

"FLOOD HAZARD AREA"
 CITY OF CHICAGO
 RAHM EMANUEL, MAYOR

DEPARTMENT OF TRANSPORTATION
 REBEKAH SCHEINFELD, COMMISSIONER

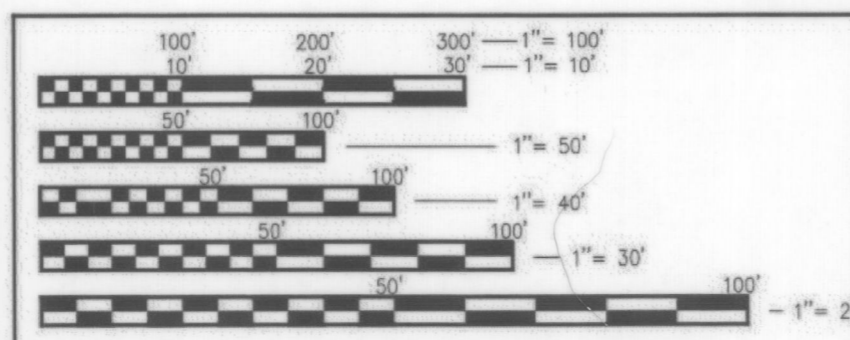
DIVISION OF ENGINEERING
 DANIEL BURKE, P.E., S.E.
 DEPUTY COMMISSIONER

DIVISION OF PROJECT DEVELOPMENT
 LUANN HAMILTON
 DEPUTY COMMISSIONER

NORTH BRANCH RIVERWALK:
 UNDERBRIDGE CONNECTION AT ADDISON
 STREET
 FROM CLARK PARK
 TO CALIFORNIA PARK

IDOT SECTION No. 12-E9121-00-BT
 FEDERAL PROJECT No. CMM-TE-M-6000(434)
 STATE JOB No. C-88-033-13
 CDOT PROJECT No. E-9-121
 SPECIFICATION No. 75946
 GROSS LENGTH 2212.8 FT. = 0.42 MI.

TRAFFIC DATA
 ADDISON: ADT 25,200 (2010)
 IRVING PARK: ADT 42,100 (2006)



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

- ISSUED BY -
 DEPARTMENT OF PROCUREMENT SERVICES.
 JAMIE L. RHEE, CHIEF PROCUREMENT OFFICER

ALL WORK SHALL BE DONE IN ACCORDANCE WITH THE DRAWINGS PROVIDED TO THE OFFICE OF UNDERGROUND CONSTRUCTION (OUC). IF CONTRACTOR CHOOSES TO REVISE CONSTRUCTION MEANS, METHODS OR CALCULATIONS THE CONTRACTOR SHALL MAKE A RESUBMITTAL TO OUC.

CHICAGO PARK DISTRICT

APPROVED: *Nick P. Neary*
 GENERAL SUPERINTENDENT

APPROVED: *Heather Hanson*
 DIRECTOR

DATE: 8/10/16

FLOOD CONTROL CERTIFICATION STATEMENT

I hereby certify that the following is true and correct:

1. I am registered professional engineer in the State of Illinois.
2. My license number, issued by the Illinois Department of Financial and Professional Regulation, is 062-046563
3. I am an engineer working on the development located at Addison Street and the North Branch of the Chicago River which is known as the Addison Underbridge Trail Connection from Clark Park to California Park.
4. I have reviewed the requirements set forth in Chapter 16-6 of the City of Chicago Municipal Code and the plans for the above referenced development.
5. Pursuant to section 16-6-040(a)(v), I hereby certify that the above referenced development complies with the requirements of Chapter 16-6 of the City of Chicago Municipal Code.

Name: *John Karlovitz* Date: _____

Professional License Stamp (Wet Ink Only)

John Karlovitz
 JOHN KARLOVITZ
 ILLINOIS REGISTRATION NUMBER 062-046563
 EXPIRATION DATE 11/30/17

Raveesh Varma
 RAVEESH VARMA, S.E.
 ILLINOIS REGISTRATION NUMBER 081-006790
 EXPIRATION DATE 11/30/16

Rashesh Kumar D. Patel
 RASHESHKUMAR D. PATEL
 ILLINOIS REGISTRATION NUMBER 062-064617
 EXPIRATION DATE 11/30/17

Douglas E. Hoerr
 DOUGLAS E. HOERR
 ILLINOIS REGISTRATION NUMBER 157-000877
 EXPIRATION DATE 8/31/17

CITY OF CHICAGO
 DEPARTMENT OF TRANSPORTATION

APPROVED: *John J. Kelly*
 PROJECT MANAGER/DEVELOPMENT MANAGER

APPROVED: *Janet Pottam*
 PROJECT DIRECTOR

APPROVED: *Paul L. Santos*
 DEPUTY COMMISSIONER
 DIVISION OF PROJECT DEVELOPMENT

APPROVED: *Tamir Burke*
 DEPUTY COMMISSIONER
 DIVISION OF ENGINEERING

APPROVED: *Willis*
 COMMISSIONER

DATE: 07/21/15

CITY OF CHICAGO
 DEPARTMENT OF STREETS AND SANITATION

APPROVED: _____

APPROVED: *Charles L. Williams*
 DEPUTY COMMISSIONER, BUREAU OF FORESTRY
 COMMISSIONER

DATE: 8/12/15

CITY OF CHICAGO
 DEPARTMENT OF WATER MANAGEMENT

APPROVED: *Paul R. ...*
 DEPUTY COMMISSIONER, BUREAU OF ENGINEERING SERVICES

APPROVED: *John ...*
 COMMISSIONER

DATE: _____

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

PASSED: NOVEMBER 22 2016

C. Helt
 CHRISTOPHER HOLT
 DISTRICT 1 ENGINEER OF LOCAL ROADS & STREETS

RELEASING FOR BID BASED ON LIMITED REVIEW: November 28 2016

John ...
 REGION 1 ENGINEER

THIS CERTIFIES THAT THESE DRAWINGS HAVE BEEN REVIEWED TO THE BEST OF MY KNOWLEDGE AND THAT I BELIEVE THAT THEY ARE IN ACCORDANCE WITH AMERICANS WITH DISABILITIES ACT (ADA), AND ALL CODES AND BUILDING ORDINANCES OF THE CITY OF CHICAGO, STATE OF ILLINOIS.

John Karlovitz
 LICENSED ARCHITECT / LICENSED ENGINEER

EPSTEIN

Architecture Interiors Engineering Construction

Chicago New York Bucharest Warsaw

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 Chicago, IL 60661
 312.454.9100

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156	CROSS SECTIONS - CALIFORNIA - STA 10+50 TO STA 11+75
157	CROSS SECTIONS - IRVING PARK - STA 1+60.24 TO STA 3+19.32
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January 24 2016 10:28:34 - CDOT - Addison Underbridge Connection - \DISCIPLINE\Civil\Working\Sheets\012_SheetIndex-Standard.dwg

SECTION NO.: 12-E9121-00-BT
 DRAWN BY: JRK
 CHECKED BY: JRK
 APPROVED BY: GAO
 DATE: 8/12/2016
 SCALE: -----

NO.	DATE	REVISIONS/ISSUANCES
1	8/12/16	ISSUED FOR FINAL
2	5/6/16	ISSUED FOR FINAL
3	6/05/15	ISSUED FOR FINAL REVIEW

CITY OF CHICAGO

DEPARTMENT OF TRANSPORTATION
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ADDISON UNDERBRIDGE TRAIL CONNECTION FROM CLARK PARK TO CALIFORNIA PARK

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C-2

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REVISION

January 28, 2016 2:00 PM 28346 - CDOT - Addison Underbridge Connection - DISCIPLINE (Civil/Working) Sheets/013_Summary of Quantities.dwg

ITEM NO.	CODE NO.	DESCRIPTION	UNIT	ESTIMATED QUANTITY
1	CDOT2010010	ROOT PRUNING	FOOT	200
2	CDOT2010020	TREE PROTECTION	EACH	26
3	20100500	TREE REMOVAL, ACRES	ACRE	0.5
4	20200100	EARTH EXCAVATION	CU YD	625
5	20201200	REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL	CU YD	1,140
6	20700220	POROUS GRANULAR EMBANKMENT	CU YD	2,615
7	20800150	TRENCH BACKFILL	CU YD	34
8	CDOT2110040	PLANTER SOIL BLEND	CU YD	525
9	21101505	TOPSOIL EXCAVATION AND PLACEMENT	CU YD	1,710
10	25000100	SEEDING, CLASS 1	ACRE	0.50
11	25000300	SEEDING, CLASS 3	ACRE	0.50
12	25000400	NITROGEN FERTILIZER NUTRIENT	POUND	12
13	25000600	POTASSIUM FERTILIZER NUTRIENT	POUND	40
14	25100630	EROSION CONTROL BLANKET	SQ YD	2,063
15	25100900	TURF REINFORCEMENT MAT	SQ YD	548
16	28000250	TEMPORARY EROSION CONTROL SEEDING	POUND	100
17	28000400	PERIMETER EROSION BARRIER	FOOT	1,211
18	28000510	INLET FILTERS	EACH	5
19	28100107	STONE RIPRAP, CLASS A4	SQ YD	506
20	28100711	STONE DUMPED RIPRAP, CLASS A6	SQ YD	160
21	28200200	FILTER FABRIC	SQ YD	666
22	31101400	SUBBASE GRANULAR MATERIAL, TYPE B 6"	SQ YD	2,304
23	40600290	BITUMINOUS MATERIALS (TACK COAT)	POUND	2,746
24	40603080	HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N50	TON	224
25	40603335	HOT-MIX ASPHALT SURFACE COURSE, MX"D", N50	TON	96
26	42000060	WELDED WIRE REINFORCEMENT	SQ YD	1,131
27	42000100	PORTLAND CEMENT CONCRETE PAVEMENT 6"	SQ YD	1,131
28	44000100	PAVEMENT REMOVAL	SQ YD	586
29	CDOT5010010	REMOVAL OF EXISTING STRUCTURES	L. SUM	1
30	CDOT 5030035	HIGH STRENGTH HIGH PERFORMANCE CONCRETE STRUCTURES	CU YD	111.6
31	CDOT 5030040	HIGH STRENGTH HIGH PERFORMANCE CONCRETE SUPERSTRUCTURES	CU YD	845.3
32	50400305	PRECAST PRESTRESSED CONCRETE DECK BEAMS (17" DEPTH)	SQ FT	680
33	50800205	REINFORCEMENT BARS, EPOXY COATED	POUND	398,970
34	50800530	MECHANICAL SPLICERS	EACH	2,376
35	51500100	NAME PLATES	EACH	1
36	51602000	PERMANENT CASING	FOOT	503
37	51603000	DRILLED SHAFT IN SOIL	CU YD	469.5
38	52000050	PREFORMED JOINT SEAL, 4"	FOOT	112
39	52100010	ELASTOMERIC BEARING ASSEMBLY, TYPE I	EACH	24
40	52200015	PERMANENT SHEET PILING	SQ FT	30,584
41	CDOT5870010	PROTECTIVE CONCRETE SEALER	SQ YD	3,244
42	*****	GEO FOAM (POLYSTYRENE FILL)	CU YD	132
43	60108204	PIPE UNDERDRAINS, TYPE 2, 4"	FOOT	161

ITEM NO.	CODE NO.	DESCRIPTION	UNIT	ESTIMATED QUANTITY
44	*****	CATCH BASINS, TYPE A, 4' DIAMETER TYPE 8 GRATE (CITY OF CHICAGO)	EACH	1
45	60207605	CATCH BASINS, TYPE C, TYPE 8 GRATE	EACH	2
46	60236200	INLETS TYPE A, TYPE 8 GRATE	EACH	1
47	CDOT6050020	REMOVING CATCH BASINS	EACH	1
48	*****	STORM SEWERS, EXTRA STRENGTH VITRIFIED CLAY PIPE, TYPE 2, 6" (CDOT)	FOOT	46
49	*****	STORM SEWERS, DUCTILE IRON PIPE, CLASS 52, 4" (CDOT)	FOOT	21
50	*****	STORM SEWERS, DUCTILE IRON PIPE, CLASS 52, 10" (CDOT)	FOOT	59
51	60600605	CONCRETE CURB, TYPE B	FOOT	79
52	66400305	CHAIN LINK FENCE, 6'	FOOT	309
53	CDOT6640020	TEMPORARY CHAIN LINK FENCE WITH SCREENING, 8'	FOOT	474
54	66900200	NON-SPECIAL WASTE DISPOSAL	CU YD	1,895
55	66900205	SPECIAL WASTE DISPOSAL	CU YD	45
56	66900450	SPECIAL WASTE PLANS AND REPORTS	L. SUM	1
57	66900530	SOIL DISPOSAL ANALYSIS	EACH	16
58	CDOT6700010	ENGINEER'S FIELD OFFICE	CAL. MO.	16
59	67100100	MOBILIZATION	L. SUM	1
60	72000100	SIGN PANEL - TYPE 1	SQ FT	8
61	72800100	TELESCOPING STEEL SIGN SUPPORT	FOOT	44
62	78000200	THERMOPLASTIC PAVEMENT MARKING - LINE 4"	FOOT	1,767
63	78008210	POLYUREA PAVEMENT MARKING TYPE 1 - LINE 4"	FOOT	4,996
64	81028320	UNDERGROUND CONDUIT, PVC, 1" DIA.	FOOT	20
65	81028340	UNDERGROUND CONDUIT, PVC, 1 1/2" DIA.	FOOT	1,745
66	81100200	CONDUIT ATTACHED TO STRUCTURE, 3/4" DIA., GALVANIZED STEEL	FOOT	60
67	81100500	CONDUIT ATTACHED TO STRUCTURE, 1 1/2" DIA., GALVANIZED STEEL	FOOT	20
68	81200210	CONDUIT EMBEDDED IN STRUCTURE, 1" DIA. PVC	FOOT	3,408
69	81300100	JUNCTION BOX, STAINLESS STEEL, ATTACHED TO STRUCTURE, 4"x4"x3"	EACH	5
70	*****	JUNCTION BOX, STAINLESS STEEL, ATTACHED TO STRUCTURE, 6" X 4" X 2"	EACH	194
71	*****	JUNCTION BOX, STAINLESS STEEL, ATTACHED TO STRUCTURE, 6" X 4" X 2", IP 68 RATED	EACH	72
72	81300550	JUNCTION BOX, STAINLESS STEEL, ATTACHED TO STRUCTURE, 12"x12"x8"	EACH	1
73	81603010	UNIT DUCT, 600V, 2-1C NO. 10, 1/C NO. 10 GROUND, (XLP TYPE USE), 3/4" DIA POLYETHYLENE	FOOT	70
74	81702100	ELECTRIC CABLE IN CONDUIT, 600V(XLP-TYPE USE) 1/C NO. 12	FOOT	1,542
75	81702110	ELECTRIC CABLE IN CONDUIT, 600V(XLP-TYPE USE) 1/C NO. 10	FOOT	4,694
76	81702120	ELECTRIC CABLE IN CONDUIT, 600V(XLP-TYPE USE) 1/C NO. 8	FOOT	14,463
77	81702130	ELECTRIC CABLE IN CONDUIT, 600V(XLP-TYPE USE) 1/C NO. 6	FOOT	8,076
78	*****	FLEXIBLE LIQUID TIGHT METALLIC CONDUIT, 1/2" DIA.	FOOT	262
79	*****	FLEXIBLE LIQUID TIGHT METALLIC CONDUIT, 1" DIA.	FOOT	528
80	*****	PEDESTRIAN LIGHT POLE, CONCRETE, 20 FT, WITH FOUNDATION, CHICAGO PARK DISTRICT	EACH	13
81	*****	LUMINAIRE, LED, HORIZONTAL MOUNT, 101 WATT	EACH	13
82	*****	LINEAR LED WALL GRAZER LIGHT INCLUDING MOUNTING BRACKET (1FT), IP 65 RATED	EACH	192
83	*****	LINEAR LED WALL GRAZER LIGHT INCLUDING MOUNTING BRACKET (1FT), IP 68 RATED	EACH	70
84	*****	ELECTRICAL PULLBOX, 12" X 12" X 12"	EACH	17
85	*****	ELECTRICAL PULLBOX, 24" X 36" X 18"	EACH	3
86	*****	MODIFY EXISTING POWER PANEL	EACH	1

ITEM NO.	CODE NO.	DESCRIPTION	UNIT	ESTIMATED QUANTITY
87	X8950130	MODIFY EXISTING LIGHTING CONTROLLER	EACH	1
88	*****	ROD AND CLEAN DUCT IN EXISTING CONDUIT SYSTEM	FOOT	60
89	*****	MAINTAIN EXISTING LIGHTING SYSTEM	L. SUM	1
90	*****	TREE PRUNING	EACH	40
91	*****	ARBORIST INSPECTION	L. SUM	1
92	*****	PERENNIAL PLANTS, 2.5" PLUG INSTALLATION, ECHINACEA PALLIDA (PALE PURPLE CONEFLOWER), 2.5" PLUG FORM, @ 15" O.C.	EACH	385
93	*****	PERENNIAL PLANTS, 2.5" PLUG INSTALLATION, AGASTACHE FOENICULUM (ANISE HYSSOP), 2.5" PLUG FORM, @ 15" O.C.	EACH	350
94	*****	PERENNIAL PLANTS, 2.5" PLUG INSTALLATION, PYCNANTHEMUM VIRGINIANUM (MOUNTAIN MINT), 2.5" PLUG FORM, @ 15" O.C.	EACH	390
95	*****	TREE, GYMNOCLADUS DIOICUS (KENTUCKY COFFEETREE), 2" CALIPER, BALLED AND BURLAPPED	EACH	3
96	*****	TREE, PLATANUS X ACERIFOLIUM 'BLOODGOOD' (BLOODGOOD LONDON PLANE TREE), 2" CALIPER, BALLED AND BURLAPPED	EACH	5
97	*****	TREE, AMELANCHIER X GRANDIFLORA AUTUM BRILLIANCE (AUTUMN BRILLIANCE SERVICEBERRY), 8' HEIGHT, SHRUB FORM	EACH	3
98	X0321809	PERMANENT GROUND ANCHORS	EACH	47
99	X0321865	ANTI-GRAFFITI PROTECTION SYSTEM	SQ FT	900
100	X0326935	CROSSHOLE SONIC LOGGING	EACH	6
101	X5030306	CONCRETE WEARING SURFACE, 6"	SQ YD	76
102	X5091725	BICYCLE RAILING, SPECIAL	FOOT	3,008
103	X6064200	COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12 (SPECIAL)	FOOT	56
104	Z0003850	BENCHES	EACH	2
105	Z0013798	CONSTRUCTION LAYOUT	L. SUM	1
106	Z0018800	DRAINAGE SYSTEM	L. SUM	1
107	Z0022800	FENCE REMOVAL	FOOT	342
108	Z0046304	PIPE UNDERDRAINS FOR STRUCTURES 4"	FOOT	571
109	Z0064600	SELECTIVE CLEARING	ACRE	1
110	Z0073500	TEMPORARY SUPPORT SYSTEM	L. SUM	1
111	*****	BENCH REMOVAL	EACH	2
112	*****	LIFE RINGS	EACH	2
113	*****	BIO-INFILTRATION SYSTEM	CU YD	65
114	*****	VEGETATED FILTER STRIP	CU YD	135
115	*****	TERRACED WALL BLOCK 2' X 2' X 2'	EACH	3
116	*****	TERRACED WALL BLOCK 2' X 2' X 3'	EACH	8
117	*****	TERRACED WALL BLOCK 2' X 2' X 4'	EACH	18
118	*****	TERRACED WALL BLOCK 2' X 2' X 5'	EACH	16
119	*****	TERRACED WALL BLOCK 2' X 2' X 6'	EACH	2
120	*****	TERRACED WALL BLOCK 2' X 2' X 7'	EACH	17
121	*****	TERRACED WALL BLOCK 4' X 4' X 4'	EACH	50
122	Z0076600	TRAINEES	HOUR	3,000
123	*****	GEOCELL CONFINEMENT SYSTEM	SQ YD	548
124	*****	NAVIGATION LIGHTING	L. SUM	1

SECTION NO.: 12-E9121-00-BT
 DRAWN BY: JRK
 CHECKED BY: JRK
 APPROVED BY: GAO
 DATE: 8/12/2016
 SCALE: N.T.S.

8/12/16 ISSUED FOR FINAL
 5/6/16 ISSUED FOR FINAL
 6/05/15 ISSUED FOR FINAL REVIEW
 NO. DATE REVISIONS/ISSUANCES

CITY OF CHICAGO

DEPARTMENT OF TRANSPORTATION
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Architecture
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ADDISON UNDERBRIDGE TRAIL CONNECTION FROM
 CLARK PARK TO CALIFORNIA PARK

SUMMARY OF QUANTITIES

SHEET NO.
 C-3

SHEET 3 OF 158

REVISION

GENERAL NOTES:

- ALL DIMENSIONS SHOWN ON THE PLANS ARE TO EDGE OF PAVEMENT UNLESS OTHERWISE SPECIFIED.
- ANY REFERENCE TO "STANDARD SPECIFICATIONS; THROUGHOUT THE PLANS OR SPECIAL PROVISIONS SHALL BE INTERPRETED TO BE THE ILLINOIS DEPARTMENT OF TRANSPORTATION "STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION" AND "SUPPLEMENTAL SPECIFICATIONS AND RECURRING SPECIAL PROVISIONS", ADOPTED APRIL 1, 2016, RESPECTIVELY.
- IN ACCORDANCE WITH ARTICLE XI OF THE GENERAL CONDITIONS, THE CONTRACTOR SHALL SUBMIT A TIME SCHEDULE FOR THE CONSTRUCTION OF THIS PROJECT. THE SCHEDULE SHALL INCLUDE THE PERIODS OF TIME IN WHICH THE WORK ITEMS ARE TO BE COMPLETED. PARTICULAR ATTENTION SHOULD BE GIVEN TO REMOVAL AND REPLACEMENT OF CURB AND GUTTER, SIDEWALKS, ALLEYS AND DRIVEWAYS. ANY OF THE ITEMS REMOVED SHALL BE REPLACED IN THE SHORTEST PERIOD OF TIME AND, AT NO TIME, SHALL THEIR REPLACEMENT EXCEED TEN (10) WORKING DAYS OR THE NUMBER OF WORKING DAYS GIVEN IN THE APPROVED PROJECT SCHEDULE. THE COST OF COMPLIANCE WITH THIS REQUIREMENT SHALL BE INCIDENTAL TO EACH RESPECTIVE PAY ITEM OF WORK.
- THE CONTRACTOR SHALL OBTAIN ALL NECESSARY PERMITS FROM THE CITY OF CHICAGO PRIOR TO COMMENCING CONSTRUCTION.
- IT IS CALLED TO THE CONTRACTOR'S ATTENTION THAT HE MAY BE REQUIRED TO PAY INSPECTION FEES TO THE VARIOUS DEPARTMENTS OF THE CITY OF CHICAGO AS NOTED IN ARTICLE IX OF THE GENERAL CONDITIONS.
- A PERMIT IS REQUIRED FROM THE DEPARTMENT OF WATER MANAGEMENT PRIOR TO THE CONSTRUCTION OF, OR REPAIR TO UNDERGROUND SEWERS, DRAIN CONNECTIONS OR SEWER STRUCTURES, INCLUDING ADJUSTMENT OF SEWER STRUCTURES AND REMOVAL/REPLACEMENT OF FRAMES AND LIDS. THE PERMIT MUST BE OBTAINED BY A DRAINLAYER CURRENTLY LICENSED BY THE DEPARTMENT OF SEWERS.
- THE CONTRACTOR SHALL MAKE HIS INVESTIGATION TO DETERMINE THE EXISTENCE, NATURE AND EXACT LOCATION OF ALL UTILITY LINES AND APPURTENANCES WITHIN THE LIMITS OF THIS IMPROVEMENT. THE COST OF THIS WORK WILL BE CONSIDERED INCIDENTAL TO EARTH EXCAVATION.
- THE CONTRACTOR SHALL PROTECT EXISTING AND NEW UTILITIES WHEN CONSIDERED NECESSARY BY THE COMMISSIONER, AND BY METHODS APPROVED BY THE COMMISSIONER, AND HE SHALL BRACE AND SUPPORT THE UTILITIES PROPERLY TO PREVENT SETTLEMENT, DISPLACEMENT OR CHANGE TO THE UTILITIES. THE PROTECTION OF THE UTILITIES AS SPECIFIED HEREIN WILL NOT BE PAID FOR SEPARATELY, BUT THE COST THEREOF SHALL BE CONSIDERED INCIDENTAL TO EARTH EXCAVATION.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE PROTECTION OF ALL UNDERGROUND UTILITIES WITHIN THE PROJECT LIMITS WHETHER OR NOT THE UTILITIES ARE SHOWN ON THE PLANS. ANY UTILITY THAT IS DAMAGED DURING CONSTRUCTION SHALL BE REPAIRED OR REPLACED BY THE RESPECTIVE UTILITY OR THE CONTRACTOR IF SO DIRECTED BY THE UTILITY AT THE CONTRACTOR'S EXPENSE TO THE SATISFACTION OF THE RESPECTIVE UTILITY.
- THE CITY DOES NOT GUARANTEE THE COMPLETENESS OR ACCURACY OF THE INFORMATION SHOWN ON THE PLANS REGARDING UTILITIES, EITHER PUBLIC OR PRIVATE, SUCH AS SEWERS, MANHOLES, CATCH BASINS, GAS AND WATER MAINS, TELEPHONE AND ELECTRICAL DUCT LINES AND SIMILAR STRUCTURES. THE CONTRACTOR SHALL VERIFY THE EXACT LOCATION OF ALL UTILITIES THAT MAY INTERFERE WITH CONSTRUCTION OPERATIONS, AND SHALL REPORT TO THE COMMISSIONER ANY OMISSIONS AND DIFFERENCES FROM THE LOCATIONS SHOWN ON THE PLANS. THE COST OF THIS WORK WILL BE CONSIDERED INCIDENTAL TO EARTH EXCAVATION.
- THE UTILITY COMPANIES OR THEIR CONTRACTORS WILL PERFORM THE REQUIRED ADJUSTMENT TO THEIR UTILITIES UNLESS OTHERWISE NOTED ON THE PLANS, OR DIRECTED BY THE COMMISSIONER.
- 48 HOURS BEFORE STARTING EXCAVATION, THE CONTRACTOR WILL CALL DIGGER C.U.A.N. (CHICAGO UTILITY ALERT NETWORK) AT 312-744-7000 TO HAVE THE LOCATION OF EXISTING UNDERGROUND UTILITIES STAKED.
- PRIOR TO STARTING CONSTRUCTION AN INSPECTION OF EXISTING MANHOLES AND CATCH BASINS WILL BE MADE BY THE CITY AND THE CONTRACTOR TO DETERMINE THE AMOUNT OF EXISTING DEBRIS IN THESE STRUCTURES. UPON COMPLETION OF THE CONTRACT, THE CONTRACTOR SHALL CLEAN ONLY THOSE STRUCTURES WHERE DEBRIS HAS BEEN ADDED DUE TO CONSTRUCTION. THIS WORK WILL NOT BE PAID FOR SEPARATELY, BUT SHALL BE CONSIDERED INCIDENTAL TO THE STORM SEWER PAY ITEMS.
- EXISTING CATCH BASIN LATERALS TO BE REUSED MUST BE RODDED AND FLUSHED IN THE PRESENCE OF THE DEPARTMENT OF WATER MANAGEMENT INSPECTOR. A NEW CONNECTION TO THE MAIN SEWER IS REQUIRED IF THE EXISTING CATCH BASIN LATERAL IS NOT APPROVED BY THE SEWER INSPECTOR. THE FLUSHING OF THE EXISTING LATERAL WILL BE CONSIDERED INCIDENTAL TO THE SEWER WORK.
- IN LOCATIONS WHERE THE MAIN SEWER IS NOT BEING REPLACED AND THE EXISTING DRAINAGE FACILITIES ARE DISTURBED OR DAMAGED DURING CONSTRUCTION BY THE CONTRACTOR, IT WILL BE THE CONTRACTOR'S RESPONSIBILITY TO RESTORE AND REPLACE THE DAMAGED FACILITIES AT HIS EXPENSE TO THE SATISFACTION OF THE COMMISSIONER.
- THE CONTRACTOR SHALL TAKE NECESSARY SAFETY PRECAUTIONS TO PROTECT ABUTTING PROPERTY, UTILITIES, PEDESTRIANS AND VEHICULAR TRAFFIC.
- WHEN THE PAVEMENT CONSTRUCTED IS PORTLAND CEMENT CONCRETE BASE COURSE OR PORTLAND CEMENT CONCRETE PAVEMENT, IT SHALL NOT BE OPENED TO TRAFFIC, INCLUDING CONSTRUCTION TRAFFIC, UNTIL AFTER THE SPECIFIED CURING PERIOD, AS DEFINED IN ARTICLE 701.17(c)(5) OF THE 'STANDARD SPECIFICATIONS' AND UNTIL THE JOINTS HAVE BEEN SEALED.
- THE CONTRACTOR SHALL TAKE NECESSARY PRECAUTIONS TO PROTECT THE EXISTING FENCES DURING EXCAVATION AND ANY DAMAGE TO THE FENCES SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR. THE CONTRACTOR SHALL REPLACE THE DAMAGED FENCE AT HIS EXPENSE AND NO ADDITIONAL COMPENSATION SHALL BE ALLOWED.
- AT ANY EARTH EXCAVATION NEAR EXISTING TREES CARE MUST BE TAKEN SO THAT DAMAGE TO THE TREE ROOTS DOES NOT OCCUR. HAND EXCAVATION SHOULD BE PERFORMED AROUND TREES TO REMAIN IF ROOTS ARE PRESENT AND ROOT PRUNING PERFORMED AS DIRECTED BY THE COMMISSIONER.
- THE CONTRACTOR SHALL MAINTAIN COMPLETE ACCESS TO ALL BUILDINGS AND PROPERTY ADJACENT TO THE PROPOSED IMPROVEMENT.
- ALL EXCAVATED MATERIALS, NOT SUITABLE FOR BACKFILL, SHALL BE CONSIDERED SURPLUS AND MUST BE DISPOSED OFF-SITE BY THE CONTRACTOR. THE MANNER AND LOCATION OF DISPOSAL SHALL BE SUBJECT TO THE APPROVAL OF THE COMMISSIONER. EXCAVATED MATERIAL THAT IS SUITABLE FOR BACKFILL SHALL BE USED WHERE NEEDED WITHIN THE SITE OR AS DIRECTED BY THE COMMISSIONER.
- ADEQUATE PROVISIONS FOR PEDESTRIAN TRAFFIC MUST BE PROVIDED AT ALL TIMES.
- THE ENTIRE AREA WHICH IS TO RECEIVE 'BITUMINOUS MATERIALS (TACK COAT)' MUST BE SWEEPED CLEAN BEFORE THE MATERIAL IS APPLIED. THE SWEEPINGS WILL NOT BE DEPOSITED IN THE GUTTER OR ON THE CURB, PARKWAY, OR SIDEWALK, BUT ARE TO BE PICKED UP AND DISPOSED OF PROPERLY BEYOND THE LIMITS OF THE PROJECT ON THE SAME DAY THAT THE SWEEPING IS DONE. THE WORK WILL NOT BE PAID FOR SEPARATELY, BUT WILL BE CONSIDERED INCIDENTAL TO 'BITUMINOUS MATERIALS (TACK COAT)'.
- THE CONTRACTOR SHALL REMOVE AND RE-ERECT EXISTING FENCES AS REQUIRED TO PERFORM HIS CONSTRUCTION ACTIVITIES UNLESS OTHERWISE INDICATED ON THE PLANS
- THE CONTRACTOR SHALL REMOVE AND RE-ERECT EXISTING FENCES AS REQUIRED TO PERFORM HIS CONSTRUCTION ACTIVITIES UNLESS OTHERWISE INDICATED ON THE PLANS
- THE CONTRACTOR SHALL USE EXTREME CAUTION NEAR COMED FACILITIES. THE COMED TRANSMISSION DIVISION SHALL BE NOTIFIED 5 BUSINESS DAYS PRIOR TO THE START OF WORK.

CONTRACTOR UTILITY COORDINATION	INVOLVEMENT
Chicago Department of Water Management - Sewer 312 744 7641 Contact: Paul Bates	Protection of existing facilities and permit approval
Chicago Department of Water Management - Water 312 742 1875 Contact: Stacey Lindeman	Protection of existing facilities
Chicago Division of Electrical Operations 312 744 2450 Contact: Christopher Hein	Approval of lighting plan
Chicago Bureau of Engineering 312 742 0647 Contact: Ozzie Chavez	Harbor permit is required
Comcast 773 394 8557 Contact: C. Adams	Protection of existing facilities
ComEd Transmission 630 437 4767 Contact: Leslie Paschal	Protection of existing facilities
ComEd Distribution 773 509 3741 Contact: Jesus Neris	Protection of existing facilities
JC Decaux North America 312 456 2977 Contact Quiana Carter	Protection of existing facilities
Metropolitan Water Reclamation District 312 751 3184 Hanif Munshi	Protection of existing facilities
Peoples Gas 312 240 3652 Contact: Aaron Meyer	Protection of existing facilities
RCN 312 955 3950 Contact: Frank Rog	Protection of existing facilities
Chicago Park District 312 742 4678 Contact: Vasudeva Vadali	Permit required if using Park District property for construction access or storage

PLANT MATERIAL

WHEREVER PLANT MATERIAL IS TO BE USED, PLEASE NOTE THAT THE FOLLOWING WILL ALSO APPLY:

THE CONTRACTOR SHALL SECURE ALL SPECIFIED PLANT MATERIAL IMMEDIATELY UPON CONTRACT AWARD. PROOF THAT THE PLANT MATERIAL IS SECURED SHALL BE ON FILE WITH THE COMMISSIONER PRIOR TO THE PROCESSING OF ANY PAYMENT TO THE CONTRACTOR. HOWEVER, IF THE CONTRACTOR PROCEEDED TO DO EVERYTHING REQUIRED TO SECURE THE SPECIFIED PLANTS AND FOR REASONS BEYOND THE CONTRACTOR'S CONTROL PLANT SUBSTITUTIONS ARE REQUIRED, SUCH SUBSTITUTION SHALL FOLLOW THE PROCESS DETAILED BELOW.

SUBSTITUTION OF PLANT MATERIAL

THE CONTRACTOR SHALL MAKE EVERY EFFORT TO MAINTAIN THE DESIGN INTENT OF ALL LANDSCAPE PLANS. TO THIS END, SUBSTITUTIONS OF PLANT MATERIAL SHALL BE KEPT TO AN ABSOLUTE MINIMUM AND REQUESTS FOR SUBSTITUTIONS SHALL ADHERE TO THE FOLLOWING REQUIREMENTS.

REQUESTS FOR SUBSTITUTION OF PLANT MATERIAL SHALL BE SUBMITTED FOR REVIEW WITHIN TWO (2) WEEKS FOLLOWING THE AWARD DATE. ALL PROPOSED SUBSTITUTIONS ARE SUBJECT TO APPROVAL BY THE COMMISSIONER.

REQUESTS FOR PLANT SUBSTITUTIONS SHALL BE SUBMITTED IN WRITING AND LIST CONTRACT ITEM NUMBERS, QUANTITY, ORIGINAL PLANT NAME (BOTANICAL AND COMMON), ORIGINAL SIZE, NURSERIES CONTACTED (WITH PHONE NUMBERS) FOR ORIGINAL MATERIAL (10 MINIMUM), NAME OF SUBSTITUTION (BOTANICAL AND COMMON), AND SIZE. THE CONTRACTOR SHALL CONTACT A MINIMUM OF TEN (10) NURSERIES IN SEARCH OF A PLANT BEFORE THAT PLANT CAN BE ELIGIBLE FOR SUBSTITUTION.

IF SUBSTITUTIONS ARE APPROVED FOR SMALLER SIZED PLANT MATERIAL, NEW LINE ITEMS WILL BE ADDED TO THE CONTRACT AS A CONTRACT MODIFICATION. THE UNIT PRICE WILL BE ADJUSTED TO REFLECT THE LOWER COST OF SMALLER PLANT MATERIAL. THE UNITS WILL BE INCREASED AS TO KEEP THE DESIGN INTENT.

EARTHWORK

	EARTH EXCAVATION (CU YD)	EARTH EXCAVATION ADJUSTED FOR SHRINKAGE (15%)	EMBANKMENT (CU YD)	BALANCE (SUITABLE CUT MINUS EMBANKMENT) (CU YD)	TOPSOIL EXCAVATION (±15") (CU YD)	6" TOPSOIL PLACEMENT (CU YD)	REMAINING TOPSOIL (REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL) (CU YD)
TOTAL	625	530	3290	-2760	1890	740	1150

January 24, 2016 10:00 AM - Addison Underbridge Connection - DISCIPLINE: Civil Working\Sheets\04_GeneralNotes.dwg - CDOT - Addison Underbridge Connection - 12-E9121-00-2436

SECTION NO.:	12-E9121-00-BT	NO.	DATE	REVISIONS/ISSUANCES
DRAWN BY:	JRK			
CHECKED BY:	JRK	8/12/16		ISSUED FOR FINAL
APPROVED BY:	GAO	5/6/16		ISSUED FOR FINAL REVIEW
DATE:	8/12/2016	6/05/15		ISSUED FOR FINAL REVIEW
SCALE:	N.T.S.			

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ADDISON UNDERBRIDGE TRAIL CONNECTION FROM CLARK PARK TO CALIFORNIA PARK

GENERAL NOTES

SHEET NO.
C-4

SHEET 4 OF 158

REVISION

SPECIAL WASTE

SURPLUS SOILS GENERATED FROM THE UPPER FILL MATERIAL AT THE SITE, TO A DEPTH OF AT LEAST 5 FEET BELOW GROUND SURFACE (BGS), SHOULD BE DISPOSED OF AT A LICENSED SUBTITLE D LANDFILL, IN ACCORDANCE WITH APPLICABLE LOCAL, STATE, AND FEDERAL REGULATIONS. THE ACTUAL VOLUME OF EACH TYPE OF MATERIALS WILL DEPEND ON THE PROPOSED DESIGN PLANS AND METHODS EMPLOYED DURING CONSTRUCTION. A SOIL MANAGEMENT PLAN SHALL BE PREPARED DESCRIBING PROPOSED EXCAVATION WORK SEQUENCES AND PROCEDURES TO SEPARATE EACH TYPE OF MATERIAL TO BE REMOVED FROM THE SITE. THE SOIL MANAGEMENT PLAN SHALL ALSO INCLUDE INFORMATION REGARDING ENGINEERING CONTROLS DURING EXCAVATION, LOCATION AND VOLUME OF CONTAMINATED MATERIALS TO BE REMOVED, LOCATION OF TEMPORARY STOCKPILES OF EACH TYPE OF MATERIAL TO BE STORED ON SITE, DAILY RECORDS OF SOIL EXCAVATION ACTIVITIES, DISPOSAL RECEIPTS, RECORD KEEPING PROCESS, AND OTHER SOIL HANDLING AND MANAGEMENT REQUIREMENTS. THE SOIL MANAGEMENT PLAN SHOWS THE DEPTHS OF IMPACTED MATERIALS THROUGHOUT THE SITE.

BASED ON THE ABOVE DISCUSSION AND THE ANALYTICAL DATA PROVIDED HEREIN, DISPOSAL OPTION FOR SURPLUS SOILS GENERATED FROM THE PROJECT AREAS ARE AS FOLLOWS:

- SOILS BELOW THE MAXIMUM ALLOWABLE CONCENTRATIONS, AS DEFINED UNDER 35IL ADM CODE 1100 REGULATIONS, SHOULD BE CONSIDERED CLEAN (CCDD); AND CAN BE DISPOSED AT APPROVED CCDD LOCATIONS. THE TOTAL EXTENT OF THIS AREA WAS NOT DELINEATED AND IT APPEARS TO BE EXTREMELY SMALL, THEREFORE WE WOULD RECOMMEND THAT ALL SURPLUS SOIL GENERATED BE DISPOSED AT A PERMITTED SUBTITLE D LANDFILL.
- SOILS WITH VALUES EXCEEDING THE MAC AND TACO TIER 1 VALUES SHOULD BE CONSIDERED "CONTAMINATED", AND SHOULD BE DISPOSED AT A PERMITTED SUBTITLE D LANDFILL IN ACCORDANCE WITH ALL APPLICABLE FEDERAL, STATE, AND LOCAL REGULATIONS.

THE FOLLOWING TABLE PRESENTS THE DISPOSAL SCENARIOS:

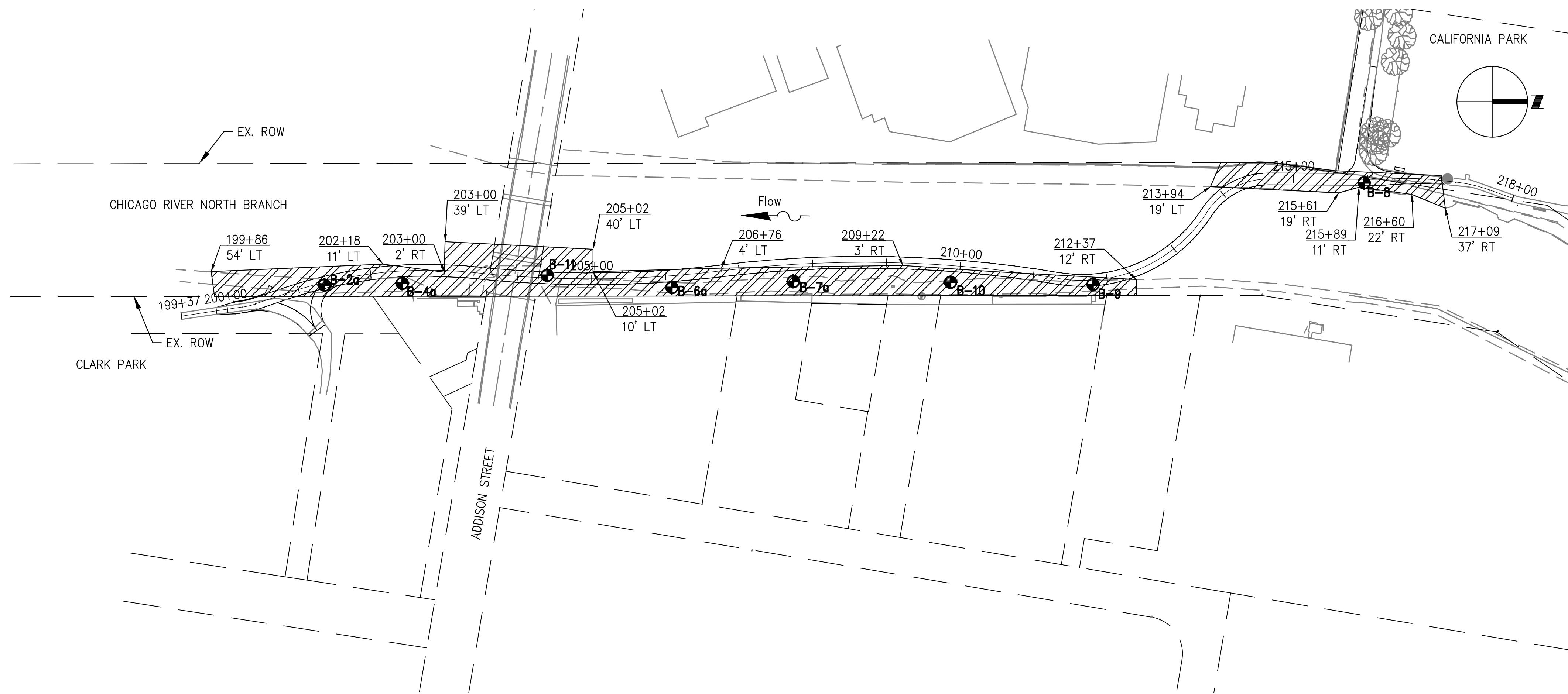
APPROXIMATE STATION	DEPTH	DISPOSAL FACILITY
STA 199+85 TO STA 217+00	5 FEET BGS	SUBTITLE D

MATERIALS BELOW THE DEPTHS INDICATED IN THE TABLE ABOVE MAY REMAIN IN-PLACE. THE SOIL MANAGEMENT PLAN, SHOWS THE SOIL HANDLING REQUIREMENTS.

THE CONTRACTOR SHALL BE RESPONSIBLE FOR HIRING AN ENVIRONMENTAL FIRM WITH AT LEAST FIVE (5) DOCUMENTED LEAKING UNDERGROUND STORAGE TANKS (LUST) CLEANUPS OR THAT IS PRE-QUALIFIED IN HAZARDOUS WASTE BY THE DEPARTMENT TO REMEDIATE THE SOIL CONTAMINATION AND MONITOR FOR WORKER PROTECTION.

LEGEND

- ENVIRONMENTAL SOIL SAMPLE LOCATION
- ▨ SUBTITLE D MATERIAL; 0 TO 5 FEET BGS



January 24, 2016 10:00:00 AM - Addision Underbridge Connection - CDOT - Addision Underbridge Connection - DISCIPLINE\Civil\Working\Sheets\005_Special Waste.dwg

SECTION NO.: 12-E9121-00-BT
 DRAWN BY: JRK
 CHECKED BY: JRK
 APPROVED BY: GAO
 DATE: 8/12/2016
 SCALE: N.T.S.

NO.	DATE	REVISIONS/ISSUANCES
1	8/12/16	ISSUED FOR FINAL
2	5/6/16	ISSUED FOR FINAL
3	6/05/15	ISSUED FOR FINAL REVIEW

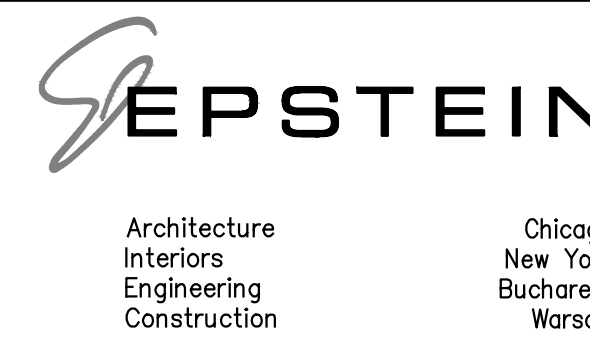
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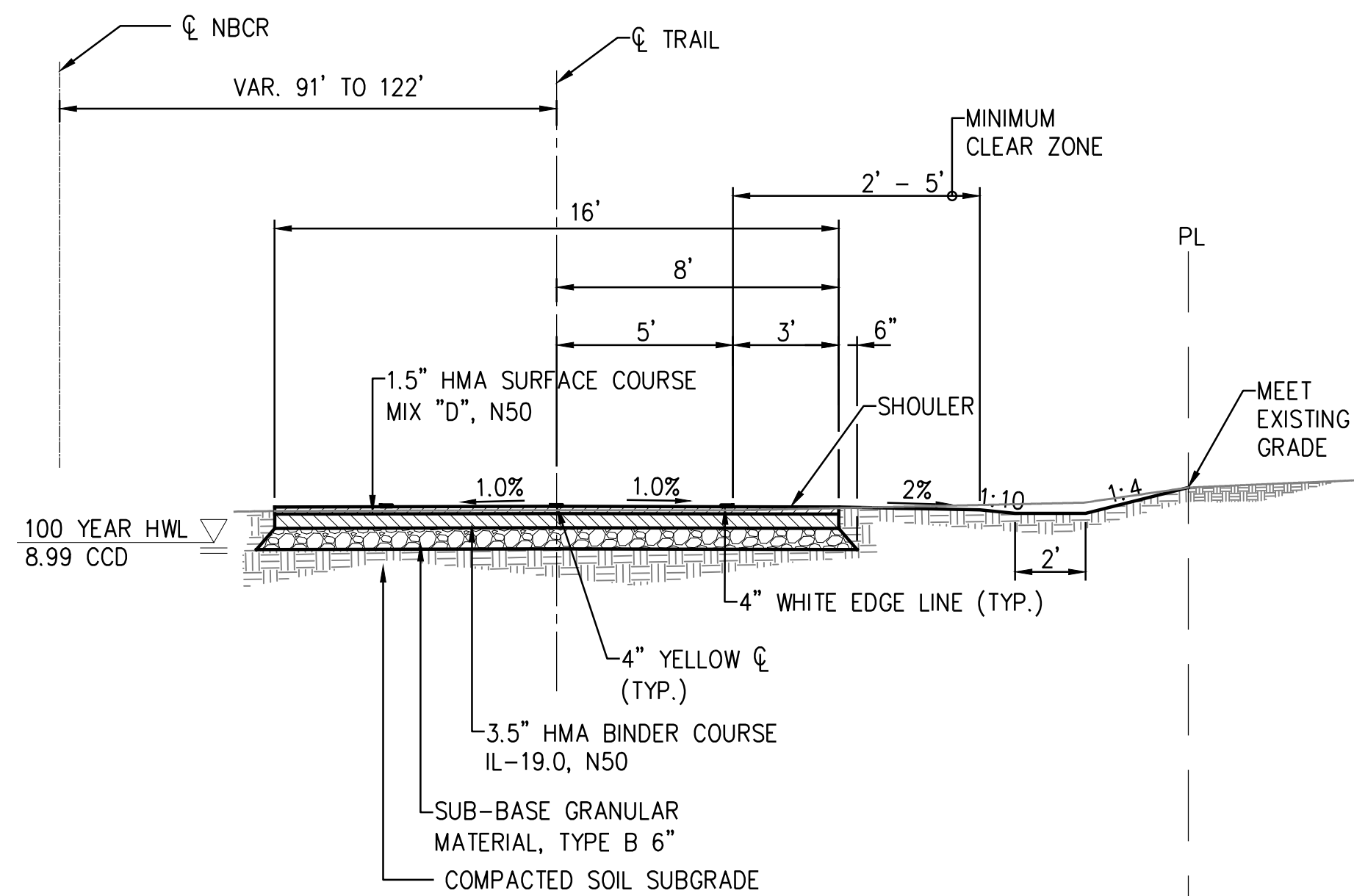
ADDISON UNDERBRIDGE TRAIL CONNECTION FROM CLARK PARK TO CALIFORNIA PARK

SPECIAL WASTE AND SOIL MANAGEMENT PLAN

SHEET NO. C-5

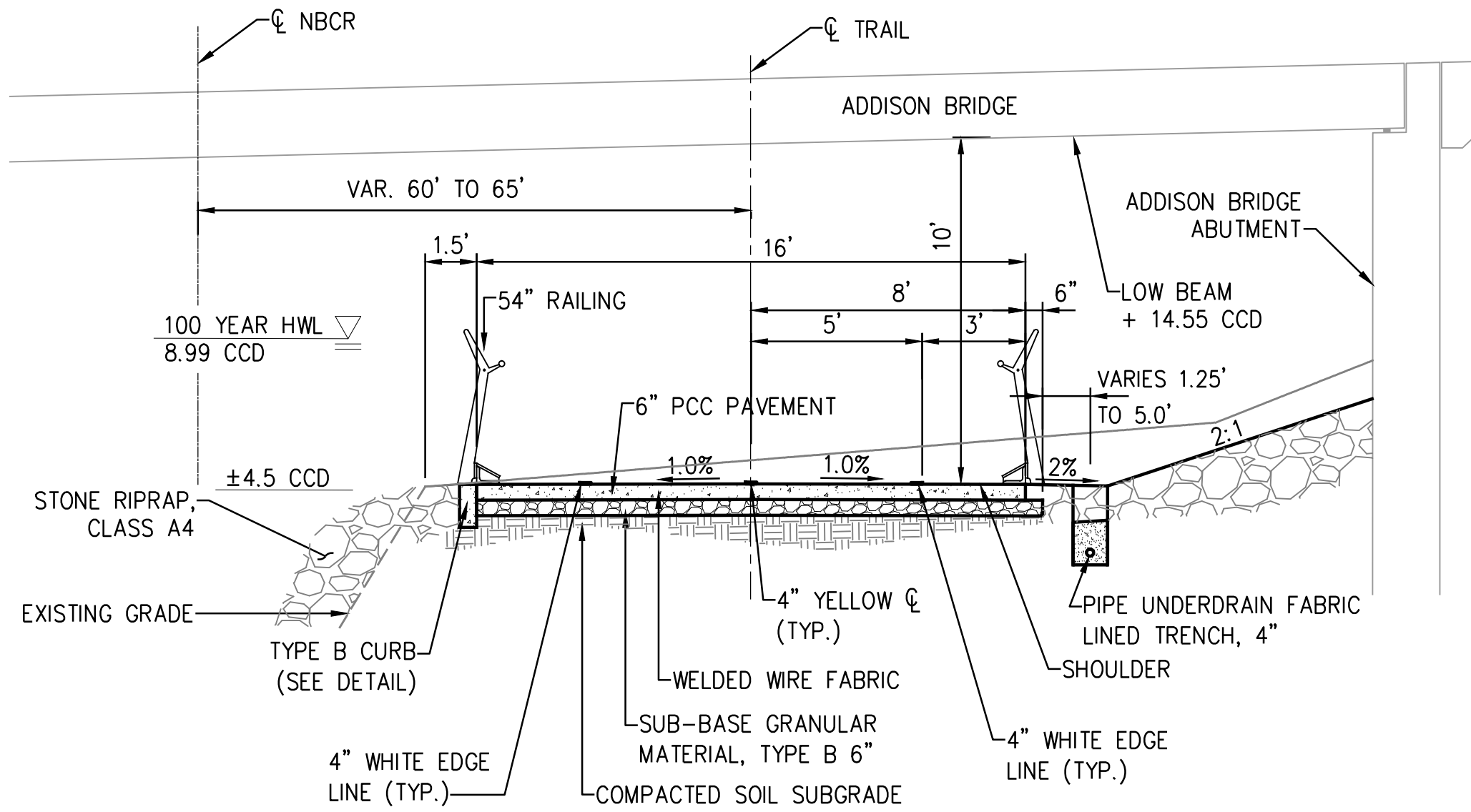
SHEET 5 OF 158

REVISION



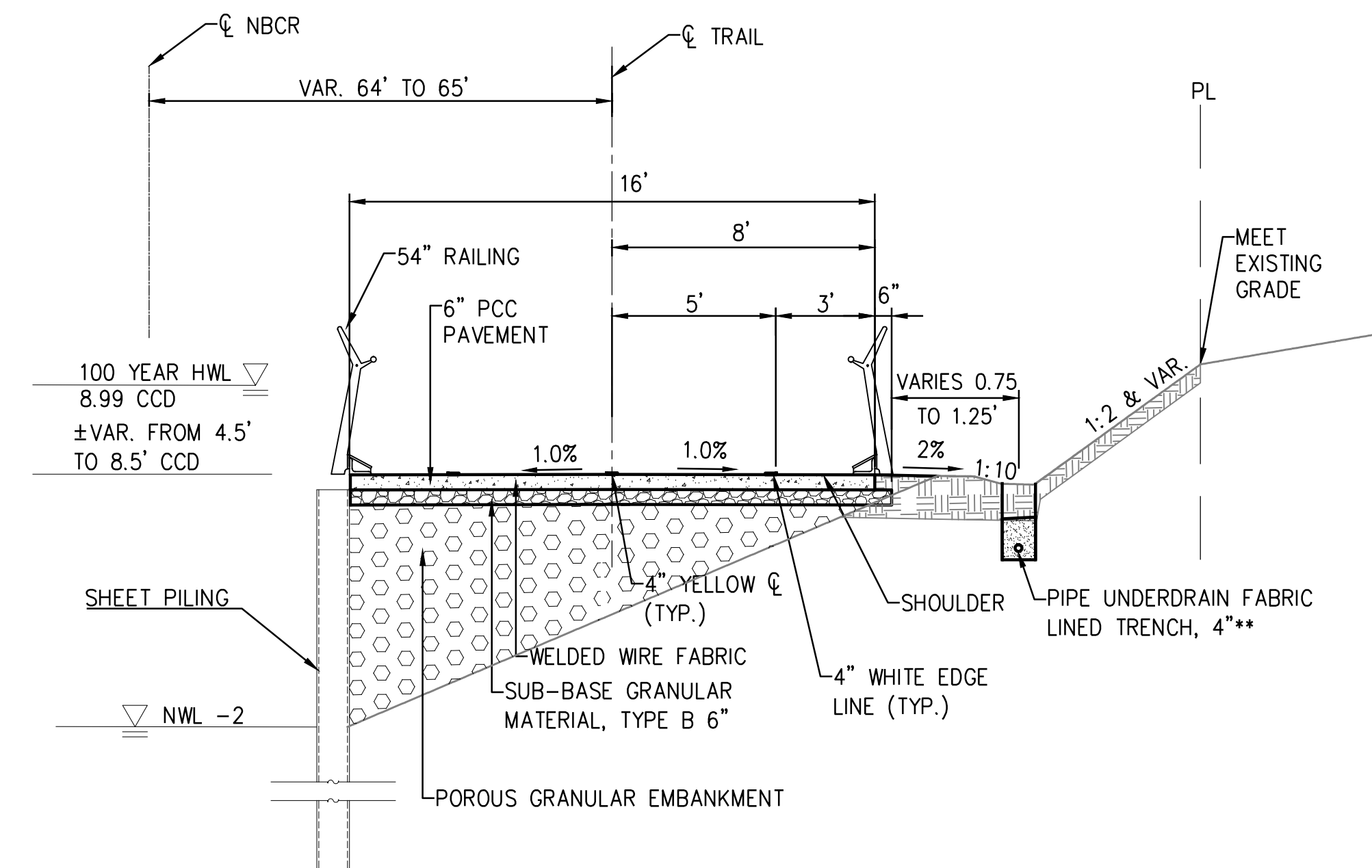
TYPICAL ON-GRADE SECTION

STA. 199+85 TO STA. 200+63.27
 *STA. 200+28 TO STA. 200+63.27 6" PCC PAVEMENT WITH WELDED WIRE FABRIC



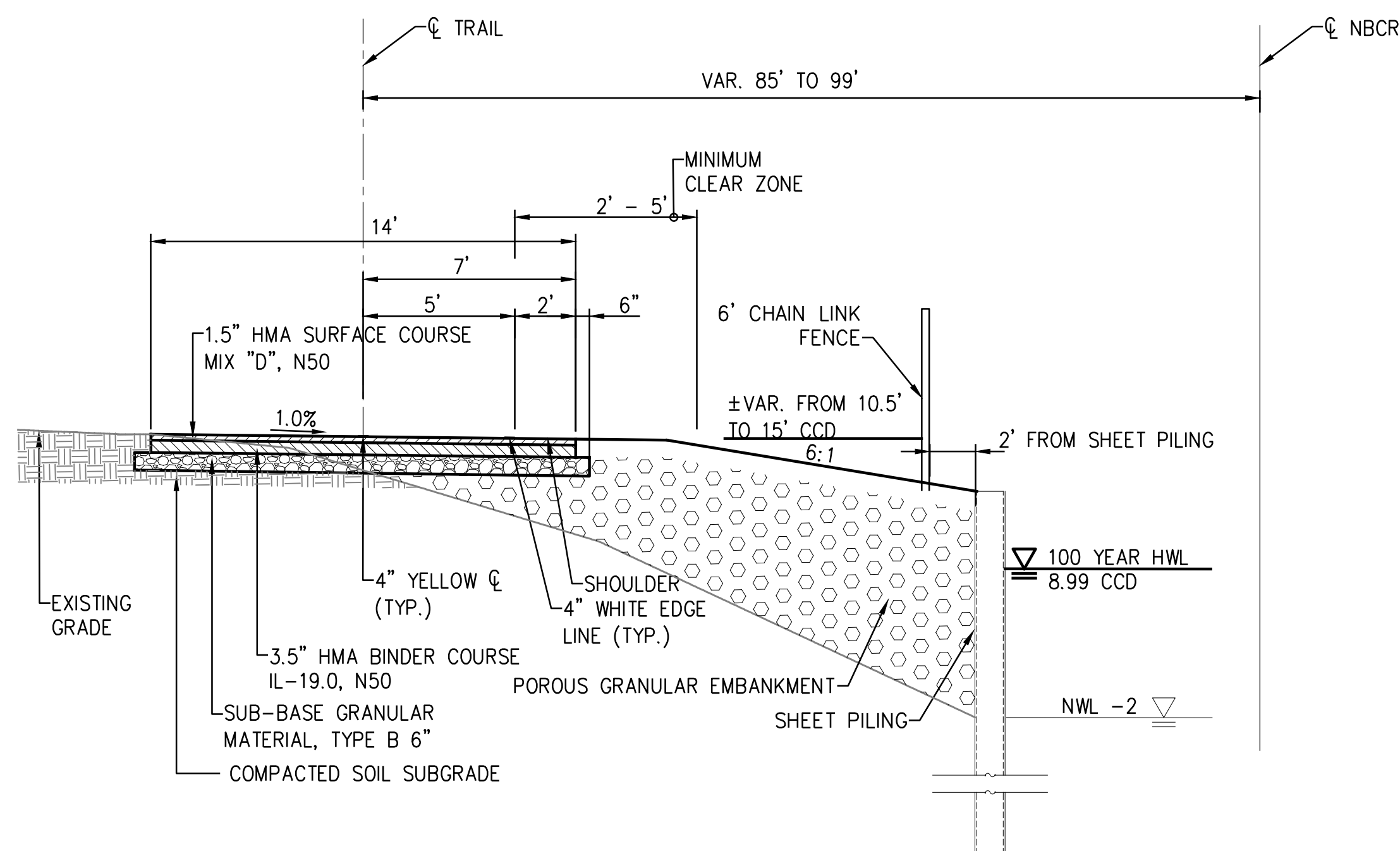
TYPICAL ON-GRADE SECTION

STA. 203+61.25 TO STA. 204+36.50



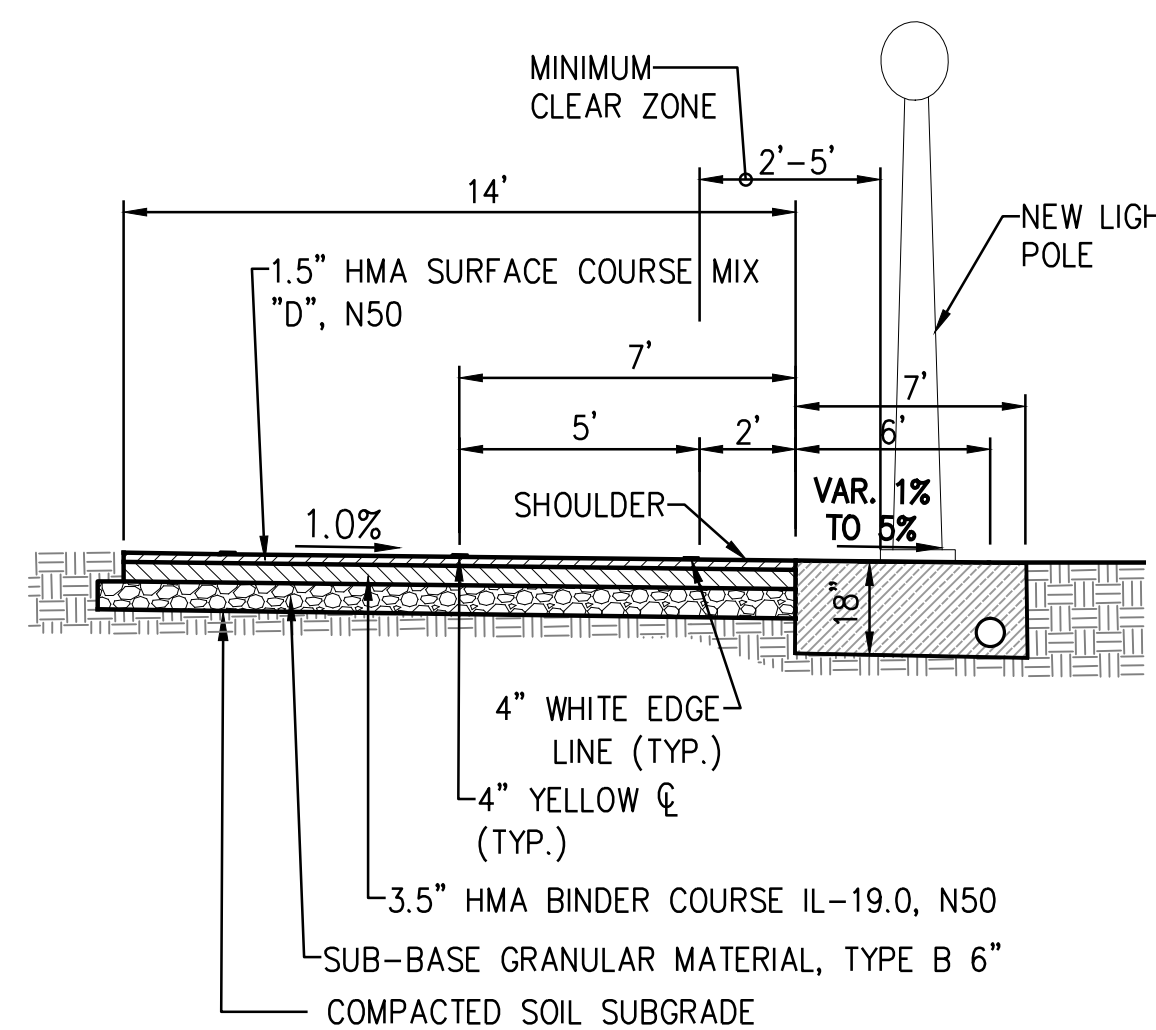
TYPICAL ON-GRADE SECTION, SHEET PILE

STA. 200+63.27 TO STA. 203+61.25
 STA. 204+36.50 TO STA. 205+46.54
 * STA. 202+83.25 TO STA. 203+25.75 BRIDGE OMISSION
 ** STA. 204+36.50 TO STA. 205+46.54



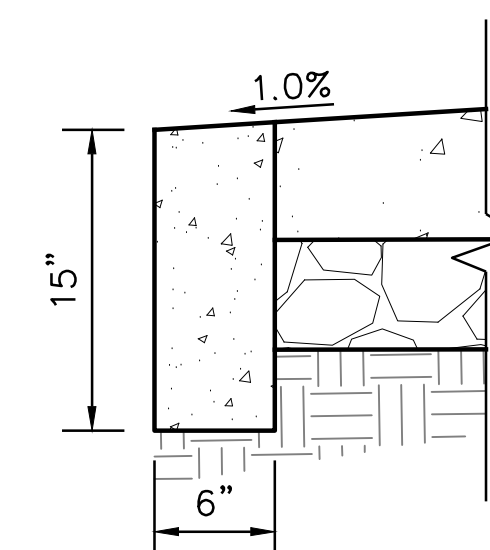
TYPICAL ON-GRADE SECTION, SHEET PILE

STA. 215+61.48 TO STA. 217+00.47
 *STA. 215+61 TO STA. 216+36 6" PCC PAVEMENT WITH WELDED WIRE FABRIC



TYPICAL ON-GRADE SECTION

STA. 0+00 TO STA. 5+24.30 (CALIFORNIA PARK)



TYPE B CURB DETAIL

FOR BRIDGE OMISSIONS SEE STRUCTURAL PLANS

MIXTURES TABLE	
MIXTURES TYPE	AIR VOIDS @ Ndes
HOT-MIX ASPHALT BINDER COURSE, IL 19.0	4% @ N50
HOT-MIX ASPHALT SURFACE COURSE, MIX D, IL 9.5	4% @ N50

THE UNIT WEIGHT USED TO CALCULATE ALL HMA SURFACE MIXTURE QUANTITIES IS 112 LBS/SQ YD/IN.

THE AC TYPE FOR NON-POLYMERIZED HMA MIXES SHALL BE PG 64-22 UNLESS MODIFIED BY DISTRICT ONE SPECIAL PROVISIONS.

FOR USE OF RECYCLED MATERIALS SEE SPECIAL PROVISIONS.

PAVEMENT MARKING TABLE		
	STATION	TOTAL FEET
THERMOPLASTIC PAVEMENT MARKING, LINE 4"	199+85-200+30 216+36-217+04 1+53.65-5+21.30 10+50-11+62	1,782'
POLYUREA PAVEMENT MARKING, LINE 4"	200+30-216+36	4,994'

NBCR=NORTH BRANCH CHICAGO RIVER
 CPD = CHICAGO PARK DISTRICT

January 24, 2016 10:00:00 AM - Addison Underbridge Connection - DISCIPLINE: Civil Working\Sheets\006_Typ_Section.dwg - CDOT - Chicago Department of Transportation

SECTION NO.: 12-E9121-00-BT	NO. DATE REVISIONS/ISSUANCES
DRAWN BY: AP	
CHECKED BY: JRK	8/12/16 ISSUED FOR FINAL
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ADDISON UNDERBRIDGE TRAIL CONNECTION FROM CLARK PARK TO CALIFORNIA PARK

TYPICAL SECTIONS

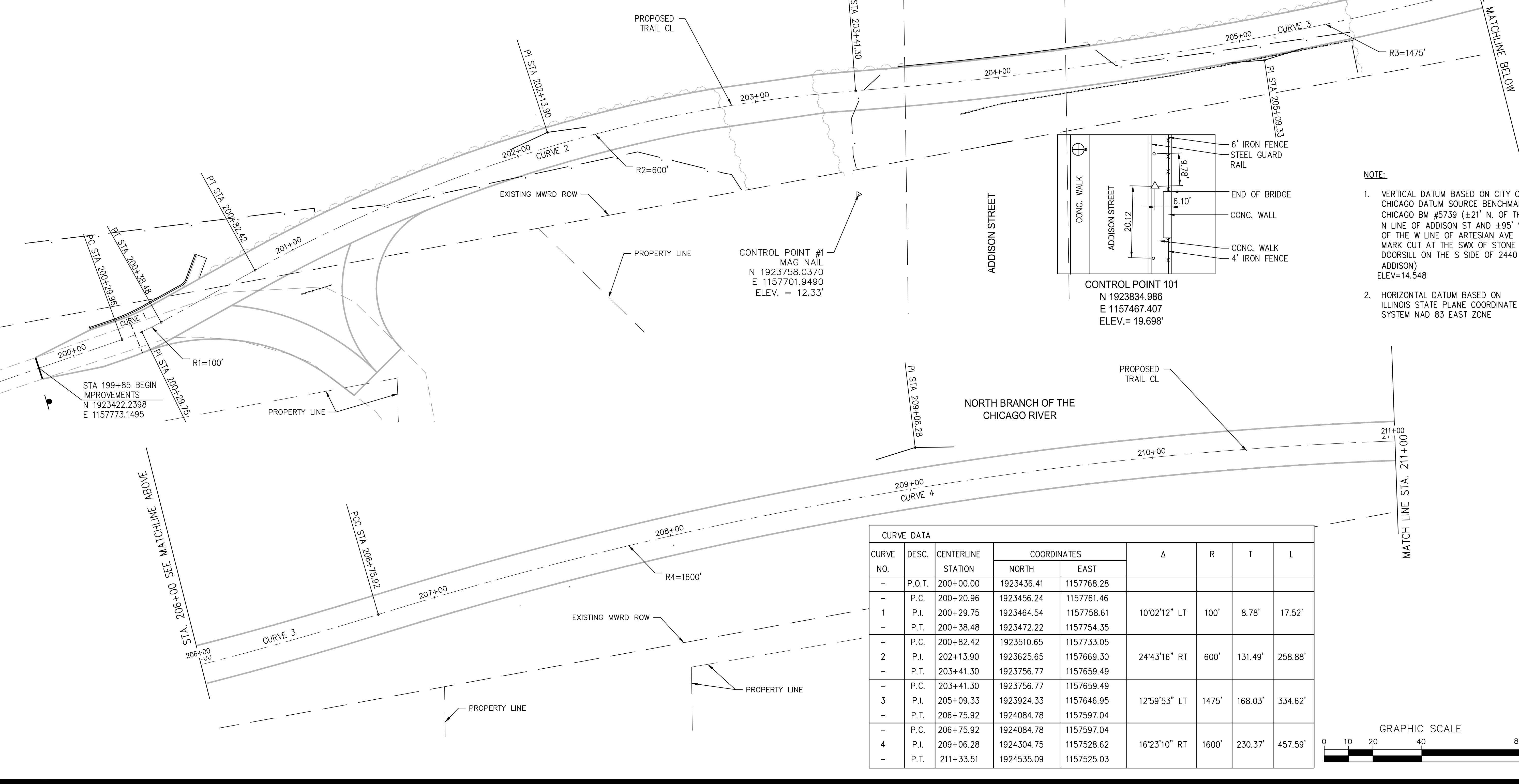
SHEET NO.

C-6

SHEET 6 OF 158

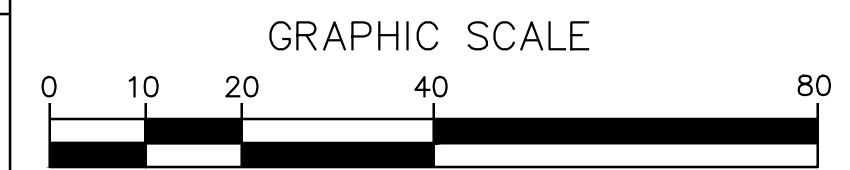
REVISION

PR CURVE 1 PI STA = 200+29.75 $\Delta = 10'02''12''$ (LT) R1=100' T=8.78' L=17.52' E=0.38' e= N/A T.R.=N/A S.E. RUN=N/A PC STA =200+20.96 PT STA =200+38.48	PR CURVE 2 PI STA = 202+13.90 $\Delta = 24'43''16''$ (RT) R2=600' T=131.49' L=258.88' E=14.24' e= N/A T.R.=N/A S.E. RUN=N/A PC STA =200+82.42 PT STA =203+41.30	PR CURVE 3 PI STA = 205+09.33 $\Delta = 12'59''53''$ (LT) R3=1475' T=168.03' L=334.62' E=9.54' e= N/A T.R.=N/A S.E. RUN=N/A P.C. STA =203+41.30 P.T. STA =206+75.92	PR CURVE 4 PI STA = 209+06.28 $\Delta = 16'23''10''$ (RT) R4=1600' T=230.37' L=457.59' E=16.50' e= N/A T.R.=N/A S.E. RUN=N/A P.C. STA =206+75.92 P.T. STA =211+33.51
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- NOTE:**
- VERTICAL DATUM BASED ON CITY OF CHICAGO DATUM SOURCE BENCHMARK CHICAGO BM #5739 ($\pm 21'$ N. OF THE N LINE OF ADDISON ST AND $\pm 95'$ W OF THE W LINE OF ARTESIAN AVE MARK CUT AT THE SWX OF STONE DOORSILL ON THE S SIDE OF 2440 W ADDISON) ELEV=14.548
 - HORIZONTAL DATUM BASED ON ILLINOIS STATE PLANE COORDINATE SYSTEM NAD 83 EAST ZONE

CURVE NO.	DESC.	CENTERLINE STATION	COORDINATES		Δ	R	T	L
			NORTH	EAST				
-	P.O.T.	200+00.00	1923436.41	1157768.28				
-	P.C.	200+20.96	1923456.24	1157761.46				
1	P.I.	200+29.75	1923464.54	1157758.61	10'02''12'' LT	100'	8.78'	17.52'
-	P.T.	200+38.48	1923472.22	1157754.35				
-	P.C.	200+82.42	1923510.65	1157733.05				
2	P.I.	202+13.90	1923625.65	1157669.30	24'43''16'' RT	600'	131.49'	258.88'
-	P.T.	203+41.30	1923756.77	1157659.49				
-	P.C.	203+41.30	1923756.77	1157659.49				
3	P.I.	205+09.33	1923924.33	1157646.95	12'59''53'' LT	1475'	168.03'	334.62'
-	P.T.	206+75.92	1924084.78	1157597.04				
-	P.C.	206+75.92	1924084.78	1157597.04				
4	P.I.	209+06.28	1924304.75	1157528.62	16'23''10'' RT	1600'	230.37'	457.59'
-	P.T.	211+33.51	1924535.09	1157525.03				



January 24, 2016 10:00:00 AM - Addison Underbridge Connection - DISCIPLINE: Civil Working\Sheets\007 - Alignment - Title 1.dwg

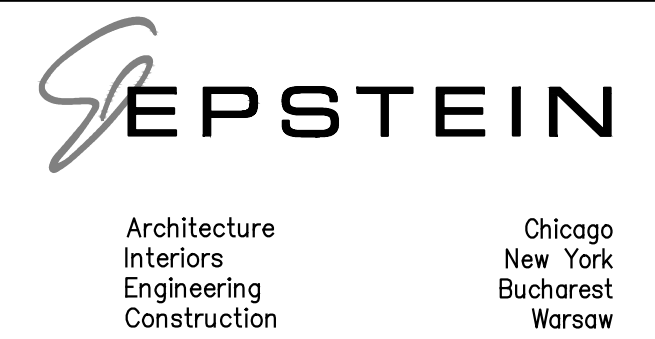
SECTION NO.: 12-E9121-00-BT
 DRAWN BY: AP
 CHECKED BY: JRK
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 DATE: 8/12/2016
 SCALE: 1"=20'

NO. DATE REVISIONS/ISSUANCES

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ADDISON UNDERBRIDGE TRAIL CONNECTION FROM CLARK PARK TO CALIFORNIA PARK

ALIGNMENT AND TIES

SHEET NO. C-7
 SHEET 7 OF 158

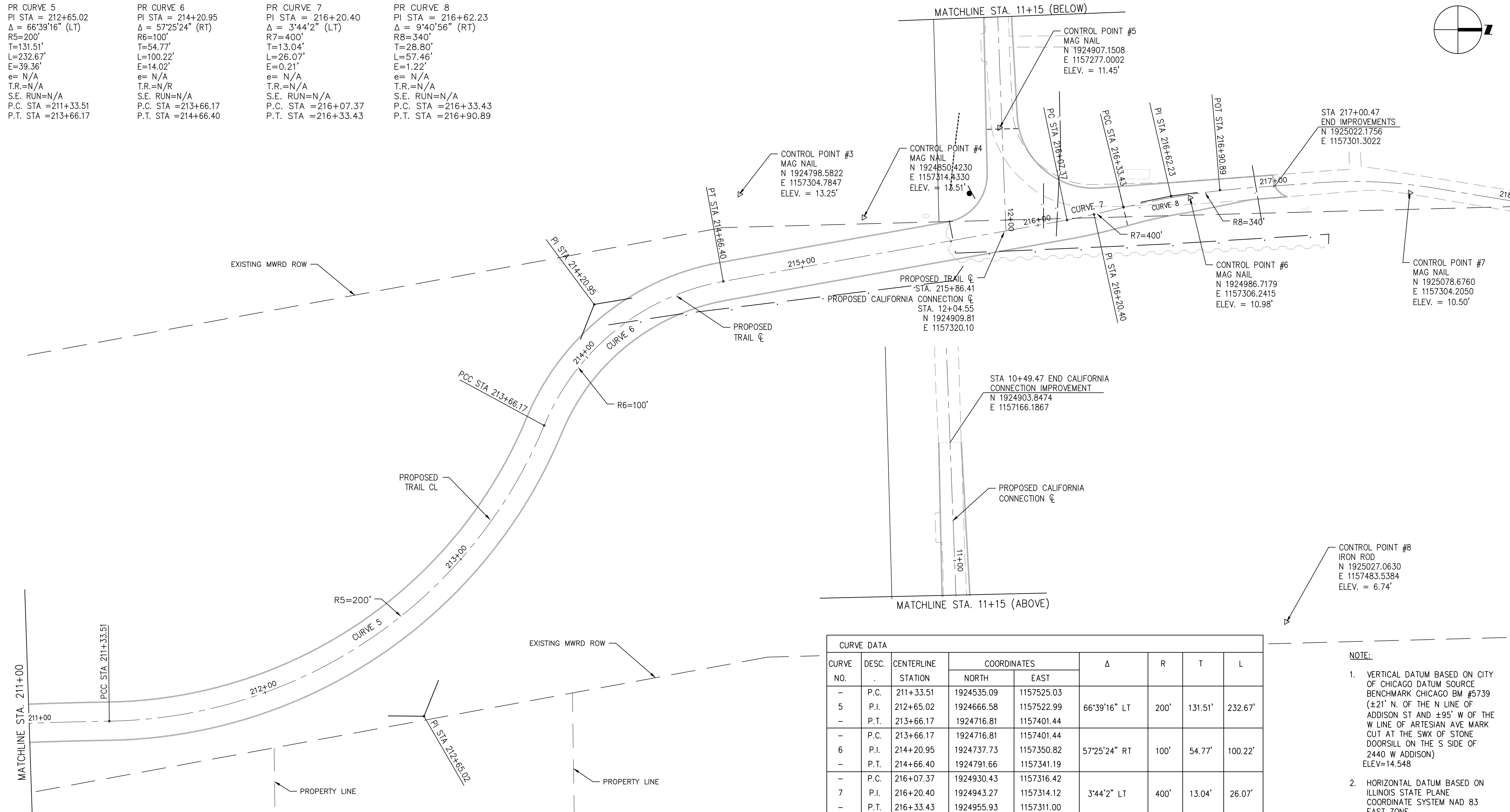
REVISION

PR CURVE 5
 PI STA = 212+65.02
 $\Delta = 66^{\circ}39'16''$ (LT)
 R5=200'
 T=131.51'
 L=232.67'
 E=39.36'
 e= N/A
 T.R.=N/A
 S.E. RUN=N/A
 P.C. STA =211+33.51
 P.T. STA =213+66.17

PR CURVE 6
 PI STA = 214+20.95
 $\Delta = 57^{\circ}25'24''$ (RT)
 R6=100'
 T=54.77'
 L=100.22'
 E=14.02'
 e= N/A
 T.R.=N/R
 S.E. RUN=N/A
 P.C. STA =213+66.17
 P.T. STA =214+66.40

PR CURVE 7
 PI STA = 216+07.37
 $\Delta = 3^{\circ}44'2''$ (LT)
 R7=400'
 T=13.04'
 L=26.07'
 E=0.21'
 e= N/A
 T.R.=N/A
 S.E. RUN=N/A
 P.C. STA =216+07.37
 P.T. STA =216+33.43

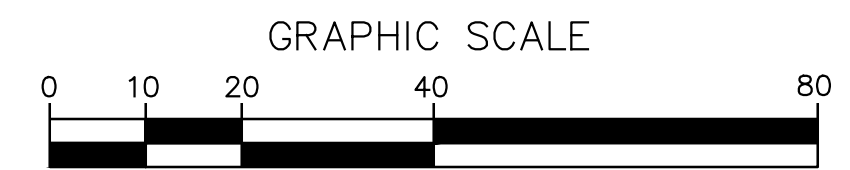
PR CURVE 8
 PI STA = 216+62.23
 $\Delta = 9^{\circ}40'56''$ (RT)
 R8=340'
 T=28.80'
 L=57.46'
 E=1.22'
 e= N/A
 T.R.=N/A
 S.E. RUN=N/A
 P.C. STA =216+62.23
 P.T. STA =217+00.89



CURVE NO.	DESC.	CENTERLINE STATION	COORDINATES		Δ	R	T	L
			NORTH	EAST				
-	P.C.	211+33.51	1924535.09	1157525.03				
5	P.I.	212+65.02	1924666.58	1157522.99	66°39'16" LT	200'	131.51'	232.67'
-	P.T.	213+66.17	1924716.81	1157401.44				
-	P.C.	213+66.17	1924716.81	1157401.44				
6	P.I.	214+20.95	1924737.73	1157350.82	57°25'24" RT	100'	54.77'	100.22'
-	P.T.	214+66.40	1924791.66	1157341.19				
-	P.C.	216+07.37	1924930.43	1157316.42				
7	P.I.	216+20.40	1924943.27	1157314.12	3°44'2" LT	400'	13.04'	26.07'
-	P.T.	216+33.43	1924955.93	1157311.00				
-	P.C.	216+33.43	1924955.93	1157311.00				
8	P.I.	216+62.23	1924983.88	1157304.10	9°40'56" RT	340'	28.80'	57.46'
-	P.T.	216+90.89	1925012.60	1157302.01				
-	P.O.T.	217+00.47	1925022.18	1157301.30				

NOTE:

- VERTICAL DATUM BASED ON CITY OF CHICAGO DATUM SOURCE BENCHMARK CHICAGO BM #5739 ($\pm 21'$ N. OF THE N LINE OF ADDISON ST AND $\pm 95'$ W OF THE W LINE OF ARTESIAN AVE MARK CUT AT THE SWX OF STONE DOORSILL ON THE S SIDE OF 2440 W ADDISON) ELEV=14.548
- HORIZONTAL DATUM BASED ON ILLINOIS STATE PLANE COORDINATE SYSTEM NAD 83 EAST ZONE



January 24, 2016 10:00:00 AM - Addison Underbridge Connection - CDOT - DISCIPLINE (Civil) Working\Sheets\008_Alignment_Ties_2.dwg

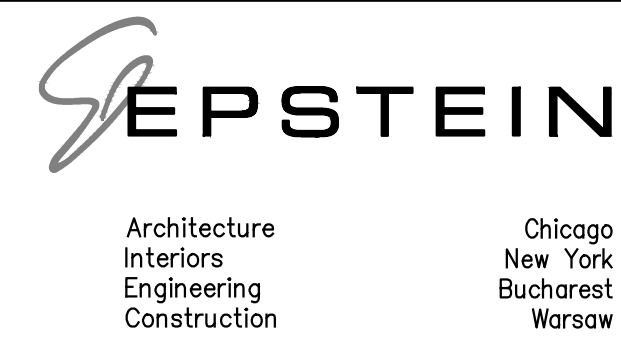
SECTION NO.: 12-E9121-00-BT
 DRAWN BY: AP
 CHECKED BY: JRK
 APPROVED BY: GAO
 DATE: 8/12/2016
 SCALE: 1"=20'

NO.	DATE	REVISIONS/ISSUANCES
1	8/12/16	ISSUED FOR FINAL
2	5/6/16	ISSUED FOR FINAL
3	6/05/15	ISSUED FOR FINAL REVIEW

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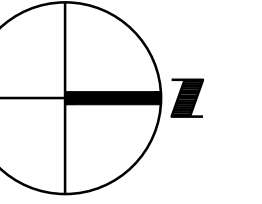


ADDISON UNDERBRIDGE TRAIL CONNECTION FROM CLARK PARK TO CALIFORNIA PARK

ALIGNMENT AND TIES

SHEET NO. **C-8**
 SHEET 8 OF 158

REVISION



PR CURVE 9
 PI STA = 1+12.79
 $\Delta = 28^{\circ}23'11''$ (LT)
 R9=100'
 T=25.29'
 L=49.04'
 E=3.15'
 e= N/A
 T.R.=N/A
 S.E. RUN=N/A
 P.C. STA =0+87.50
 P.T. STA =1+37.52

PR CURVE 10
 PI STA = 2+18.84
 $\Delta = 60^{\circ}44'07''$ (RT)
 R10=100'
 T=58.59'
 L=101.11'
 E=15.90'
 e= N/A
 T.R.=N/A
 S.E. RUN=N/A
 P.C. STA =1+60.22
 P.T. STA =2+66.22

PR CURVE 11
 PI STA = 2+93.42
 $\Delta = 30^{\circ}24'42''$ (LT)
 R11=100'
 T=27.18'
 L=52.46'
 E=3.63'
 e= N/A
 T.R.=N/A
 S.E. RUN=N/A
 P.C. STA =2+66.22
 P.T. STA =3+19.30

- NOTE:
- VERTICAL DATUM BASED ON CITY OF CHICAGO DATUM SOURCE BENCHMARK CHICAGO BM #5739 ($\pm 21'$ N. OF THE N LINE OF ADDISON ST AND $\pm 95'$ W OF THE W LINE OF ARTESIAN AVE MARK CUT AT THE SWX OF STONE DOORSILL ON THE S SIDE OF 2440 W ADDISON) ELEV=14.548
 - HORIZONTAL DATUM BASED ON ILLINOIS STATE PLANE COORDINATE SYSTEM NAD 83 EAST ZONE

PROPOSED TRAIL CL

PC STA 0+87.50

PT STA 1+37.52

PC STA 1+60.22

PI STA 2+18.84

PI STA 1+12.79

CONTROL POINT #9
 N 1925789.9610
 E 1157558.0600
 ELEV. = 11.19'

STA 1+59.30 BEGIN IMPROVEMENTS
 N 1926127.1501
 E 1157732.8207

EXISTING MWRD ROW

R10=100'

PCC STA 2+66.22

R11=100'

PT STA 3+19.30

PROPOSED TRAIL CL

NORTH BRANCH OF THE CHICAGO RIVER

CONTROL POINT #10
 N 1926067.3300
 E 1157646.3840
 ELEV. = 12.92'

