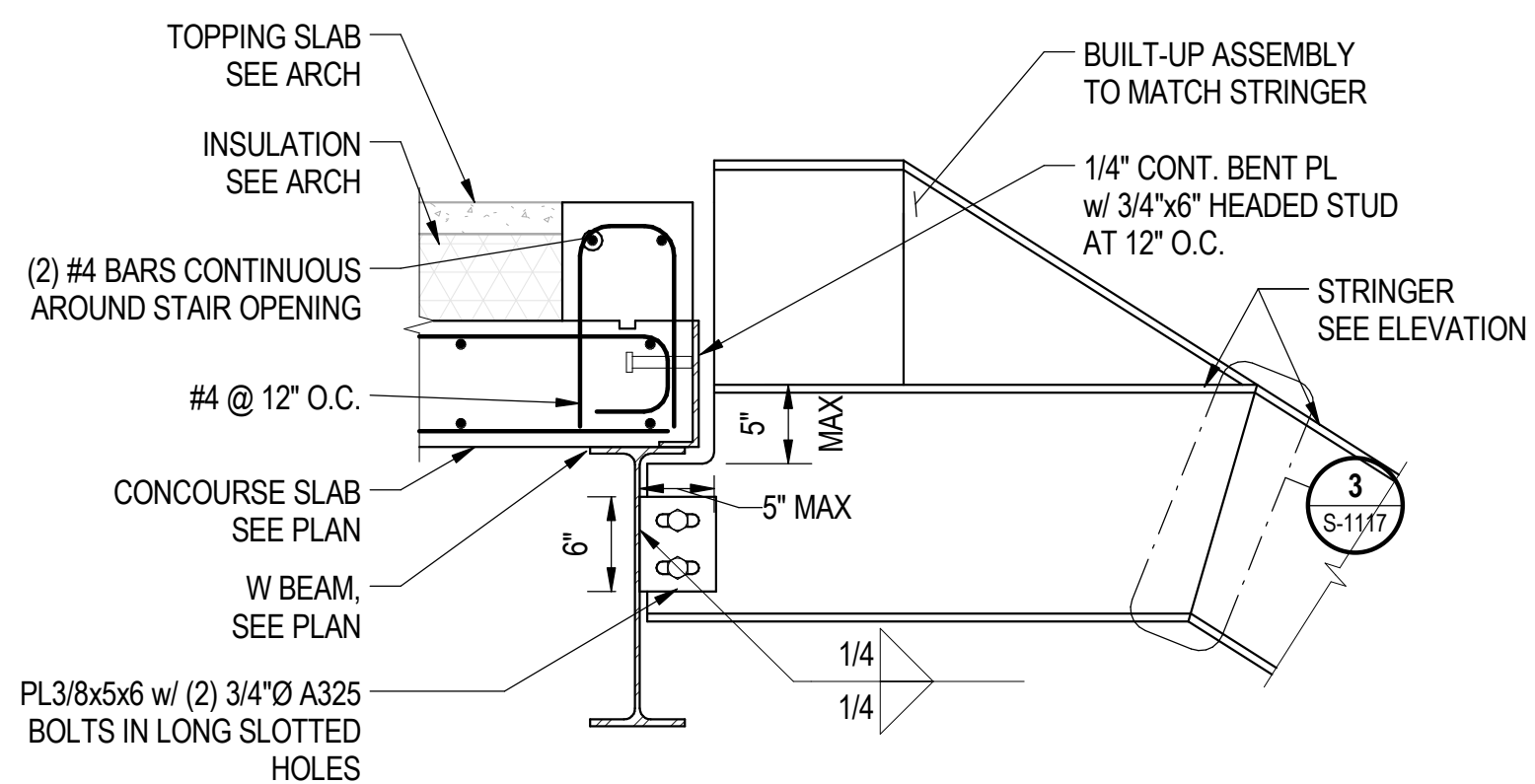
**1** PLATFORM TO CONCOURSE STAIR ELEVATION  
S-837 SCALE: 1/4" = 1'-0"



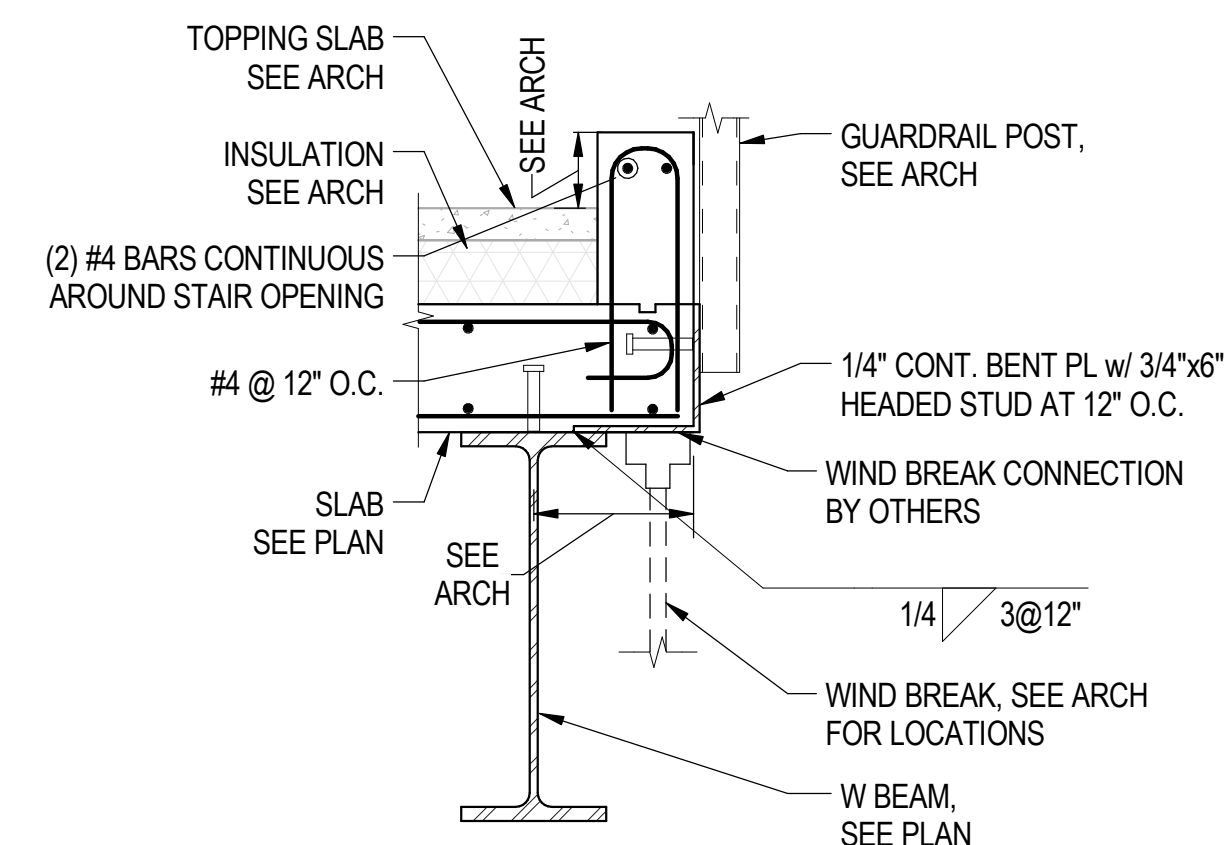
**2** CONCOURSE TO SECOND FLOOR STAIR ELEVATION  
S-837 SCALE: 1/4" = 1'-0"



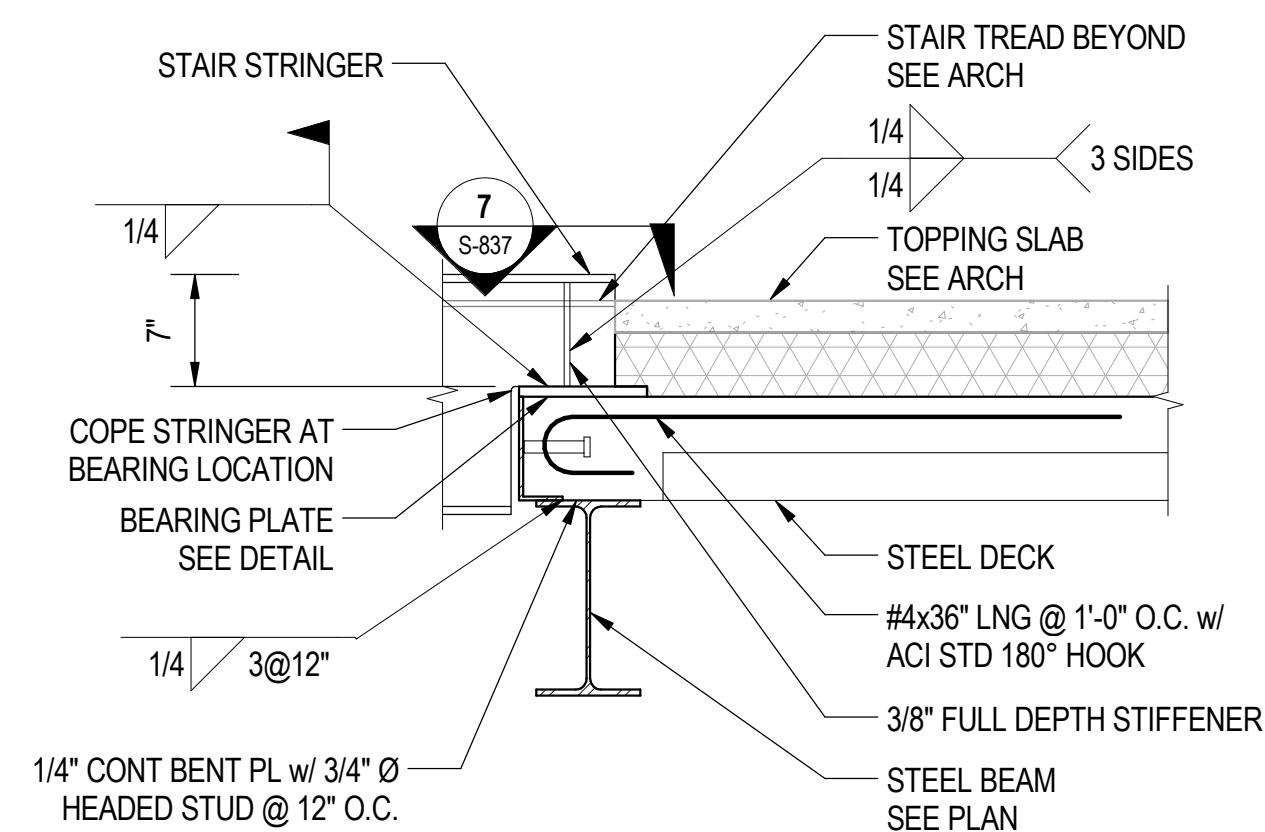
**3** SOUTH TERMINAL STAIR PLAN  
S-837 SCALE: 1/4" = 1'-0"



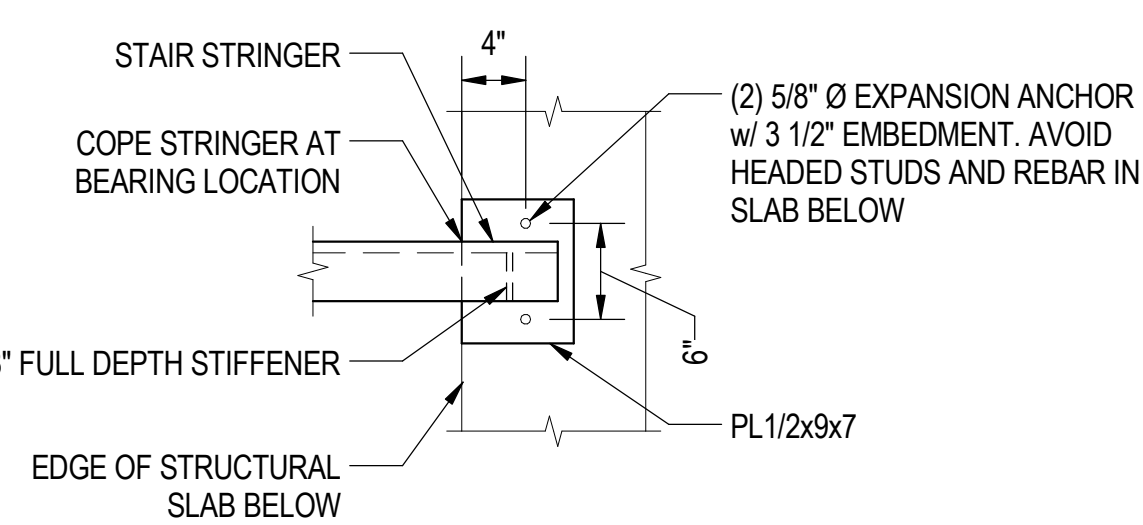
**4** STAIR STRINGER TOP SLIP CONNECTION  
S-837 SCALE: 1" = 1'-0"



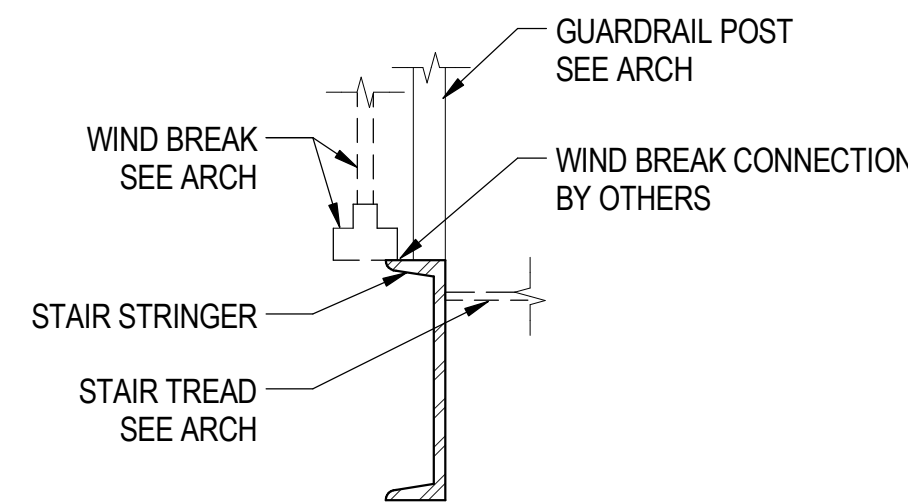
**5** GUARDRAIL POST ATTACHMENT DETAIL  
S-837 SCALE: 1" = 1'-0"



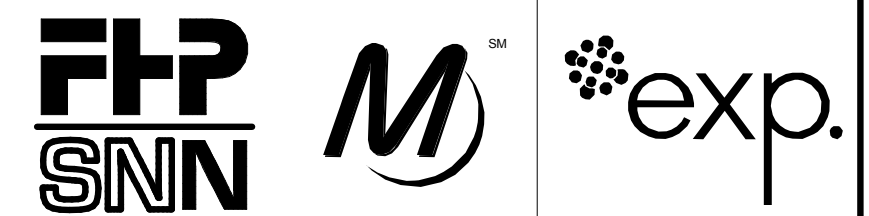
**6** STRINGER CONNECTION AT COMPOSITE SLAB  
S-837 SCALE: 1" = 1'-0"



**7** STRINGER CONNECTION DETAIL  
S-837 SCALE: 1" = 1'-0"



**8** WIND BREAK AT STRINGER  
S-837 SCALE: 1" = 1'-0"



**SENSITIVE SECURITY INFORMATION**  
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**95th Terminal Improvement**  
15 West 95th Street (South Terminal)  
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IN CHARGE	J. JAKALSKI
APPROVED BY	J. JAKALSKI
CHECKED BY	R.S. NAIR
DESIGNED BY	G. FORKAN
DRAWN BY	J. PEARSON
PROJECT NO.	2012-0021.02
FILE NAME	DR095_S-Terminal

MARK	DATE	DESCRIPTION
	2/9/17	ISSUED FOR CONSTRUCTION

LOCATION IDENTIFIER: **DR095**

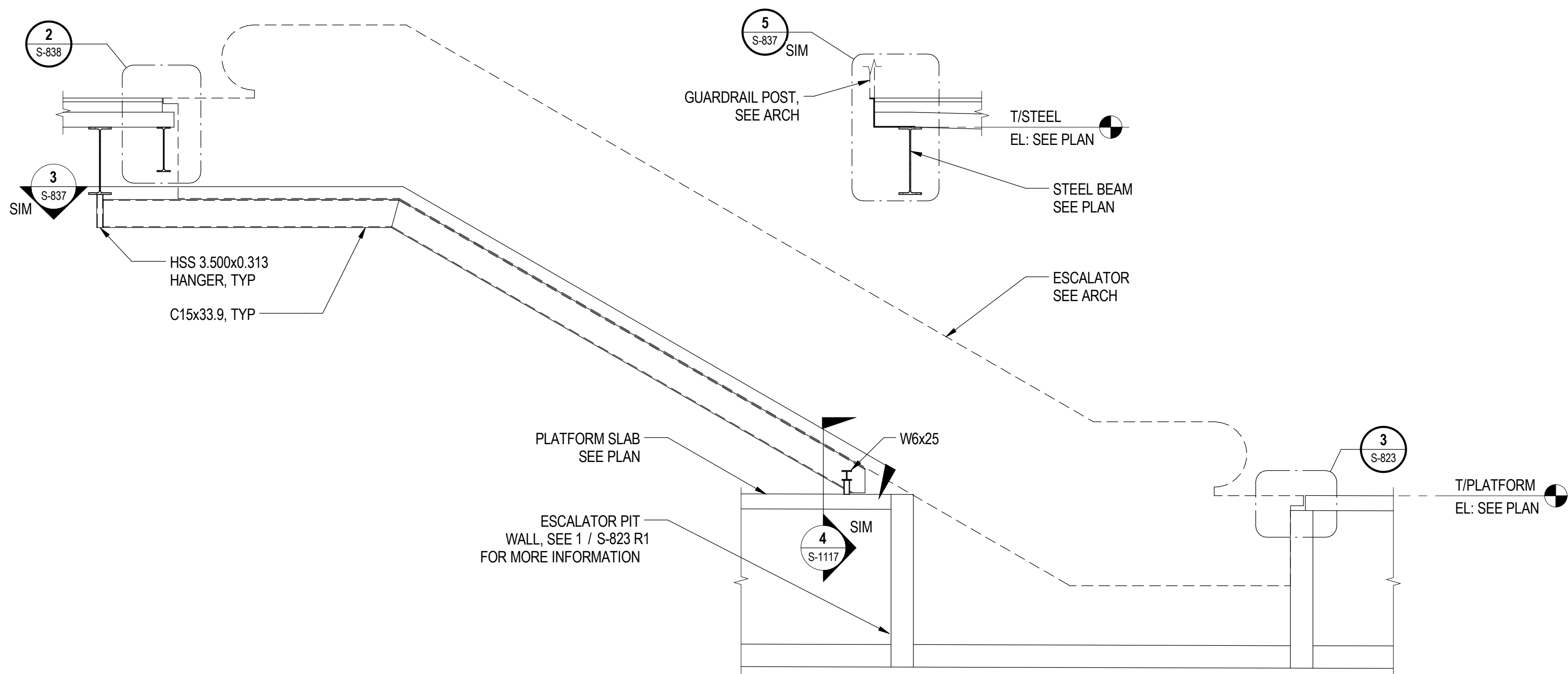
**SOUTH TERMINAL STAIR ELEVATION AND DETAILS**