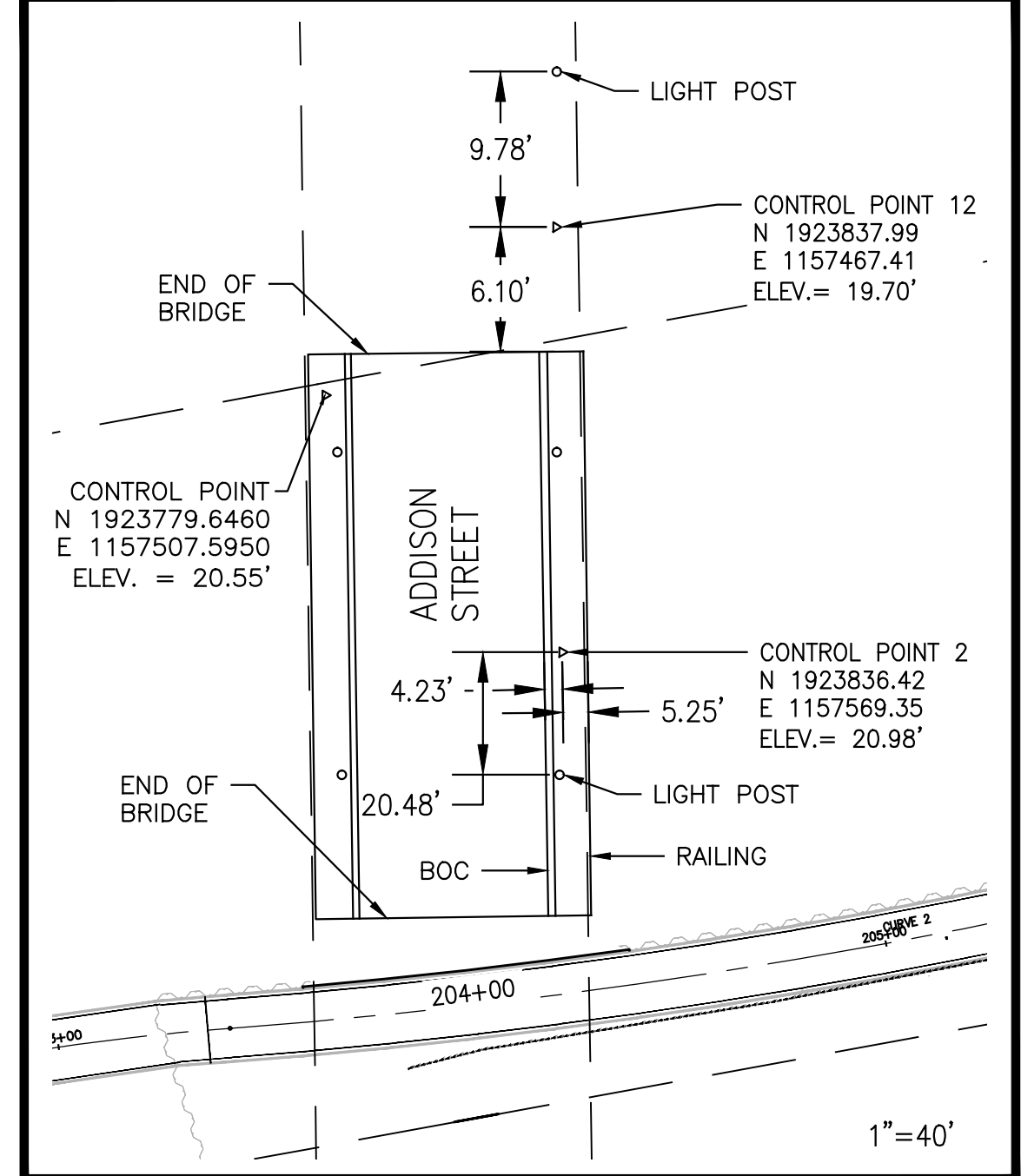
CONTROL POINT #11
 MAG NAIL
 N 1926131.1610
 E 1157729.0100
 ELEV. = 13.46'

STA 5+00.00 END IMPROVEMENTS
 N 1926127.1501
 E 1157732.8207

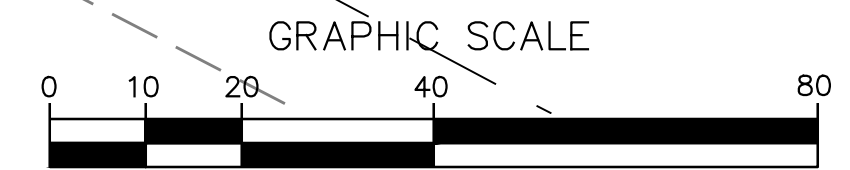
PROPERTY LINE

POT STA 5+21.30

EXISTING MWRD ROW



CURVE DATA								
CURVE NO.	DESC.	CENTERLINE STATION	COORDINATES		Δ	R	T	L
			NORTH	EAST				
-	P.O.T.	0+00.00	1925673.56	1157502.99				
-	P.C.	0+87.50	1925751.82	1157542.12				
9	P.I.	1+12.79	1925774.44	1157553.43	28°23'11" LT	100'	25.29'	49.04'
-	P.T.	1+37.52	1925799.72	1157552.62				
-	P.C.	1+60.22	1925822.91	1157551.88				
10	P.I.	2+18.84	1925881.48	1157550.02	60°44'07" RT	100'	58.59'	101.11'
-	P.T.	2+66.22	1925911.73	1157600.20				
-	P.C.	2+66.22	1925911.73	1157600.20				
11	P.I.	2+93.43	1925925.77	1157623.48	30°24'42" LT	100'	27.18'	52.46'
-	P.T.	3+19.30	1925949.65	1157600.20				
-	P.O.T.	5+21.30	1926127.15	1157732.82				



January 24, 2016 10:00:00 AM - Addison Underbridge Connection - DISCIPLINE: Civil Working\Sheets\009_Alignment_Ties_3.dwg

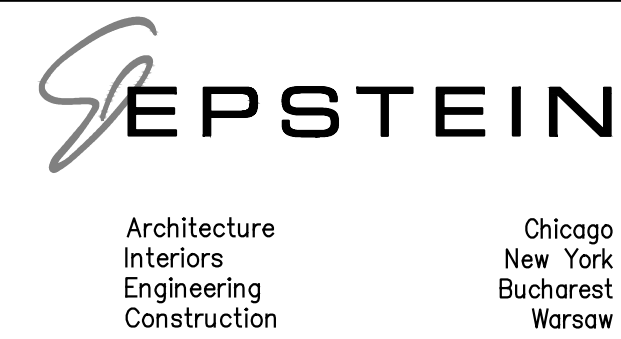
SECTION NO.: 12-E9121-00-BT
 DRAWN BY: AP
 CHECKED BY: JRK
 APPROVED BY: GAO
 DATE: 8/12/2016
 SCALE: 1"=20'

NO.	DATE	REVISIONS/ISSUANCES
1	8/12/16	ISSUED FOR FINAL
2	5/6/16	ISSUED FOR FINAL
3	6/05/15	ISSUED FOR FINAL REVIEW

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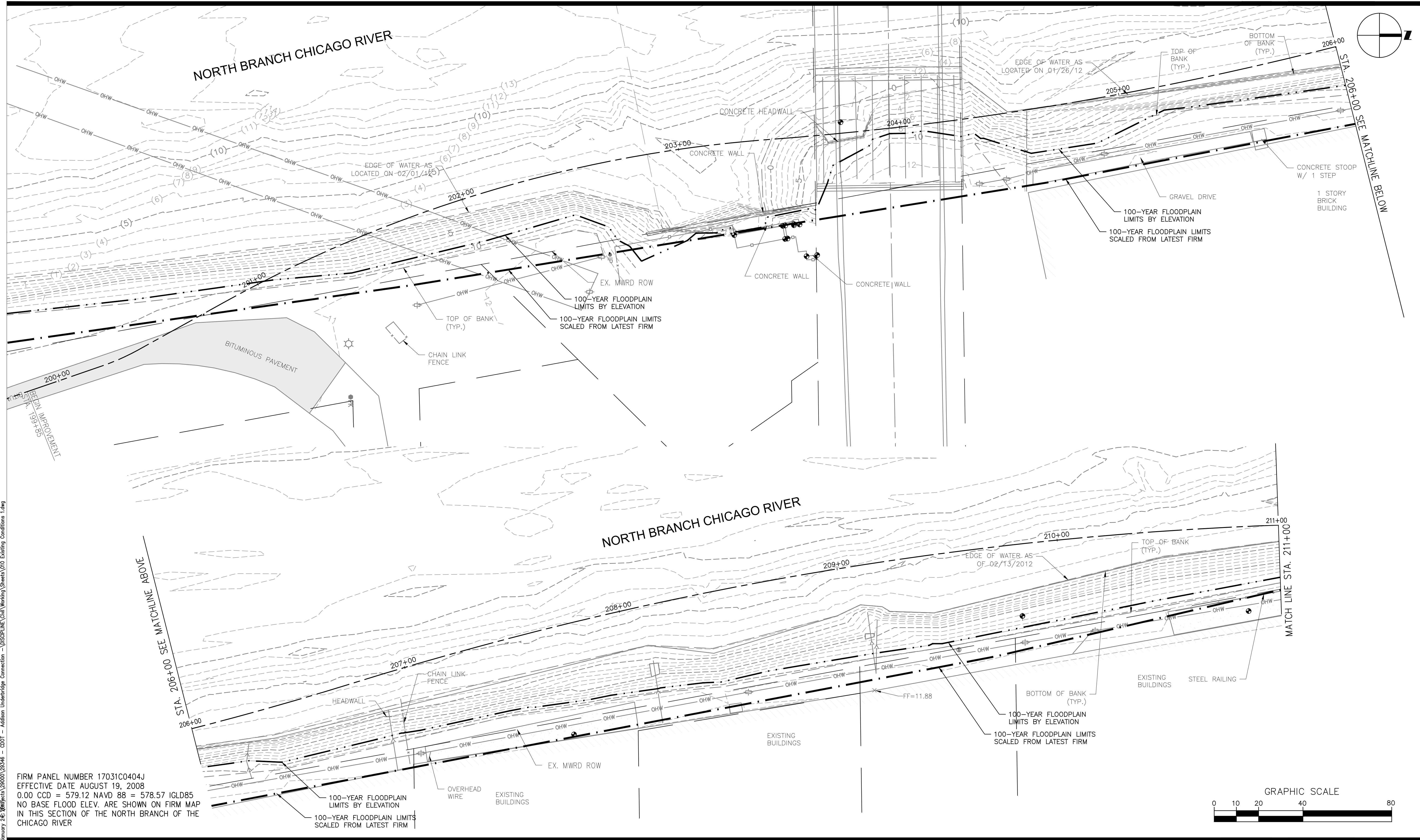
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ADDISON UNDERBRIDGE TRAIL CONNECTION FROM CLARK PARK TO CALIFORNIA PARK
 ALIGNMENT AND TIES

SHEET NO. C-9
 SHEET 9 OF 158

REVISION



FIRM PANEL NUMBER 17031C0404J
 EFFECTIVE DATE AUGUST 19, 2008
 0.00 CCD = 579.12 NAVD 88 = 578.57 IGLD85
 NO BASE FLOOD ELEV. ARE SHOWN ON FIRM MAP
 IN THIS SECTION OF THE NORTH BRANCH OF THE
 CHICAGO RIVER

SECTION NO.: 12-E9121-00-BT
 DRAWN BY: JRK
 CHECKED BY: JRK
 APPROVED BY: GAO
 DATE: 8/12/2016
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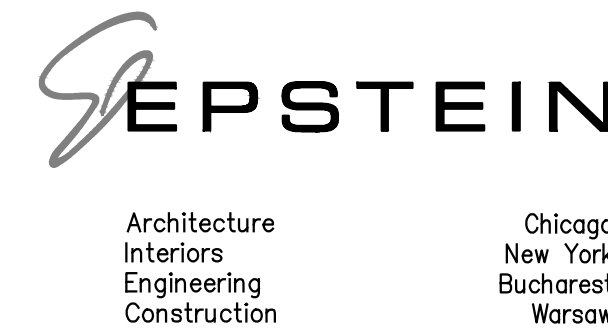
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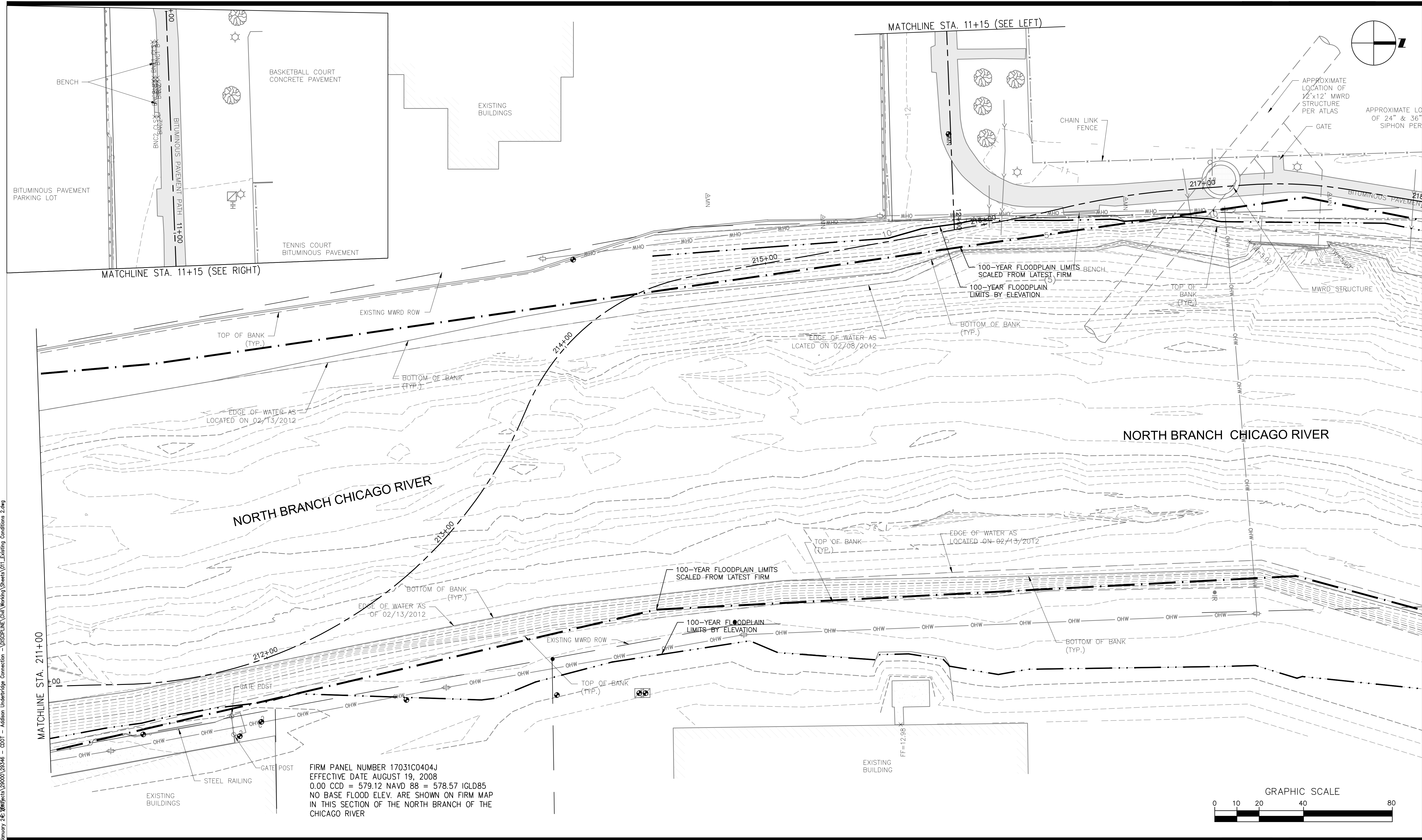
ADDISON UNDERBRIDGE TRAIL CONNECTION FROM
 CLARK PARK TO CALIFORNIA PARK

EXISTING CONDITIONS PLAN STA 200+00 TO STA 211+00

SHEET NO.
C-10

SHEET 10 OF 158

REVISION



January 24, 2016 10:00 AM - Addison Underbridge Connection - DISCIPLINE\Civil\Working\Sheets\011_Existing Conditions 2.dwg

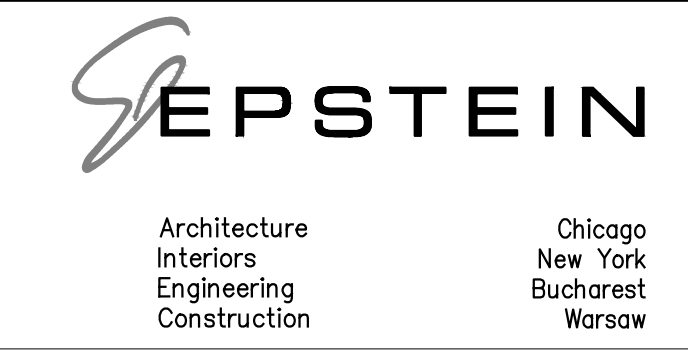
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 DRAWN BY: JRK
 CHECKED BY: JRK
 APPROVED BY: GAO
 DATE: 8/12/2016
 SCALE: 1" = 20'

NO.	DATE	REVISIONS/ISSUANCES
1	8/12/16	ISSUED FOR FINAL
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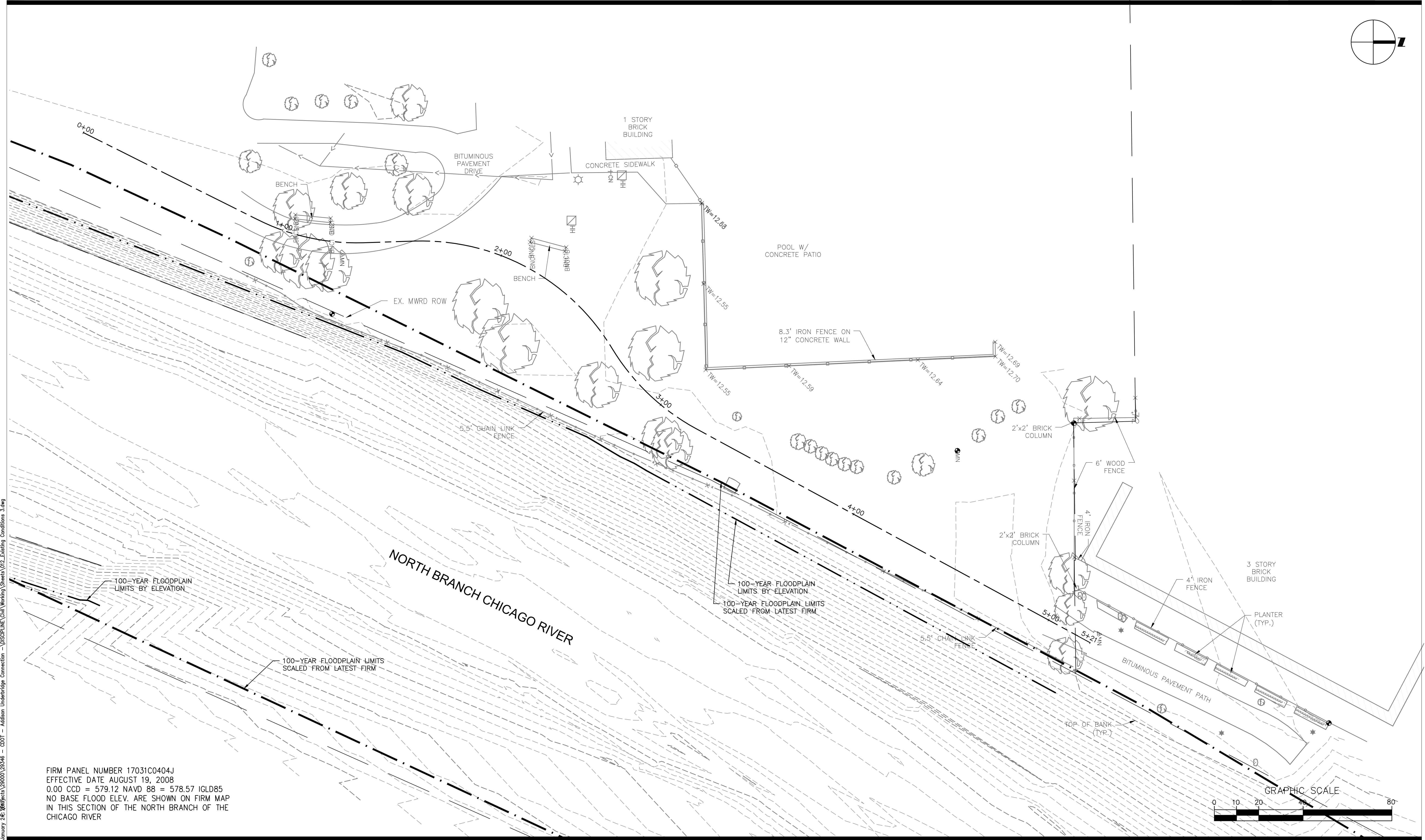
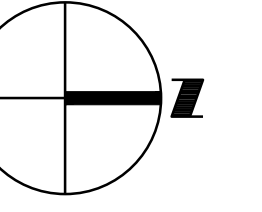
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ADDISON UNDERBRIDGE TRAIL CONNECTION FROM
 CLARK PARK TO CALIFORNIA PARK
 EXISTING CONDITIONS PLAN STA 211+00 TO STA 218+00

SHEET NO. C-11
 SHEET 11 OF 158

REVISION



FIRM PANEL NUMBER 17031C0404J
 EFFECTIVE DATE AUGUST 19, 2008
 0.00 CCD = 579.12 NAVD 88 = 578.57 IGLD85
 NO BASE FLOOD ELEV. ARE SHOWN ON FIRM MAP
 IN THIS SECTION OF THE NORTH BRANCH OF THE
 CHICAGO RIVER

January 24, 2016 10:00 AM - Addison Underbridge Connection - DISCIPLINE: Civil Working Sheets\012_Existing Conditions 3.dwg - CDOT - Addison Underbridge Connection

SECTION NO.: 12-E9121-00-BT
 DRAWN BY: JRK
 CHECKED BY: JRK
 APPROVED BY: GAO
 DATE: 8/12/2016
 SCALE: 1" = 20'

NO.	DATE	REVISIONS/ISSUANCES
1	8/12/16	ISSUED FOR FINAL
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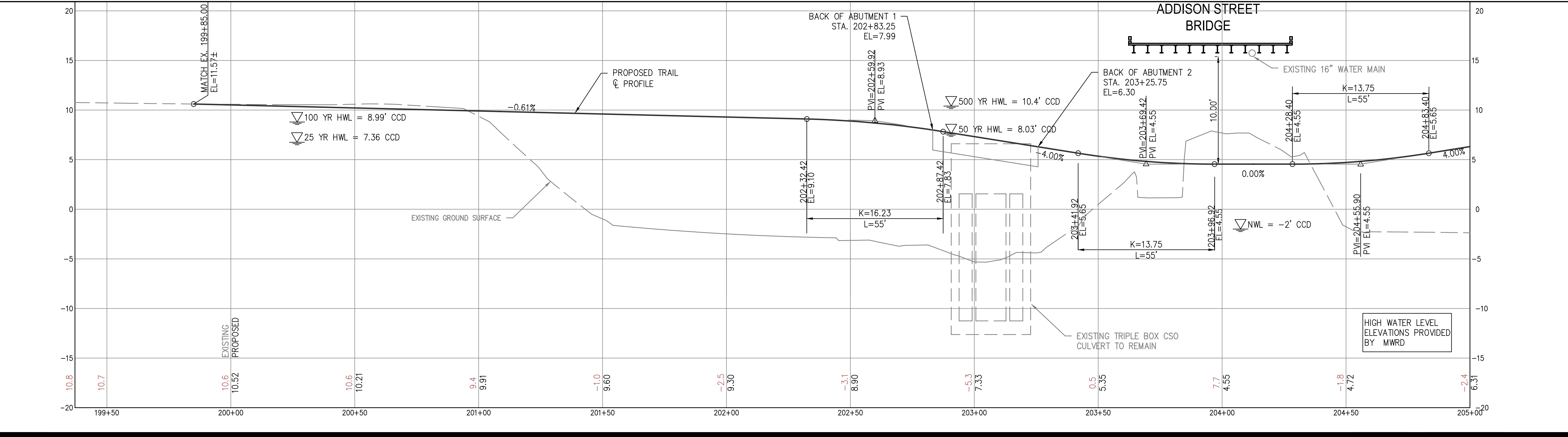
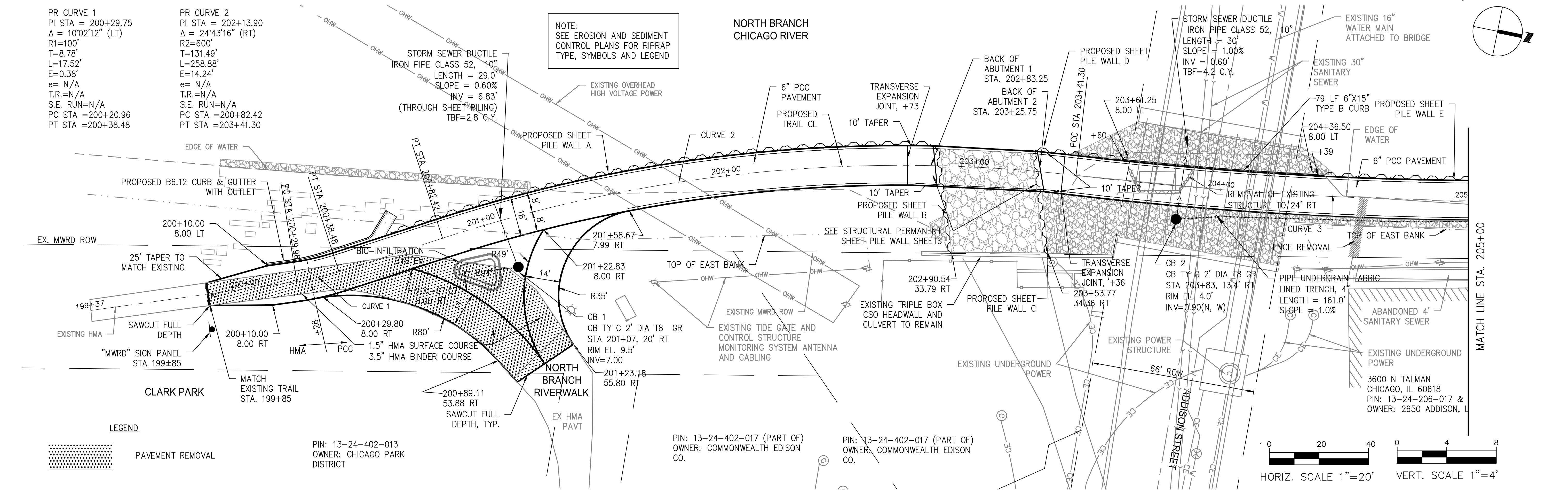
ADDISON UNDERBRIDGE TRAIL CONNECTION FROM
 CLARK PARK TO CALIFORNIA PARK

EXISTING CONDITIONS PLAN CALIFORNIA PARK

SHEET NO.
C-12

SHEET 12 OF 158

REVISION



January 24, 2016 09:30:00 (V2000) 25346 - CDOT - Addison Underbridge Connection - VISCP\LINE\Civil\Working\Sheets\013_Plan_Profile.dwg

SECTION NO.:	12-E9121-00-BT
DRAWN BY:	AP
CHECKED BY:	JRK
APPROVED BY:	GAO
DATE:	8/12/2016
SCALE:	1" = 20'

NO.	DATE	REVISIONS/ISSUANCES
1	8/12/16	ISSUED FOR FINAL
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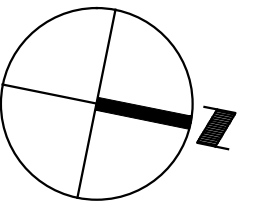
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ADDISON UNDERBRIDGE TRAIL CONNECTION FROM CLARK PARK TO CALIFORNIA PARK		SHEET NO. C-13	REVISION
PLAN & PROFILE STA 199+85 TO STA 205+00		SHEET 13 OF 158	

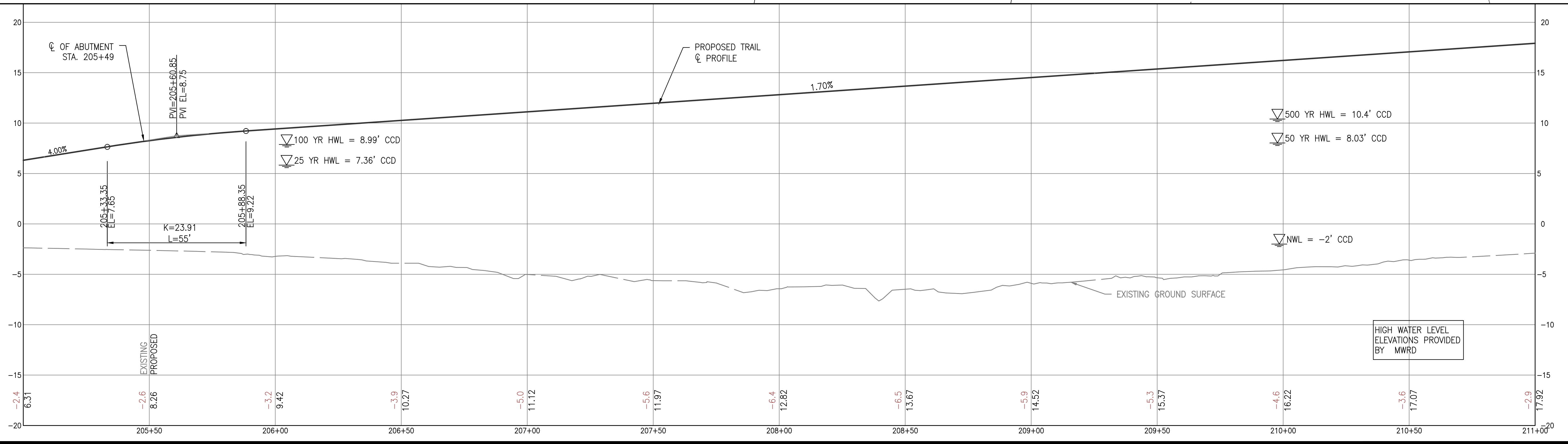
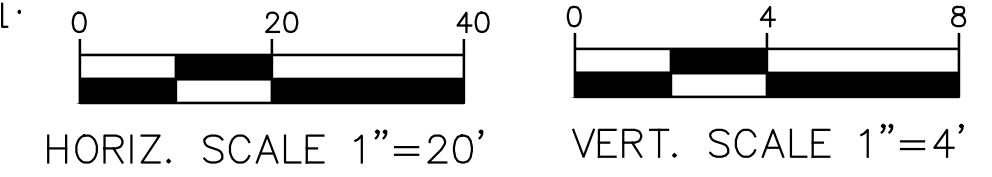
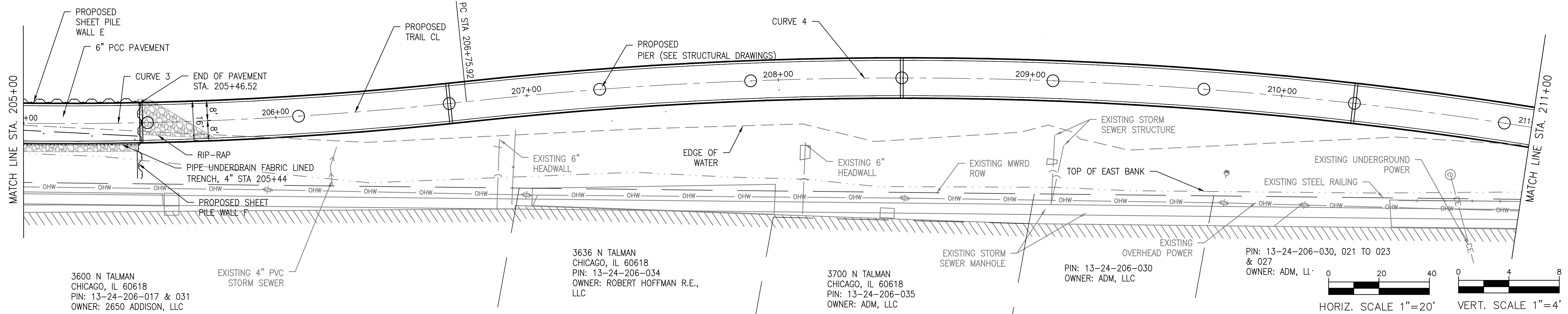
PR CURVE 3
 PI STA = 205+09.33
 $\Delta = 12^{\circ}59'53''$ (LT)
 $R3=1475'$
 $T=168.03'$
 $L=334.62'$
 $E=9.54'$
 $e= N/A$
 $T.R.=N/A$
 $S.E. RUN=N/A$
 $P.C. STA =203+41.30$
 $P.T. STA =206+75.92$

PR CURVE 4
 PI STA = 209+06.28
 $\Delta = 16^{\circ}23'10''$ (RT)
 $R4=1600'$
 $T=230.37'$
 $L=457.59'$
 $E=16.50'$
 $e= N/A$
 $T.R.=N/A$
 $S.E. RUN=N/A$
 $P.C. STA =206+75.92$
 $P.T. STA =211+33.51$

NOTE:
 SEE EROSION AND SEDIMENT
 CONTROL PLANS FOR RIPRAP
 TYPE, SYMBOLS AND LEGEND



NORTH BRANCH
 CHICAGO RIVER



January 24, 2016 12:00:00 PM - Addisom Underbridge Connection - CDOT - Addisom Underbridge Connection - \\\\discipline\civil\working\sheds\014_Plan_Profile2.dwg

SECTION NO.: 12-E9121-00-BT
 DRAWN BY: AP
 CHECKED BY: JRK
 APPROVED BY: GAO
 DATE: 8/12/2016
 SCALE: 1"=20'

NO.	DATE	REVISIONS/ISSUANCES
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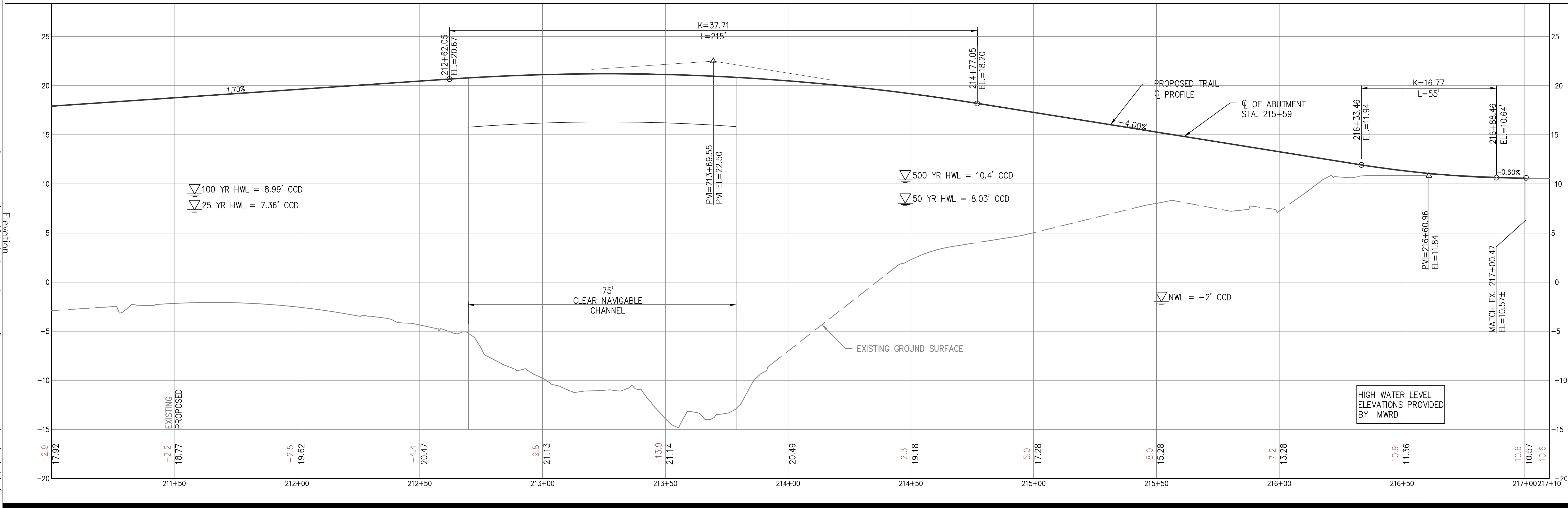
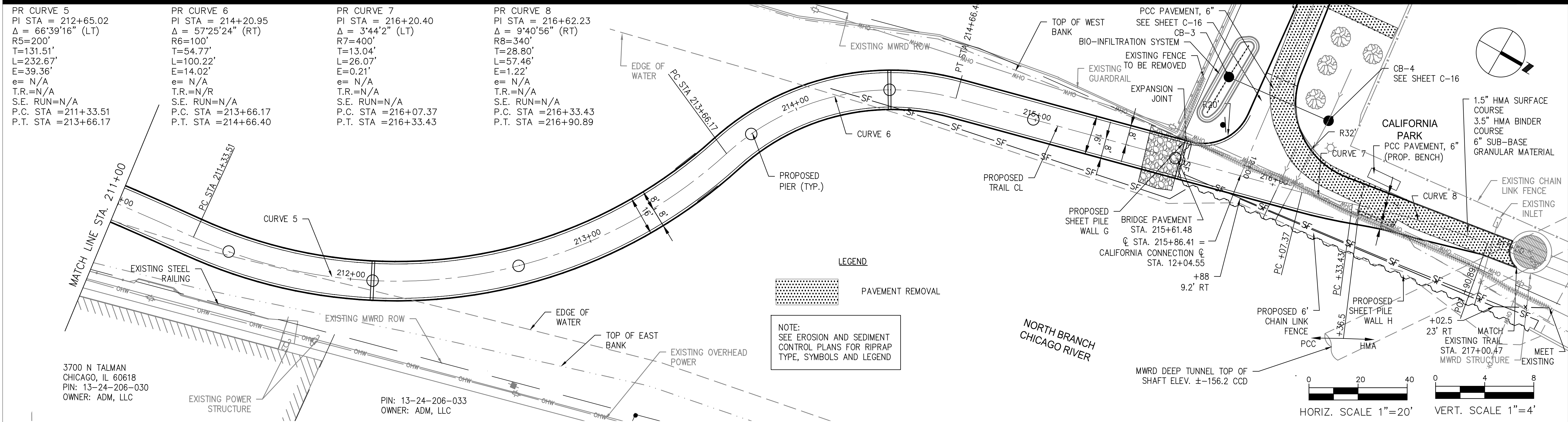
ADDISON UNDERBRIDGE TRAIL CONNECTION FROM
 CLARK PARK TO CALIFORNIA PARK

PLAN & PROFILE STA 205+00 TO STA 211+00

SHEET NO.
C-14

SHEET 14 OF 158

REVISION



January 24, 2016 10:00 AM - Addison Underbridge Connection - CDOT - Addisn Underbridge Connection - DISCOPLINE Civil Working\Sheets\015_Plan_Profile3.dwg
 User: J...

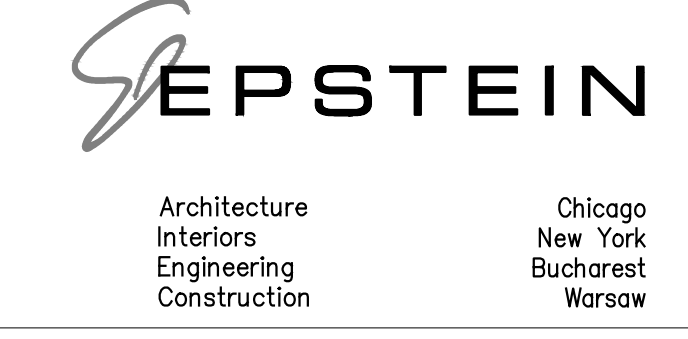
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DRAWN BY:	AP
CHECKED BY:	JRK
APPROVED BY:	GAO
DATE:	8/12/2016
SCALE:	1"=20'

NO.	DATE	REVISIONS/ISSUANCES
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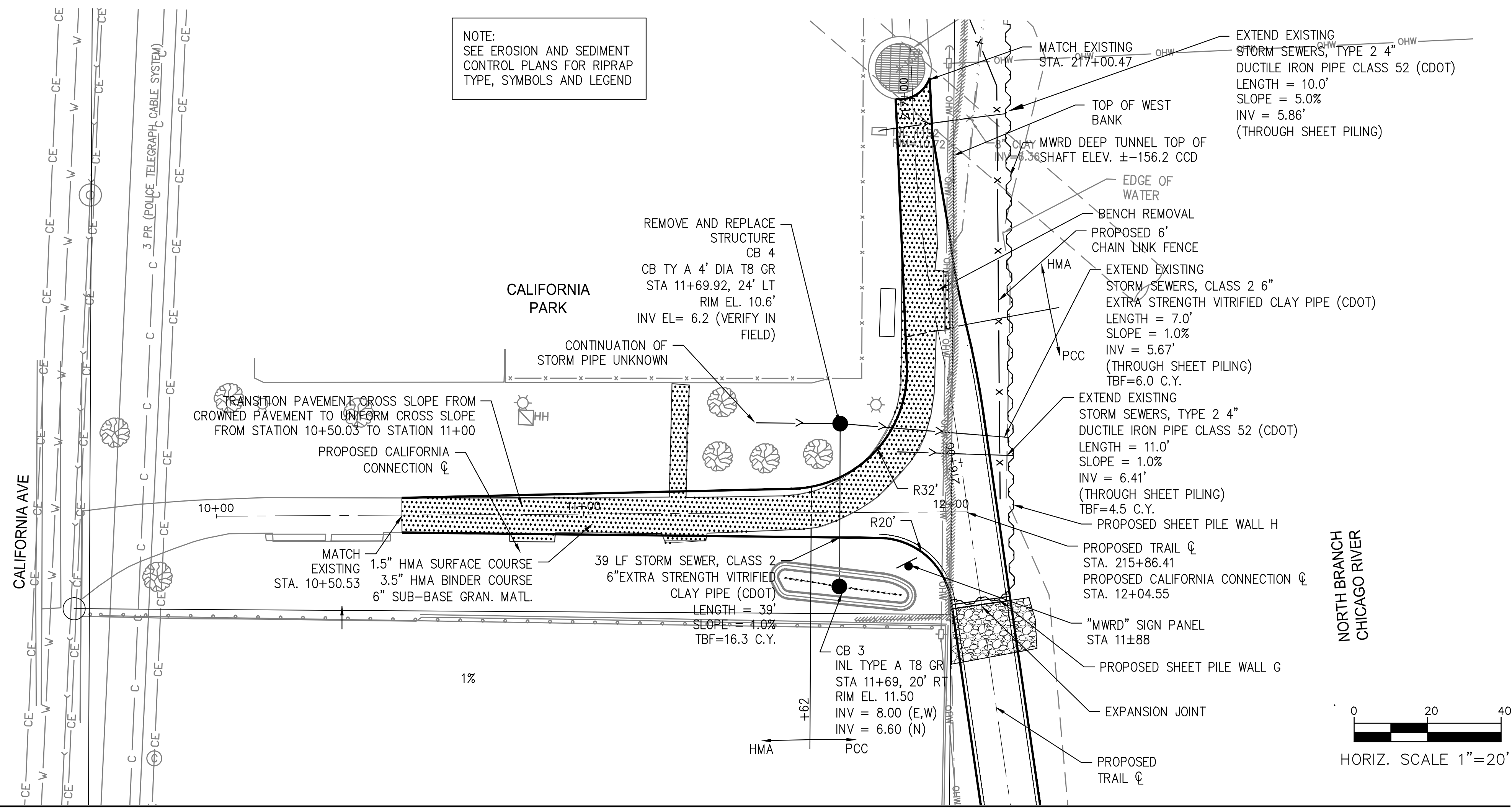
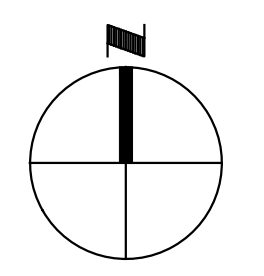


ADDISON UNDERBRIDGE TRAIL CONNECTION FROM CLARK PARK TO CALIFORNIA PARK

PLAN & PROFILE STA 211+00 TO STA 218+00

SHEET NO.	C-15
SHEET 15	OF 158

REVISION



NOTE:
SEE EROSION AND SEDIMENT CONTROL PLANS FOR RIPRAP TYPE, SYMBOLS AND LEGEND

EXTEND EXISTING STORM SEWERS, TYPE 2 4" DUCTILE IRON PIPE CLASS 52 (CDOT) LENGTH = 10.0' SLOPE = 5.0% INV = 5.86' (THROUGH SHEET PILING)

TRANSITION PAVEMENT FROM CROWNED PAVEMENT TO UNIFORM CROSS SLOPE FROM STATION 10+50.03 TO STATION 11+00

PROPOSED CALIFORNIA CONNECTION

MATCH EXISTING STA. 10+50.53
1.5" HMA SURFACE COURSE
3.5" HMA BINDER COURSE
6" SUB-BASE GRAN. MATL.

CALIFORNIA PARK

REMOVE AND REPLACE STRUCTURE CB 4
CB TY A 4' DIA T8 GR
STA 11+69.92, 24' LT
RIM EL. 10.6'
INV EL = 6.2 (VERIFY IN FIELD)

CONTINUATION OF STORM PIPE UNKNOWN

39 LF STORM SEWER, CLASS 2 6" EXTRA STRENGTH VITRIFIED CLAY PIPE (CDOT) LENGTH = 39' SLOPE = 4.0% TBF = 16.3 C.Y.

CB 3
INL TYPE A T8 GR
STA 11+69, 20' RT
RIM EL. 11.50
INV = 8.00 (E,W)
INV = 6.60 (N)

MATCH EXISTING STA. 217+00.47
TOP OF WEST BANK
MWRD DEEP TUNNEL TOP OF SHAFT ELEV. ±-156.2 CCD

EDGE OF WATER

BENCH REMOVAL
PROPOSED 6' CHAIN LINK FENCE

HMA
PCC
EXTEND EXISTING STORM SEWERS, CLASS 2 6" EXTRA STRENGTH VITRIFIED CLAY PIPE (CDOT) LENGTH = 7.0' SLOPE = 1.0% INV = 5.67' (THROUGH SHEET PILING) TBF = 6.0 C.Y.

EXTEND EXISTING STORM SEWERS, TYPE 2 4" DUCTILE IRON PIPE CLASS 52 (CDOT) LENGTH = 11.0' SLOPE = 1.0% INV = 6.41' (THROUGH SHEET PILING) TBF = 4.5 C.Y.

PROPOSED SHEET PILE WALL H

PROPOSED TRAIL
STA. 215+86.41
PROPOSED CALIFORNIA CONNECTION STA. 12+04.55

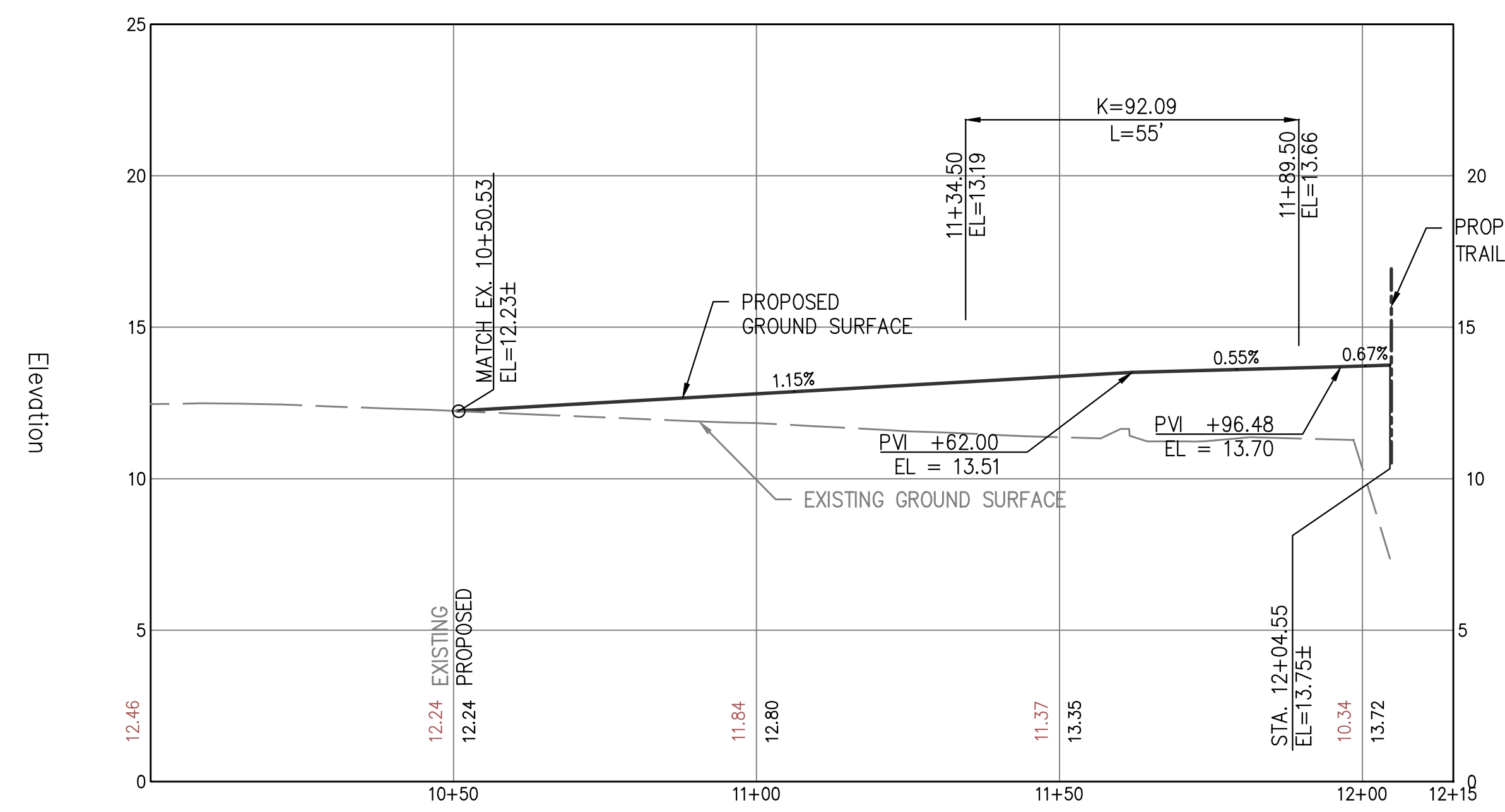
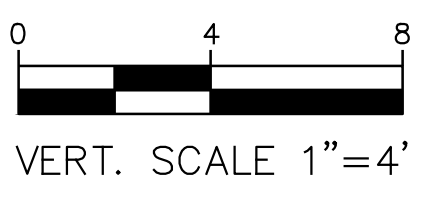
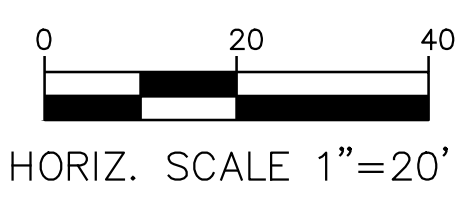
"MWRD" SIGN PANEL STA 11±88

PROPOSED SHEET PILE WALL G

EXPANSION JOINT

PROPOSED TRAIL

NORTH BRANCH CHICAGO RIVER



LEGEND
HMA PAVEMENT REMOVAL

HIGH WATER LEVEL ELEVATIONS PROVIDED BY MWRD

January 24, 2016 10:00:00 AM - Addison Underbridge Connection - DISCIPLINE (Civil) Working\Sheets\016_Plan_Profile.dwg

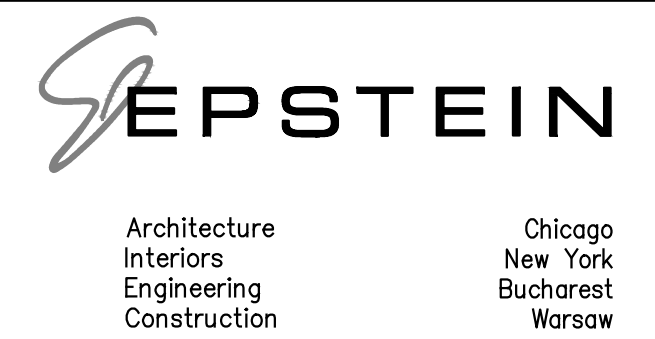
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DRAWN BY:	AP
CHECKED BY:	JRK
APPROVED BY:	GAO
DATE:	8/12/2016
SCALE:	1" = 20'

NO.	DATE	REVISIONS/ISSUANCES
1	8/12/16	ISSUED FOR FINAL
2	5/6/16	ISSUED FOR FINAL
3	6/05/15	ISSUED FOR FINAL REVIEW

CITY OF CHICAGO
DEPARTMENT OF TRANSPORTATION
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Chicago, IL 60602
312.744.3600



CONSULTANT
EPSTEIN
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ADDISON UNDERBRIDGE TRAIL CONNECTION FROM CLARK PARK TO CALIFORNIA PARK
PLAN & PROFILE CALIFORNIA PARK SOUTH

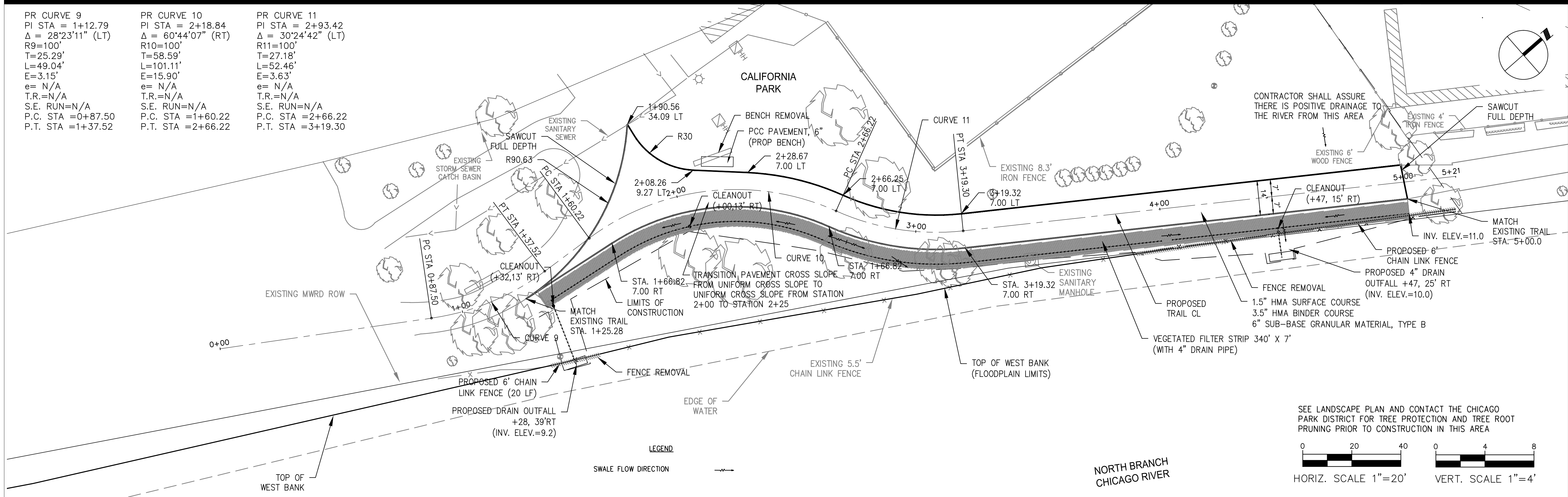
SHEET NO. C-16
SHEET 16 OF 158

REVISION

PR CURVE 9
 PI STA = 1+12.79
 $\Delta = 28^{\circ}23'11''$ (LT)
 R9=100'
 T=25.29'
 L=49.04'
 E=3.15'
 e= N/A
 T.R.=N/A
 S.E. RUN=N/A
 P.C. STA =0+87.50
 P.T. STA =1+37.52

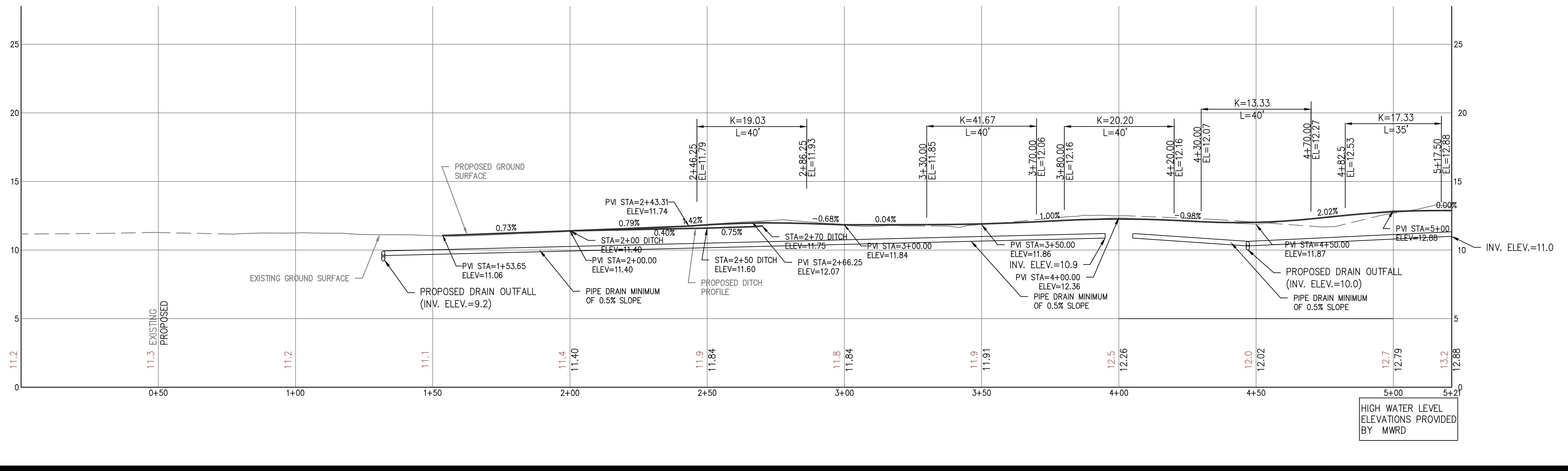
PR CURVE 10
 PI STA = 2+18.84
 $\Delta = 60^{\circ}44'07''$ (RT)
 R10=100'
 T=58.59'
 L=101.11'
 E=15.90'
 e= N/A
 T.R.=N/A
 S.E. RUN=N/A
 P.C. STA =1+60.22
 P.T. STA =2+66.22

PR CURVE 11
 PI STA = 2+93.42
 $\Delta = 30^{\circ}24'42''$ (LT)
 R11=100'
 T=27.18'
 L=52.46'
 E=3.63'
 e= N/A
 T.R.=N/A
 S.E. RUN=N/A
 P.C. STA =2+66.22
 P.T. STA =3+19.30



SEE LANDSCAPE PLAN AND CONTACT THE CHICAGO PARK DISTRICT FOR TREE PROTECTION AND TREE ROOT PRUNING PRIOR TO CONSTRUCTION IN THIS AREA

HORIZ. SCALE 1"=20'
 VERT. SCALE 1"=4'



January 24, 2016 10:00 AM - Addison Underbridge Connection - CDOT - Addison Underbridge Connection - VISCP\PLN\Civil\Working\Sheets\017_Plan_Profile5.dwg

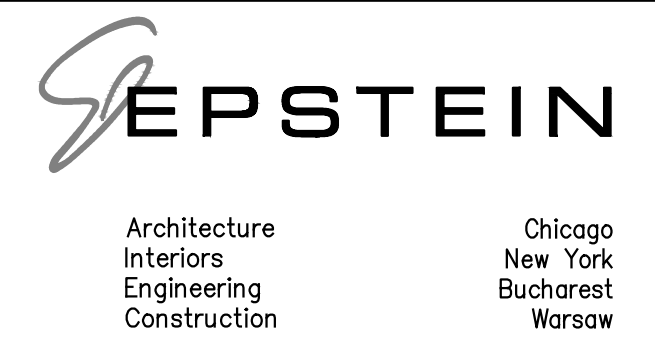
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DRAWN BY:	AP
CHECKED BY:	JRK
APPROVED BY:	GAO
DATE:	8/12/2016
SCALE:	1"=20'

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 312.744.3600



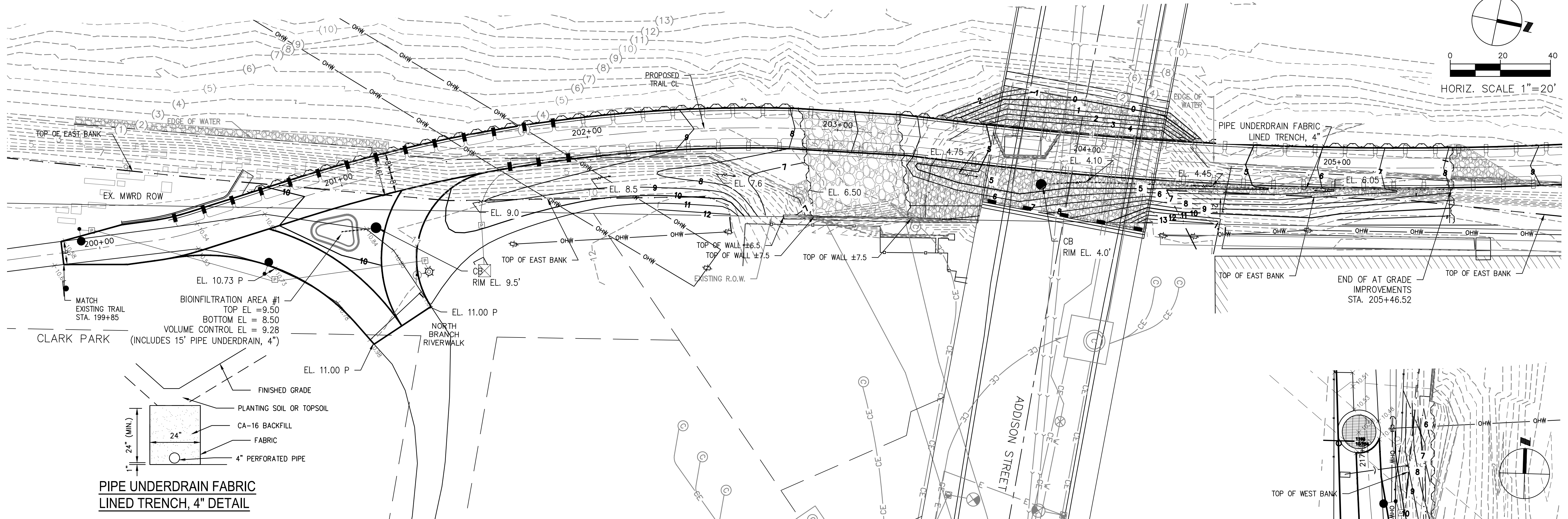
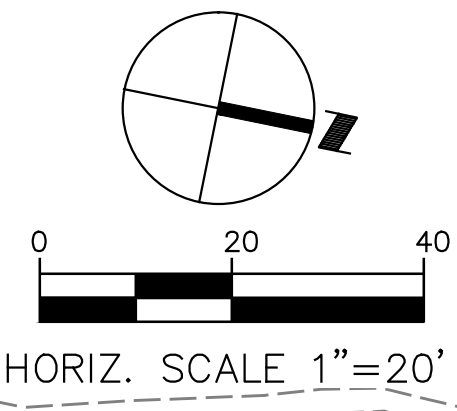
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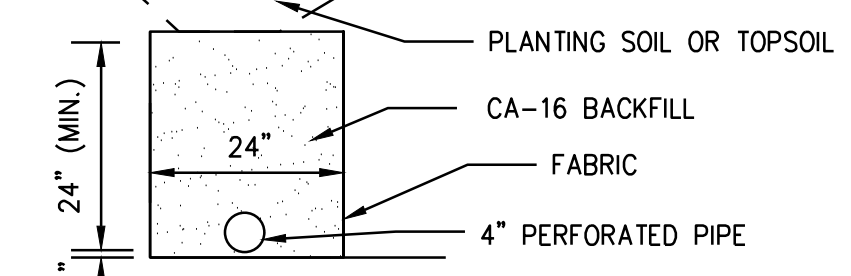
ADDISON UNDERBRIDGE TRAIL CONNECTION FROM CLARK PARK TO CALIFORNIA PARK

PLAN & PROFILE CALIFORNIA PARK NORTH

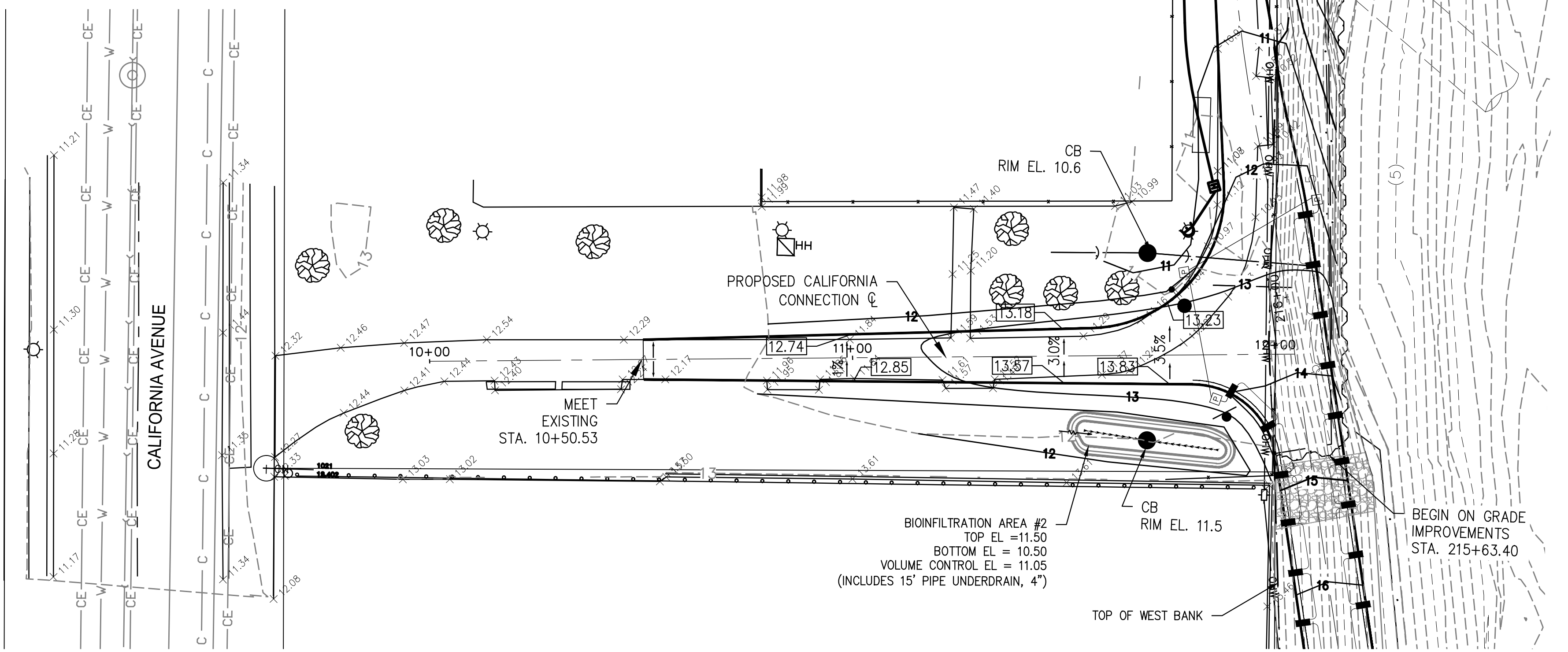
SHEET NO.	C-17
SHEET 17	OF 158
REVISION	



PIPE UNDERDRAIN FABRIC LINED TRENCH, 4" DETAIL



LEGEND
 XX.XX PROPOSED PAVEMENT GRADE



January 24, 2016 10:00 AM - Addison Underbridge Connection - DISCIPLINE: Civil Working\Sheets\018_Grading.dwg

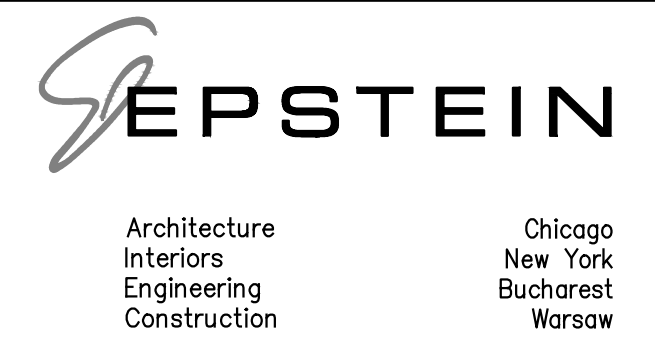
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DRAWN BY:	O. RIAD
CHECKED BY:	JRK
APPROVED BY:	GAO
DATE:	8/12/2015
SCALE:	1"=20'

NO.	DATE	REVISIONS/ISSUANCES
1	8/12/16	ISSUED FOR FINAL
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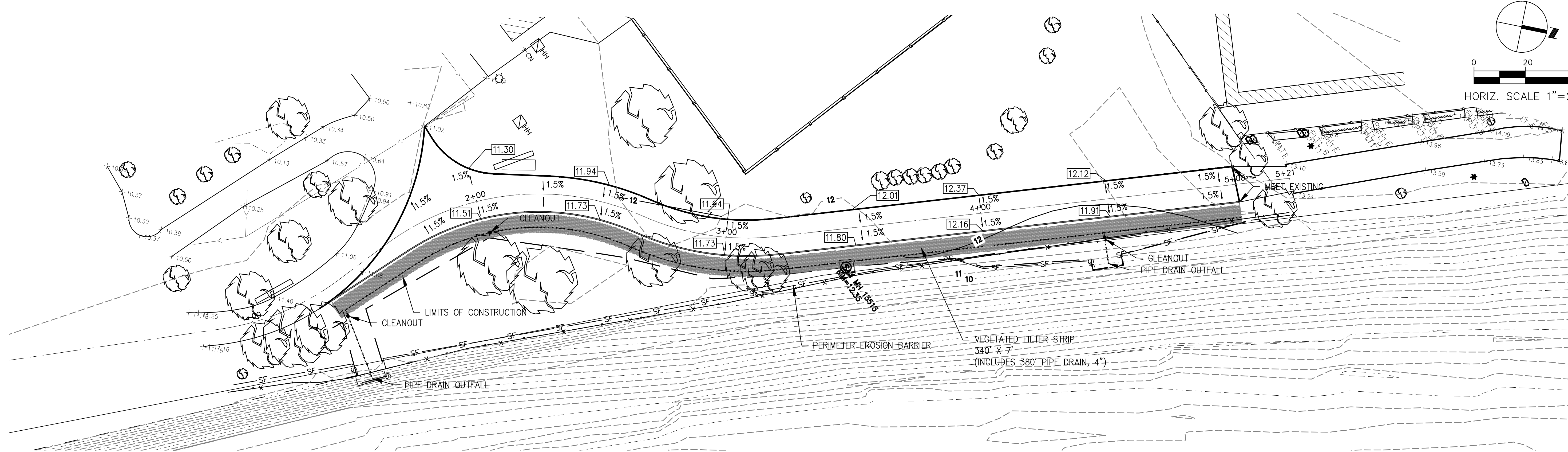
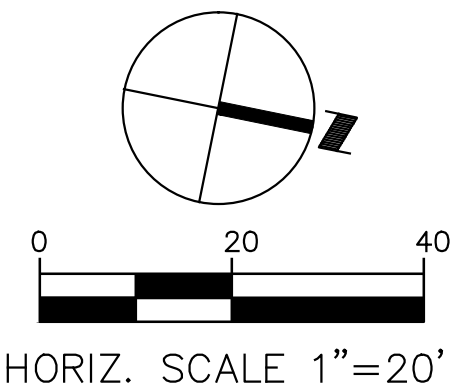
ADDISON UNDERBRIDGE TRAIL CONNECTION FROM CLARK PARK TO CALIFORNIA PARK

GRADING PLAN

SHEET NO.
C-18

SHEET 18 OF 158

REVISION



LEGEND
 [XX.XX] PROPOSED PAVEMENT GRADE

January 24, 2016 10:00 AM - Addison Underbridge Connection - DISCIPLINE\Civil\Working\Sheets\019_Grading_California Park_Cap.dwg

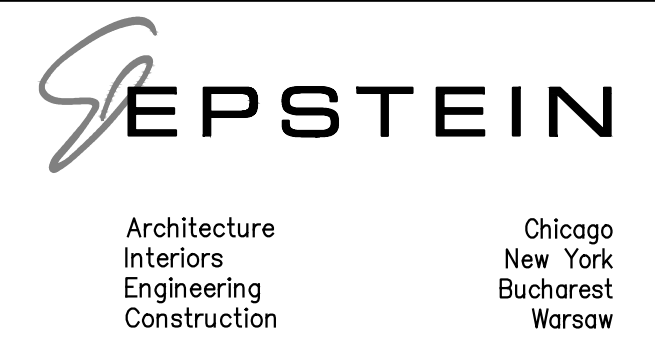
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DATE:	8/12/2016
SCALE:	1" = 20'

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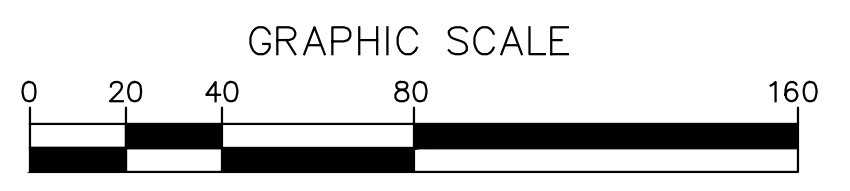
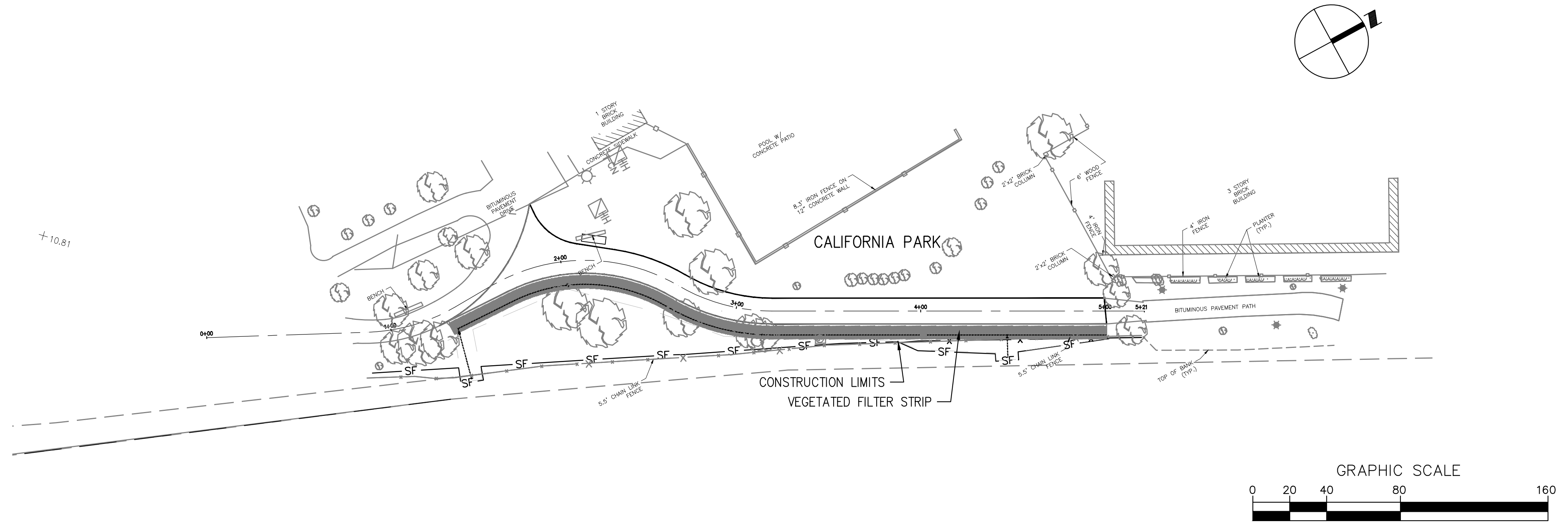
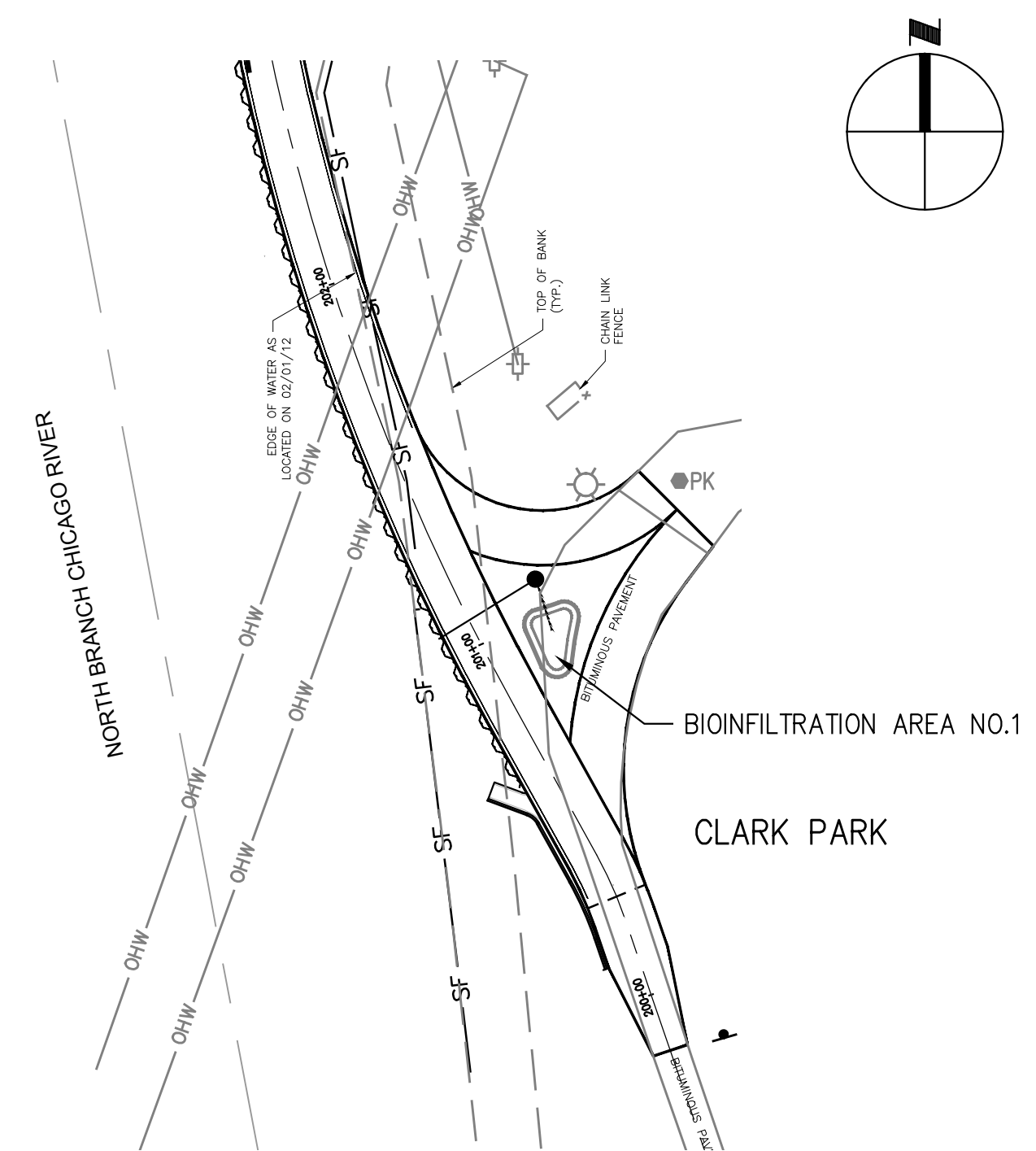
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ADDISON UNDERBRIDGE TRAIL CONNECTION FROM CLARK PARK TO CALIFORNIA PARK
 GRADING PLAN CALIFORNIA PARK

SHEET NO. **C-19**
 SHEET 19 OF 158

REVISION



GENERAL

THE BMPs PRESENTED IN THIS PLAN SET SHALL BE MAINTAINED ACCORDING TO THE STANDARDS, CODES, AND REGULATIONS OF THE CHICAGO STORMWATER ORDINANCE MANUAL. THE BMPs WILL BE EXECUTED DURING THE PROPER PHASES OF CONSTRUCTION. THE MAINTENANCE, UPKEEP, AND HIRING/TRAINING OF PROPER EMPLOYEES WILL BE DONE AT THE OWNER'S DISCRETION.

BMP DESCRIPTIONS

- 1) STORMWATER COLLECTED FROM THE PLAZA WILL BE DIRECTED THROUGH A SEWER TO A HYDRODYNAMIC SEPARATOR FOR FILTRATION THEN TO AN IRRIGATION TANK AND THROUGH AN 18" DISCHARGE PIPE AND DIRECTED INTO THE RIVER.
- 2) STORMWATER COLLECTED FROM THE RIVERWALK WILL INFILTRATE THROUGH LANDSCAPED AREAS TO UNDERGROUND DETENTION AND TO THE RIVER.

O&M PLAN MINIMUM DESIGN REQUIREMENTS

- 1) O&M PLAN PROCEDURES AND PRACTICES MUST BE REVIEWED AND ASSESSED ANNUALLY.
- 2) ACCESS ROUTES, INCLUDING ROADWAYS AND SIDEWALKS, SHALL BE INSPECTED ANNUALLY AND MAINTAINED AS NEEDED.
- 3) DRAINAGE STRUCTURES AND FLOW RESTRICTORS MUST BE INSPECTED AND CLEANED SEMI-ANNUALLY.
- 4) VOLUME CONTROL BMPs SHALL BE INSPECTED SEMI-ANNUALLY AND AFTER SIGNIFICANT RAINFALL EVENTS EXCEEDING 1.5 INCHES.
- 5) THE OWNER SHALL KEEP AN UPDATED LOG BOOK DOCUMENTING THE PERFORMANCE OF THE REQUIRED O&M ACTIVITIES FOR PERPETUITY. LOG BOOKS MUST BE PRODUCED UPON THE REQUEST OF A CITY INSPECTOR.
- 6) VEGETATION SHALL BE MAINTAINED ON A REGULAR BASIS.
- 7) PEST CONTROL MEASURES SHALL BE IMPLEMENTED TO ADDRESS INSECTS AND RODENTS.
- 8) SIGNAGE AND FENCING SHALL BE INSTALLED AND MAINTAINED WHERE NECESSARY TO PROTECT PROPERTY AND THE PUBLIC.
- 9) UNDERGROUND VAULTS MUST INCLUDE DESIGN MEASURES TO FACILITATE CLEANING AND MAINTENANCE. CONFINED SPACE SAFETY PROCEDURES MUST BE FOLLOWED.

MAINTENANCE CONTACT INFORMATION

CHICAGO PARK DISTRICT
541 NORTH FAIRBANKS
CHICAGO, IL 60611
(312) 742-7529

ACTIVITY SCHEDULES

BIOINFILTRATION LANDSCAPE AREAS

- AS NEEDED:
- WATER PLANTS DURING FIRST GROWING SEASON
 - REMULCH VOID AREAS
 - TREAT DISEASED TREES AND SHRUBS
 - KEEP OVERFLOW FREE AND CLEAR OF LEAVES
- MONTHLY
- INSPECT SOIL AND REPAIR ERODED AREAS
 - REMOVE LITTER AND DEBRIS
 - CLEAR LEAVES AND DEBRIS FROM OVERFLOW
- SEMI-ANNUALLY
- INSPECT TREES AND SHRUBS TO EVALUATE HEALTH
- ANNUALLY
- ADD ADDITIONAL MULCH
 - INSPECT FACILITY AND PRETREATMENT AREAS FOR SEDIMENT BUILDUP, EROSION, VEGETATIVE CONDITIONS, ETC.

VEGETATIVE STRIP AREAS

- AS NEEDED:
- MOWING AND/OR TRIMMING OF VEGETATION
- MONTHLY
- INSPECT ALL VEGETATED STRIP COMPONENTS EXPECTED TO RECEIVE AND/OR TRAP DEBRIS AND SEDIMENT FOR CLOGGING AND EXCESSIVE DEBRIS AND SEDIMENT ACCUMULATION; REMOVE SEDIMENT DURING DRY PERIODS.
- SEMI-ANNUALLY
- VEGETATED AREAS SHOULD BE INSPECTED FOR EROSION, SCOUR AND UNWANTED GROWTH. EROSION REPAIR AND REMOVAL OF UNWANTED GROWTH SHOULD HAVE MINIMUM DISRUPTION TO THE SOIL PLANTING BED AND REMAINING VEGETATION.
- ANNUALLY
- REMOVE SEDIMENT AND CORRECT GRADING AND FLOW CHANNELS DURING DRY PERIODS.

OPERATION AND MAINTENANCE OWNER'S CERTIFICATION STATEMENT

PROPERTY NAME: CLARK PARK AND CALIFORNIA PARK
PROPERTY ADDRESS: 2650 WEST ADDISON
CHICAGO, IL

AS THE OWNER OF SUBJECT PROPERTY, BY SIGNING THIS DOCUMENT I ACKNOWLEDGE THAT I HAVE RECEIVED AND REVIEWED THE OPERATION AND MAINTENANCE PLAN AND UNDERSTAND ITS CONTENTS. (AS REQUIRED BY THE STORMWATER MANAGEMENT ORDINANCE, SECTION 5).

IN THE EVENT THAT I WERE TO SELL THIS PROPERTY, I AGREE TO GIVE A COPY OF THE PLAN TO THE NEW OWNER AND THIS OWNER'S CERTIFICATION STATEMENT FOR SIGNATURE. THIS SIGNED CERTIFICATION STATEMENT MUST BE SUBMITTED TO THE CITY'S DEPARTMENT OF BUILDINGS UPON TRANSFER OF OWNERSHIP.

I FURTHER AGREE TO ADHERE TO THE MAINTENANCE SCHEDULE OF BEST MANAGEMENT PRACTICES STIPULATED IN THE PLAN. I ALSO ACKNOWLEDGE THAT IF I DON'T MAINTAIN THE MEASURES AS SHOWN IN THE PLAN, UPON CITY INSPECTION, I COULD BE LIABLE FOR A VIOLATION OF THE CITY'S MUNICIPAL CODE (ACCORDING TO STORMWATER MANAGEMENT ORDINANCE SECTION 5).