**S-837**

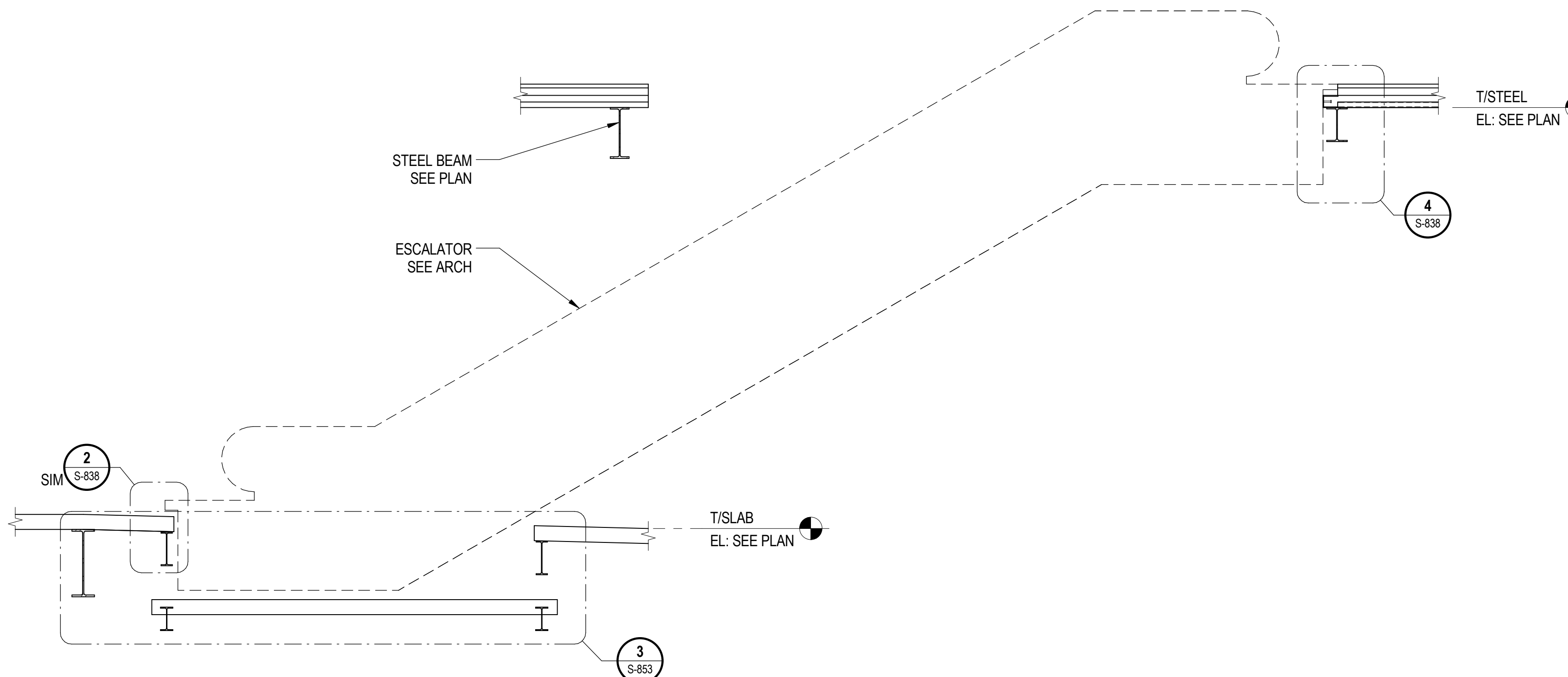


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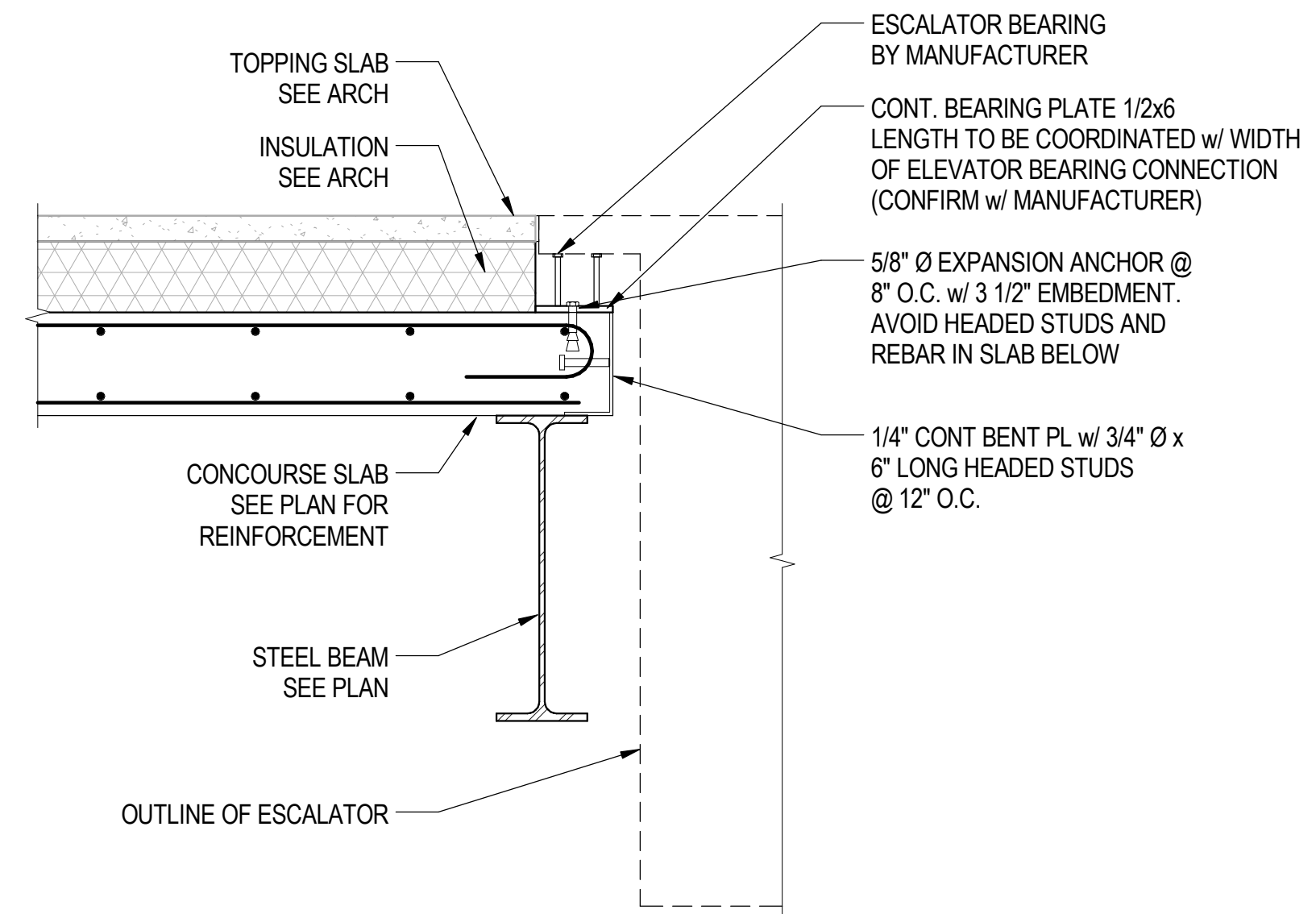
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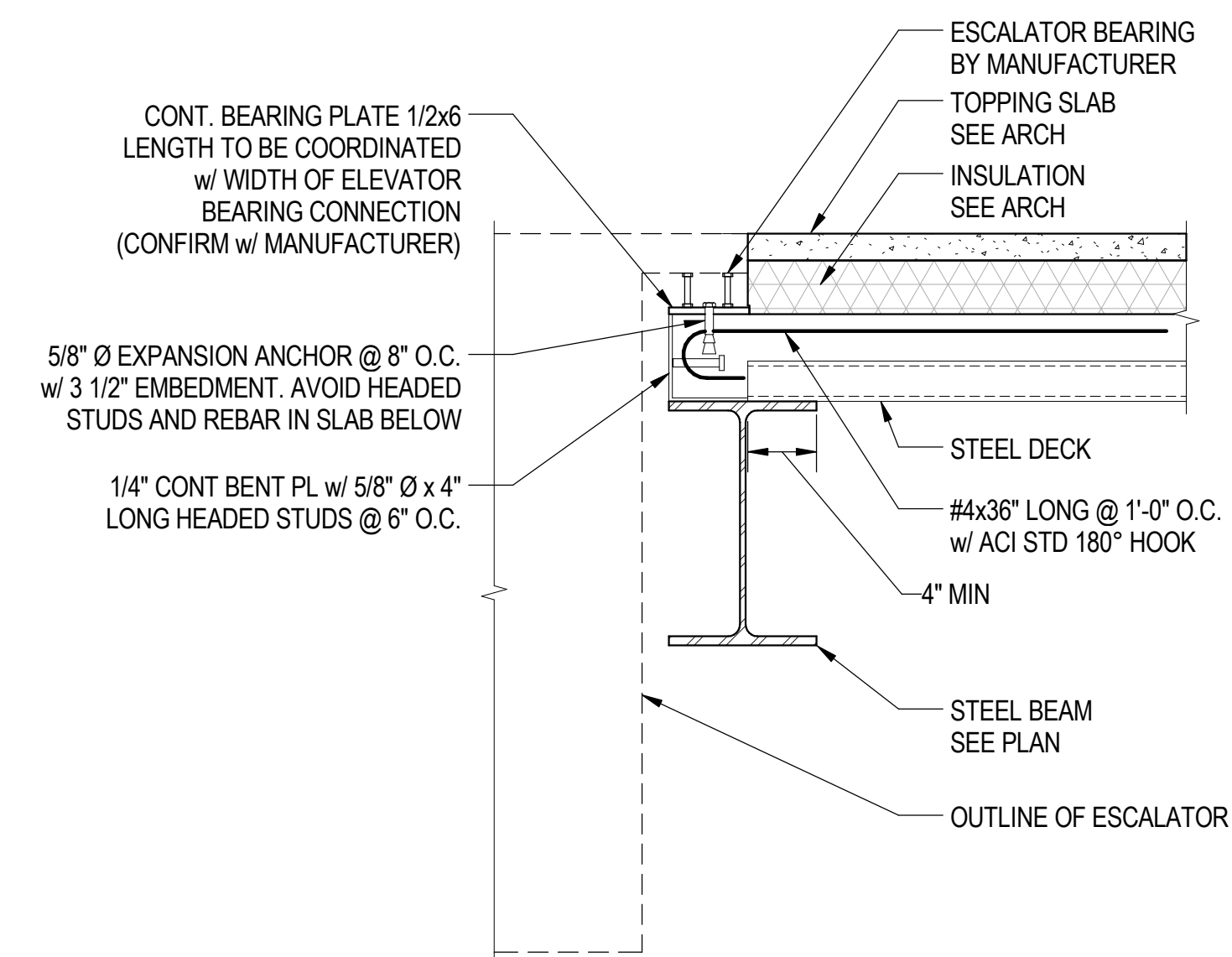
**1 ESCALATOR 1 ELEVATION**  
S-838 SCALE: 1/4" = 1'-0"



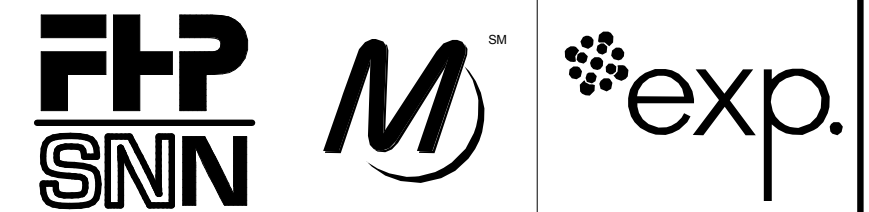
**3 SOUTH TERMINAL ESCALATOR ELEVATION**  
S-838 SCALE: 1/4" = 1'-0"



**2 ESCALATOR SUPPORT AT CONCOURSE SLAB**  
S-838 SCALE: 1" = 1'-0"



**4 ESCALATOR SUPPORT DETAIL**  
S-838 SCALE: 1" = 1'-0"



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**95th Terminal Improvement**  
15 West 95th Street (South Terminal)  
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APPROVED BY	J. JAKALSKI
CHECKED BY	R.S. NAIR
DESIGNED BY	G. FORKAN
DRAWN BY	J. PEARSON
PROJECT NO.	2012-0021.02
FILE NAME	DR095_S-Terminal

MARK	DATE	DESCRIPTION
	2/9/17	ISSUED FOR CONSTRUCTION

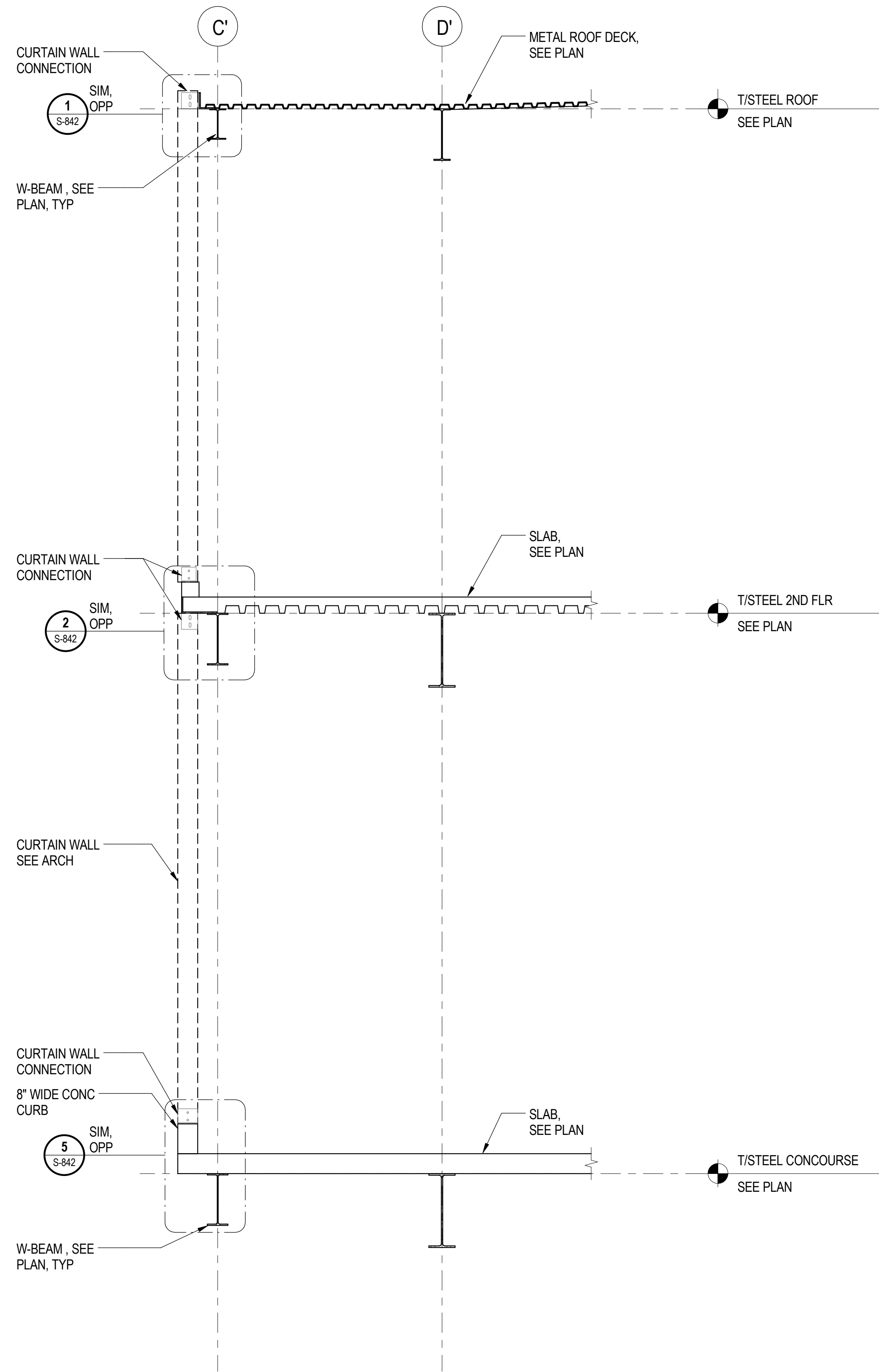
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**SOUTH TERMINAL ESCALATOR ELEVATION AND DETAILS**

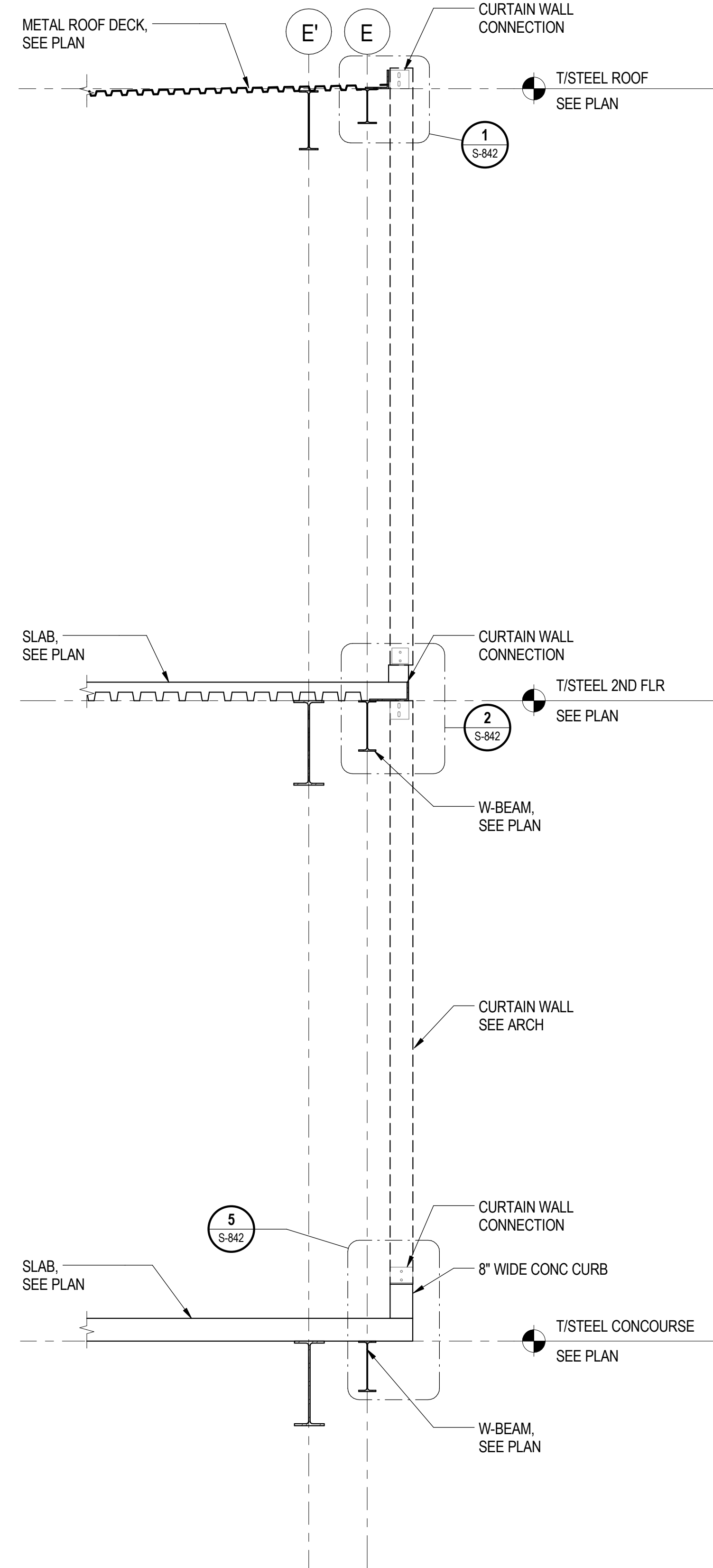
**S-838**

07/10/2016 2:23:25 PM

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**1** TYPICAL WEST EDGE SECTION  
S-840 SCALE: 3/8" = 1'-0"



**2** TYPICAL EAST EDGE SECTION  
S-840 SCALE: 3/8" = 1'-0"

**SHEET NOTES:**  
1. FOR GENERAL NOTES, SEE DWGS S-001 TO S-004.  
2. FOR TYPICAL DETAILS REFER TO S-1112 TO S-1120.



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FILE NAME	DR095_S-Terminal

MARK	DATE	DESCRIPTION
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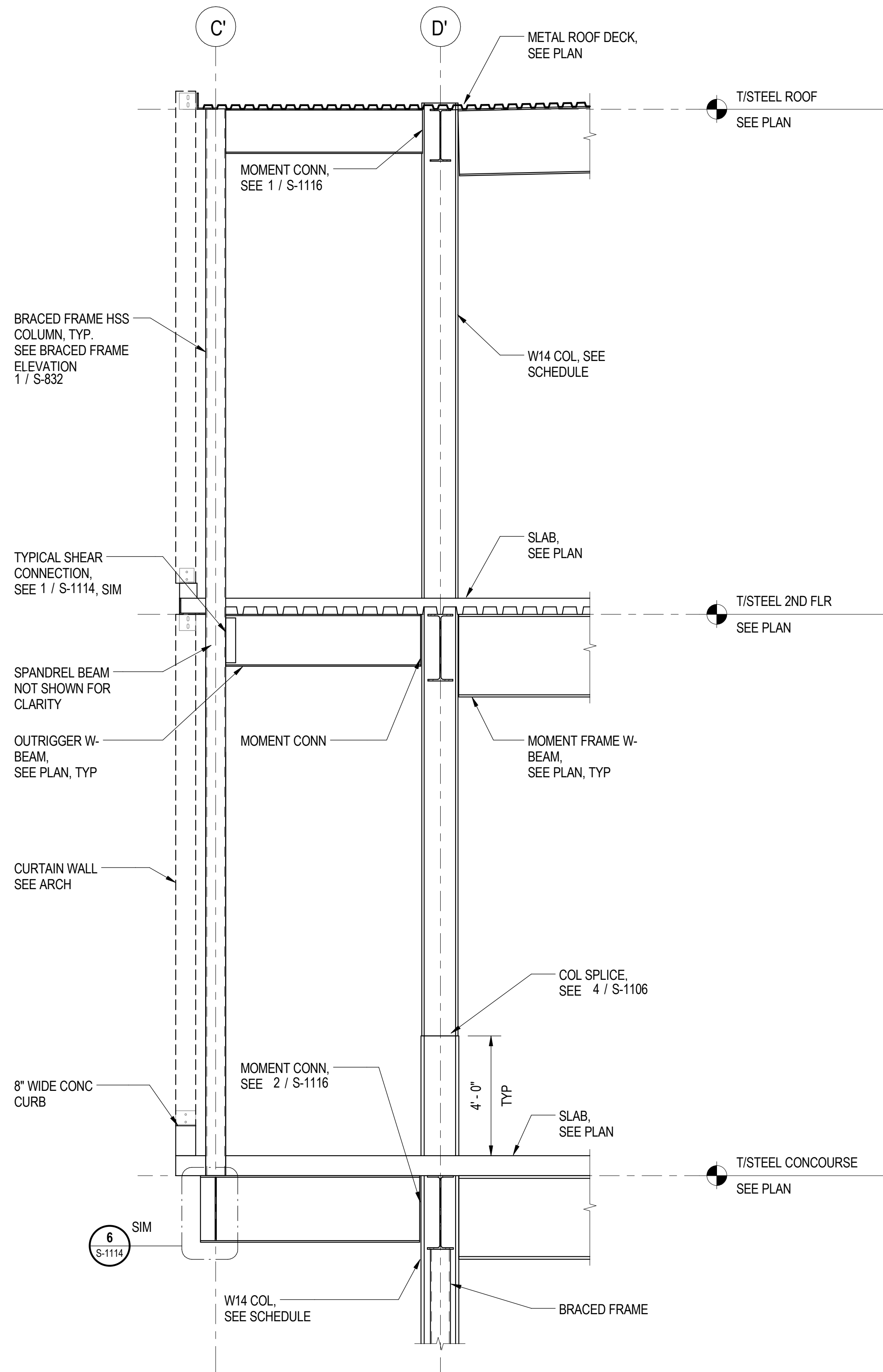
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**SOUTH TERMINAL SECTIONS AND DETAILS**

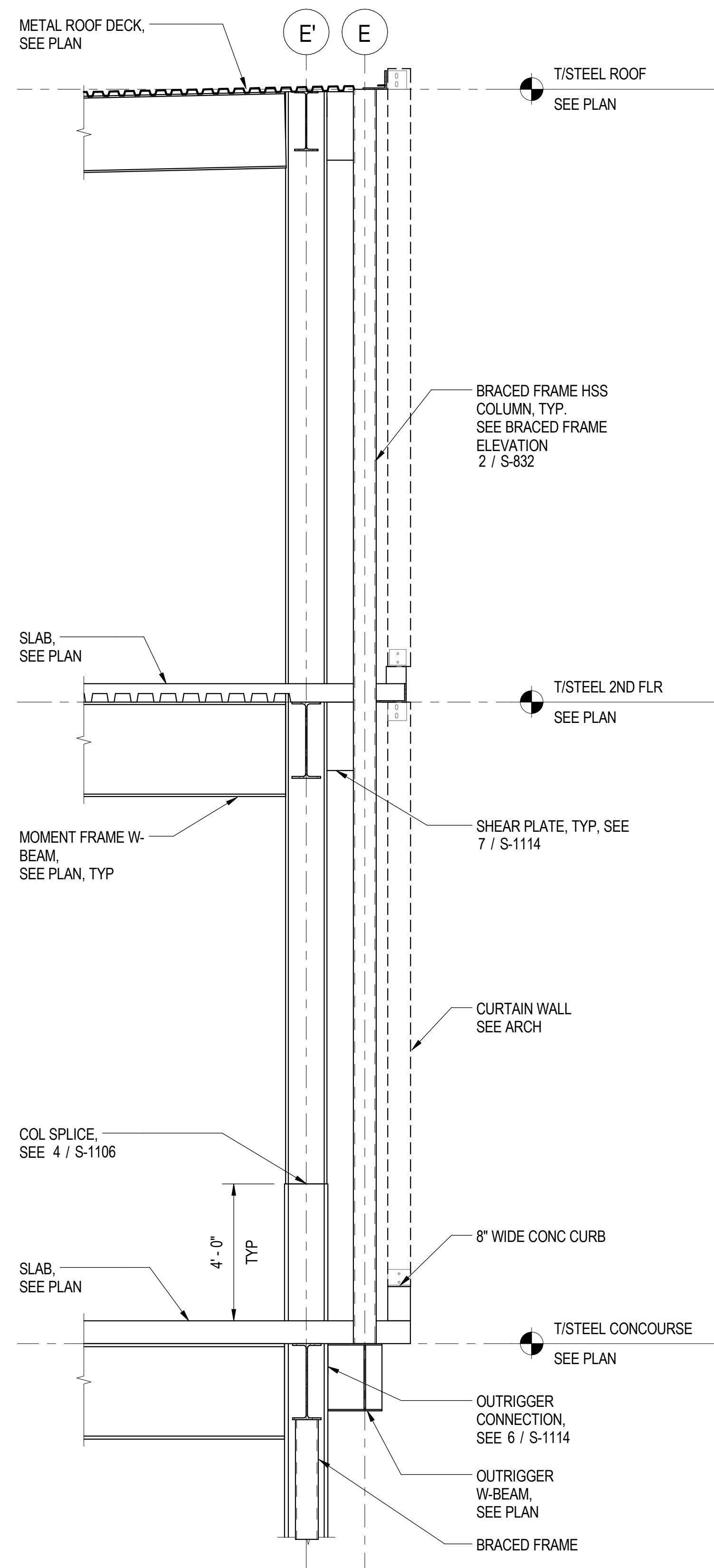
**S-840**

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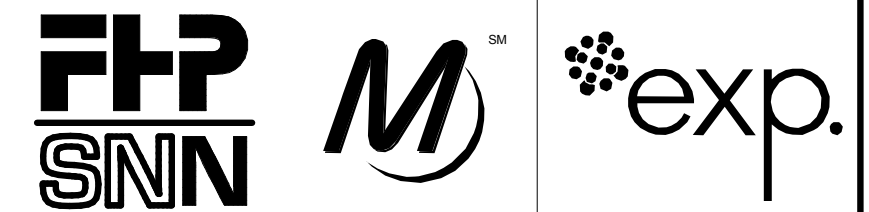
**1** TYPICAL WEST EDGE SECTION AT BRACED FRAME  
S-841 SCALE: 3/8" = 1'-0"



**2** TYPICAL EAST EDGE SECTION AT BRACED FRAME  
S-841 SCALE: 3/8" = 1'-0"

**SHEET NOTES:**

- FOR GENERAL NOTES, SEE DWGS S-001 TO S-004.
- FOR TYPICAL DETAILS REFER TO S-1112 TO S-1120.