OWNER'S PRINTED NAME _____
OWNER'S SIGNATURE _____ DATE _____ NOTARY PUBLIC _____

January 24, 2016 10:28:34 AM - Addison Underbridge Connection - CDOT - DISCIPLINE Civil Working\Sheets\020_0 & M Plan.dwg

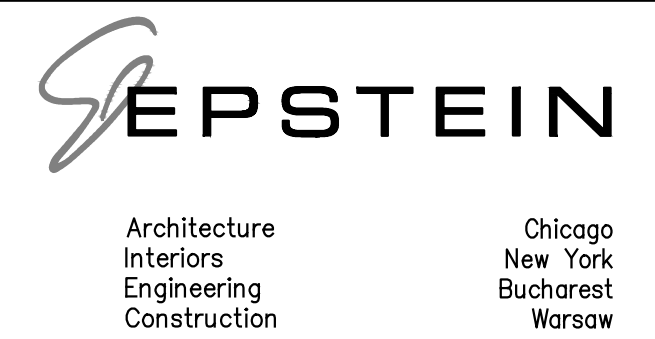
SECTION NO.:	12-E9121-00-BT
DRAWN BY:	JRK
CHECKED BY:	JRK
APPROVED BY:	GAO
DATE:	8/12/2016
SCALE:	1" = 40'

NO.	DATE	REVISIONS/ISSUANCES
1	8/12/16	ISSUED FOR FINAL
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3	6/05/15	ISSUED FOR FINAL REVIEW

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Chicago, IL 60602
312.744.3600

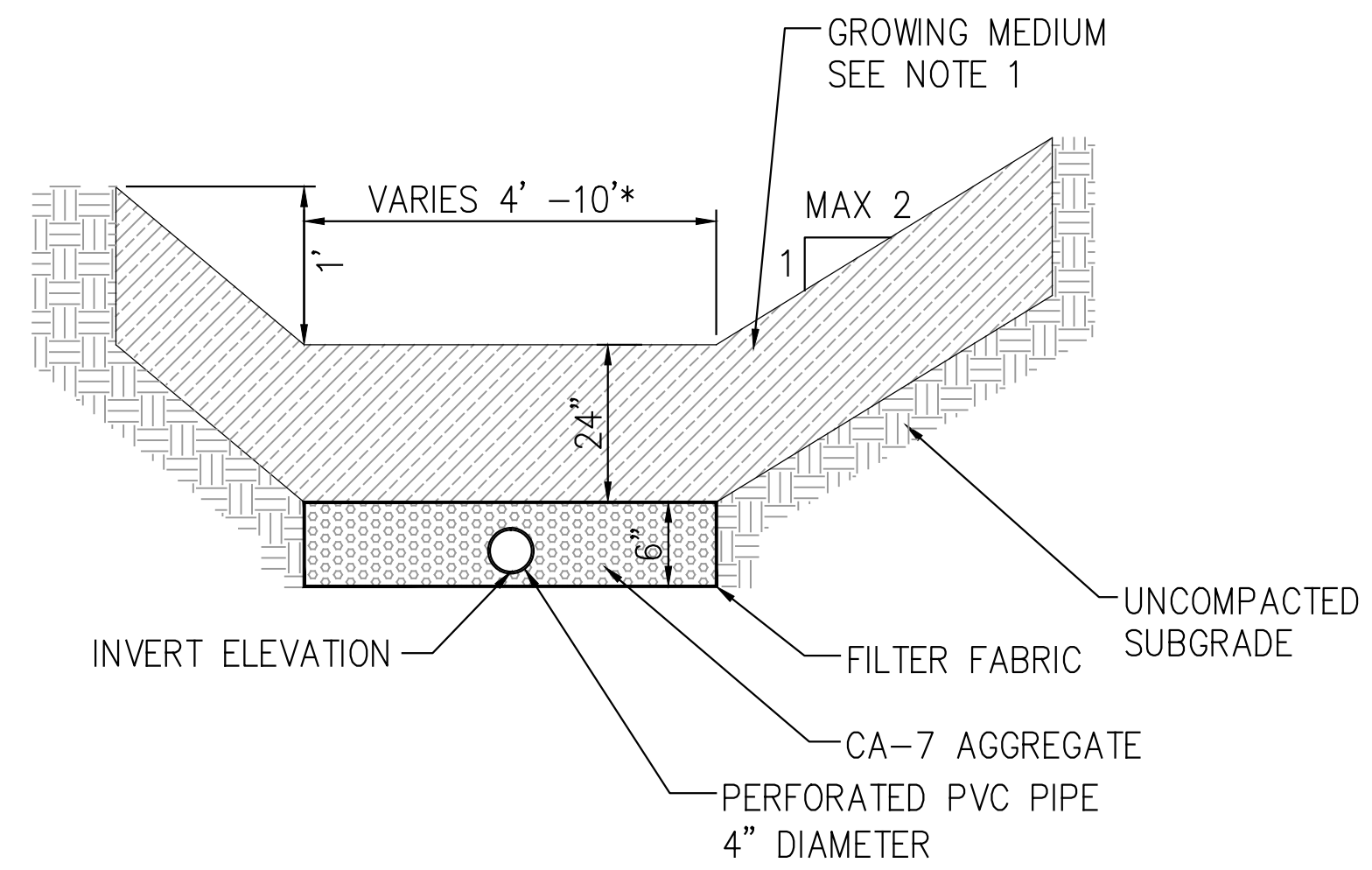


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ADDISON UNDERBRIDGE TRAIL CONNECTION FROM CLARK PARK TO CALIFORNIA PARK
OPERATION AND MAINTENANCE PLAN

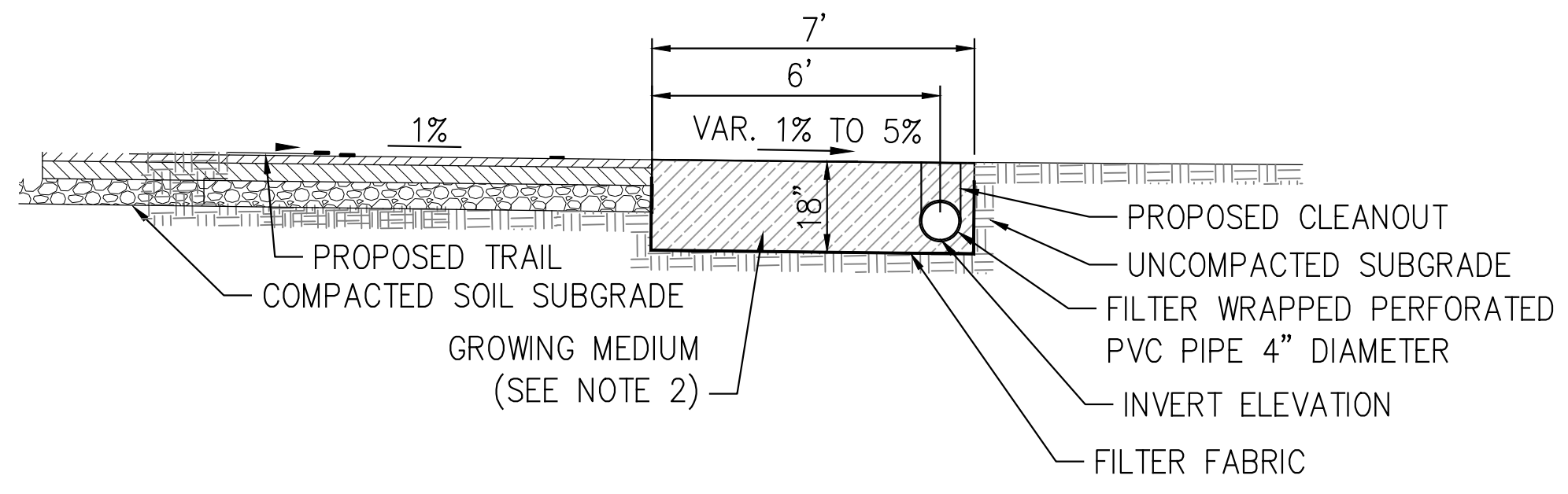
SHEET NO.	C-20	REVISION
SHEET 20	OF 158	



BIO-INFILTRATION SYSTEM DETAIL

NOTES

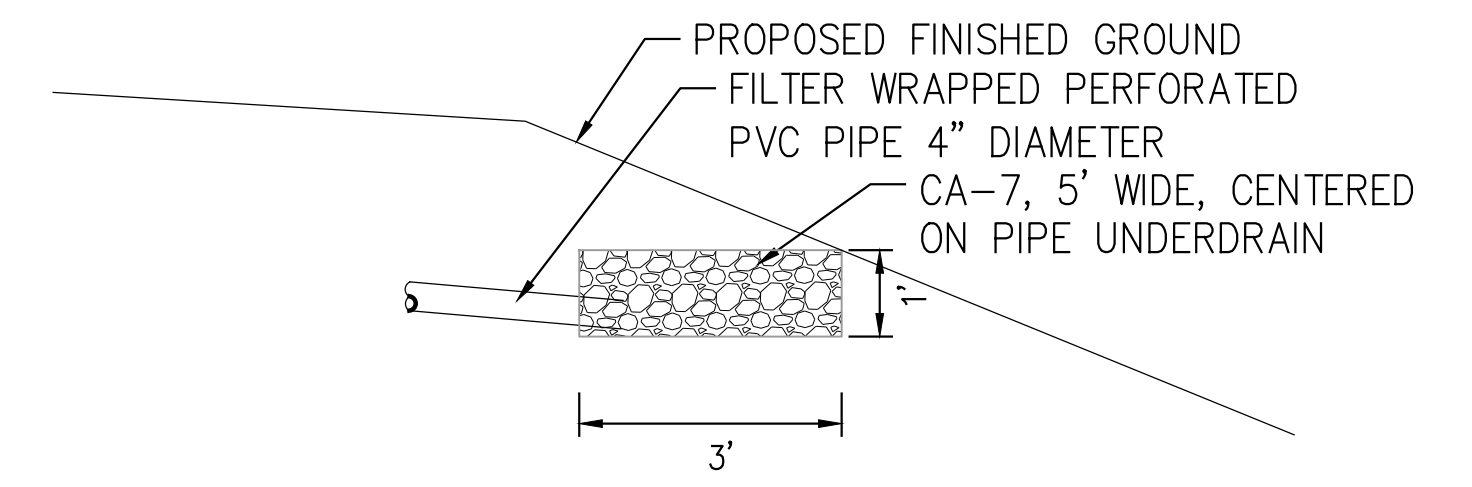
- 1. BIOINFILTRATION AREA #1
REQUIRED VOLUME CONTROL = 112 CF
PROVIDED VOLUME CONTROL = 154.5 CF
- 2. BIOINFILTRATION AREA #2
REQUIRED VOLUME CONTROL = 129 CF
PROVIDED VOLUME CONTROL = 272 CF



VEGETATED FILTER STRIP DETAIL

NOTES

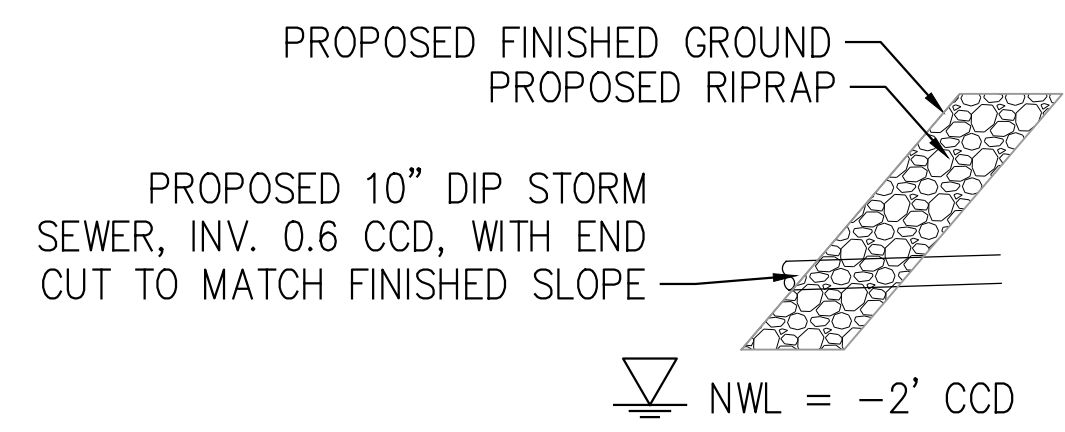
- 1. VEGETATED FILTER STRIP
REQUIRED VOLUME CONTROL = 92 CF
PROVIDED VOLUME CONTROL = 100 CF



PIPE DRAIN OUTFALL FROM VEGETATED FILTER STRIP

GENERAL NOTES

- 1. THE GROWING MEDIUM SOIL MUST BE A MIX OF 40% SAND, 30% TOPSOIL, AND 30% COMPOST. THE SOIL SHALL BE AT LEAST 2' DEEP AND MUST BE 4" DEEPER THAN THE LARGEST PLANTED ROOTBALL.
- 2. THE GROWING MEDIUM SOIL MUST BE A MIX OF 40% SAND, 30% TOPSOIL, AND 30% COMPOST. THE SOIL SHALL BE AT LEAST 1.5' DEEP.
- 3. REFER TO SPECIFICATIONS FOR GROWING MEDIUM SOIL REQUIREMENTS.



PIPE OUTFALL AT STA 203Q85

January 24, 2016 2:00 PM Projects\2000\20346 - CDOT - Addison Underbridge Connection - \DISCIPLINE\Civil\Working\Sheets\021_BMP Construction Details.dwg

SECTION NO.: 12-E9121-00-BT
 DRAWN BY: AP
 CHECKED BY: JRK
 APPROVED BY: GAO
 DATE: 8/12/2016
 SCALE: N.T.S.

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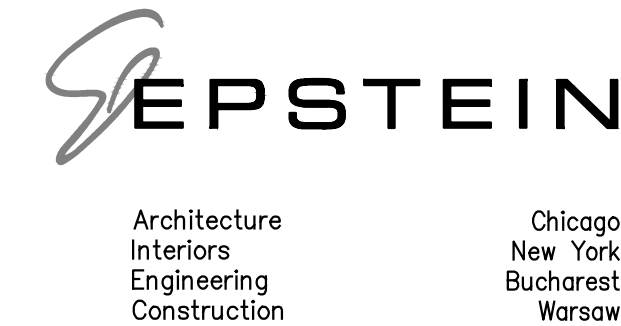
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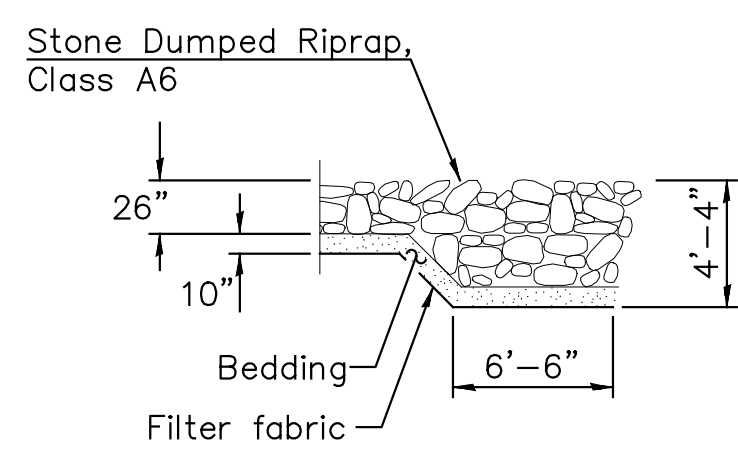
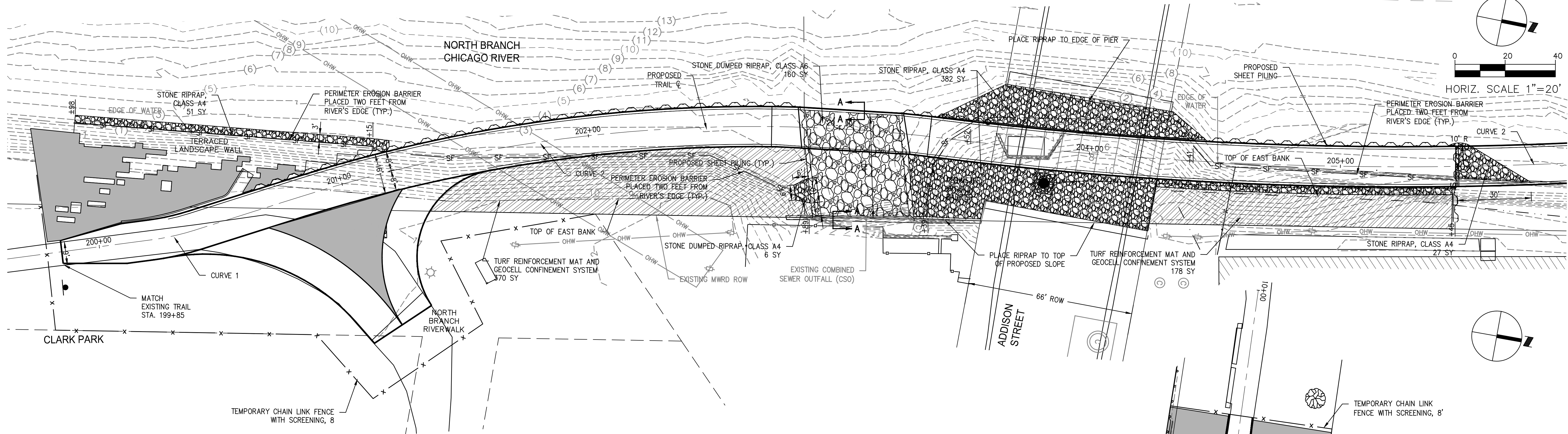
ADDISON UNDERBRIDGE TRAIL CONNECTION FROM
 CLARK PARK TO CALIFORNIA PARK

BMP CONSTRUCTION DETAILS

SHEET NO.
C-21

SHEET 21 OF 158

REVISION

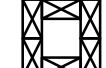
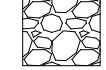
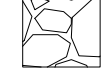




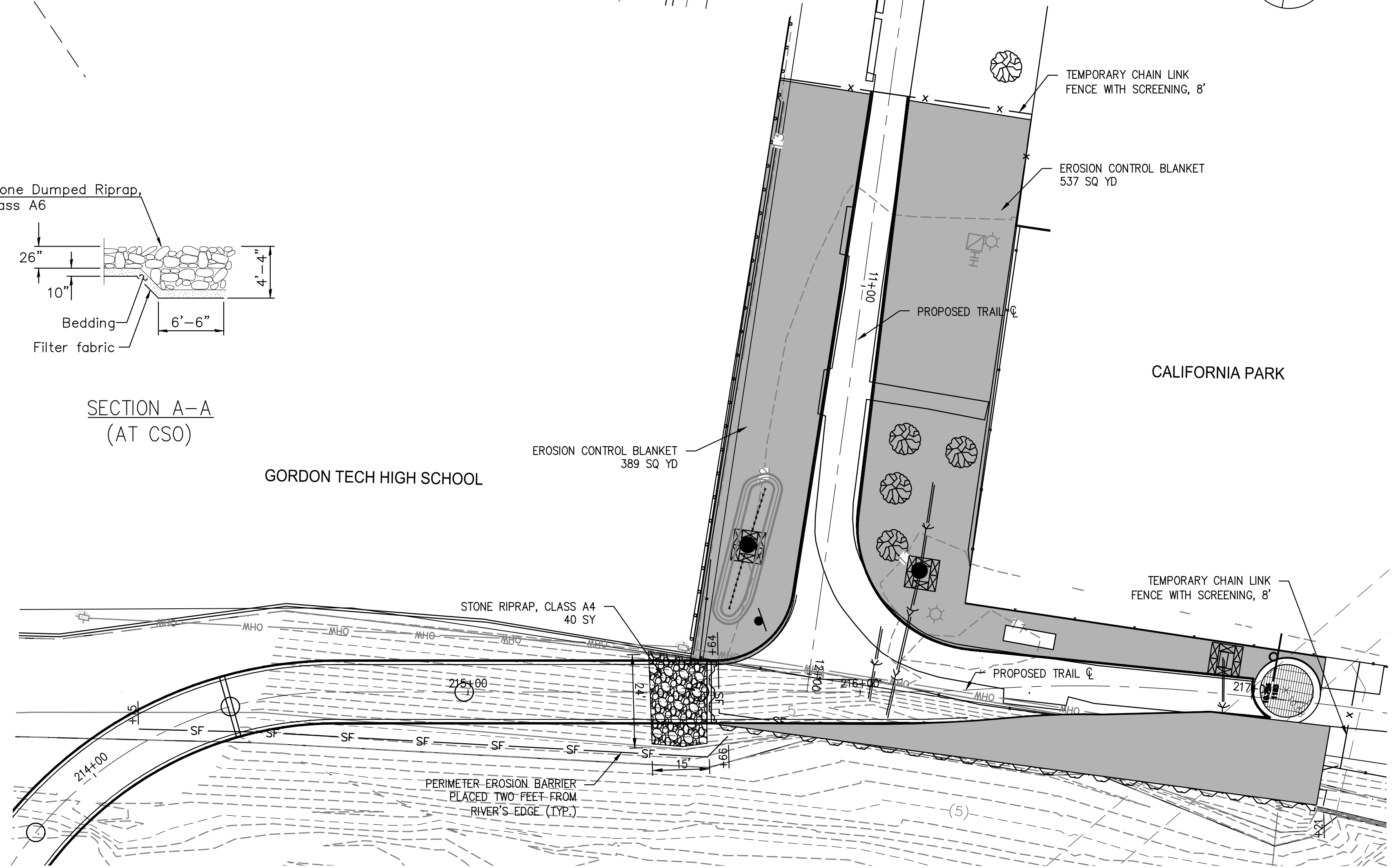
NOTES:

- CONTRACTOR SHALL COMPLY WITH THE NORTH COOK SOIL AND WATER CONSERVATION DISTRICT'S (SWCD) WRITTEN AND VERBAL RECOMMENDATIONS REGARDING THE SOIL EROSION AND SEDIMENT CONTROL (SESC) PLAN AND THE INSTALLATION AND MAINTENANCE OF THE SESC PRACTICES ON-SITE.
- CONTRACTOR SHALL SCHEDULE A PRECONSTRUCTION MEETING WITH NORTH COOK SWCD TO DISCUSS THE SESC PLAN AND THE INSTALLATION AND MAINTENANCE REQUIREMENTS OF THE SESC PRACTICES ON THE SITE.
- CONTRACTOR SHALL NOTIFY THE NORTH COOK SWCD OF ANY CHANGES OR MODIFICATIONS TO THE APPROVED PLAN SET. FIELD CONDITIONS DURING PROJECT CONSTRUCTION MAY REQUIRE THE IMPLEMENTATION OF ADDITIONAL SESC MEASURES. SHOULD THE CONTRACTOR FAIL TO IMPLEMENT CORRECTIVE MEASURES, THE COMMISSIONER, OR OTHER JURISDICTIONAL AGENCIES, MAY REQUIRE MORE FREQUENT SITE INSPECTIONS TO ENSURE THE INSTALLED SESC MEASURES ARE ACCEPTABLE.
- PRIOR TO COMMENCEMENT OF ANY IN-STREAM WORK, CONTRACTOR SHALL SUBMIT CONSTRUCTION PLANS AND A DETAILED NARRATIVE TO THE NORTH COOK SWCD THAT DISCLOSE THE CONTRACTOR'S PREFERRED METHOD OF COFFERDAM AND DEWATERING METHOD. WORK IN THE WATERWAY SHALL NOT COMMENCE UNTIL THE NORTH COOK SWCD NOTIFIES YOU, IN WRITING, THAT THE PLANS HAVE BEEN APPROVED.
- CONTRACTOR SHALL COMPLY TO THE REQUIREMENTS OF THE UNITED STATES CORPS OF ENGINEERS, CHICAGO DISTRICT PERMIT NUMBER LRC-2013-161.

NOTE:
ALL WORK NECESSARY FOR THE PLACEMENT OF RIPRAP, INCLUDING BUT NOT LIMITED TO, EXCAVATION AND DEWATERING SHALL BE INCLUDED IN THE COST OF THE RIPRAP. REMOVAL AND DISPOSAL OF REGULATED SUBSTANCES WILL BE PAID SEPARATELY.

LEGEND

- TEMPORARY CHAIN LINK FENCE WITH SCREENING, 8' — x — x —
- INLET FILTERS 
- STONE RIP RAP, CLASS A4 
- STONE DUMPED RIP RAP, CLASS A6 
- EROSION CONTROL BLANKET AND SEEDING, CLASS 1A 
- TURF REINFORCEMENT MAT AND GEOCELL CONFINEMENT SYSTEM 
- PERIMETER EROSION BARRIER — SF —



SECTION NO.: 12-E9121-00-BT
 DRAWN BY: JG
 CHECKED BY: JRK
 APPROVED BY: GAO
 DATE: 8/12/2016
 SCALE: 1"=20'

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 Warsaw

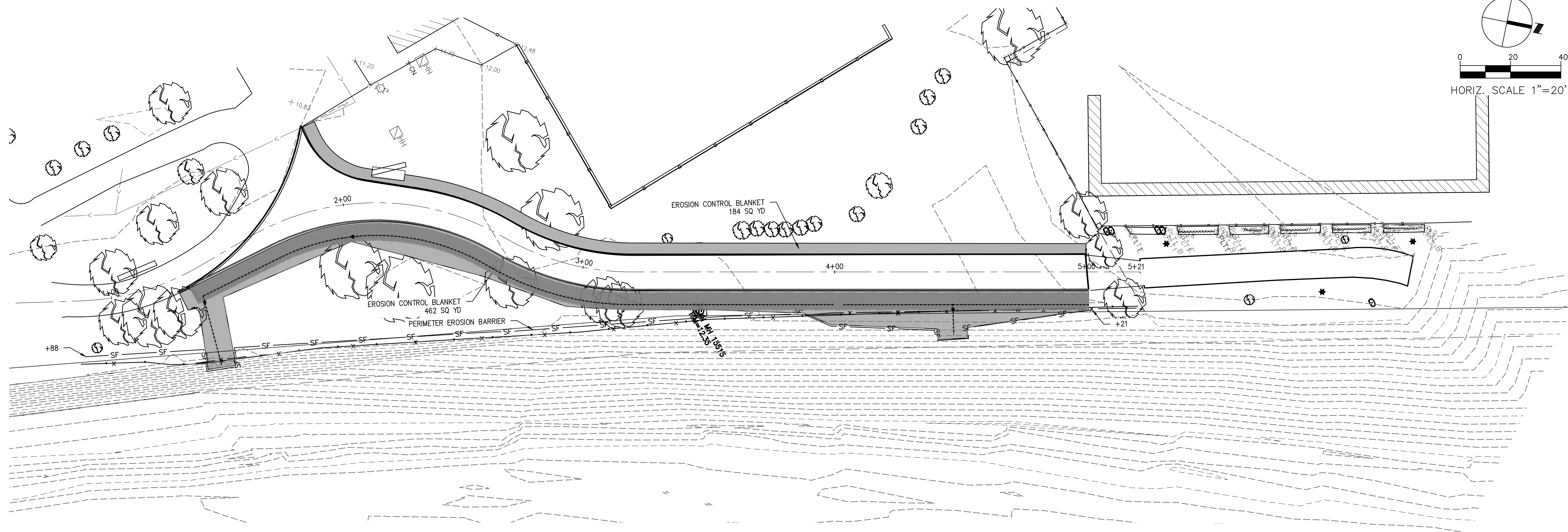
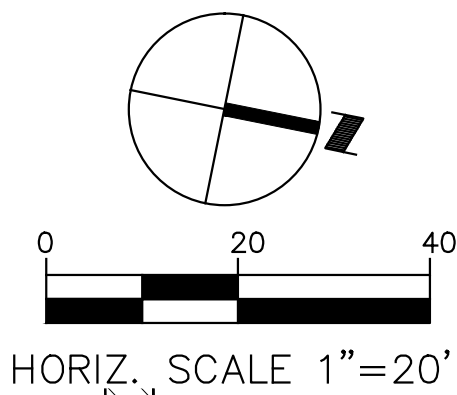
ADDISON UNDERBRIDGE TRAIL CONNECTION FROM CLARK PARK TO CALIFORNIA PARK

EROSION AND SEDIMENT CONTROL PLAN

SHEET NO.
C-22

SHEET 22 OF 158

REVISION



LEGEND

- TEMPORARY CHAIN LINK FENCE WITH SCREENING, 8' — x — x —
- INLET FILTERS
- STONE RIP RAP, CLASS A4
- STONE DUMPED RIP RAP, CLASS A6
- EROSION CONTROL BLANKET AND SEEDING, CLASS 1A
- PERIMETER EROSION BARRIER — SF —

NOTE:
 ALL WORK NECESSARY FOR THE PLACEMENT OF RIPRAP, INCLUDING BUT NOT LIMITED TO, EXCAVATION AND DEWATERING SHALL BE INCLUDED IN THE COST OF THE RIPRAP. REMOVAL AND DISPOSAL OF REGULATED SUBSTANCES WILL BE PAID SEPARATELY.

January 24, 2016 12:00:00 2016 02-023_erosion_control.dwg

SECTION NO.: 12-E9121-00-BT
 DRAWN BY: JM
 CHECKED BY: JRK
 APPROVED BY: GAO
 DATE: 8/12/2016
 SCALE: 1"=40'

NO.	DATE	REVISIONS/ISSUANCES
1	8/12/16	ISSUED FOR FINAL
2	5/6/16	ISSUED FOR FINAL
3	6/05/15	ISSUED FOR FINAL REVIEW

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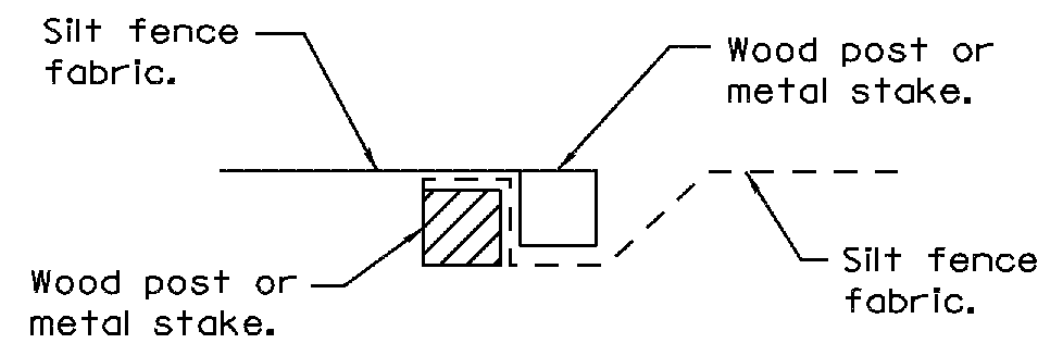
ADDISON UNDERBRIDGE TRAIL CONNECTION FROM
 CLARK PARK TO CALIFORNIA PARK

EROSION AND SEDIMENT CONTROL PLAN CALIFORNIA
 PARK

SHEET NO.
C-23

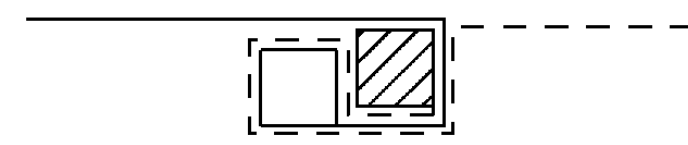
SHEET 23 OF 158

REVISION



Place end-post (stake) of first silt fence adjacent to end-post (stake) of second silt fence with fabric positioned as shown.

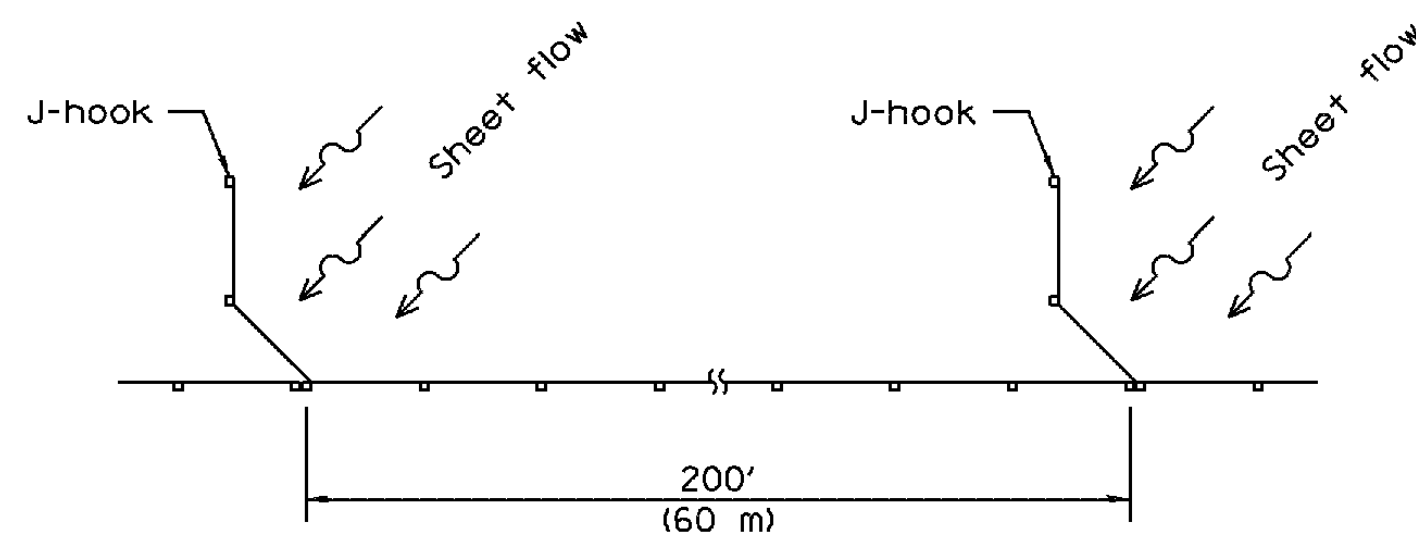
STEP 1



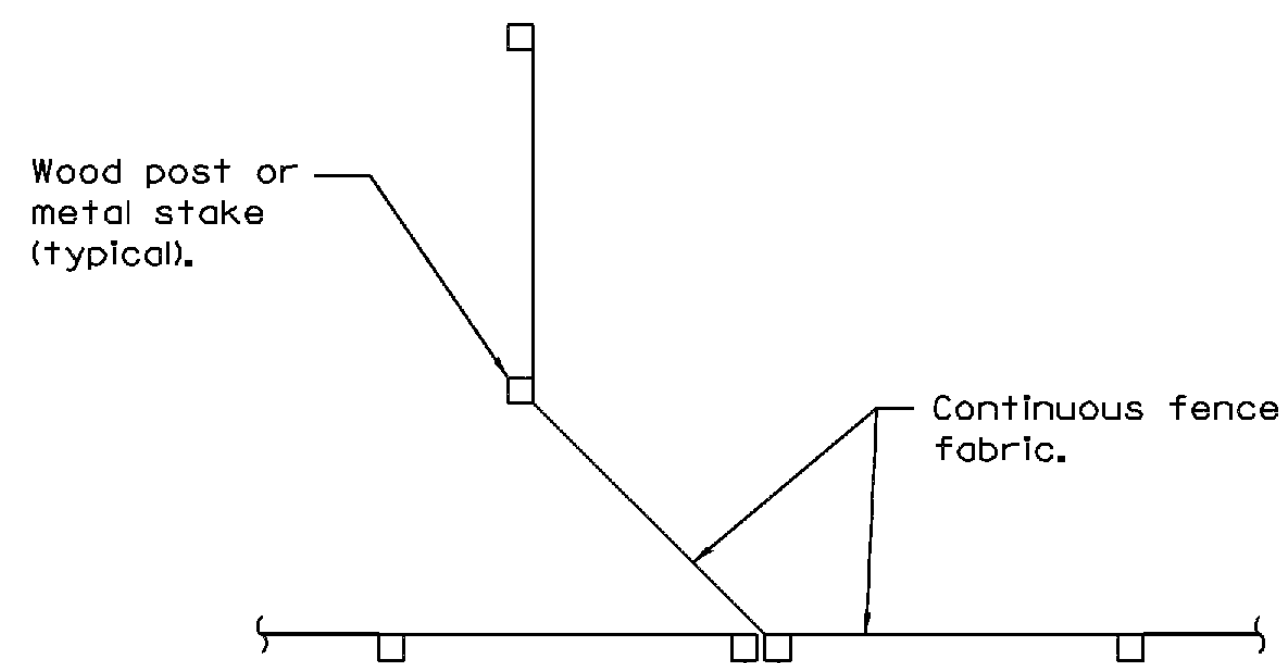
Rotate posts (stakes) together 180° clockwise and drive both posts (stakes) 18 (450) into ground.

STEP 2

ATTACHING TWO SILT FILTER FENCES
(Not applicable for J-hooks)

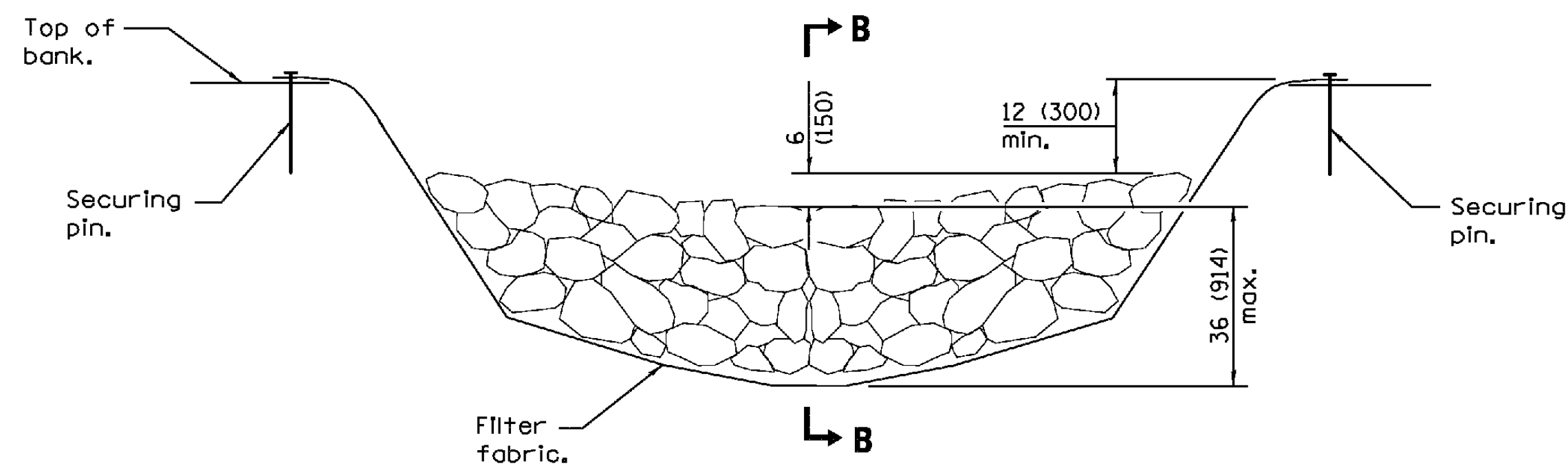


SILT FILTER J-HOOK PLACEMENT



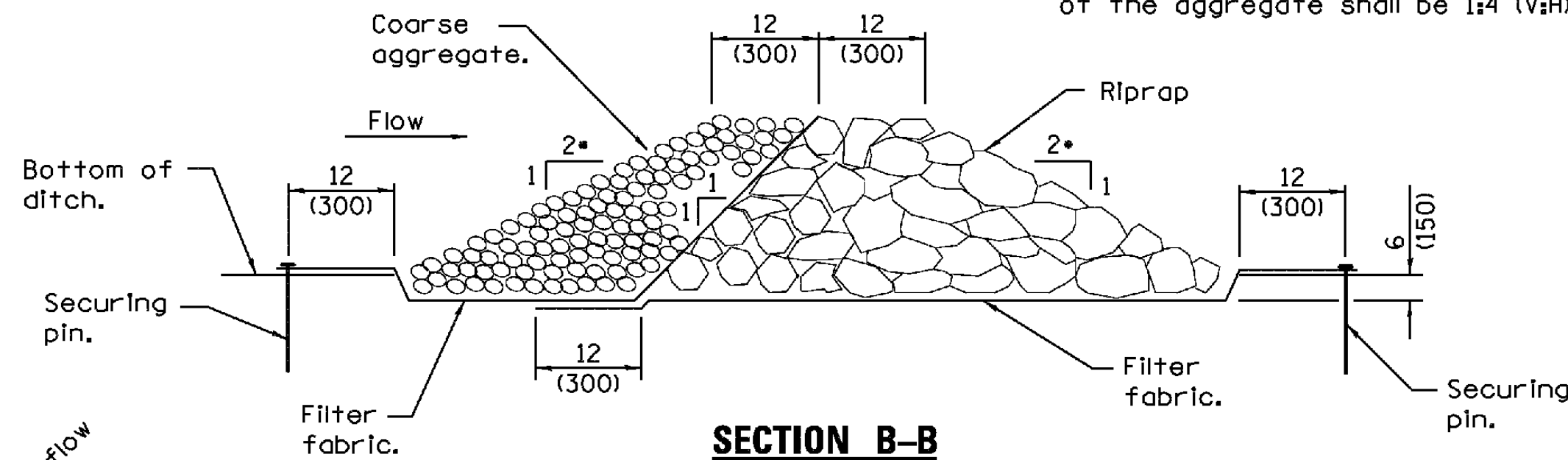
J-HOOK

Illinois Department of Transportation	
PASSED	January 1, 2013
Michael Beard ENGINEER OF POLICY AND PROCEDURES	
APPROVED	January 1, 2013
ENGINEER OF DESIGN AND ENVIRONMENT	
ISSUED	1-1-97



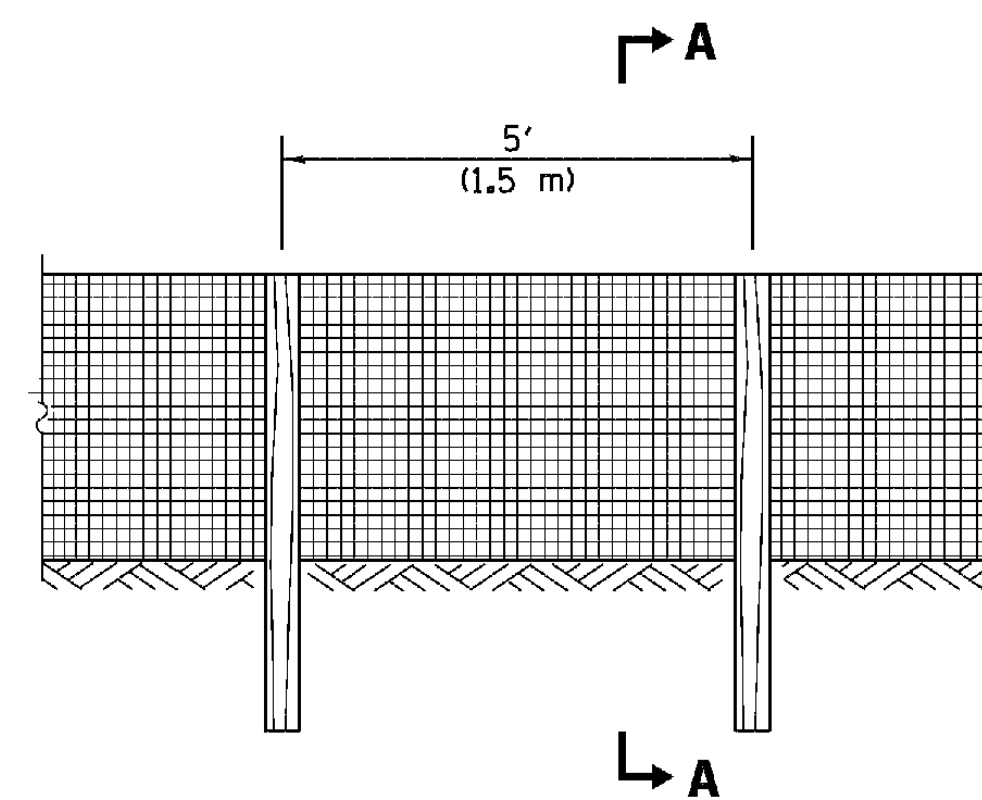
ELEVATION

When the ditch check is within the clear zone and the road is open to traffic, the traffic approach slope of the aggregate shall be 1:4 (V:H).



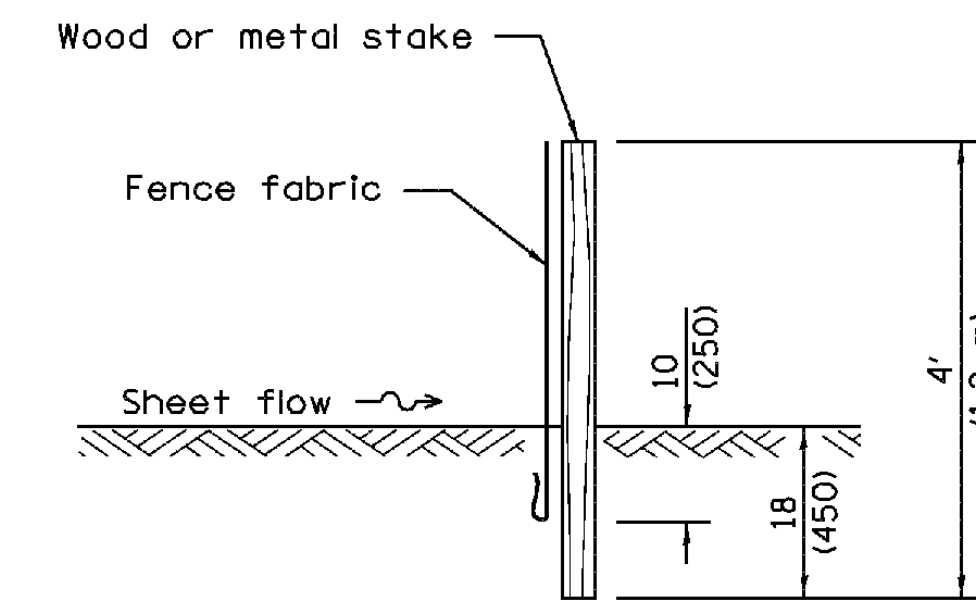
SECTION B-B

AGGREGATE DITCH CHECK

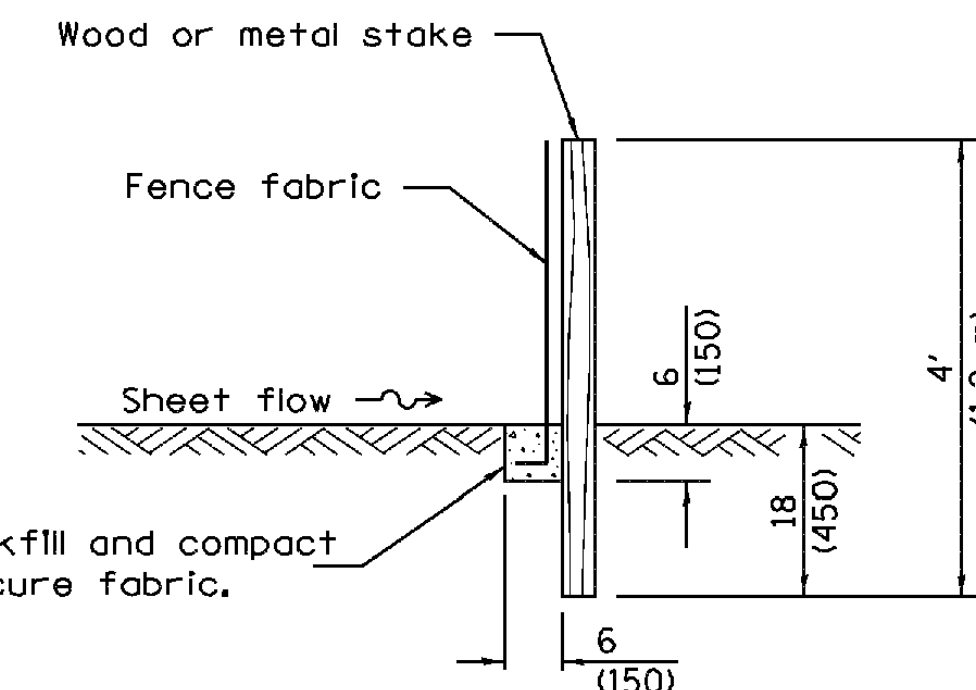


ELEVATION

SILT FILTER FENCE AS A PERIMETER EROSION BARRIER



SLICE METHOD



TRENCH METHOD

SECTION A-A

Excavate, backfill and compact trench to secure fabric.

GENERAL NOTES

The installation details and dimensions shown for perimeter erosion barriers shall also apply for inlet and pipe protection.

All dimensions are in inches (millimeters) unless otherwise shown.

DATE	REVISIONS
1-1-13	Corrected notation for flowline (f) on SEDIMENT BASIN ELEVATION.
1-1-12	Omitted hay/straw perimeter barrier. Added SLICE METHOD to SECTION A-A.

TEMPORARY EROSION CONTROL SYSTEMS
(Sheet 1 of 2)

STANDARD 280001-07

SECTION NO.: 12-E9121-00-BT
DRAWN BY: AP
CHECKED BY: JRK
APPROVED BY: GAO
DATE: 8/12/2016
SCALE: N.T.S.

NO.	DATE	REVISIONS/ISSUANCES
1	8/12/16	ISSUED FOR FINAL
2	5/6/16	ISSUED FOR FINAL
3	6/05/15	ISSUED FOR FINAL REVIEW

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312.744.3600



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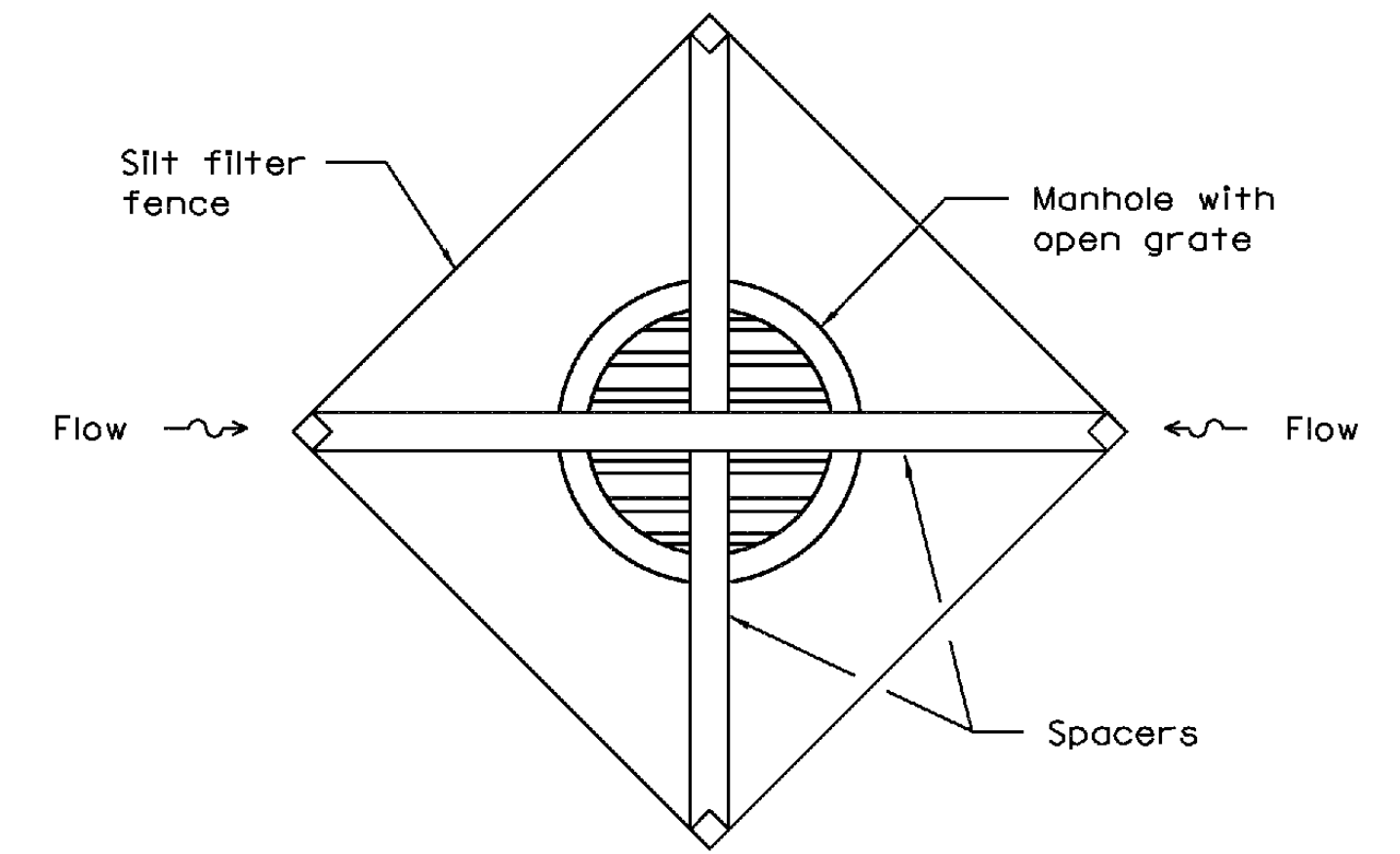
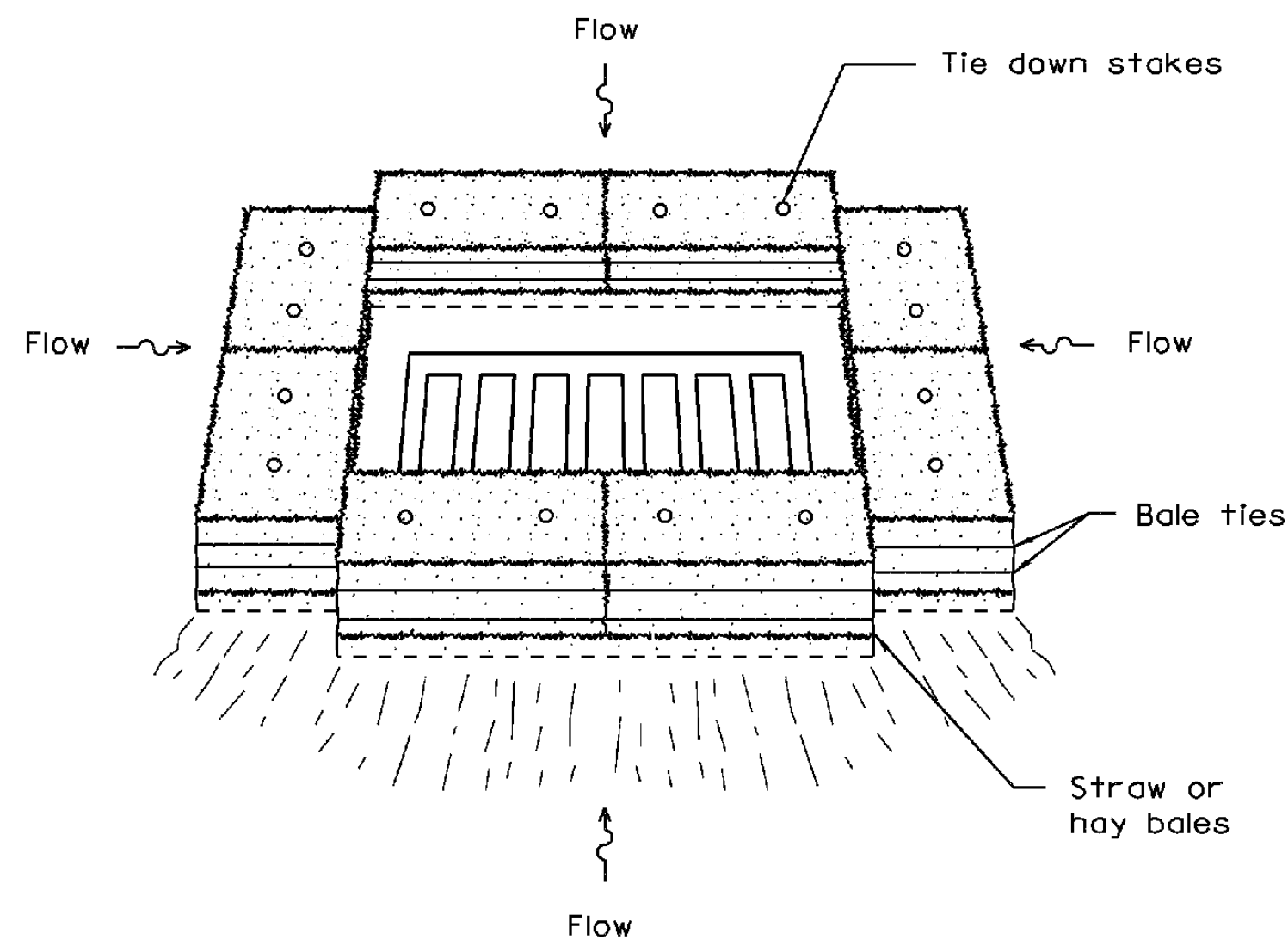
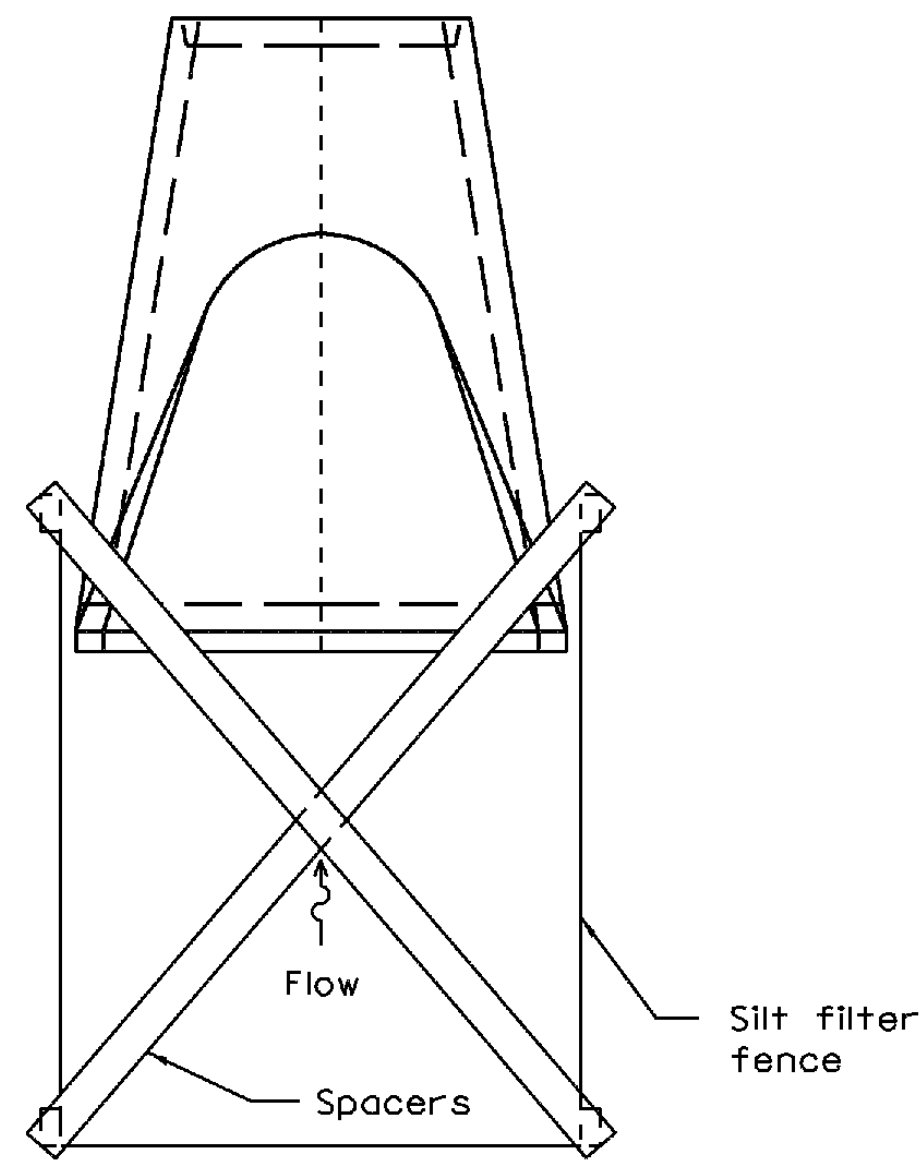
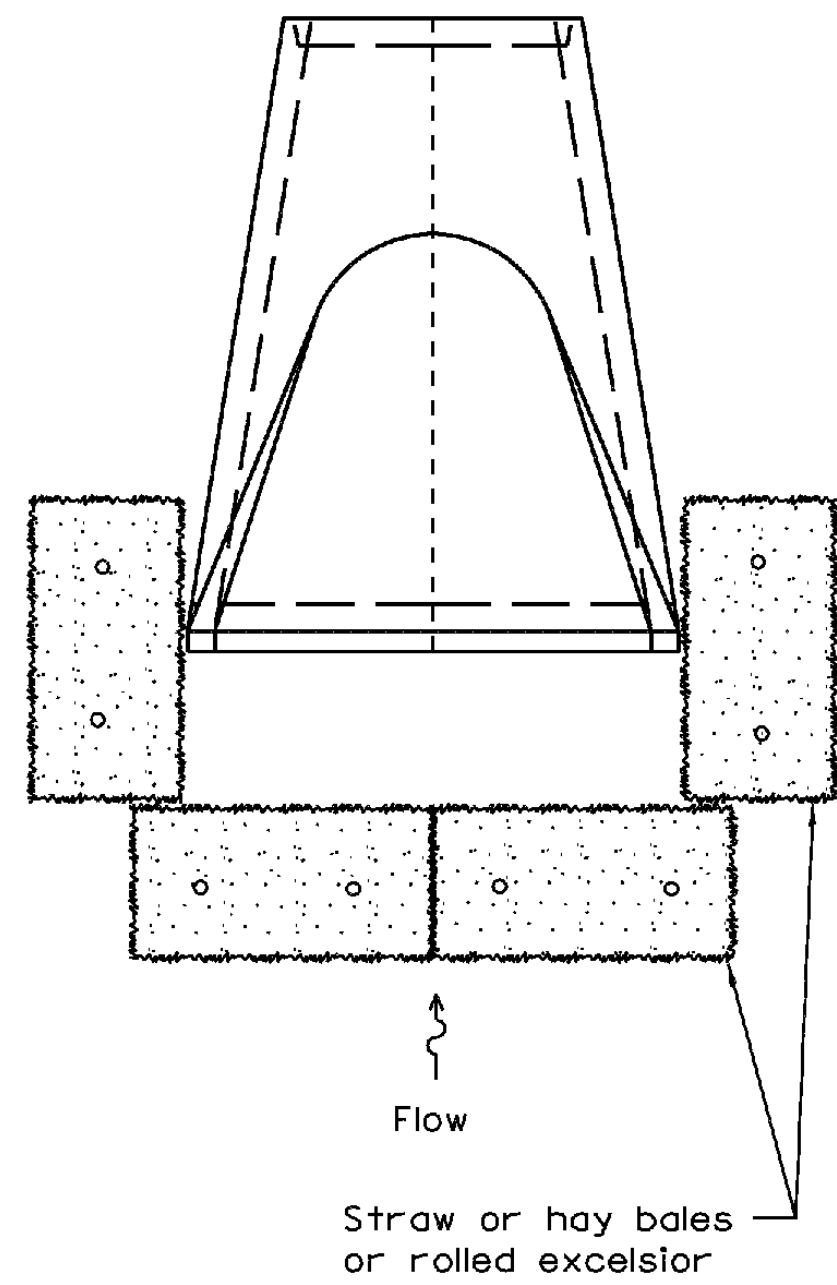
ADDISON UNDERBRIDGE TRAIL CONNECTION FROM CLARK PARK TO CALIFORNIA PARK

EROSION AND SEDIMENT CONTROL DETAILS

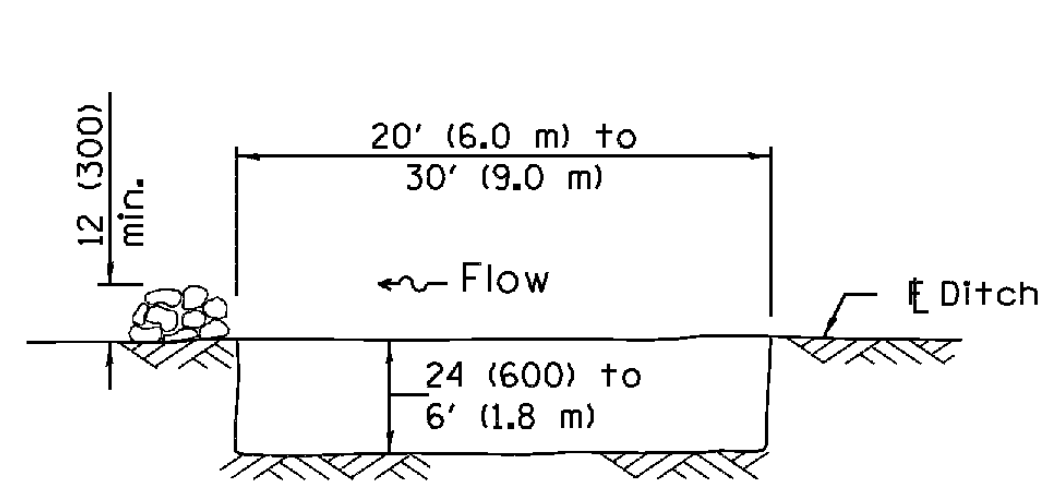
SHEET NO.
C-24

SHEET 24 OF 158

REVISION

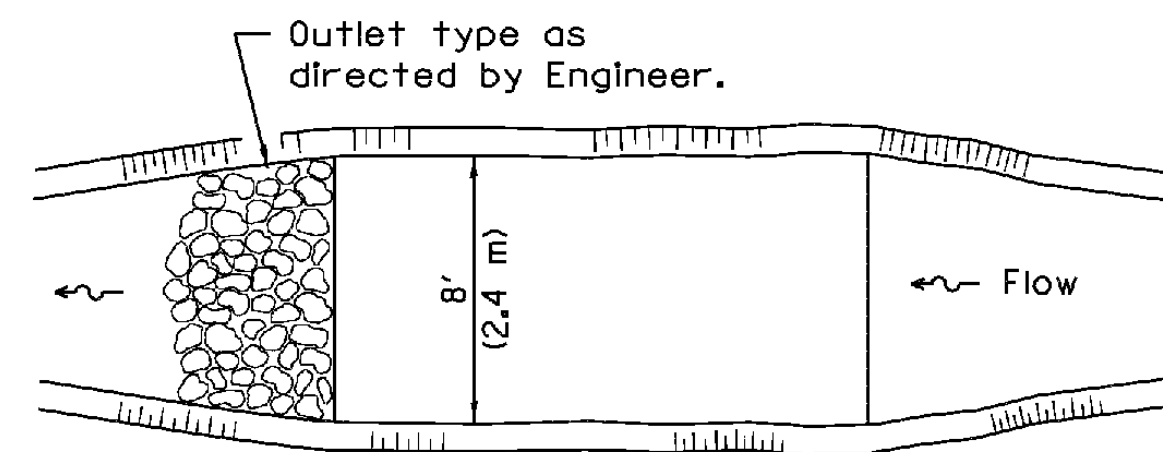


INLET AND PIPE PROTECTION



The performance of the basin will improve if put into a series.

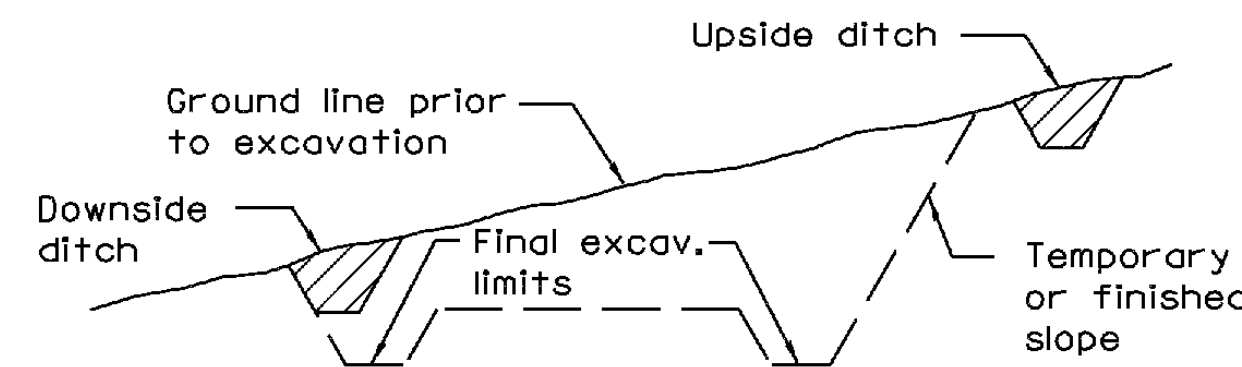
ELEVATION



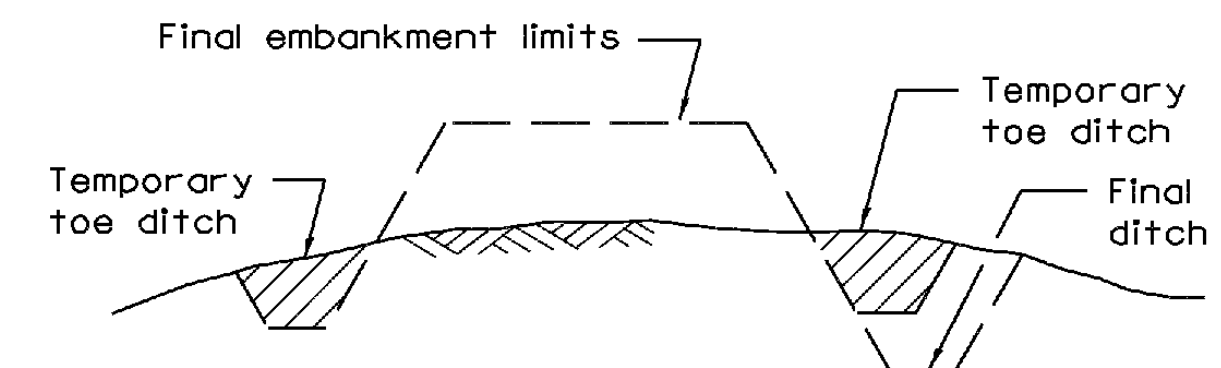
The long dimension should be parallel with the direction of the flow. Accumulated silt shall be removed anytime the basins become 75% filled.

PLAN

SEDIMENT BASIN



TYPICAL CUT CROSS-SECTION



TYPICAL FILL CROSS-SECTION

TEMPORARY DITCHES FOR CUT & FILL SECTIONS

Illinois Department of Transportation	
PASSED	January 1, 2013
Michael Brand ENGINEER OF POLICY AND PROCEDURES	
APPROVED	January 1, 2013
[Signature] ENGINEER OF DESIGN AND ENVIRONMENT	
ISSUED	1-1-97

TEMPORARY EROSION CONTROL SYSTEMS

(Sheet 2 of 2)

STANDARD 280001-07

SECTION NO.: 12-E9121-00-BT
 DRAWN BY: AP
 CHECKED BY: JRK
 APPROVED BY: GAO
 DATE: 8/12/2016
 SCALE: N.T.S.

NO.	DATE	REVISIONS/ISSUANCES
1	8/12/16	ISSUED FOR FINAL
2	5/6/16	ISSUED FOR FINAL
3	6/05/15	ISSUED FOR FINAL REVIEW

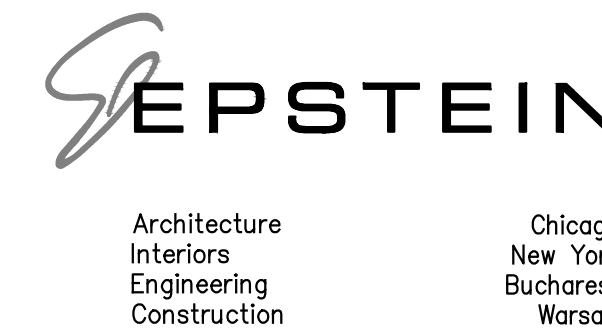
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ADDISON UNDERBRIDGE TRAIL CONNECTION FROM CLARK PARK TO CALIFORNIA PARK

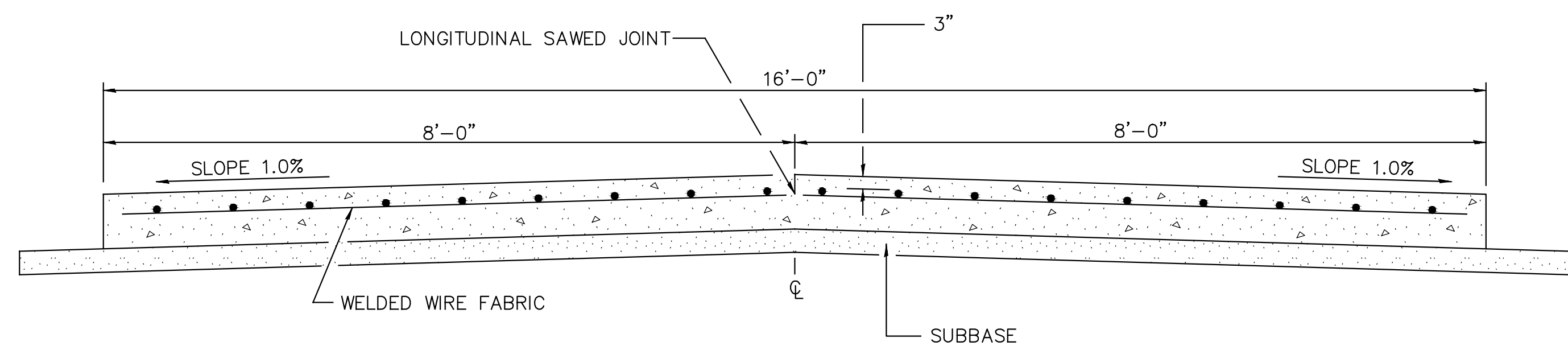
EROSION AND SEDIMENT CONTROL DETAILS

SHEET NO. C-25

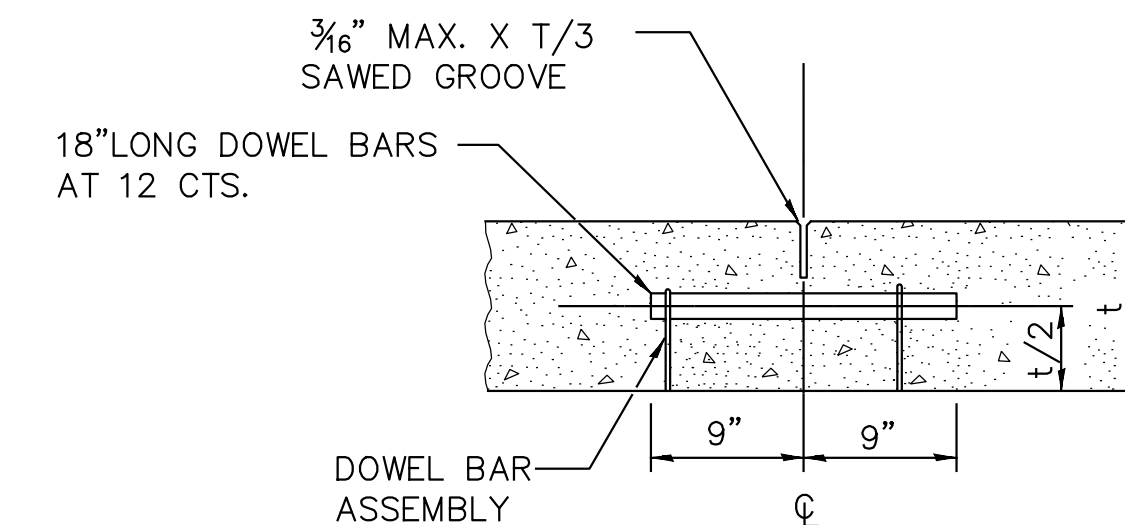
SHEET 25 OF 158

REVISION

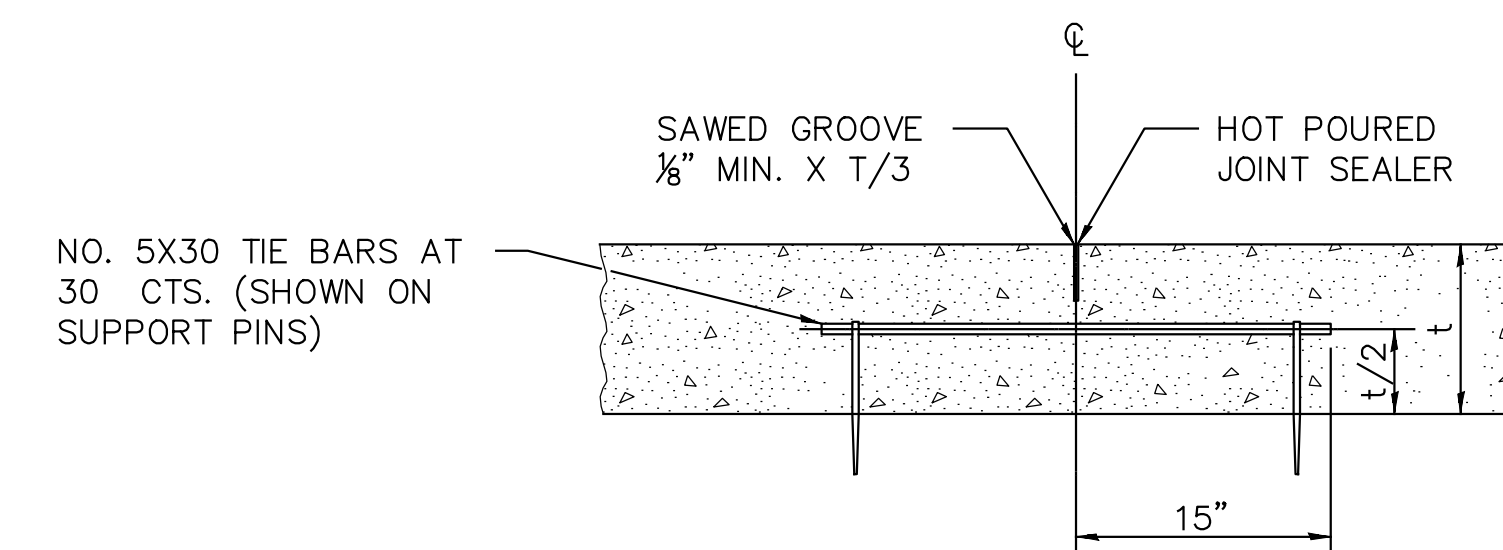
January 24, 2016 10:34:46 AM - CDOT - Addison Underbridge Connection - \DISCIPLINE\CH\Working\Sheets\026_PCC_Joining.dwg



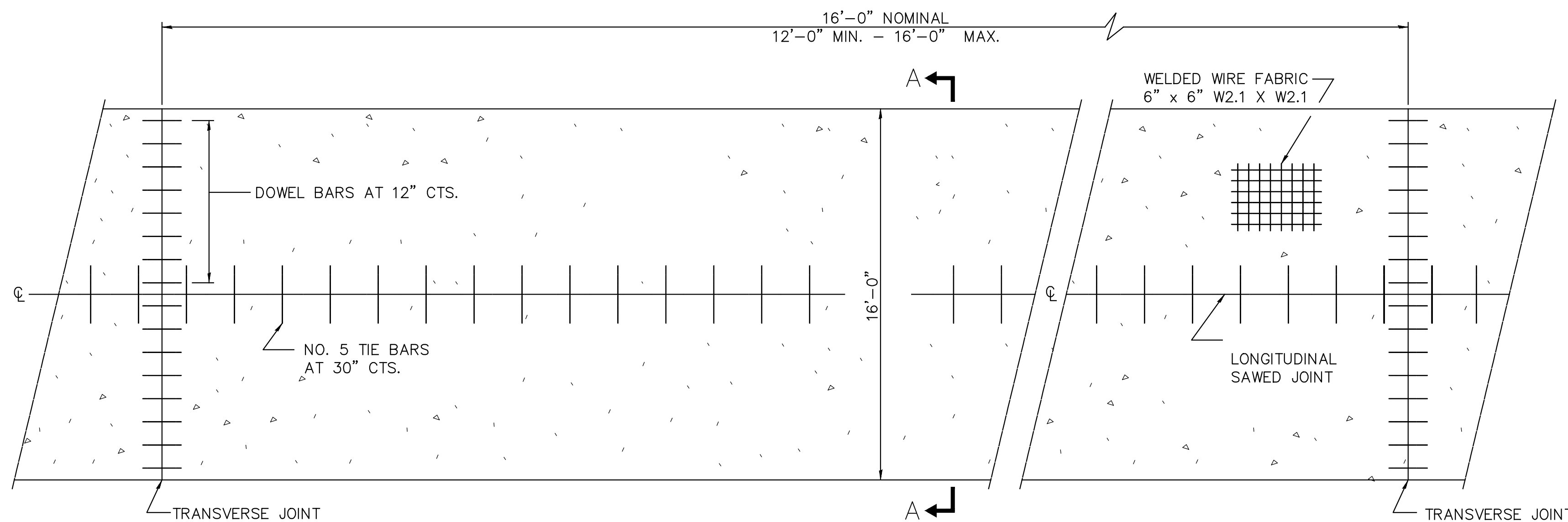
SECTION A-A



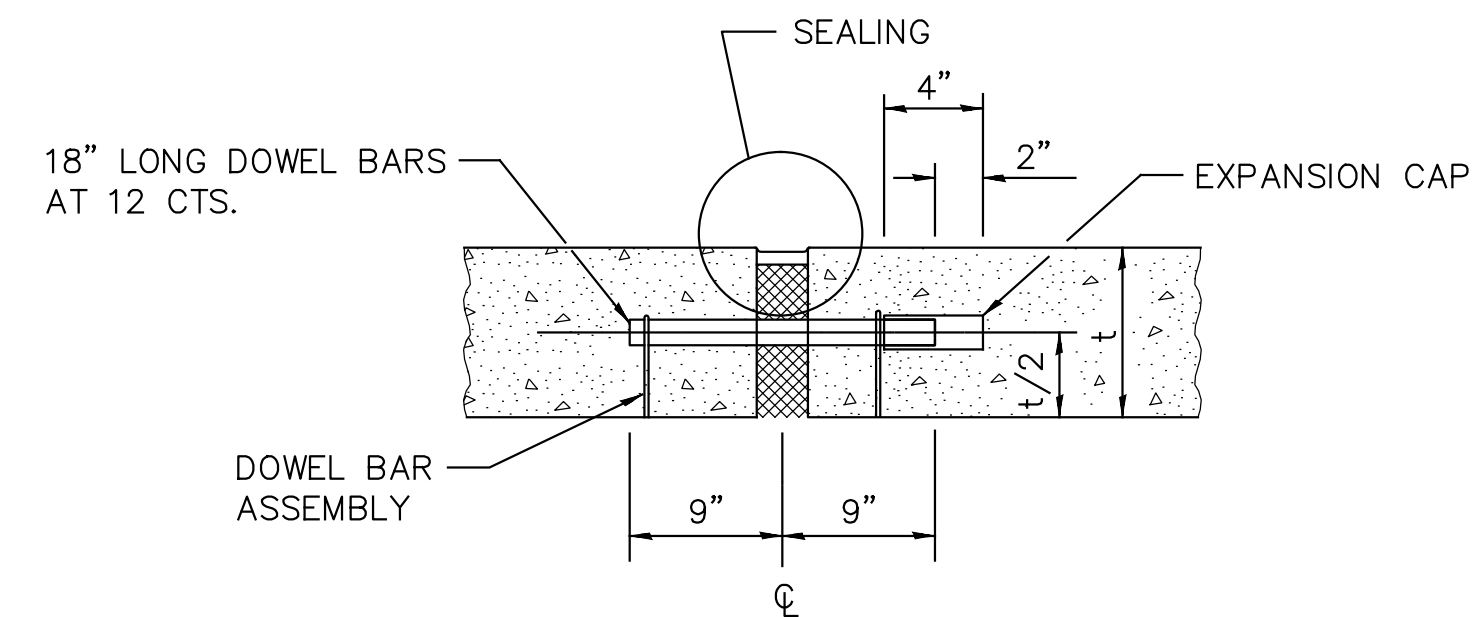
TRANSVERSE CONTRACTION JOINT



LONGITUDINAL SAWED JOINT



PLAN



TRANSVERSE EXPANSION JOINT

GENERAL NOTES

1. TRANSVERSE JOINTS SHALL NOT BE CONSTRUCTED WITHIN ONE FOOT OF RAILING ANCHOR
2. TRANSVERSE EXPANSION JOINTS SHALL BE PLACED IN THE PCC PAVEMENT TEN FEET FROM THE ABUTMENTS AT THE CSO BRIDGE (STATION 202+73 AND STATION 203+36)

SECTION NO.: 12-E9121-00-BT
 DRAWN BY: AP
 CHECKED BY: JRK
 APPROVED BY: GAO
 DATE: 8/12/2016
 SCALE: N.T.S.

NO.	DATE	REVISIONS/ISSUANCES
1	8/12/16	ISSUED FOR FINAL
2	5/6/16	ISSUED FOR FINAL
3	6/05/15	ISSUED FOR FINAL REVIEW

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ADDISON UNDERBRIDGE TRAIL CONNECTION FROM
 CLARK PARK TO CALIFORNIA PARK

PAVEMENT JOINT DETAILS

SHEET NO.
C-26

SHEET 26 OF 158

REVISION

STANDARD DRAINAGE STRUCTURES FOR PRIVATE DEVELOPMENT

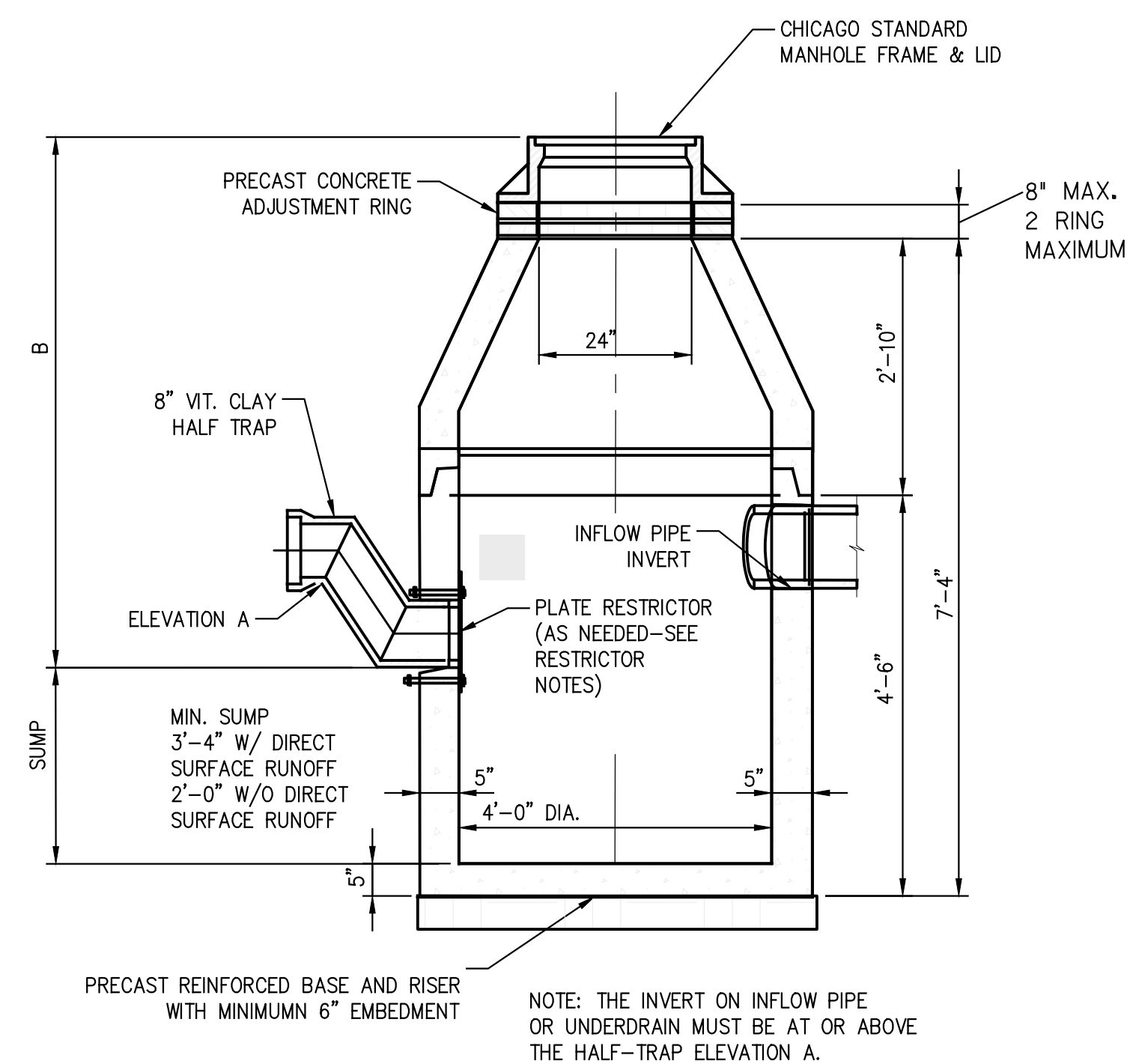
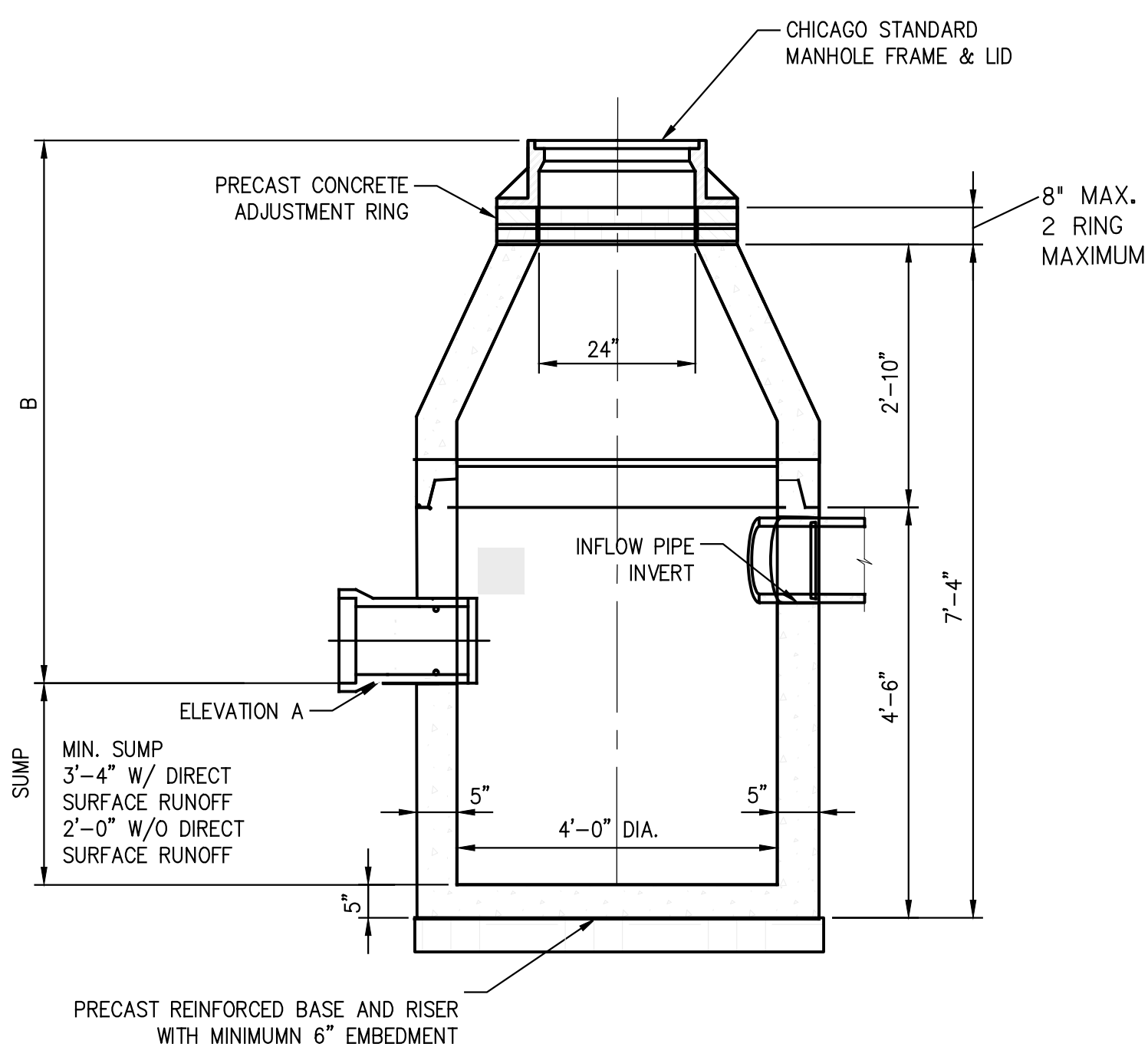
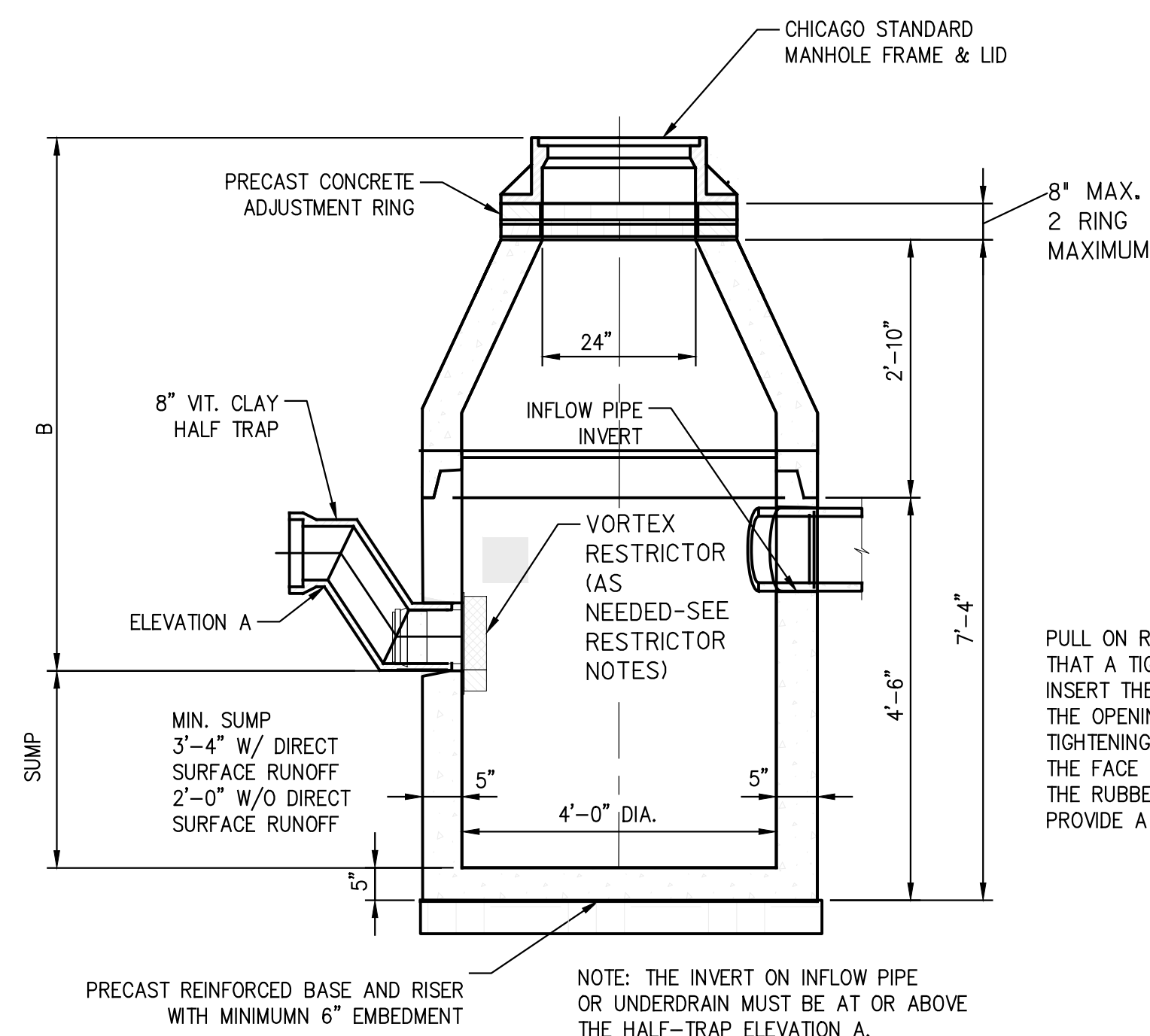


PLATE RESTRICTOR CATCH BASIN
CONICAL SECTION
N.T.S.

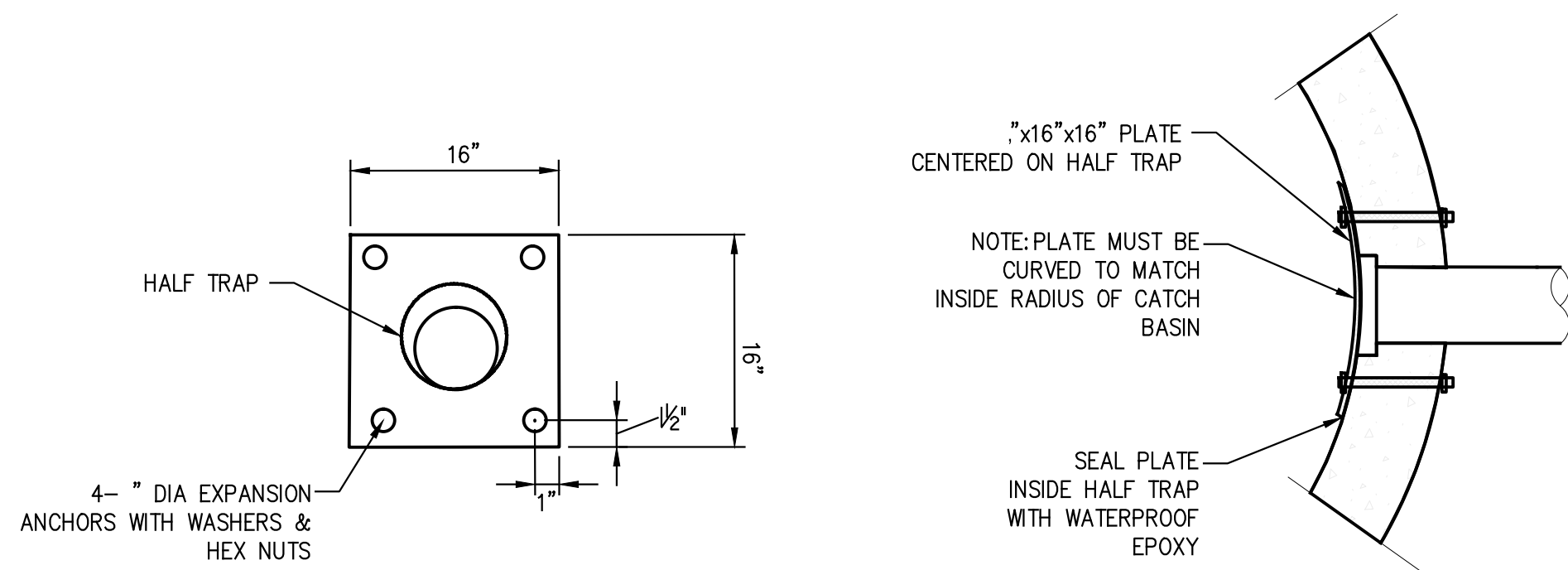


STANDARD CATCH BASIN
CONICAL SECTION
N.T.S.



VORTEX RESTRICTOR CATCH BASIN
CONICAL SECTION
N.T.S.

PULL ON RESTRICTOR TO VERIFY THAT A TIGHT FIT IS MADE. INSERT THE RESTRICTOR WITH THE OPENING DOWN. UPON TIGHTENING OF THE 2 BOLTS ON THE FACE OF THE RESTRICTOR, THE RUBBER O-RINGS WILL PROVIDE A WATER-TIGHT SEAL.

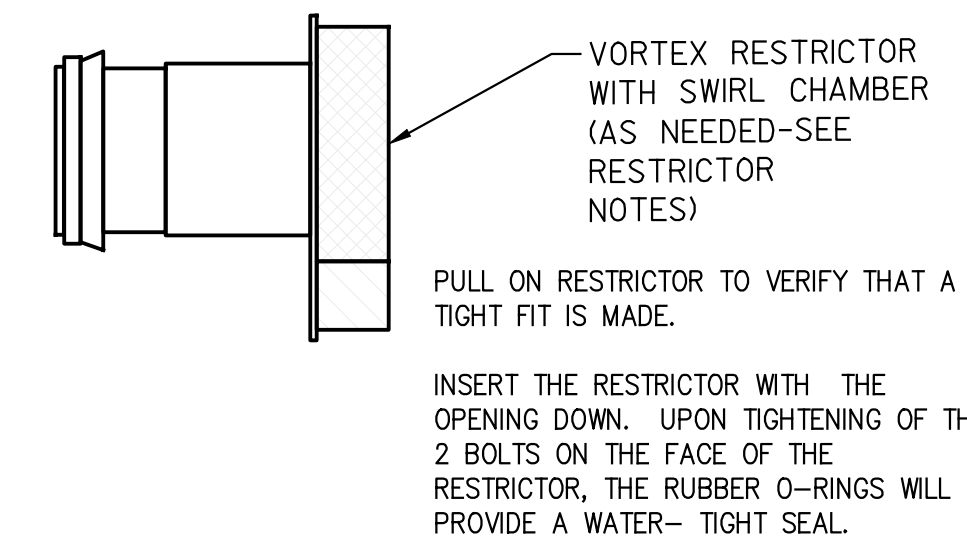


NOTES:

1. THE RESTRICTOR MUST BE INSTALLED IN CATCH BASINS OR MANHOLES WITH 2' MIN. SUMPS.
2. PLATE AND FASTENERS MUST BE FABRICATED IN STAINLESS STEEL, DUCTILE IRON, OR EQUIVALENT WATERPROOF/WEATHER PROOF MATERIALS.
3. MIN. ORIFICE SIZE= 2.5" DIA.
4. IF B < 4 FEET, THEN USE A FLAT TOP SLAB CATCH BASIN AS NECESSARY.

GENERAL NOTES:

1. CATCH BASIN TO CATCH BASIN CONNECTIONS ARE ALLOWED IN PRIVATE SITES & ALLEYS. ONLY THE DOWNSTREAM CATCH BASIN IS REQUIRED TO HAVE A HALF-TRAP.
2. IF B < 4 FEET, THEN USE A FLAT TOP SLAB CATCH BASIN AS NECESSARY.
3. INLETS AND 3' DIAMETER CATCH BASINS ARE TO BE USED ONLY WITH PRIOR APPROVAL OF THE DEPT OF WATER MANAGEMENT- ENGINEERING SERVICES, SEWER SECTION, AND THE DEPT. OF BUILDINGS STORMWATER REVIEWER.



NOTES:

1. IF B < 4 FEET, THEN USE A FLAT TOP SLAB CATCH BASIN AS NECESSARY.
2. THE RESTRICTORS CAN BE OBTAINED FROM DWM CENTRAL DISTRICT AT 3901 S. ASHLAND AVE. THE CONTRACTOR SHOULD ARRANGE FOR PICK UP BY CONTACTING 312-747-1177 (7AM TO 3PM, M-F)

USE FOR ITEM: CATCH BASIN TYPE A, 4' DIAMETER (CITY OF CHICAGO)

STANDARD REVISIONS		PERCENT COMPLETE	DATE	CITY OF CHICAGO DEPARTMENT OF WATER MANAGEMENT BUREAU OF ENGINEERING SERVICES	DRAWN: DESIGNED: CHECKED: REVIEWED:	SHEET NO. A.16
DATE	DESCRIPTION					
1/22/14	APPROVED PLAN	30		DRAINAGE STRUCTURE DETAILS PRIVATE DEVELOPMENT	OF	PN
		60				
		75				
		90				
		100				
		BULLETIN				

SECTION NO.: 12-E9121-00-BT
DRAWN BY: AP
CHECKED BY: JRK
APPROVED BY: GAO
DATE: 8/12/2016
SCALE: N.T.S.

NO.	DATE	REVISIONS/ISSUANCES
1	8/12/16	ISSUED FOR FINAL
2	5/6/16	ISSUED FOR FINAL
3	6/05/15	ISSUED FOR FINAL REVIEW

CITY OF CHICAGO
DEPARTMENT OF TRANSPORTATION
30 N LaSalle St. Suite 1100
Chicago, IL 60602
312.744.3600



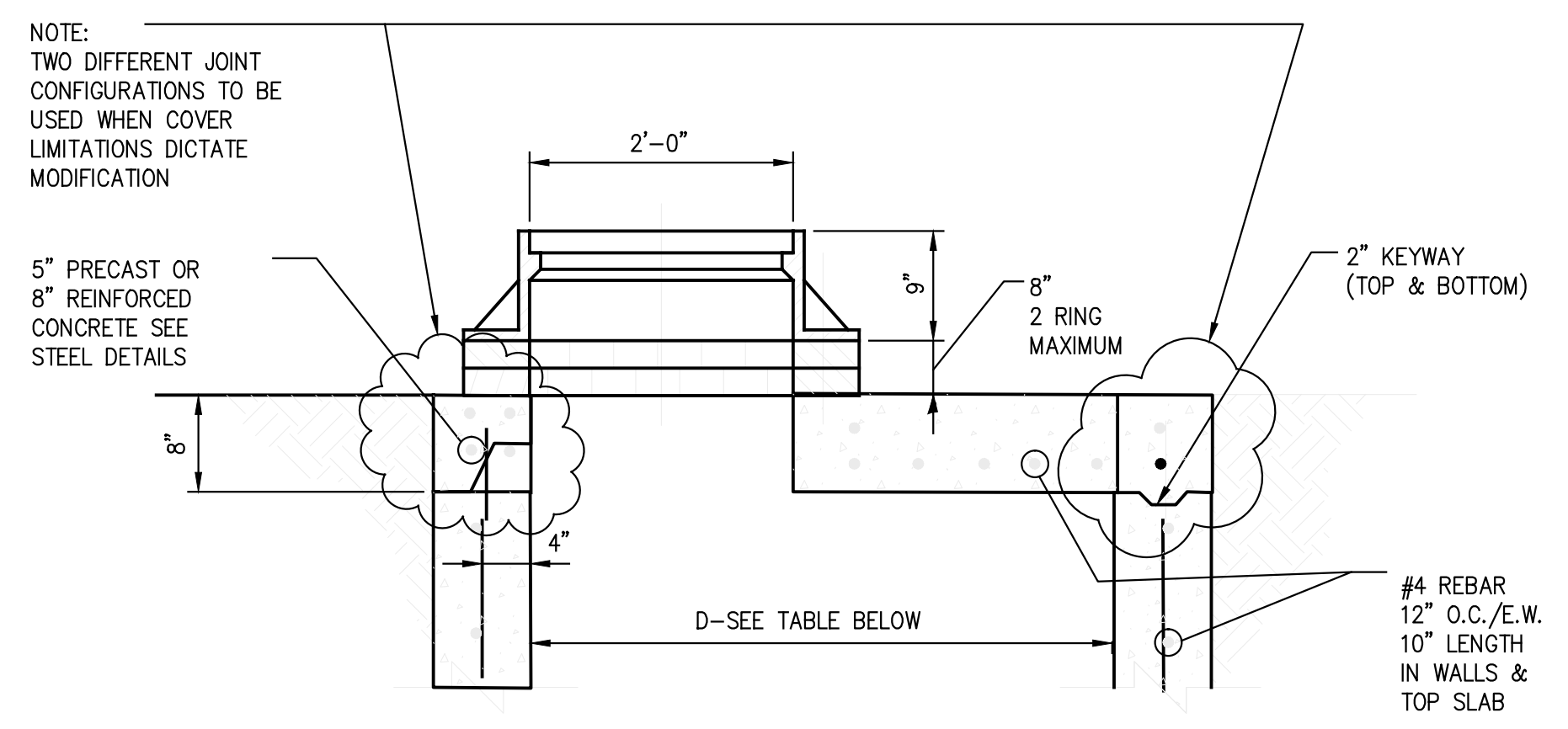
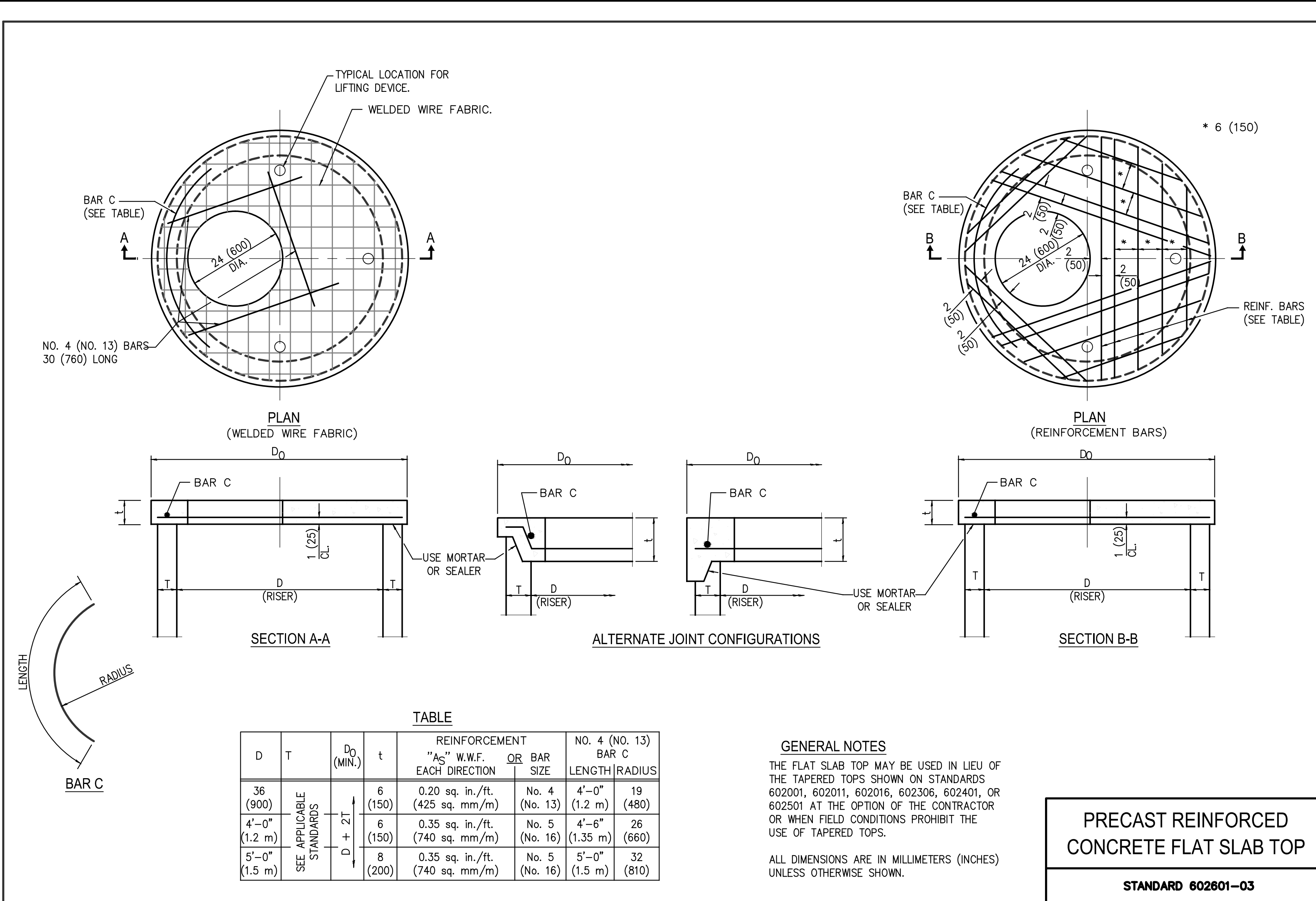
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ADDISON UNDERBRIDGE TRAIL CONNECTION FROM CLARK PARK TO CALIFORNIA PARK
CHICAGO DOWM CATCH BASIN DETAIL

SHEET NO. C-27
SHEET 27 OF 158
REVISION

SPECIAL DRAINAGE STRUCTURES
FOR PUBLIC STREETS AND ALLEYS



STANDARD FLAT TOP SLAB
FOR CATCH BASINS

NOTES:
FLAT TOP SLAB CAN ONLY BE USED WITH WRITTEN PERMISSION FROM CDWM.
USE LATEST IDOT DETAIL, #602601

January 24, 2016 10:28:34 AM - Addison Underbridge Connection - CDOT - Addison Underbridge Connection - DISCIPLINE: CHA Working Sheets 028_FLAT SLAB TOP.dwg

STANDARD REVISIONS		PERCENT COMPLETE	DATE	CITY OF CHICAGO DEPARTMENT OF WATER MANAGEMENT BUREAU OF ENGINEERING SERVICES	DRAWN: SSW DESIGNED: --- CHECKED: --- REVIEWED: ---
DATE	DESCRIPTION				
1/22/14	APPROVED PLAN	30	-----	FLAT TOP SLAB DETAILS	A.8 OF PN
---	---	60	-----		
---	---	75	-----		
---	---	90	-----		
---	---	100	-----		
		BULLETIN			

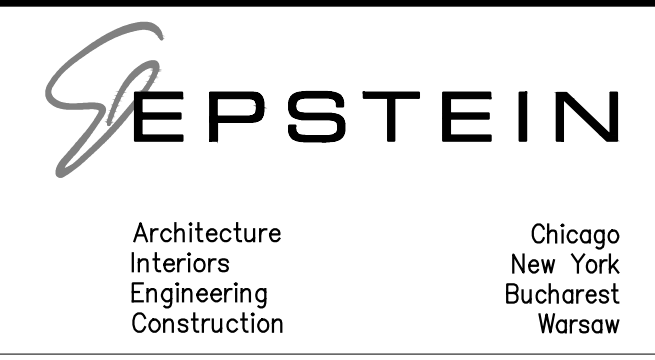
SECTION NO.: 12-E9121-00-BT
DRAWN BY: AP
CHECKED BY: JRK
APPROVED BY: GAO
DATE: 8/12/2016
SCALE: N.T.S.

NO.	DATE	REVISIONS/ISSUANCES
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2	5/6/16	ISSUED FOR FINAL
3	6/05/15	ISSUED FOR FINAL REVIEW

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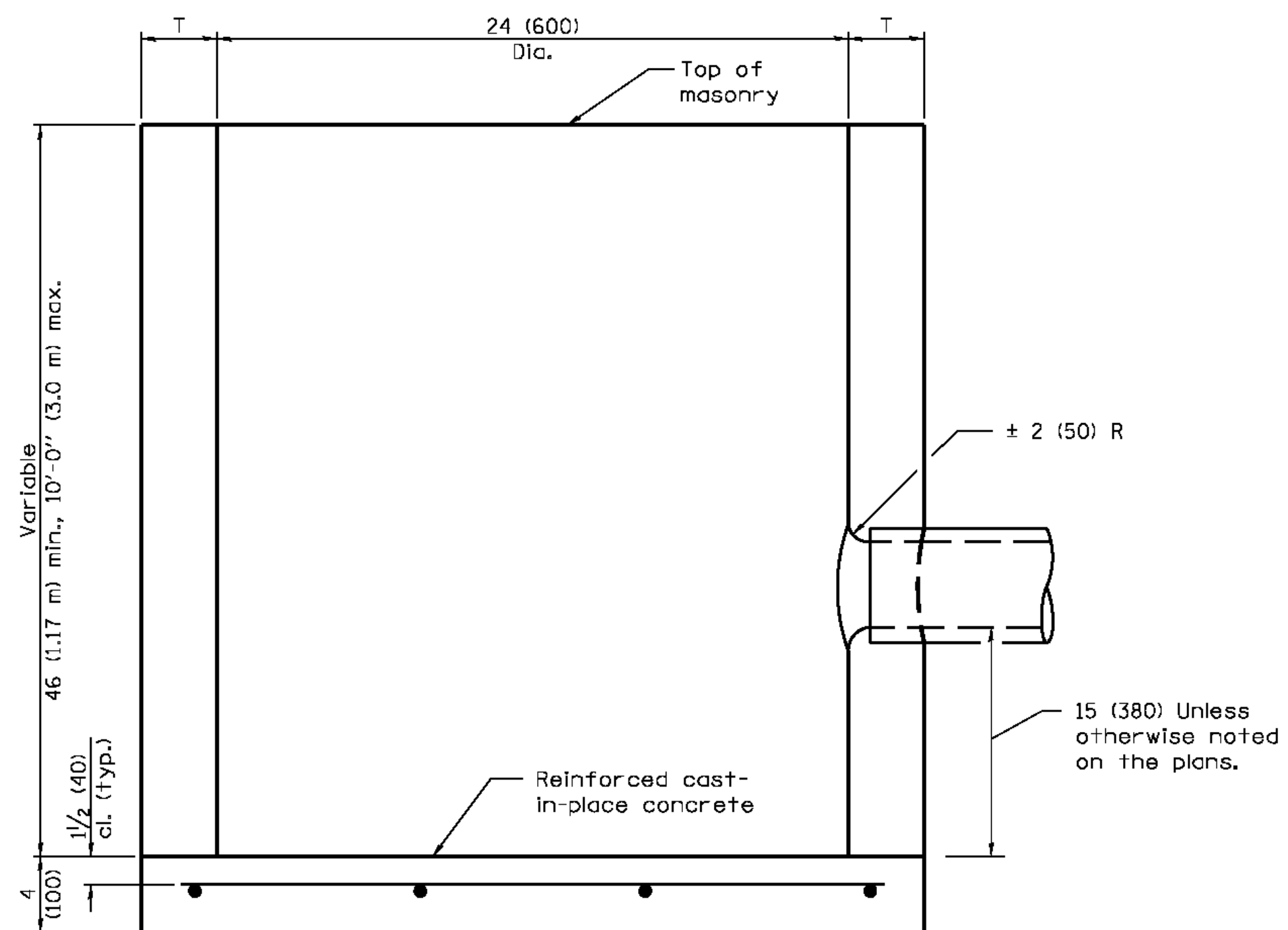


ADDISON UNDERBRIDGE TRAIL CONNECTION FROM CLARK PARK TO CALIFORNIA PARK
CHICAGO DOWNM FLAT SLAB TOP DETAIL

SHEET NO. C-28
SHEET 28 OF 158

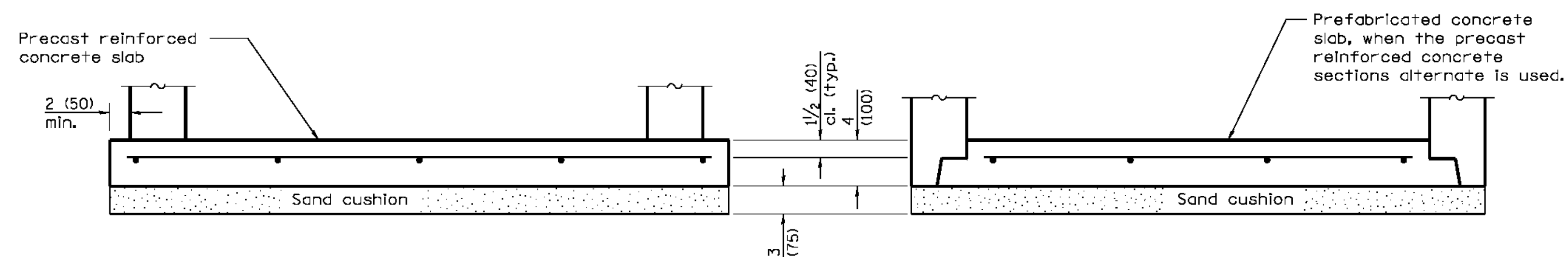
REVISION

January 24, 2016 10:34:46 - CDOT - Addison Underbridge Connection - \DISCIPLINE\CH\Working\Sheets\028a_CB_T1_C.dwg



ELEVATION

ALTERNATE MATERIALS FOR WALLS	T (min)
Precast Reinforced Concrete Section	3 (75)
Concrete Masonry Unit	5 (125)
Cast-In-Place Concrete	6 (150)
Brick Masonry	8 (200)



ALTERNATE BOTTOM SLAB

GENERAL NOTES

Bottom slabs shall be reinforced with a minimum of 0.27 sq. in./ft. (570 sq. mm/m) in both directions with a maximum spacing of 9 (230).

Bottom slabs may be connected to the riser as determined by the fabricator; however, only a single row of reinforcement around the perimeter may be utilized.

All dimensions are in inches (millimeters) unless otherwise shown.

DATE	REVISIONS	CATCH BASIN TYPE C
1-1-11	Detailed rein. in slabs.	
	Added max. limit to height.	
	Added general notes.	
1-1-09	Switched units to	
	English (metric).	
		STANDARD 602011-02

Illinois Department of Transportation	
PASSED	January 1, 2011
Michael Beard ENGINEER OF POLICY AND PROCEDURES	
APPROVED	January 1, 2011
Scott Smith ENGINEER OF DESIGN AND ENVIRONMENT	
ISSUED	1-1-16

SECTION NO.: 12-E9121-00-BT
 DRAWN BY: JRK
 CHECKED BY: JRK
 APPROVED BY: GAO
 DATE: 8/12/2016
 SCALE: N.T.S.

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3	6/05/15	ISSUED FOR FINAL REVIEW

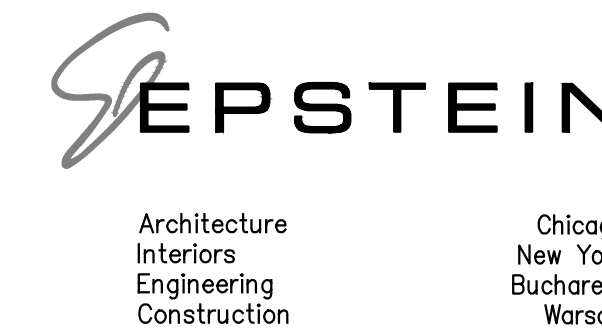
CITY OF CHICAGO

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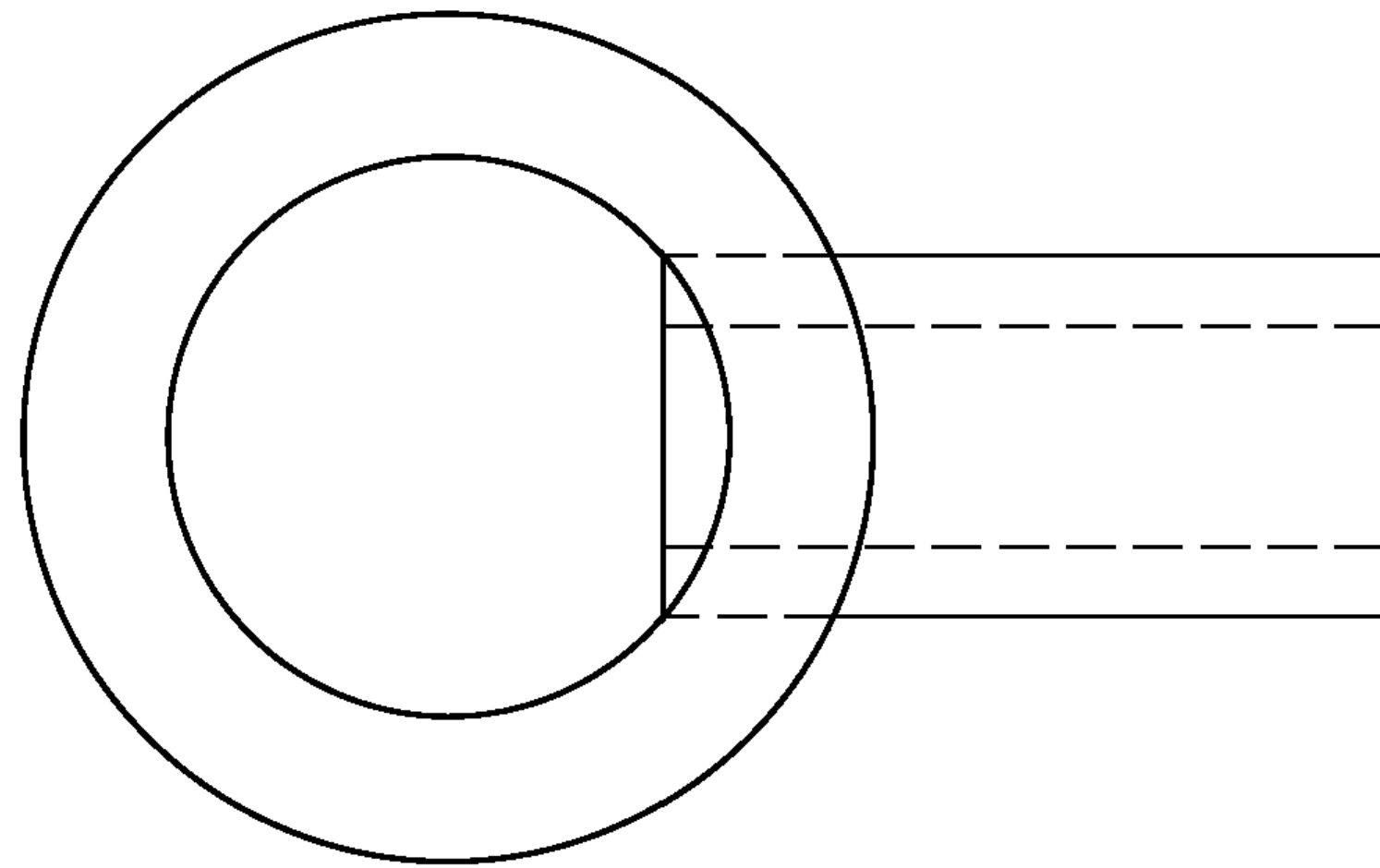
ADDISON UNDERBRIDGE TRAIL CONNECTION FROM
 CLARK PARK TO CALIFORNIA PARK

CATCH BASIN TYPE C

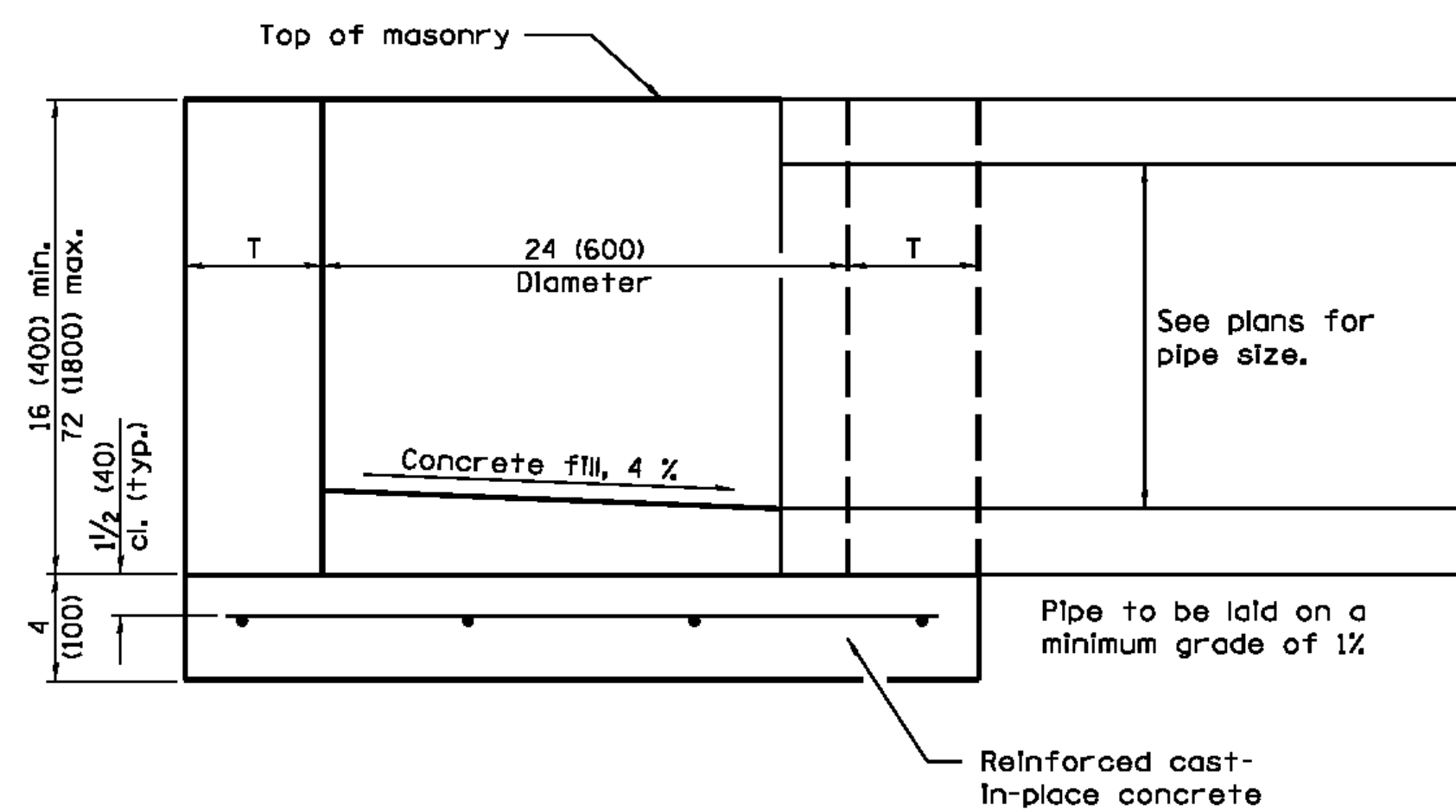
SHEET NO.
C-29

SHEET 29 OF 158

REVISION

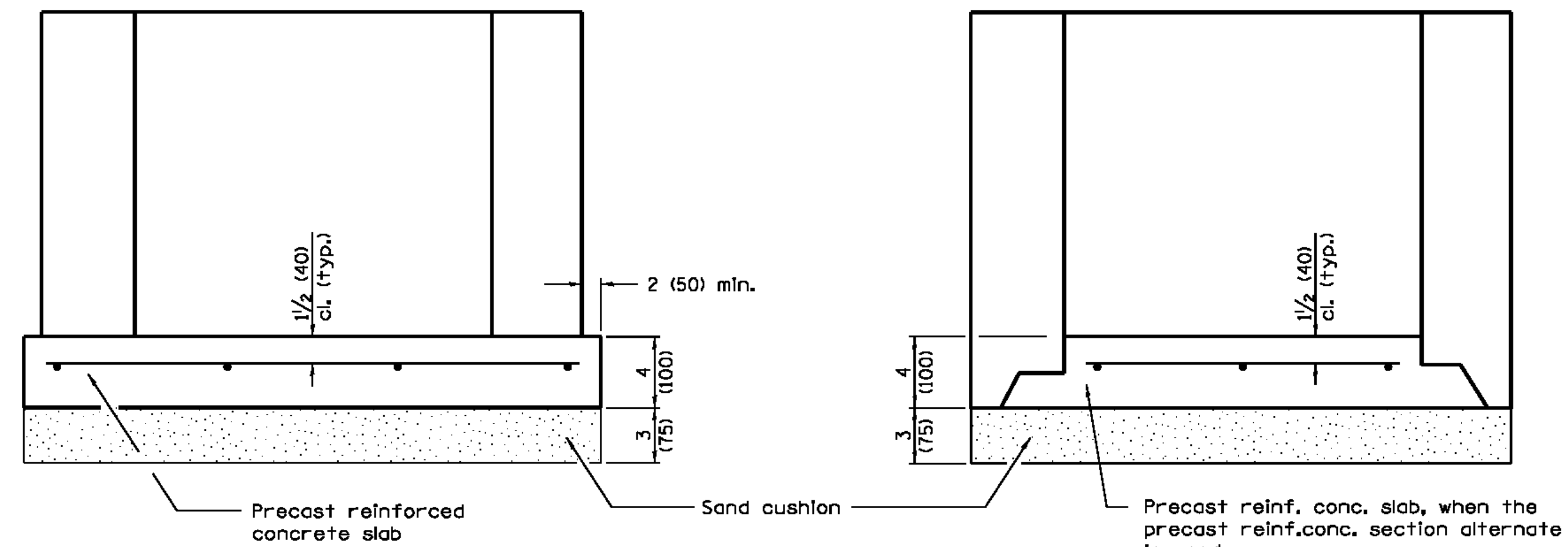


PLAN



ELEVATION

ALTERNATE MATERIALS FOR WALLS	T
BRICK MASONRY	8 (200)
CAST-IN-PLACE CONCRETE	6 (150)
CONCRETE MASONRY UNIT	5 (125)
PRECAST REINFORCED CONCRETE SECTION	3 (75)



ALTERNATE METHODS

GENERAL NOTES

Bottom slabs shall be reinforced with a minimum of 0.24 sq. in./ft. (510 sq. mm/m) in both directions with a maximum spacing of 10 (250).

Bottom slabs may be connected to the riser as determined by the fabricator; however, only a single row of reinforcement around the perimeter may be utilized.

All dimensions are in inches (millimeters) unless otherwise shown.

DATE	REVISIONS	
1-1-14	Increased height to 72 (1800) maximum.	INLET - TYPE A
1-1-11	Detailed rein. in slabs.	STANDARD 602301-04
	Added max. limit to height.	
	Added general notes.	

Illinois Department of Transportation

PASSED January 1, 2014

ENGINEER OF POLICY AND PROCEDURES

APPROVED January 1, 2014

ENGINEER OF DESIGN AND ENVIRONMENT

ISSUED 1-1-14

January 24, 2016 10:00 AM - CDOT - Addison Underbridge Connection - (DISCIPLINE) (CH) (Working) (Sheets) (0208) (IN) (TY) (A) (mg)

SECTION NO.: 12-E9121-00-BT
 DRAWN BY: JRK
 CHECKED BY: JRK
 APPROVED BY: GAO
 DATE: 8/12/2016
 SCALE: N.T.S.

NO.	DATE	REVISIONS/ISSUANCES
1	8/12/16	ISSUED FOR FINAL
2	5/6/16	ISSUED FOR FINAL
3	6/05/15	ISSUED FOR FINAL REVIEW

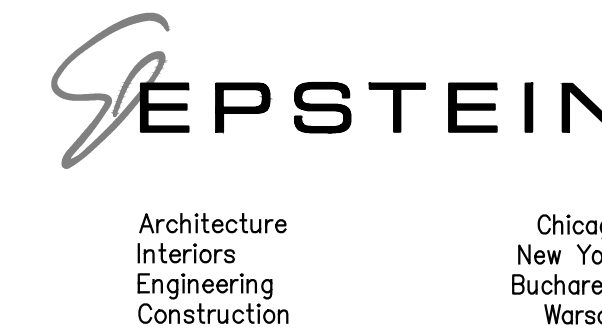
CITY OF CHICAGO

DEPARTMENT OF TRANSPORTATION
 30 N LaSalle St. Suite 1100
 Chicago, IL 60602
 312.744.3600



CONSULTANT

EPSTEIN
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ADDISON UNDERBRIDGE TRAIL CONNECTION FROM CLARK PARK TO CALIFORNIA PARK

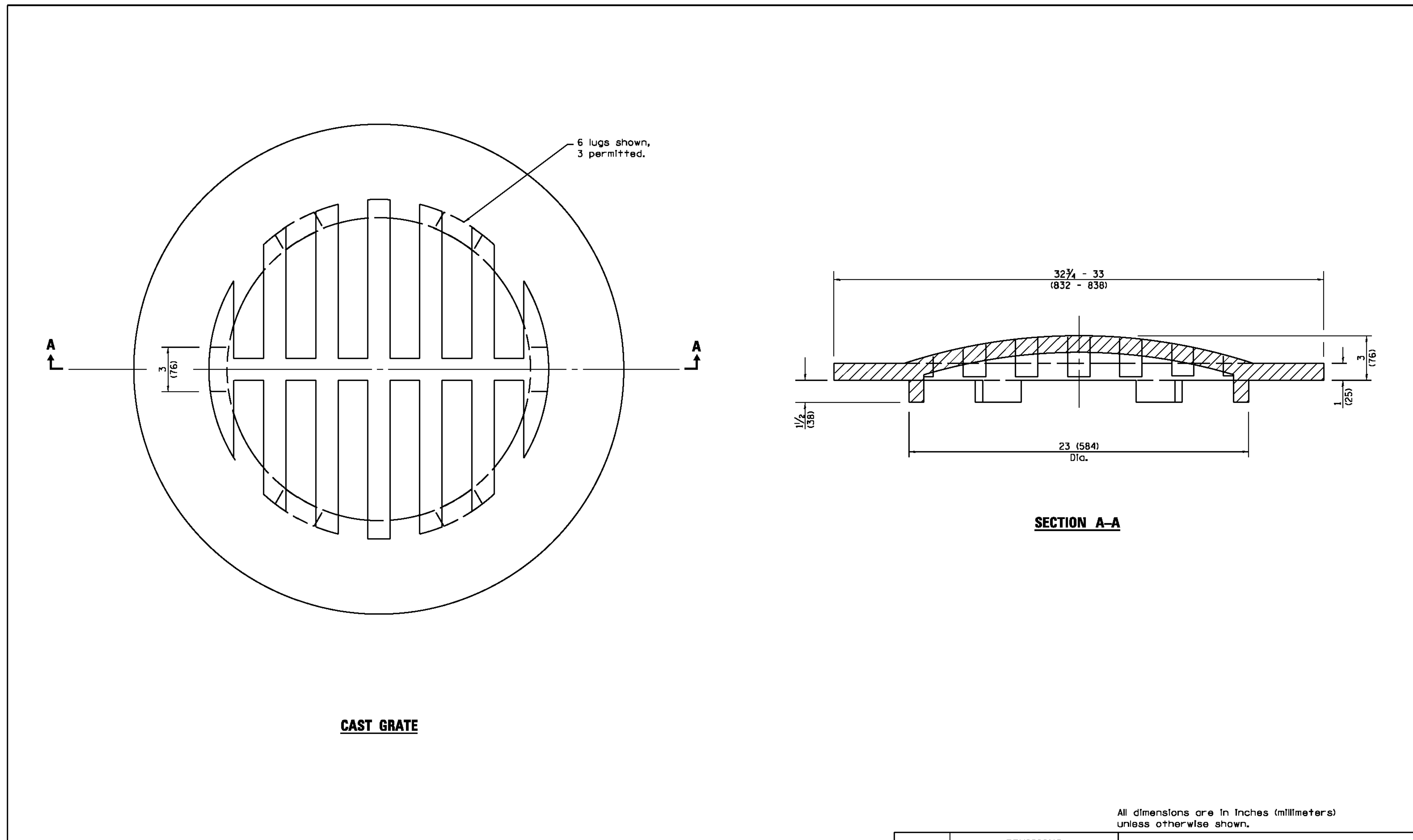
INLET TYPE A

SHEET NO. C-30

SHEET 30 OF 158

REVISION

January 24, 2016 10:00:00 AM C:\Users\Addison Underbridge Connection\DISCIPLINE\CH\Working\Sheets\029_Grates\Types8.dwg



CAST GRATE

SECTION A-A

All dimensions are in inches (millimeters) unless otherwise shown.

Illinois Department of Transportation	
PASSED	January 1, 2015
<i>Michael Beard</i> ENGINEER OF POLICY AND PROCEDURES	
APPROVED	January 1, 2015
 ENGINEER OF DESIGN AND ENVIRONMENT	
ISSUED	1-1-15

DATE	REVISIONS	<p style="text-align: center;">GRATE TYPE 8</p> <p style="text-align: center;">STANDARD 604036-03</p>
1-1-15	Revised dimensions.	
1-1-09	Switched units to English (metric).	

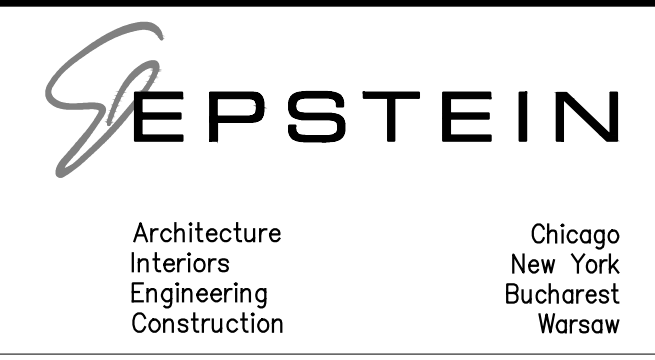
SECTION NO.: 12-E9121-00-BT
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 DATE: 8/12/2016
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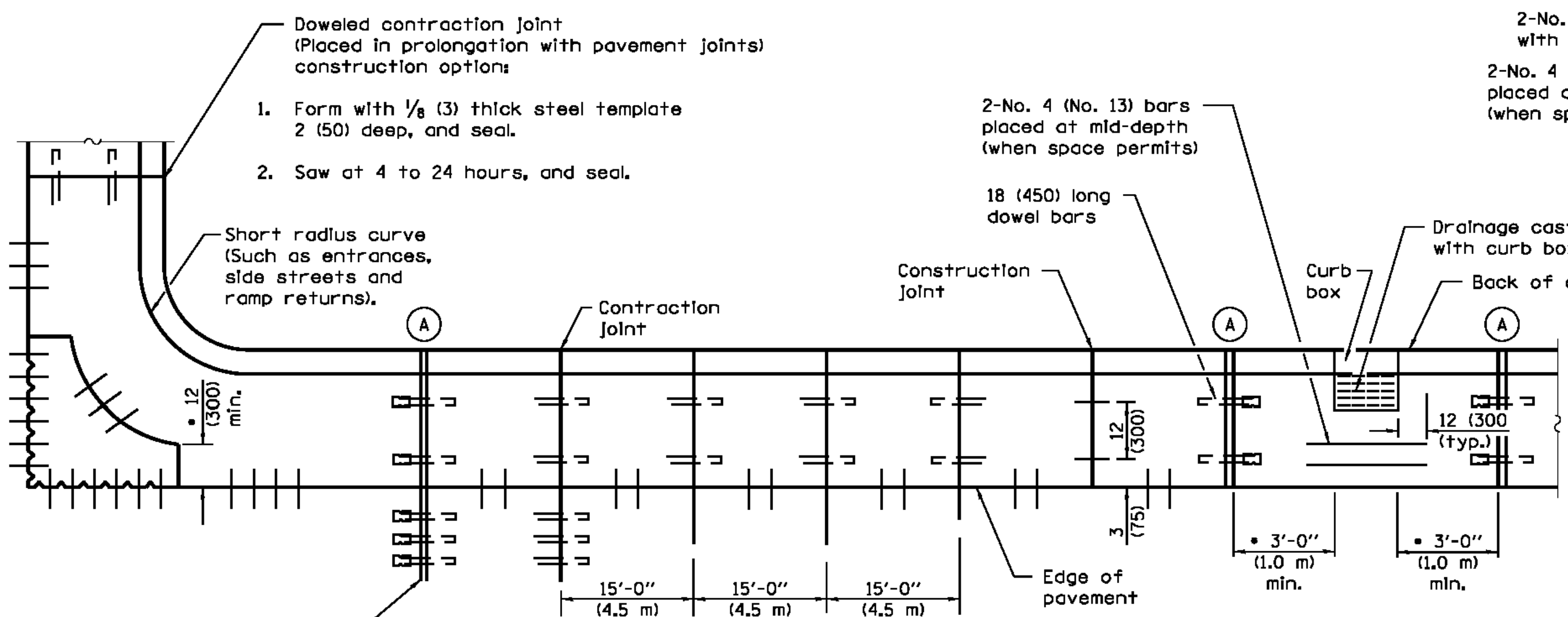


ADDISON UNDERBRIDGE TRAIL CONNECTION FROM CLARK PARK TO CALIFORNIA PARK

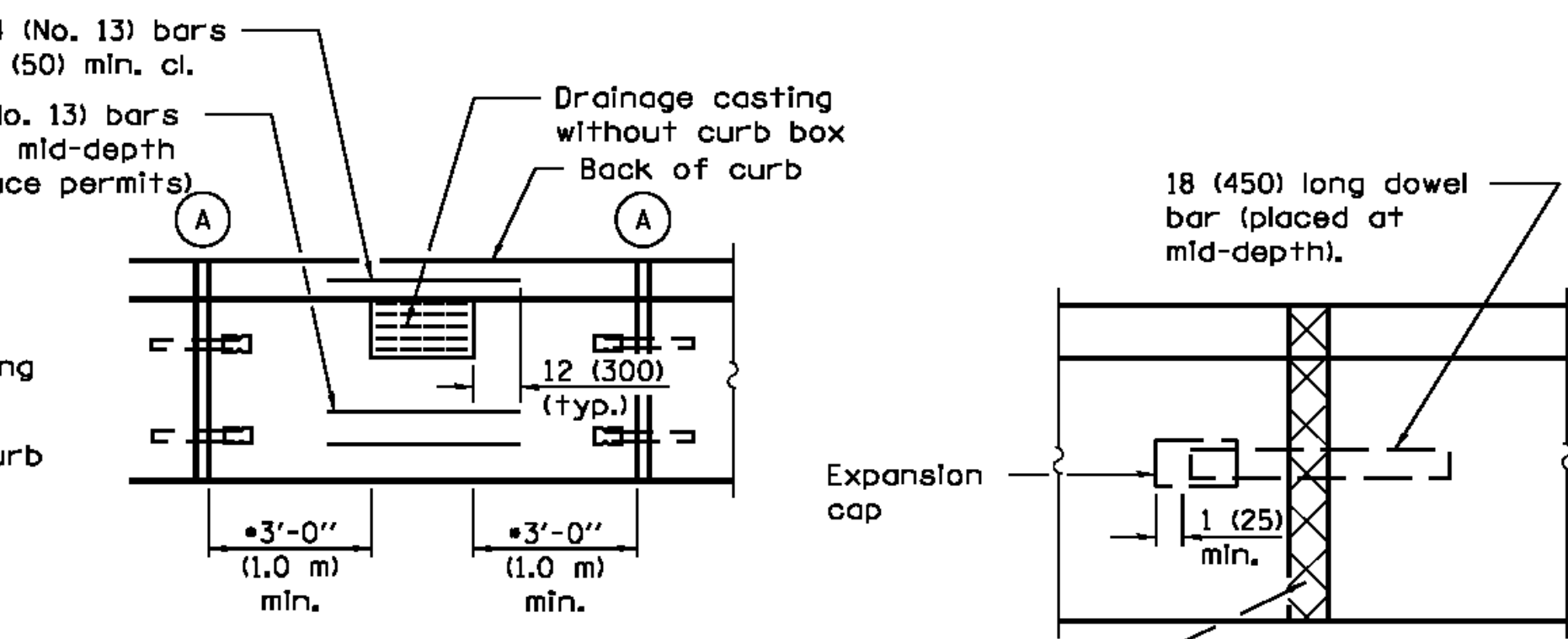
TYPE 8 GRATE DETAIL

SHEET NO. C-31
 SHEET 31 OF 158

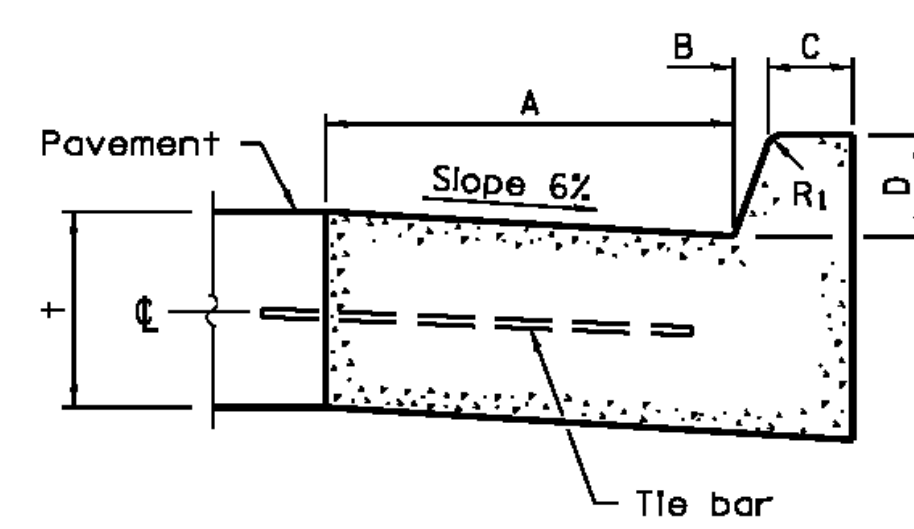
REVISION



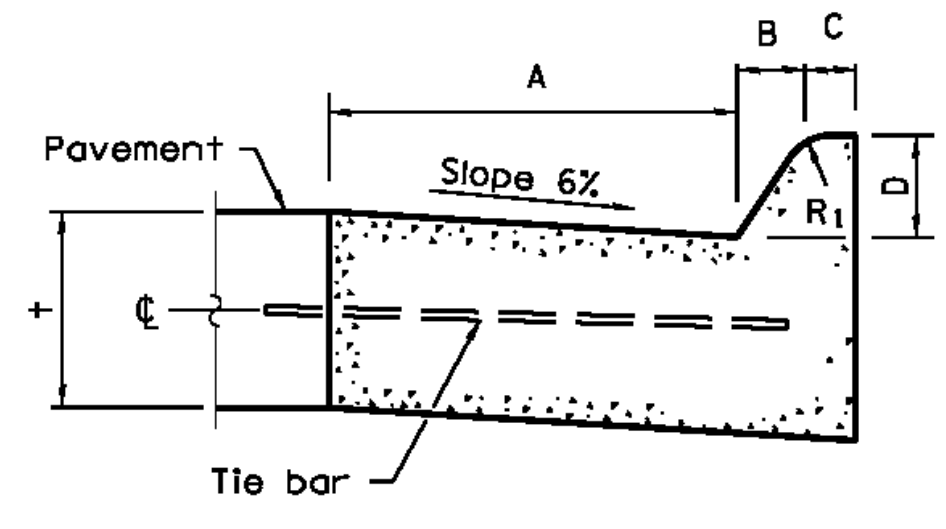
PLAN
ADJACENT TO PCC PAVEMENT OR PCC BASE COURSE



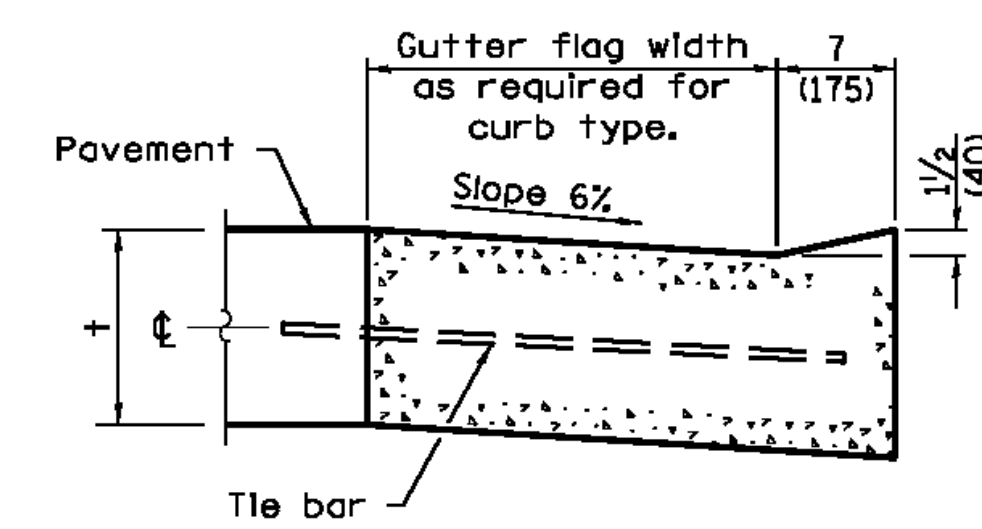
DETAIL A
EXPANSION JOINT



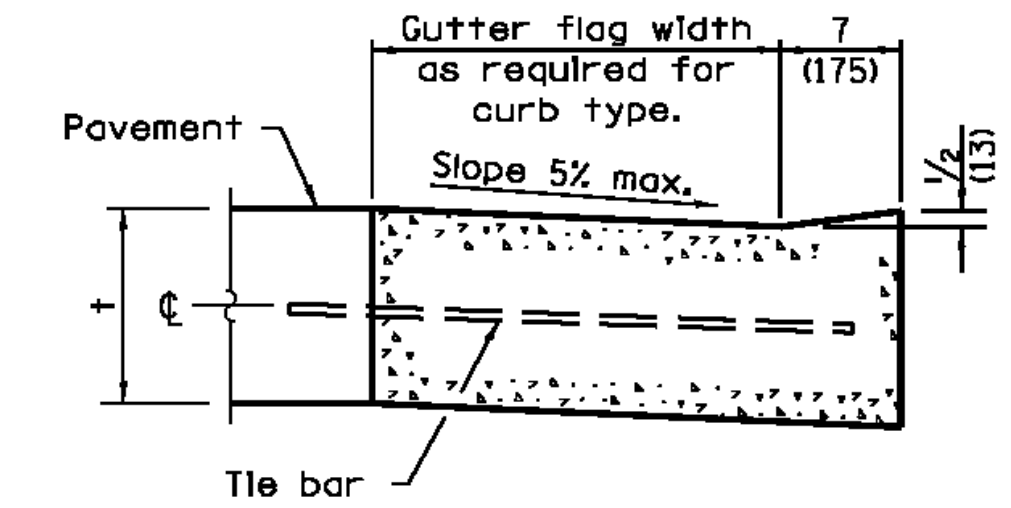
BARRIER CURB



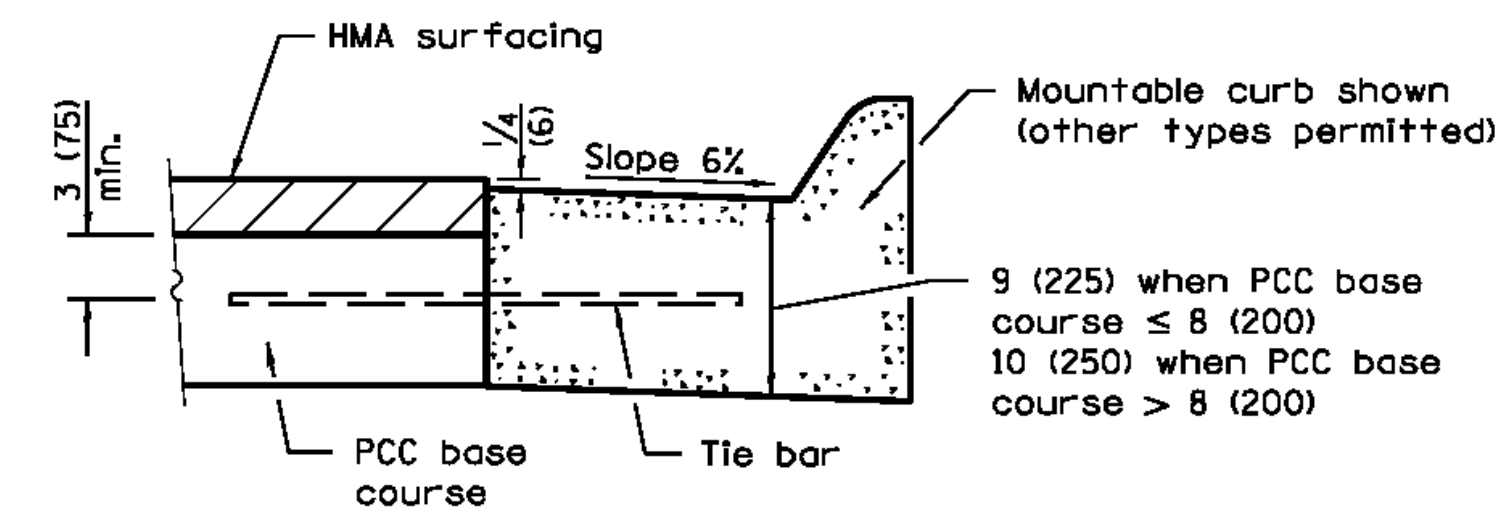
MOUNTABLE CURB



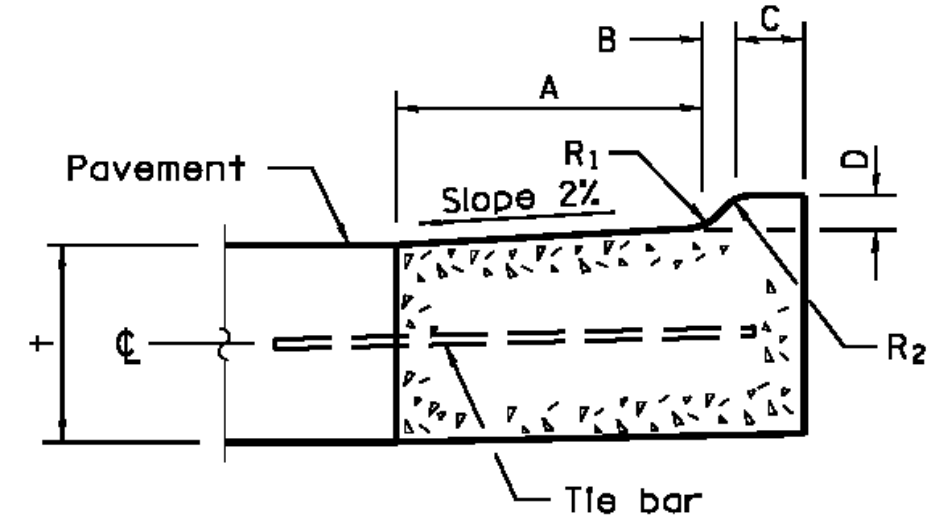
DEPRESSED CURB (TYPICAL)



DEPRESSED CURB ADJACENT TO CURB RAMP ACCESSIBLE TO THE DISABLED



ADJACENT TO PCC BASE COURSE WITH HMA SURFACING



M-2.06 (M-5.15) and M-2.12 (M-5.30)

TABLE OF DIMENSIONS BARRIER CURB					
TYPE	A	B	C	D	R ₁
B-6.06*	6	1	6	6	1
(B-15.15)	(150)	(25)	(150)	(150)	(25)
B-6.12	12	1	6	6	1
(B-15.3)	(300)	(25)	(150)	(150)	(25)
B-6.18	18	1	6	6	1
(B-15.45)	(450)	(25)	(150)	(150)	(25)
B-6.24	24	1	6	6	1
(B-15.60)	(600)	(25)	(150)	(150)	(25)
B-9.12	12	2	5	9	1
(B-22.30)	(300)	(50)	(125)	(225)	(25)
B-9.18	18	2	5	9	1
(B-22.45)	(450)	(50)	(125)	(225)	(25)
B-9.24	24	2	5	9	1
(B-22.60)	(600)	(50)	(125)	(225)	(25)

* For corner islands only.

TABLE OF DIMENSIONS MOUNTABLE CURB							
TYPE	A	B	C	D	R ₁	R ₂	
M-2.06	6	2	4	2	3	2	
(M-5.15)	(150)	(50)	(100)	(50)	(75)	(50)	
M-2.12	12	2	4	2	3	2	
(M-5.30)	(300)	(50)	(100)	(50)	(75)	(50)	
M-4.06	6	4	3	4	3	NA	
(M-10.15)	(150)	(100)	(75)	(100)	(75)	NA	
M-4.12	12	4	3	4	3	NA	
(M-10.30)	(300)	(100)	(75)	(100)	(75)	NA	
M-4.18	18	4	3	4	3	NA	
(M-10.45)	(450)	(100)	(75)	(100)	(75)	NA	
M-4.24	24	4	3	4	3	NA	
(M-10.60)	(600)	(100)	(75)	(100)	(75)	NA	
M-6.06	6	6	2	6	2	NA	
(M-15.15)	(150)	(150)	(50)	(150)	(50)	NA	
M-6.12	12	6	2	6	2	NA	
(M-15.30)	(300)	(150)	(50)	(150)	(50)	NA	
M-6.18	18	6	2	6	2	NA	
(M-15.45)	(450)	(150)	(50)	(150)	(50)	NA	
M-6.24	24	6	2	6	2	NA	
(M-15.60)	(600)	(150)	(50)	(150)	(50)	NA	

Illinois Department of Transportation
 PASSED January 1, 2015
 Michael Brand
 ENGINEER OF POLICY AND PROCEDURES
 APPROVED January 1, 2015
 ENGINEER OF DESIGN AND ENVIRONMENT
 ISSUED 1-1-15

GENERAL NOTES

The bottom slope of combination curb and gutter constructed adjacent to pcc pavement shall be the same slope as the subbase or 6% when subbase is omitted.

t = Thickness of pavement.

Longitudinal joint tie bars shall be No. 6 (No. 19) at 24 (600) centers in accordance with details for longitudinal construction joint shown on Standard 420001.

A minimum clearance of 2 (50) between the end of the tie bar and the back of the curb shall be maintained.

The dowel bars shown in contraction joints will only be required for monolithic construction.

See Standard 606301 for details of corner islands.

All dimensions are in inches (millimeters) unless otherwise shown.

DATE	REVISIONS
1-1-15	Added B-6.06 (B-15.15) barrier curb and gutter to table (corner islands only).
1-1-13	Added general note regarding requirement for dowel bars.

CONCRETE CURB TYPE B AND COMBINATION CONCRETE CURB AND GUTTER
 (Sheet 1 of 2)
STANDARD 606001-06

January 24, 2015 10:00 AM - Addison Underbridge Connection - CDOT - Addison Underbridge Connection - (DISC)PUNE (Civil Working) Sheets (030_ConcreteTypeBAndGutter.dwg)

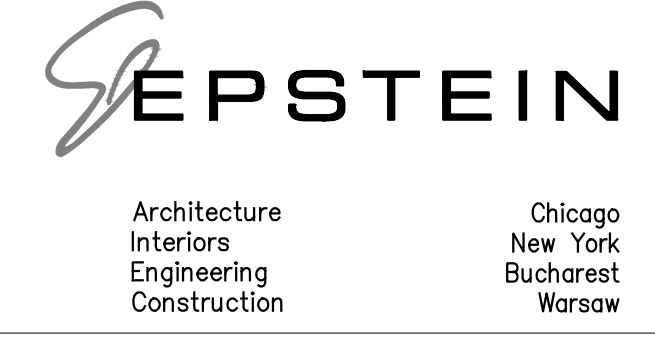
SECTION NO.: 12-E9121-00-BT
 DRAWN BY: AP
 CHECKED BY: JRK
 APPROVED BY: GAO
 DATE: 8/12/2016
 SCALE: N.T.S.

NO. DATE REVISIONS/ISSUANCES
 1 8/12/16 ISSUED FOR FINAL
 2 5/6/16 ISSUED FOR FINAL
 3 6/05/15 ISSUED FOR FINAL REVIEW

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 DEPARTMENT OF TRANSPORTATION
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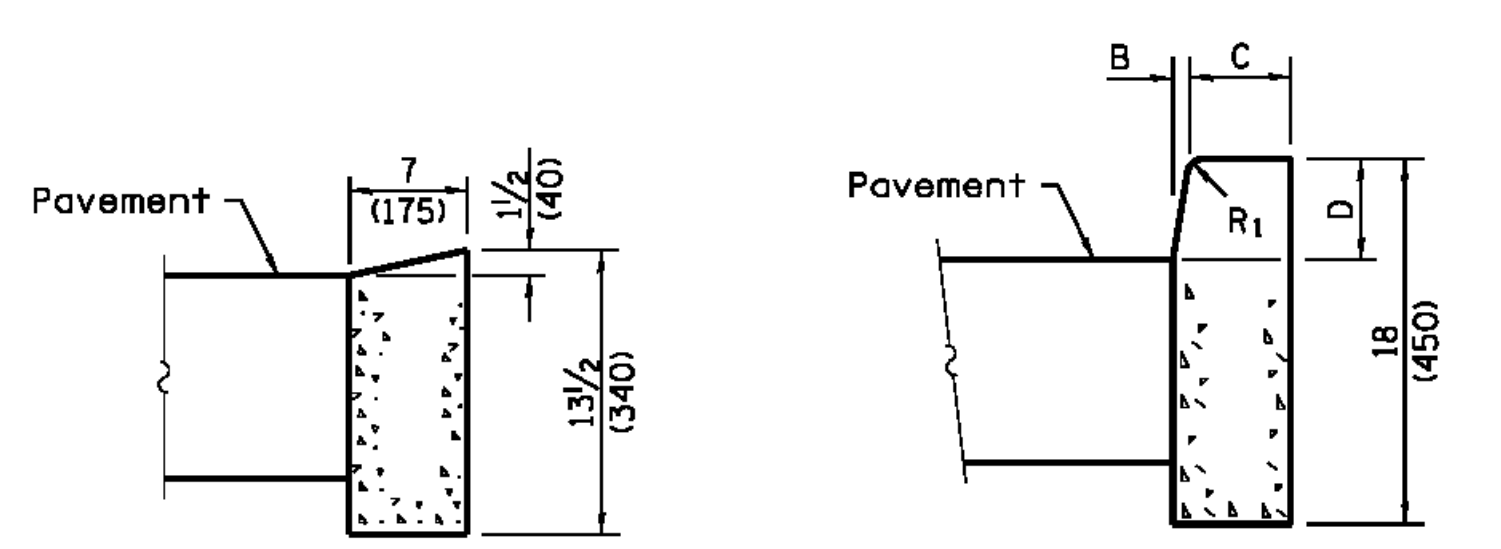
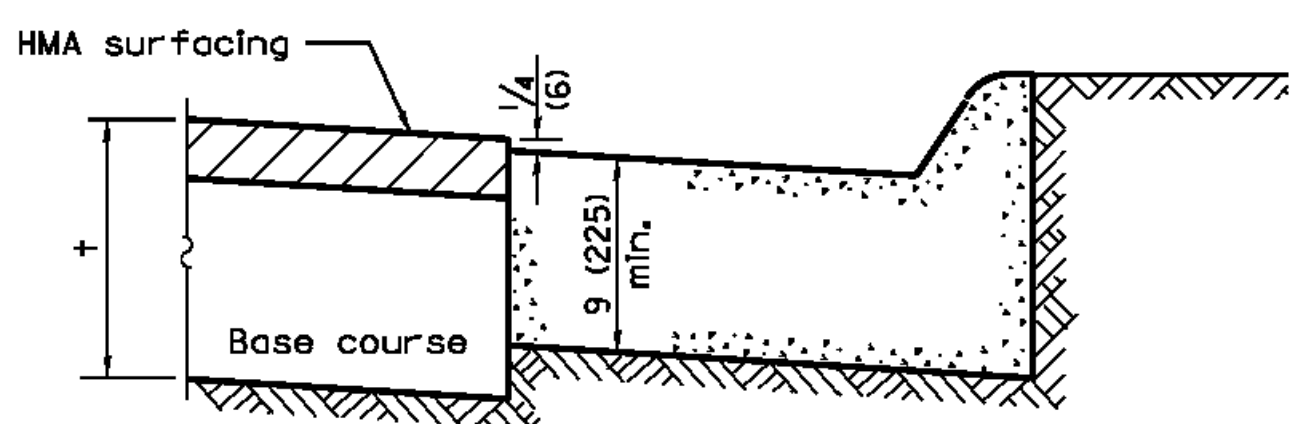
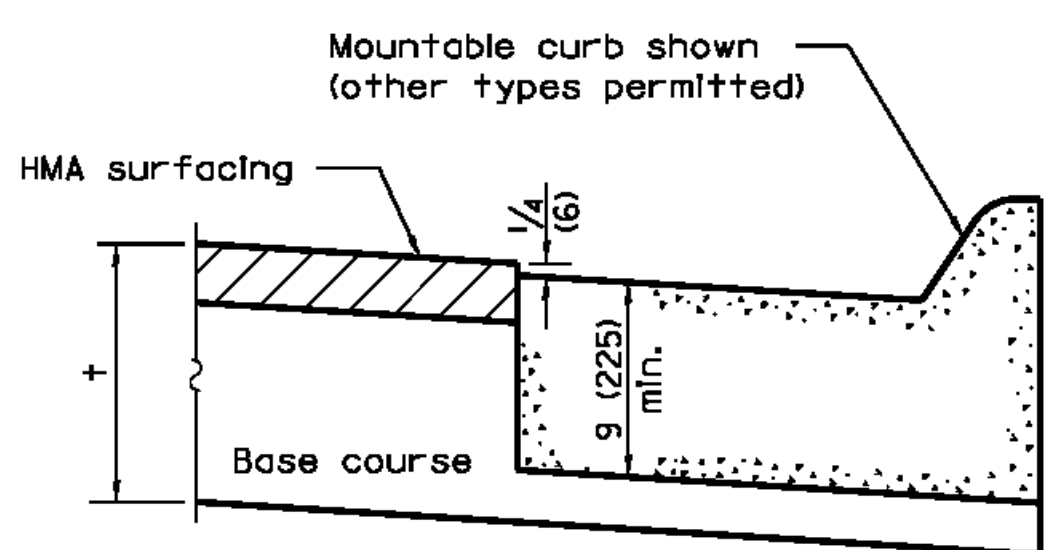
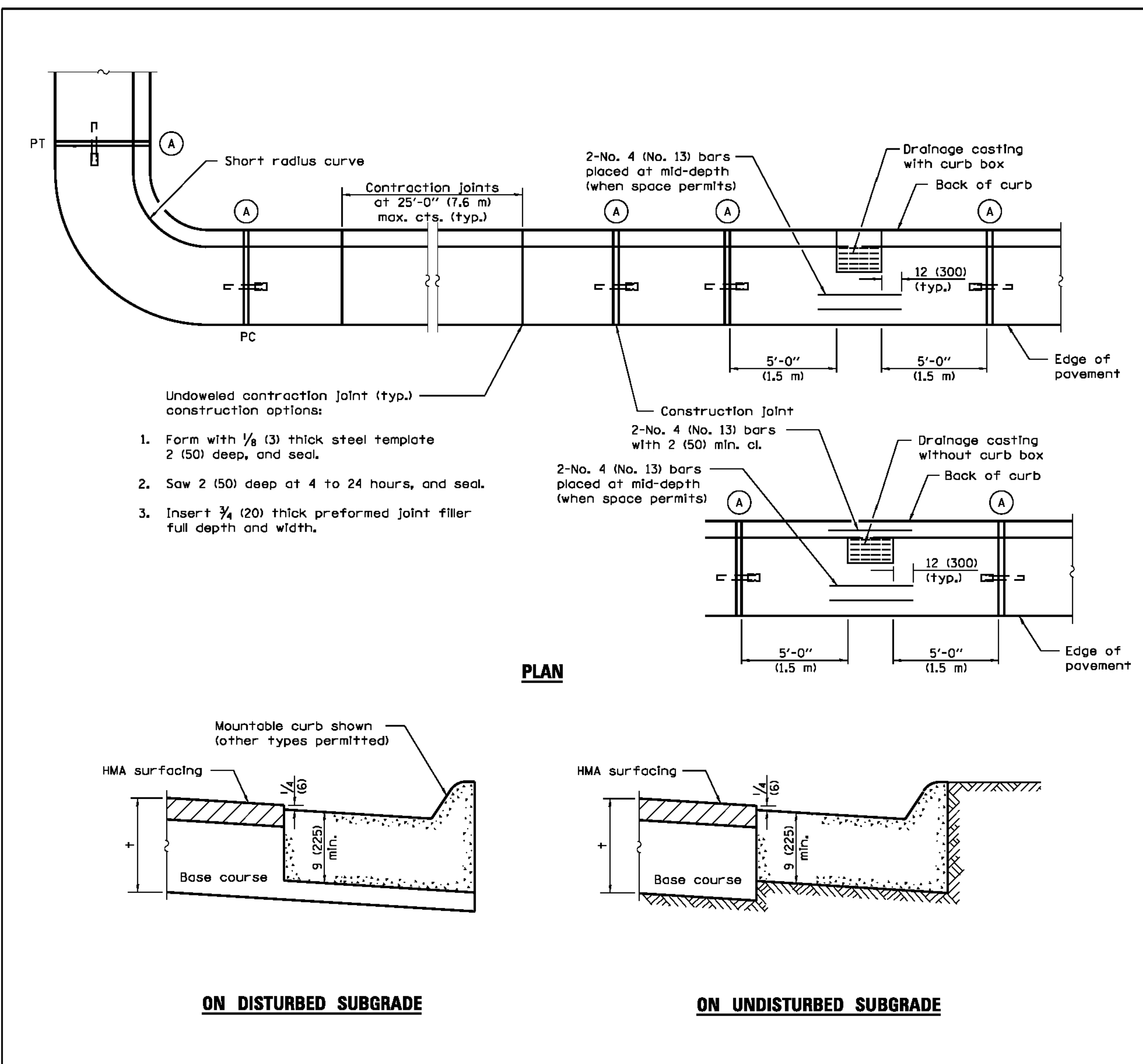
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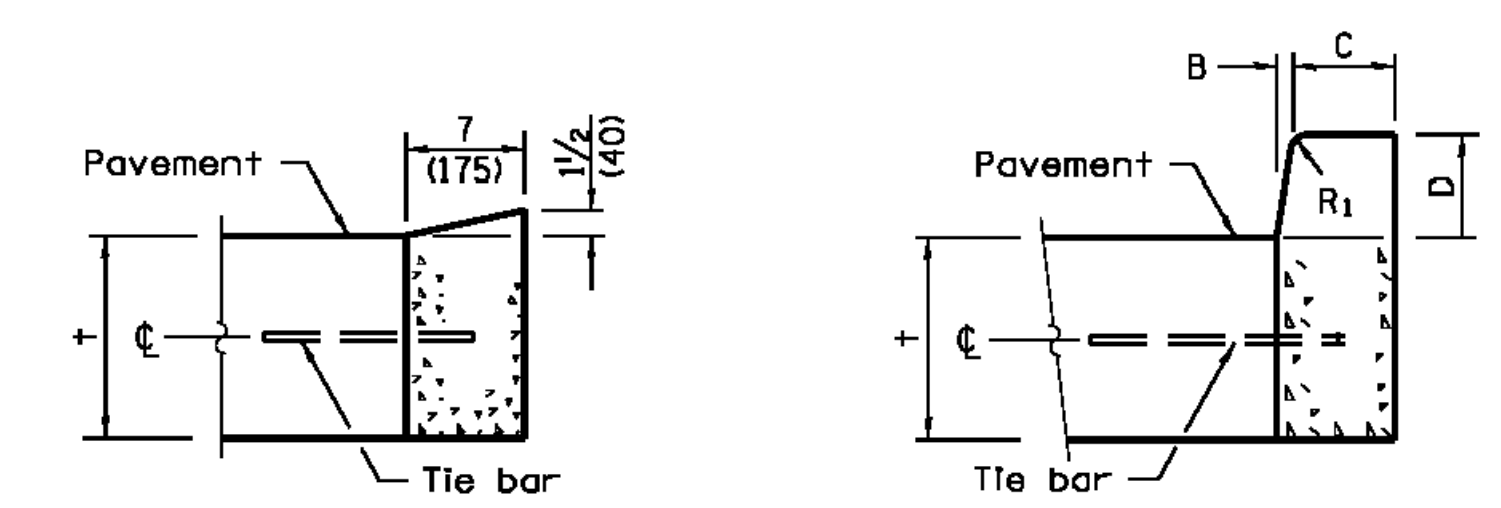
ADDISON UNDERBRIDGE TRAIL CONNECTION FROM CLARK PARK TO CALIFORNIA PARK
 COMBINATION CONCRETE CURB AND GUTTER DETAILS

SHEET NO. C-32
 SHEET 32 OF 158
 REVISION

January 24, 2016 10:00 AM C:\Users\jbradley\Documents\Projects\CDOT\Addison Underbridge Connection\CDOT - Addison Underbridge Connection - DISCIPLINE\CH\Working\Sheets\03 - Concrete\typeb\combcnccg.dwg



ADJACENT TO FLEXIBLE PAVEMENT



ADJACENT TO PCC PAVEMENT OR PCC BASE COURSE

CONCRETE CURB TYPE B

**CONCRETE CURB TYPE B
AND COMBINATION
CONCRETE CURB AND GUTTER**
(Sheet 2 of 2)

STANDARD 606001-06

Illinois Department of Transportation

PASSED January 1, 2015
Michael Brand
ENGINEER OF POLICY AND PROCEDURES

APPROVED January 1, 2015
RE
ENGINEER OF DESIGN AND ENVIRONMENT

ISSUED 1-1-15
15-97

ADJACENT TO FLEXIBLE PAVEMENT

SECTION NO.: 12-E9121-00-BT
DRAWN BY: AP
CHECKED BY: JRK
APPROVED BY: GAO
DATE: 8/12/2016
SCALE: N.T.S.