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**95th Terminal Improvement**  
 15 West 95th Street (South Terminal)  
 Chicago, IL 60628

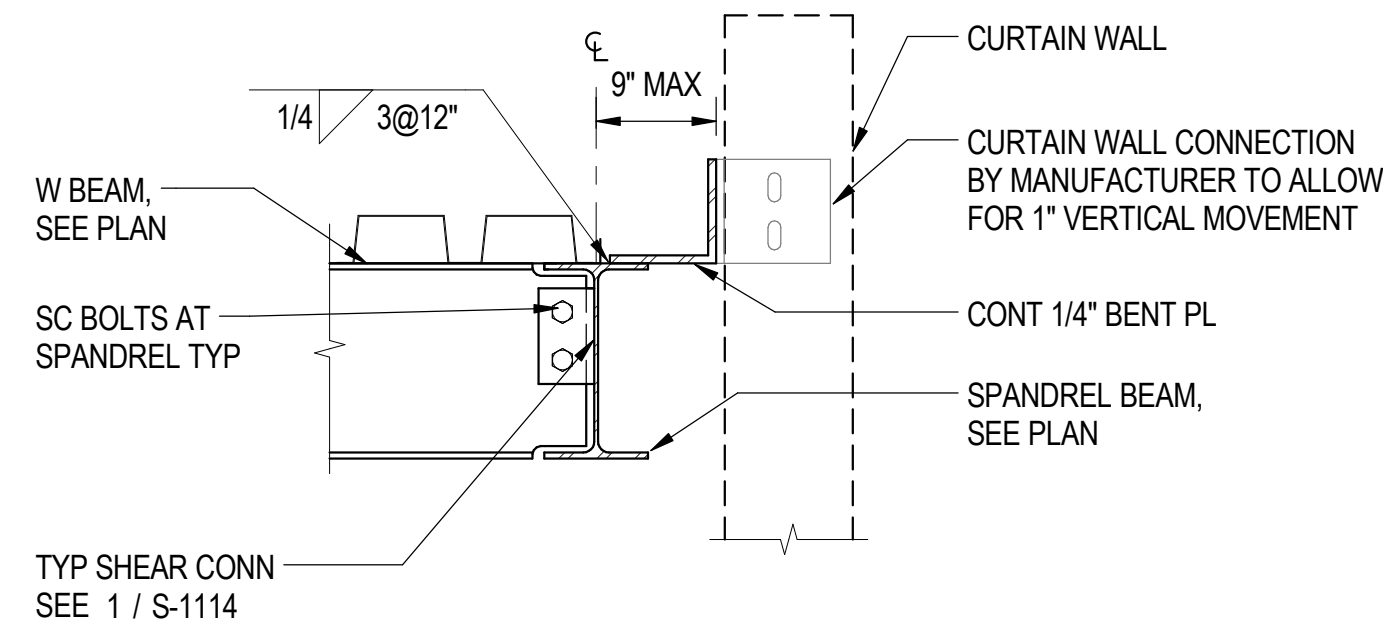
IN CHARGE	J. JAKALSKI
APPROVED BY	J. JAKALSKI
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PROJECT NO.	2012-0021.02
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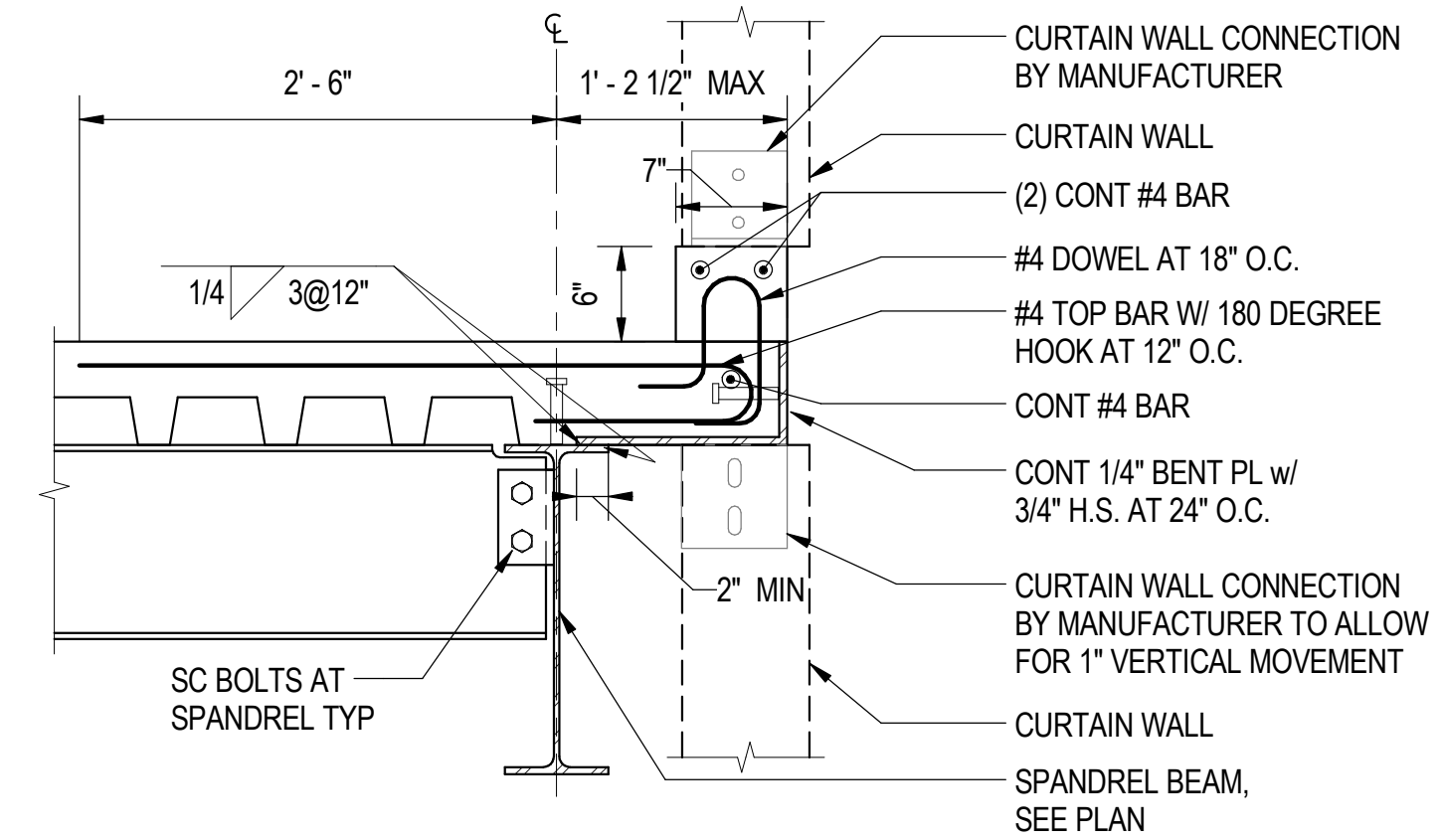
LOCATION IDENTIFIER: **DR095**

**SOUTH TERMINAL SECTIONS AND DETAILS**

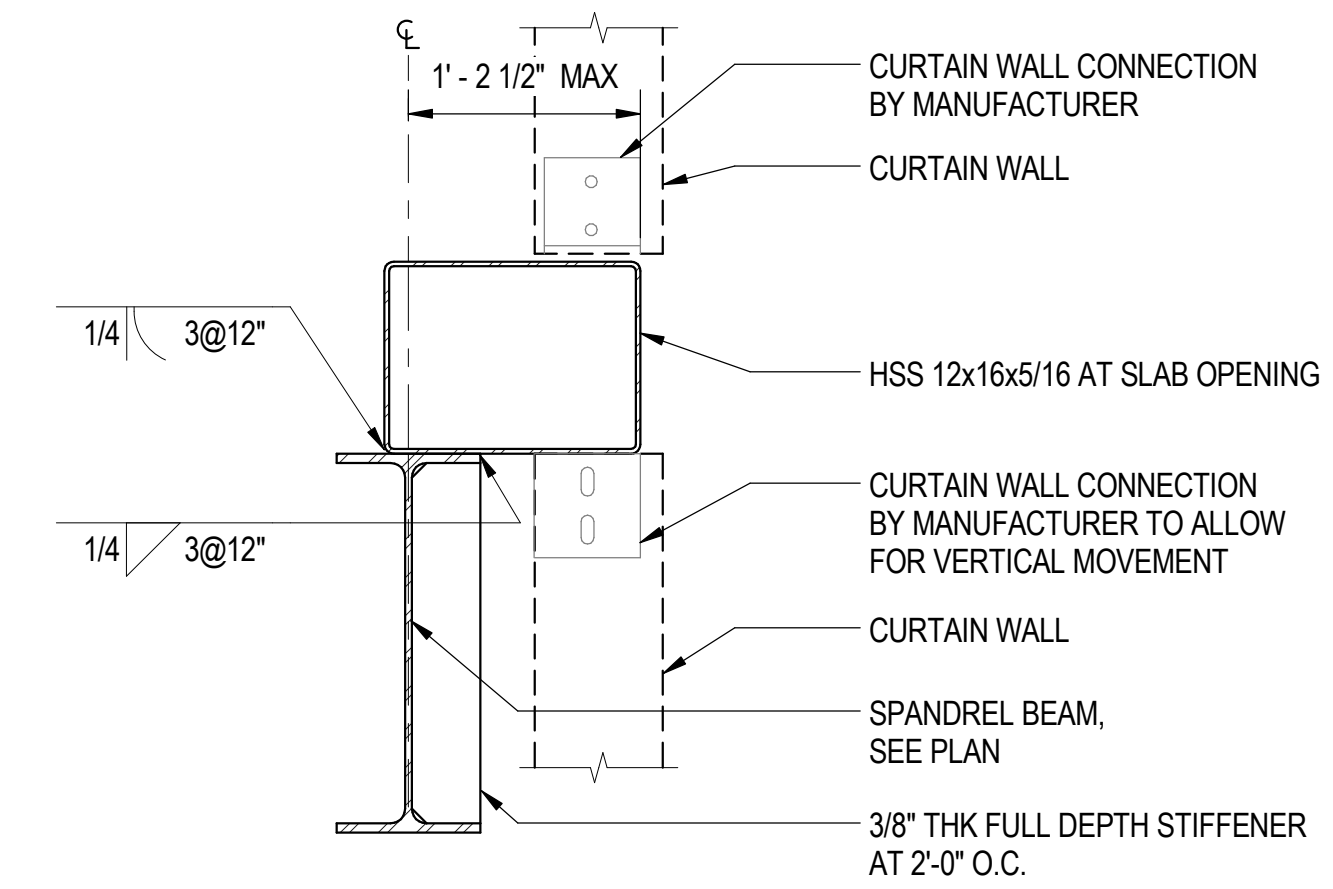
**S-841**



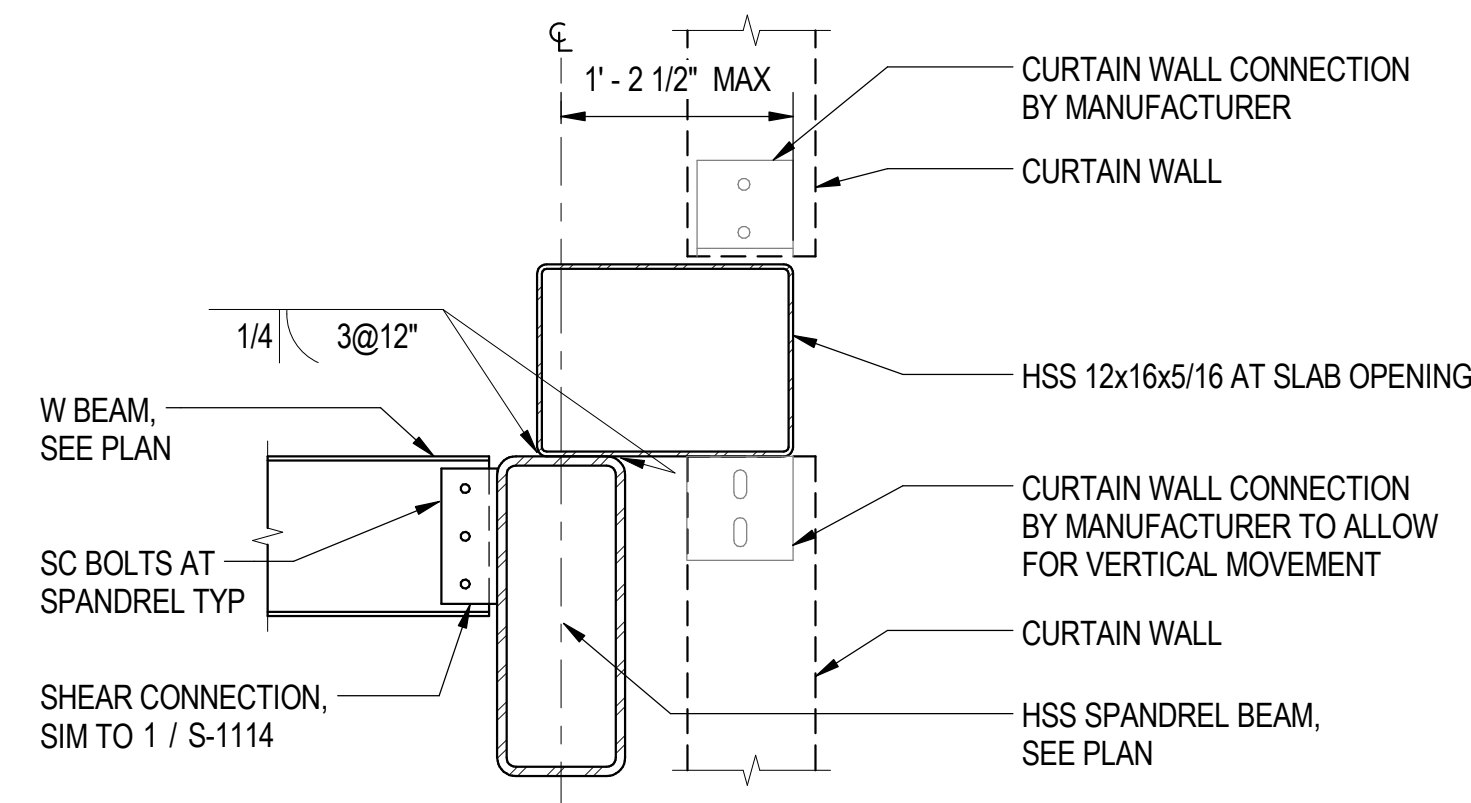
**1** SOUTH TERMINAL - CLADDING CONNECTION AT ROOF  
S-842 SCALE: 1" = 1'-0"



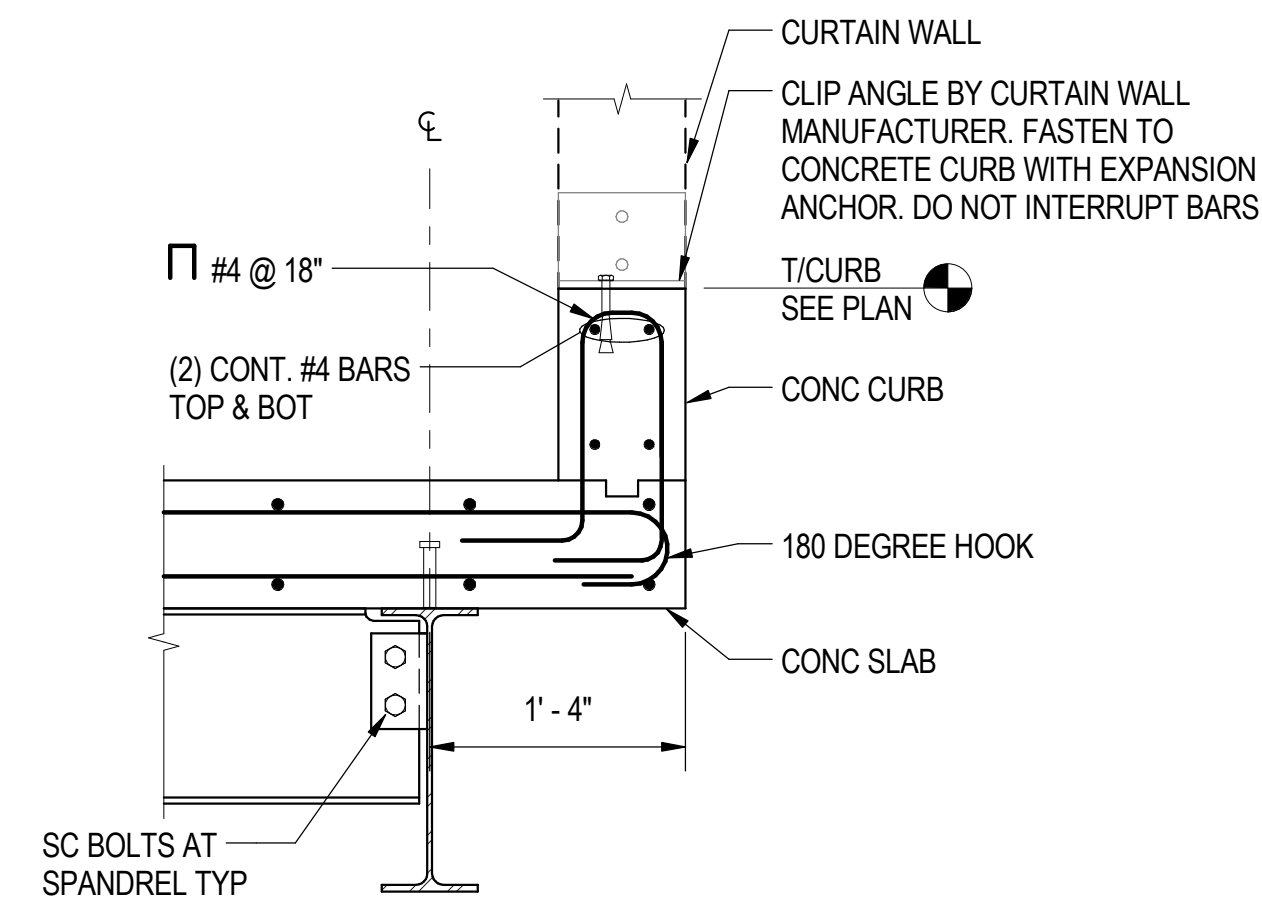
**2** SOUTH TERMINAL - CLADDING CONNECTION AT LEVEL 02  
S-842 SCALE: 1" = 1'-0"



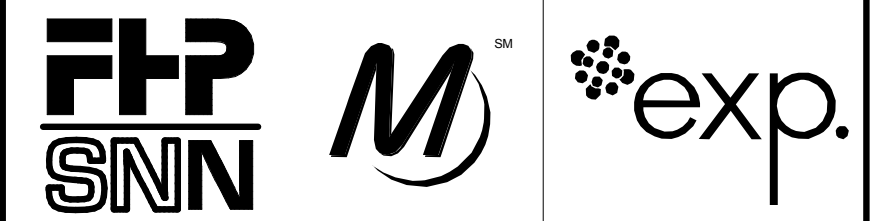
**3** SOUTH TERMINAL - CLADDING CONNECTION AT OPENING  
S-842 SCALE: 1" = 1'-0"



**4** SOUTH TERMINAL - CLADDING CONNECTION AT HSS  
S-842 SCALE: 1" = 1'-0"



**5** SOUTH TERMINAL - CLADDING CONNECTION AT LEVEL 01  
S-842 SCALE: 1" = 1'-0"



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15 West 95th Street (South Terminal)  
Chicago, IL 60628

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APPROVED BY	J. JAKALSKI
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PROJECT NO.	2012-0021.02
FILE NAME	DR095_S-Terminal

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	2/9/17	ISSUED FOR CONSTRUCTION

LOCATION IDENTIFIER: **DR095**

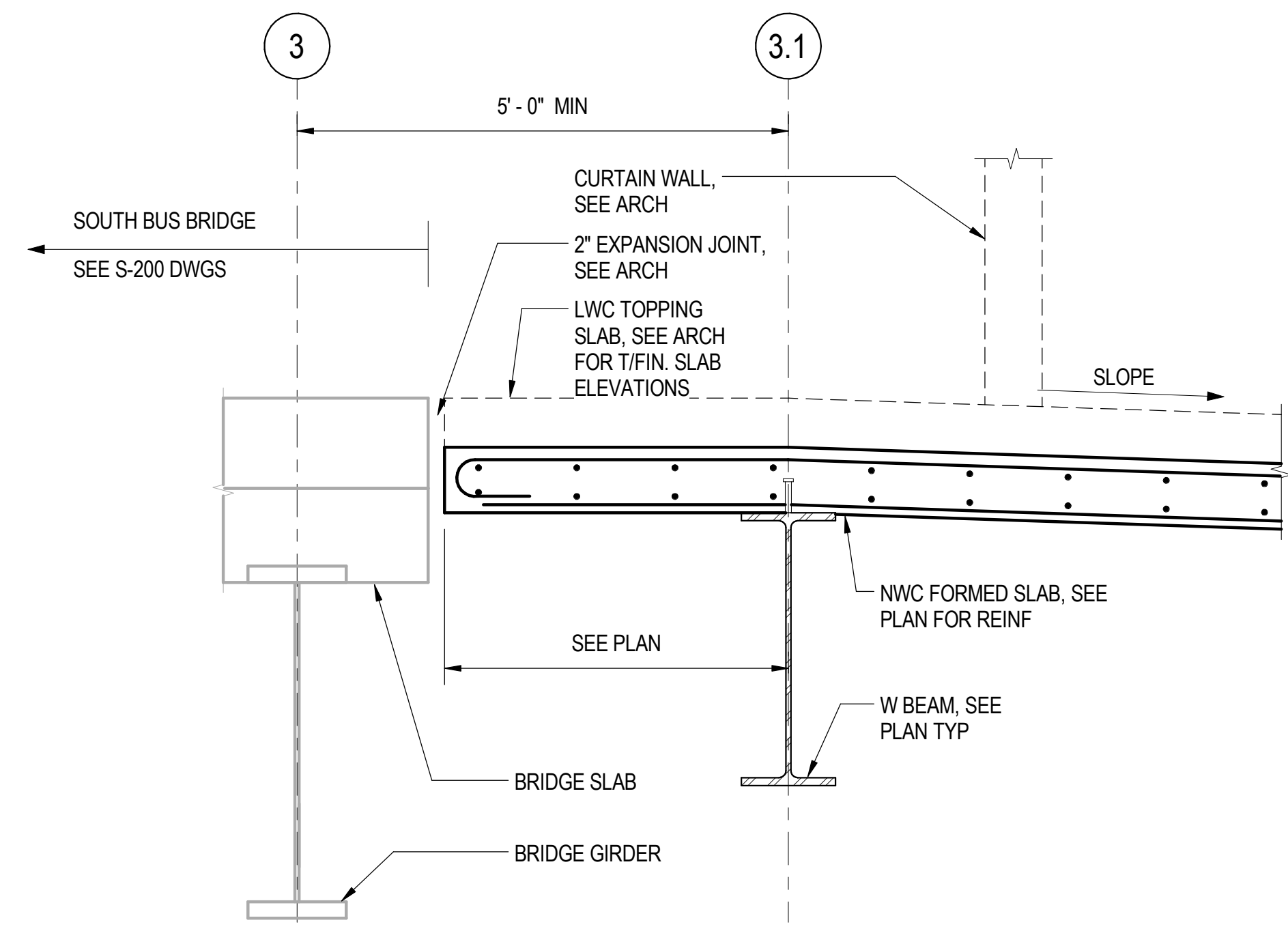
**SOUTH TERMINAL SECTIONS AND DETAILS**

**S-842**

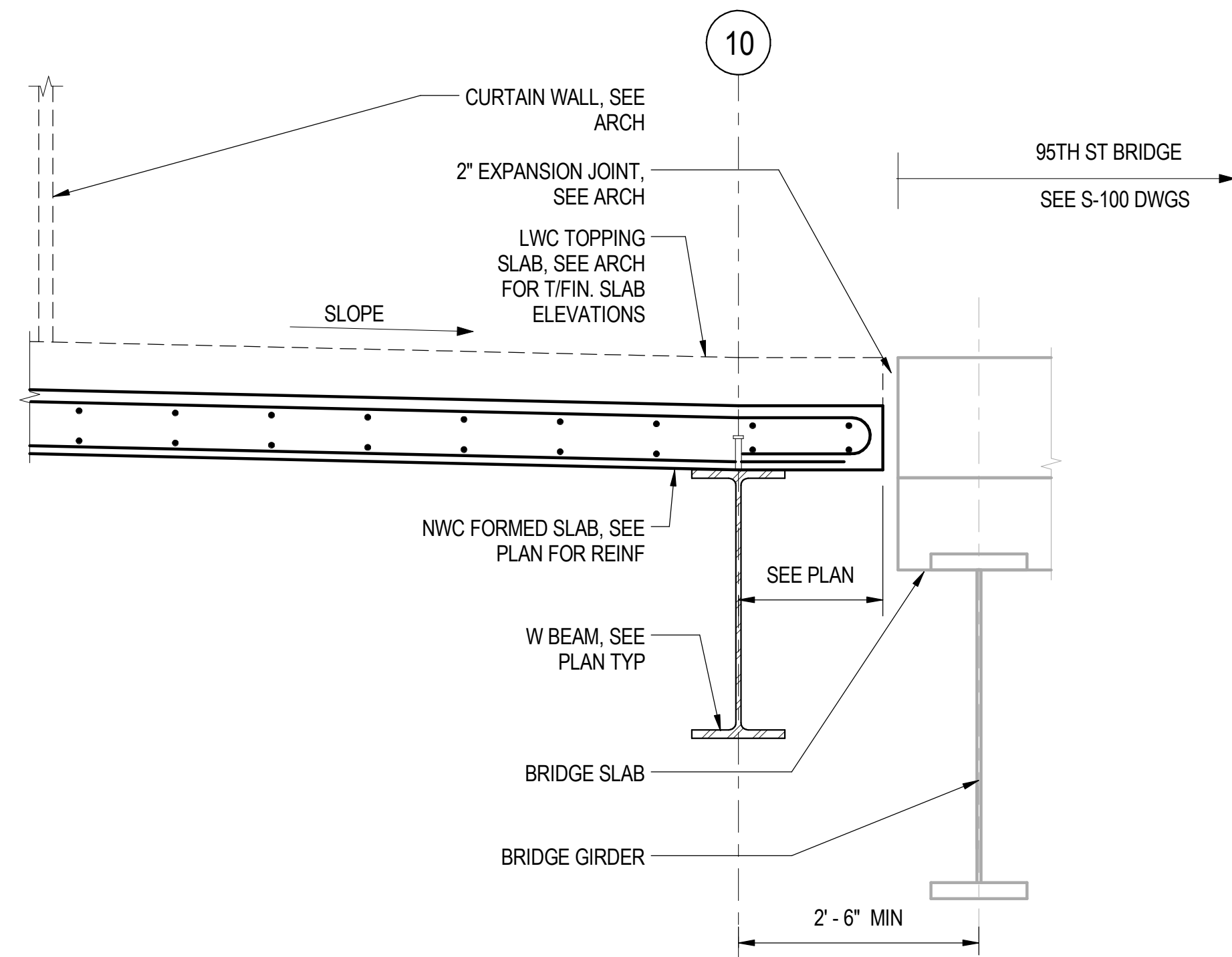
**SHEET NOTES:**  
1. FOR GENERAL NOTES, SEE DWGS S-001 TO S-004.  
2. FOR TYPICAL DETAILS REFER TO S-1112 TO S-1120.

30/11/2016 7:06:26 PM

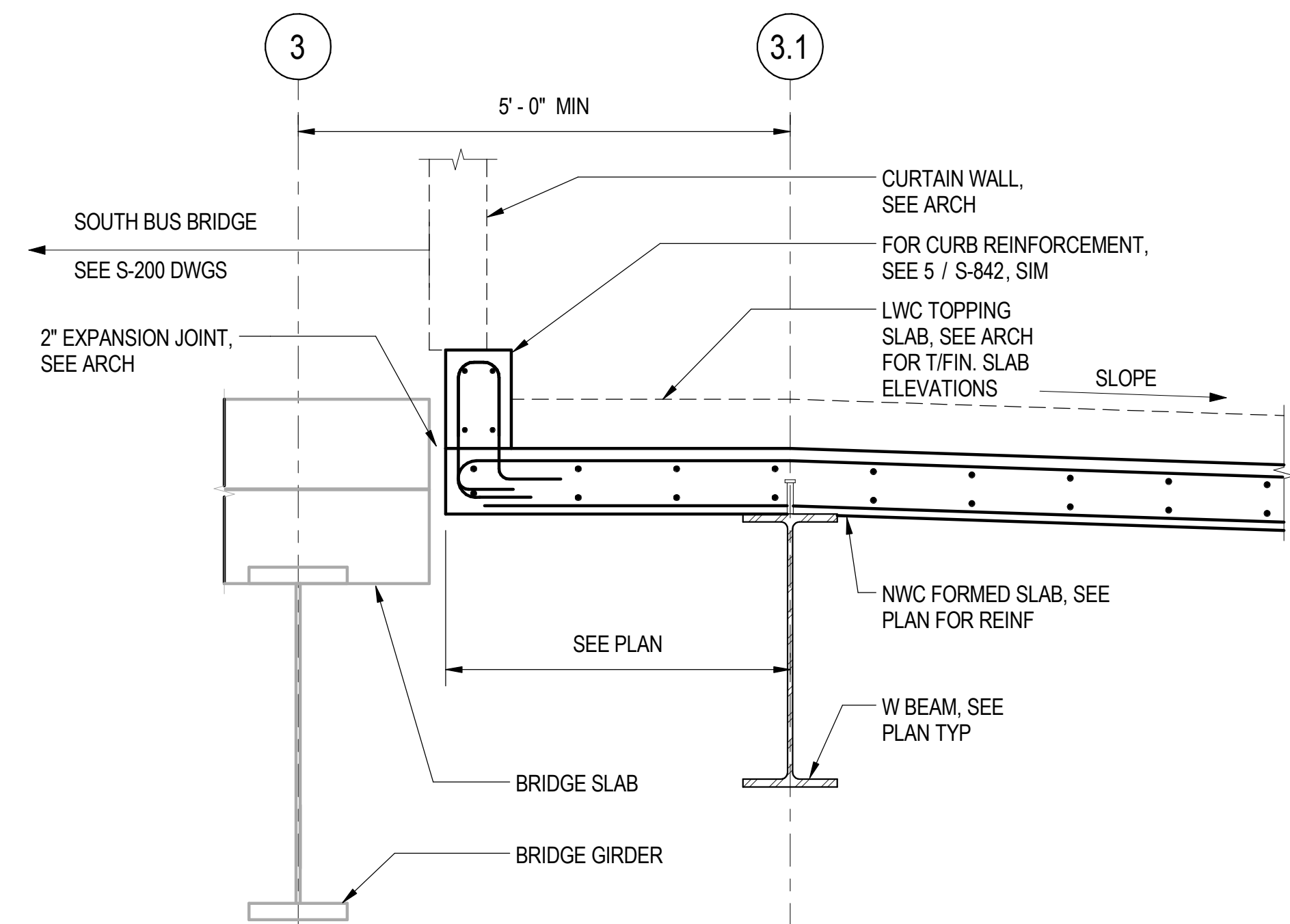
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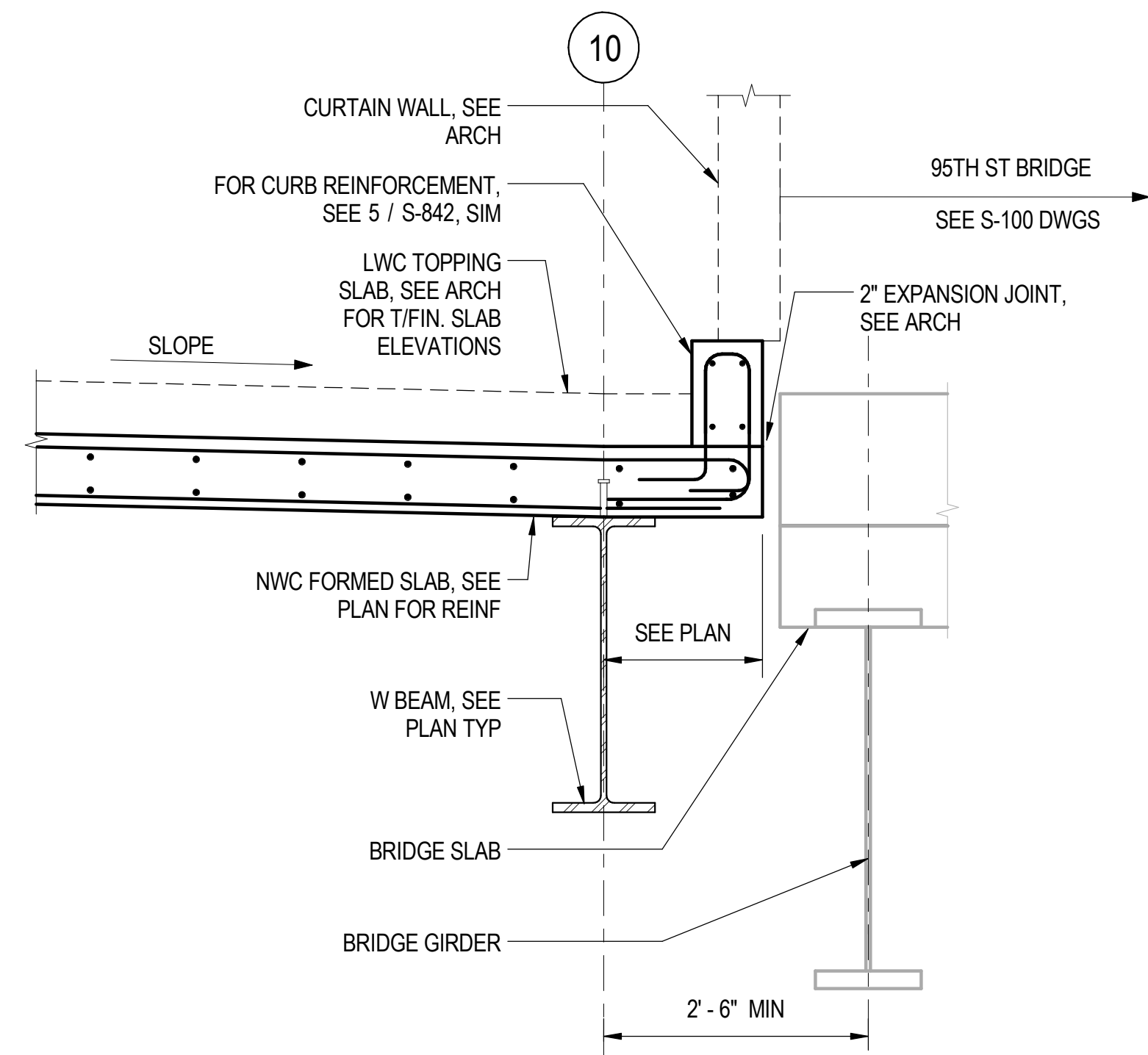
**1** SOUTH BUS BRIDGE/BUILDING INTERFACE  
S-843 SCALE: 3/4" = 1'-0"



**2** 95TH STREET BRIDGE/SOUTH TERMINAL INTERFACE  
S-843 SCALE: 3/4" = 1'-0"



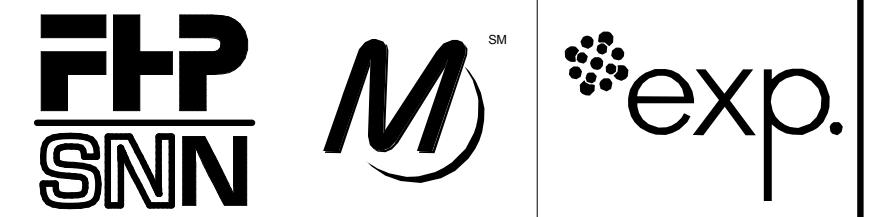
**3** SOUTH BUS BRIDGE/BUILDING INTERFACE AT CURB  
S-843 SCALE: 3/4" = 1'-0"



**4** 95TH STREET BRIDGE/SOUTH TERMINAL INTERFACE AT CURB  
S-843 SCALE: 3/4" = 1'-0"

**SHEET NOTES:**

1. FOR GENERAL NOTES, SEE DWGS S-001 TO S-004.
2. FOR TYPICAL DETAILS REFER TO S-1112 TO S-1120.
3. FOR TOP OF FINISHED FLOOR ELEVATION AND INFORMATION REGARDING SLOPED SLABS, SEE PLANS AND ARCH DWGS.



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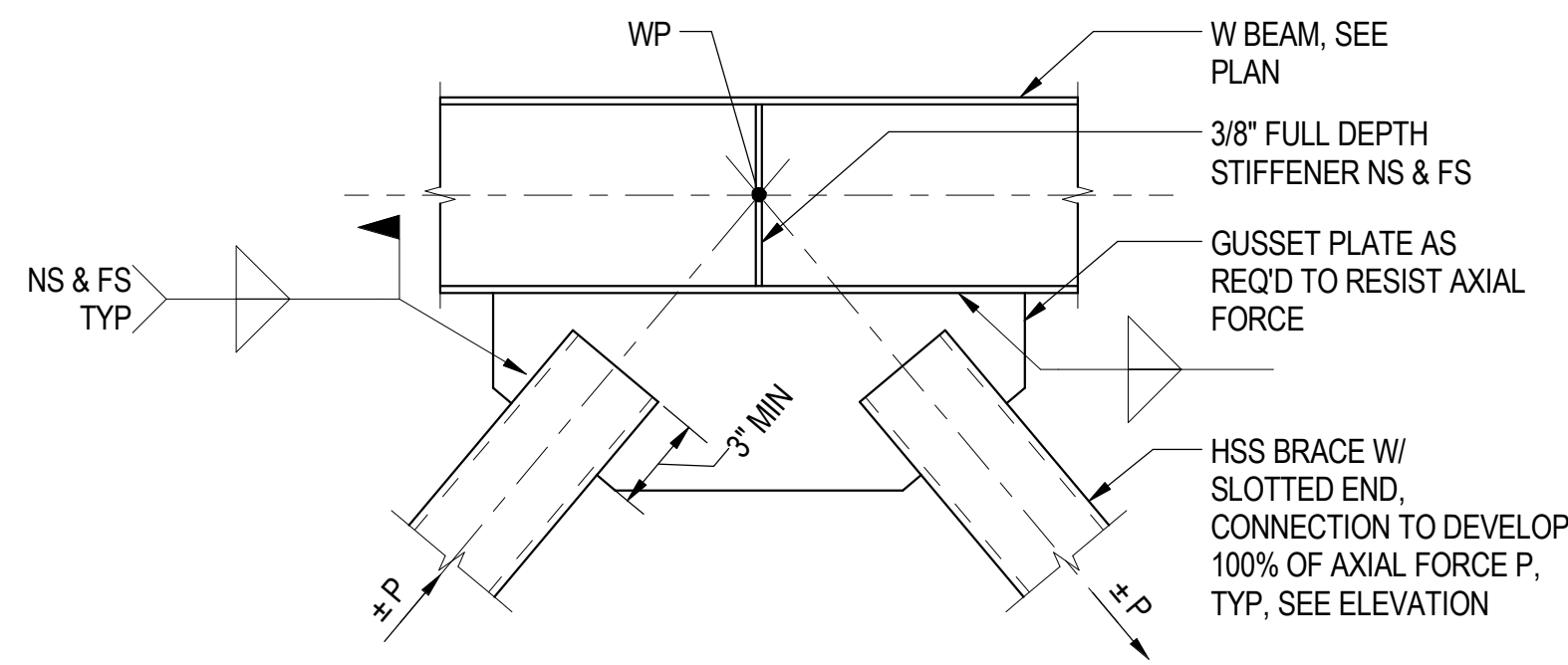
IN CHARGE	J. JAKALSKI
APPROVED BY	J. JAKALSKI
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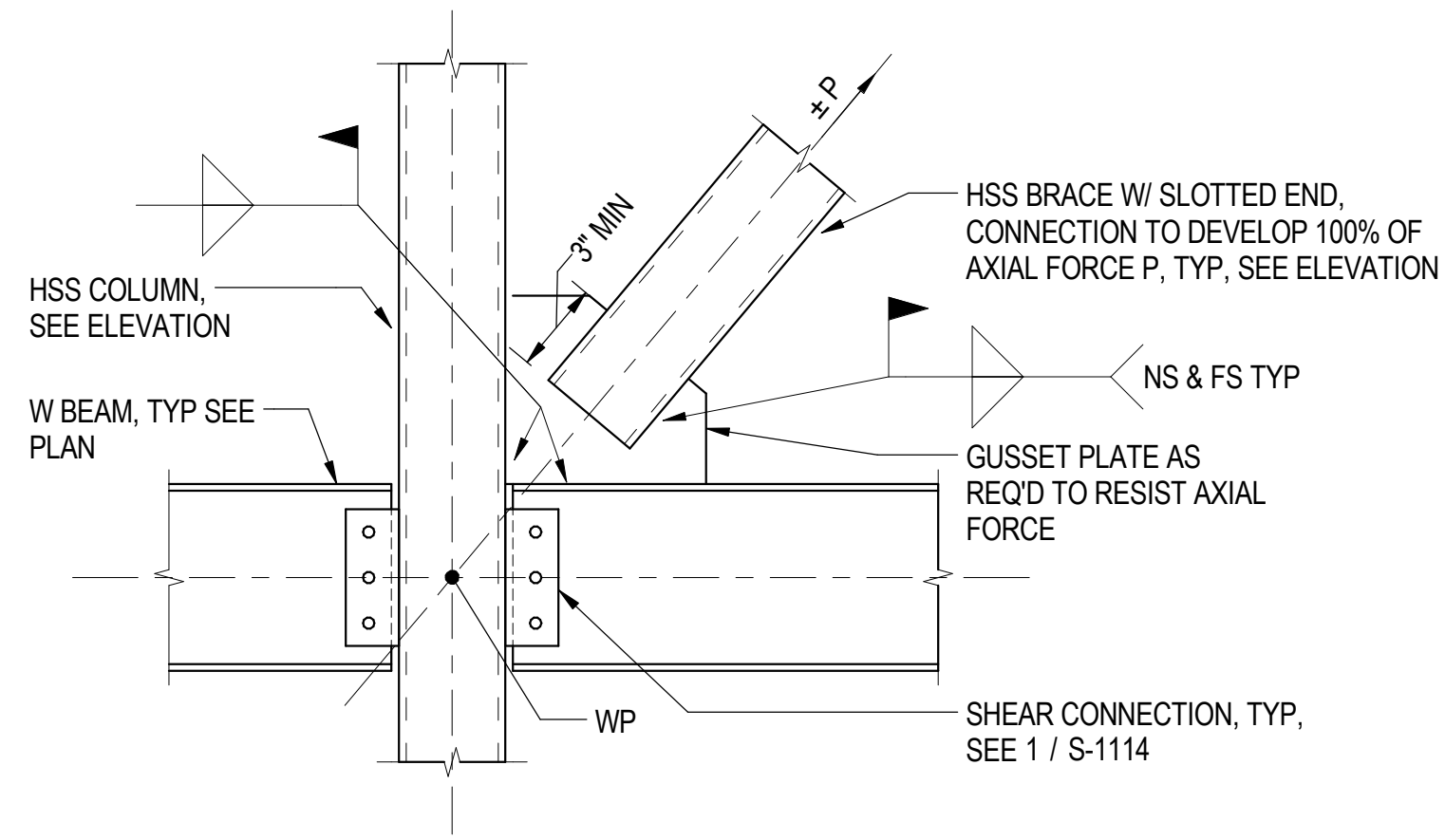
LOCATION IDENTIFIER: **DR095**

**SOUTH TERMINAL SECTIONS AND DETAILS**

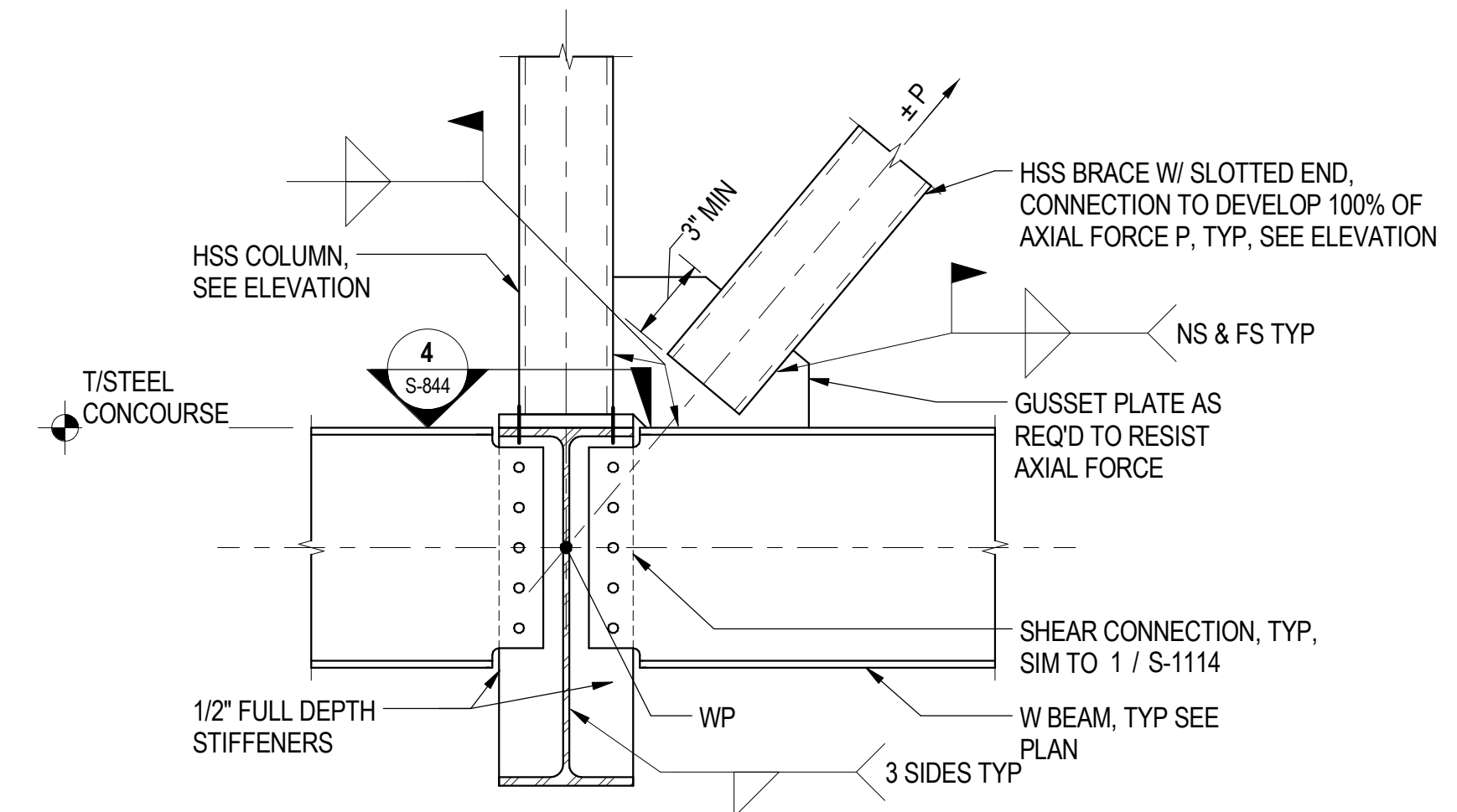
**S-843**



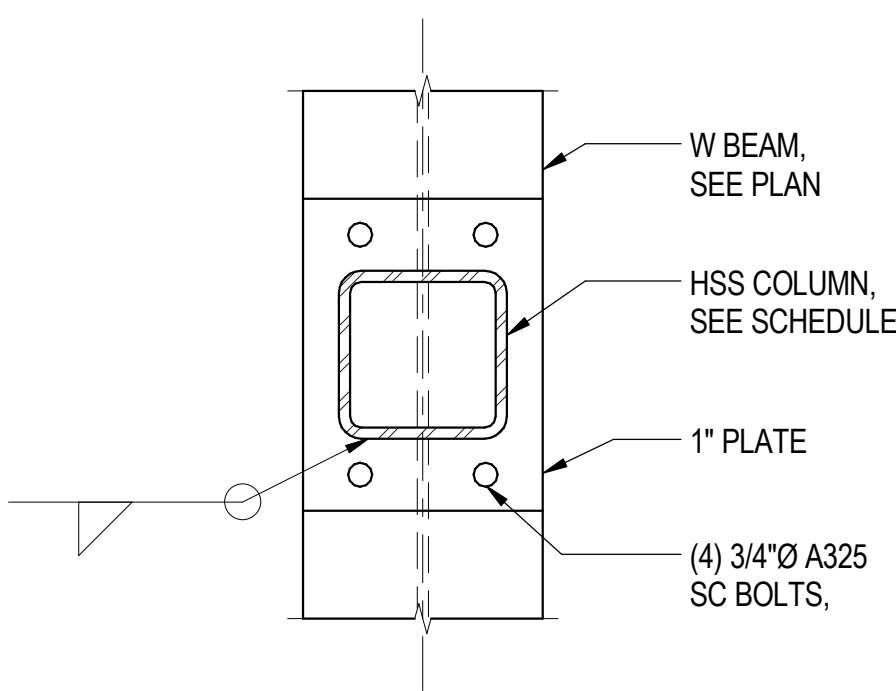
**1** BRACED FRAME CHEVRON CONNECTION  
S-844 SCALE: 1" = 1'-0"



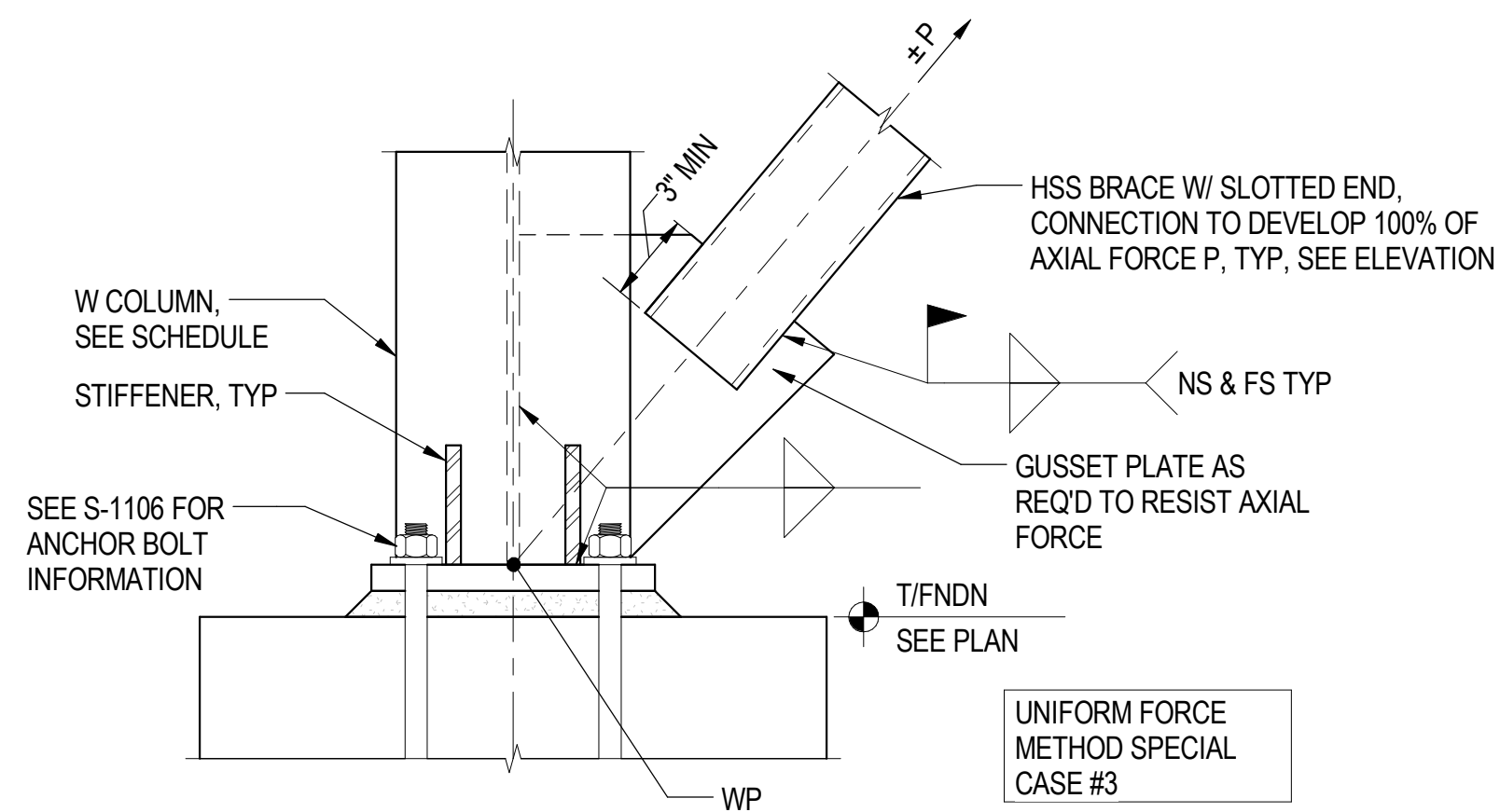
**2** TYPICAL BRACED FRAME CORNER CONNECTION  
S-844 SCALE: 1" = 1'-0"



**3** BRACED FRAME CORNER CONN. AT CONCOURSE  
S-844 SCALE: 1" = 1'-0"



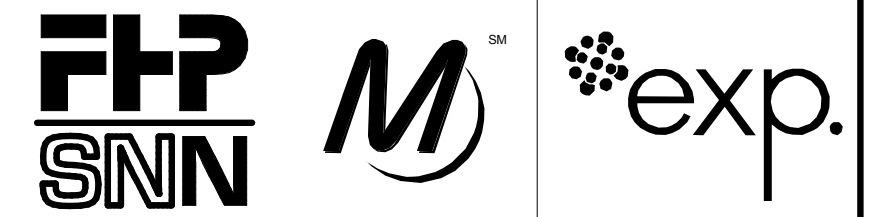
**4** HSS COLUMN BASE PLATE  
S-844 SCALE: 1 1/2" = 1'-0"



**5** TYP BRACED FRAME BASE CONNECTION ALONG GRIDS D' & E'  
S-844 SCALE: 1" = 1'-0"

**SHEET NOTES:**

- FOR GENERAL NOTES, SEE DWGS S-001 TO S-004.
- BRACE CONNECTIONS TO BE DESIGNED BY LICENSED SE IN THE STATE OF IL TO DEVELOP 100% OF INDICATED BRACE FORCES.
- ALL INDICATED BRACE FORCES ARE FACTORED AXIAL LOADS. FORCES TO BE CONSIDERED EITHER TENSION OR COMPRESSION.
- BRACED FRAME JOINTS SHALL BE DESIGNED USING THE AISC UNIFORM FORCE METHOD TO AVOID MOMENTS AT CONNECTION INTERFACES OR EQUIVALENT UNLESS NOTED OTHERWISE.