NO.	DATE	REVISIONS/ISSUANCES
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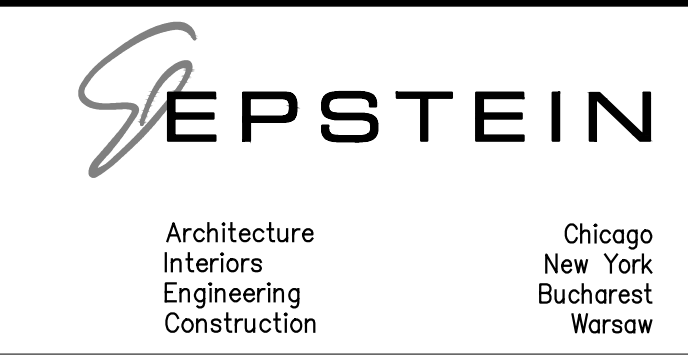
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DEPARTMENT OF TRANSPORTATION
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Chicago, IL 60602
312.744.3600



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ADDISON UNDERBRIDGE TRAIL CONNECTION FROM CLARK PARK TO CALIFORNIA PARK

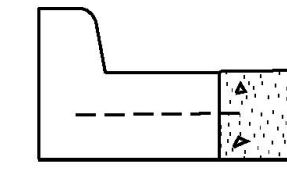
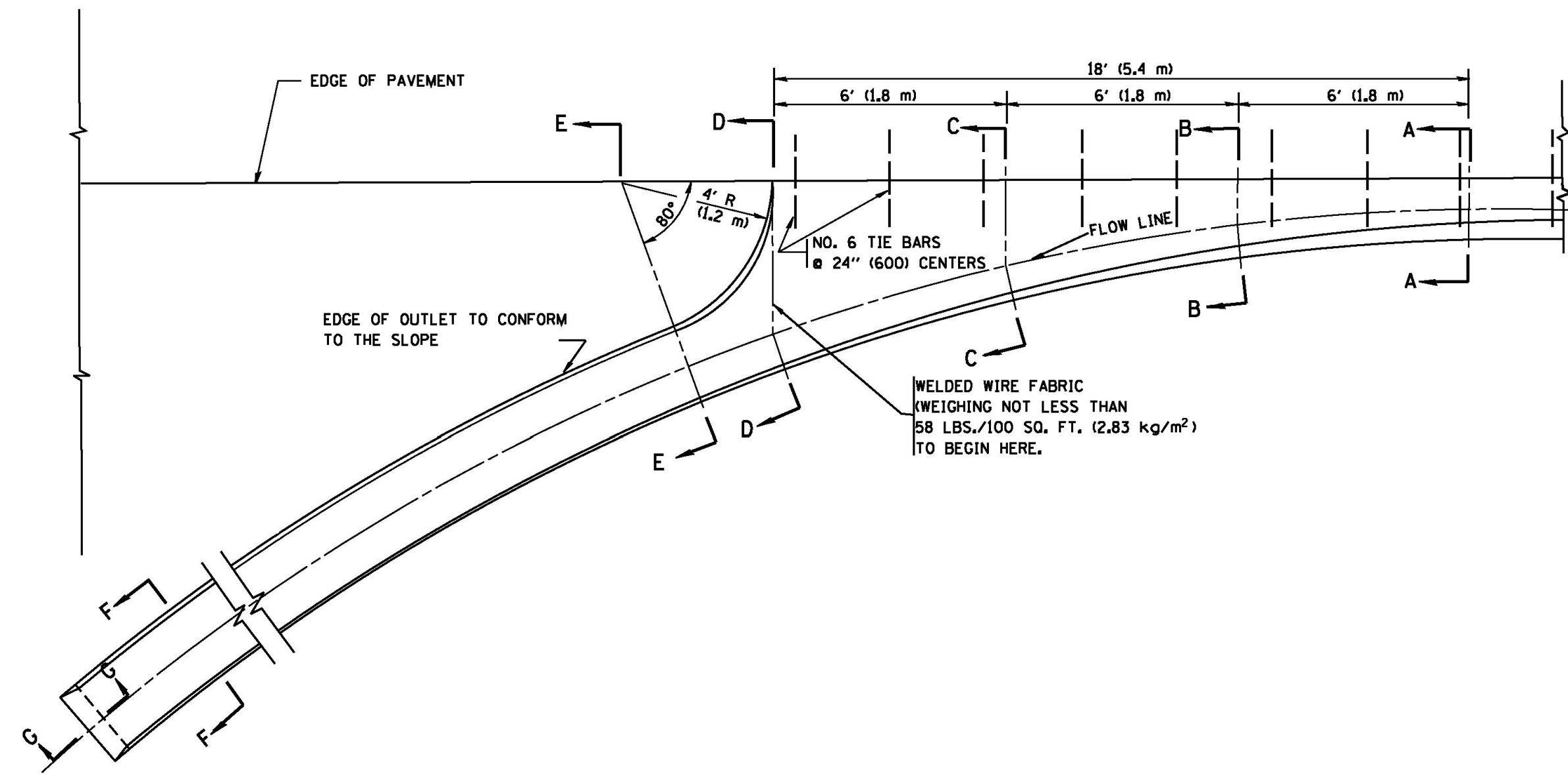
COMBINATION CONCRETE CURB AND GUTTER DETAILS

SHEET NO. C-33

SHEET 33 OF 158

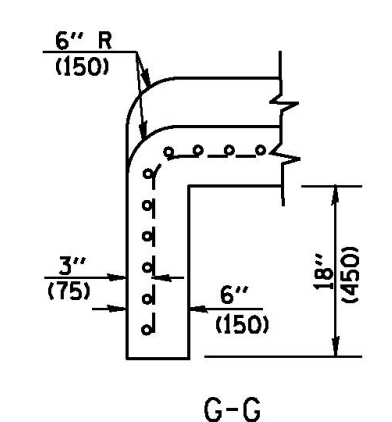
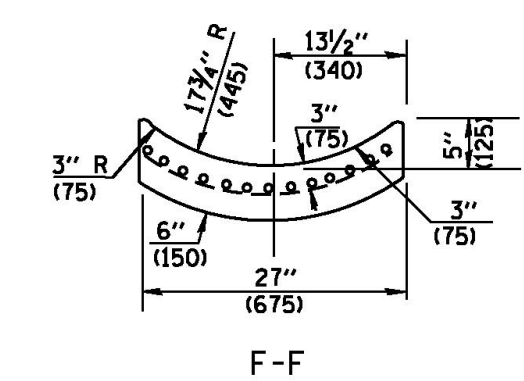
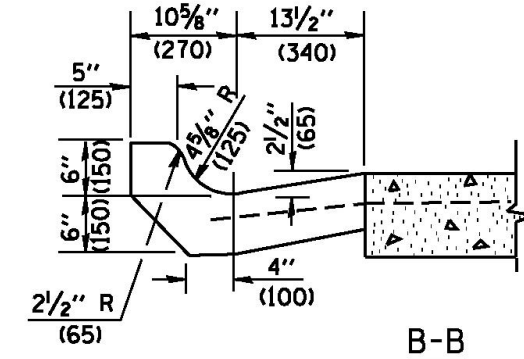
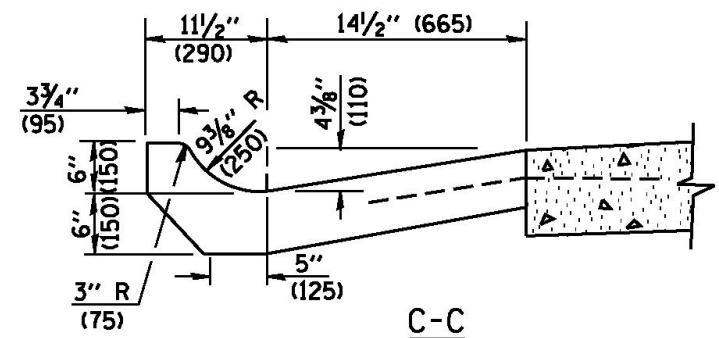
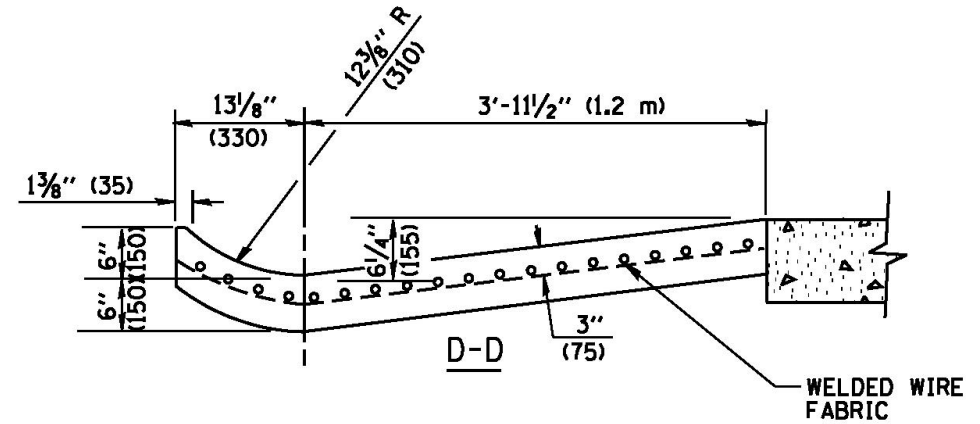
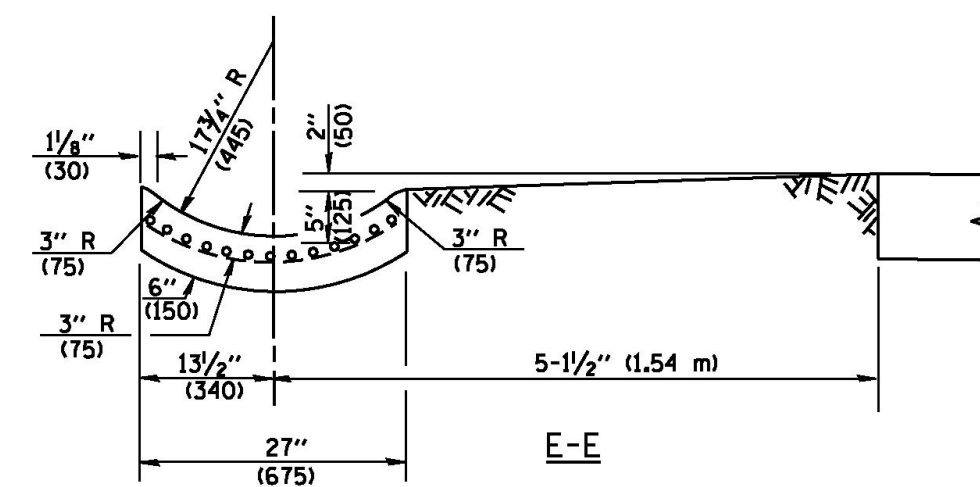
REVISION

January 24, 2016 10:00 AM - Addison Underbridge Connection - CDOT - Addison Underbridge Connection - DISCIPLINE: Civil Working Sheets (032_Curb and Gutter Outlet.dwg)



A-A *

* DIMENSIONS OF THE CURB & GUTTER AT SECTION A-A ARE SHOWN ON STATE STANDARD 606001. FOR DETAILS OF OUTLET FOR CONCRETE CURB & GUTTER, TYPE B-6.24 (B-15.60) SEE STATE STANDARD 606006.



GENERAL NOTES

GUTTER OUTLET SHALL BE TIED TO THE PAVEMENT IN ACCORDANCE WITH DETAILS FOR LONGITUDINAL CONSTRUCTION JOINT SHOWN ON IDOT STANDARD 420001.

TIE BARS SHALL BE NO. 6 AT 24" CENTERS UNLESS OTHERWISE SHOWN.

IF THE AVERAGE GRADE OF PAVEMENT FOR THE DISTANCE FROM SECTION A-A TO D-D EXCEEDS 2%, THIS DISTANCE SHALL BE INCREASED 6' FOR EACH 1% INCREASE IN GRADE.

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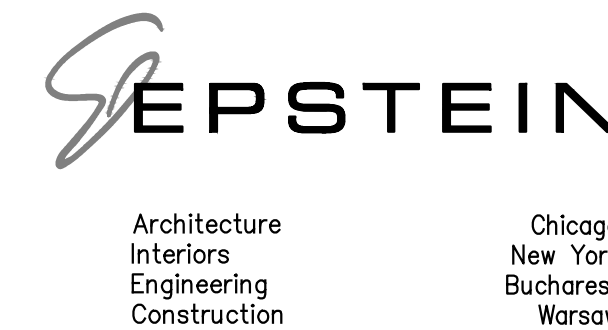
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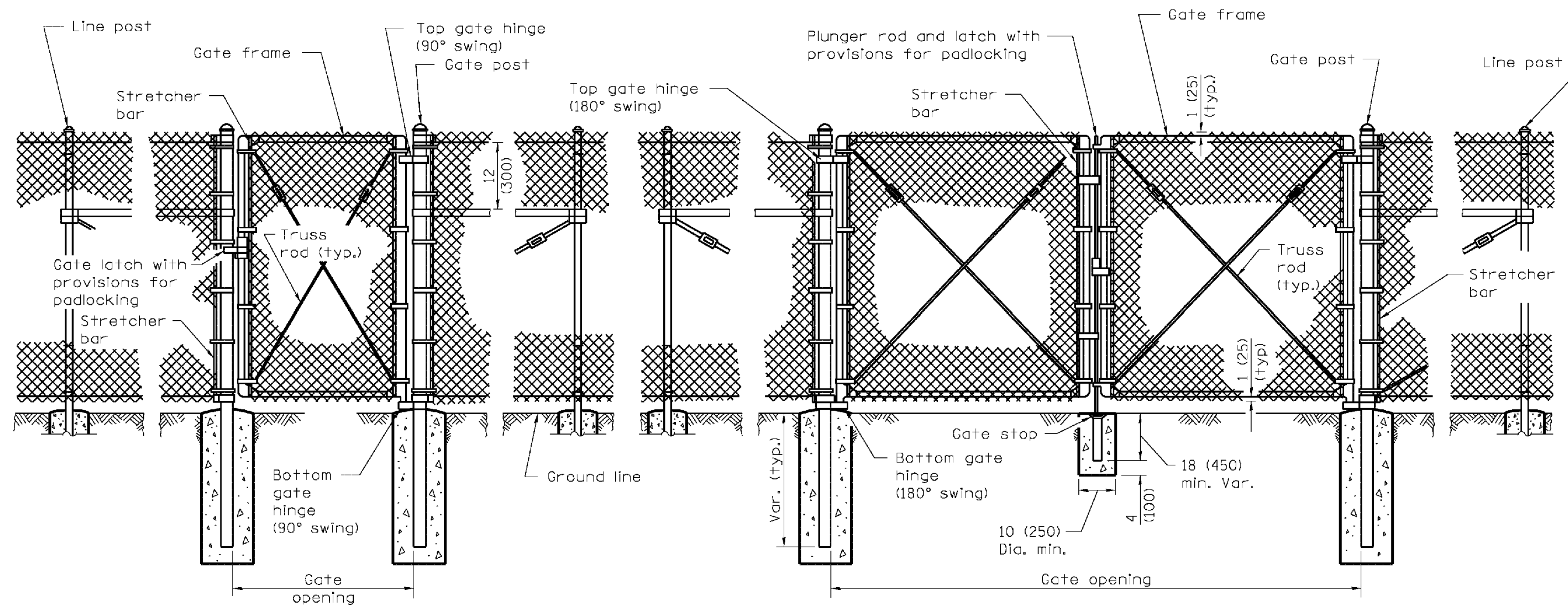
ADDISON UNDERBRIDGE TRAIL CONNECTION FROM CLARK PARK TO CALIFORNIA PARK

CURB AND GUTTER OUTLET DETAIL

SHEET NO. C-34

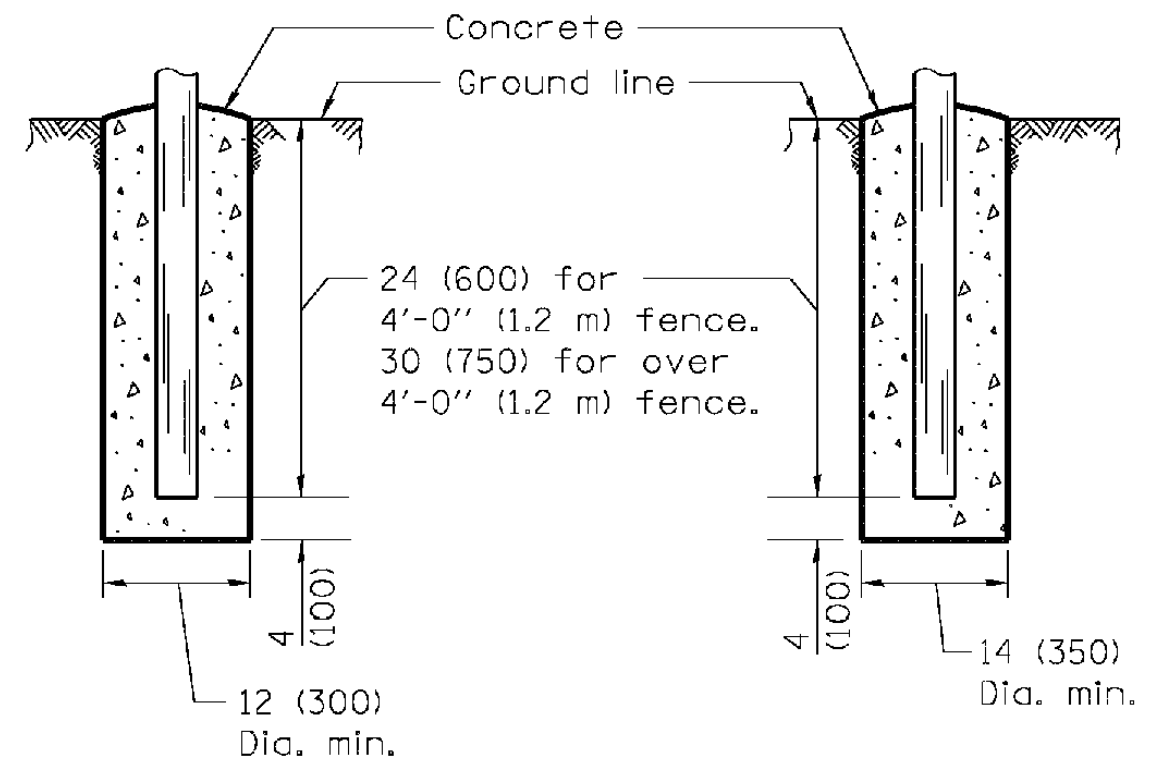
SHEET 34 OF 158

REVISION



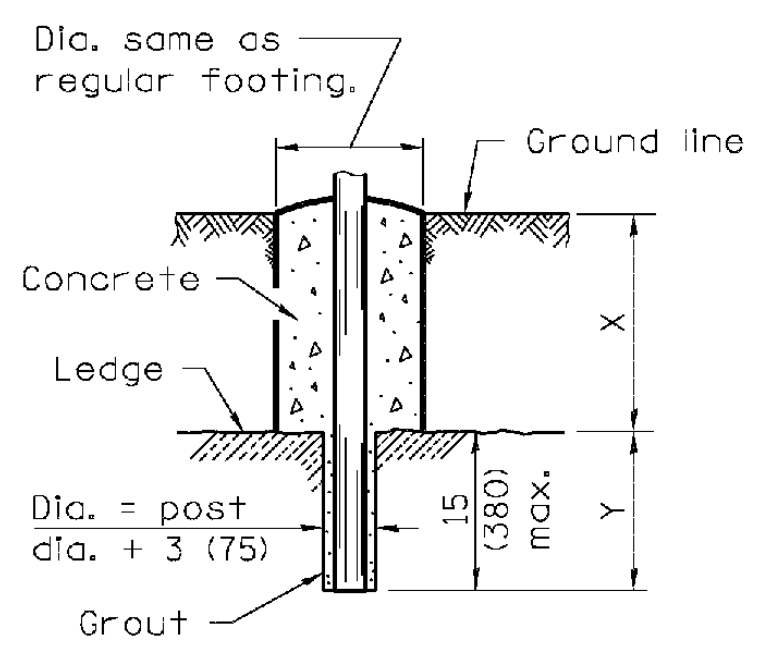
PEDESTRIAN GATE ARRANGEMENT

VEHICLE GATE ARRANGEMENT

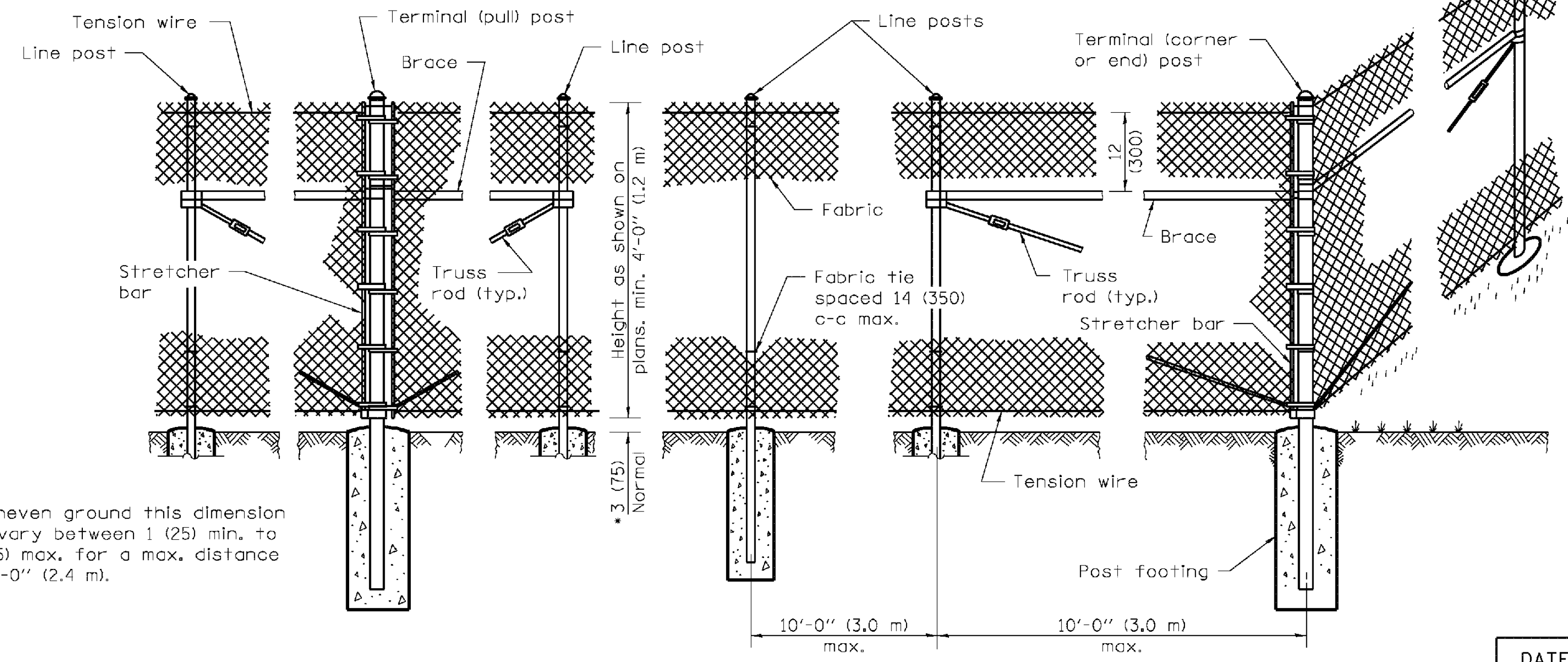


FOOTING FOR LINE POST

FOOTING FOR GATE & TERMINAL POST



FOOTING FOR POST IN ROCK LEDGE



PULL POST ARRANGEMENT

LINE POST ARRANGEMENT

CORNER OR END POST ARRANGEMENT

On uneven ground this dimension may vary between 1 (25) min. to 5 (125) max. for a max. distance of 8'-0" (2.4 m).

GENERAL NOTES

Pull posts shall be placed at locations determined by the Engineer. They shall be placed at 660' (200 m) intervals between posts to which the ends of the fabric are clamped or midway between such posts when the distance is less than 1320' (400 m) and greater than 660' (200 m).

X + Y shall not exceed 24 (600), 30 (750), or 36 (900), as applicable. When X is 0 - 9 (0 - 225), 15 (380), or 21 (525), then Y = 15 (375) and the post shall be shortened as required. When X exceeds 9 (225), 15 (380), or 21 (525), then Y shall be decreased correspondingly.

All dimensions are in inches (millimeters) unless otherwise shown.

PASSED	January 1, 2009
ENGINEER OF POLICY AND PROCEDURES	<i>[Signature]</i>
APPROVED	January 1, 2009
ENGINEER OF DESIGN AND ENVIRONMENT	<i>[Signature]</i>

DATE	REVISIONS
1-1-09	Switched units to English (metric).
1-1-99	Rev. "pans" to "plans" in LINE POST ARRANGEMENT.

CHAIN LINK FENCE

(Sheet 1 of 3)

STANDARD 664001-02

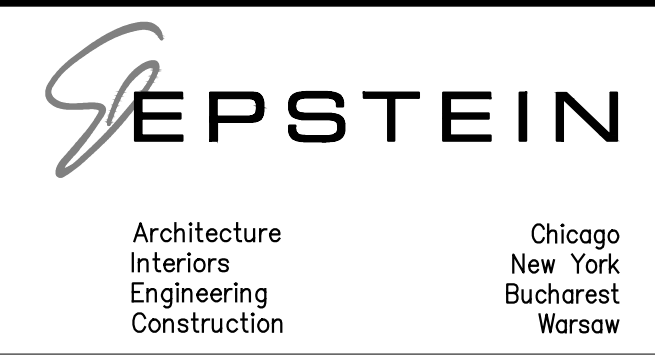
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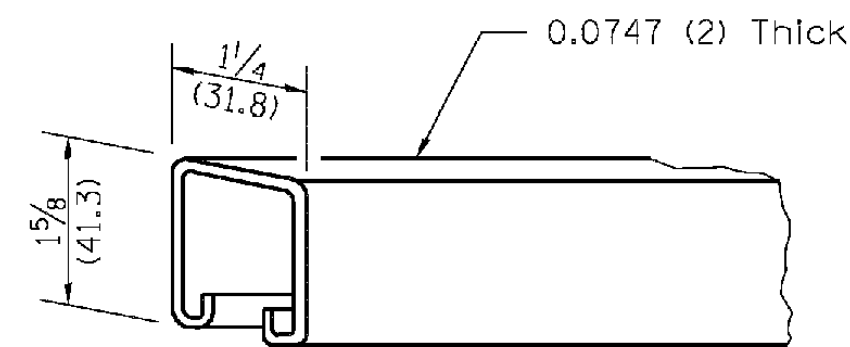
ADDISON UNDERBRIDGE TRAIL CONNECTION FROM CLARK PARK TO CALIFORNIA PARK

CHAIN LINK FENCE DETAILS

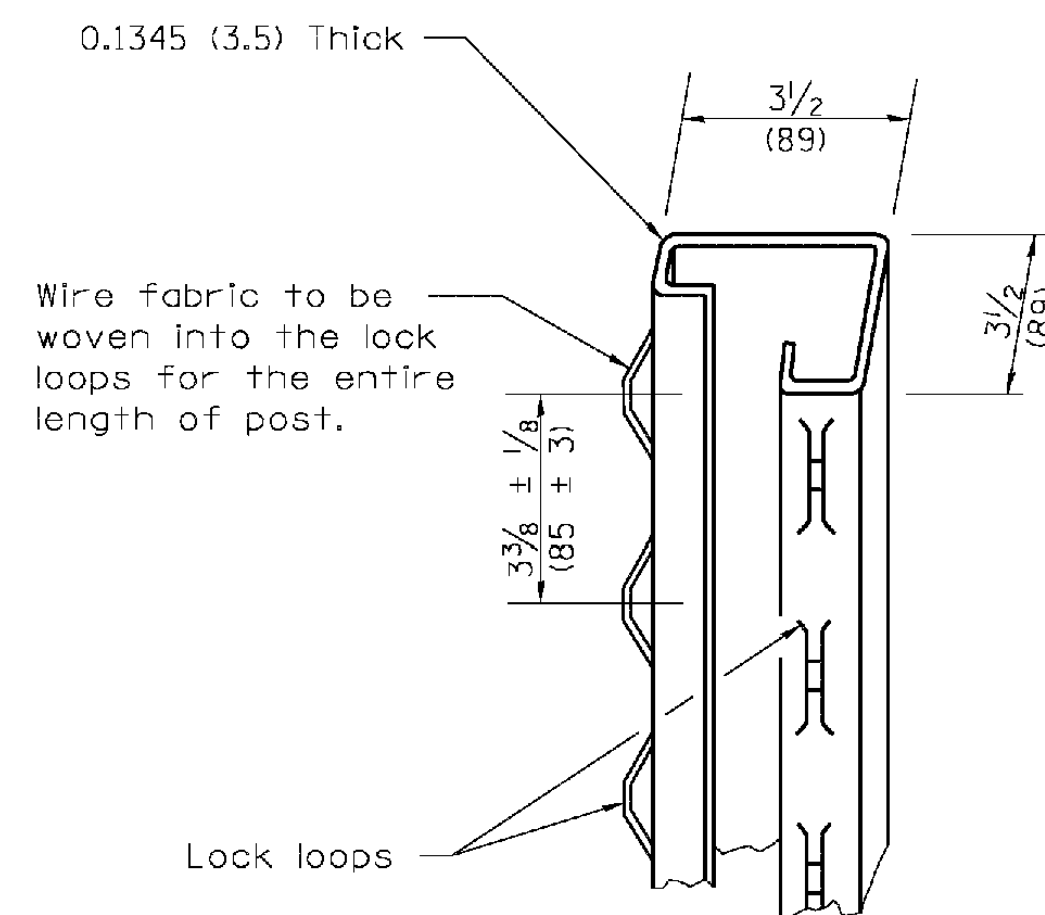
SHEET NO. C-35

SHEET 35 OF 158

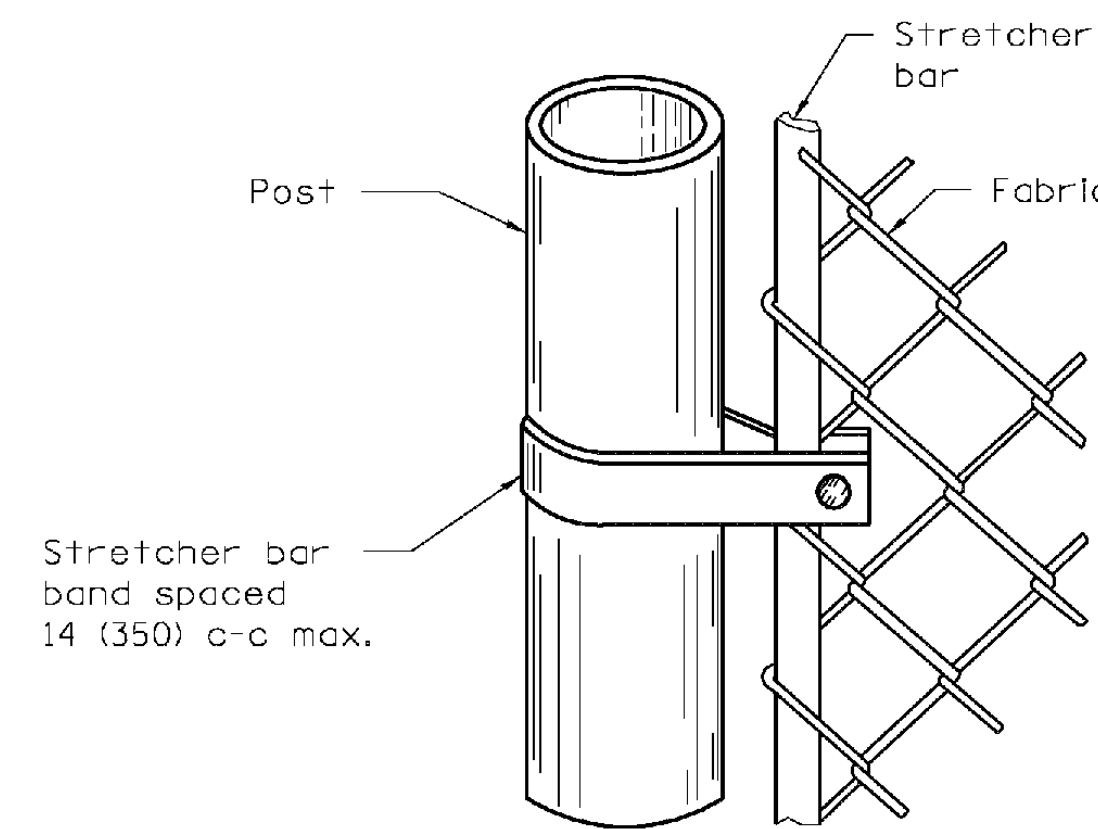
REVISION



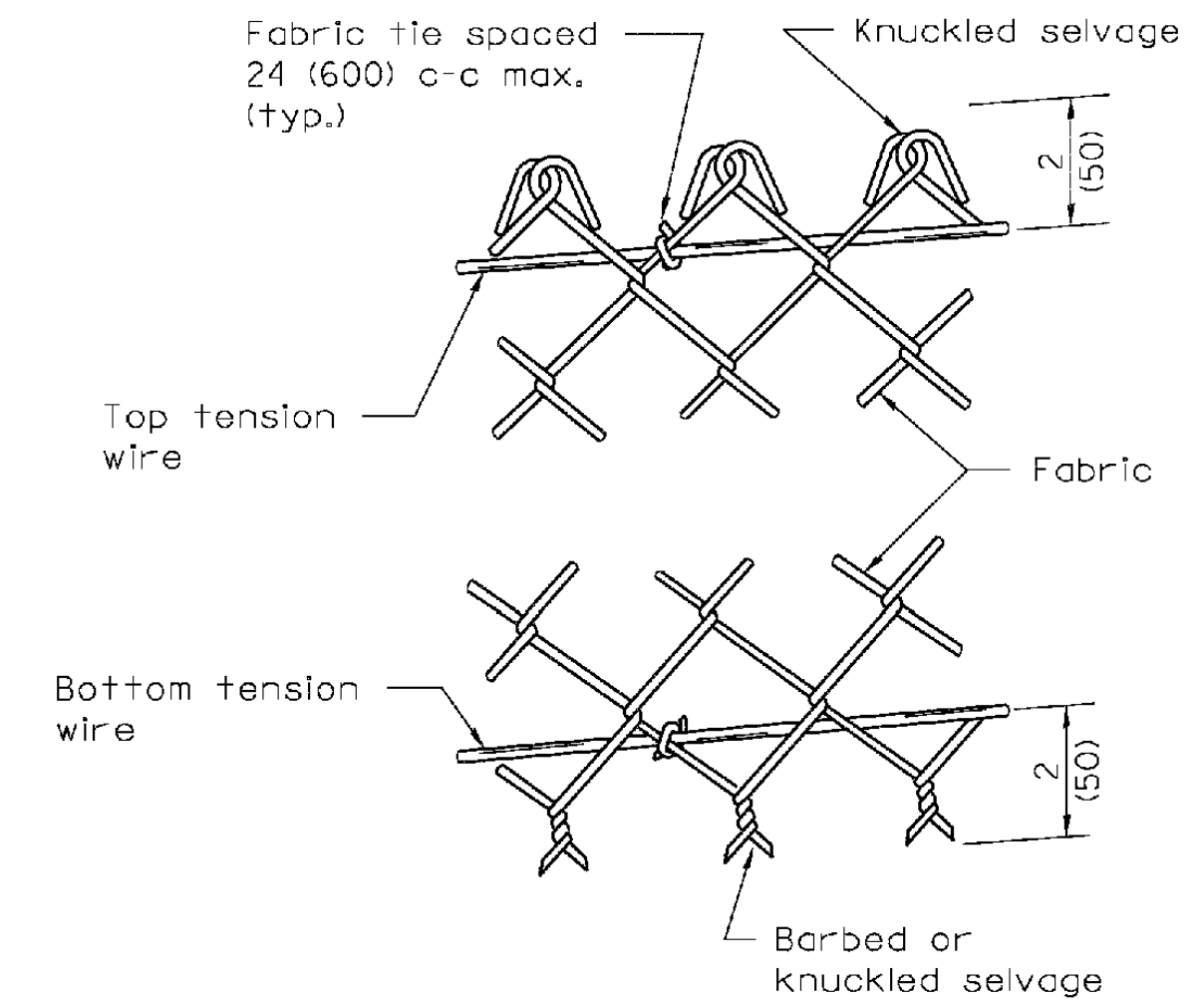
ROLL FORMED SECTION OF BRACE



ROLL FORMED SECTION OF TERMINAL & GATE POST



METHOD OF FASTENING STRETCHER BAR TO POST



METHOD OF TYING FABRIC TO TENSION WIRES

LINE POST	
Section	lbs./ft. (kg/m)
Pipe Type A 1.90 (48.3) O.D.	2.72 (4.05)
Pipe Type B 1.90 (48.3) O.D.	2.28 (3.39)
Pipe Type C 1.90 (48.3) O.D.	2.26 (3.36)
H 1.875x1.625 (47.6x41.3)	2.72 (4.05)
□	1.60 (2.38)
I	2.30 (3.42)

TERMINAL POST	
Section	lbs./ft. (kg/m)
Pipe Type A 2.375 (60.3) O.D.	3.65 (5.43)
Pipe Type B 2.375 (60.3) O.D.	3.11 (4.63)
Pipe Type C 2.375 (60.3) O.D.	3.09 (4.60)
Roll Formed 3 1/2 x 3 1/2 (89.0 x 89.0)	See detail
Sq. Tubing 2 1/2 x 2 1/2 (63.5 x 63.5)	4.32 (6.43)

HORIZONTAL BRACES	
Section	lbs./ft. (kg/m)
Pipe Type A 1.66 (42.2) O.D.	2.27 (3.38)
Pipe Type B 1.66 (42.2) O.D.	1.83 (2.72)
Pipe Type C 1.66 (42.2) O.D.	1.82 (2.71)
H 1.31x1.5 (33.3x38.1)	2.25 (3.35)
Roll Formed 1 5/8 x 1 1/4 (41.3 x 31.8)	See detail

GATE FRAMES	
Section	lbs./ft. (kg/m)
Pipe Type A 1.66 (42.2) O.D.	2.27 (3.38)
Pipe Type B 1.66 (42.2) O.D.	1.83 (2.72)
Pipe Type C 1.66 (42.2) O.D.	1.82 (2.71)

GATE POSTS *							
Gate Opening * ft. (m)		Pipe Type A		Sq. Tubing	Pipe Type B		
Single	Double	Size (O.D.)	lbs./ft. (kg/m)	Size (O.D.)	lbs./ft. (kg/m)	Size (O.D.)	kg/m (lbs./ft.)
Up to 4 (1.2)	Up to 8 (2.5)	2.375 (60.3)	3.65 (5.43)	2 1/2 (63.5)	4.32 (6.43)	2.375 (60.3)	3.11 (4.63)
Over 4 (1.2) to 8 (2.5)	Over 8 (2.5) to 16 (5.0)	2.875 (73.0)	5.79 (8.62)	3 (76.2)	5.78 (8.60)	2.875 (73.0)	4.64 (6.91)
Over 8 (2.5) to 12 (3.6)	Over 16 (5.0) to 24 (7.4)	3.5 (89.0)	7.58 (11.28)	3 (76.2)	8.80 (13.10)	3.5 (89)	5.707 (8.49)

* The 3 1/2 x 3 1/2 (89.0 x 89.0) roll formed section as detailed may be used as gate posts for single gate up to 6' (1.8 m) and double gate up to 12' (3.6 m).

Illinois Department of Transportation

PASSED January 1, 2009

ENGINEER OF POLICY AND PROCEDURES

APPROVED January 1, 2009

ENGINEER OF DESIGN AND ENVIRONMENT

ISSUED 7-1-11

CHAIN LINK FENCE

(Sheet 2 of 3)

STANDARD 664001-02

January 26, 2009 2:00 PM Addison Underbridge Connection - CDOT - Addison Underbridge Connection - DISCIPLINE: CHA Working Sheets 034 - ChainLinkFence.dwg

SECTION NO.: 12-E9121-00-BT
 DRAWN BY: AP
 CHECKED BY: JRK
 APPROVED BY: GAO
 DATE: 8/12/2016
 SCALE: N.T.S.

NO. DATE REVISIONS/ISSUANCES

8/12/16 ISSUED FOR FINAL

5/6/16 ISSUED FOR FINAL

6/05/15 ISSUED FOR FINAL REVIEW

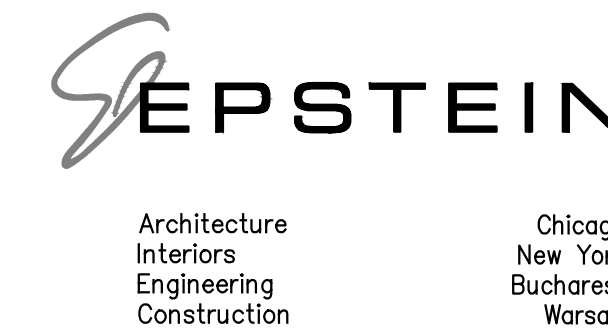
CITY OF CHICAGO

DEPARTMENT OF TRANSPORTATION
 30 N LaSalle St. Suite 1100
 Chicago, IL 60602
 312.744.3600



CONSULTANT

EPSTEIN
 600 W. Fulton Street
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 312.454.9100
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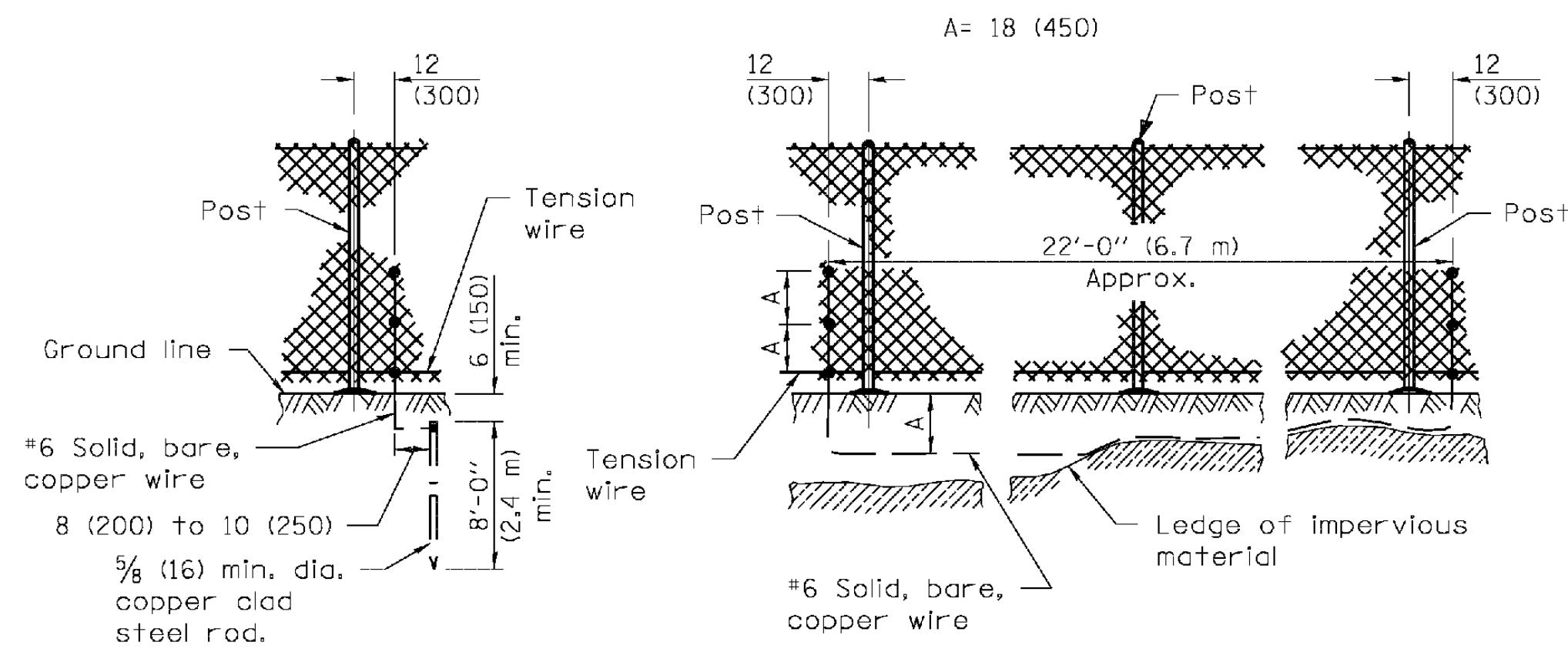
ADDISON UNDERBRIDGE TRAIL CONNECTION FROM CLARK PARK TO CALIFORNIA PARK

CHAIN LINK FENCE DETAILS

SHEET NO. C-36

SHEET 36 OF 158

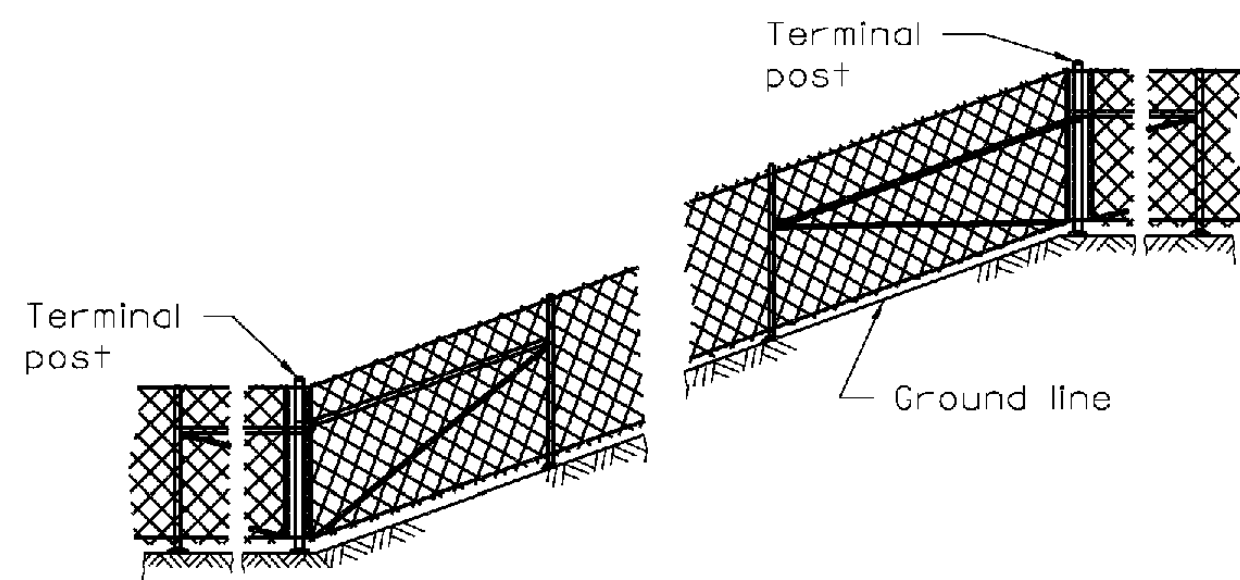
REVISION



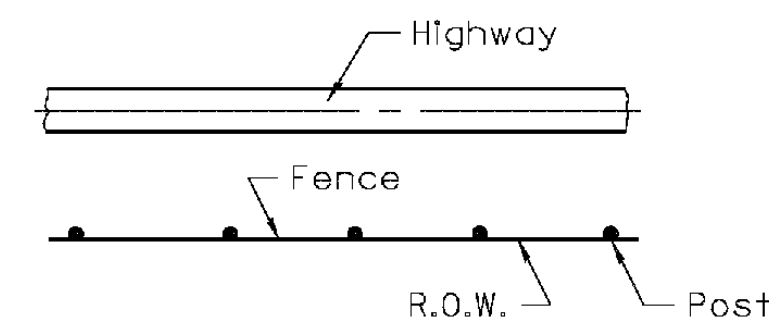
STANDARD GROUND

**COUNTERPOISE GROUND
(ALTERNATE)**

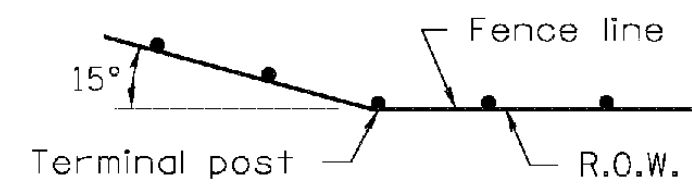
PROTECTIVE ELECTRICAL GROUNDS



INSTALLATION ON SLOPES



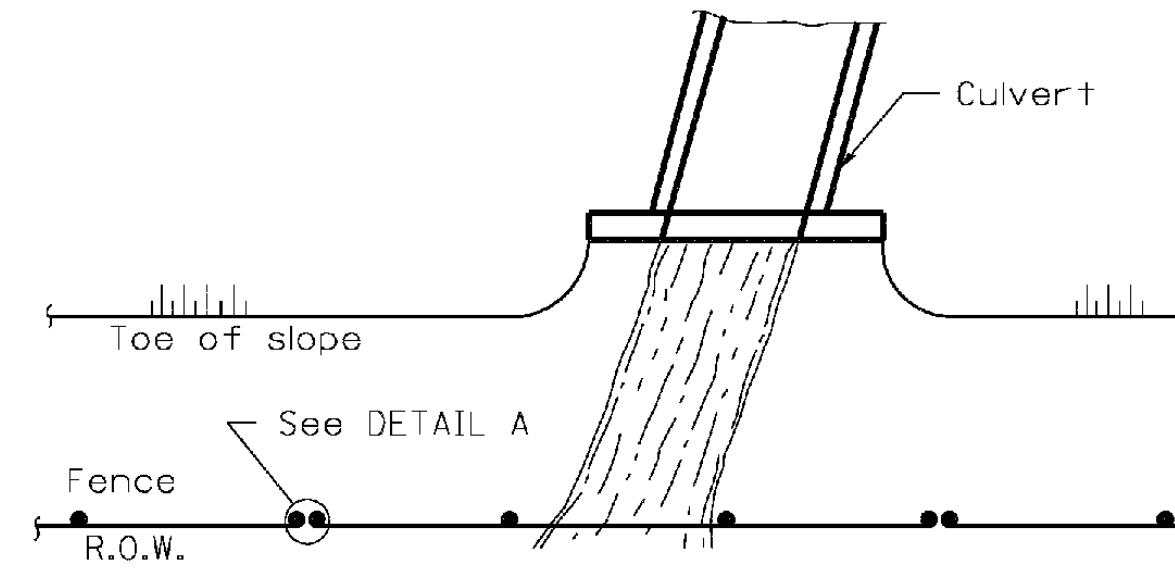
PLAN



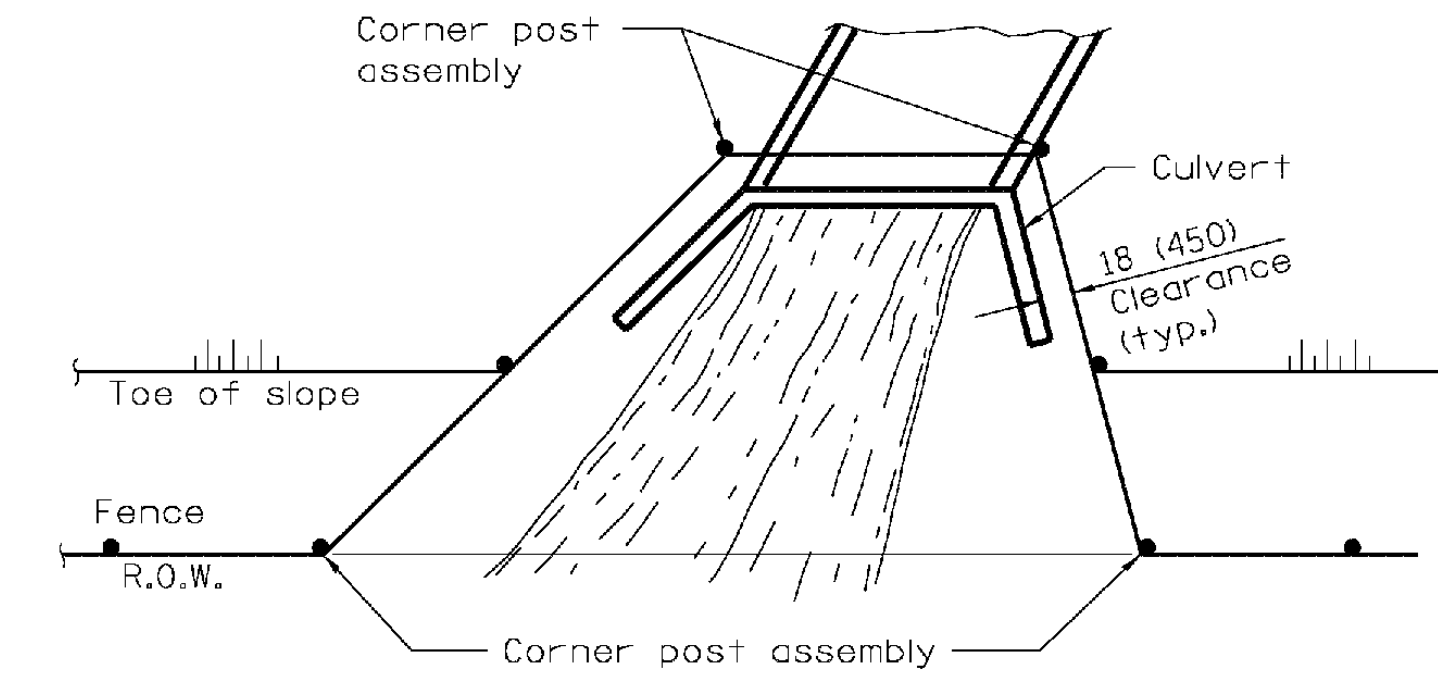
When fence line has a change in direction of 15° or more, a terminal post shall be placed as shown above.

Where angle is less than 15° and existing conditions require a terminal post, they shall be placed as directed by the Engineer.

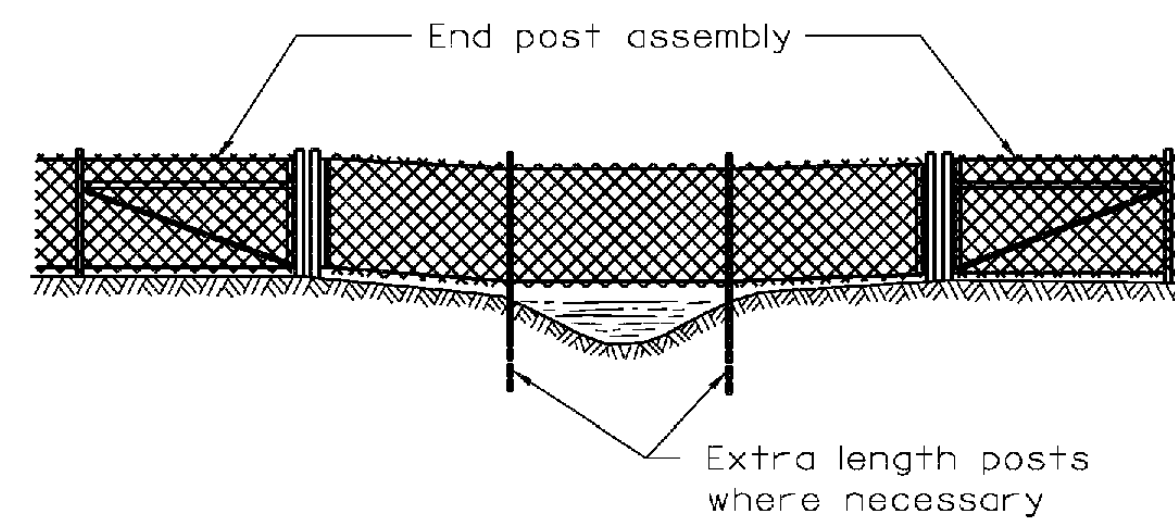
INSTALLATION AT CORNERS



**PLAN
AT STREAM CROSSING**

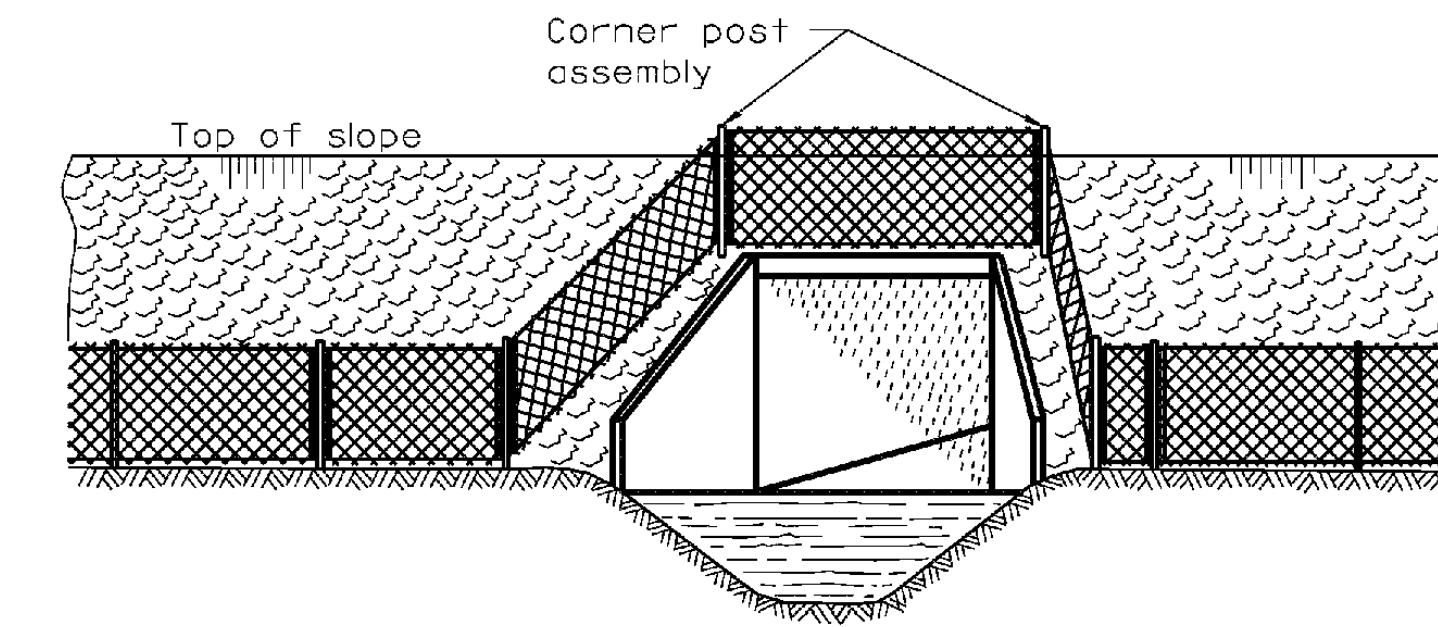


**PLAN
AT HEADWALL**



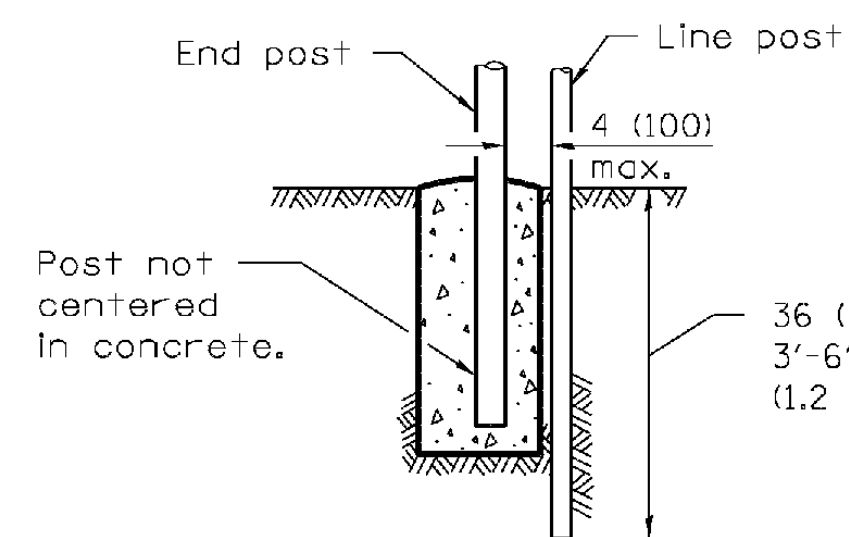
**ELEVATION
INSTALLATION OVER STREAM**

The chain link fabric shall be replaced by barbed wire strands at 12 (300) maximum centers between the double posts shown on DETAIL A when shown on the plans.



**ELEVATION
INSTALLATION AROUND HEADWALL**

When the width of the culvert makes it necessary to anchor a post to the top of the culvert, a cast iron shoe or other device approved by the Engineer shall be used.



DETAIL A

PASSED	January 1, 2009
ENGINEER OF POLICY AND PROCEDURES	
APPROVED	January 1, 2009
ENGINEER OF DESIGN AND ENVIRONMENT	
ISSUED	15-1-1 01/09

CHAIN LINK FENCE

(Sheet 3 of 3)

STANDARD 664001-02

SECTION NO.: 12-E9121-00-BT
 DRAWN BY: AP
 CHECKED BY: JRK
 APPROVED BY: GAO
 DATE: 8/12/2016
 SCALE: N.T.S.

NO.	DATE	REVISIONS/ISSUANCES
1	8/12/16	ISSUED FOR FINAL
2	5/6/16	ISSUED FOR FINAL
3	6/05/15	ISSUED FOR FINAL REVIEW

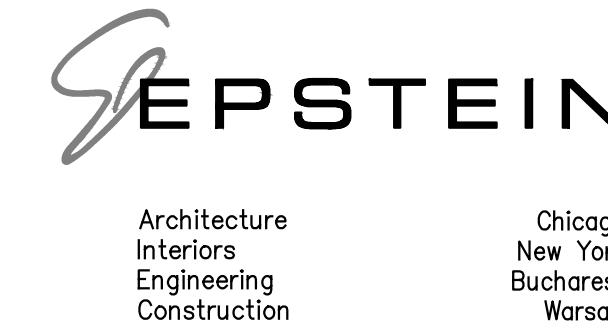
CITY OF CHICAGO

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Architecture
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ADDISON UNDERBRIDGE TRAIL CONNECTION FROM
 CLARK PARK TO CALIFORNIA PARK

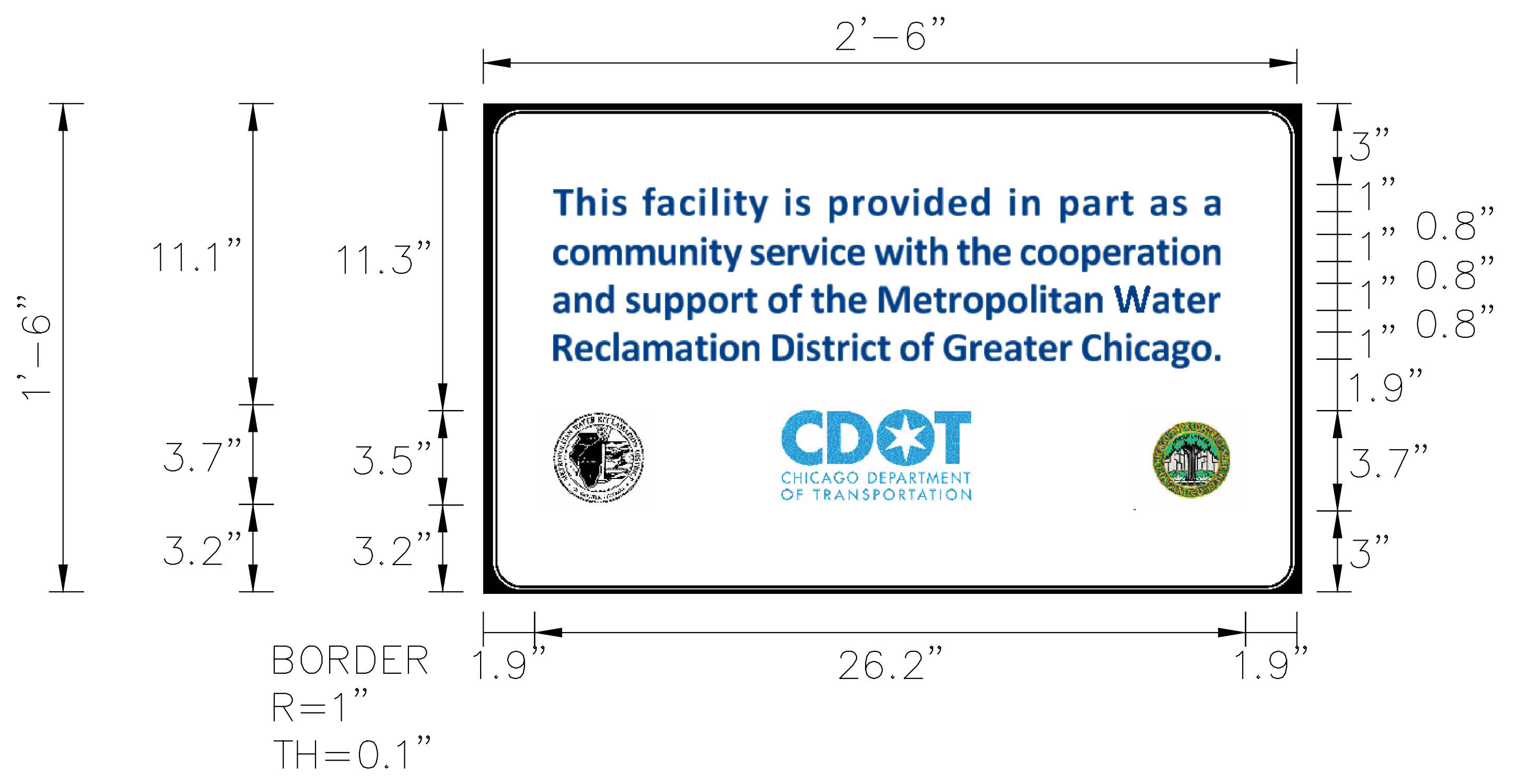
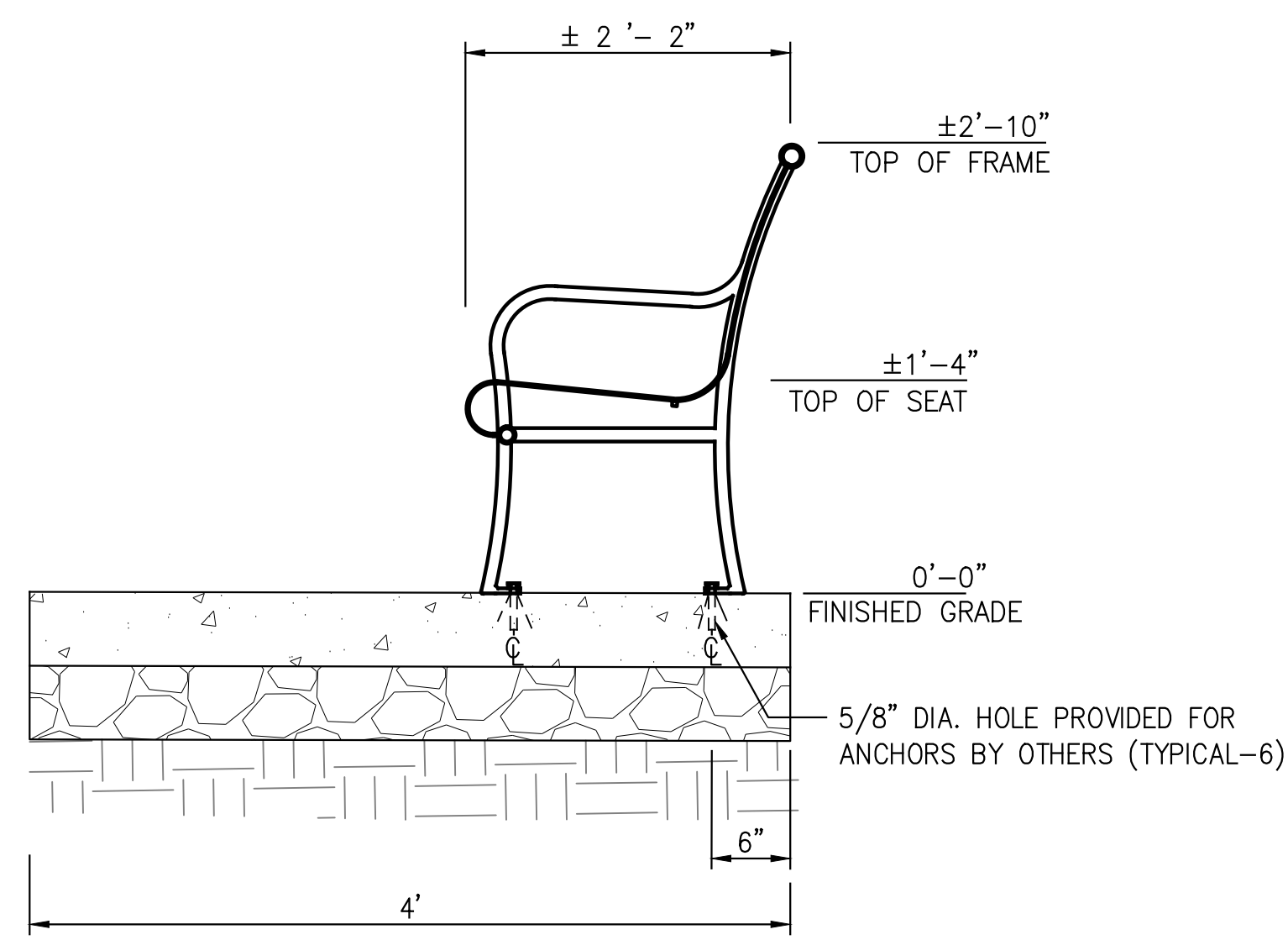
CHAIN LINK FENCE DETAILS

SHEET NO.
C-37

SHEET 37 OF 158

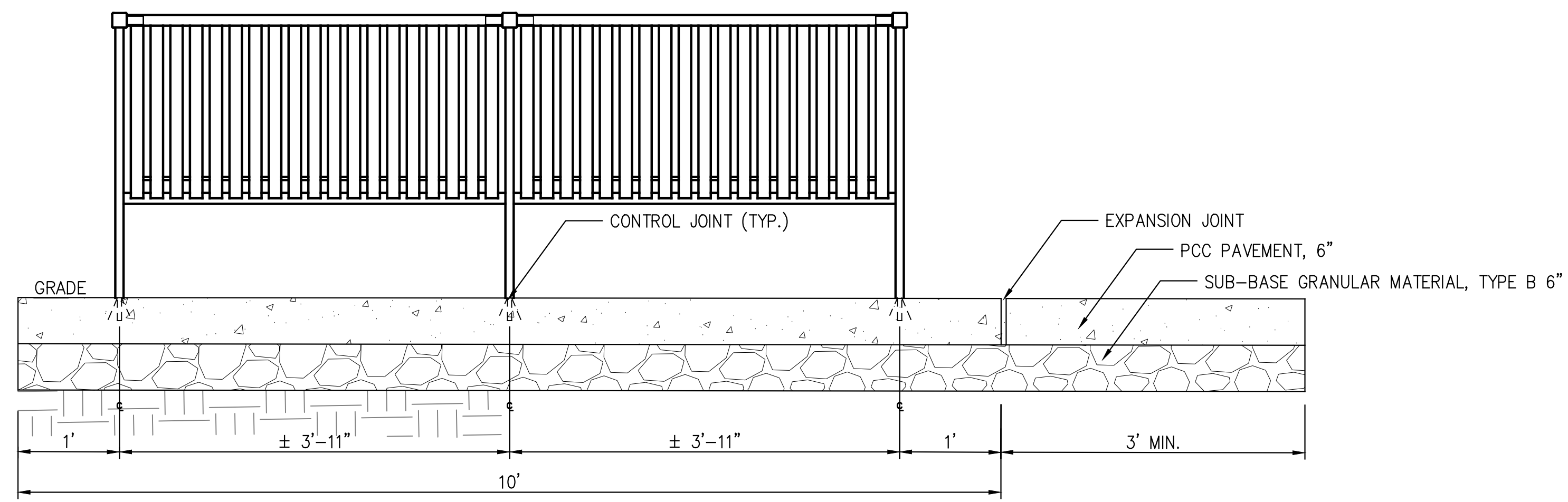
REVISION

January 24, 2016 2:00 PM \Projects\2000\20346 - CDOT - Addison Underbridge Connection - \DISCIPLINE\CHA Working\Sheets\036_Bench AND Sign.dwg



FONT:
Calibri Bold

"MWRD" SIGN PANEL



TIMBERFORM STEEL BENCH (MODEL 2807-8) FROM COLUMBIA CASCADE CO.; OR
DUMOR STEEL BENCH (MODEL #93-40-2) FROM NUTOYS; OR
CITY VIEW BENCHES (MODEL #CV1-2001) FROM SITESCAPES

GENERAL NOTES

- SEE IDOT STANDARD 420001 FOR DETAILS NOT SHOWN.

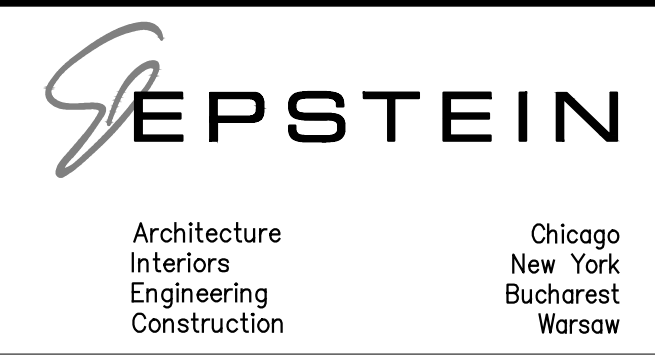
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 DRAWN BY: JRK
 CHECKED BY: JRK
 APPROVED BY: GAO
 DATE: 8/12/2016
 SCALE: N.T.S.

NO.	DATE	REVISIONS/ISSUANCES
1	8/12/16	ISSUED FOR FINAL
2	5/6/16	ISSUED FOR FINAL
3	6/05/15	ISSUED FOR FINAL REVIEW

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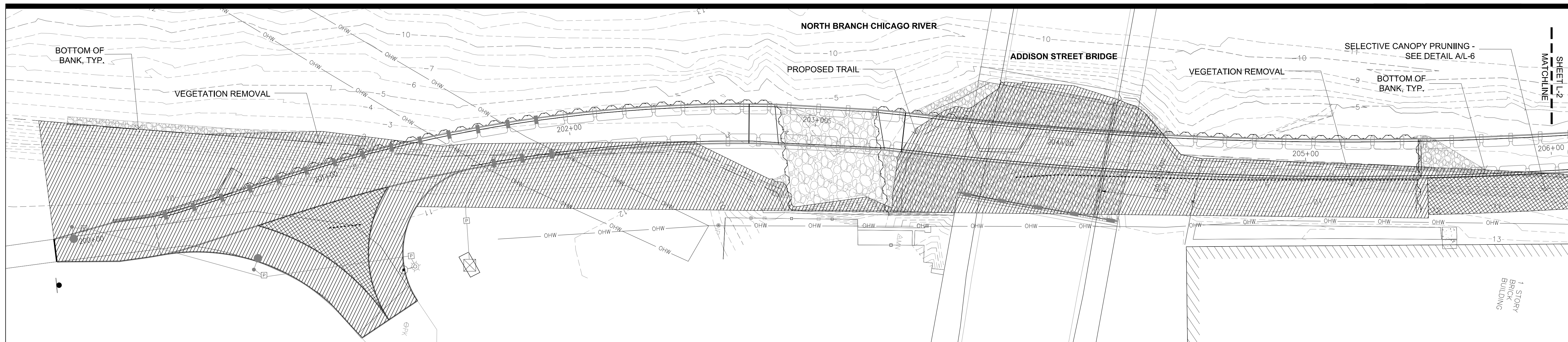
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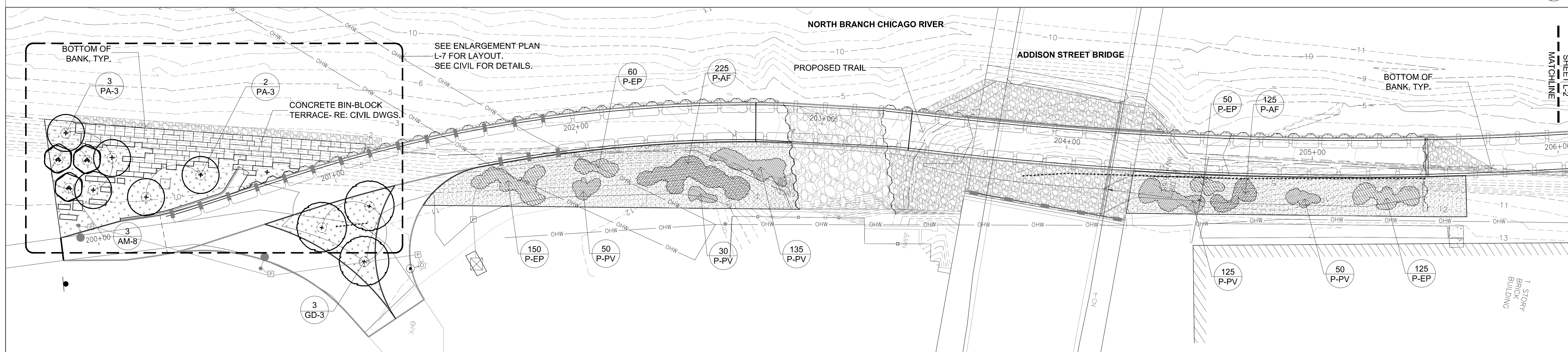
ADDISON UNDERBRIDGE TRAIL CONNECTION FROM
 CLARK PARK TO CALIFORNIA PARK
 BENCH AND MWRD SIGN DETAILS

SHEET NO.
C-38
 SHEET 38 OF 158

REVISION



1 LANDSCAPE PROTECTION & REMOVALS PLAN



2 PROPOSED LANDSCAPE PLAN

LEGEND: LANDSCAPE PROTECTION & REMOVALS

- | | | | | |
|--|--|--|---|---|
| | EXISTING TREE - IDENTIFIED IN SURVEY | | TREE PROTECTION FENCE | 1. SEE A/L-6 FOR VEGETATION CLEARANCES |
| | ROOT PRUNE EXISTING TREE, VERIFY CRITICAL ROOT ZONE IN FIELD | | VEGETATION REMOVAL: CLEAR ALL VEGETATION & DEBRIS IN CONFLICT WITH PATH | 2. EXTENT OF REMOVALS TO BE VERIFIED IN FIELD AND DETERMINED LANDSCAPE ARCHITECT. |
| | | | SELECTIVE CANOPY PRUNING: POSSIBLE OVERHEAD CLEARANCE CONFLICT, REMOVE GROUND PLANE VEGETATION IF IN CONFLICT WITH PROPOSED PLANTING. | 3. STUMP AND GRIND TREES IN PATH CONFLICT AREA TO GRADE, UNLESS NOTED OTHERWISE |
| | | | TURF REMOVAL DUE TO CONSTRUCTION ACTIVITY. CONFIRM EXTENTS WITH SITE SUPERINTENDANT. | 4. GROUND PLANE VEGETATION TO REMAIN UNDISTURBED TO THE EXTENT POSSIBLE WHERE NOT IN A PROPOSED PLANTING BED. |

LEGEND: PROPOSED LANDSCAPE PLAN

- | | | |
|--|---|---|
| TREES: | NO-MOW MATRIX & SEEDED LAWN: | PLUGS: |
| | | |
| GD-3 - GYMNOCLADUS DIOICUS (KENTUCKY COFFEETREE), 3" BB | IDOT CLASS 3 NORTHERN ILLINOIS SLOPE MIXTURE INTERPLANTED WITH PLUGS - SEE PLANT SCHEDULE | P-EP - ECHINACEA PALLIDA (PALE PURPLE CONEFLOWER) @ 15" O.C |
| | | P-AF - AGASTACHE FOENICULUM (PURPLE GIANT HYSSOP) @ 15" O.C |
| AM-8 - AMELANCHIER G. 'AUTUMN BRILLIANCE' (AUTUMN BRILLIANCE SERVICEBERRY, 8" SHRUB FORM | IDOT CLASS 1 LAWN MIXTURE | P-PV - PYCANTHEMUM VIRGINIANUM (MOUNTAIN MINT) @ 15" O.C |
| | | |
| PA-3 - PLATANUS X A. 'BLOODGOOD (BLOODGOOD LONDON PLANE TREE), 3" BB | AMENITIES: | |
| | BENCH | |

SOILS NOTES:

- TREES SHALL HAVE 24" DEPTH SOIL PROFILE TO 3' FROM TRUNK.
- PLUGS & NO-MOW MATRIX SHALL HAVE 12" DEPTH SOIL PROFILE UNLESS NOTED OTHERWISE.
- SEEDED LAWN SHALL HAVE 8" DEPTH SOIL PROFILE UNLESS NOTED OTHERWISE.
- BIO-INFILTRATION AREAS SHALL HAVE 24" DEPTH SOIL PROFILE. SEE GRADING PLAN FOR LOCATIONS.
- VEGETATIVE FILTER STRIPS SHALL HAVE 18" DEPTH SOIL PROFILE. SEE GRADING PLAN FOR LOCATION.
- REFER TO SPECIFICATIONS FOR SOIL PROFILE COMPOSITIONS AND REQUIREMENTS.

SECTION NO.: 12-E9121-00-BT
 DRAWN BY: AGB
 CHECKED BY: MS
 APPROVED BY: JE
 DATE: 03/27/2015
 SCALE: 1" = 20'-0"

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 312.744.3600

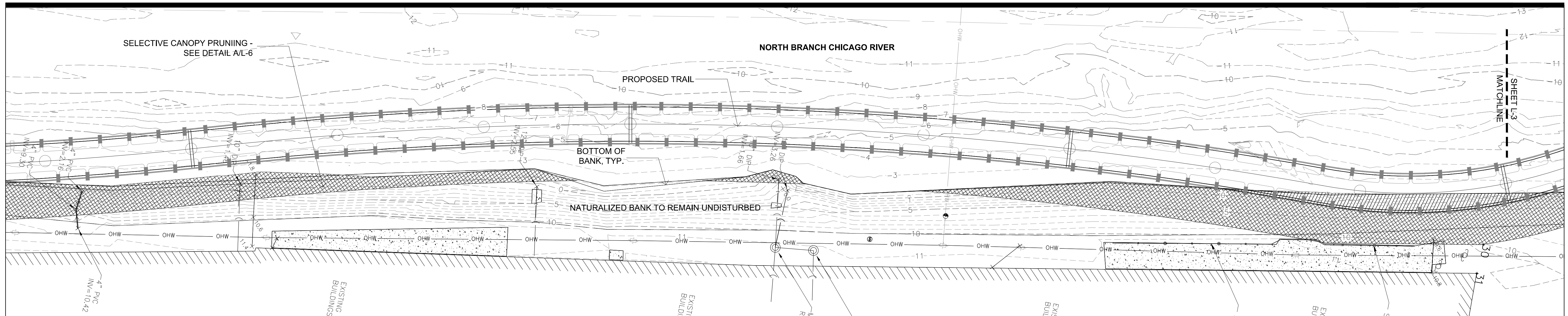


CONSULTANT
 HOERR SCHAUDT LANDSCAPE ARCHITECTS
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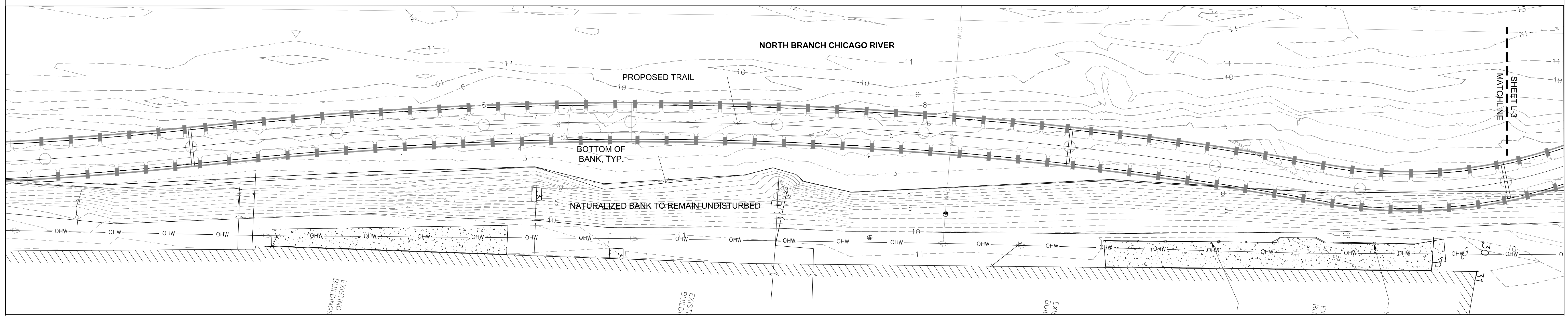
HOERR SCHAUDT
 landscape architects

ADDISON UNDERBRIDGE TRAIL CONNECTION
 FROM CLARK PARK TO CALIFORNIA PARK
 LANDSCAPE PLAN
 STA. 200+00 TO STA. 206+00

SHEET NO. L-1
 SHEET 39 OF 158
 REVISION



1 LANDSCAPE PROTECTION & REMOVALS PLAN



2 PROPOSED LANDSCAPE PLAN

LEGEND: LANDSCAPE PROTECTION & REMOVALS

- | | | | | |
|--|---|--|--|---|
| | EXISTING TREE - IDENTIFIED IN SURVEY | | TREE PROTECTION FENCE | <ol style="list-style-type: none"> 1. SEE A/L-6 FOR VEGETATION CLEARANCES 2. EXTENT OF REMOVALS TO BE VERIFIED IN FIELD AND DETERMINED LANDSCAPE ARCHITECT. 3. STUMP AND GRIND TREES IN PATH CONFLICT AREA TO GRADE, UNLESS NOTED OTHERWISE 4. GROUND PLANE VEGETATION TO REMAIN UNDISTURBED TO THE EXTENT POSSIBLE WHERE NOT IN A PROPOSED PLANTING BED. |
| | ROOT PRUNE EXISTING TREE, VERIFY CRITICAL ROOT ZONE IN FIELD | | VEGETATION REMOVAL: CLEAR ALL VEGETATION & DEBRIS IN CONFLICT WITH PATH | |
| | SELECTIVE CANOPY PRUNING: POSSIBLE OVERHEAD CLEARANCE CONFLICT, REMOVE GROUND PLANE VEGETATION IF IN CONFLICT WITH PROPOSED PLANTING. | | TURF REMOVAL DUE TO CONSTRUCTION ACTIVITY. CONFIRM EXTENTS WITH SITE SUPERINTENDANT. | |
| | | | | |

SECTION NO.: 12-E9121-00-BT
 DRAWN BY: AGB
 CHECKED BY: MS
 APPROVED BY: JE
 DATE: 03/27/2015
 SCALE: 1" = 20'-0"

NO. DATE REVISIONS/ISSUANCES
 05/06/2016 ISSUED FOR FINAL

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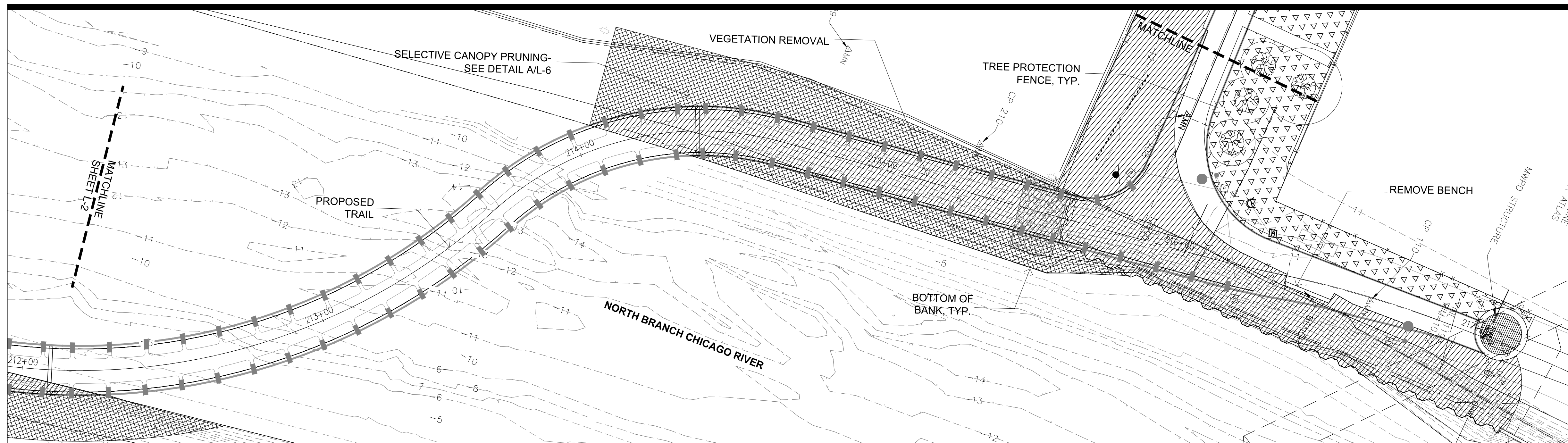
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 landscape architects

ADDISON UNDERBRIDGE TRAIL CONNECTION
 FROM CLARK PARK TO CALIFORNIA PARK

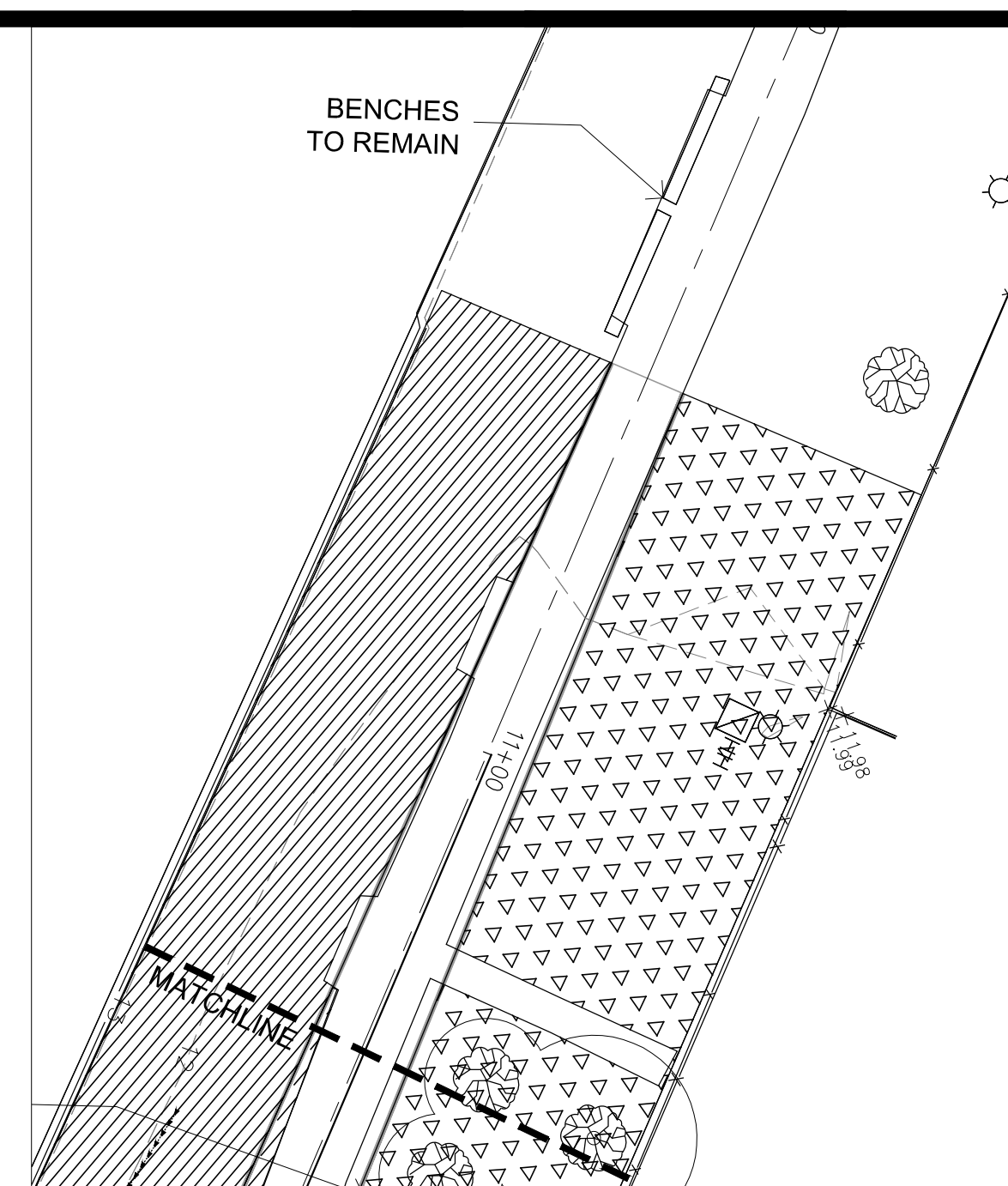
LANDSCAPE PLAN
 STA. 206+00 TO STA. 212+00

SHEET NO. L-2

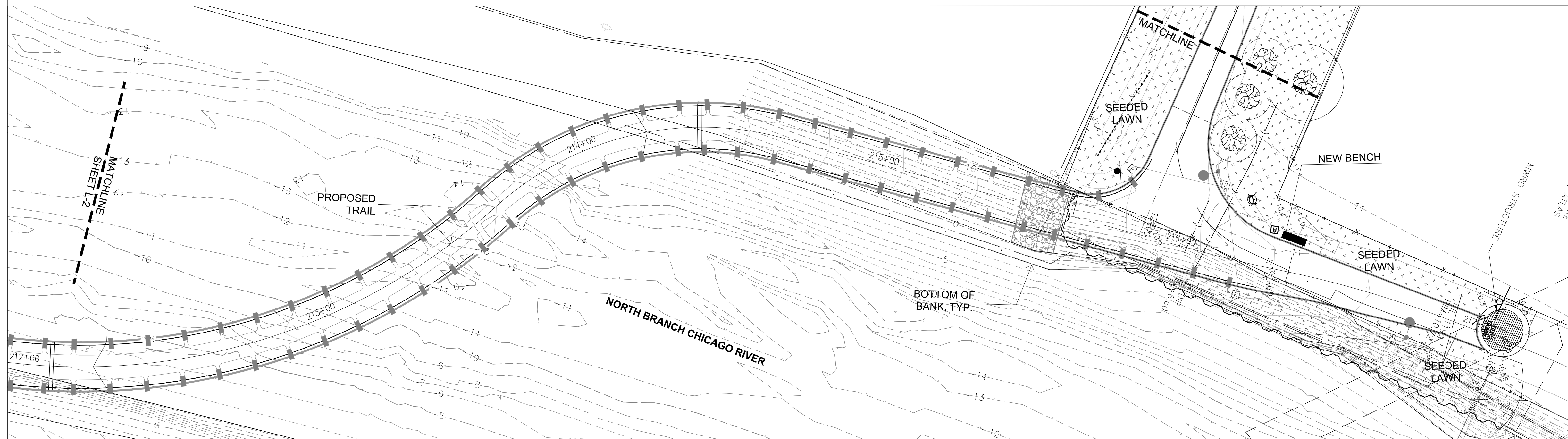
SHEET 40 OF 158



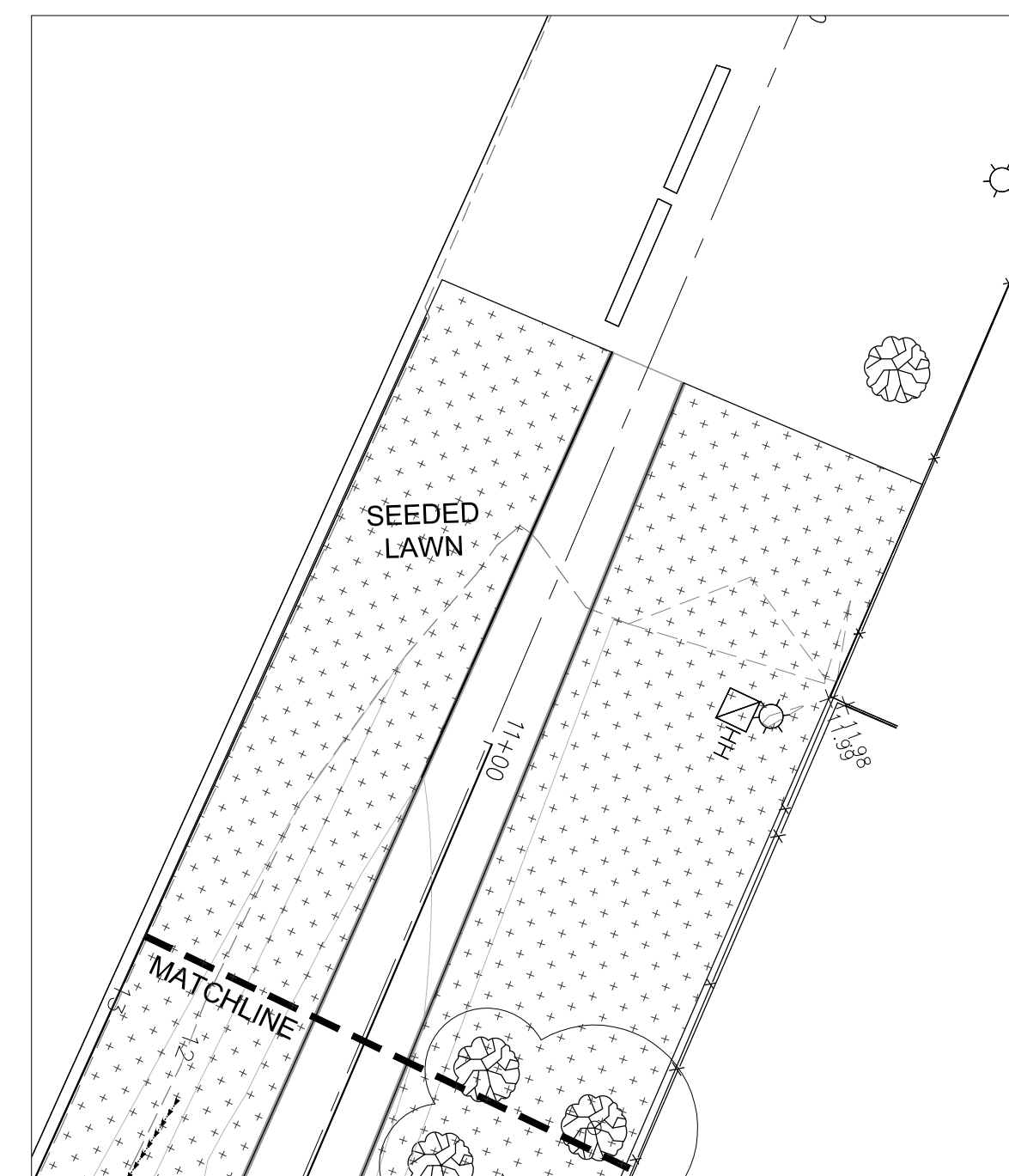
1 LANDSCAPE PROTECTION & REMOVALS PLAN



0 20 40



2 PROPOSED LANDSCAPE PLAN



0 20 40

LEGEND: LANDSCAPE PROTECTION & REMOVALS

- EXISTING TREE - IDENTIFIED IN SURVEY
 - ROOT PRUNE EXISTING TREE, VERIFY CRITICAL ROOT ZONE IN FIELD
 - TREE PROTECTION FENCE
 - VEGETATION REMOVAL: CLEAR ALL VEGETATION & DEBRIS IN CONFLICT WITH PATH
 - SELECTIVE CANOPY PRUNING: POSSIBLE OVERHEAD CLEARANCE CONFLICT, REMOVE GROUND PLANE VEGETATION IF IN CONFLICT WITH PROPOSED PLANTING.
 - TURF REMOVAL DUE TO CONSTRUCTION ACTIVITY. CONFIRM EXTENTS WITH SITE SUPERINTENDANT.
1. SEE A/L-6 FOR VEGETATION CLEARANCES
 2. EXTENT OF REMOVALS TO BE VERIFIED IN FIELD AND DETERMINED LANDSCAPE ARCHITECT.
 3. STUMP AND GRIND TREES IN PATH CONFLICT AREA TO GRADE, UNLESS NOTED OTHERWISE
 4. GROUND PLANE VEGETATION TO REMAIN UNDISTURBED TO THE EXTENT POSSIBLE WHERE NOT IN A PROPOSED PLANTING BED.