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	2/9/17	ISSUED FOR CONSTRUCTION

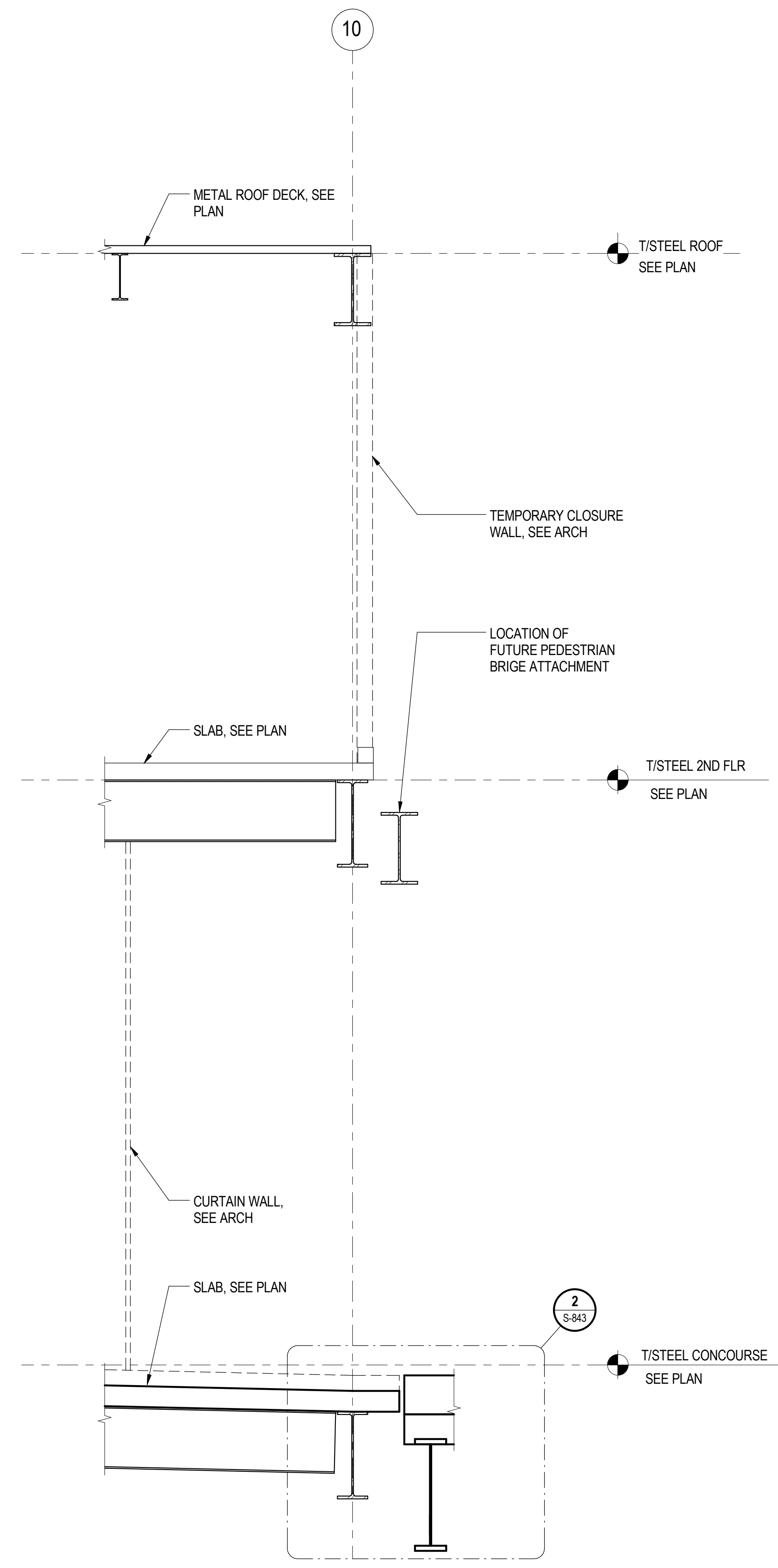
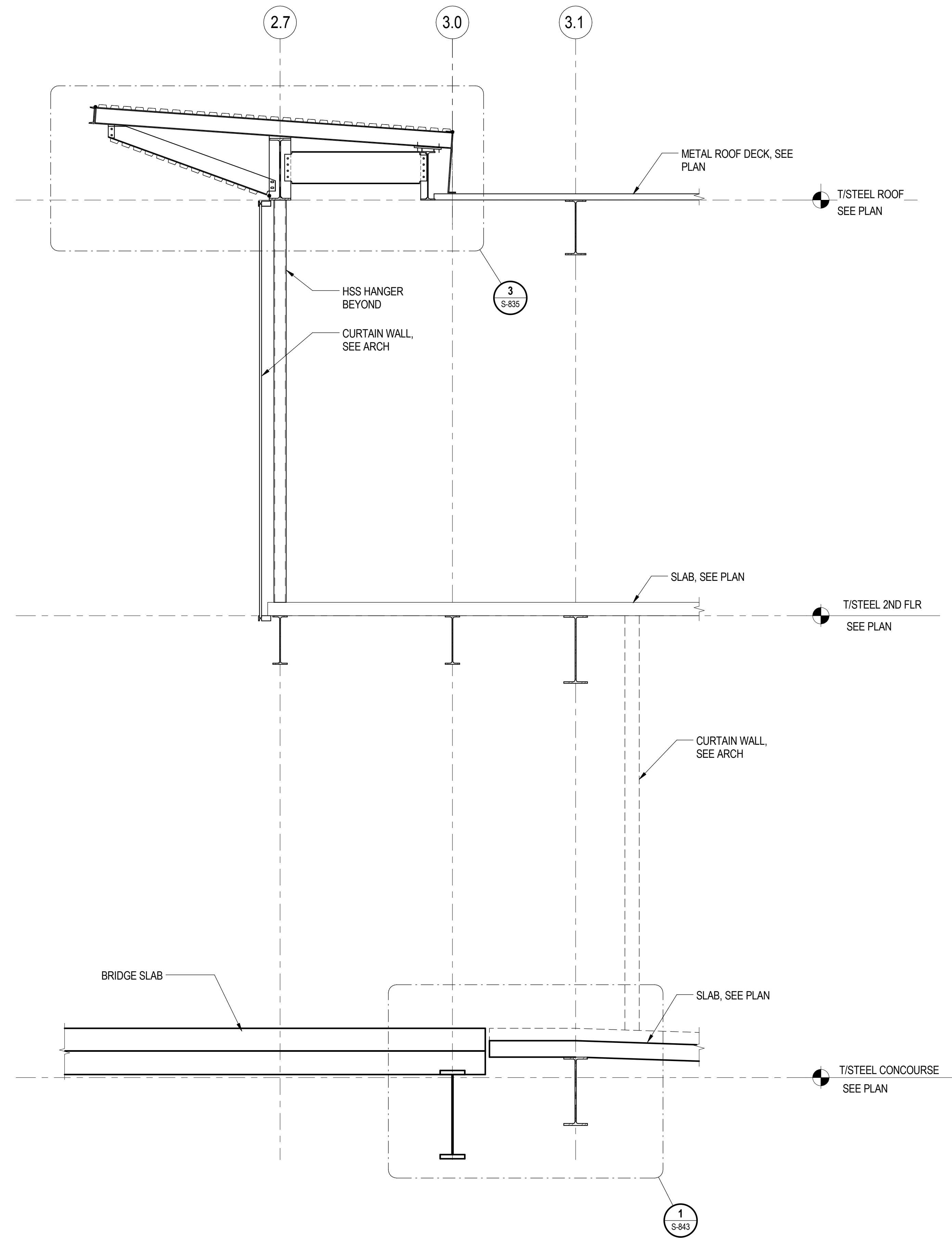
LOCATION IDENTIFIER: **DR095**

**SOUTH TERMINAL SECTIONS AND DETAILS**

**S-844**

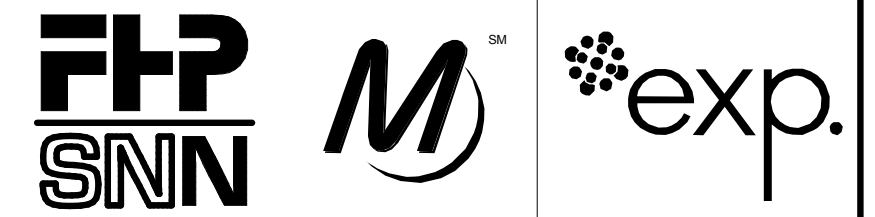
30/11/2016 7:06:41 PM

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**1** TYPICAL SOUTH EDGE SECTION  
S-845 SCALE: 3/8" = 1'-0"

**2** TYPICAL NORTH EDGE SECTION  
S-845 SCALE: 3/8" = 1'-0"



**SENSITIVE SECURITY INFORMATION**  
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**95th Terminal Improvement**  
 15 West 95th Street (South Terminal)  
 Chicago, IL 60628

IN CHARGE	J. JAKALSKI
APPROVED BY	J. JAKALSKI
CHECKED BY	R.S. NAIR
DESIGNED BY	G. FORKAN
DRAWN BY	J. PEARSON
PROJECT NO.	2012-0021.02
FILE NAME	DR095_S-Terminal

MARK	DATE	DESCRIPTION
	2/9/17	ISSUED FOR CONSTRUCTION

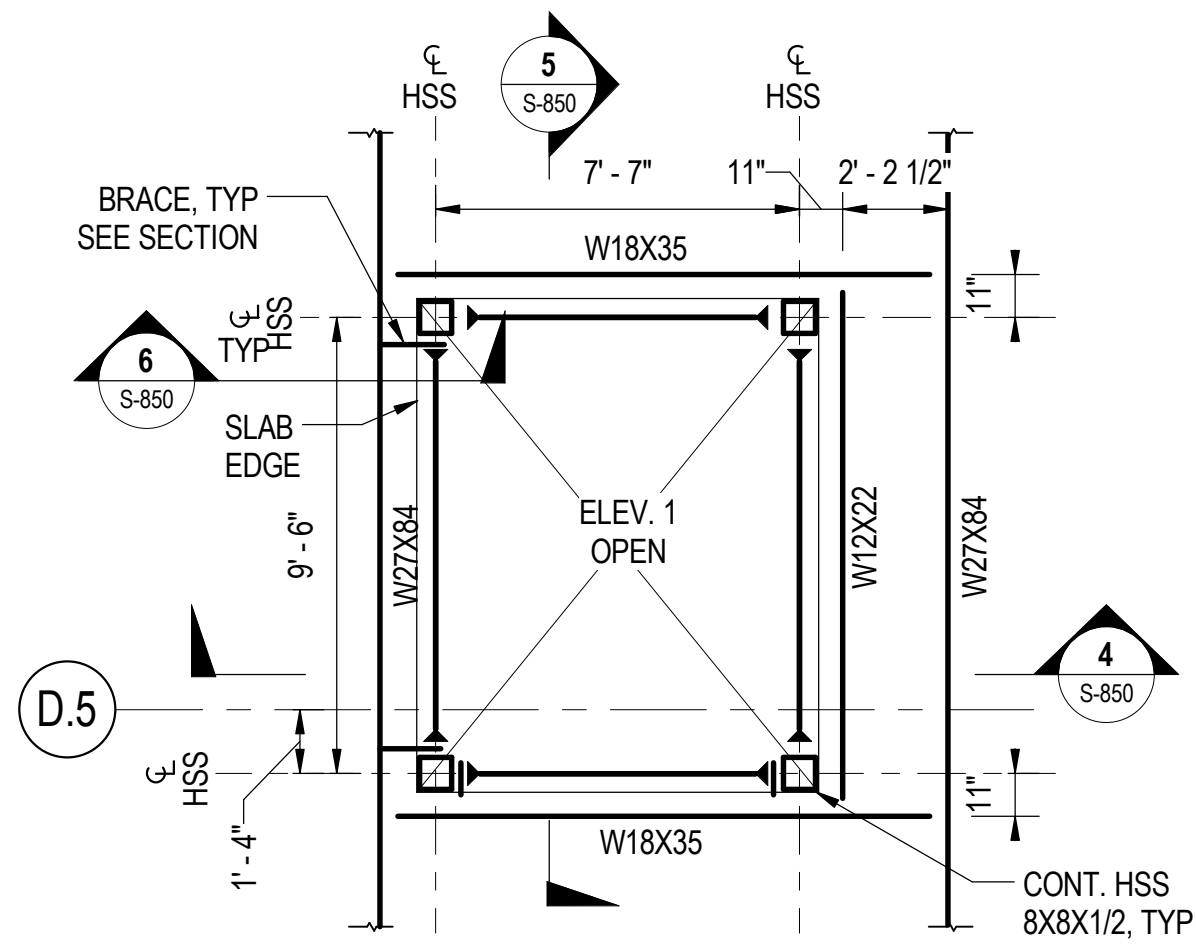
LOCATION IDENTIFIER: **DR095**

**SOUTH TERMINAL SECTIONS AND DETAILS**

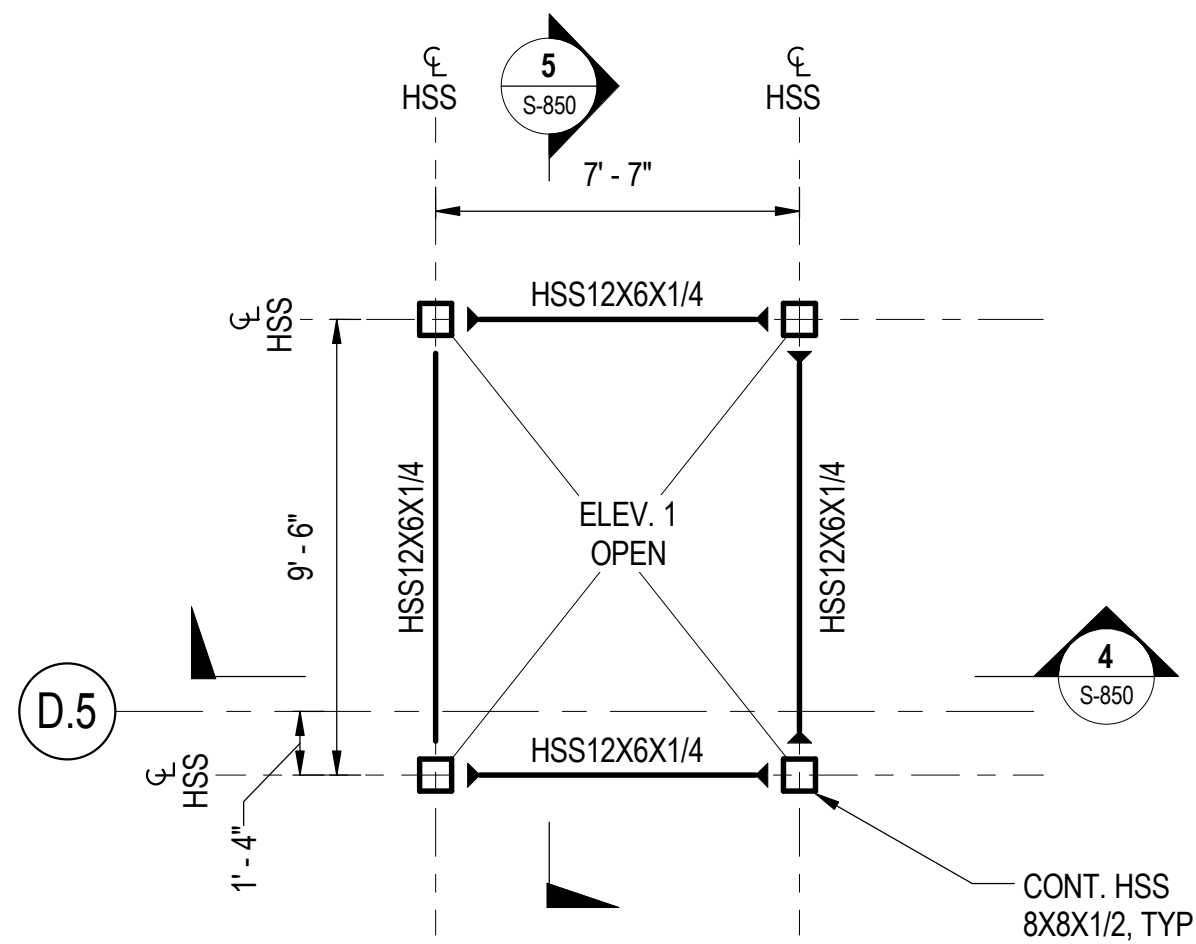
**S-845**

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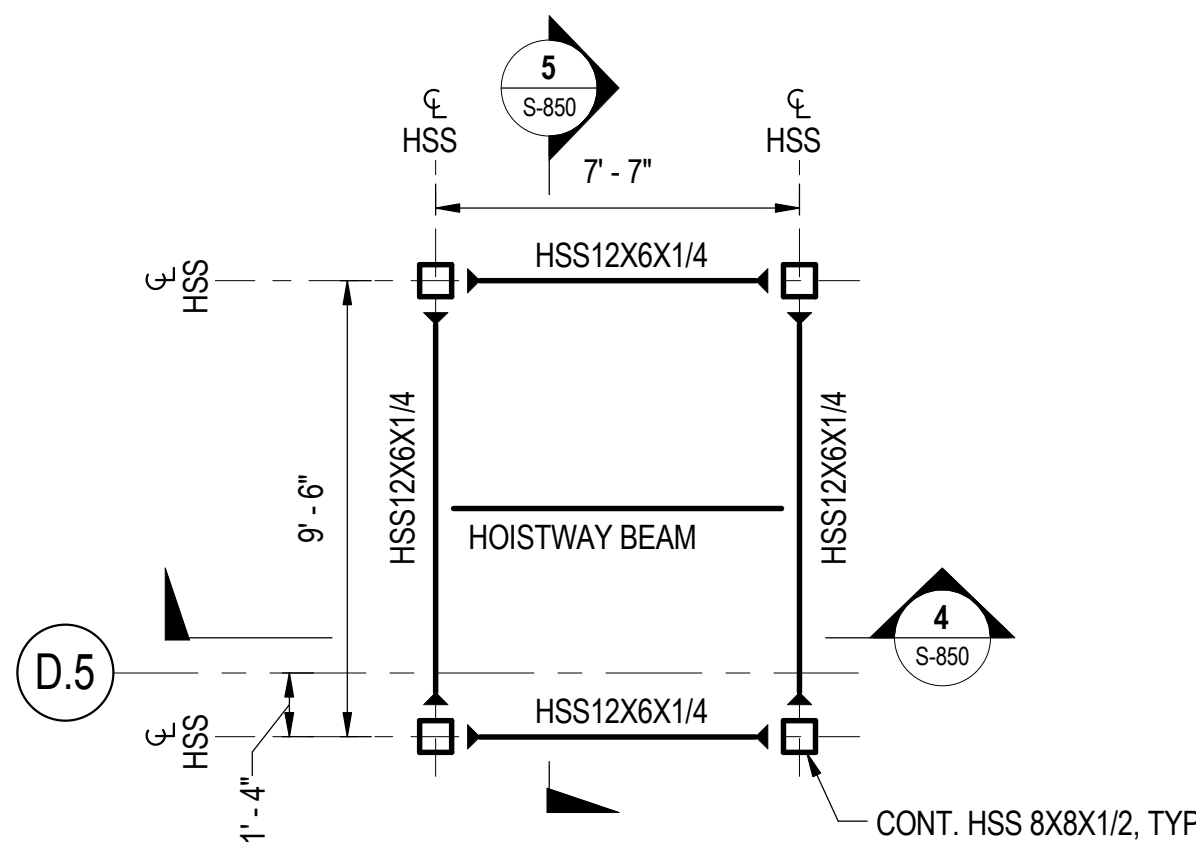
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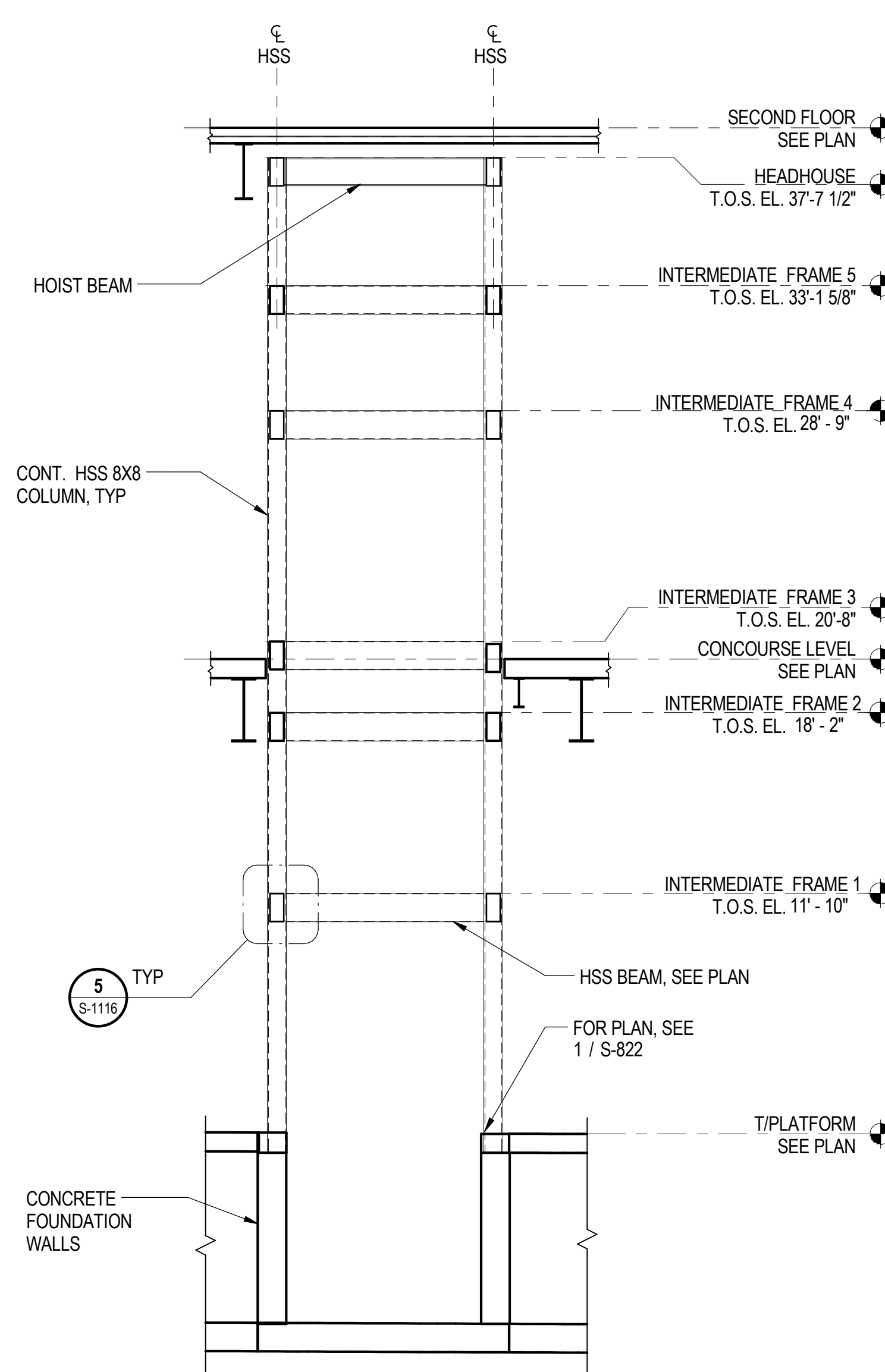
**1 ELEVATOR 1 CONCOURSE PLAN**  
S-850 SCALE: 1/4" = 1'-0"



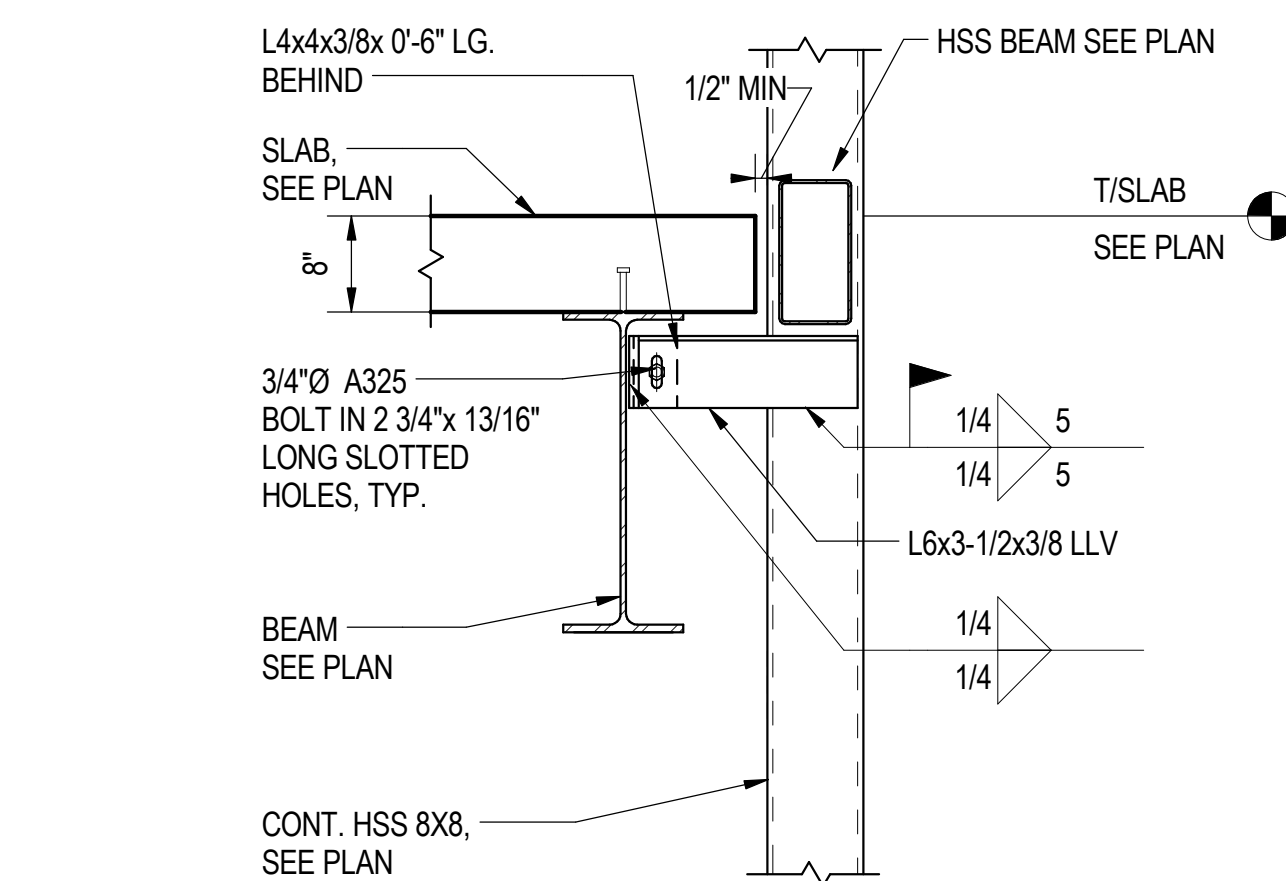
**2 ELEVATOR 1 TYPICAL INTERMEDIATE PLAN**  
S-850 SCALE: 1/4" = 1'-0"



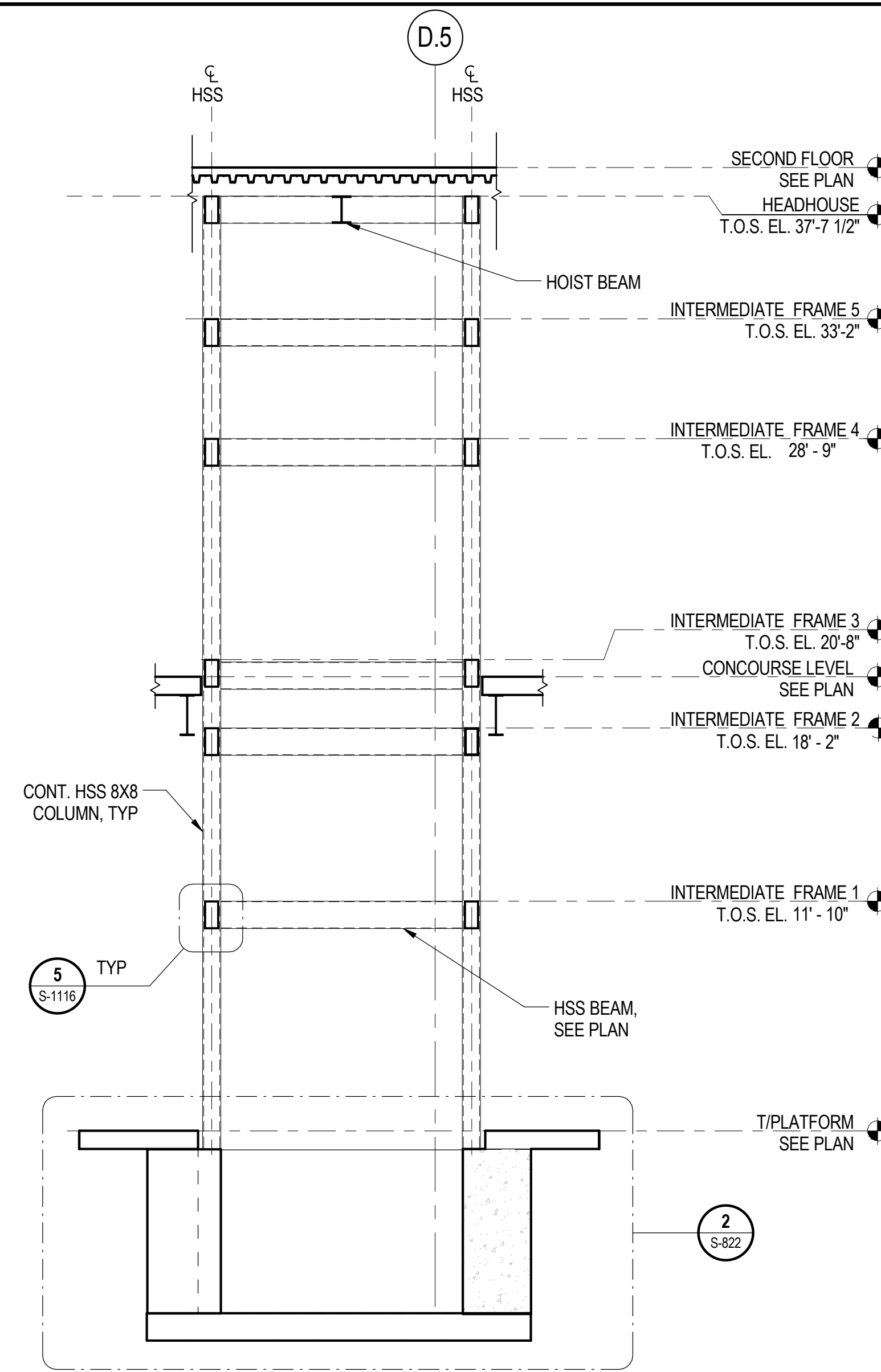
**3 ELEVATOR 1 HEADHOUSE PLAN**  
S-850 SCALE: 1/4" = 1'-0"



**4 ELEVATOR 1 SECTION - NS**  
S-850 SCALE: 1/4" = 1'-0"



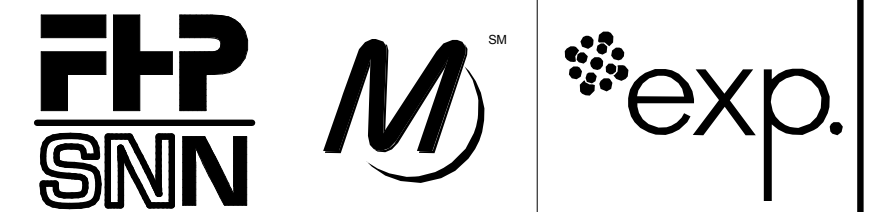
**6 BRACING DETAIL**  
S-850 SCALE: 3/4" = 1'-0"



**5 ELEVATOR 1 SECTION - EW**  
S-850 SCALE: 1/4" = 1'-0"

**SHEET NOTES:**

- FOR GENERAL NOTES, SEE DWGS S-001 TO S-004.
- FOR TYPICAL DETAILS REFER TO S-1112 TO S-1120.
- HOISTWAY BEAM AND THE CONNECTIONS TO STRUCTURAL STEEL SHALL BE DESIGNED TO RESIST A CONCENTRATED LOAD OF 7,500 LBS LOCATED AT MID-SPAN IN ADDITION TO DEAD LOAD. NUMBER OF HOIST BEAMS, THEIR LOCATION AND ELEVATION SHALL BE COORDINATED WITH ELEVATOR MANUFACTURER REQUIREMENTS.
- CONTRACTOR TO VERIFY ALL DIMENSIONS SHOWN BASED ON ELEVATOR MANUFACTURER REQUIREMENTS PRIOR TO PROCEEDING WITH FABRICATION OF STRUCTURAL STEEL FRAMING.
- SEE S-809 FOR TYPICAL ROOF CONSTRUCTION.



**SENSITIVE SECURITY INFORMATION**  
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**95th Terminal Improvement**  
15 West 95th Street (South Terminal)  
Chicago, IL 60628

IN CHARGE	J. JAKALSKI
APPROVED BY	J. JAKALSKI
CHECKED BY	R.S. NAIR
DESIGNED BY	G. FORKAN
DRAWN BY	D. LALLY
PROJECT NO.	2012-0021.02
FILE NAME	DR095_S-Terminal

MARK	DATE	DESCRIPTION
	2/9/17	ISSUED FOR CONSTRUCTION

LOCATION IDENTIFIER: **DR095**

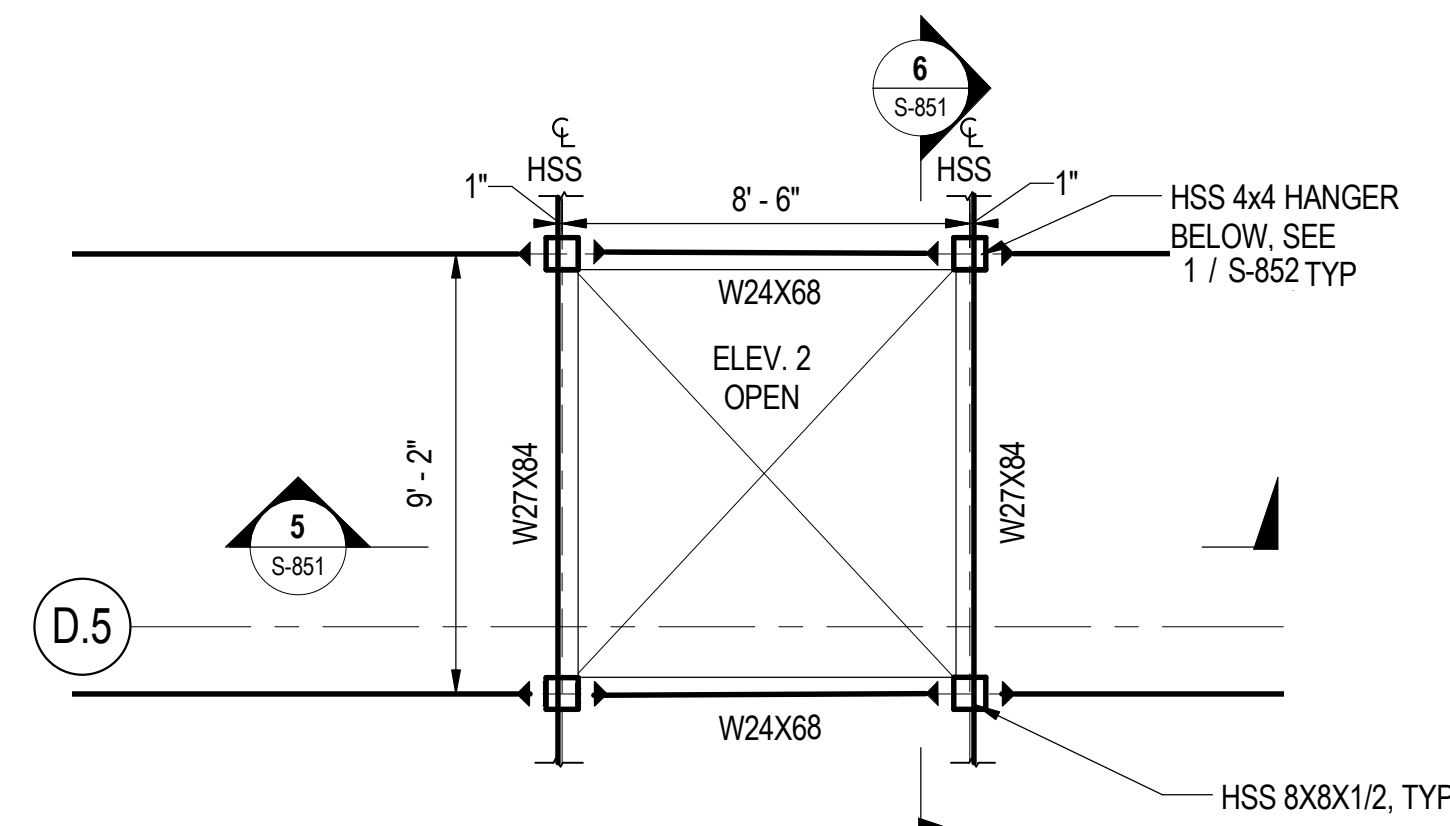
**SOUTH TERMINAL ELEVATOR #1  
FRAME ELEVATIONS**

**S-850**

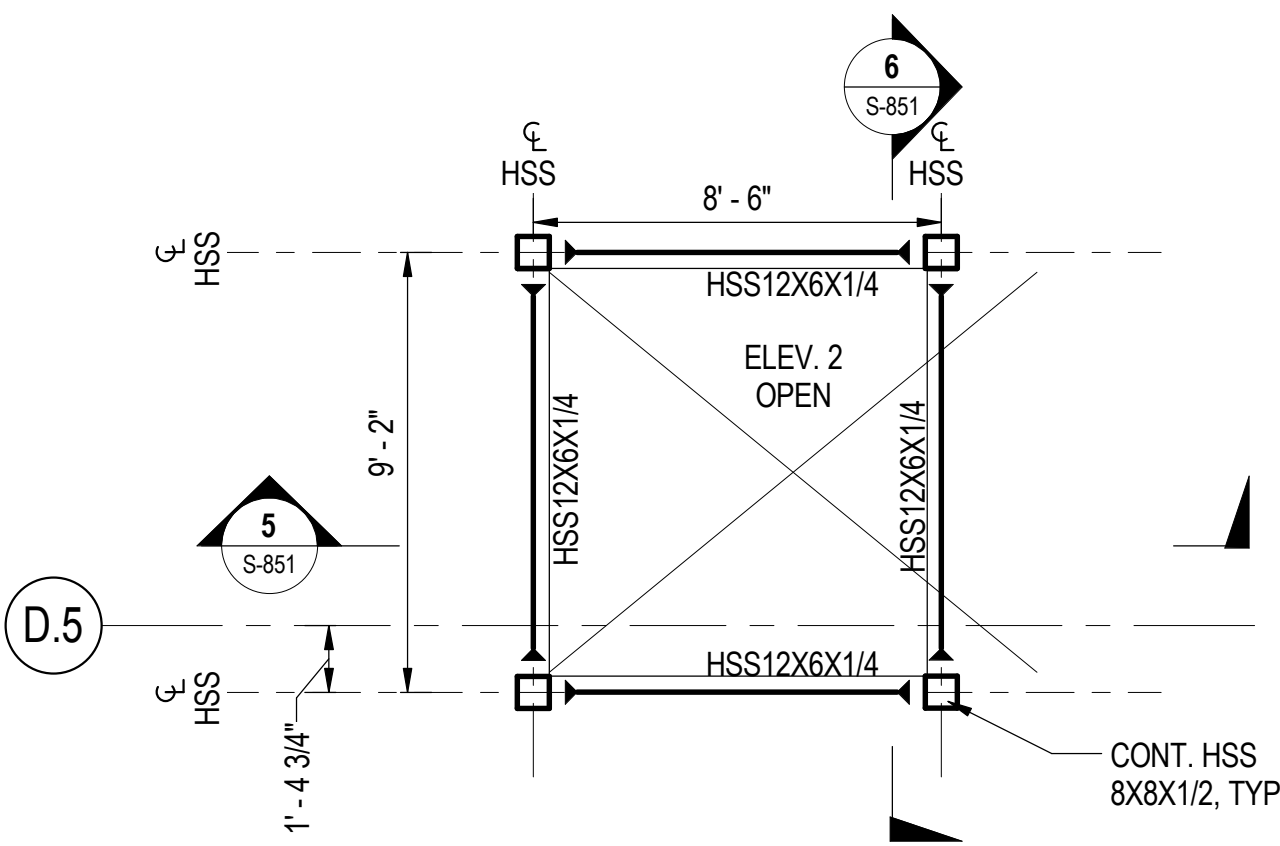


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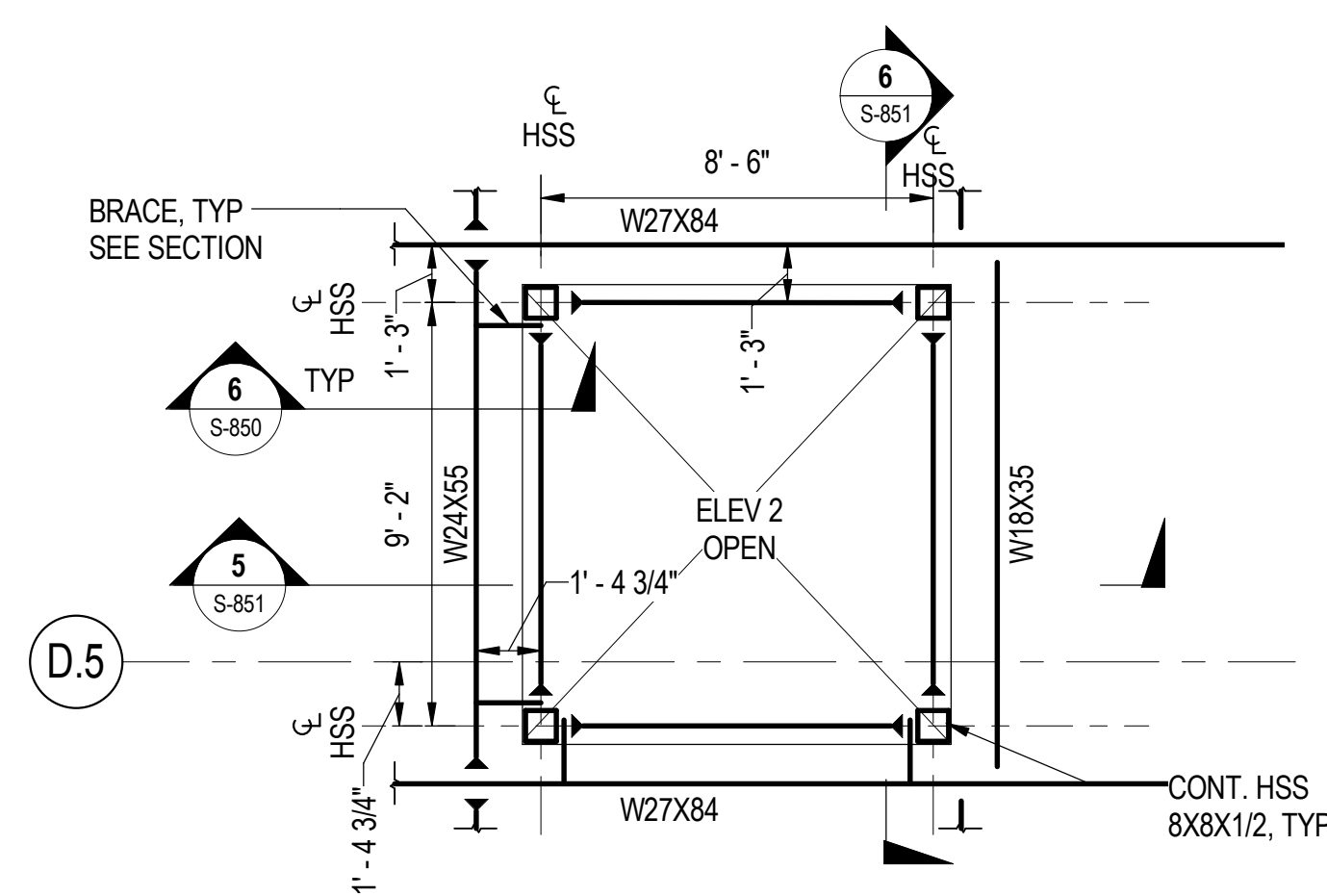
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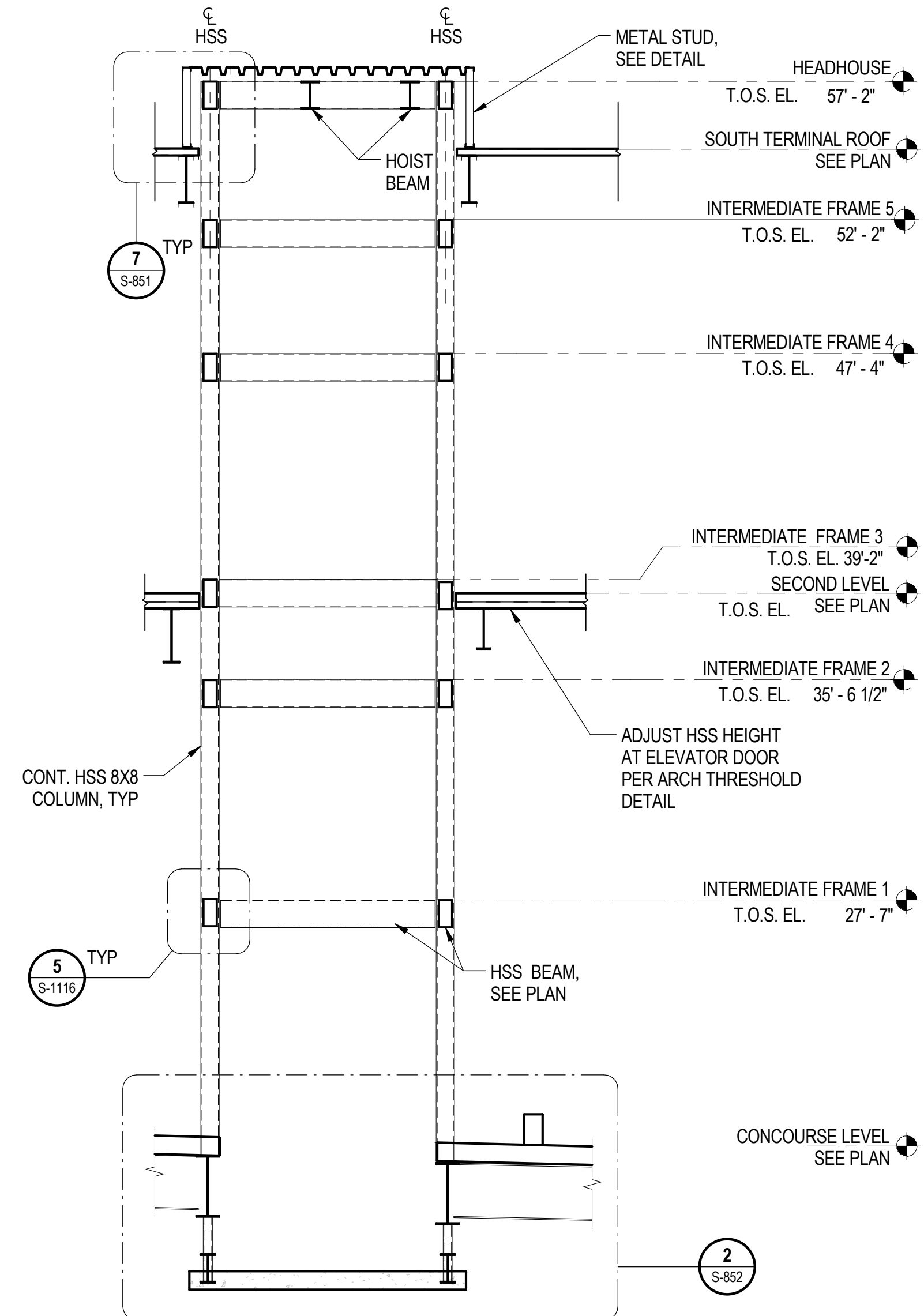
**1 ELEVATOR 2 CONCOURSE PLAN**  
S-851 SCALE: 1/4" = 1'-0"