LEGEND: PROPOSED LANDSCAPE PLAN

- TREES:**
- GD-3 - GYMNOCLADUS DIOICUS (KENTUCKY COFFEETREE), 3" BB
 - AM-8 - AMELANCHIER G. 'AUTUMN BRILLIANCE' (AUTUMN BRILLIANCE SERVICEBERRY, 8" SHRUB FORM
 - PA-3 - PLATANUS X A. 'BLOODGOOD (BLOODGOOD LONDON PLANE TREE), 3" BB
- NO-MOW MATRIX & SEEDED LAWN:**
- IDOT CLASS 3 NORTHERN ILLINOIS SLOPE MIXTURE INTERPLANTED WITH PLUGS - SEE PLANT SCHEDULE
 - IDOT CLASS 1 LAWN MIXTURE
- AMENITIES:**
- BENCH
- PLUGS:**
- P-EP - ECHINACEA PALLIDA (PALE PURPLE CONEFLOWER) @ 15" O.C
 - P-AF - AGASTACHE FOENICULUM (PURPLE GIANT HYSSOP) @ 15" O.C
 - P-PV - PYCANthemum VIRGINIANUM (MOUNTAIN MINT) @ 15" O.C

SOILS NOTES:

1. TREES SHALL HAVE 24" DEPTH SOIL PROFILE TO 3' FROM TRUNK.
2. PLUGS & NO-MOW MATRIX SHALL HAVE 12" DEPTH SOIL PROFILE UNLESS NOTED OTHERWISE.
3. SEEDED LAWN SHALL HAVE 8" DEPTH SOIL PROFILE UNLESS NOTED OTHERWISE.
4. BIO-INFILTRATION AREAS SHALL HAVE 24" DEPTH SOIL PROFILE. SEE GRADING PLAN FOR LOCATIONS.
5. VEGETATIVE FILTER STRIPS SHALL HAVE 18" DEPTH SOIL PROFILE. SEE GRADING PLAN FOR LOCATION.
6. REFER TO SPECIFICATIONS FOR SOIL PROFILE COMPOSITIONS AND REQUIREMENTS.

SECTION NO.:	12-E9121-00-BT
DRAWN BY:	AGB
CHECKED BY:	MS
APPROVED BY:	JE
DATE:	03/27/2015
SCALE:	1" = 20'-0"

NO.	DATE	REVISIONS/ISSUANCES
05/06/2016		ISSUED FOR FINAL

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DEPARTMENT OF TRANSPORTATION
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312.744.3600

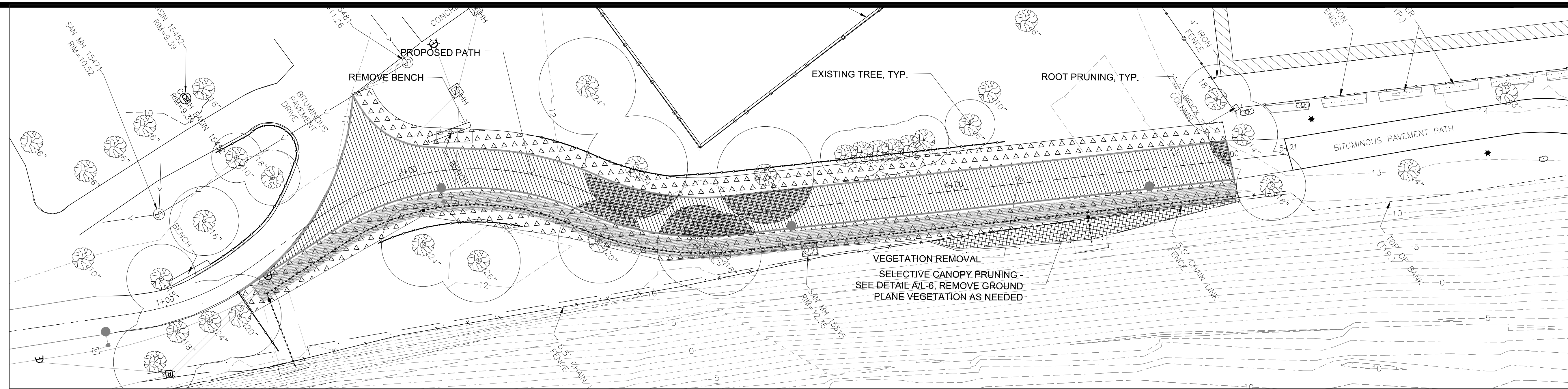


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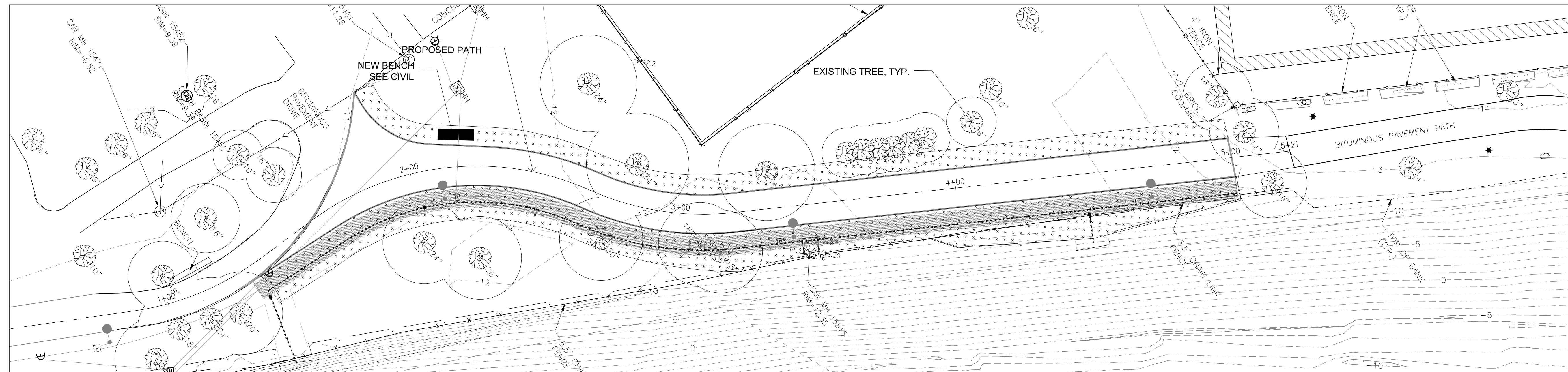
HOERR SCHAUDT
landscape architects

ADDITION UNDERBRIDGE TRAIL CONNECTION
FROM CLARK PARK TO CALIFORNIA PARK
LANDSCAPE PLAN
STA. 212+00 TO STA. 216+91

SHEET NO.	L-3	REVISION	
SHEET	41	OF	158



1 LANDSCAPE PROTECTION & REMOVALS PLAN



2 PROPOSED LANDSCAPE PLAN

LEGEND: LANDSCAPE PROTECTION & REMOVALS

- | | | | | |
|--|--|--|---|---|
| | EXISTING TREE - IDENTIFIED IN SURVEY | | TREE PROTECTION FENCE | 1. SEE A/L-6 FOR VEGETATION CLEARANCES |
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LEGEND: PROPOSED LANDSCAPE PLAN

- | | | |
|---------------|--|---------------|
| TREES: | NO-MOW MATRIX & SEEDED LAWN: | PLUGS: |
| | GD-3 - GYMNOCLADUS DIOICUS (KENTUCKY COFFEETREE), 3" BB | |
| | AM-8 - AMELANCHIER G. 'AUTUMN BRILLIANCE' (AUTUMN BRILLIANCE SERVICEBERRY, 8" SHRUB FORM | |
| | PA-3 - PLATANUS X A. 'BLOODGOOD (BLOODGOOD LONDON PLANE TREE), 3" BB | |
| | AMENITIES: | |
| | | BENCH |

SOILS NOTES:

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- BIO-INFILTRATION AREAS SHALL HAVE 24" DEPTH SOIL PROFILE. SEE GRADING PLAN FOR LOCATIONS.
- VEGETATIVE FILTER STRIPS SHALL HAVE 18" DEPTH SOIL PROFILE. SEE GRADING PLAN FOR LOCATION.
- REFER TO SPECIFICATIONS FOR SOIL PROFILE COMPOSITIONS AND REQUIREMENTS.

SECTION NO.: 12-E9121-00-BT
 DRAWN BY: AGB
 CHECKED BY: MS
 APPROVED BY: JE
 DATE: 03/27/2015
 SCALE:

NO. DATE REVISIONS/ISSUANCES

CITY OF CHICAGO

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 landscape architects

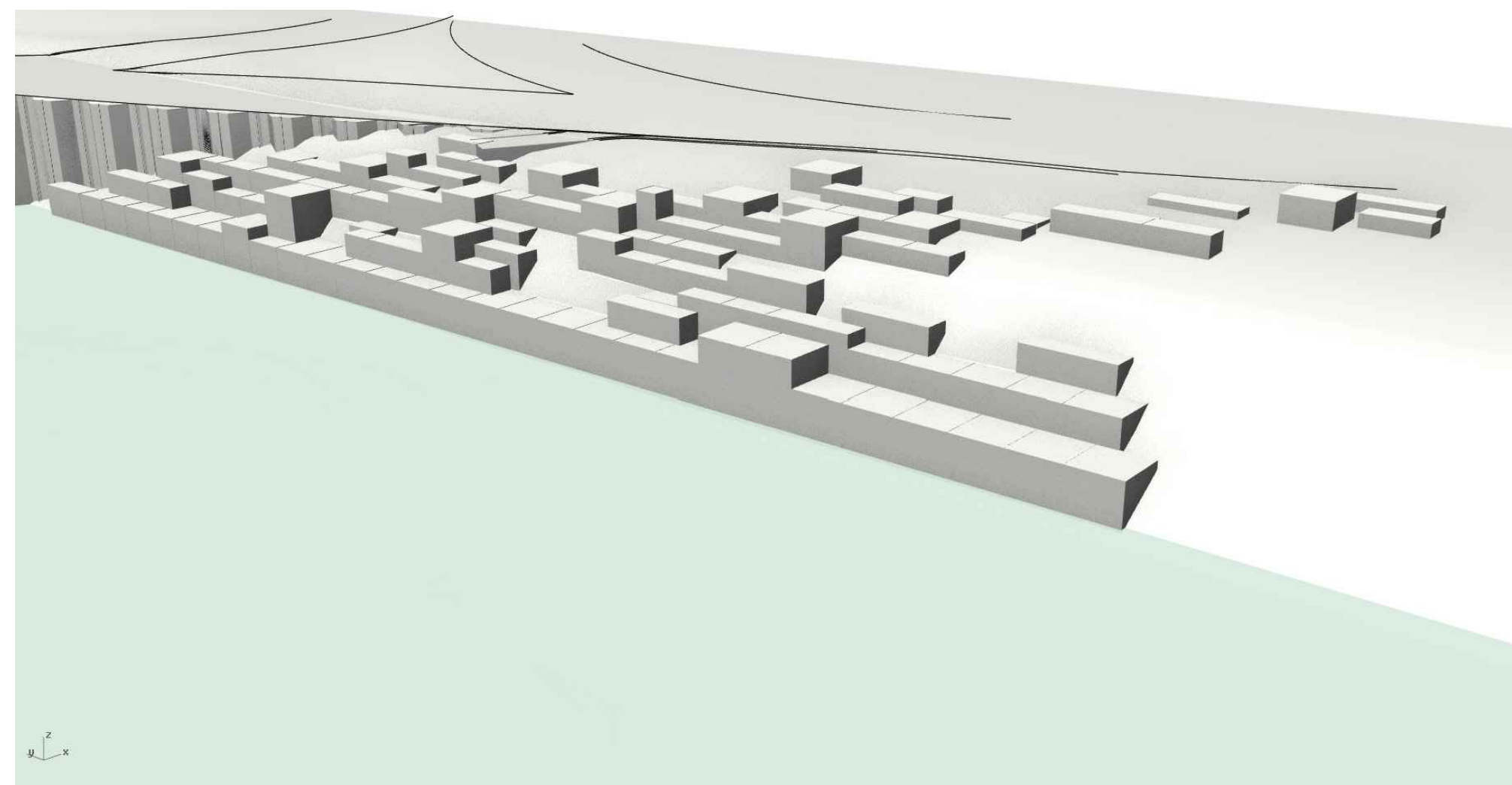
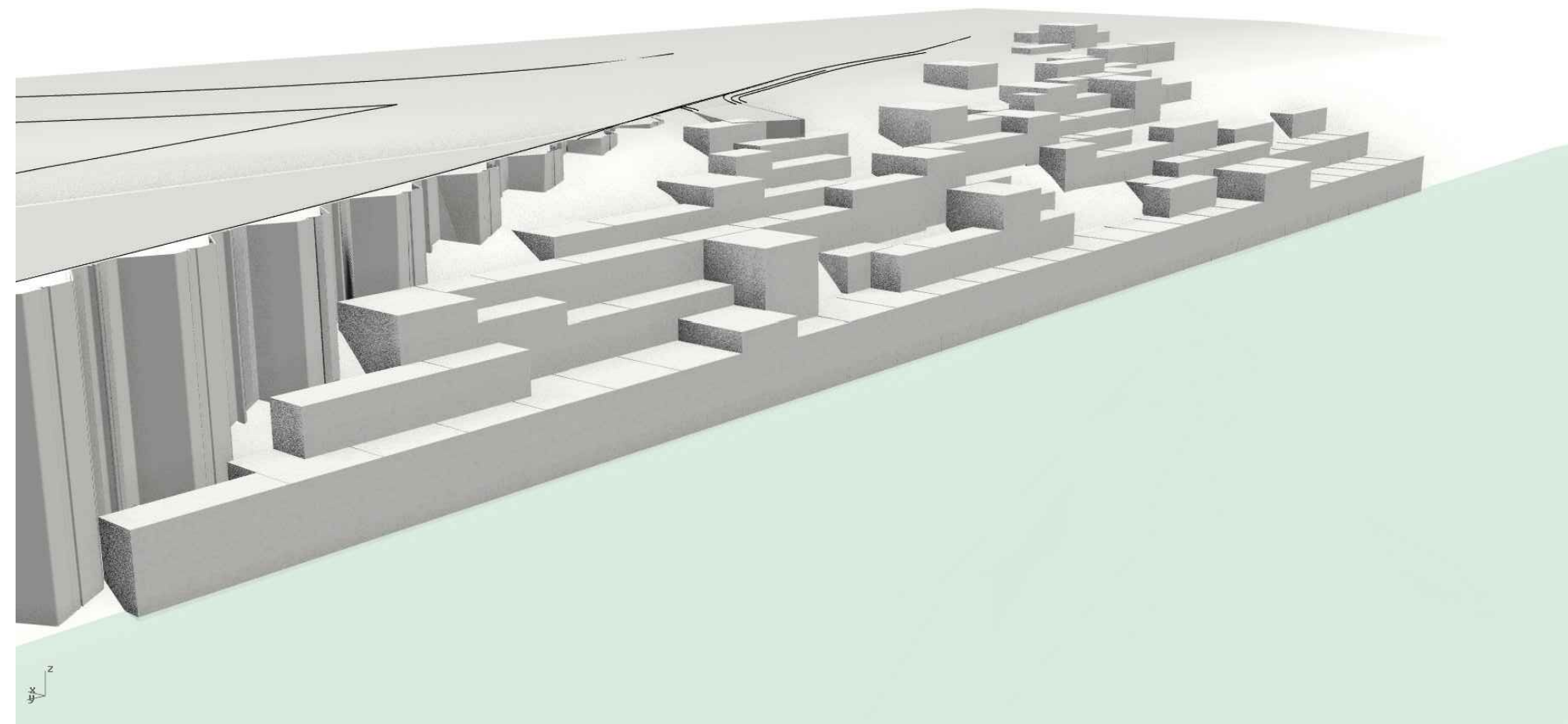
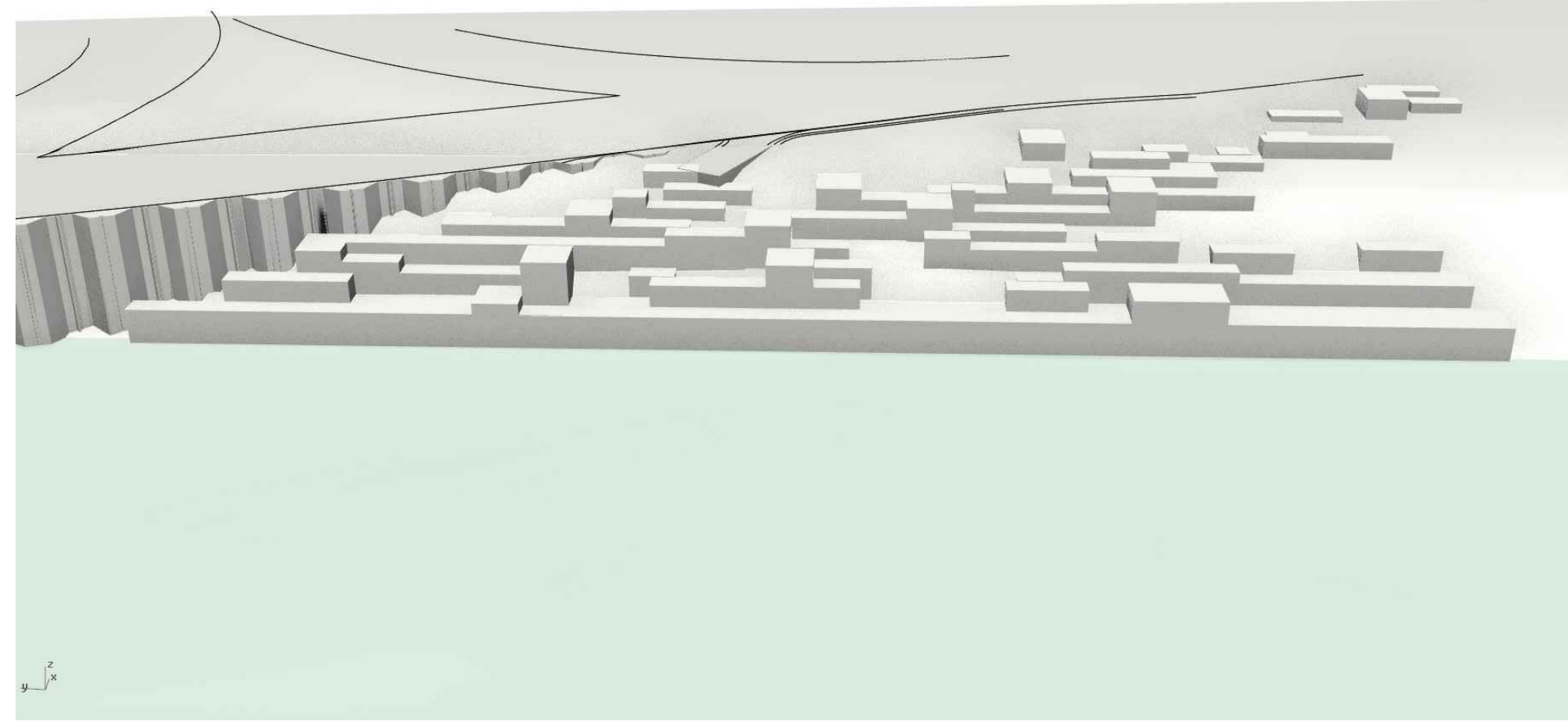
ADDISON UNDERBRIDGE TRAIL CONNECTION
 FROM CLARK PARK TO CALIFORNIA PARK

LANDSCAPE PLAN
 STA. 0+00 TO STA. 5+21

SHEET NO.
 L-4

SHEET 42 OF 158

REVISION



D CONCRETE BIN-BLOCK TERRACE (FOR REFERENCE ONLY)

SEED MIXES:

KEY	ITEM #	BOTANICAL NAME	COMMON NAME	SIZE	ROOT	QUANTITY	MATURE SIZE	BLOOM HEIGHT	REMARKS
IDOT CLASS 3	21								.5 ACRE TOTAL
		<i>Elymus canadensis</i>	Canada Wild Rye	–	Seed	2.5 LBS			5lbs./Acre
		<i>Lolium perenne</i>	Perennial Ryegrass	–	Seed	10 LBS			20lbs./Acre
		<i>Trifolium hybridum</i>	Alsike Clover	–	Seed	2.5 LBS			5lbs./Acre
		<i>Desmanthus illinoensis</i>	Illinois Bundleflower	–	Seed	1 LBS			2lbs./Acre
		<i>Schizachyrium scoparium</i>	Little Bluestem	–	Seed	6 LBS			12lbs./Acre
		<i>Bouteloua curtipendula</i>	Side-Oats Grama	–	Seed	5 LBS			10lbs./Acre
		<i>Distichlis stricta</i>	Fults Saltgrass	–	Seed	15 LBS			30lbs./Acre
		<i>Avena sp.</i>	Spring Oats	–	Seed	25 LBS			50lbs./Acre
		<i>Elymus trachycaulus</i>	Slender Wheatgrass	–	Seed	7.5 LBS			15lbs./Acre
		<i>Bouteloua dactyloides</i>	Buffalo Grass	–	Seed	2.5 LBS			5lbs./Acre
IDOT CLASS 1	42								.5 ACRE TOTAL
		<i>Elymus canadensis</i>	Kentucky Bluegrass	–	Seed	50LBS			100 lbs./acre
		<i>Lolium perenne</i>	Perennial Ryegrass	–	Seed	30 LBS			60 lbs./Acre
		<i>Festuca rubra</i>	Creeping Red Fescue	–	Seed	20 LBS			40 lbs./Acre

PERENNIAL PLUGS:

KEY	ITEM #	BOTANICAL NAME	COMMON NAME	SIZE	ROOT	QUANTITY	MATURE SIZE	BLOOM HEIGHT	REMARKS
P-AF	24	<i>Agastache foeniculum</i>	Purple Giant Hyssop	–	Plug	350	3'-6"	6'	15" O.C
P-EP	22	<i>Echinacea pallida</i>	Pale Purple Coneflower	–	Plug	385	1'-3"	3'	15" O.C
P-PV	30	<i>Pycnanthemum virginianum</i>	Mountain Mint	–	Plug	390	1'-3"	3'	15" O.C

B PLANT SCHEDULE - SEED & PERENNIAL PLUGS

DECIDUOUS TREES:

KEY	ITEM #	BOTANICAL NAME	COMMON NAME	SIZE	ROOT	QUANTITY	MATURE SIZE	BLOOM HEIGHT	REMARKS
PA-3	35	<i>Platanus x acerifolium</i> 'Bloodgood'	London Plane Tree	3"	BB	7	40' x 20'	N/A	MIN. 7' CLR. BRANCH
AM-8	43	<i>Amelanchier x g. 'Autumn Brilliance'</i>	Autumn Brilliance Serviceberry	8"	BB	5	20' x 15'	N/A	SHRUB FORM
GD-3	11	<i>Gymnocladus dioica</i>	Kentucky Coffee Tree	3"	BB	3	40' x 30'	N/A	MIN. 7' CLR. BRANCH

A PLANT SCHEDULE - TREES

SECTION NO.: 12-E9121-00-BT
 DRAWN BY: AGB
 CHECKED BY: MS
 APPROVED BY: JE
 DATE: 03/27/2015
 SCALE:

NO. DATE REVISIONS/ISSUANCES
 05/06/2016 ISSUED FOR FINAL

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ADDISON UNDERBRIDGE TRAIL CONNECTION
 FROM CLARK PARK TO CALIFORNIA PARK

PLANT SCHEDULE &
 3D TERRACE MODEL REFERENCE IMAGERY

SHEET NO.
 L-6

SHEET 43 OF 158

REVISION

NOTES:

DECIDUOUS TREES:

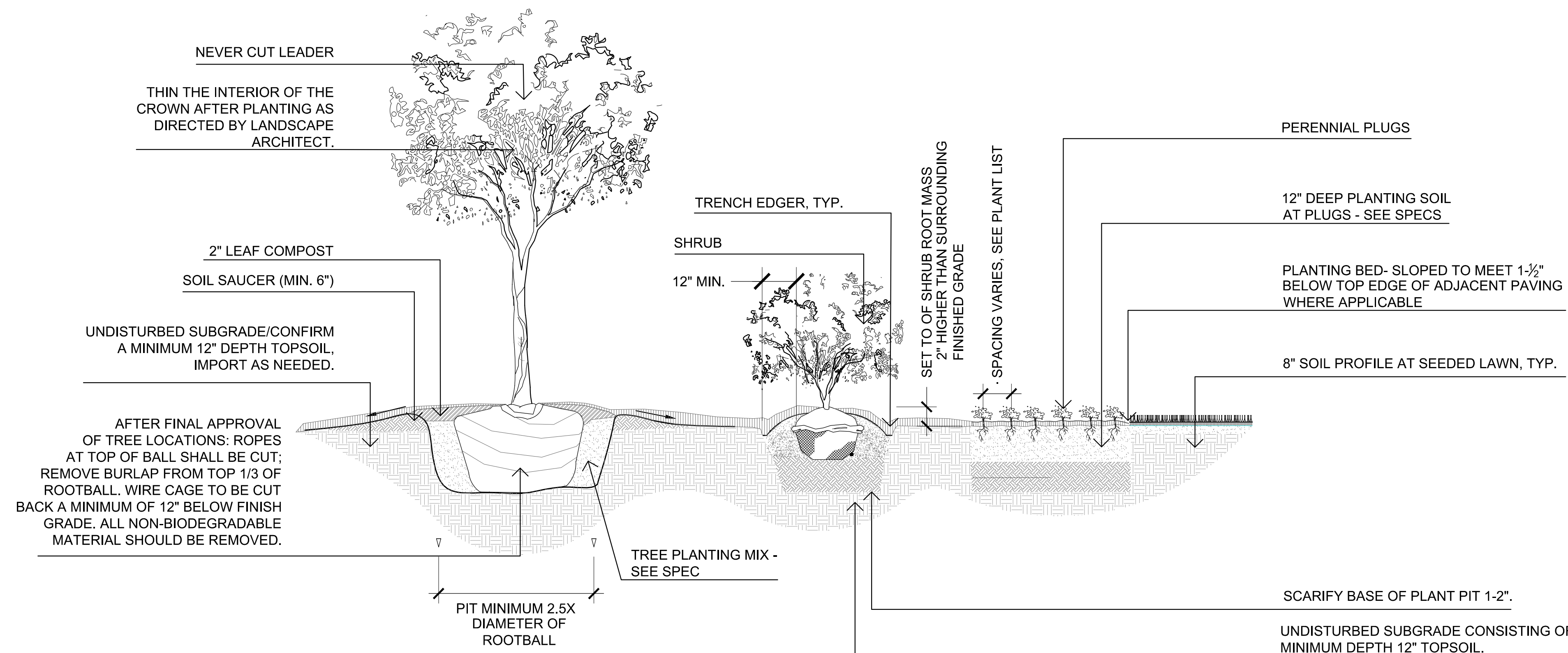
1. TREES SHALL BEAR SAME RELATION TO FINISHED GRADE AS THEY DID TO PREVIOUSLY EXISTING GRADE ON SITE OR IN NURSERY.
2. DO NOT ALLOW AIR POCKETS TO FORM IN SOIL WHEN BACKFILLING.
3. TREE STAKING/GUYING SHOULD BE REVIEWED WITH LANDSCAPE ARCHITECT WHEN CONTRACTOR DEEMS IT NECESSARY.
5. ROOT FLARE OF TREE TO BE SET NO MORE THAN 3" ABOVE FINISHED GRADE IN BORDERS & MASS PLANTINGS.
6. TREES PLANTED WITHIN OR ADJACENT TO PAVING SHALL BE PLANTED WITH ROOT FLARE NO MORE THAN 1" ABOVE ADJACENT GRADE.
7. IF PITS ARE MACHINE EXCAVATED, & GLAZING IS PRONOUNCED, ALL SIDES OF PIT ARE TO BE SCARIFIED.
8. SOAK PLANTS WITH WATER IMMEDIATELY AFTER INSTALLATION.

SHRUBS:

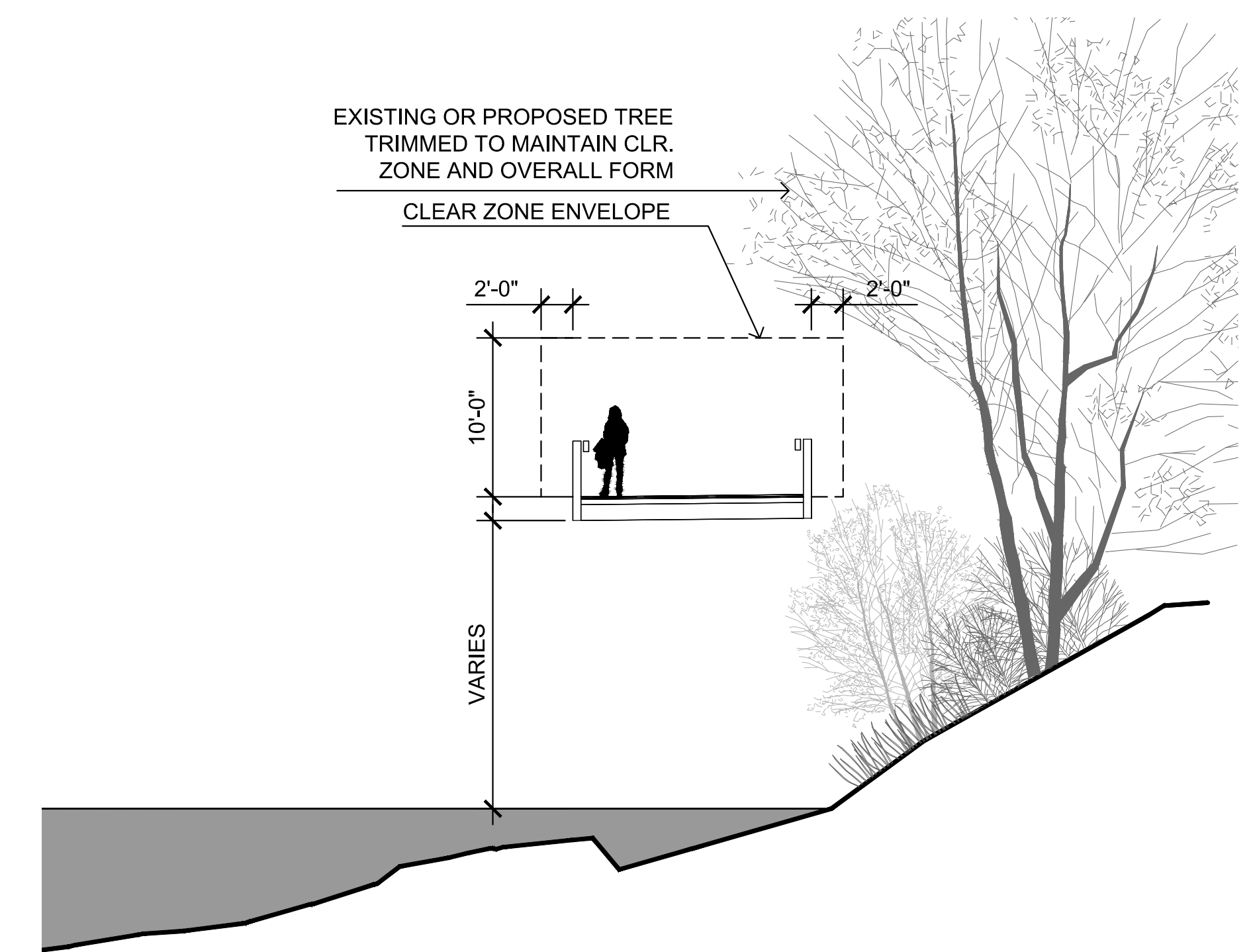
1. DO NOT ALLOW AIR POCKETS TO FORM IN SOIL WHILE BACKFILLING.
2. SOAK PLANTS WITH WATER IMMEDIATELY AFTER INSTALLATION.
3. DO NOT BREAK ROOTBALL.
4. IF B&B MATERIAL, REMOVE BURLAP & STRING FROM AROUND TOP 1/3 OF ROOTBALL. IF CONTAINERIZED, LOOSEN ROOT MASS SIGNIFICANTLY BY HAND OR CUT ROOT MASS VERTICALLY WITH KNIFE ON 4 SIDES TO A DEPTH OF 1" & CUT BOTTOM OF ROOT MASS IN SAME MANNER IN AN 'X' PATTERN.

PERENNIALS & GROUND COVER:

1. DO NOT ALLOW AIR POCKETS TO FORM IN SOIL WHILE BACKFILLING.
2. SOAK PLANTS IMMEDIATELY WITH WATER AFTER INSTALLATION.
3. LOOSEN ROOT MASS SIGNIFICANTLY BY HAND OR CUT ROOT MASS VERTICALLY WITH KNIFE ON 4 SIDES TO A DEPTH OF 1" & CUT BOTTOM OF ROOT MASS IN SAME MANNER IN AN 'X' PATTERN.
4. PLANT APPROXIMATELY 1" ABOVE PLANTING MEDIA TO ALLOW FOR TYPICAL DRESSING OF MULCH.
5. SOAK PLANTS WITH WATER IMMEDIATELY AFTER INSTALLATION.



B TYPICAL PLANTING DETAILS
SCALE: N.T.S.



A TYPICAL TRAIL VEGETATION CLEARANCES
SCALE: 1/8" = 1'-0"

SECTION NO.: 12-E9121-00-BT
 DRAWN BY: AGB
 CHECKED BY: MS
 APPROVED BY: JE
 DATE: 03/27/2015
 SCALE: AS NOTED

NO.	DATE	REVISIONS/ISSUANCES
05/06/2016	ISSUED FOR FINAL	

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ADDISON UNDERBRIDGE TRAIL CONNECTION
 FROM CLARK PARK TO CALIFORNIA PARK

LANDSCAPE DETAILS

SHEET NO.
 L-5

SHEET 44 OF 158

REVISION

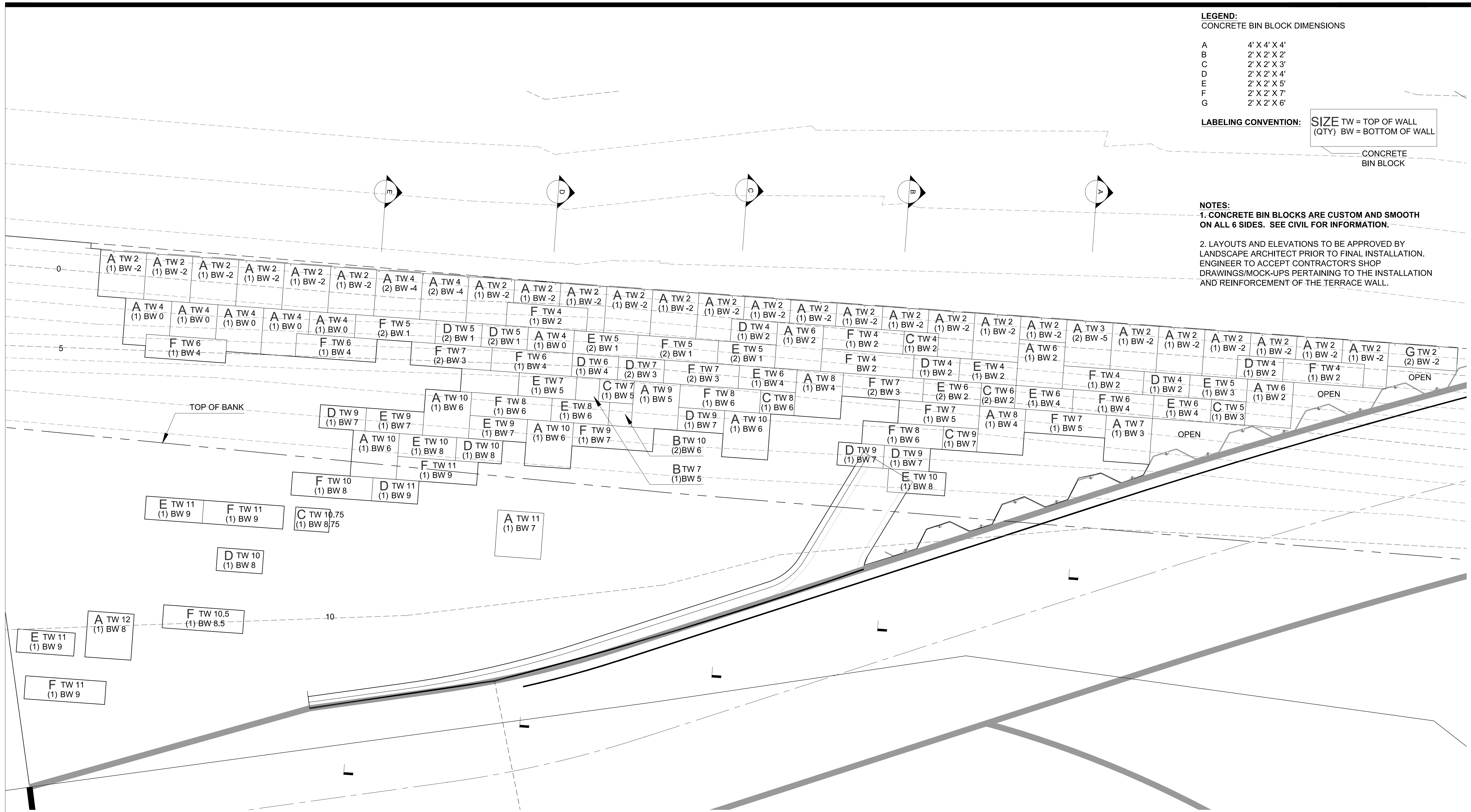
LEGEND:
CONCRETE BIN BLOCK DIMENSIONS

A	4' X 4' X 4'
B	2' X 2' X 2'
C	2' X 2' X 3'
D	2' X 2' X 4'
E	2' X 2' X 5'
F	2' X 2' X 7'
G	2' X 2' X 6'

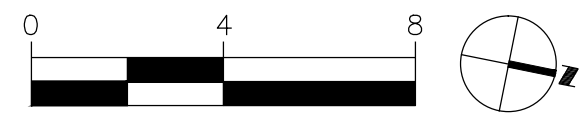
LABELING CONVENTION: SIZE TW = TOP OF WALL
(QTY) BW = BOTTOM OF WALL

CONCRETE
BIN BLOCK

- NOTES:**
1. CONCRETE BIN BLOCKS ARE CUSTOM AND SMOOTH ON ALL 6 SIDES. SEE CIVIL FOR INFORMATION.
 2. LAYOUTS AND ELEVATIONS TO BE APPROVED BY LANDSCAPE ARCHITECT PRIOR TO FINAL INSTALLATION. ENGINEER TO ACCEPT CONTRACTOR'S SHOP DRAWINGS/MOCK-UPS PERTAINING TO THE INSTALLATION AND REINFORCEMENT OF THE TERRACE WALL.



1 BOULDER SLOPE - ENLARGEMENT PLAN
1/4" = 1'-0"



SECTION NO.: 12-E9121-00-BT
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ADDISON UNDERBRIDGE TRAIL CONNECTION
 FROM CLARK PARK TO CALIFORNIA PARK

TERRACE LAYOUT ENLARGEMENT PLAN

SHEET NO.
L-7

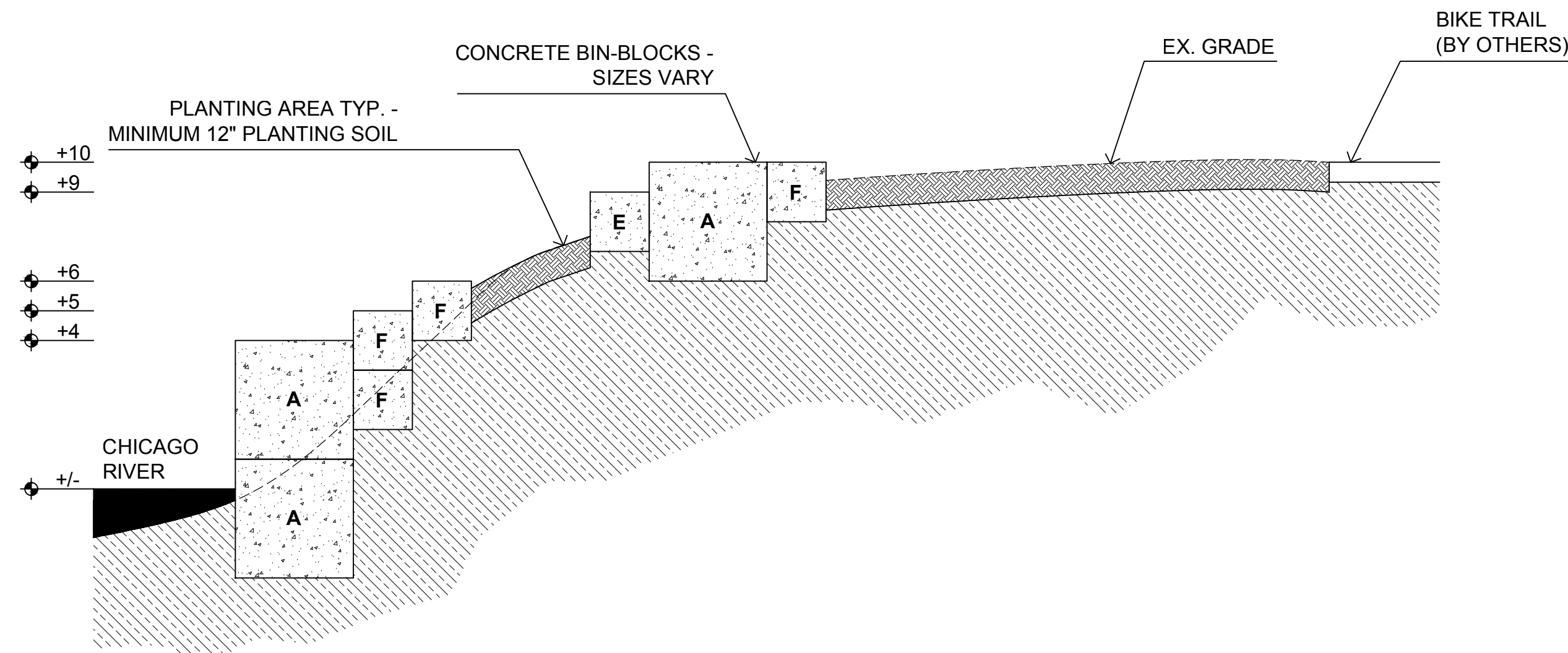
SHEET 45 OF 158

REVISION

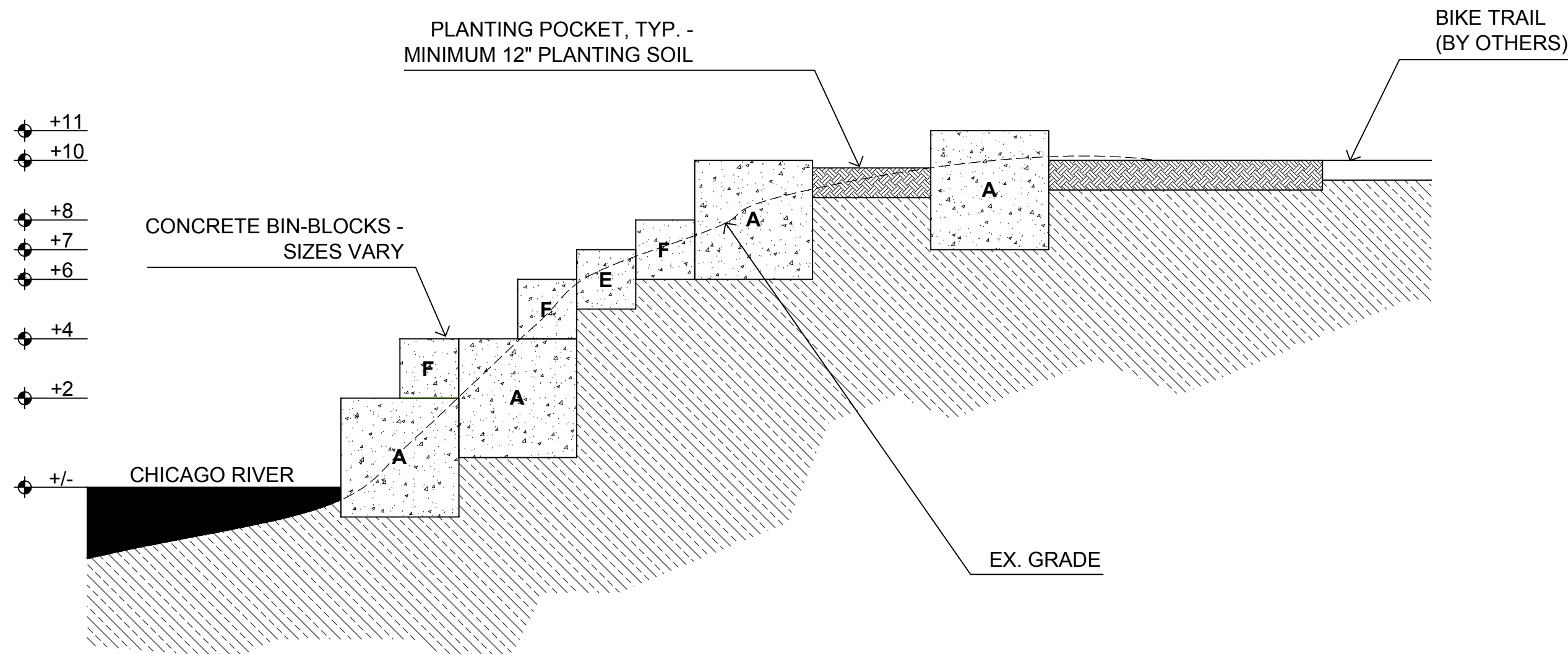
NOTES:

1. SEE ENLARGEMENT PLAN FOR CONCRETE UNIT DIMENSIONS.

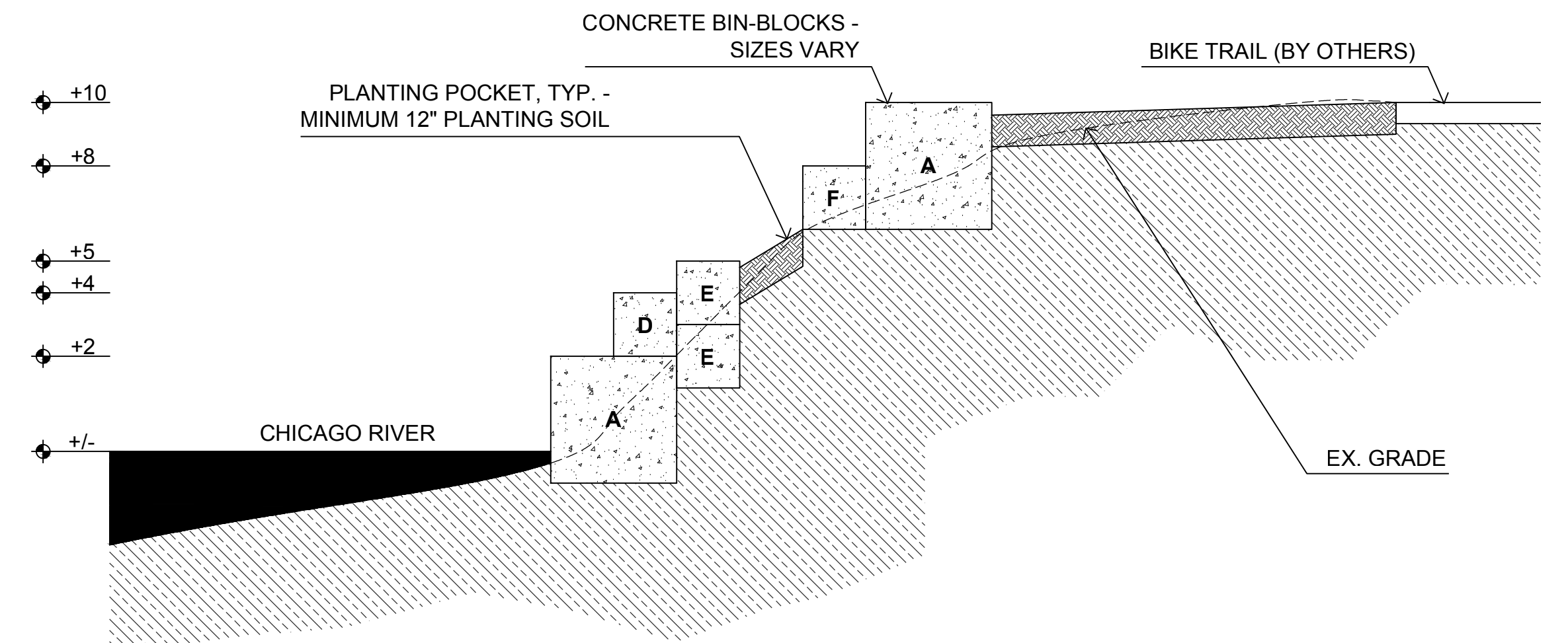
2. CONCRETE BIN-BLOCKS SHALL BE CUSTOM FORMED AND FILLED WITH "RETURN" CONCRETE. ALL 4 SIDES TO BE FLAT, ROUGH-FINISHED AND STRAIGHT. BLOCKS SHALL BE FREE OF TYPICAL MALE EXTRUSIONS & FEMALE INDENTS. REINFORCEMENT AND HOOK PLACEMENT PER MANUFACTURER REQUIREMENTS.



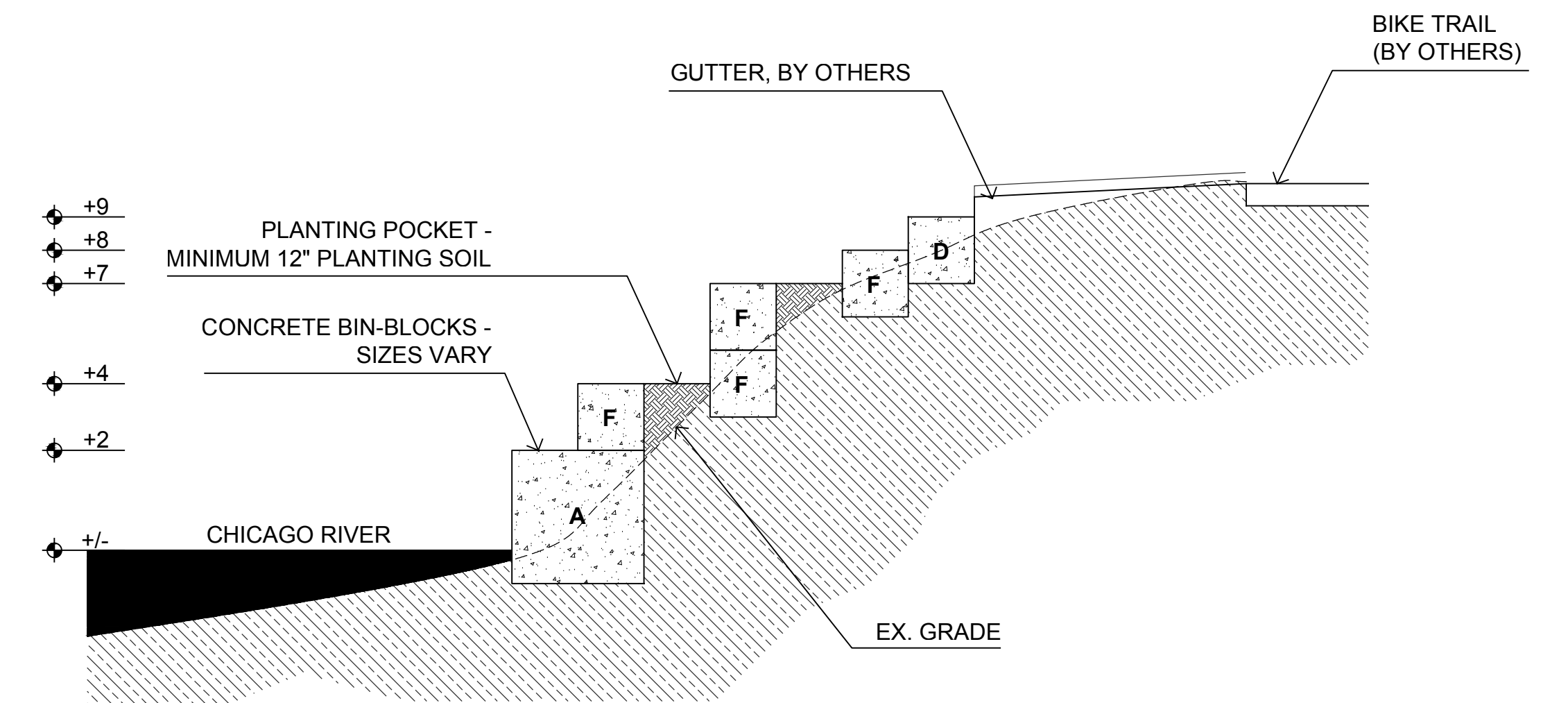
E SECTION E – CONCRETE-BIN BLOCK TERRACE (DESIGN INTENT FOR WALL LAYOUT ONLY)
SCALE: 1/4" = 1'-0"



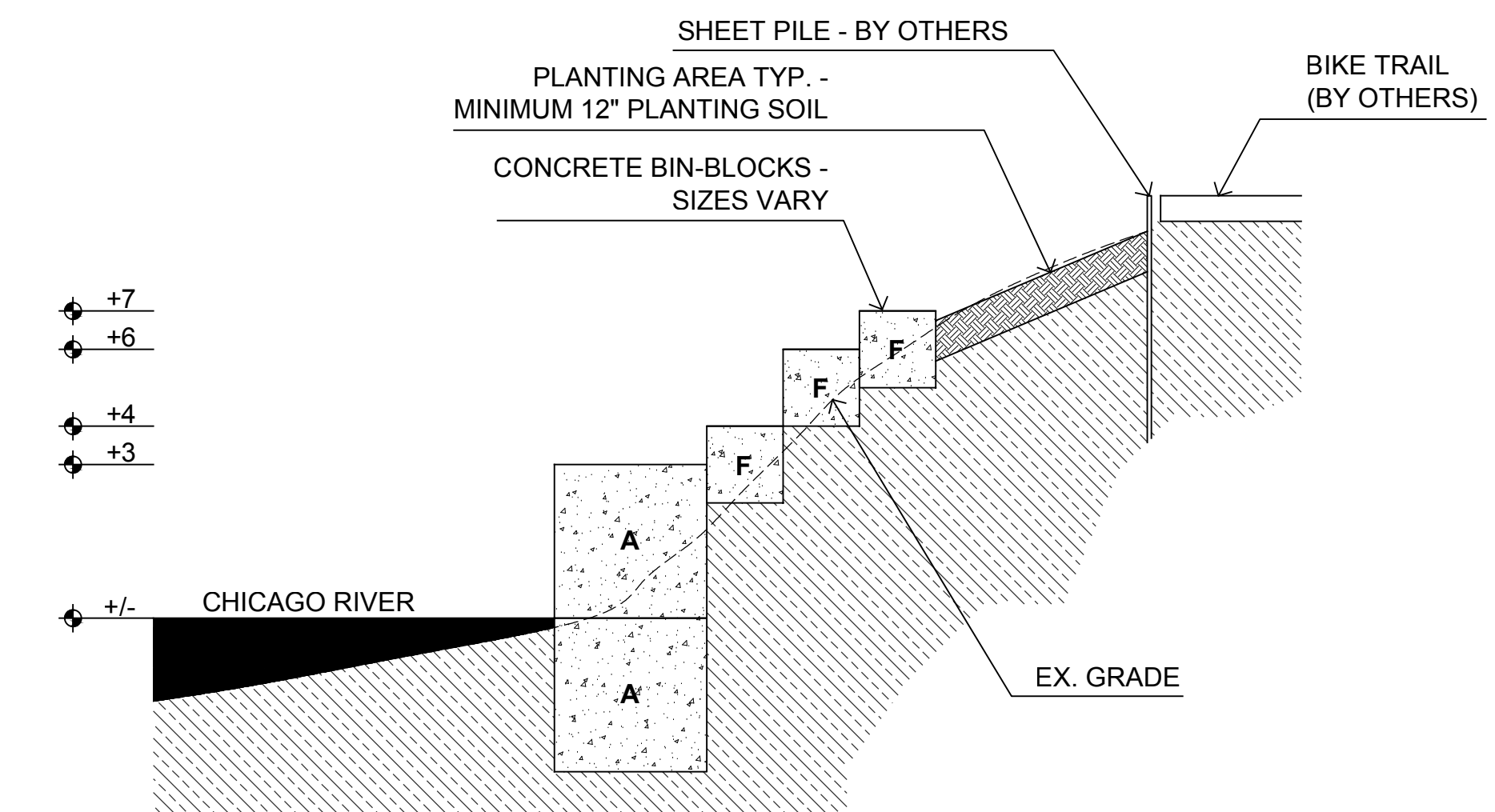
D SECTION D – CONCRETE-BIN BLOCK TERRACE (DESIGN INTENT FOR WALL LAYOUT ONLY)
SCALE: 1/4" = 1'-0"



C SECTION C – CONCRETE-BIN BLOCK TERRACE (DESIGN INTENT FOR WALL LAYOUT ONLY)
SCALE: 1/4" = 1'-0"



B SECTION B – CONCRETE-BIN BLOCK TERRACE (DESIGN INTENT FOR WALL LAYOUT ONLY)
SCALE: 1/4" = 1'-0"



A SECTION A – CONCRETE-BIN BLOCK TERRACE (DESIGN INTENT FOR WALL LAYOUT ONLY)
SCALE: 1/4" = 1'-0"

January 24, 2016 2:30:46 PM - CDOT - Addison Underbridge Connection - \DISCIPLINE\CA\Working\Sheets\046-047_Terrace Wall Detail.dwg

SECTION NO.: 12-E9121-00-BT
DRAWN BY: JRK
CHECKED BY: JRK
APPROVED BY: GAO
DATE: 8/12/2016
SCALE: -----

NO.	DATE	REVISIONS/ISSUANCES
1	8/12/16	ISSUED FOR FINAL
2	5/6/16	ISSUED FOR FINAL
3	6/05/15	ISSUED FOR FINAL REVIEW

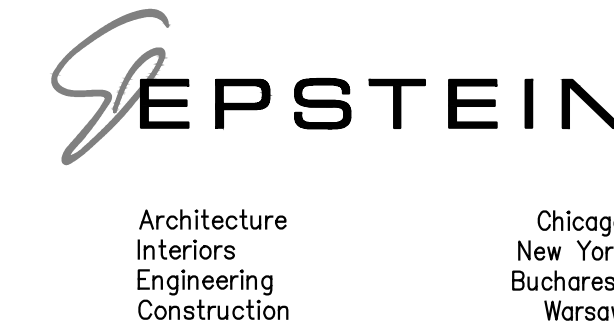
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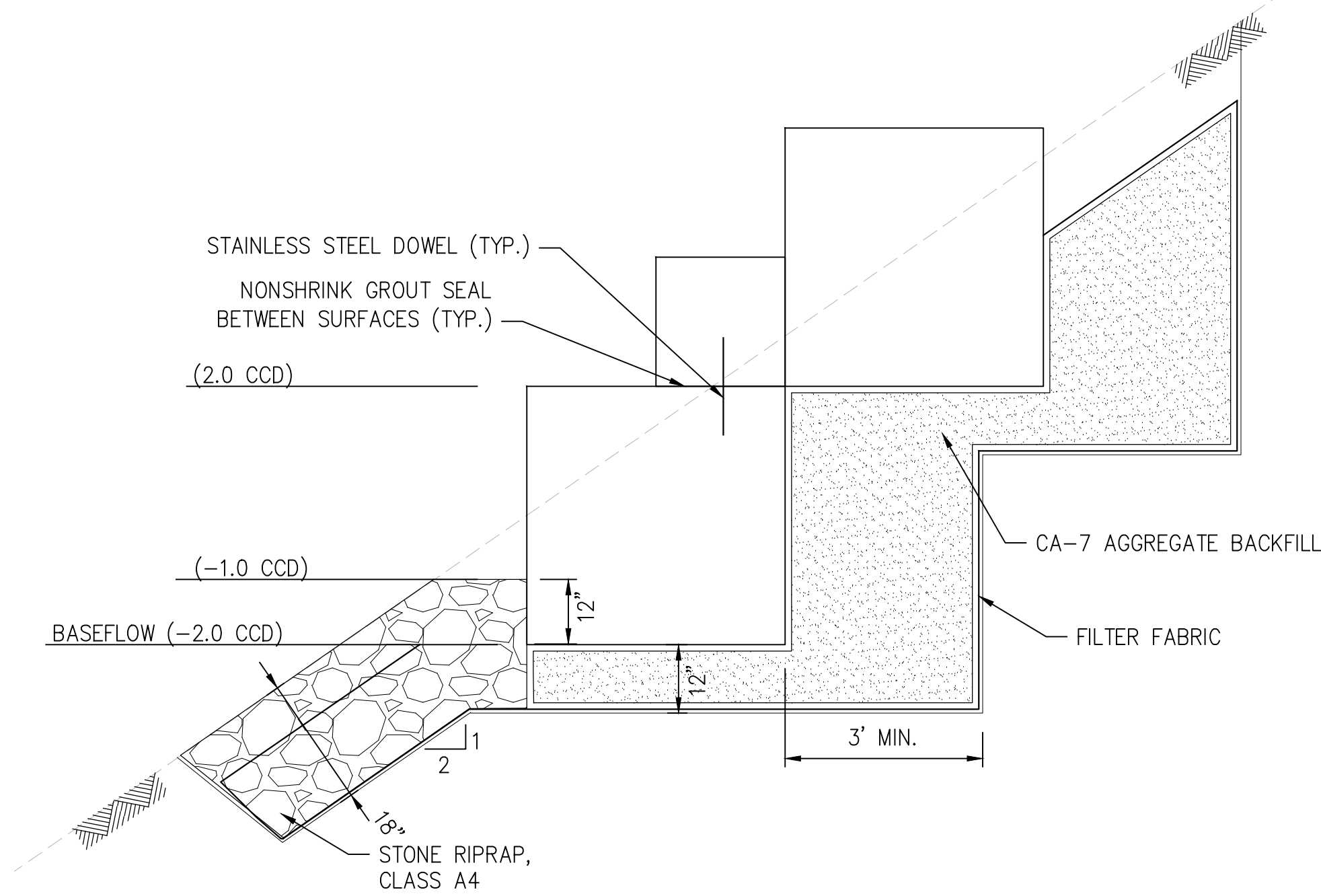
ADDISON UNDERBRIDGE TRAIL CONNECTION FROM CLARK PARK TO CALIFORNIA PARK

TERRACE WALL SECTIONS

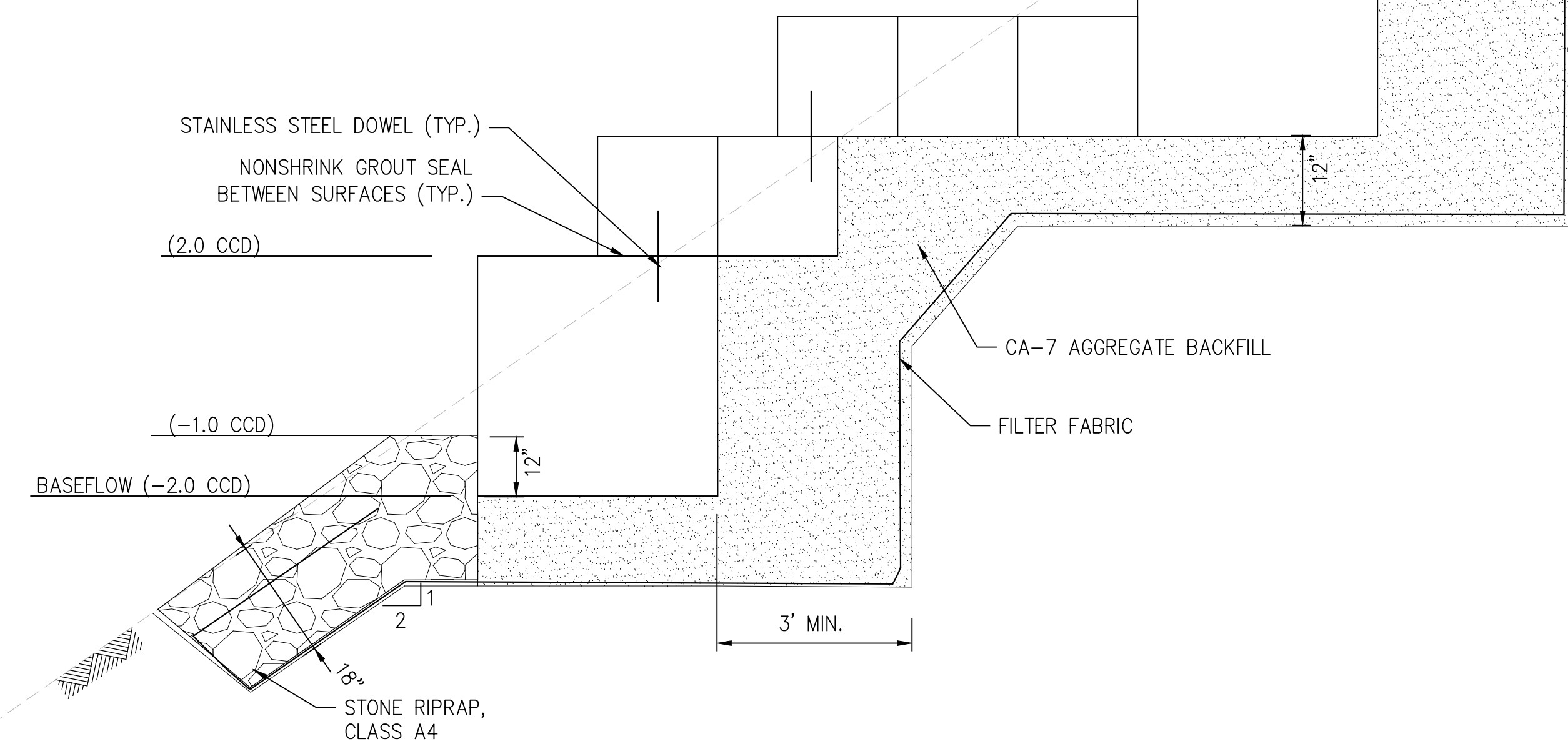
SHEET NO.
L-8

SHEET 46 OF 158

REVISION



SECTION DETAILS 1



SECTION DETAILS 2

GENERAL NOTES

1. THE FILTER FABRIC SHALL MEET THE REQUIREMENTS OF SECTION 282 OF THE IDOT STANDARD SPECIFICATIONS.
2. THE ROCK RIPRAP SHALL MEET THE IDOT REQUIREMENT FOR THE FOLLOWING GRADATION: CLASS A4. THE RIPRAP SHALL BE PLACED ACCORDING SECTION 281 OF THE IDOT STANDARD SPECIFICATIONS. THE ROCK MAY BE EQUIPMENT PLACED.
3. WHEN CONDITIONS ARE ENCOUNTERED WHERE THE EXCAVATION FOR THE WORK CANNOT BE KEPT FREE OF WATER FOR PROSECUTING THE WORK BY PUMPING AND/OR DIVERTING WATER BY THE USE OF SHEETING OR DIKES, THE CONTRACTOR, WITH THE WRITTEN PERMISSION OF THE ENGINEER, WILL BE PERMITTED TO CONSTRUCT A COFFERDAM. THE COFFERDAMS SHALL BE DESIGNED, CONSTRUCTED, AND REMOVED WITH THE ENGINEER'S APPROVAL. COFFERDAMS SHALL CONSIST OF ENGINEERED STRUCTURAL COMPONENTS.
4. THE CONTRACTOR SHALL SUBMIT DRAWINGS AND DESIGN CALCULATIONS SHOWING THE PROPOSED DESIGN, METHOD OF CONSTRUCTION, REMOVAL, AS WELL AS OTHER DETAILS LEFT OPEN TO CHOICE. THE COST OF THE COFFERDAM, IF REQUIRED, WILL BE NOT BE PAID FOR SEPARATELY BUT SHALL BE INCLUDED IN THE UNIT PRICE BID FOR THE ITEM OF CONSTRUCTION INVOLVED.
5. DOWEL BARS SHALL BE NO. 4 SPACED 18" ON CENTERS, MINIMUM TWO DOWELS PER BLOCK.

January 24, 2016 12:00:00 20346 - CDOT - Addison Underbridge Connection - VISORLINE (Civil Working) Sheets 046-047_Terrace Wall Detail.dwg

SECTION NO.: 12-E9121-00-BT
 DRAWN BY: JRK
 CHECKED BY: JRK
 APPROVED BY: GAO
 DATE: 8/12/2016
 SCALE: N.T.S.

NO.	DATE	REVISIONS/ISSUANCES
1	8/12/16	ISSUED FOR FINAL
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3	6/05/15	ISSUED FOR FINAL REVIEW

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ADDISON UNDERBRIDGE TRAIL CONNECTION FROM
 CLARK PARK TO CALIFORNIA PARK

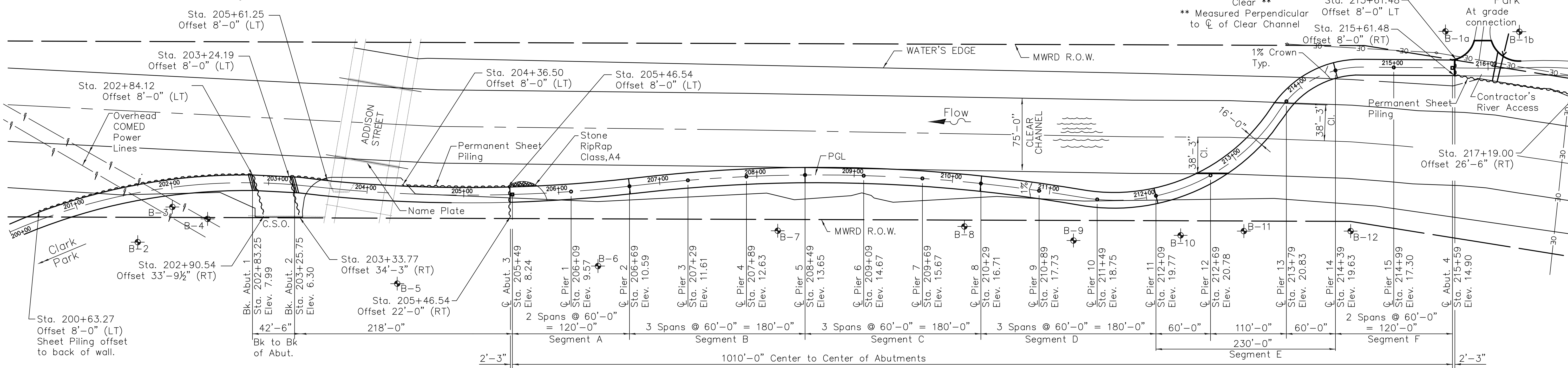
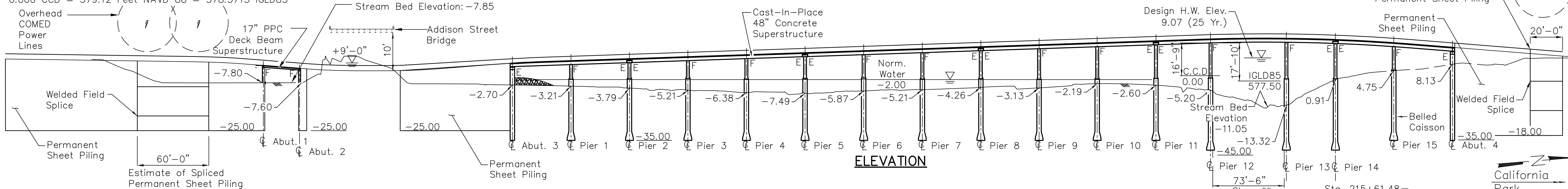
TERRACE WALL DETAILS

SHEET NO.
L-9

SHEET 47 OF 158

REVISION

Bench Mark: Chicago Benchmark #5739 ($\pm 21'$ N. of the N Line of Addison Street and $\pm 95'$ W. of the W Line of Artesian Ave.
 Mark cut at the SWX of Stone Doorsill on the S. Side of 2440 W. Addison) ELEV = 14.548
 New Structures: SN 016-P025 is a single span 31'-0" clear on spread footing abutments 16'-0" out to out of deck with a 16 span reinforced concrete modified Tee Beam superstructure on single column/caisson piers 1014'-6" edge of deck to edge of deck and 16'-0" out to out of deck.
 0.000 CCD = 579.12 Feet NAVD 88 = 578.5715 IGLD85



DESIGN SCOUR ELEVATION TABLE

Design Scour Elevation (ft.)																		
Abut. 1	Abut. 2	Abut. 3	Pier 1	Pier 2	Pier 3	Pier 4	Pier 5	Pier 6	Pier 7	Pier 8	Pier 9	Pier 10	Pier 11	Pier 12	Pier 13	Pier 14	Pier 15	Abut. 4
-18.2	-18.1	-16.7	-13.6	-13.7	-13.3	-14.0	-14.4	-13.8	-13.9	-13.5	-14.7	-14.1	-13.5	-13.2	-17.8	-11.3	-15.8	-11.7

DESIGN STRESSES

FIELD UNITS
 f'_c = 6,000 psi
 f_y = 60,000 psi (Reinforcement)
 f_y = 50,000 psi (AASHTO M270 Gr. 50) (Sheet Piling)
 f_y = 50,000 psi (AASHTO M270 Gr. 50W) (Railing)

PRECAST PRESTRESSED UNITS
 f'_c = 6,000 psi
 f'_{ci} = 5,000 psi
 f_{pu} = 270 ksi (low lax. $\frac{1}{2}$ " dia. strands)
 f_{pbt} = 201.96 ksi (low lax. $\frac{1}{2}$ " dia. strands)

DESIGN SPECIFICATIONS

2012 AASHTO LRFD Bridge Design Specifications with 2013 Interims
 2009 AASHTO LRFD Guide Specifications for the Design of Pedestrian Bridges

LOADING
 Pedestrian Loading (PL) = 90 psf (Bike + Pedestrian)
 Traffic Loading: H-10 (Not in concurrences with PL)
 Impact: Not Applicable
 Allowance for Future Wearing Surface = 50 psf

SEISMIC DATA

Soil Site Class: = C
 LRFD Seismic Performance Zone (SPZ) = 1
 Design Spectral Acceleration at 1.0 sec. (S_{D1}) = 0.060g
 Design Spectral Acceleration at 0.2 sec. (S_{D5}) = 0.104g

LIN ENGINEERING, LTD.
 VINCENT P. TABOR, S.E.
 081-007047

REVIEWED AND APPROVED FOR STRUCTURAL ADEQUACY

Vincent P. Tabor

EPSTEIN
 RAVEESH VARMA, S.E.
 081-006790

SIGNATURE AND SEAL

Raveesh Varma

I certify that to the best of knowledge, information and belief, this bridge design is structurally adequate for the design loading shown on the plans. The design is an economical one for the style of structure and complies with requirements of the current AASHTO Standard Specifications for Highway Bridges.

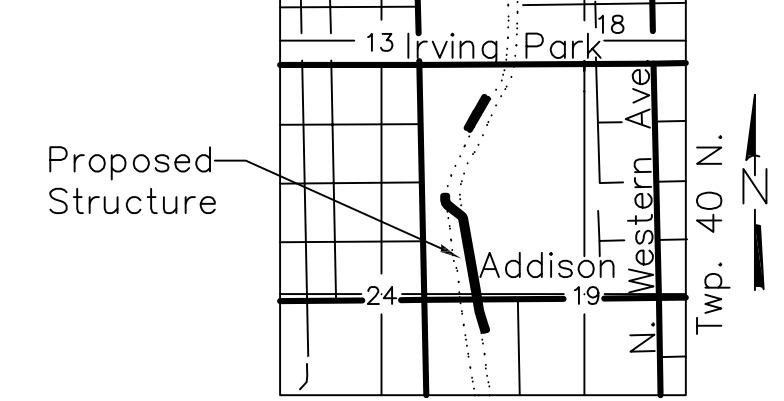
WATERWAY INFORMATION

Drainage Area = 141.6 sq. mi. Low Grade Elev. 16.75 @ Sta. 212+70.5

Flood Yr.	Freq.	Q C.F.S.	Opening Sq. Ft.		Head - Ft.		Headwater El		
			Exist.	Prop.	H.W.E.	Exist.	Prop.	Exist.	Prop.
Design	25	5977	2147.6	1951.8	9.07	0	0	9.07	9.07
Base	100	6901	2381.6	2232.5	10.67	0	.1	10.67	10.77
Overtopping	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A
Max. Calc.	500	8097	2575.1	2479.4	12.07	0	0	12.07	12.07

ALL ELEVATIONS PROVIDED ARE FROM CHICAGO CITY DATUM (CCD) UNLESS NOTED OTHERWISE.

Range 13E-14E - 3 PM



GENERAL PLAN
ADDISON UNDERBRIDGE OVER NORTH BRANCH CHICAGO RIVER SECTION 12-E-9121-00-BT COOK COUNTY

LEGEND

- Soil Borings
- OverHead Electrical Utilities

SECTION NO.: 12-E9121-00-BT
 DRAWN BY: P. DOMBROWSKI
 CHECKED BY: R. VARMA
 APPROVED BY: P. DOMBROWSKI
 DATE: 8/12/2016
 SCALE: N.T.S.

NO. DATE REVISIONS/ISSUANCES

8/12/16 ISSUED FOR FINAL

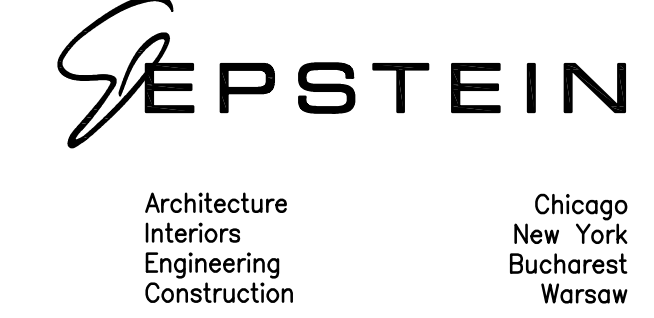
5/6/16 ISSUED FOR FINAL

6/05/15 ISSUED FOR FINAL REVIEW

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ADDISON UNDERBRIDGE TRAIL CONNECTION FROM CLARK PARK TO CALIFORNIA PARK STRUCTURE NO. 016-P025

GENERAL PLAN AND ELEVATION

SHEET NO. S-01
 SHEET 48 OF 158
 REVISION

January 28, 2014 2:00 PM 2014-08-16 10:00 AM - CDOT - Addison Underbridge Connection - \USCP\LINE\CH\Working\Sheet\08 - STRUCTURAL\016P025-20346-002-General_Data.dwg

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47	S-02	SHEET INDEX, GENERAL NOTES & SUMMARY OF QUANTITIES
48	S-03	PROFILE GRADE LINE AND ALIGNMENT
49	S-04	TYPICAL 60 FT SPAN FALSEWORK
50	S-05	110 FT SPAN FALSEWORK
51	S-06	FALSEWORK DETAILS I
52	S-07	FALSEWORK DETAILS II
53	S-08	FALSEWORK DETAILS III
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55	S-10	COMBINED SEWER OUTFALL BRIDGE DETAILS II
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57	S-12	SUPERSTRUCTURE CROSS SECTIONS - SEGMENT A
58	S-13	SUPERSTRUCTURE PLAN AND ELEVATION - SEGMENT B
59	S-14	SUPERSTRUCTURE CROSS SECTIONS - SEGMENT B
60	S-15	SUPERSTRUCTURE PLAN AND ELEVATION - SEGMENT C
61	S-16	SUPERSTRUCTURE CROSS SECTIONS - SEGMENT C
62	S-17	SUPERSTRUCTURE PLAN AND ELEVATION - SEGMENT D
63	S-18	SUPERSTRUCTURE CROSS SECTIONS - SEGMENT D
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66	S-21	SUPERSTRUCTURE PLAN AND ELEVATION - SEGMENT F
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93	S-48	EXPANSION JOINT DETAILS
94	S-49	BEARING DETAILS

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105	S-60	DOUBLE EXPANSION PIERS 8 AND 11
106	S-61	DOUBLE EXPANSIONS PIER 14
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109	S-64	PERMANENT SHEET PILE WALL 3
110	S-65	PERMANENT SHEET PILE WALL DETAILS I
111	S-66	PERMANENT SHEET PILE WALL DETAILS II
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114 thru 125	S-69 thru S-80	SOIL BORING LOGS
126 thru 132	S-81 thru S-87	EXISTING DRAWINGS FOR INFORMATION ONLY

TOTAL BILL OF MATERIAL				
DESCRIPTION	UNIT	SUPERSTRUCTURE	SUBSTRUCTURE	TOTAL
REMOVAL OF EXISTING STRUCTURES	L SUM	1	-	1
HIGH STRENGTH HIGH PERFORMANCE CONCRETE STRUCTURES	CU YD	-	111.6	111.6
HIGH STRENGTH HIGH PERFORMANCE CONCRETE SUPERSTRUCTURES	CU YD	845.3	-	845.3
PPC DECK BEAMS, 17" DEPTH	SQ FT	680	-	680
REINFORCEMENT BARS, EPOXY COATED	POUND	301,340	97,630	398,970
MECHANICAL SPLICERS	EACH	2,057	319	2,376
NAME PLATES	EACH	1	-	1
PERMANENT CASING	FOOT	-	503	503
DRILLED SHAFT IN SOIL	CU YD	-	469.5	469.5
PREFORMED JOINT SEAL, 4"	FOOT	112	-	112
ELASTOMERIC BEARING ASSEMBLY, TYPE I	EACH	24	-	24
PROTECTIVE CONCRETE SEALER	SQ YD	2,221	75	2,296
GEOFOAM (POLYSTYRENE FILL)	CU YD	132	-	132
ANTI-GRAFFITI PROTECTION SYSTEM	SQ FT	-	900	900
CROSSHOLE SONIC LOGGING	EACH	-	6	6
CONCRETE WEARING SURFACE, 6"	SQ YD	76	-	76
BICYCLE RAILING, SPECIAL	FOOT	3,008	-	3,008
PERMANENT SHEET PILING	SQ FT	-	30,584	30,584
DRAINAGE SYSTEM	L. SUM	-	1	1
PIPE UNDERDRAINS FOR STRUCTURES 4"	FOOT	-	571	571
TEMPORARY SUPPORT SYSTEM	L. SUM	1	-	1
PERMANENT GROUND ANCHORS	EACH	0	47	47

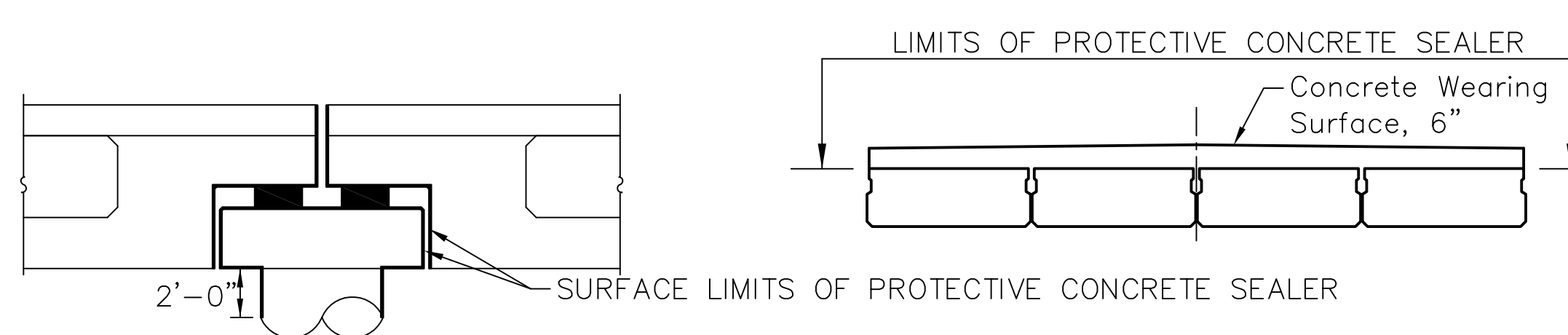
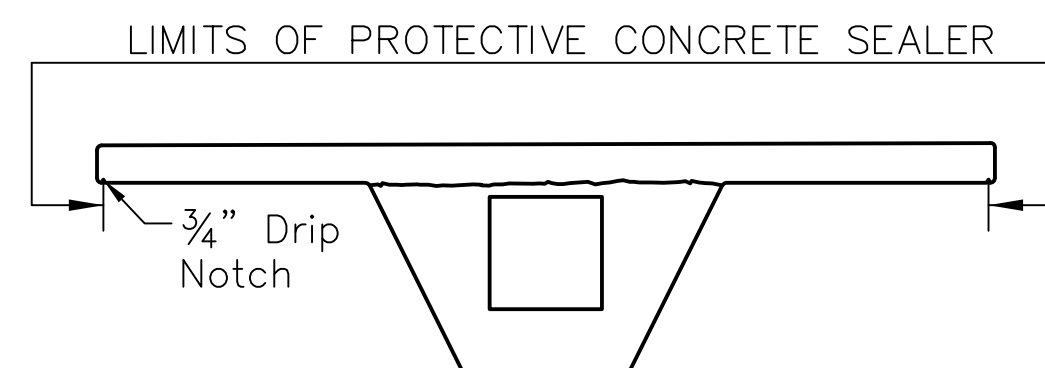
GENERAL NOTES

- No field welding is permitted except as specified in the contract documents.
- Reinforcement bars designated (E) shall be epoxy coated.
- Protective Concrete Sealer shall be applied to the designated areas of the Abutments, Expansion Piers, and Fixed Piers.
- Layout of the slope protection system may be varied in the field to suit ground conditions as directed by the Engineer.
- The Contractor shall obtain a construction permit from the Illinois Department of Natural Resources (IDNR), Office of Water Resources for any temporary construction activity placed in the water except cofferdams. This shall include the placement of material for run-arounds, causeways, etc. Any permit application by the Contractor shall refer to the IDNR 3704 Floodway Construction permit number allowing permanent construction as shown in the contract plans.
- The Contractor shall make allowance for the deflection of forms, shrinkage and settlement of falsework, in addition to allowance for dead load deflection. Forms for deck slab shall be removed prior to placement of bridge approach slab.
- The concrete for bridge decks finished according to Article 503.16(a) of the Standard Specifications shall be placed and compacted parallel to the skew in uniform increments along the centerline of bridge. The machine used for finishing shall be set parallel to the skew for striking off and screeding concrete.
- When the deck pour is stopped for the day at one or more of the transverse bonded construction joints in the deck pouring sequence as shown, the next pour shall not be made until both of the following are met:
At least 72 hours shall have elapsed from the end of the previous pour.

- The structural steel plates of the Bearing Assembly shall conform to the requirements of AASHTO M 270 Grade 50.
- Two 1/8 in. adjusting shims shall be provided for each bearing in addition to all other plates or shims and placed as shown on bearing details.
- All (embedded and separate) bearing plates, side retainers, anchor bolts, nuts, washers and pintles shall be galvanized according to AASHTO M111 or M232 as applicable.
- Backfill shall be placed behind the abutment after the superstructure has been poured and falsework removed. See Article 502.10 of the Standard Specifications.
- If a portion of the drilled shaft web walls or concrete encasement is under water, reinforcement may be placed underwater into forms. Concrete shall be tremied according to Article 503.08 of the Standard Specifications to an elevation of 1'-0" above the water line at the time of construction.
- Anti-Graffiti Coating shall be applied to exposed surfaces of Addison Street East Pier and Abutment.

SCOPE OF WORK

- Construct Permanent Steel Sheet Piling walls at designated locations of the path along the river bank.
 - Construct a multispan bridge approximately 1014'-6" long along and across the north branch of the Chicago River.
 - Construct a 42'-6" long single span bridge over combined sewer outfall (CSO).
- Both bridges will carry a shared pedestrian and bicycle trail.



STA. 204+00.00
BUILT 20__ BY
CITY OF CHICAGO
ADDISON UNDERBRIDGE OVER
NORTH BRANCH CHICAGO RIVER
SECTION 12-E-9121-00-BT
LOADING H-10

NAME PLATE

See IDOT Std. 515001

SECTION NO.: 12-E9121-00-BT
DRAWN BY: N. FIRNBACH
CHECKED BY: R. VARMA
APPROVED BY: P. DOMBROWSKI
DATE: 8/12/2016
SCALE: N.T.S.

NO.	DATE	REVISIONS/ISSUANCES
1	8/12/16	ISSUED FOR FINAL
2	5/6/16	ISSUED FOR FINAL
3	6/05/15	ISSUED FOR FINAL REVIEW

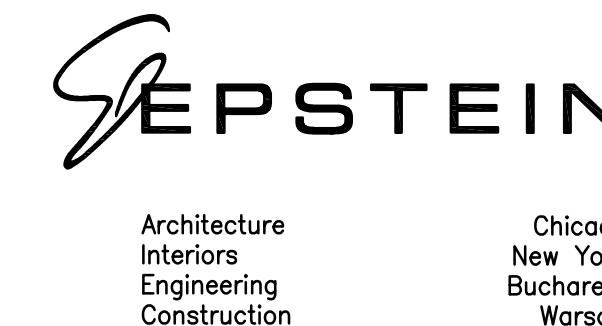
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Chicago, IL 60602
312.744.3600



CONSULTANT

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ADDISON UNDERBRIDGE TRAIL CONNECTION FROM
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STRUCTURE NO. 016-P025

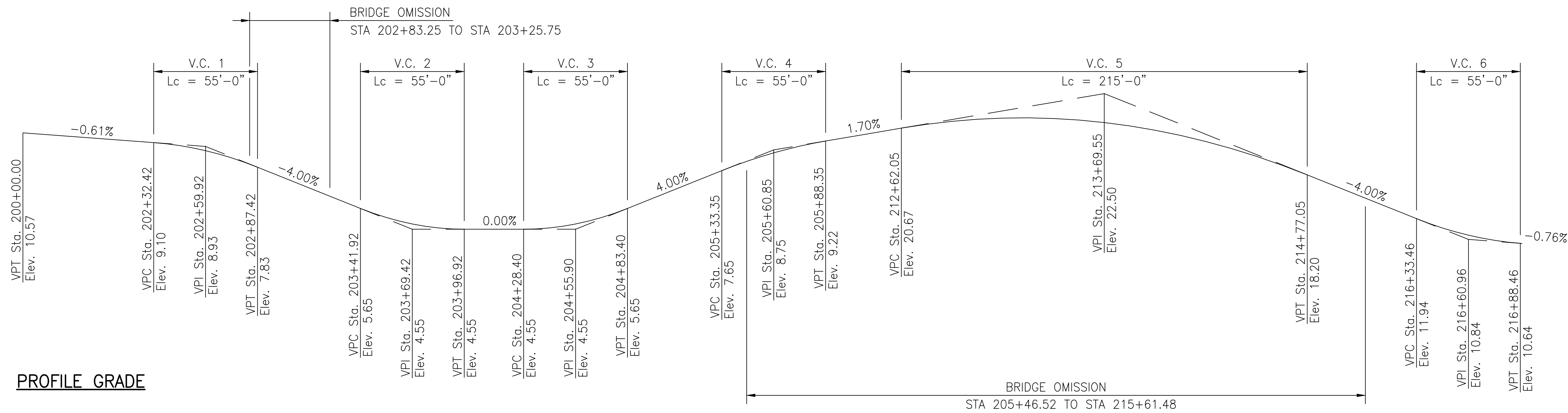
SHEET INDEX, GENERAL NOTES & SUMMARY OF QUANTITIES

SHEET NO.
S-02

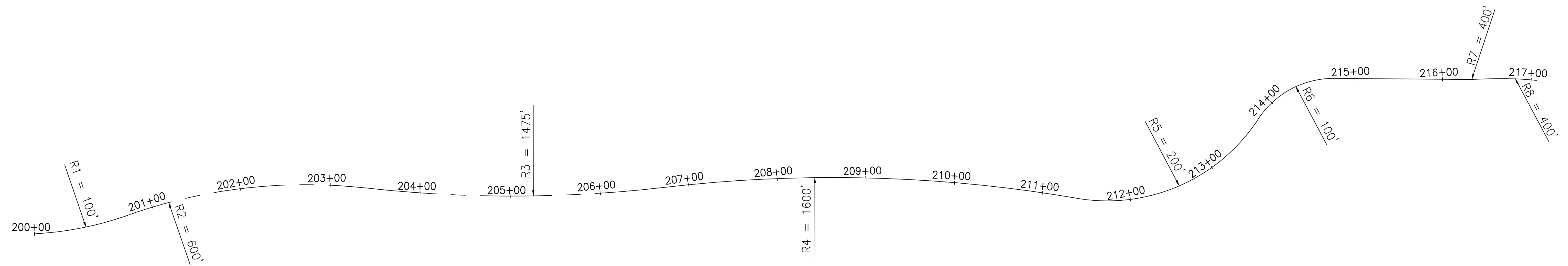
SHEET 49 OF 158

REVISION

January 28, 2014 2:00 PM 2014-01-28 10:00:00 - CDOT - Addison Underbridge Connection - \DISCIPLINE\Civil\Working\Drawings\08_STRUCTURE\016P025-2346-003-PGL.dwg



PROFILE GRADE



HORIZONTAL CURVE DATA

PR CURVE 1	PR CURVE 2	PR CURVE 3	PR CURVE 4	PR CURVE 5	PR CURVE 6	PR CURVE 7	PR CURVE 8
PI STA = 200+29.75	PI STA = 202+13.90	PI STA = 205+09.33	PI STA = 209+06.28	PI STA = 212+65.02	PI STA = 214+20.95	PI STA = 216+24.43	PI STA = 216+57.10
$\Delta = 10^{\circ}02'12''$ (RT)	$\Delta = 24^{\circ}43'16''$ (RT)	$\Delta = 12^{\circ}59'53.16''$ (LT)	$\Delta = 16^{\circ}23'10.32''$ (RT)	$\Delta = 66^{\circ}39'16.20''$ (LT)	$\Delta = 57^{\circ}25'23.52''$ (RT)	$\Delta = 5^{\circ}46'26''$ (LT)	$\Delta = 11^{\circ}44'54''$ (RT)
R1=100.00'	D=9'32'57.48"	D=3'53'4.05"	D=3'34'51.55"	D=28'38'52.43"	D=57'17'44.87"	D=23'52'23.31"	D=28'38'52.43"
T=8.78'	R2=600.00'	R3=1475.00'	R4=1600.00'	R5=200.00'	R6=100.00'	R7=240.00'	R8=200.00'
L=17.52'	T=131.49'	T=168.03'	T=230.37'	T=131.51'	T=54.77'	T=12.11'	T=20.58'
E=0.38'	L=258.88'	L=334.62'	L=457.59'	L=232.67'	L=100.22'	L=22.20'	L=41.01'
e= N/A	E=14.24'	E=9.54'	E=16.50'	E=39.36'	E=14.02'	E=0.31'	E=1.06'
T.R.=N/A	e= N/A	e=N/A	e=N/A	e=N/A	e=N/A	e=N/A	e=N/A
S.E. RUN=N/A	T.R.=N/A	T.R.=N/A	T.R.=N/A	T.R.=N/A	T.R.=N/A	T.R.=N/A	T.R.=N/A
PC STA =200+20.96	S.E. RUN=N/A	S.E. RUN=N/A	S.E. RUN=N/A	S.E. RUN=N/A	S.E. RUN=N/A	S.E. RUN=N/A	S.E. RUN=N/A
PT STA =200+38.48	PC STA =200+82.42	PC STA =203+41.30	PC STA =206+75.92	PC STA =211+33.51	PC STA =213+66.17	PC STA =216+12.32	PC STA =216+36.52
	PT STA =203+41.30	PT STA =206+75.92	PT STA =211+33.51	PT STA =213+66.17	PT STA =214+66.40	PT STA =216+36.52	PT STA =216+77.53

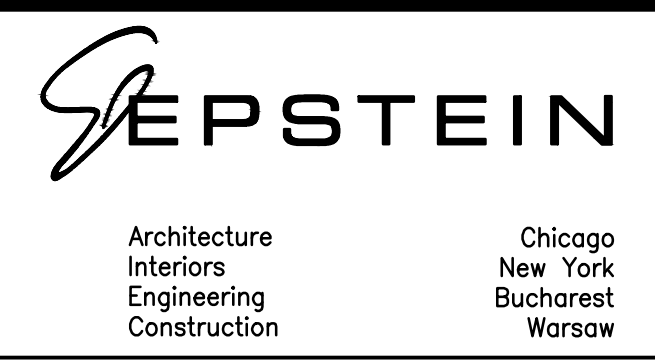
SECTION NO.: 12-E9121-00-BT
 DRAWN BY: N. FIRNBACH
 CHECKED BY: R. VARMA
 APPROVED BY: P. DOMBROWSKI
 DATE: 8/12/2016
 SCALE: N.T.S.

NO.	DATE	REVISIONS/ISSUANCES
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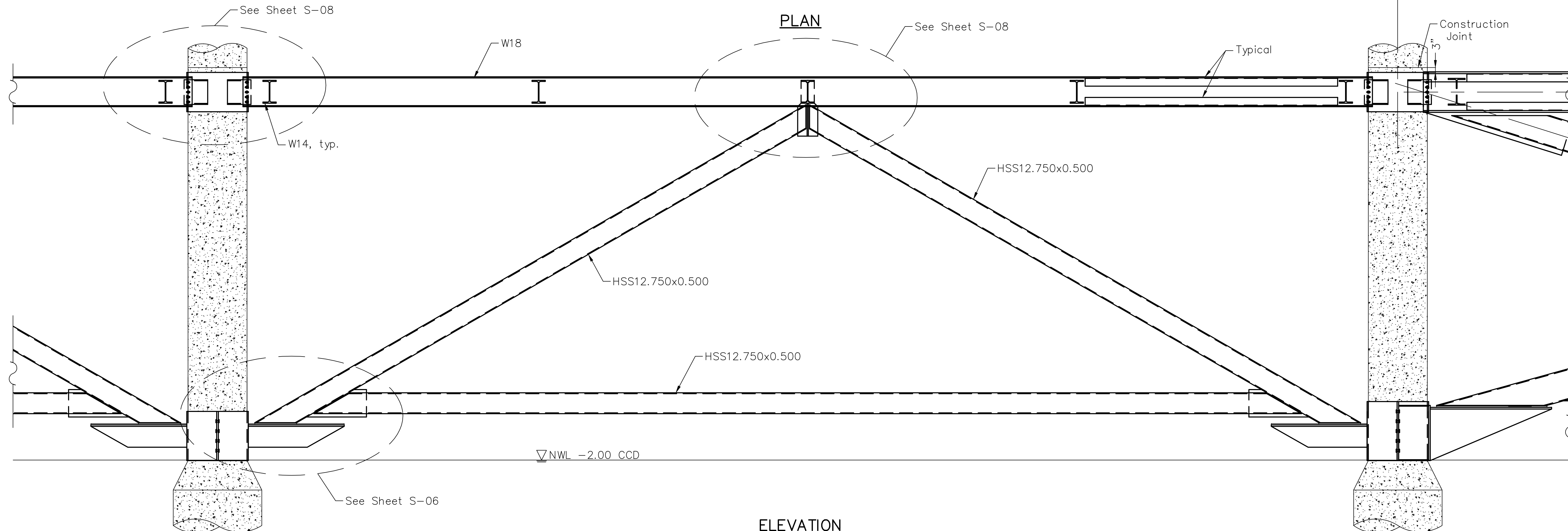
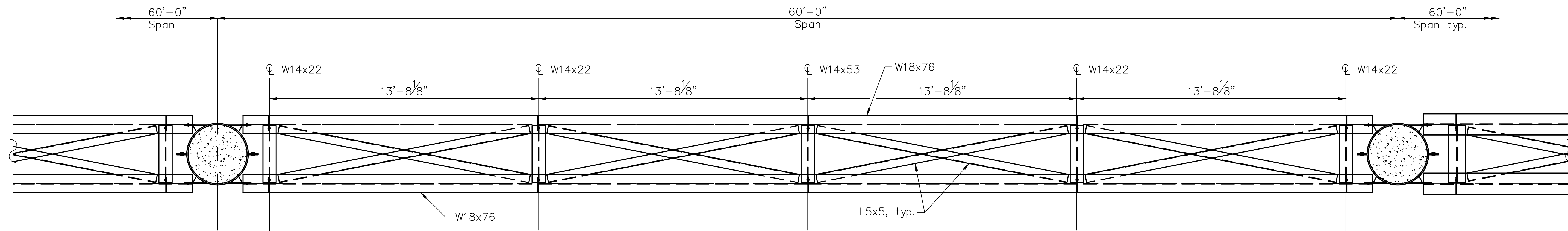
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ADDISON UNDERBRIDGE TRAIL CONNECTION FROM
 CLARK PARK TO CALIFORNIA PARK
 STRUCTURE NO. 016-P025

PROFILE GRADE LINE AND ALIGNMENT

SHEET NO. S-03
 SHEET 50 OF 158
 REVISION



ELEVATION

60' Span Deflections Due to Concrete Self Weight at:

Segment	Span	0.2L	0.4L	0.6L	0.8L
A	1	0.153"	0.219"	0.192"	0.101"
A	2	0.081"	0.183"	0.234"	0.152"
B	3	0.198"	0.295"	0.256"	0.130"
B	4	0.012"	0.037"	0.037"	0.011"
B	5	0.132"	0.258"	0.296"	0.199"
C	6	0.175"	0.255"	0.226"	0.118"
C	7	0.019"	0.046"	0.044"	0.016"
C	8	0.133"	0.259"	0.297"	0.200"

60' Span Deflections Due to Concrete Self Weight at:

Segment	Span	0.2L	0.4L	0.6L	0.8L
D	9	0.200"	0.297"	0.259"	0.134"
D	10	0.016"	0.043"	0.043"	0.016"
D	11	0.149"	0.279"	0.301"	0.186"
E	12	0.053"	0.030"	0.000"	0.000"
E	14	0.000"	0.000"	0.000"	0.013"
F	15	0.221"	0.297"	0.239"	0.084"
F	16	0.115"	0.243"	0.285"	0.194"

Segment E, Span 13 on Sheet S-05

BILL OF MATERIAL

Item	Units	Quantity
Temporary Support System (Falsework)	L. Sum	1

Notes:

- Temporary Falsework provided is one feasible construction method for estimation of bidding as required by AASHTO Article 2.5.3. Contractor is responsible for suitable method of construction and temporary falseworks.
- Design loads for falsework (AASHTO C3.4.2.1):
 Concrete self-weight = 3333 plf
 Formwork allowance = 240 plf
 Construction live load = 320 plf
- Concept drawing - NOT FOR CONSTRUCTION.

January 2016: \Bids\2000\20346 - CDOT - Addison Underbridge Connection - \DISCIPLINE\Civil\Working\Sheets\08 - STRUCTURAL\01\EP025-20346-004-008-Falsework.dwg

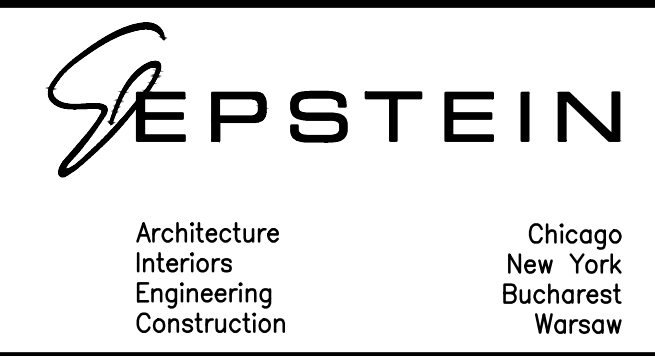
SECTION NO.: 12-E9121-00-BT
 DRAWN BY: K. KOZYS
 CHECKED BY: R. VARMA
 APPROVED BY: P. DOMBROWSKI
 DATE: 8/12/2016
 SCALE: N.T.S.

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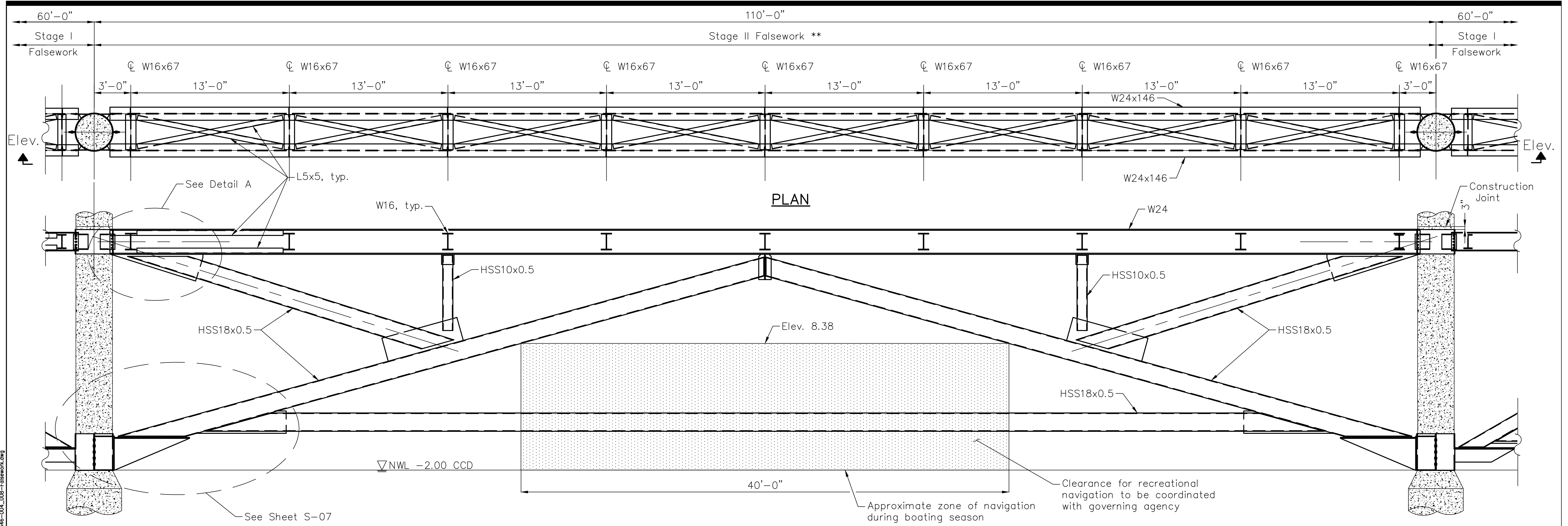
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ADDISON UNDERBRIDGE TRAIL CONNECTION FROM
 CLARK PARK TO CALIFORNIA PARK
 STRUCTURE NO. 016-P025

60 FT SPAN FALSEWORK

SHEET NO. S-04
 SHEET 51 OF 158
 REVISION



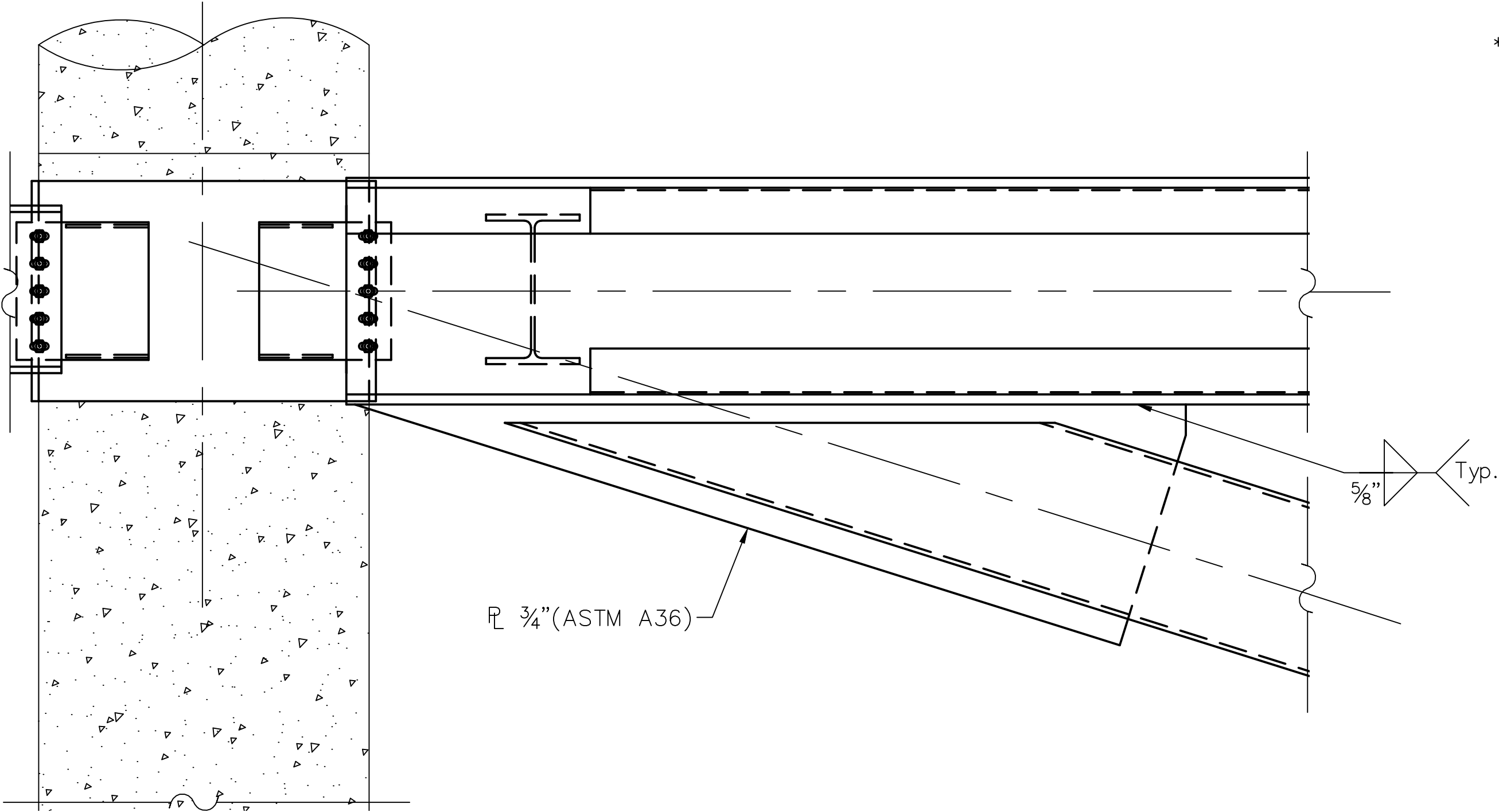
PLAN

ELEVATION

** Pour Stage I prior to 110'-0" span.

110' Span Deflections Due to Concrete Self Weight at:											
Segment	Span	L/11	2L/11	3L/11	4L/11	5L/11	6L/11	7L/11	8L/11	9L/11	10L/11
E	13	0.371"	0.834"	1.298"	1.666"	1.869"	1.873"	1.676"	1.300"	0.825"	0.382"

- Notes:
1. Temporary Falsework provided is one feasible construction method for estimation of bidding as required by AASHTO Article 2.5.3. Contractor is responsible for suitable method of construction and temporary falseworks.
 2. Design loads for falsework (AASHTO C3.4.2.1):
 Concrete self-weight = 3333 plf
 Formwork allowance = 240 plf
 Construction live load = 320 plf
 3. Concept drawing - NOT FOR CONSTRUCTION.



DETAIL A

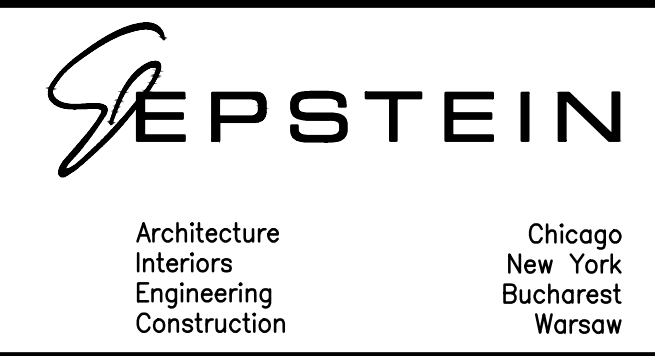
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 DRAWN BY: K. KOZYS
 CHECKED BY: R. VARMA
 APPROVED BY: P. DOMBROWSKI
 DATE: 8/12/2016
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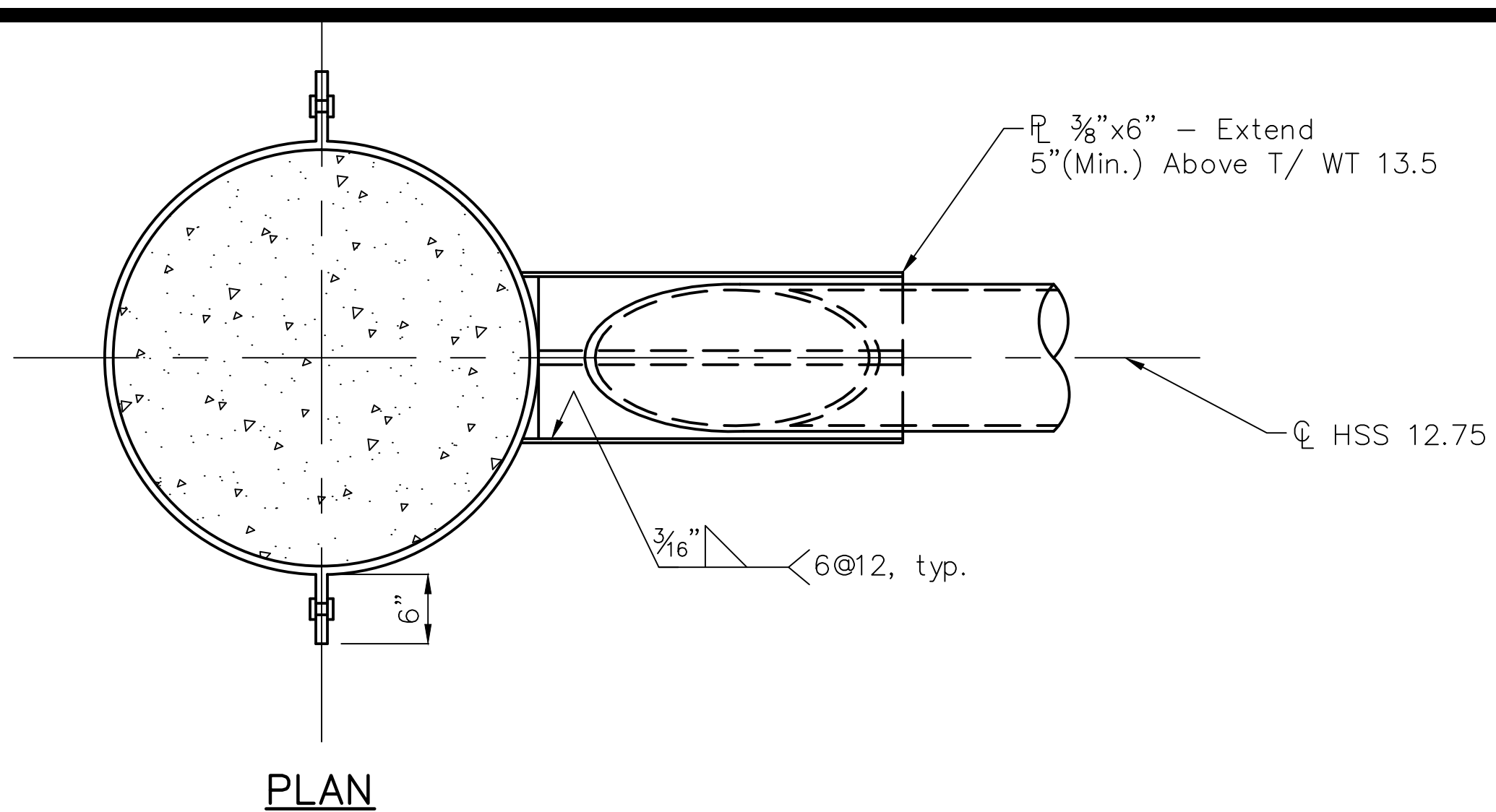
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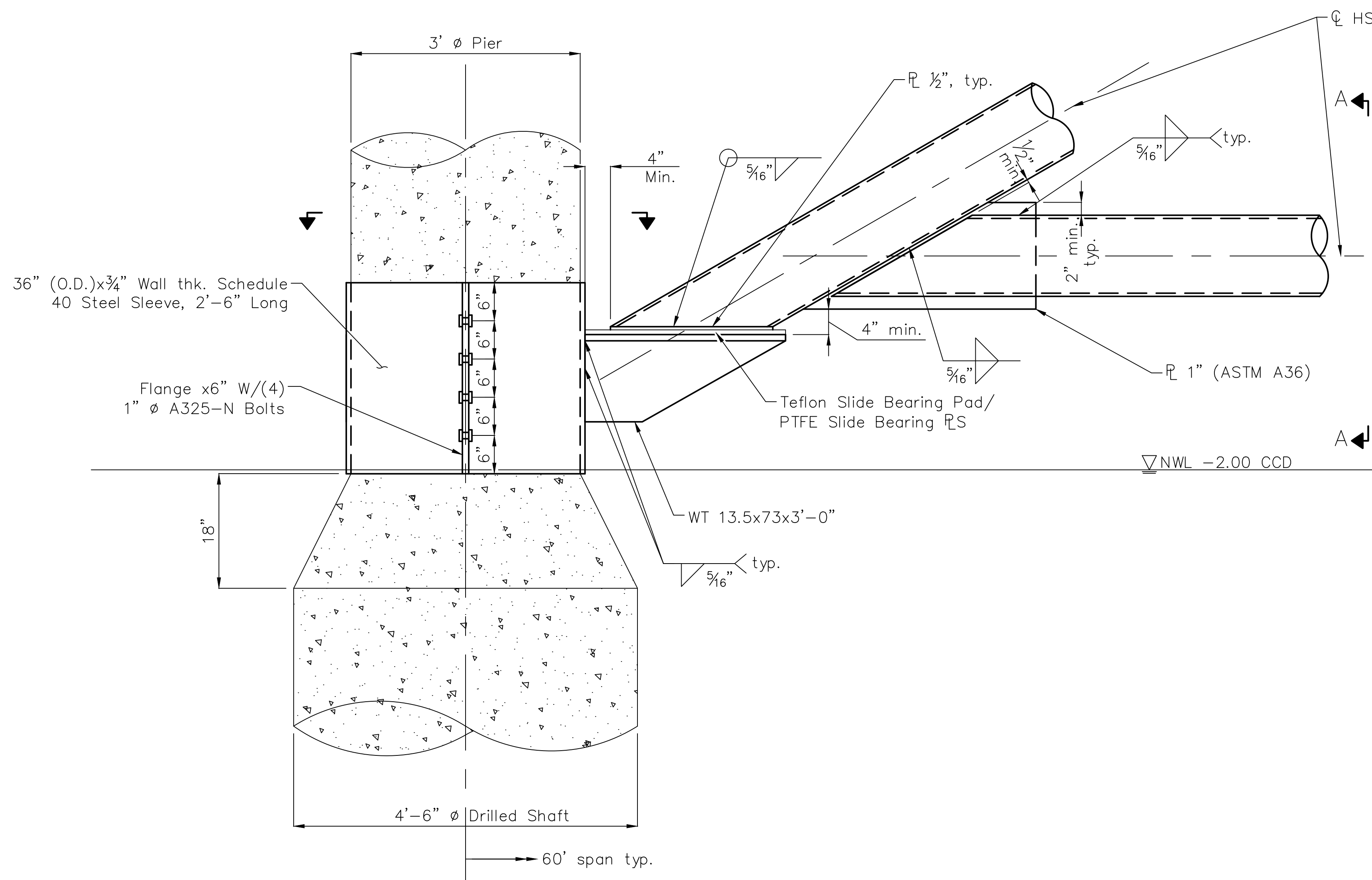
ADDISON UNDERBRIDGE TRAIL CONNECTION FROM
 CLARK PARK TO CALIFORNIA PARK
 VALUE

110 FT SPAN FALSEWORK

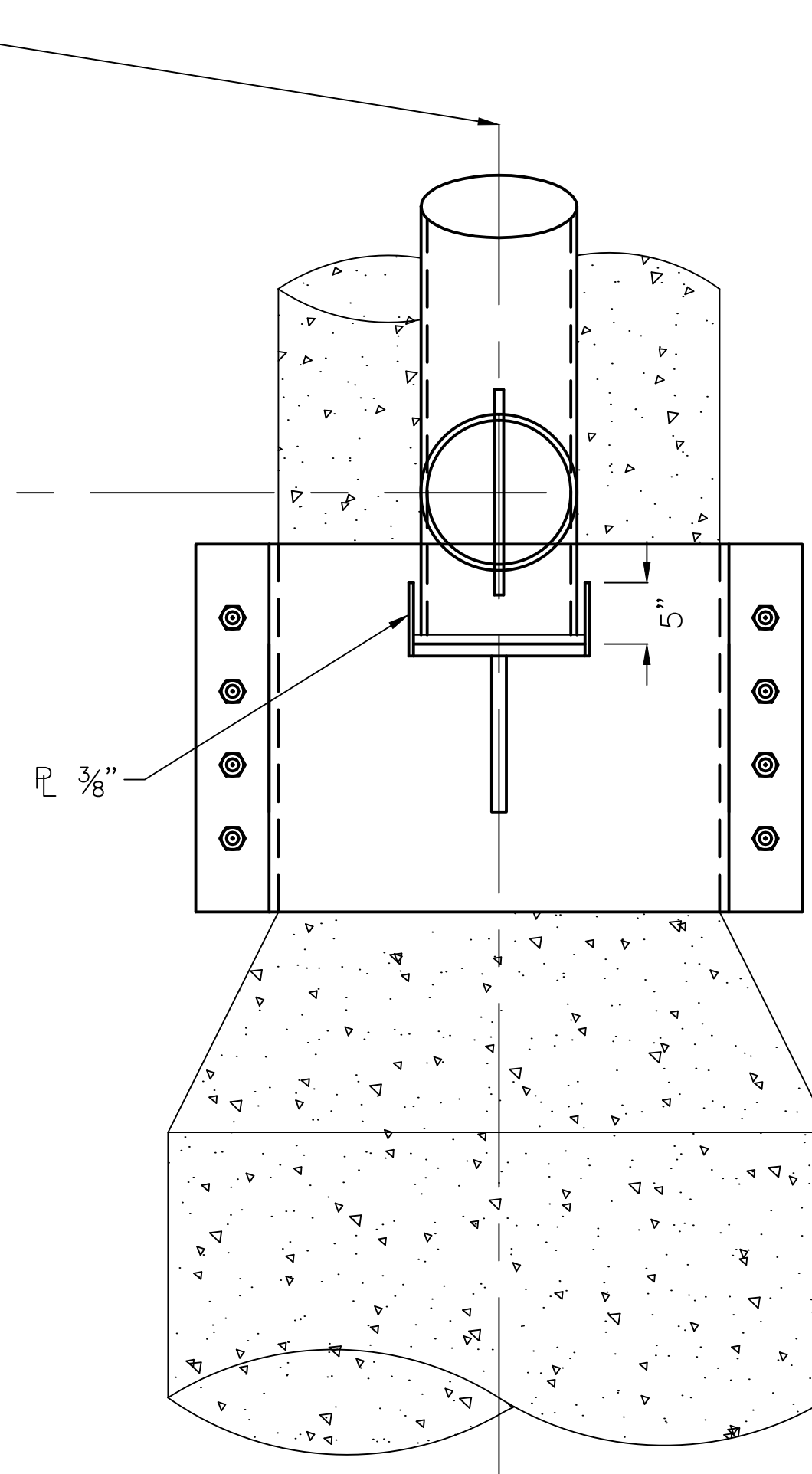
SHEET NO. S-05
 SHEET 52 OF 158



PLAN



ELEVATION



SECTION A-A

Notes:

1. Temporary Falsework provided is one feasible construction method for estimation of bidding as required by AASHTO Article 2.5.3. Contractor is responsible for suitable method of construction and temporary falseworks.
2. Concept drawing – NOT FOR CONSTRUCTION.

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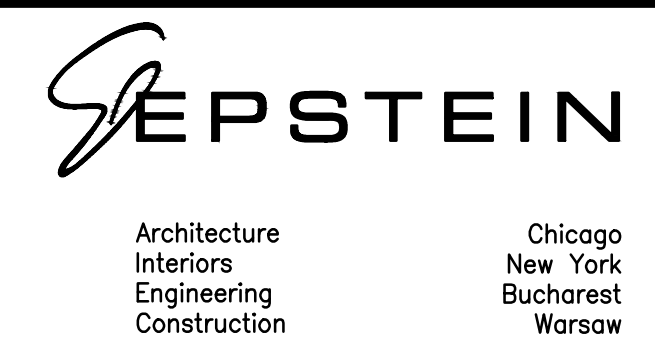
SECTION NO.: 12-E9121-00-BT
 DRAWN BY: K. KOZYS
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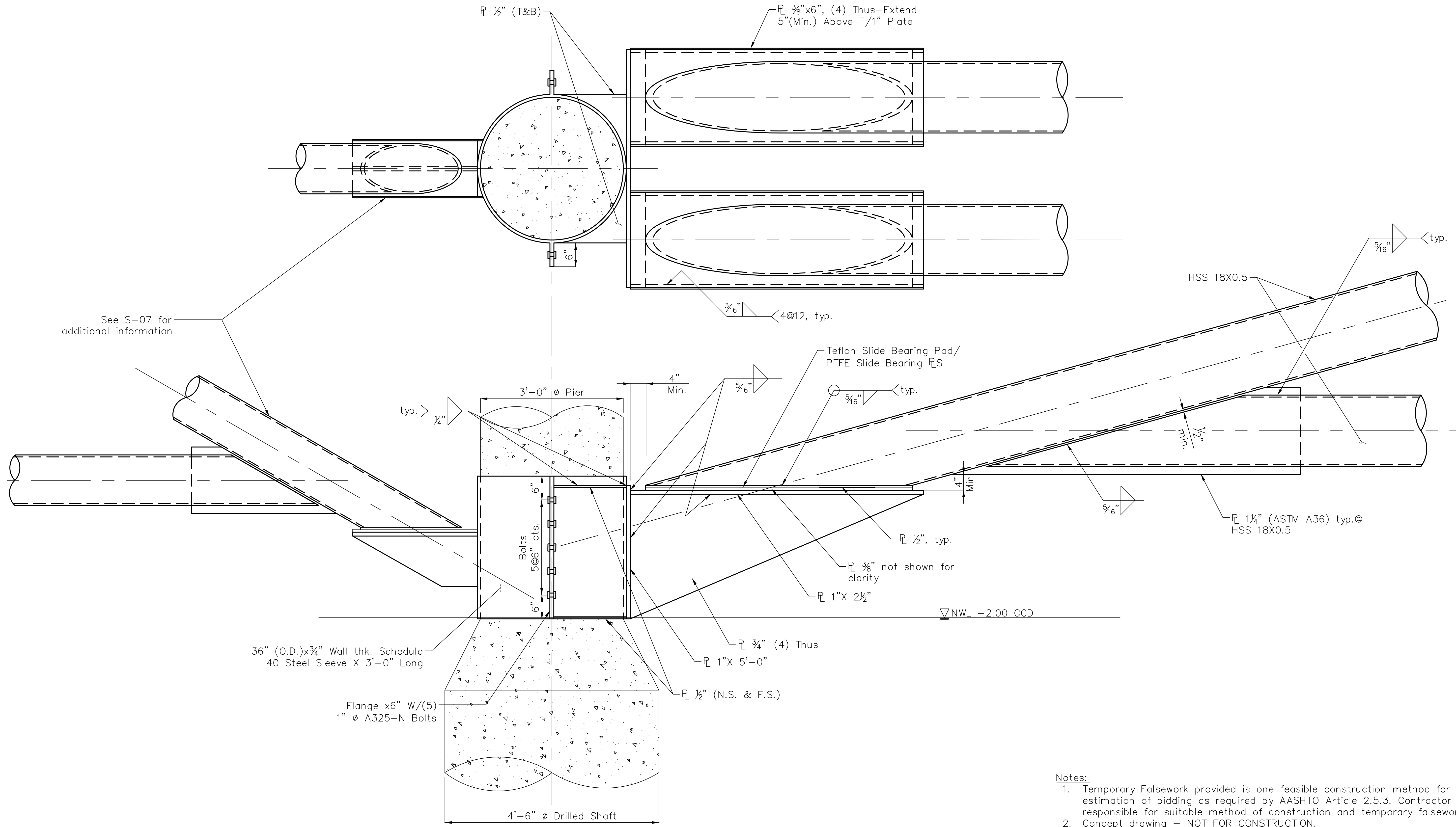
ADDISON UNDERBRIDGE TRAIL CONNECTION FROM
 CLARK PARK TO CALIFORNIA PARK
 STRUCTURE NO. 016-P025

SHEET NO.	REVISION
S-06	

SHEET 53 OF 158

FALSEWORK DETAILS I

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- Notes:**
1. Temporary Falsework provided is one feasible construction method for estimation of bidding as required by AASHTO Article 2.5.3. Contractor is responsible for suitable method of construction and temporary falseworks.
 2. Concept drawing - NOT FOR CONSTRUCTION.

SPAN = 110'-0"

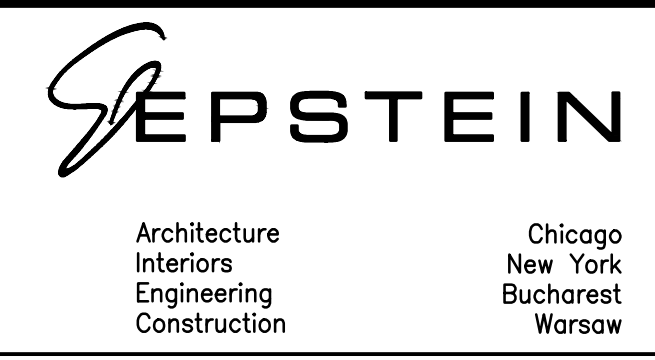
SECTION NO.: 12-E9121-00-BT
 DRAWN BY: K. KOZYS
 CHECKED BY: R. VARMA
 APPROVED BY: P. DOMBROWSKI
 DATE: 8/12/2016
 SCALE: N.T.S.

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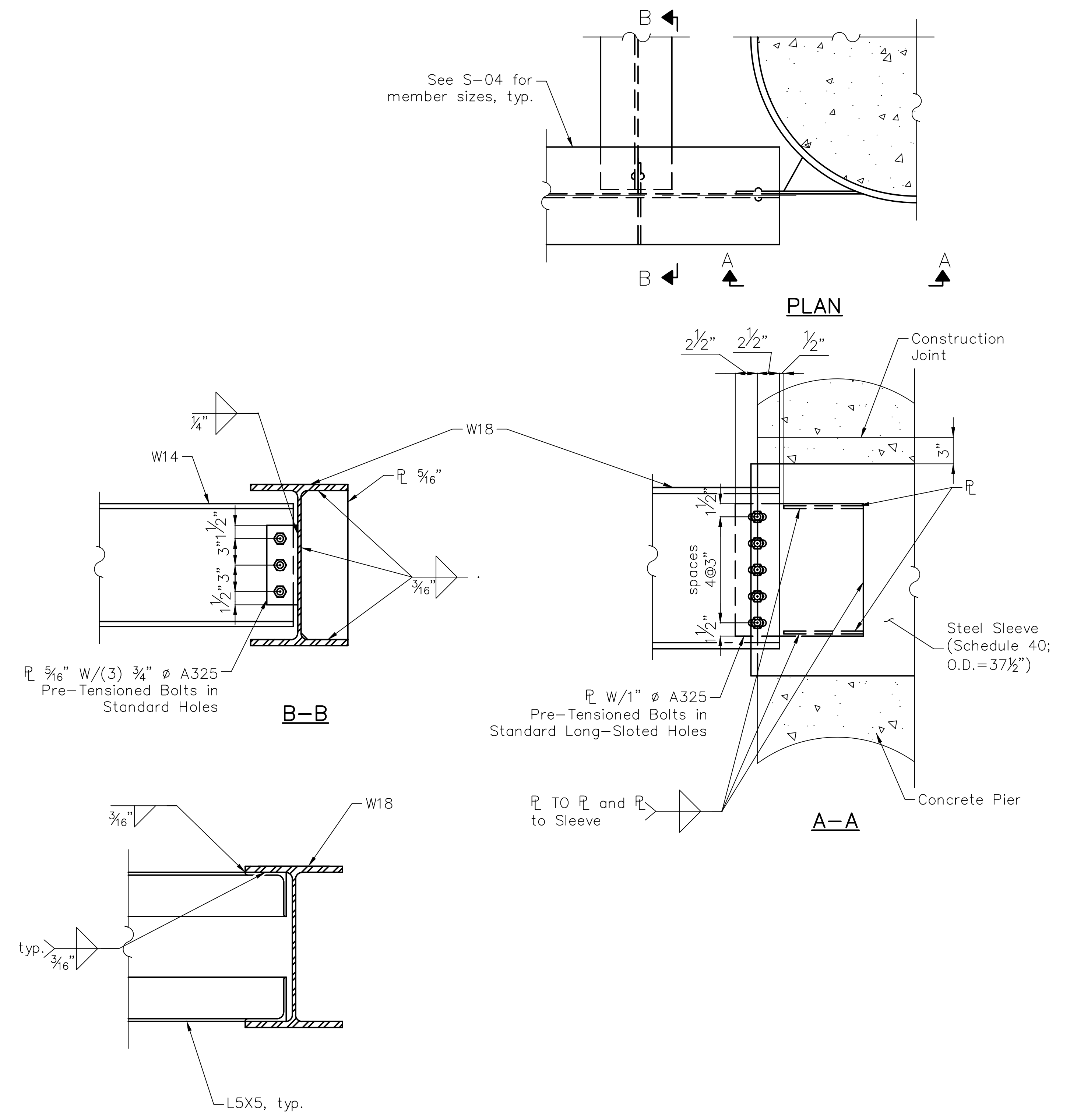
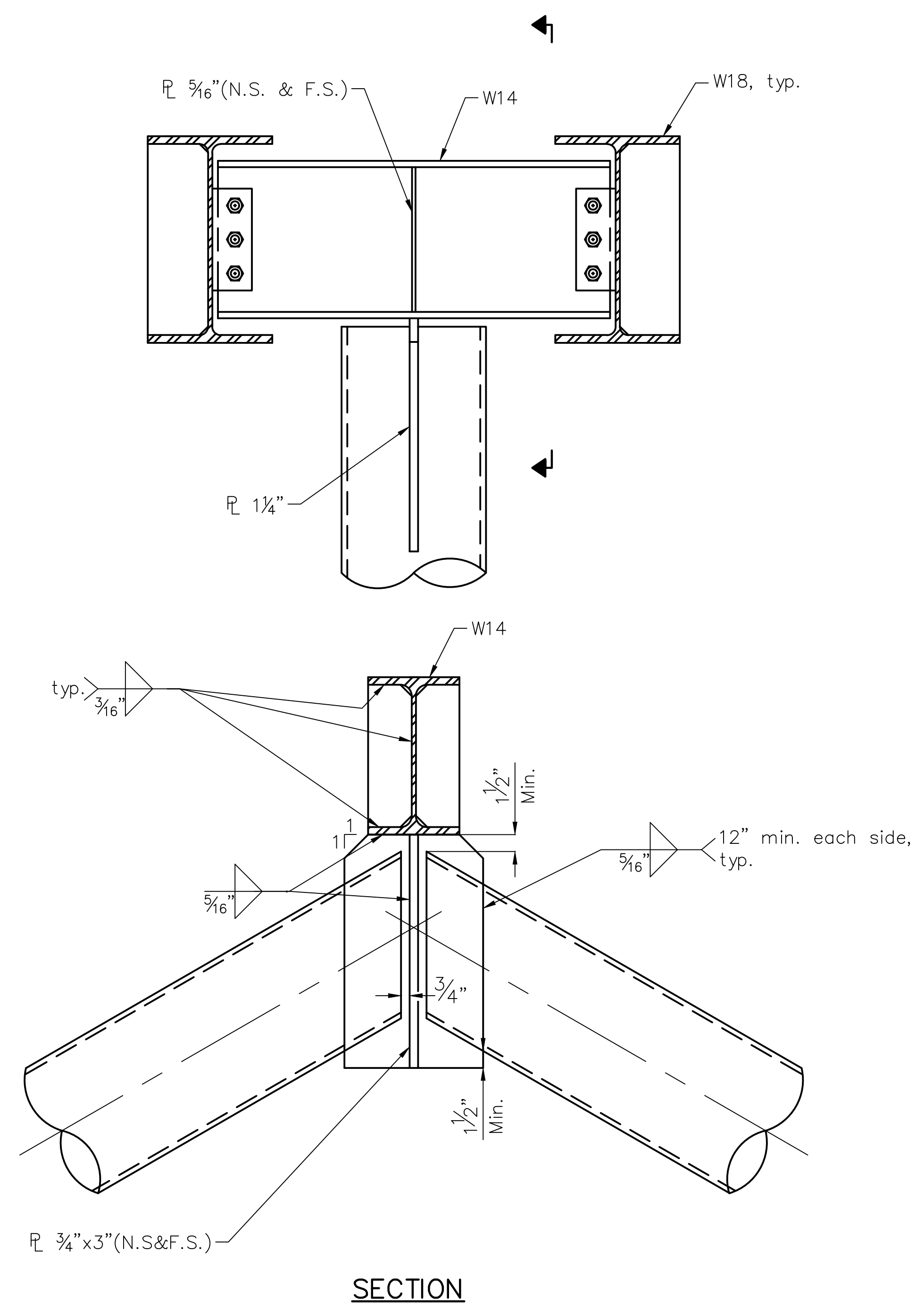
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ADDISON UNDERBRIDGE TRAIL CONNECTION FROM
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 STRUCTURE NO. 016-P025
 FALSEWORK DETAILS II

SHEET NO. S-07
 SHEET 54 OF 158
 REVISION

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- Notes:**
1. Temporary Falsework provided is one feasible construction method for estimation of bidding as required by AASHTO Article 2.5.3. Contractor is responsible for suitable method of construction and temporary falseworks.
 2. Concept drawing - NOT FOR CONSTRUCTION.

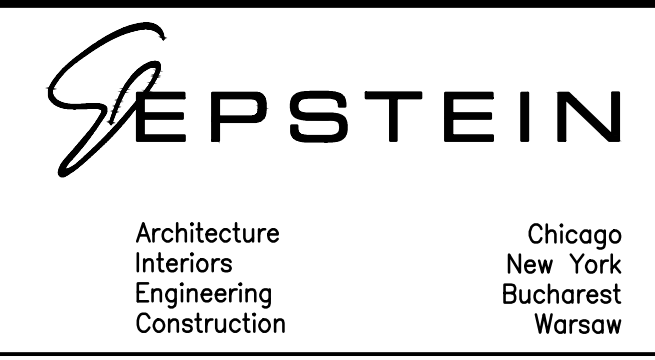
SECTION NO.: 12-E9121-00-BT
 DRAWN BY: K. KOZYS
 CHECKED BY: R. VARMA
 APPROVED BY: P. DOMBROWSKI
 DATE: 8/12/2016
 SCALE: N.T.S.

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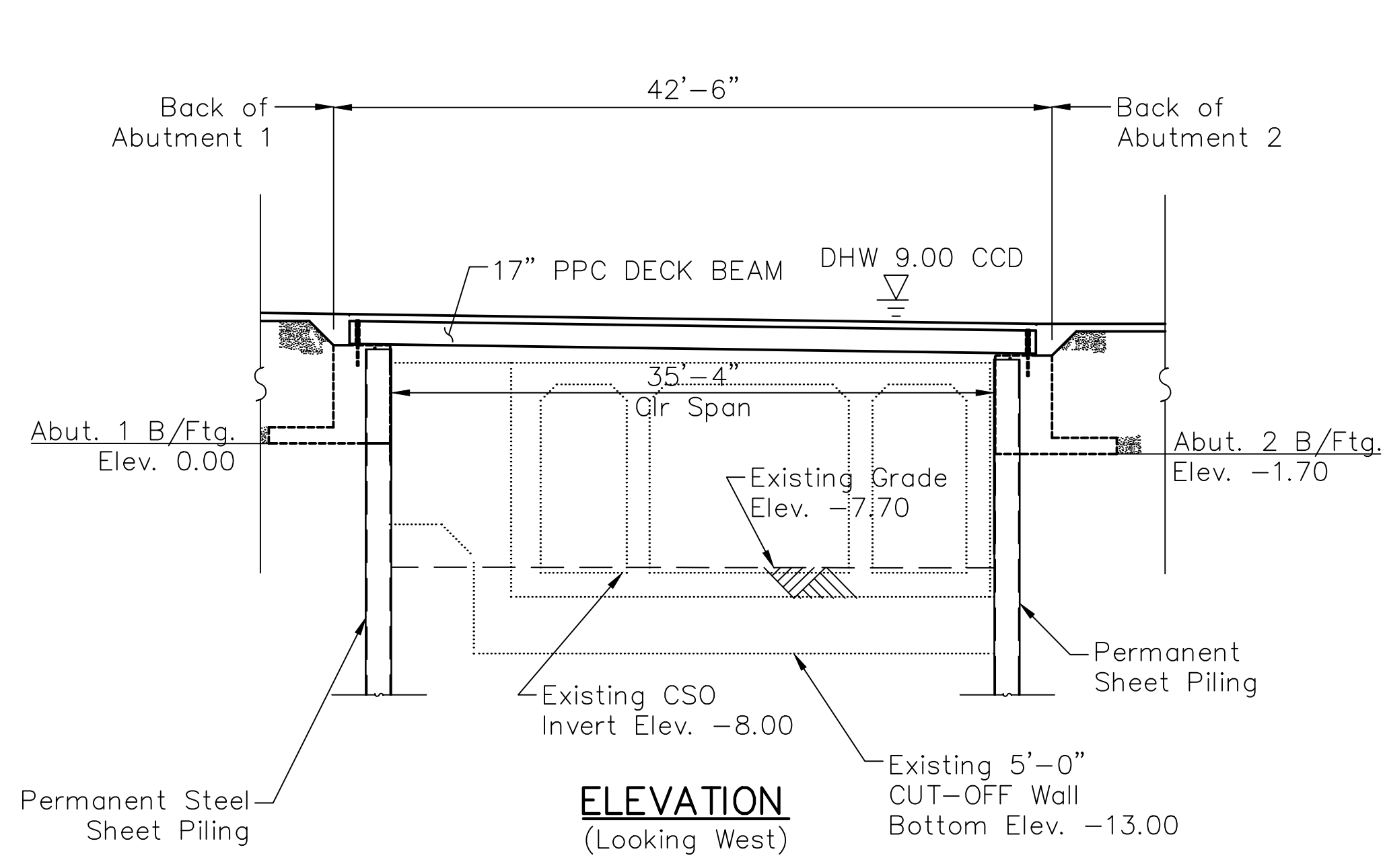
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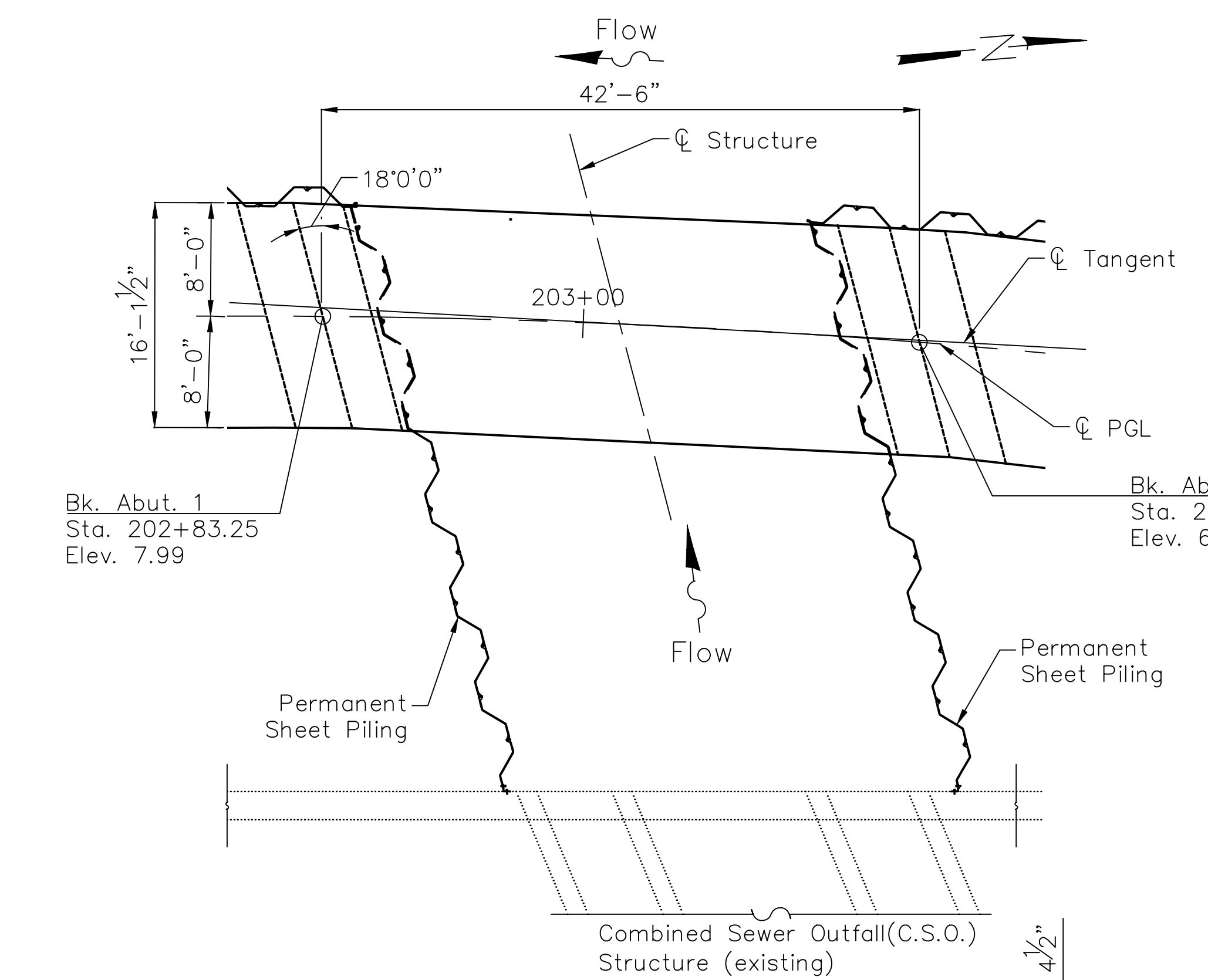
ADDISON UNDERBRIDGE TRAIL CONNECTION FROM CLARK PARK TO CALIFORNIA PARK
 STRUCTURE NO. 016-P025

FALSEWORK DETAILS III

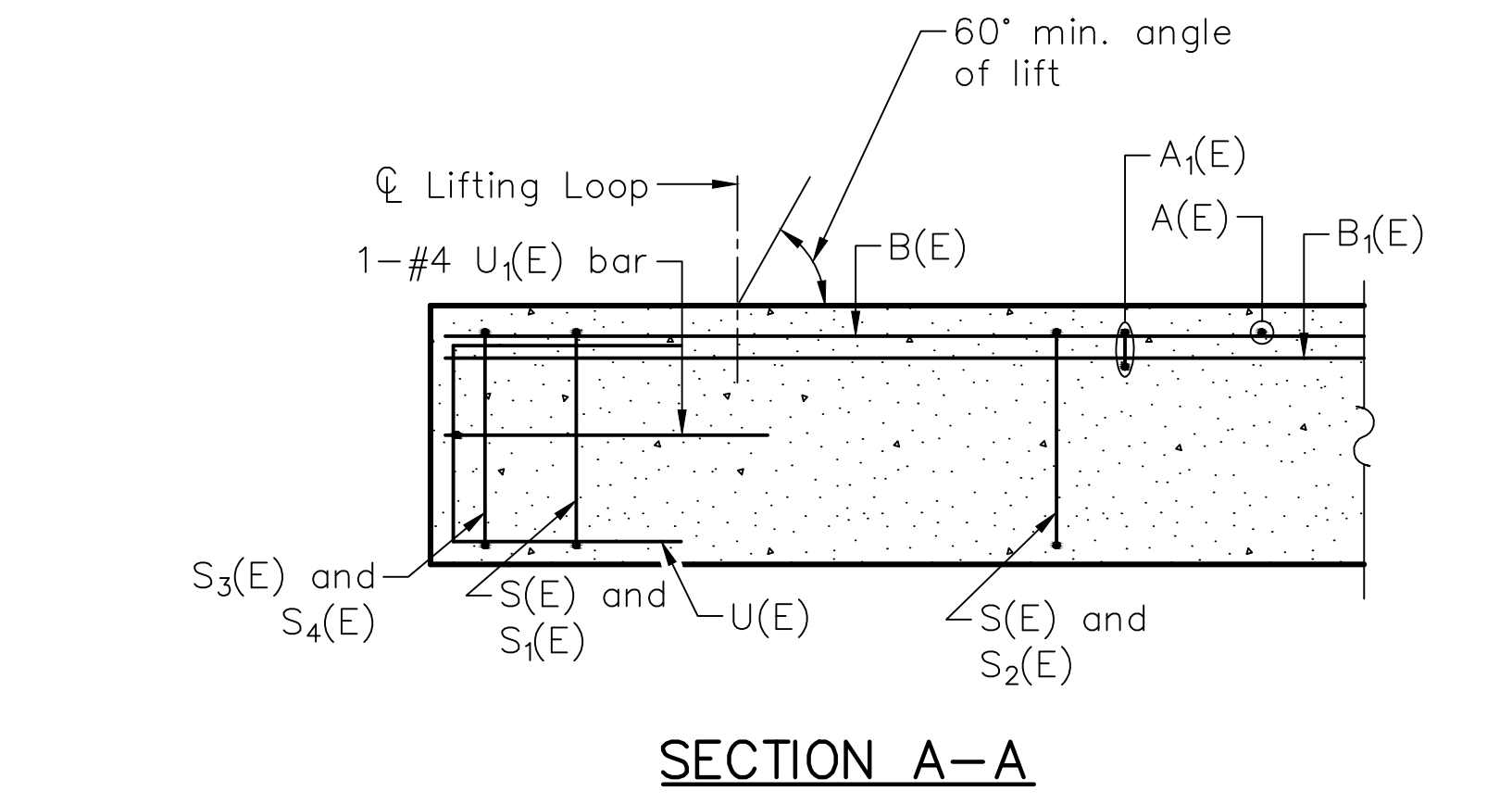
SHEET NO. S-08
 SHEET 55 OF 158



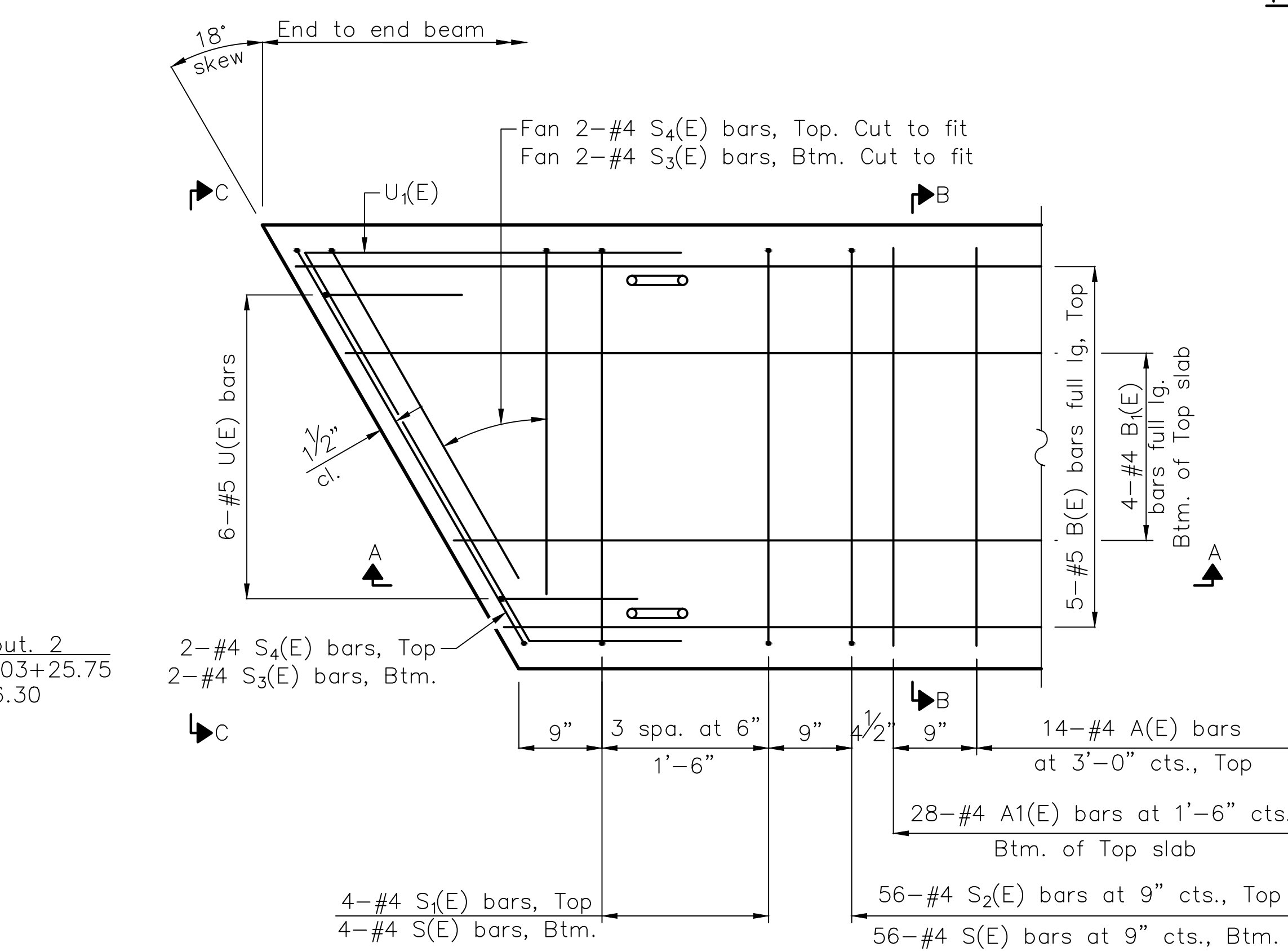
ELEVATION
(Looking West)



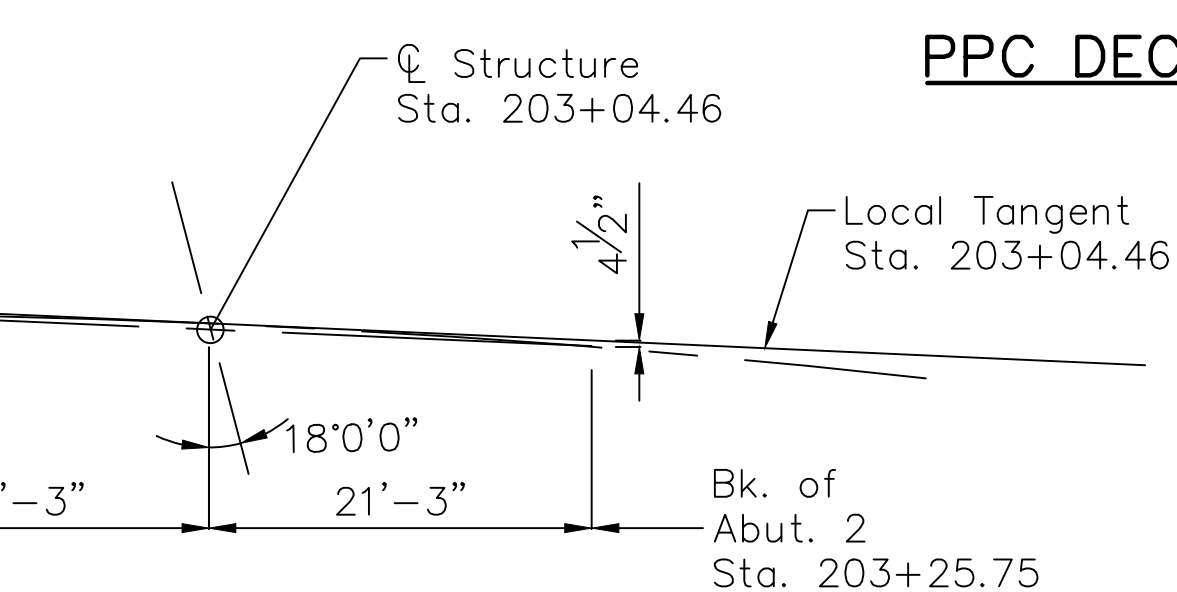
PLAN



SECTION A-A



PPC DECK BEAM PLAN VIEW

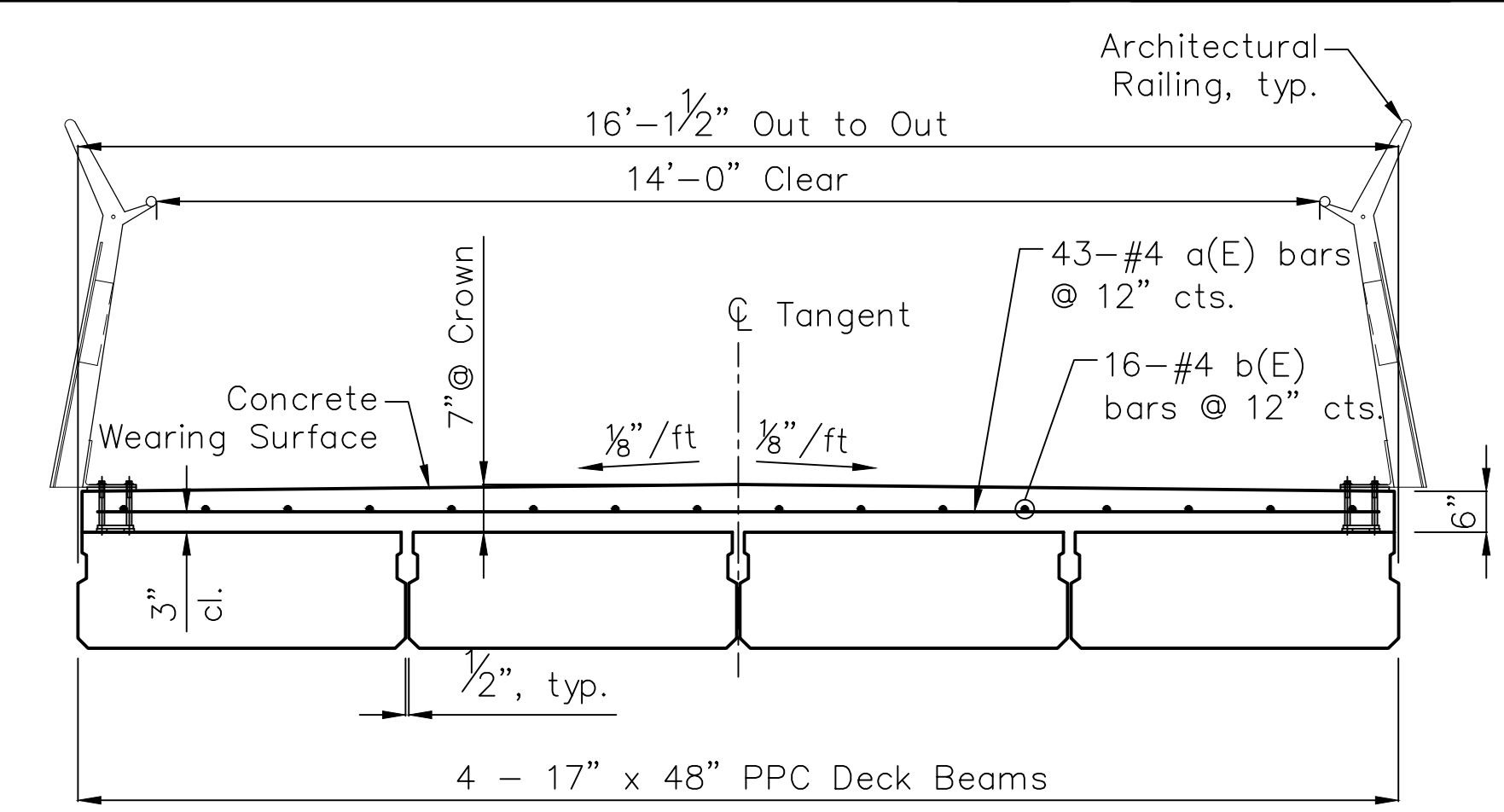


OFFSET SKETCH

Note:
Spacing of S(E) and S2(E) bars may be adjusted up to 4" in the immediate area of the transverse tie diaphragms to miss the block outs for the transverse ties.

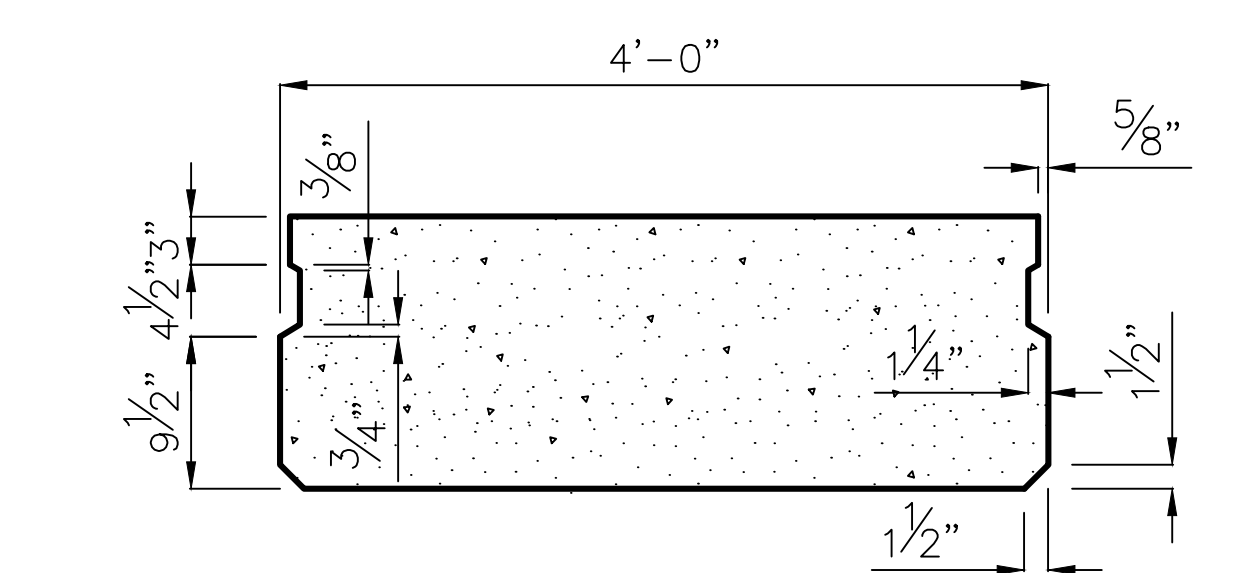
MINIMUM BAR LAP

#4 bar = 2'-0"
#5 bar = 2'-6"

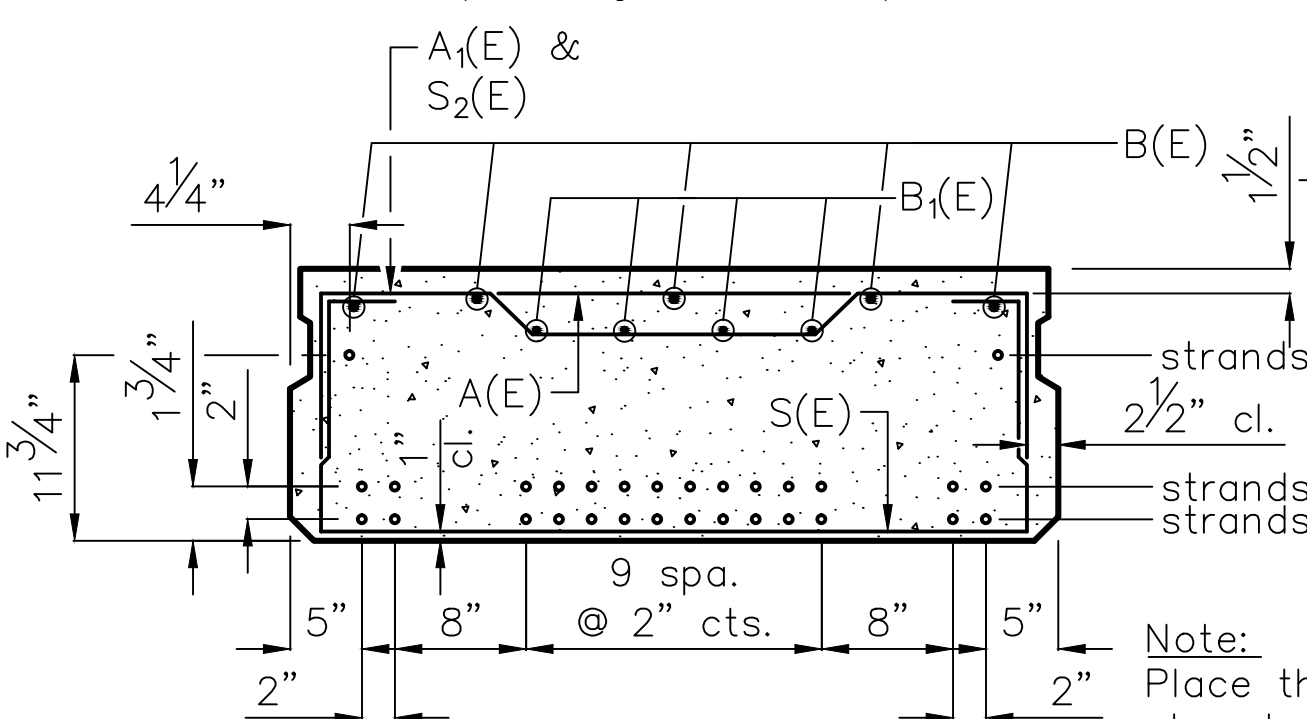


TYPICAL SECTION OVER COMBINED SEWER OUTFALL

Dimensions are perpendicular to tangent.

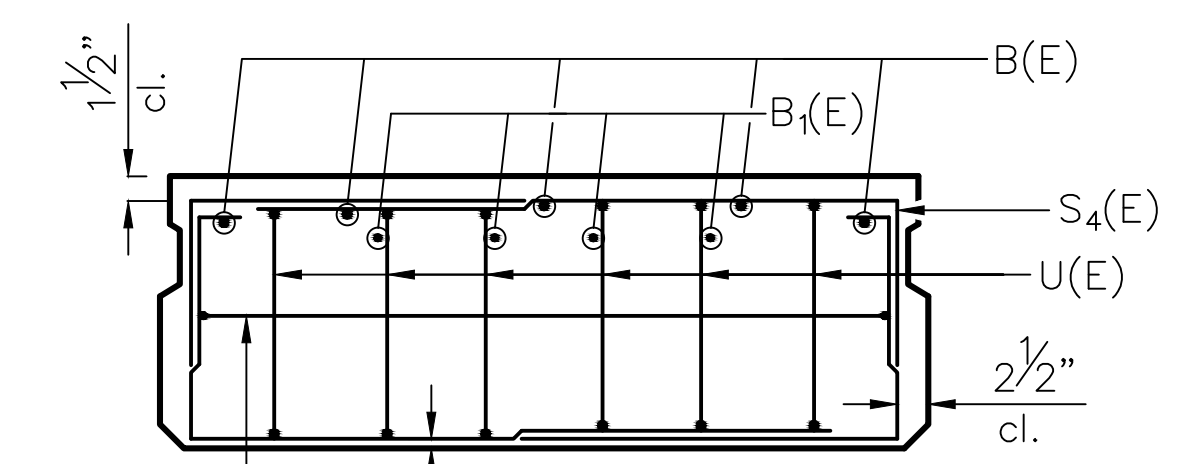


SECTION B-B
(Showing Dimensions)



SECTION B-B
(Showing reinforcement and permissible strand locations)

Note:
Place the number of strands specified in each row symmetrically about the centerline of the beam in the permissible strand locations shown



VIEW C-C

Notes:
1. For Permanent Sheet Piling, see sheets S-62 thru S-66 of S-87
2. For curve data, see sheet S-03 of S-87

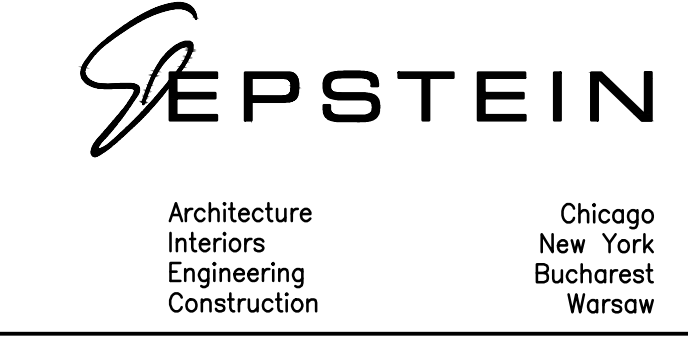
SECTION NO.: 12-E9121-00-BT
DRAWN BY: P. DOMBROWSKI
CHECKED BY: R. VARMA
APPROVED BY: P. DOMBROWSKI
DATE: 8/12/2016
SCALE: N.T.S.