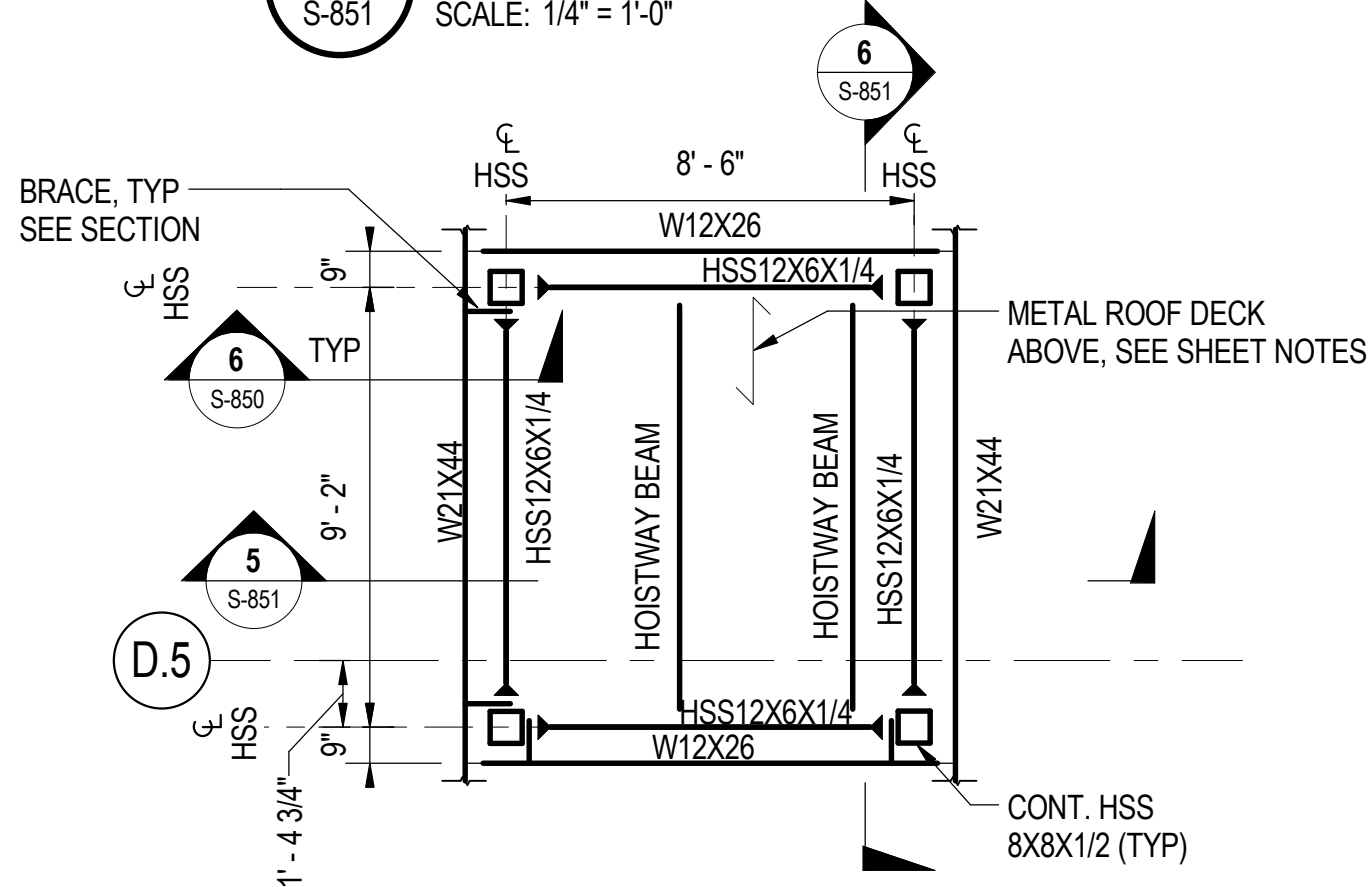
**2 ELEVATOR 2 TYPICAL INTERMEDIATE PLAN**  
S-851 SCALE: 1/4" = 1'-0"



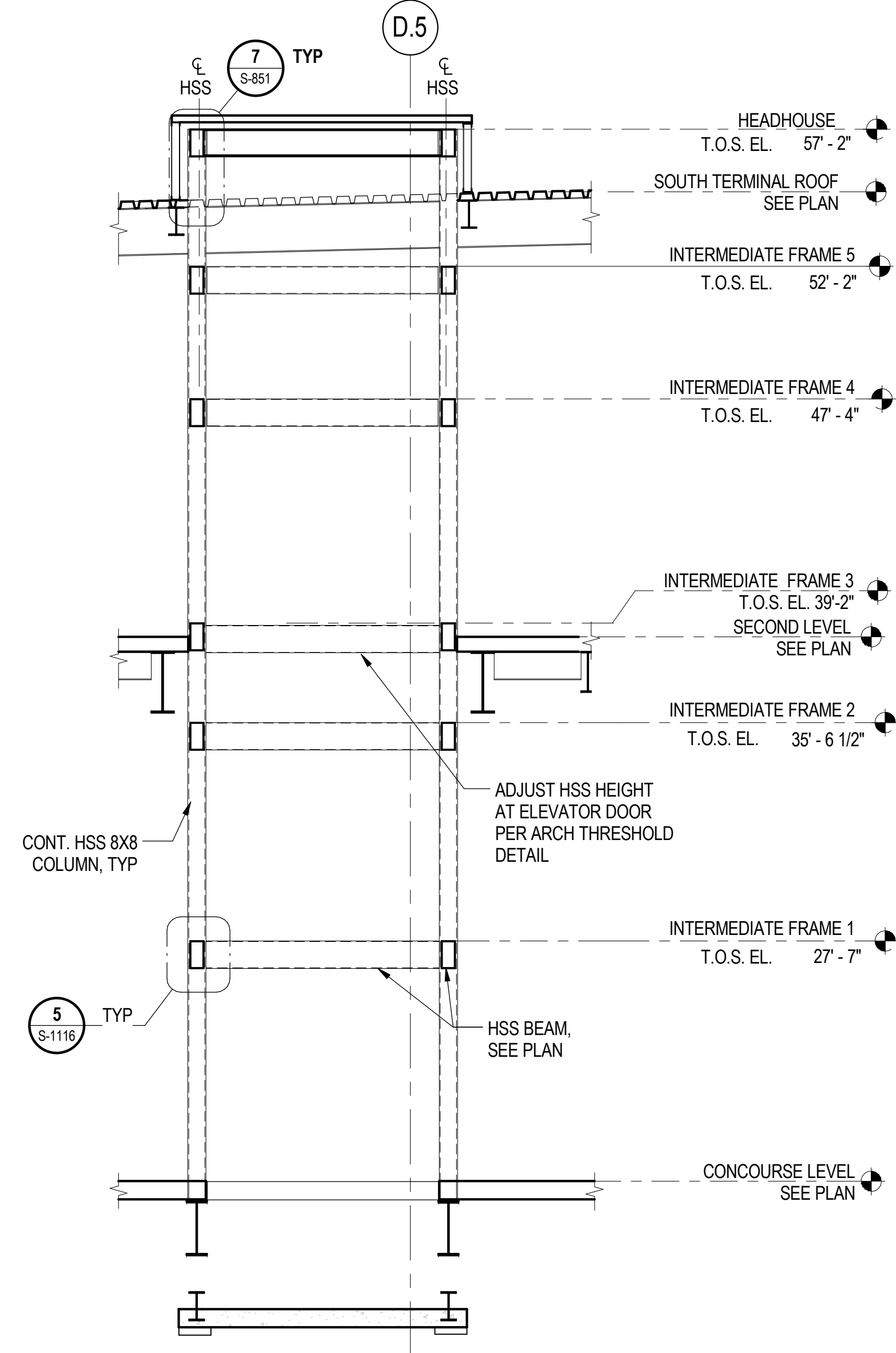
**3 ELEVATOR 2 SECOND FLOOR PLAN**  
S-851 SCALE: 1/4" = 1'-0"



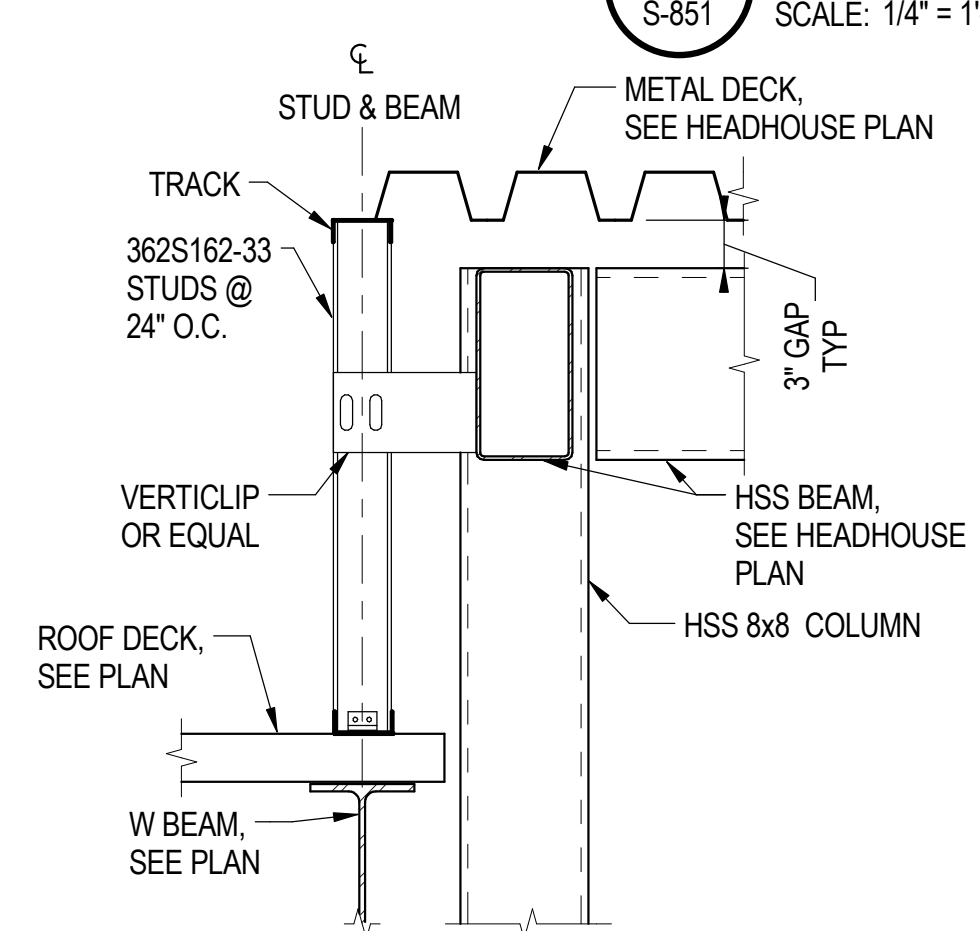
**5 ELEVATOR 2 SECTION - NS**  
S-851 SCALE: 1/4" = 1'-0"



**4 ELEVATOR 2 HEADHOUSE PLAN**  
S-851 SCALE: 1/4" = 1'-0"



**6 ELEVATOR 2 SECTION - EW**  
S-851 SCALE: 1/4" = 1'-0"



**7 ELEVATOR 2 POP-OUT ROOF**  
S-851 SCALE: 1" = 1'-0"

- SHEET NOTES:**
- FOR GENERAL NOTES, SEE DWGS S-001 TO S-004.
  - FOR TYPICAL DETAILS REFER TO S-1112 TO S-1120.
  - HOISTWAY BEAM AND THE CONNECTIONS TO STRUCTURAL STEEL SHALL BE DESIGNED TO RESIST A CONCENTRATED LOAD OF 7,500 LBS LOCATED AT MID-SPAN IN ADDITION TO DEAD LOAD. NUMBER OF HOIST BEAMS, THEIR LOCATION AND ELEVATION SHALL BE COORDINATED WITH ELEVATOR MANUFACTURER REQUIREMENTS.
  - CONTRACTOR TO VERIFY ALL DIMENSIONS SHOWN BASED ON ELEVATOR MANUFACTURER REQUIREMENTS PRIOR TO PROCEEDING WITH FABRICATION OF STRUCTURAL STEEL FRAMING.
  - SEE S-809 FOR TYPICAL ROOF CONSTRUCTION.



**CHICAGO TRANSIT AUTHORITY ENGINEERING**

**SENSITIVE SECURITY INFORMATION**  
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**95th Terminal Improvement**  
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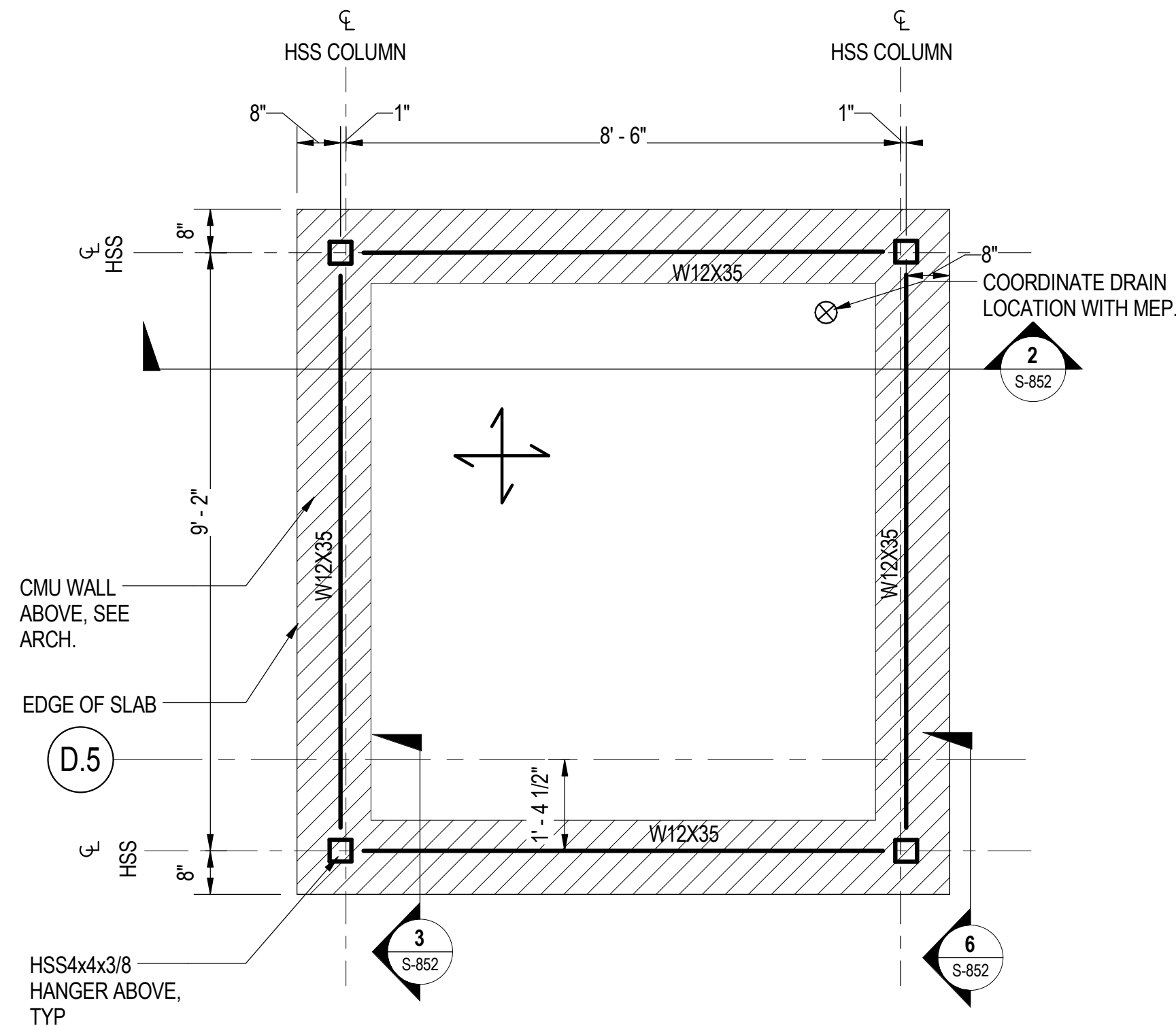
IN CHARGE	J. JAKALSKI
APPROVED BY	J. JAKALSKI
CHECKED BY	R.S. NAIR
DESIGNED BY	G. FORKAN
DRAWN BY	D. LALLY
PROJECT NO.	2012-0021.02
FILE NAME	DR095_S-Terminal

MARK	DATE	DESCRIPTION
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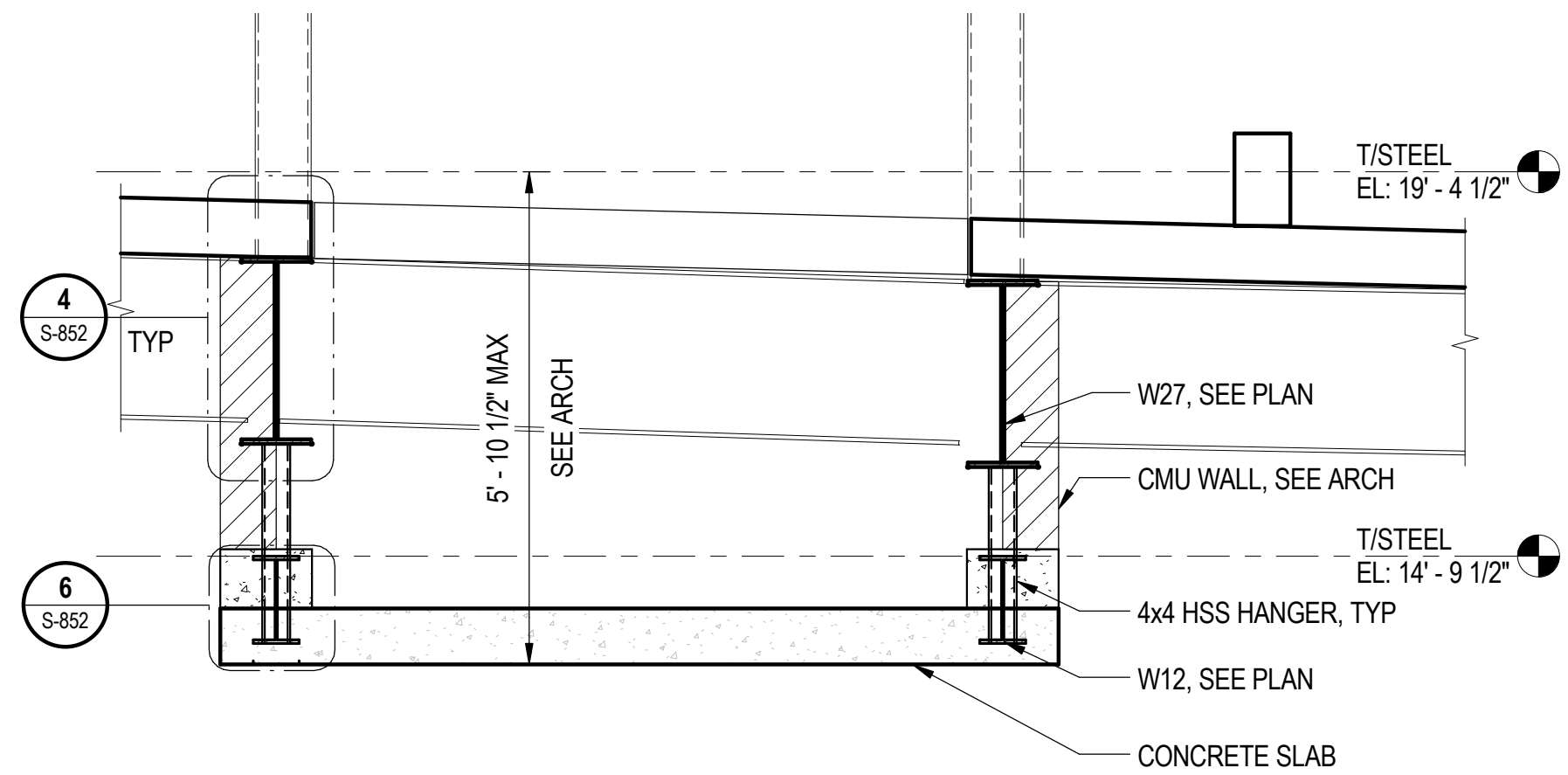
LOCATION IDENTIFIER: **DR095**

**SOUTH TERMINAL ELEVATOR #2 FRAME ELEVATIONS**

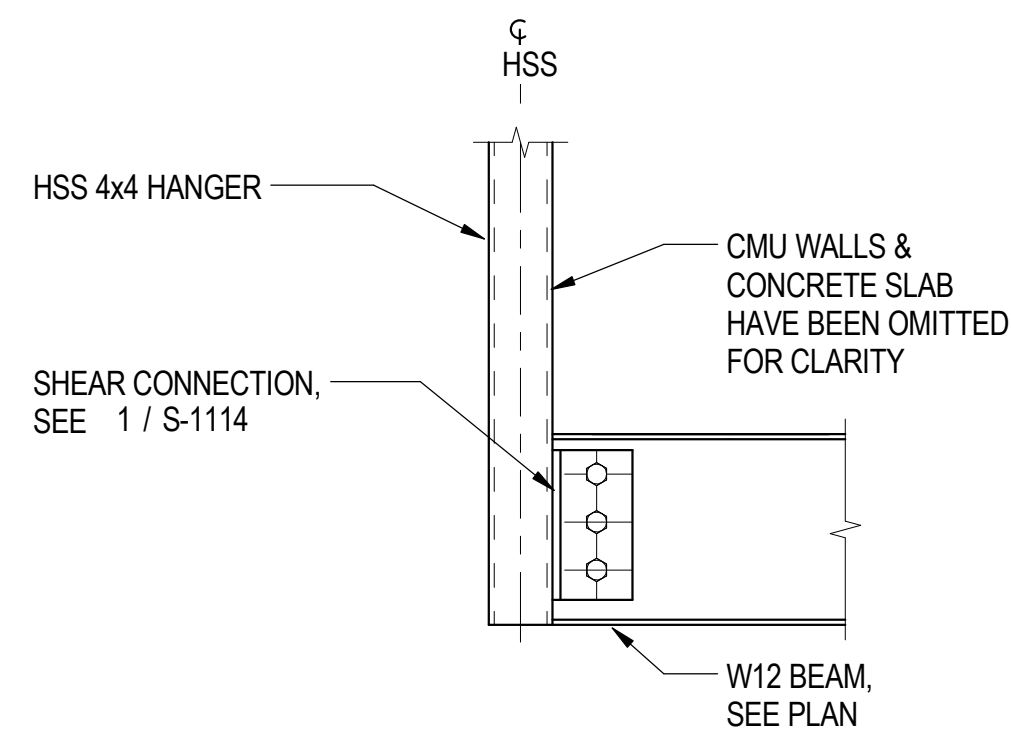
**S-851**



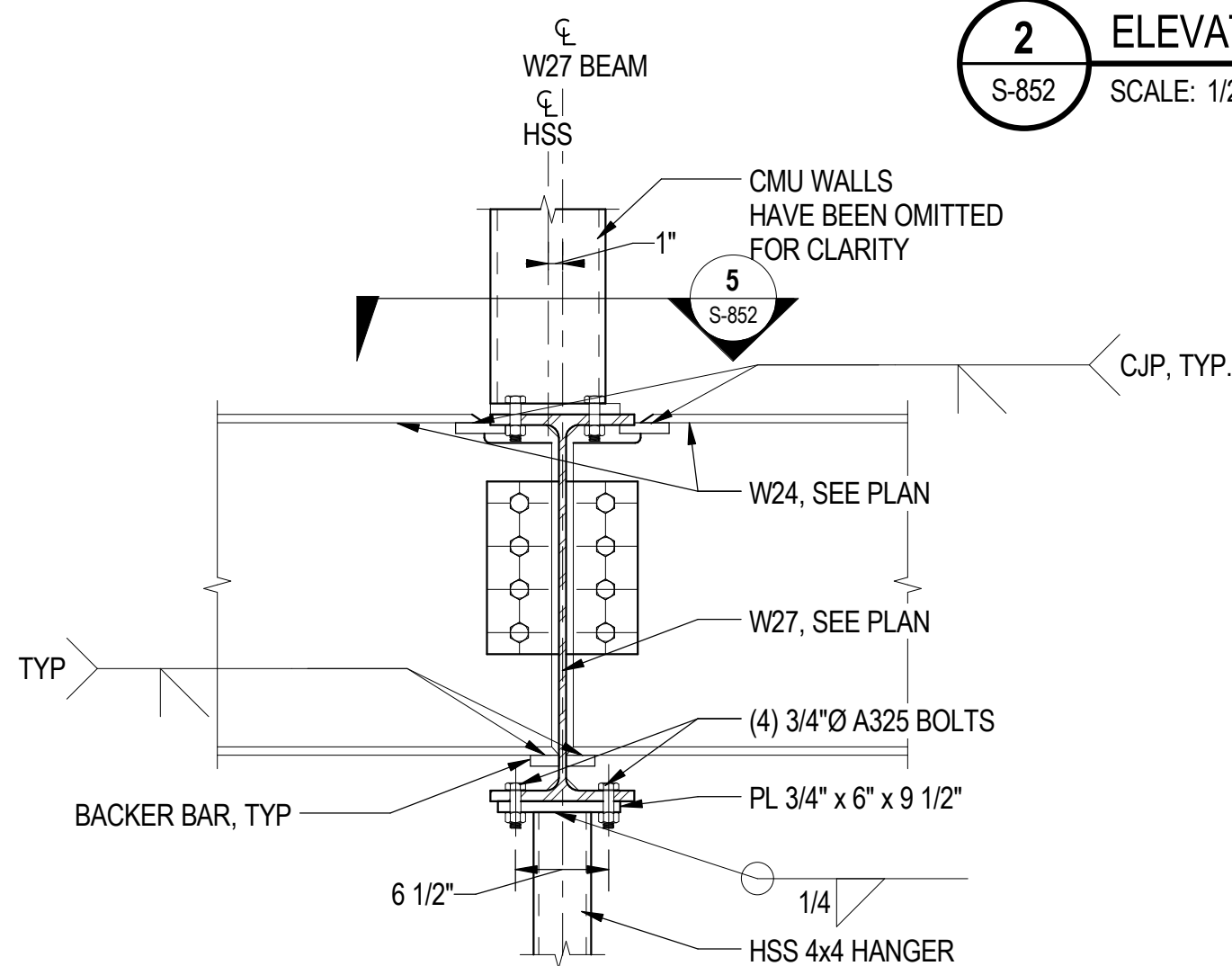
**1** ELEVATOR 2 HUNG PIT PLAN  
S-852 SCALE: 1/2" = 1'-0"



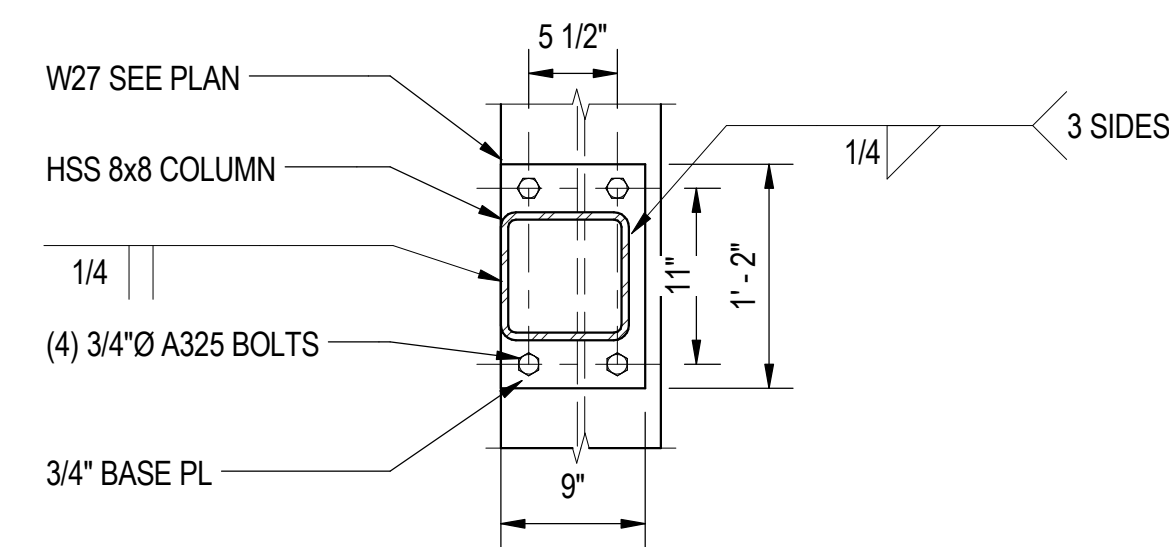
**2** ELEVATOR 2 HUNG PIT SECTION  
S-852 SCALE: 1/2" = 1'-0"



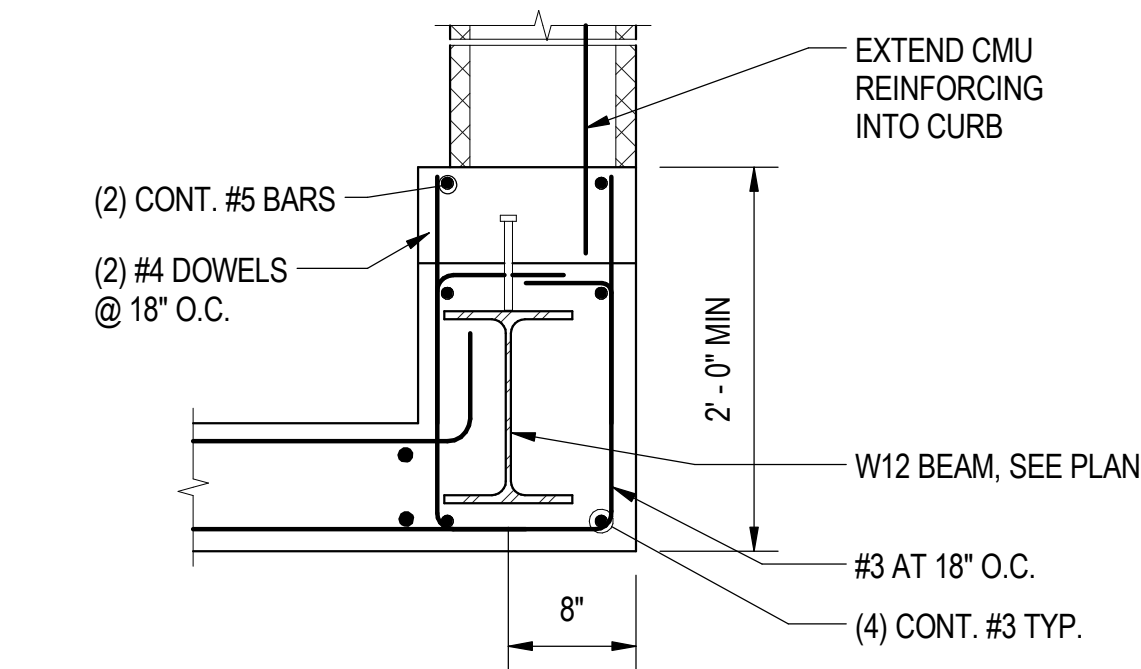
**3** ELEVATOR PIT BASE CONNECTION  
S-852 SCALE: 1" = 1'-0"



**4** HANGER CONNECTION  
S-852 SCALE: 1" = 1'-0"



**5** COLUMN BASE PLATE  
S-852 SCALE: 1" = 1'-0"



**6** SLAB EDGE DETAIL  
S-852 SCALE: 1" = 1'-0"

**SHEET NOTES:**

- FOR GENERAL NOTES, SEE DWGS S-001 TO S-004.
- FOR TYPICAL DETAILS REFER TO S-1112 TO S-1120.
- CONTRACTOR TO COORDINATE PIT DIMENSIONS WITH FINAL ELEVATOR AND ESCALATOR REQUIREMENTS.
- TYPICAL FLOOR CONSTRUCTION: 8" NORMAL WEIGHT CONCRETE FORMED SLAB W/ #6 BOTTOM BARS EW @ 6" O.C., #4 TOP BARS EW @ 12" O.C.
- ALL PIT LEVEL STEEL FRAMING TO BE GALVANIZED.



**cta** CHICAGO TRANSIT AUTHORITY ENGINEERING

**SENSITIVE SECURITY INFORMATION**  
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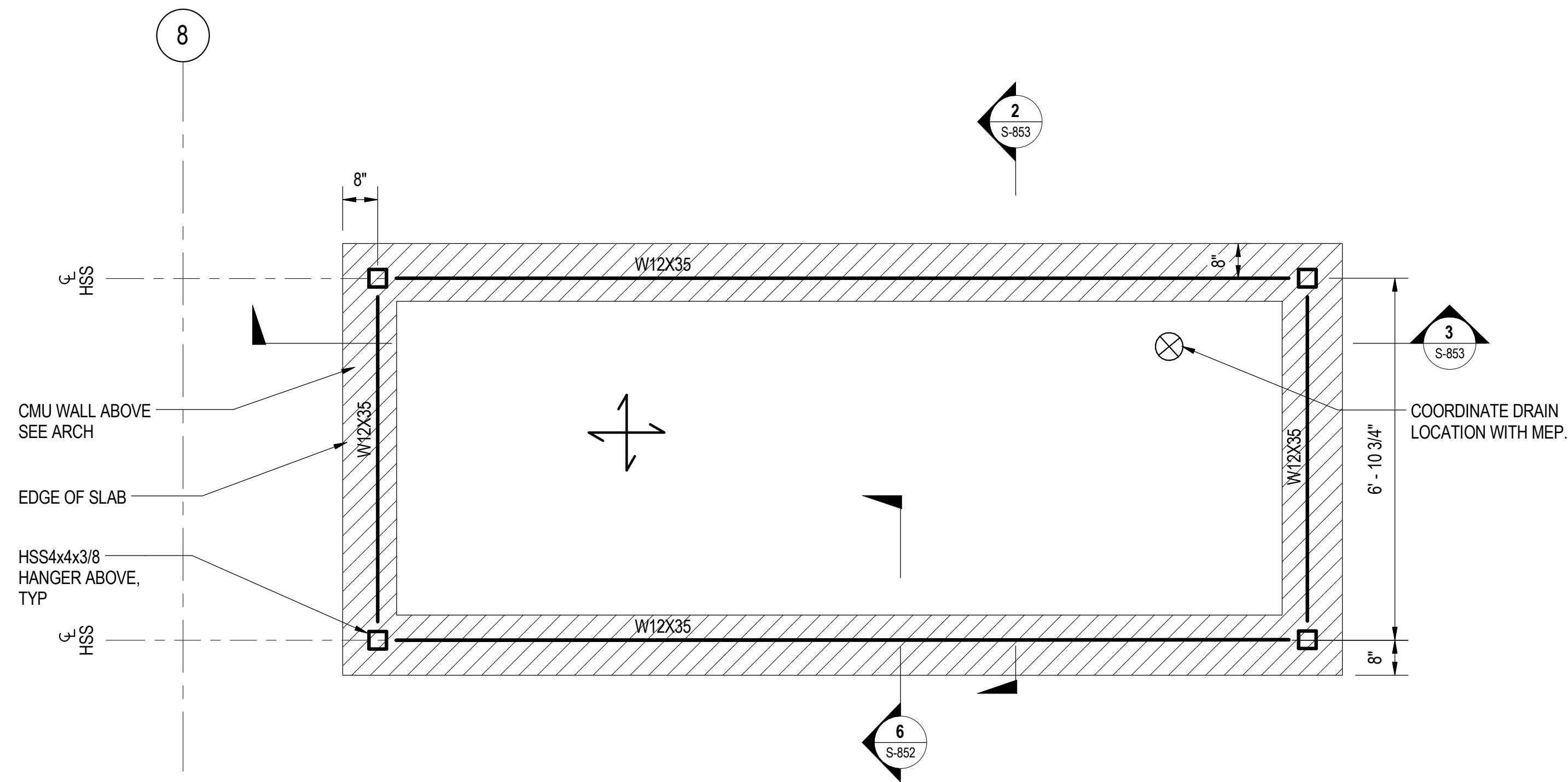
LOCATION IDENTIFIER: **DR095**

**SOUTH TERMINAL ELEVATOR #2 PIT DETAILS**

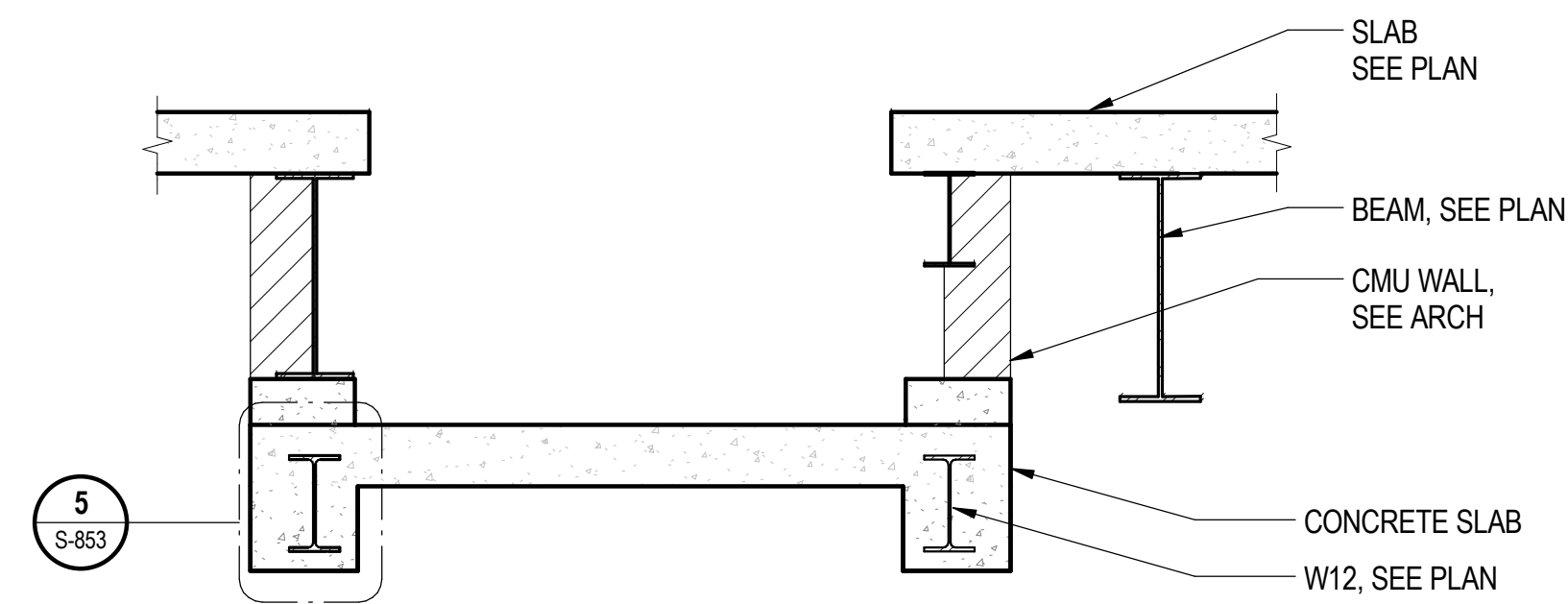
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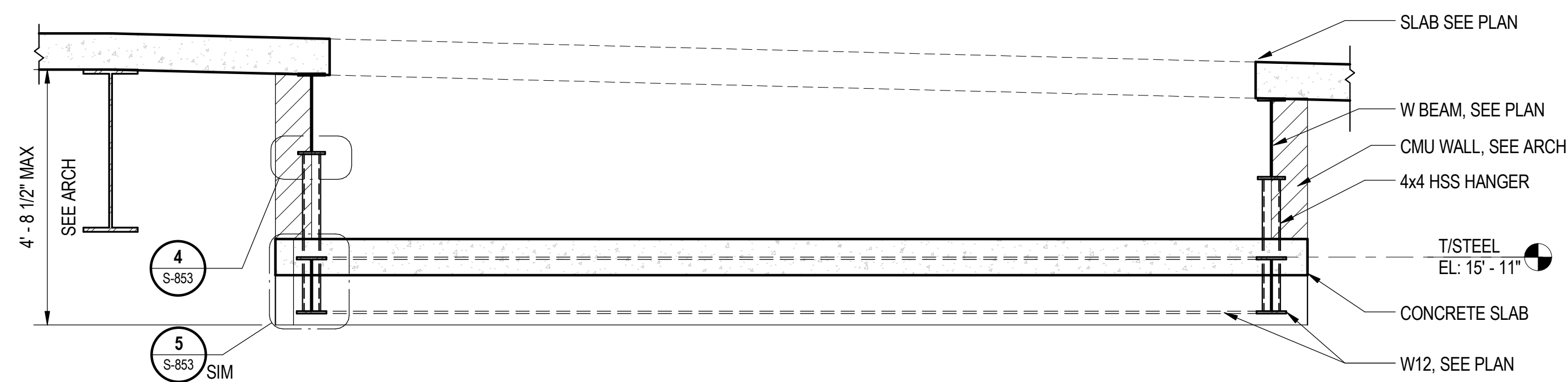
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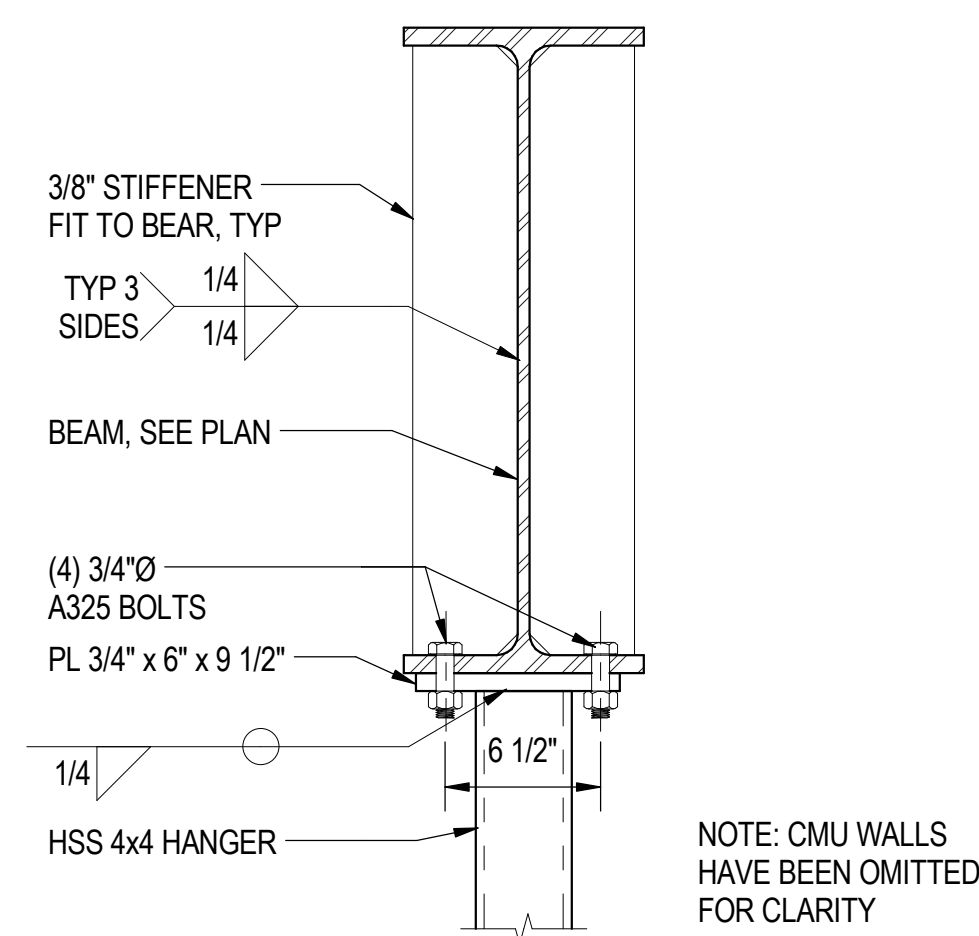
**1** TYPICAL ESCALATOR HUNG PIT PLAN  
S-853 SCALE: 1/2" = 1'-0"



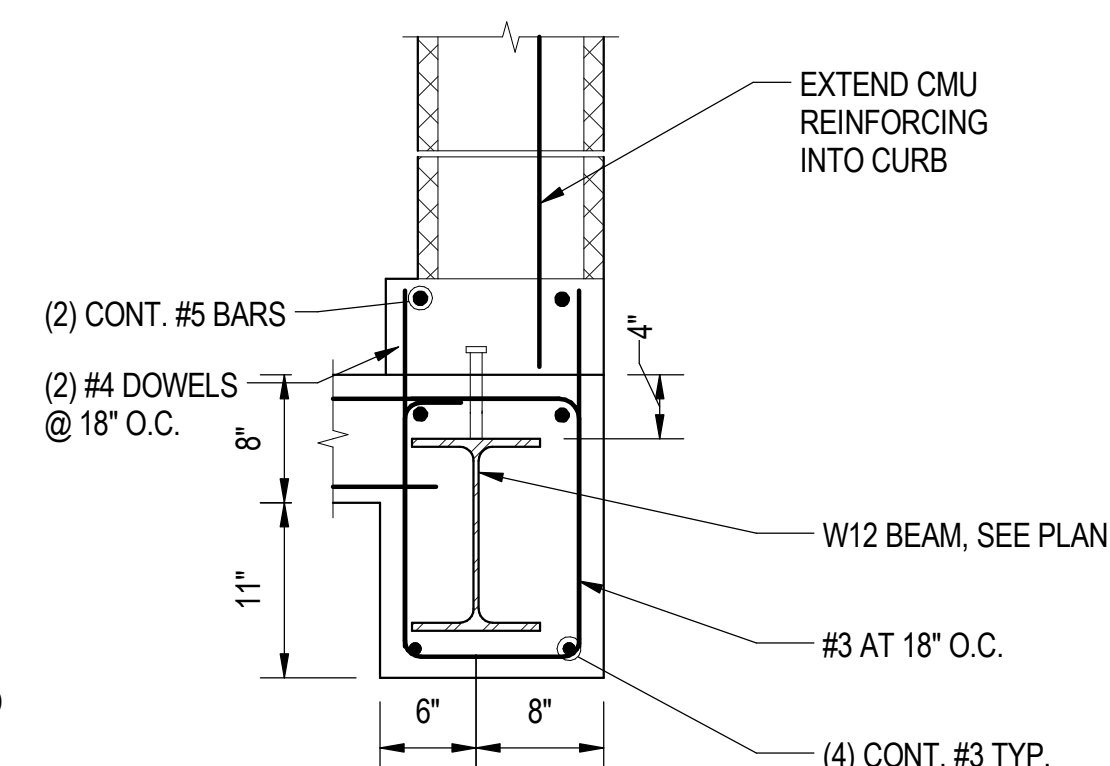
**2** SECTION  
S-853 SCALE: 1/2" = 1'-0"



**3** SECTION  
S-853 SCALE: 1/2" = 1'-0"



**4** HANGER CONNECTION  
S-853 SCALE: 1 1/2" = 1'-0"



**5** SLAB EDGE DETAIL  
S-853 SCALE: 1" = 1'-0"

**SHEET NOTES:**

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- FOR TYPICAL DETAILS REFER TO S-1112 TO S-1120.
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- TYPICAL FLOOR CONSTRUCTION: 8" NORMAL WEIGHT CONCRETE FORMED SLAB W/ #6 BOTTOM BARS EW @ 6" O.C., #4 TOP BARS EW @ 12" O.C.
- ALL PIT LEVEL STEEL FRAMING TO BE GALVANIZED.



**SENSITIVE SECURITY INFORMATION**  
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DATE		DESCRIPTION

LOCATION IDENTIFIER: **DR095**

**ESCALATOR #2 AND #3 PIT DETAILS**

**S-853**