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1	8/12/16	ISSUED FOR FINAL
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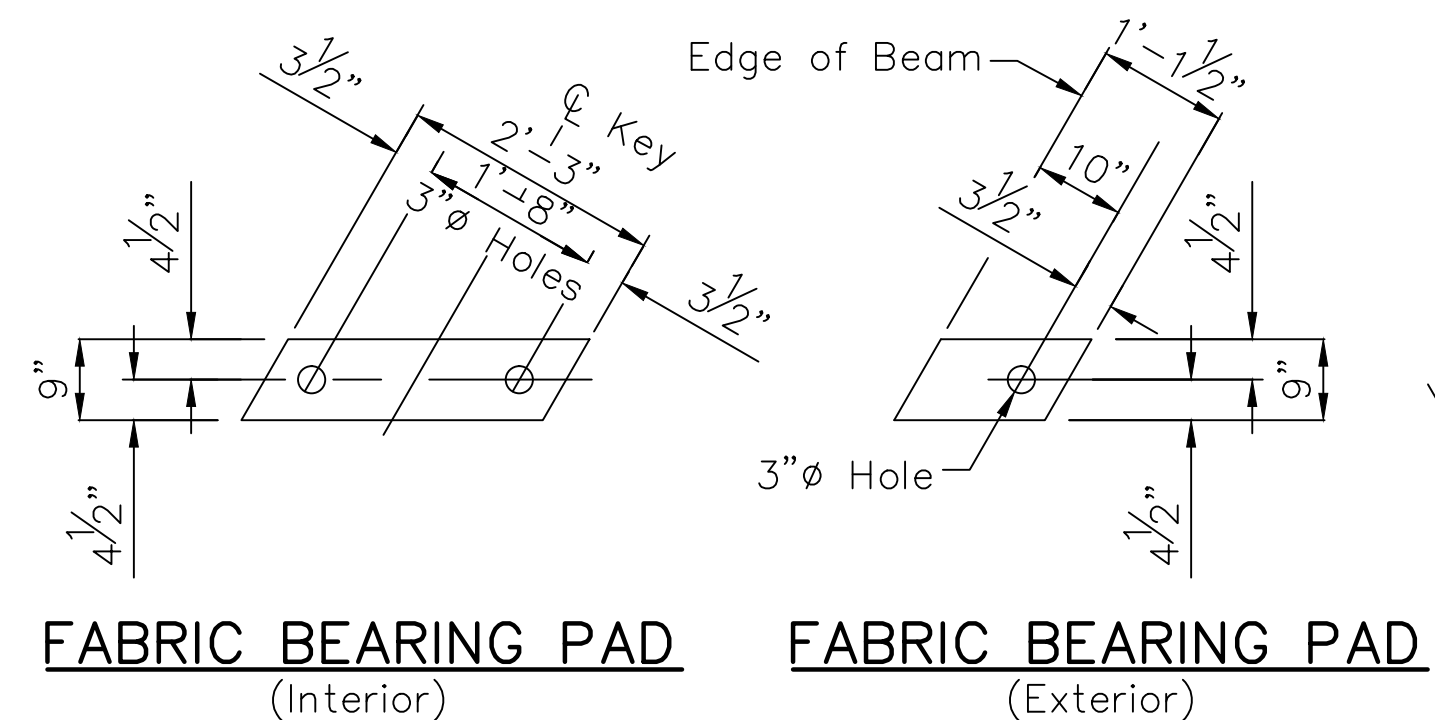


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ADDISON UNDERBRIDGE TRAIL CONNECTION FROM CLARK PARK TO CALIFORNIA PARK VALUE
COMBINED SEWER OUTFALL BRIDGE DETAILS I

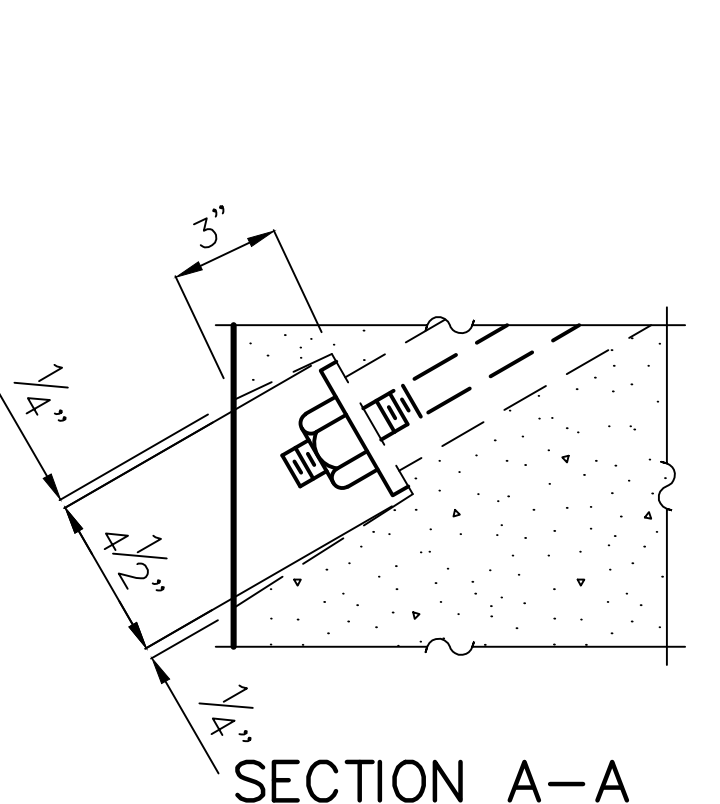
SHEET NO. S-09
SHEET 56 OF 158
REVISION



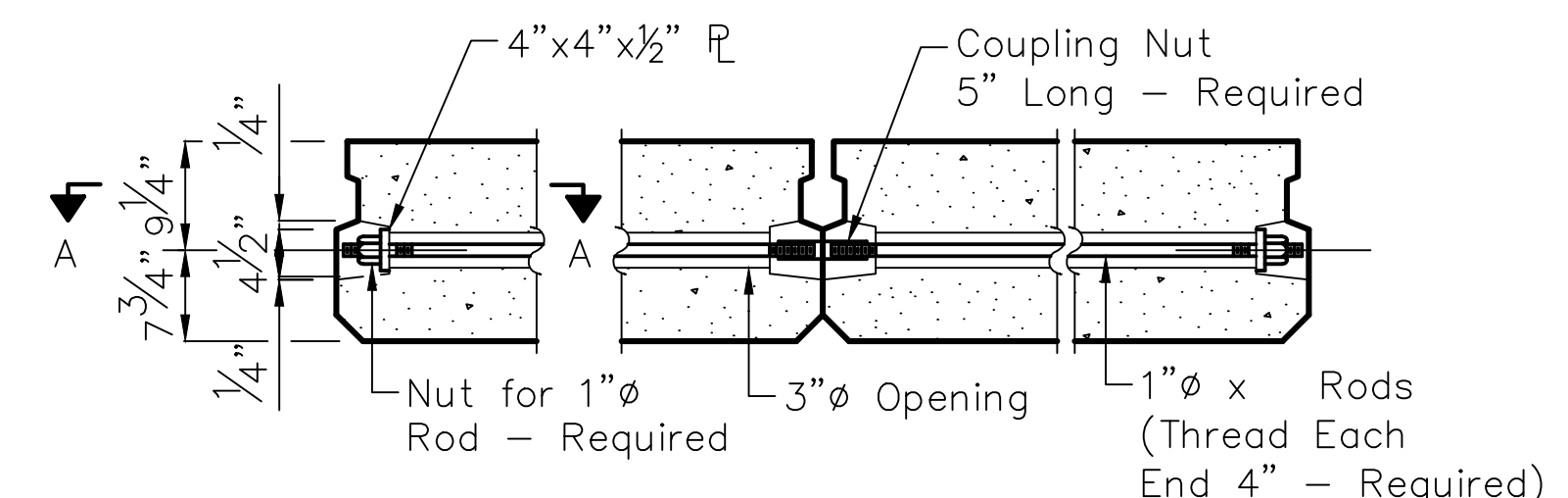
FABRIC BEARING PAD
(Interior) **FABRIC BEARING PAD**
(Exterior)

FIXED

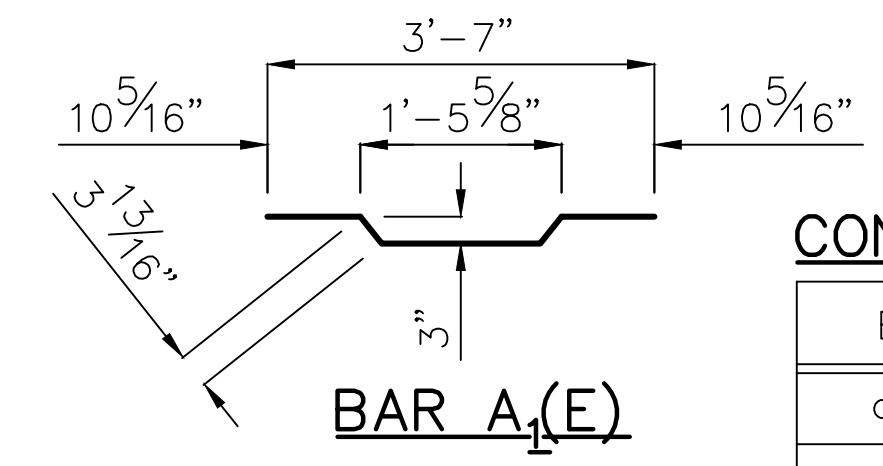
Notes:
All bearing pads shall be 1" thick.
Omit holes when using expansion bearings.
Expansion bearing pad shall be bonded to the substructure.



SECTION A-A



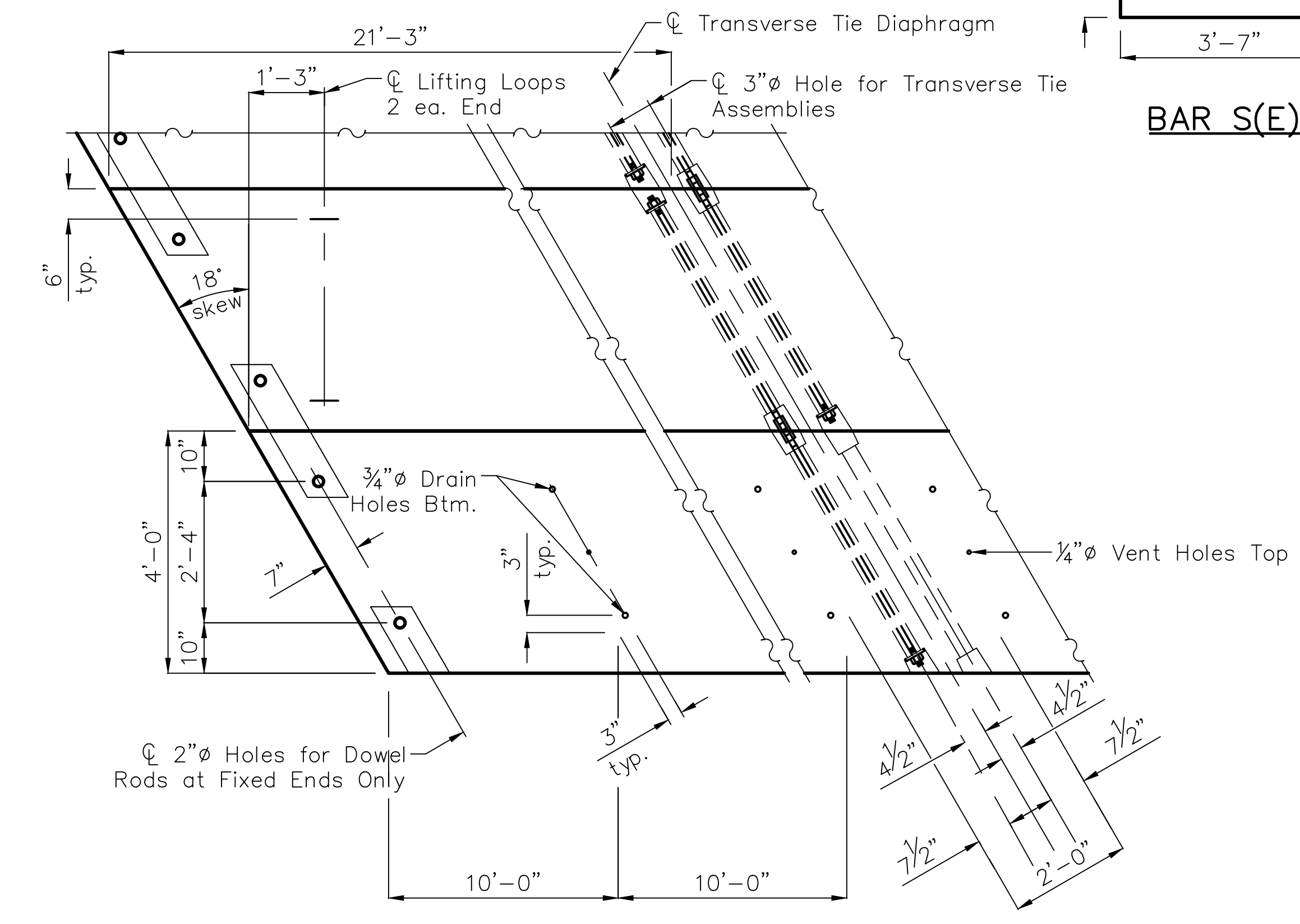
TYPICAL TRANSVERSE TIE ASSEMBLY



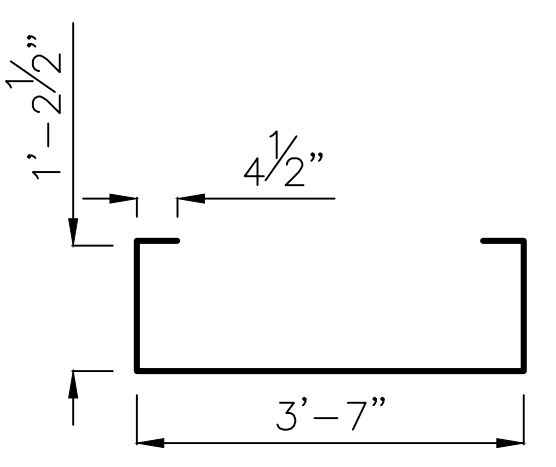
BAR A₁(E)

BAR LIST
CONCRETE WEARING SURFACE ONLY

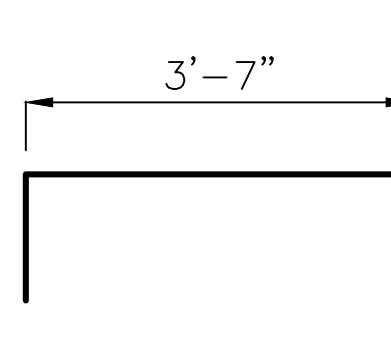
Bar	No.	Size	Length	Shape
a(E)	43	#4	42'-0"	—
b(E)	16	#4	15'-8"	—



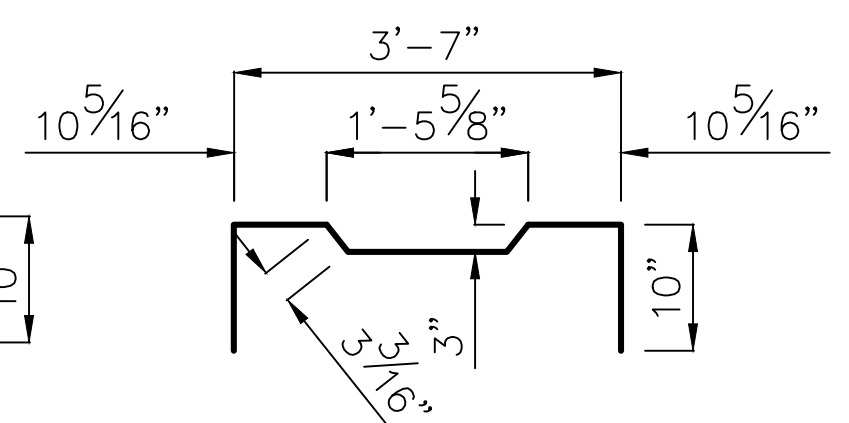
PLAN VIEW



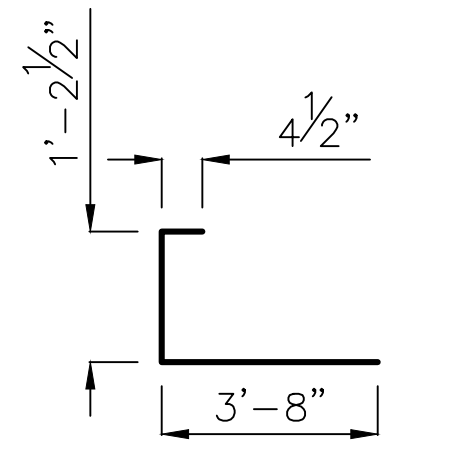
BAR S(E)



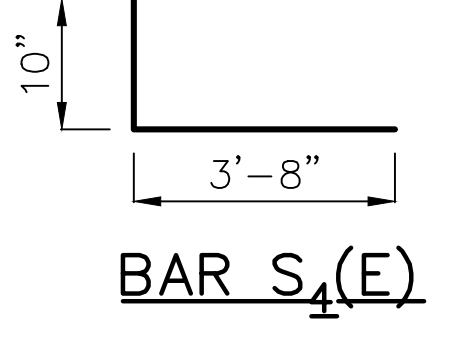
BAR S₁(E)



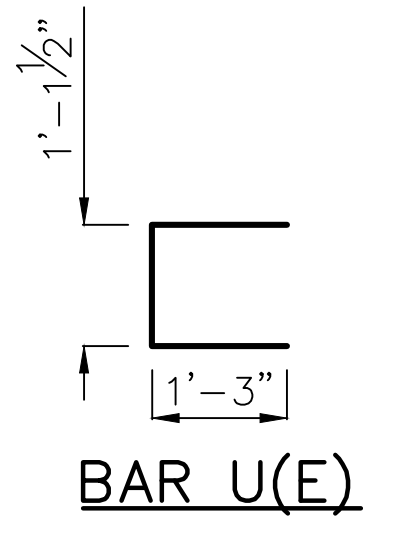
BAR S₂(E)



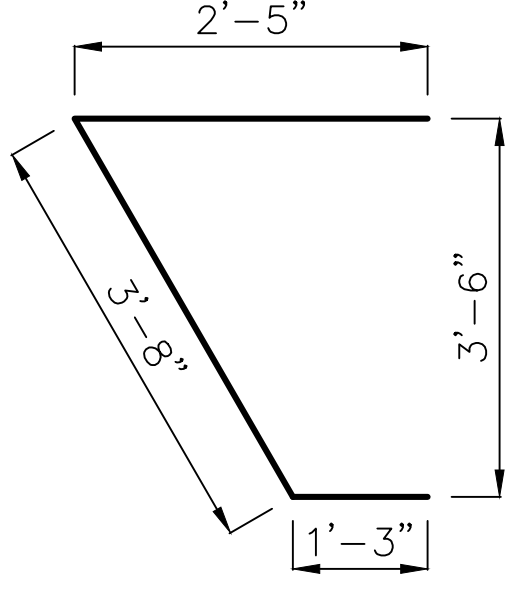
BAR S₃(E)



BAR S₄(E)



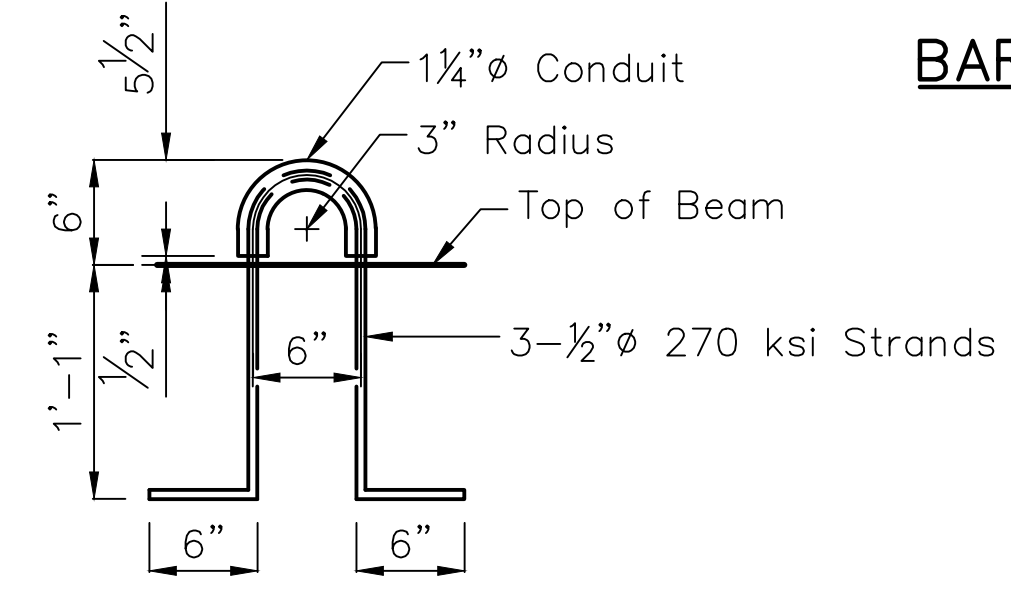
BAR U(E)



BAR U₁(E)

BAR LIST
ONE BEAM ONLY
(For information only)

Bar	No.	Size	Length	Shape
A(E)	14	#4	3'-7"	—
A ₁ (E)	28	#4	3'-10"	—
B(E)	5	#5	42'-3"	—
B ₁ (E)	4	#4	42'-3"	—
S(E)	60	#4	6'-9"	—
S ₁ (E)	8	#4	5'-3"	—
S ₂ (E)	56	#4	5'-6"	—
S ₃ (E)	8	#4	5'-3"	—
S ₄ (E)	8	#4	4'-6"	—
U(E)	12	#5	3'-8"	—
U ₁ (E)	2	#4	7'-4"	—



LIFTING LOOP DETAIL

BILL OF MATERIAL

Item	Unit	Qty
Precast Prestressed Concrete Deck Beams. (17" depth)	Sq. Ft.	680
Concrete Wearing Surface, 6"	Sq. Yd.	76
* Reinforcement Bars, Epoxy Coated	Pound	900

* Quantity for Reinforcement Bars, Epoxy Coated are only for bars in Concrete Wearing Surface.

- Notes:
1. Prestressing steel shall be uncoated high strength, low relaxation 7-wire strand, Grade 270.
 2. The nominal diameter shall be 1/2" and the nominal cross-sectional area shall be 0.153 sq. in.
 3. The 1" rods in the transverse tie assembly shall be tightened to a snug fit and the threads set.
 4. Pockets on exterior faces of bridge shall be filled with grout after transverse tie assembly is in place.
 5. Reinforcement bars shall conform to ASTM A 706, Grade 60. (See Special Provisions).
 6. Two 1/8" fabric adjusting shims of the dimensions of the exterior bearing pad shall be provided for each bearing pad location.
 7. A minimum 2 1/2" diameter lifting pin shall be used to engage the lifting loops during handling.
 8. Corrosion Inhibitor, per Article 1020.05(b)(12) and 1021.06 of the Standard Specifications, shall be used in the concrete for precast prestressed concrete deck beams.
 9. Compressive strength of prestressed concrete, f'c, shall be 6000 psi.
 10. Compressive strength of prestressed concrete at release, f'ci, shall be 5000 psi.

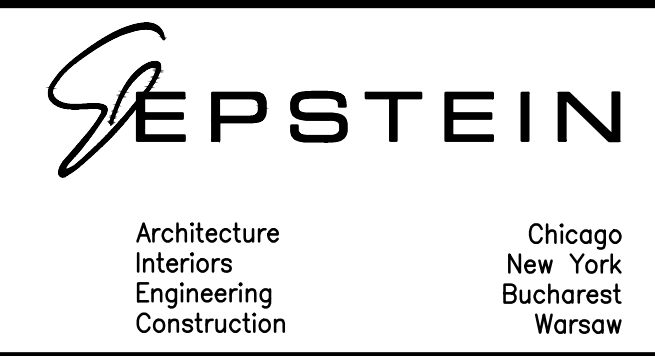
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DRAWN BY: P. DOMBROWSKI
CHECKED BY: R. VARMA
APPROVED BY: P. DOMBROWSKI
DATE: 8/12/2016
SCALE: N.T.S.

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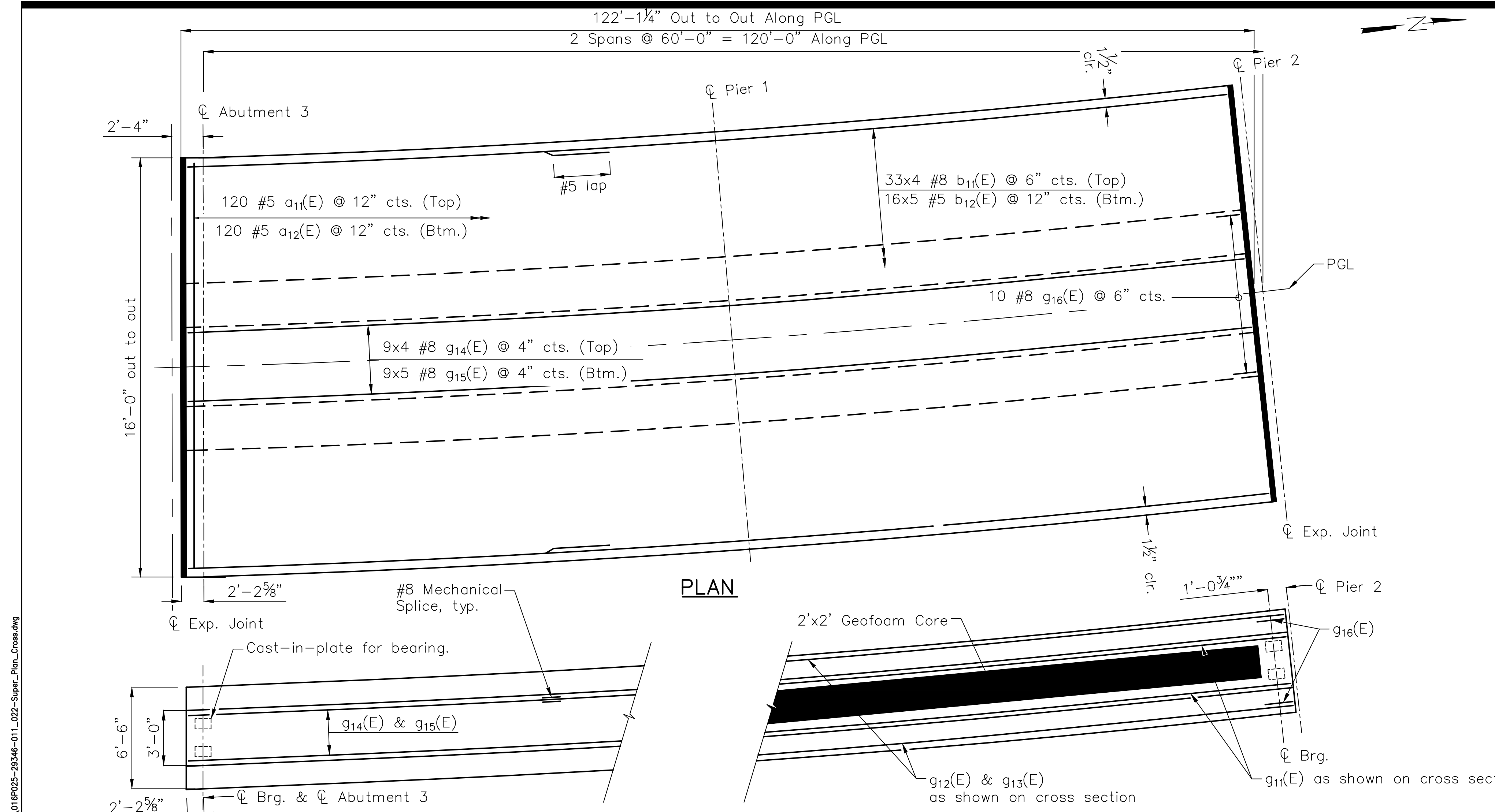
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ADDISON UNDERBRIDGE TRAIL CONNECTION FROM
CLARK PARK TO CALIFORNIA PARK
VALUE

COMBINED SEWER OUTFALL BRIDGE DETAILS II

SHEET NO.
S-10
SHEET 57 OF 158



Minimum Lap Lengths	
bar Size to be Spliced	Length
#4**	1'-1"
#5	2'-1"

** #4 spiral bars shall have a minimum lap length of 3'-0".

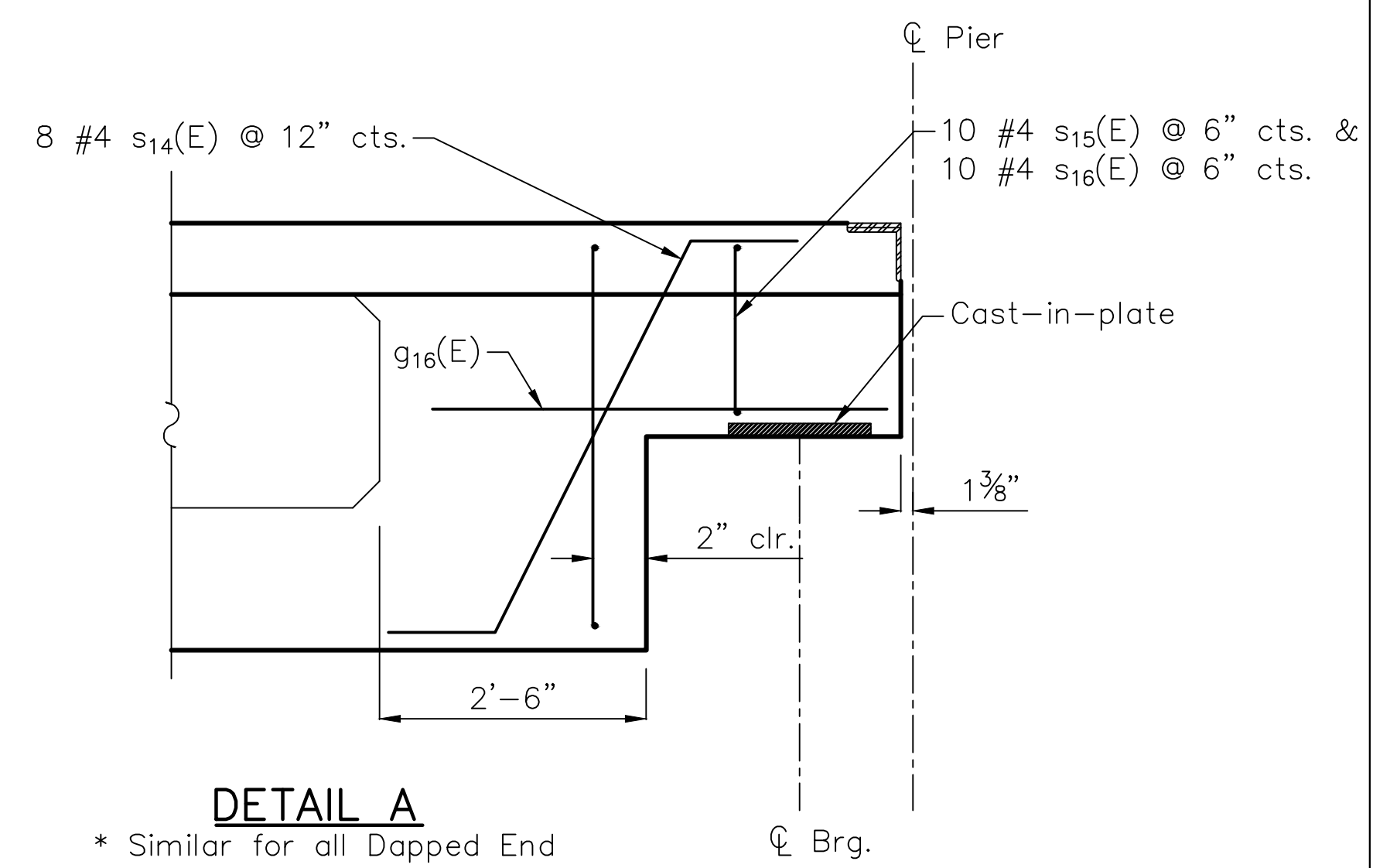
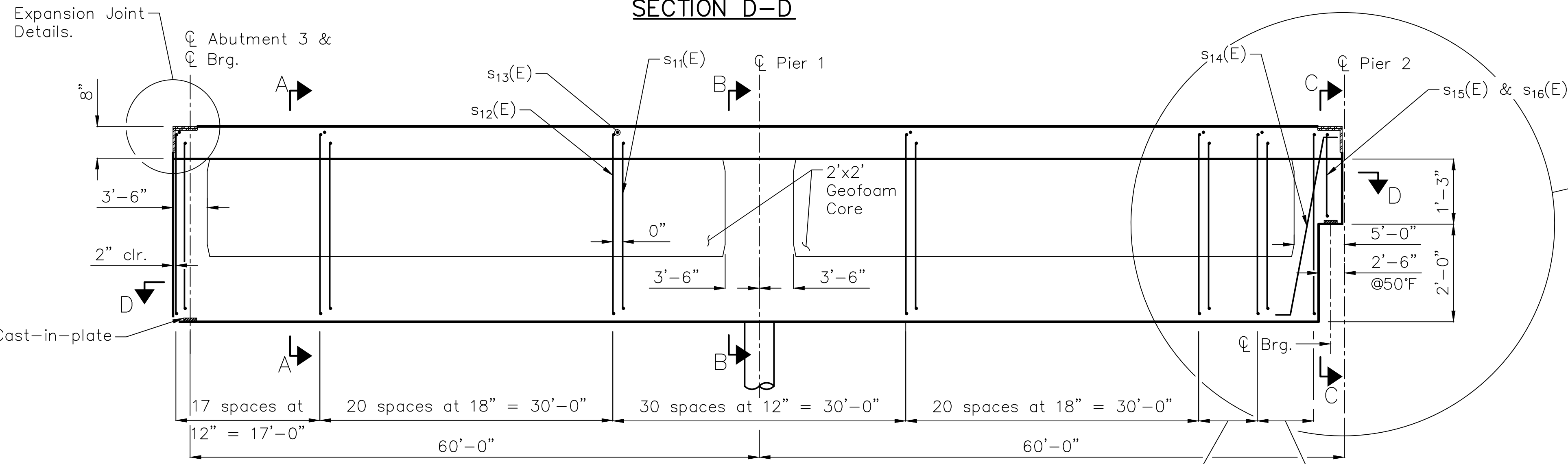
SEGMENT A BILL OF MATERIALS

Bar	No.	Size	Length	Shape
a1(E)	120	#5	15'-9"	—
a2(E)	120	#5	15'-9"	—
b1(E)	132	#8	30'-5"	—
b2(E)	80	#5	32'-7"	—
g1(E)	30	#8	23'-10"	—
g2(E)	24	#8	30'-5"	—
g3(E)	16	#8	29'-9"	—
g4(E)	36	#8	29'-9"	—
g5(E)	45	#8	23'-10"	—
g6(E)	10	#8	4'-7"	—
s1(E)	105	#4	10'-11"	U
s2(E)	105	#4	11'-9"	U
s3(E)	210	#4	6'-2"	U
s4(E)	8	#5	5'-9"	U
s5(E)	10	#4	16'-9"	U
s6(E)	10	#4	7'-10"	U
Reinforcement Bars, Epoxy Coated			Pound	31,040
High Strength High Performance Concrete Superstructures			Cu. Yd.	102.1
Geofoam (Polystyrene Fill)			Cu. Yd.	15.9

LOWER STEM REINFORCEMENT

UPPER STEM REINFORCEMENT

SECTION D-D



- Notes:**
1. For Sections A-A, B-B, & C-C bar bending diagrams and Detail A, see sheet S-14 of S-87.
 2. For Expansion Joint Details, see sheet S-48 of S-87.
 3. For Bearing Details & Cast-in-steel plate, see sheet S-49 of S-87.
 4. For Bar Splicers, see sheet S-67 of S-87.
 5. Bars indicated thus 100 x 2 - #5, etc. indicate 100 lines of bars with 2 lengths per line.

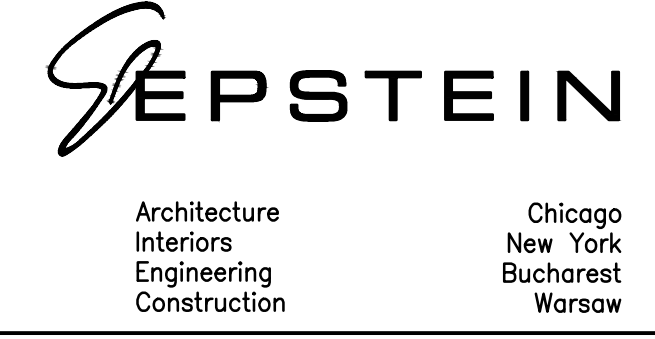
SECTION NO.: 12-E9121-00-BT
 DRAWN BY: N. FIRNBACH
 CHECKED BY: R. VARMA
 APPROVED BY: P. DOMBROWSKI
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 SCALE: N.T.S.

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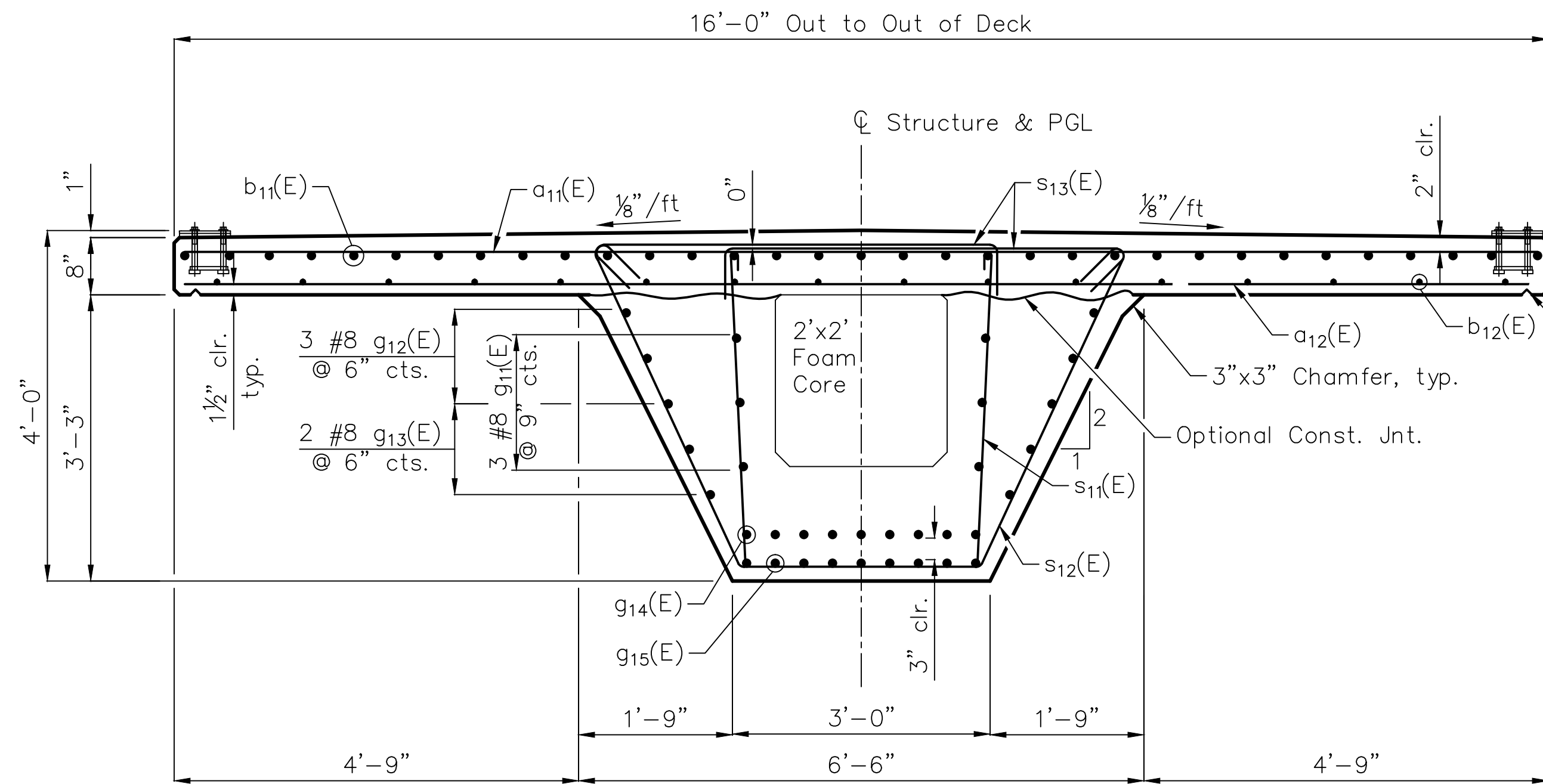
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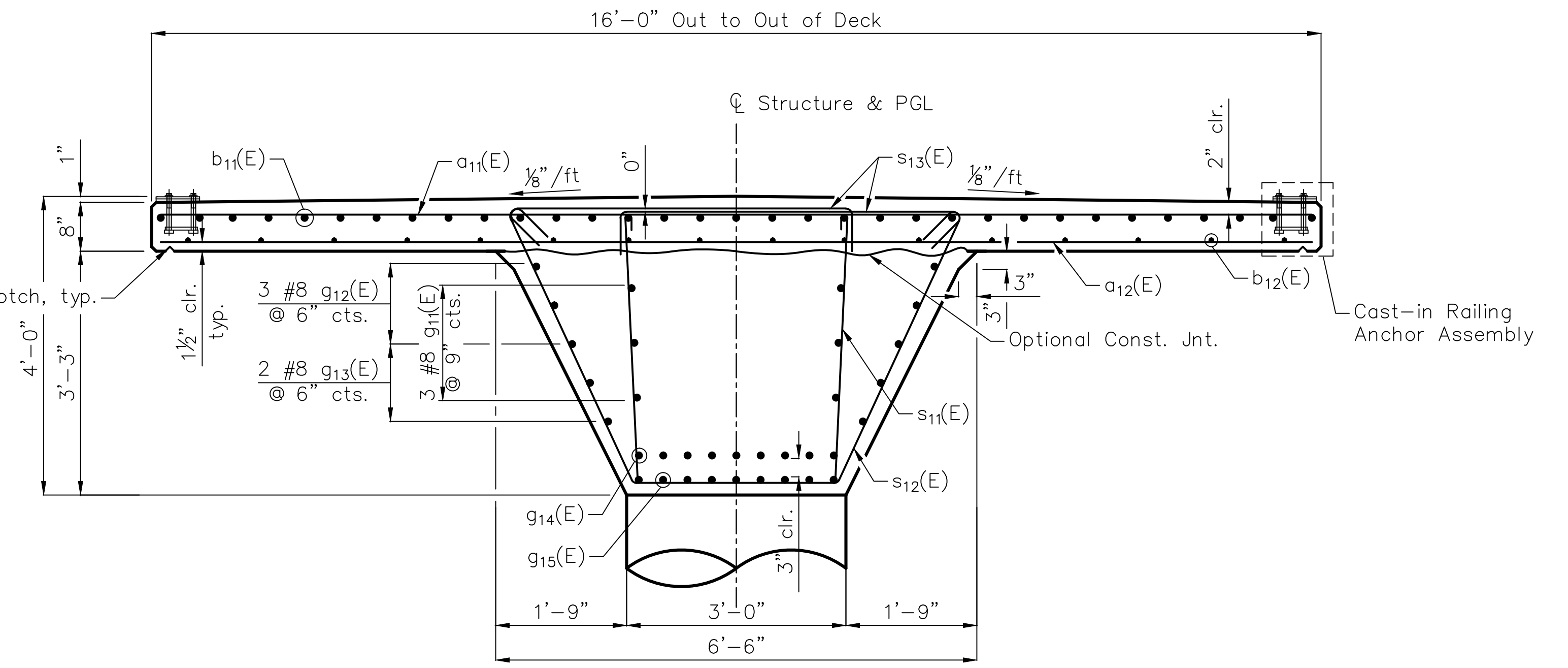
ADDISON UNDERBRIDGE TRAIL CONNECTION FROM
 CLARK PARK TO CALIFORNIA PARK
 STRUCTURE NO. 016-P025

SUPERSTRUCTURE PLAN & ELEVATION - SEGMENT A

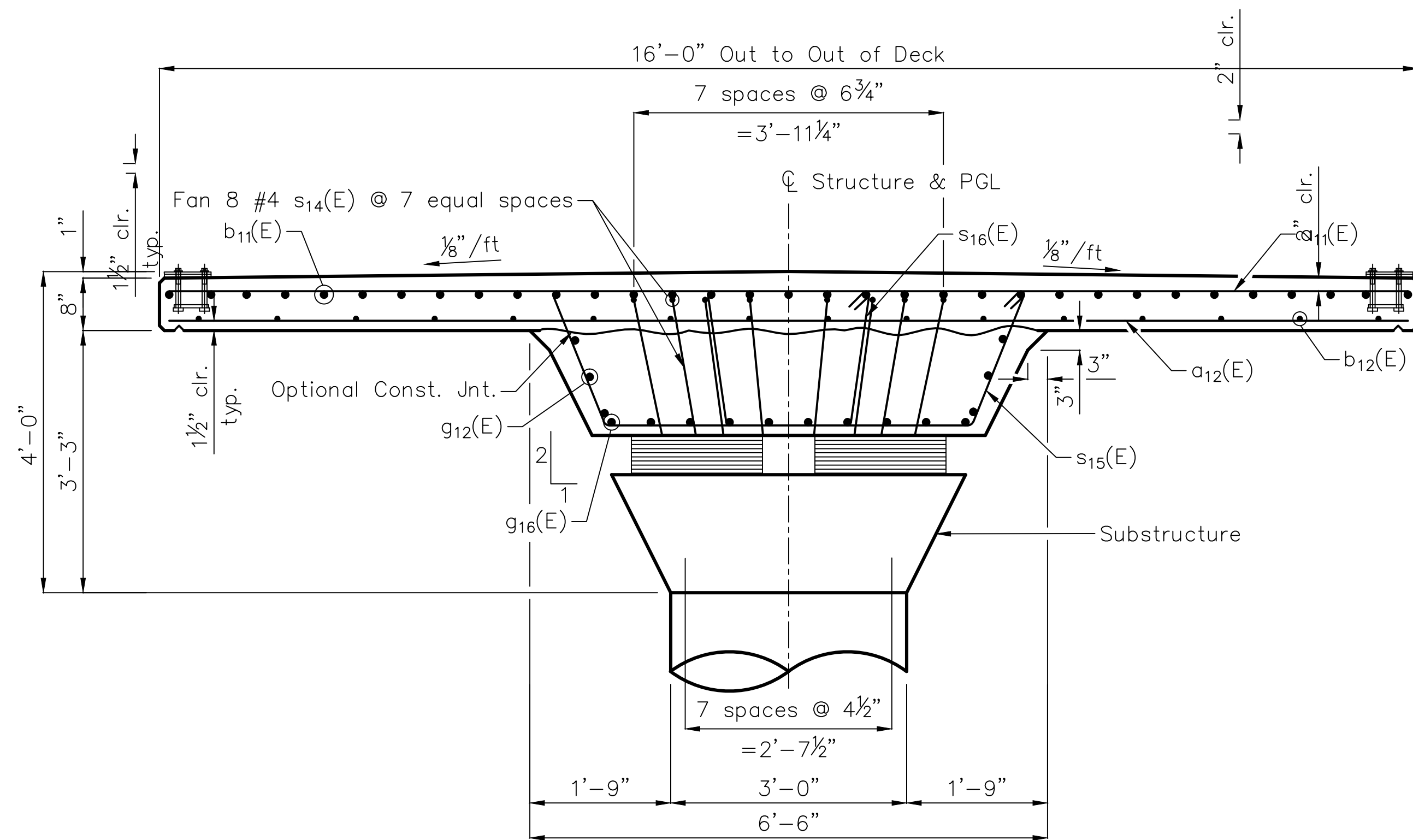
SHEET NO. S-11	REVISION
SHEET 58 OF 158	



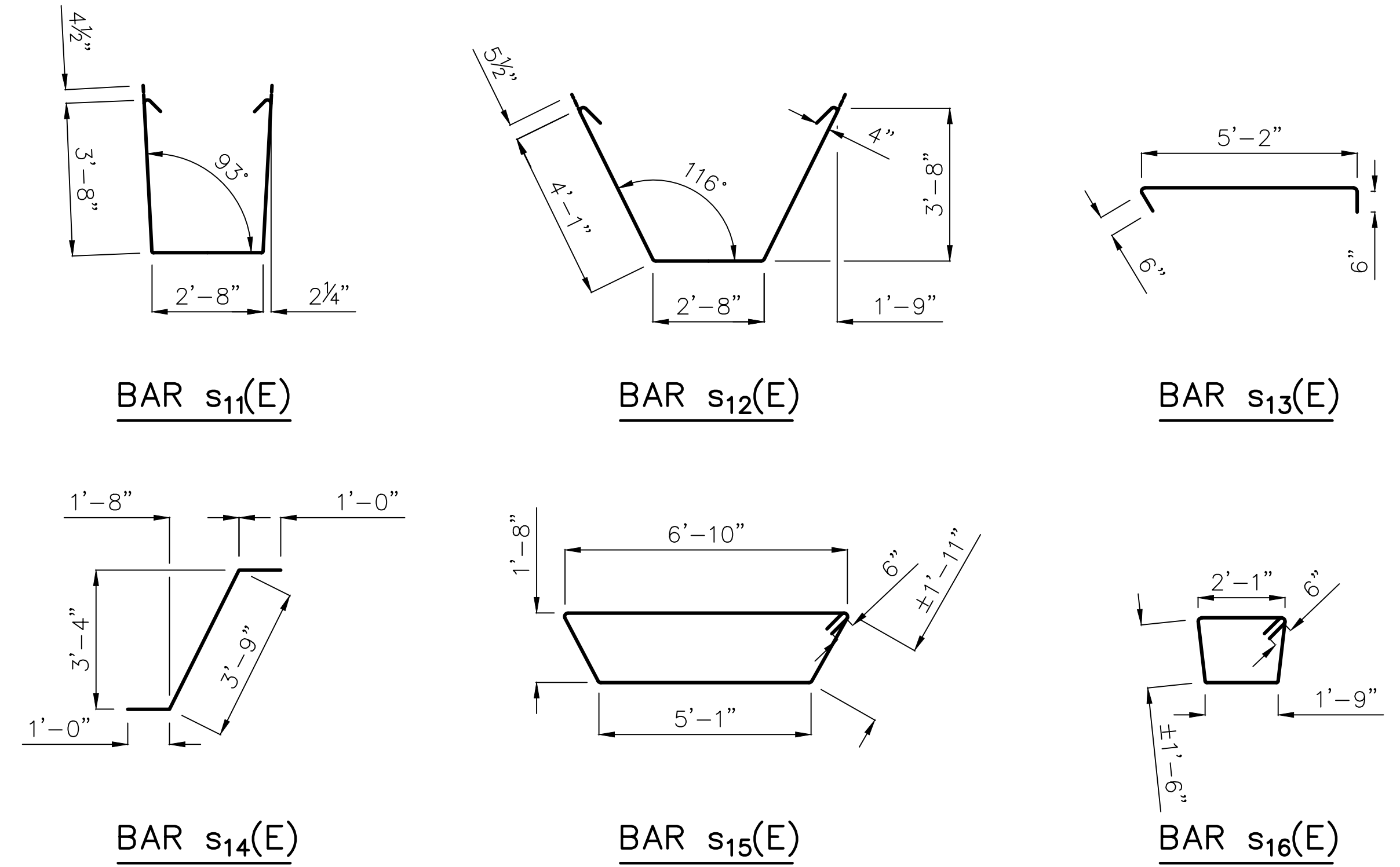
SECTION A-A



SECTION B-B



SECTION C-C



Notes:
1. For Cast-in Railing Anchor Assembly, see sheet S-24 of S-87.

SECTION NO.: 12-E9121-00-BT
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ADDISON UNDERBRIDGE TRAIL CONNECTION FROM
CLARK PARK TO CALIFORNIA PARK
STRUCTURE NO. 016-P025

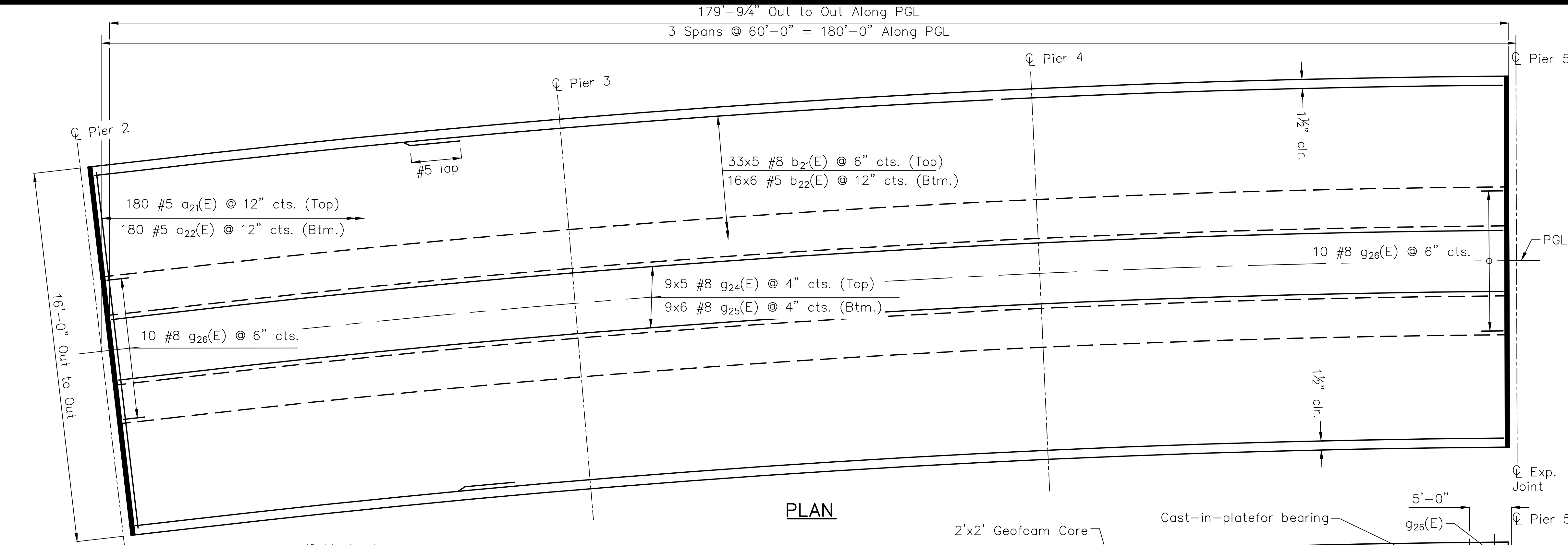
SUPERSTRUCTURE CROSS SECTIONS - SEGMENT A

SHEET NO.
S-12

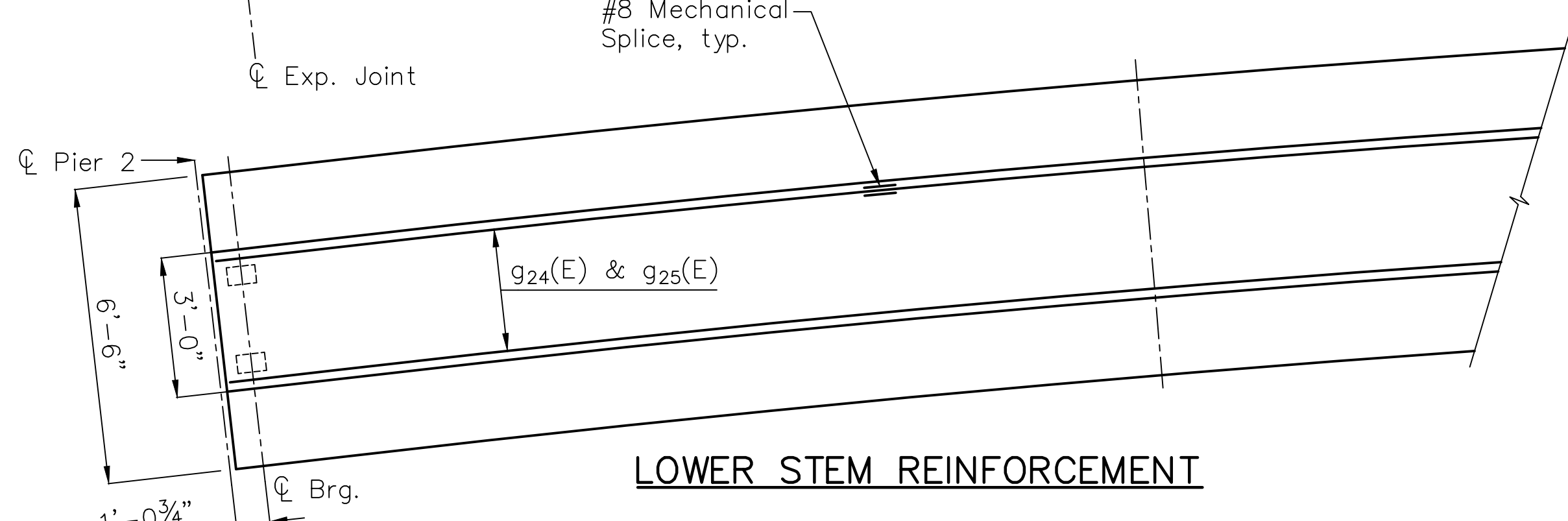
SHEET 59 OF 158

REVISION

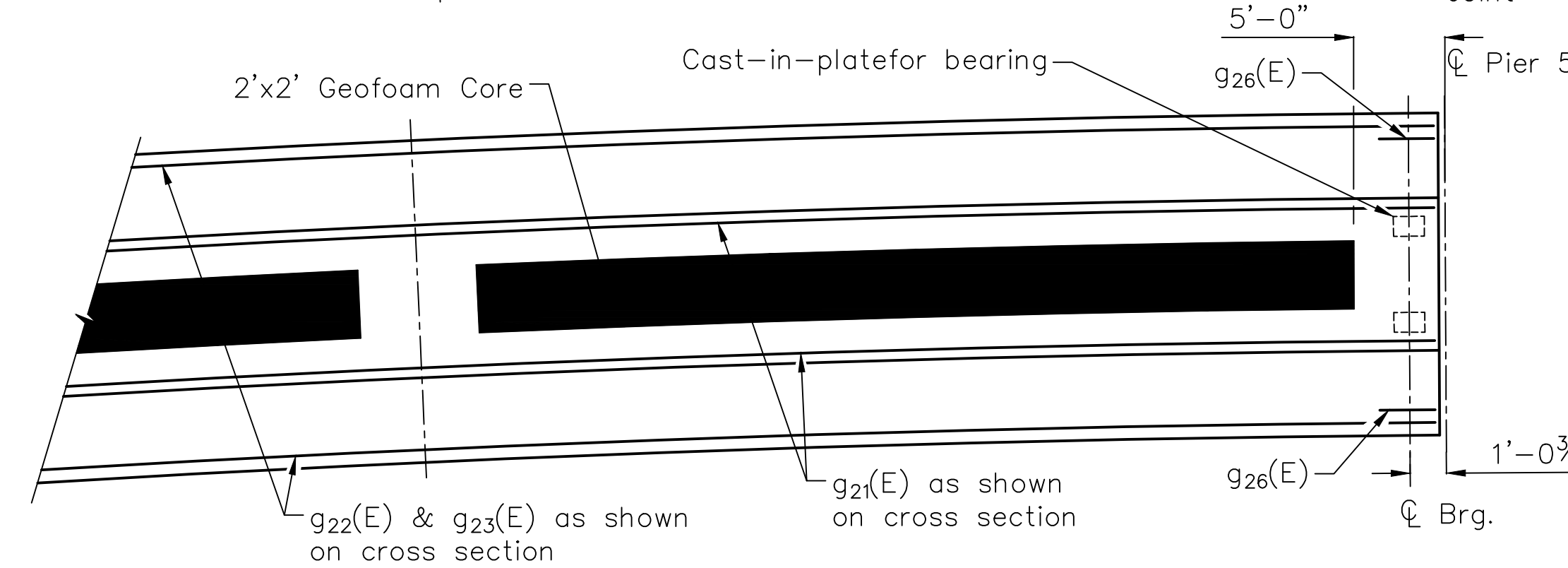
179'-9 1/4" Out to Out Along PGL
3 Spans @ 60'-0" = 180'-0" Along PGL



PLAN

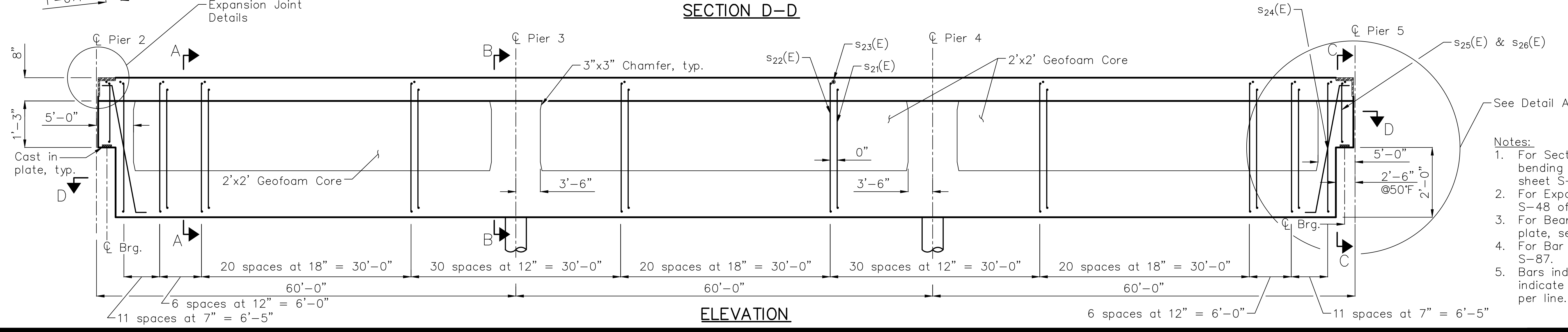


LOWER STEM REINFORCEMENT



UPPER STEM REINFORCEMENT

SECTION D-D



ELEVATION

SEGMENT B
BILL OF MATERIALS

Bar	No.	Size	Length	Shape
a21(E)	180	#5	15'-9"	—
a22(E)	180	#5	15'-9"	—
b21(E)	165	#8	35'-10"	—
b22(E)	96	#5	31'-0"	—
g21(E)	36	#8	29'-1"	—
g22(E)	30	#8	35'-10"	—
g23(E)	20	#8	34'-11"	—
g24(E)	45	#8	34'-11"	—
g25(E)	54	#8	29'-1"	—
g26(E)	10	#8	4'-7"	—
s21(E)	155	#4	10'-11"	U
s22(E)	155	#4	11'-9"	U
s23(E)	310	#4	6'-2"	U
s24(E)	16	#5	5'-9"	S
s25(E)	20	#4	16'-9"	U
s26(E)	20	#4	8'-2"	U
Reinforcement Bars, Epoxy Coated			Pound	44,940
High Strength High Performance Concrete Superstructures			Cu. Yd.	150
Geof foam (Polystyrene Fill)			Cu. Yd.	23.2

Minimum Lap Lengths	
bar Size to be Spliced	Length
#4**	1'-1"
#5	2'-1"

** #4 spiral bars shall have a minimum lap length of 3'-0".

Notes:

- For Sections A-A, B-B, & C-C bar bending diagrams and Detail A, see sheet S-14 of S-87.
- For Expansion Joint Details, see sheet S-48 of S-87.
- For Bearing Details & Cast-in-steel plate, see sheet S-49 of S-87.
- For Bar Splicers, see sheet S-67 of S-87.
- Bars indicated thus 100 x 2 - #5, etc. indicate 100 lines of bars with 2 lengths per line.

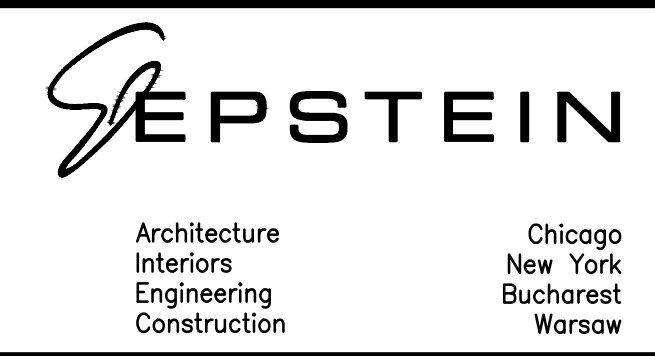
SECTION NO.: 12-E9121-00-BT
DRAWN BY: N. FIRNBACH
CHECKED BY: R. VARMA
APPROVED BY: P. DOMBROWSKI
DATE: 8/12/2016
SCALE: N.T.S.

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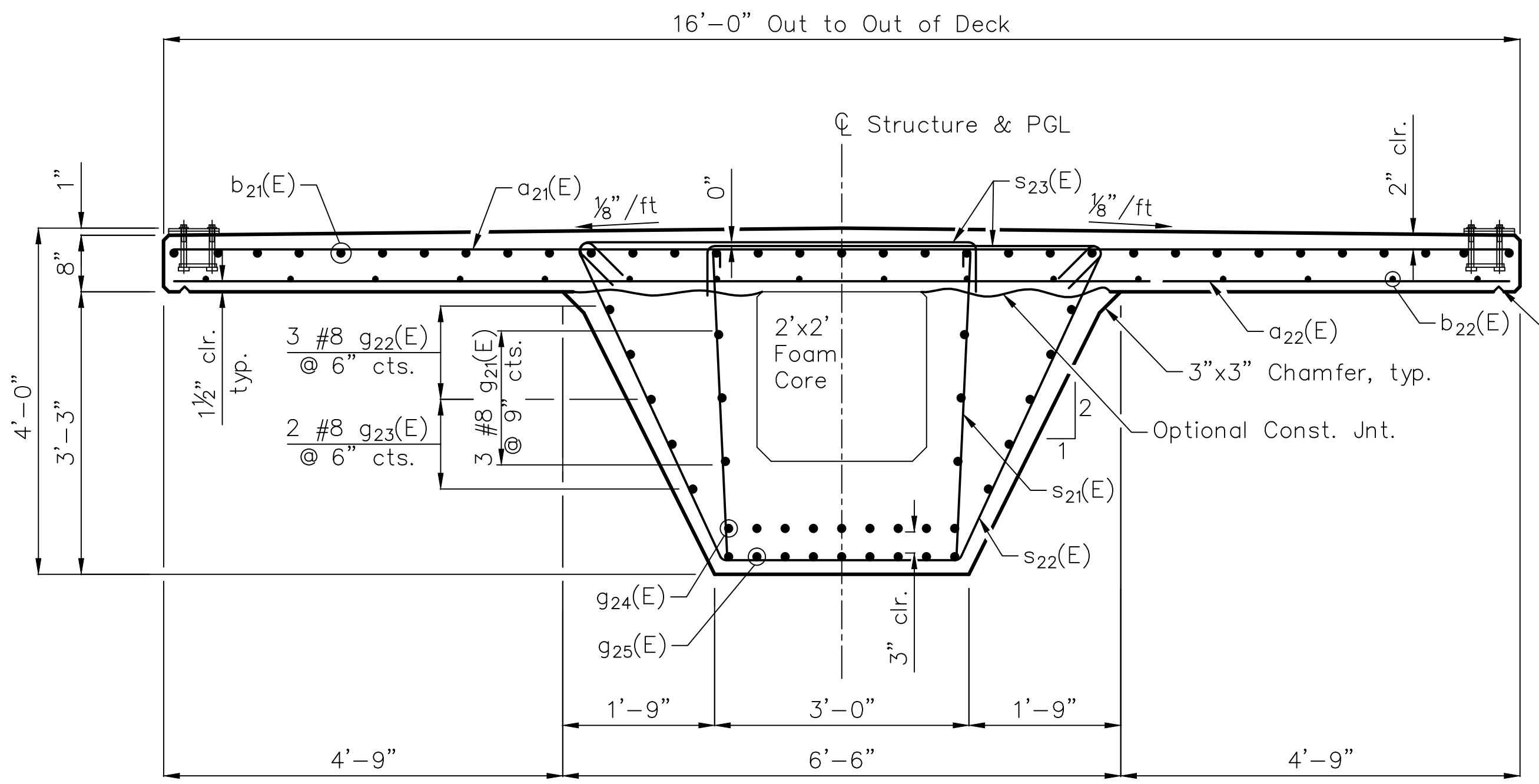
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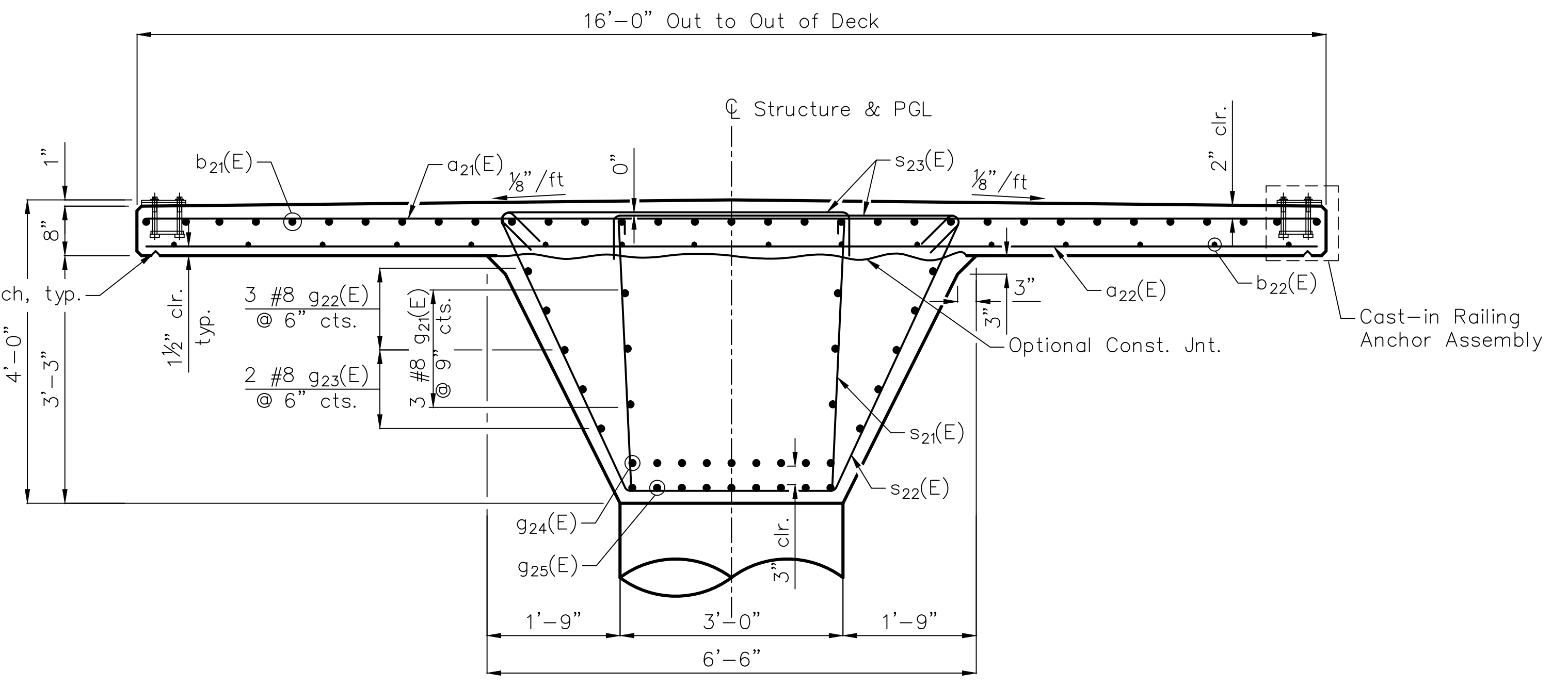
ADDISON UNDERBRIDGE TRAIL CONNECTION FROM
CLARK PARK TO CALIFORNIA PARK
STRUCTURE NO. 016-P025

SUPERSTRUCTURE PLAN AND ELEVATION - SEGMENT B

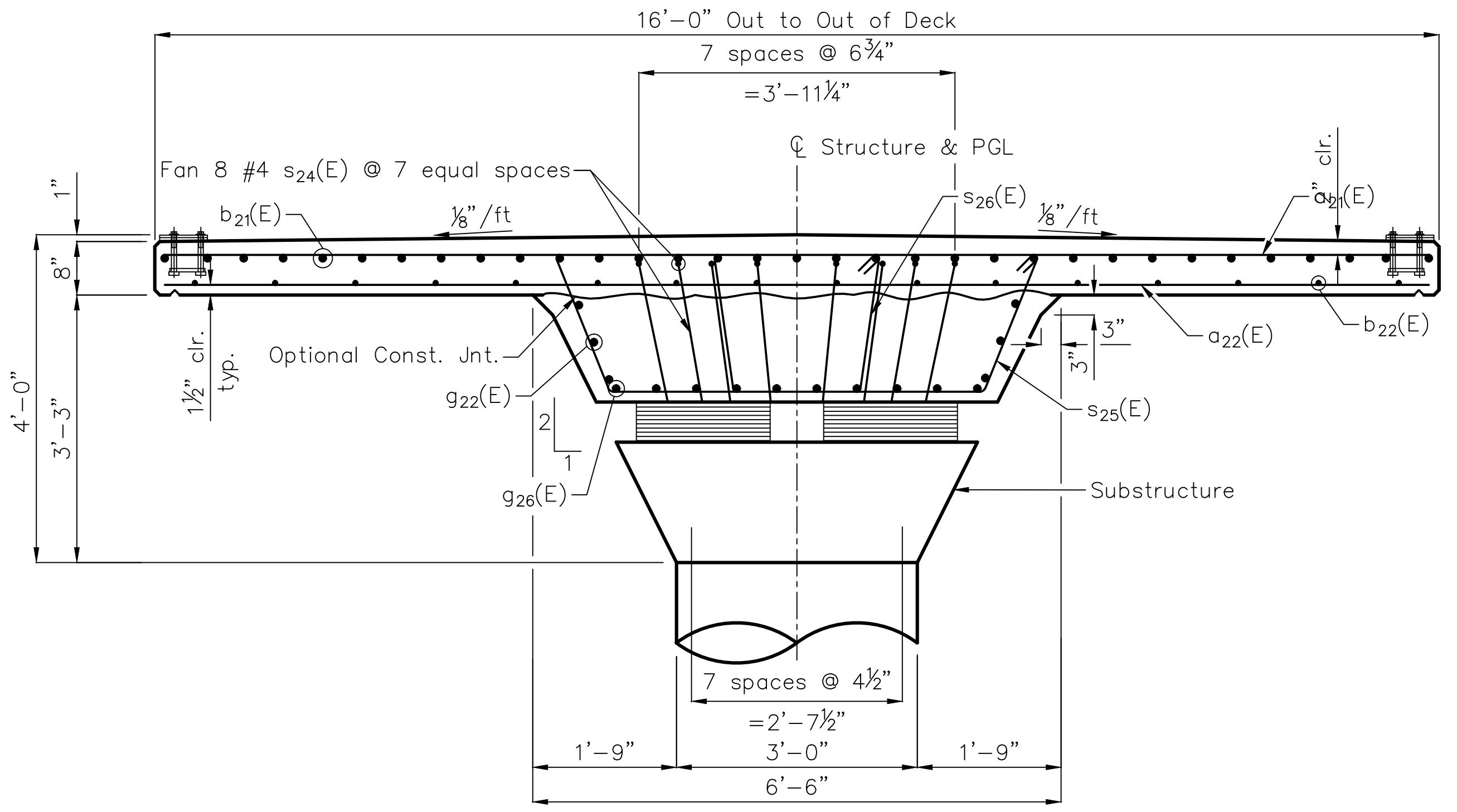
SHEET NO.
S-13
SHEET 60 OF 158



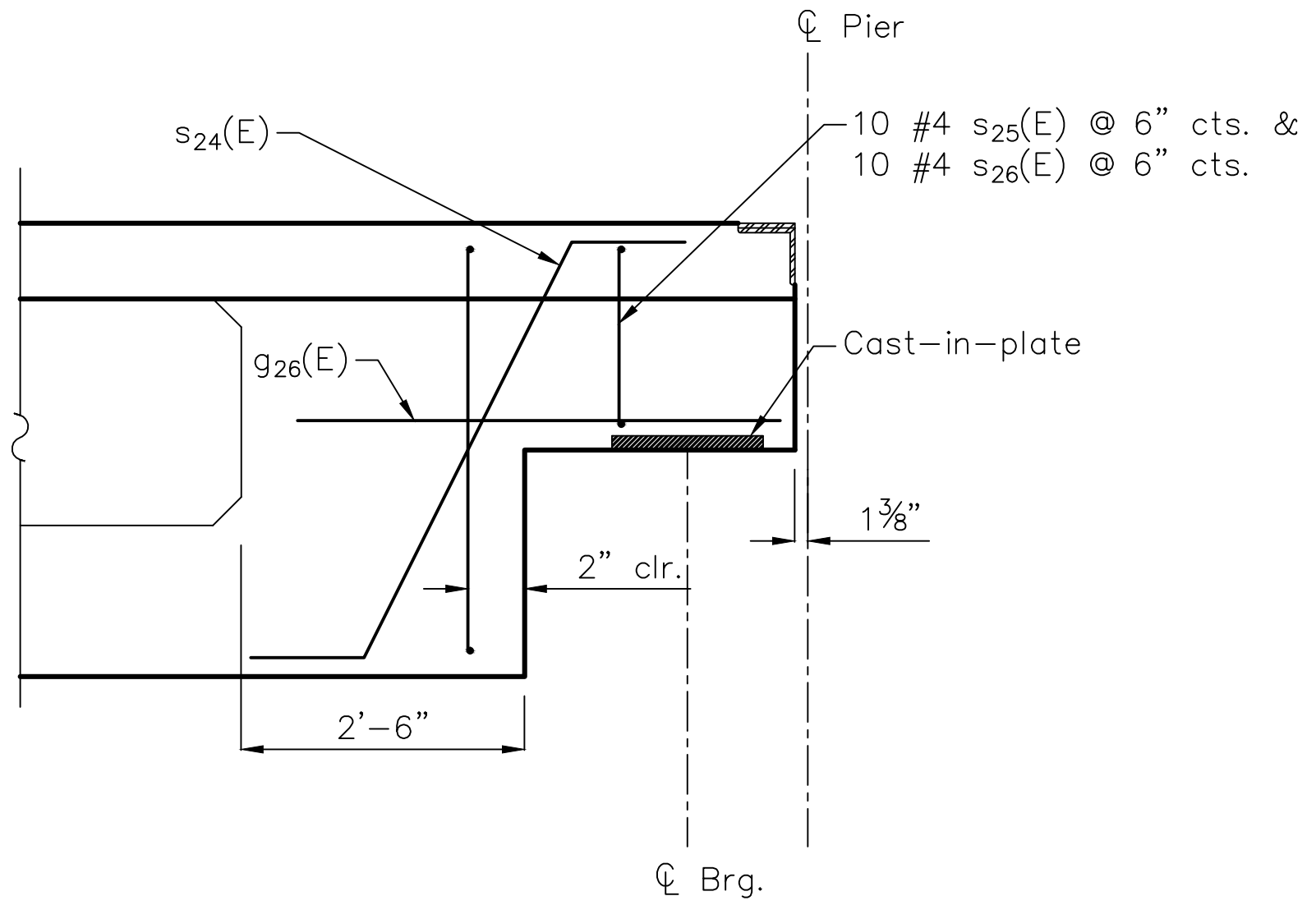
SECTION A-A



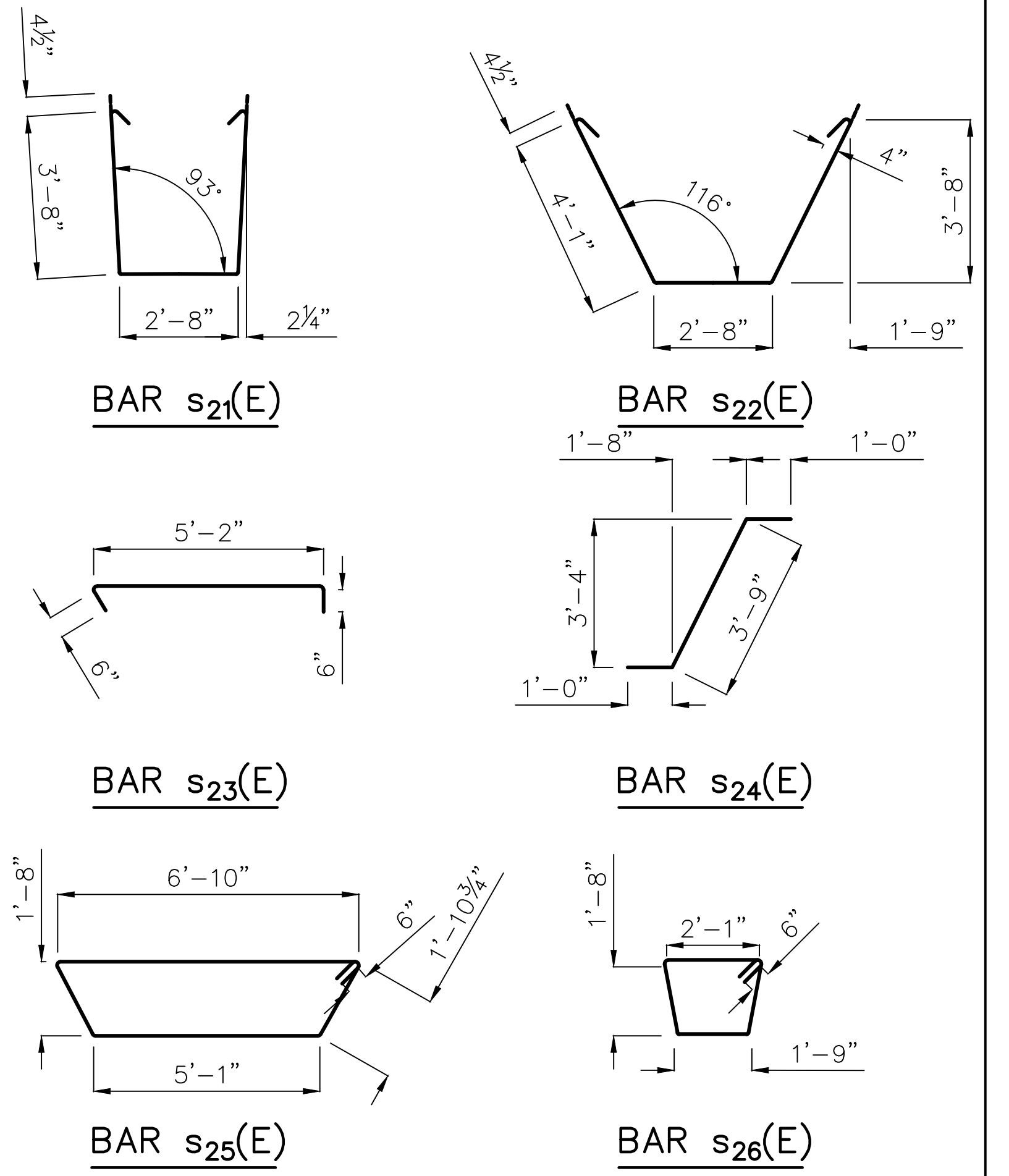
SECTION B-B



SECTION C-C



DETAIL A
* Similar for all Dapped End



Notes:
1. For Cast-in Railing Anchor Assembly, see sheet S-24 of S-87.

January 28, 2016 10:48:57 AM
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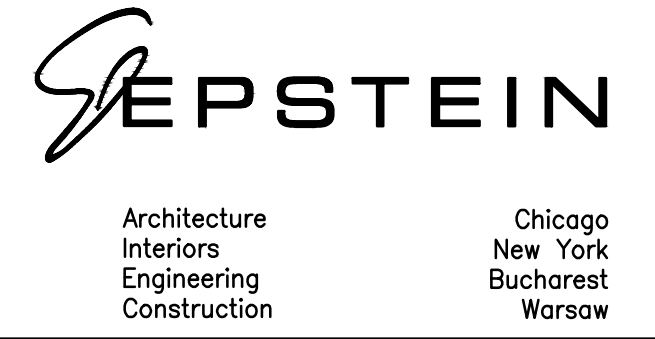
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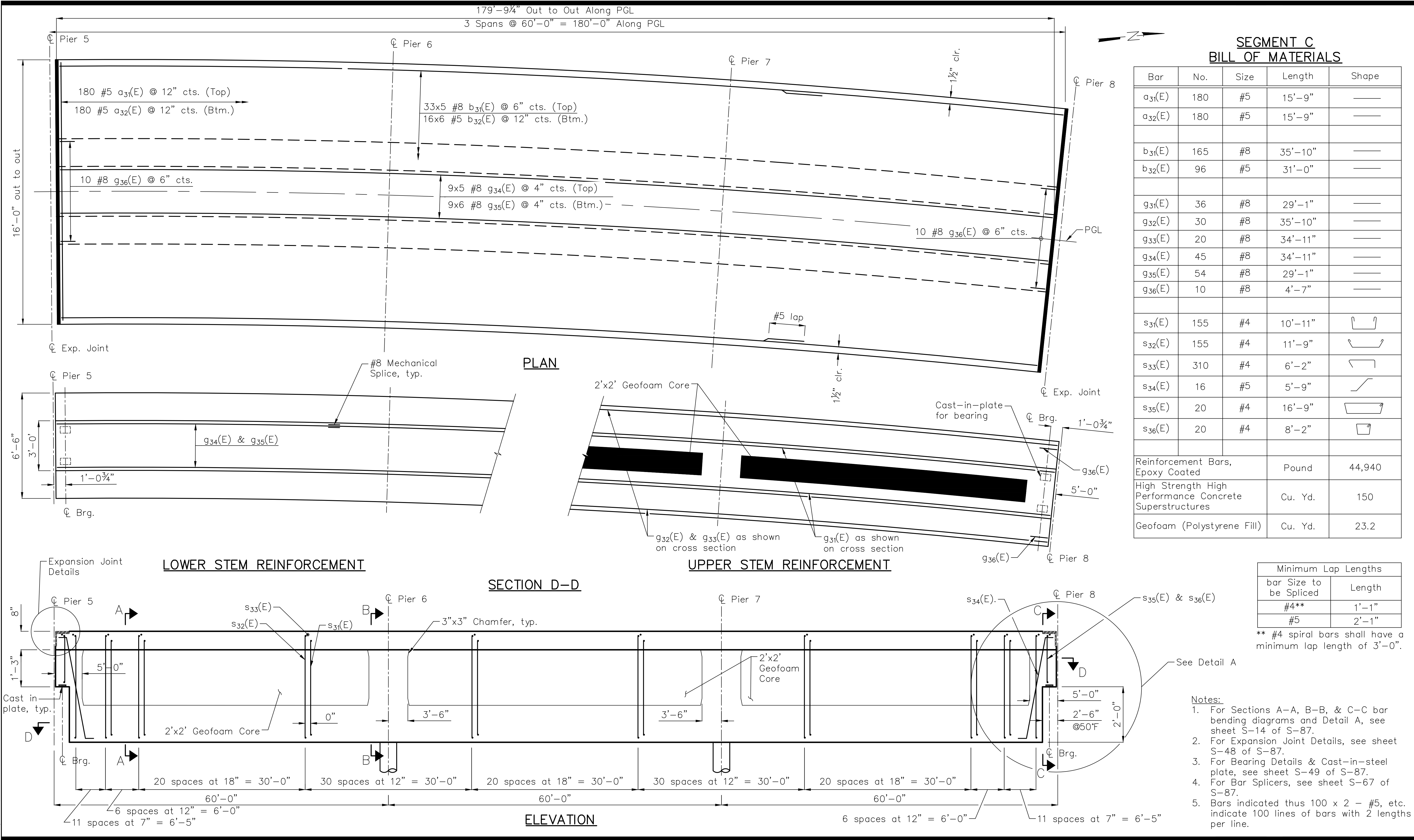
ADDISON UNDERBRIDGE TRAIL CONNECTION FROM
 CLARK PARK TO CALIFORNIA PARK
 STRUCTURE NO. 016-P025

SUPERSTRUCTURE CROSS SECTIONS - SEGMENT B

SHEET NO.
 S-14

SHEET 61 OF 158

REVISION



**SEGMENT C
BILL OF MATERIALS**

Bar	No.	Size	Length	Shape
a ₃₁ (E)	180	#5	15'-9"	—
a ₃₂ (E)	180	#5	15'-9"	—
b ₃₁ (E)	165	#8	35'-10"	—
b ₃₂ (E)	96	#5	31'-0"	—
g ₃₁ (E)	36	#8	29'-1"	—
g ₃₂ (E)	30	#8	35'-10"	—
g ₃₃ (E)	20	#8	34'-11"	—
g ₃₄ (E)	45	#8	34'-11"	—
g ₃₅ (E)	54	#8	29'-1"	—
g ₃₆ (E)	10	#8	4'-7"	—
s ₃₁ (E)	155	#4	10'-11"	⌋
s ₃₂ (E)	155	#4	11'-9"	⌋
s ₃₃ (E)	310	#4	6'-2"	⌋
s ₃₄ (E)	16	#5	5'-9"	⌋
s ₃₅ (E)	20	#4	16'-9"	⌋
s ₃₆ (E)	20	#4	8'-2"	⌋
Reinforcement Bars, Epoxy Coated			Pound	44,940
High Strength High Performance Concrete Superstructures			Cu. Yd.	150
Geofoam (Polystyrene Fill)			Cu. Yd.	23.2

Minimum Lap Lengths	
bar Size to be Spliced	Length
#4**	1'-1"
#5	2'-1"

** #4 spiral bars shall have a minimum lap length of 3'-0".

Notes:

- For Sections A-A, B-B, & C-C bar bending diagrams and Detail A, see sheet S-14 of S-87.
- For Expansion Joint Details, see sheet S-48 of S-87.
- For Bearing Details & Cast-in-steel plate, see sheet S-49 of S-87.
- For Bar Splicers, see sheet S-67 of S-87.
- Bars indicated thus 100 x 2 - #5, etc. indicate 100 lines of bars with 2 lengths per line.

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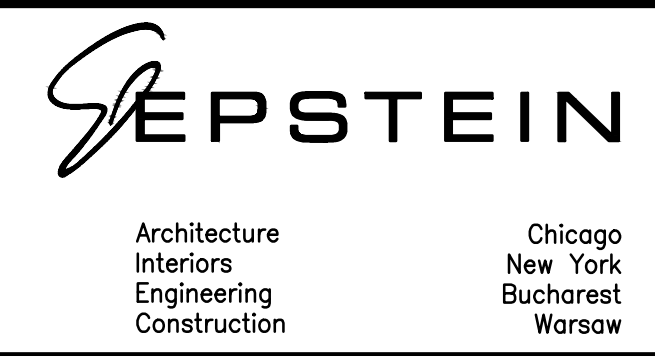
SECTION NO.: 12-E9121-00-BT
 DRAWN BY: N. FIRNBACH
 CHECKED BY: R. VARMA
 APPROVED BY: P. DOMBROWSKI
 DATE: 8/12/2016
 SCALE: N.T.S.

NO.	DATE	REVISIONS/ISSUANCES
1	8/12/16	ISSUED FOR FINAL
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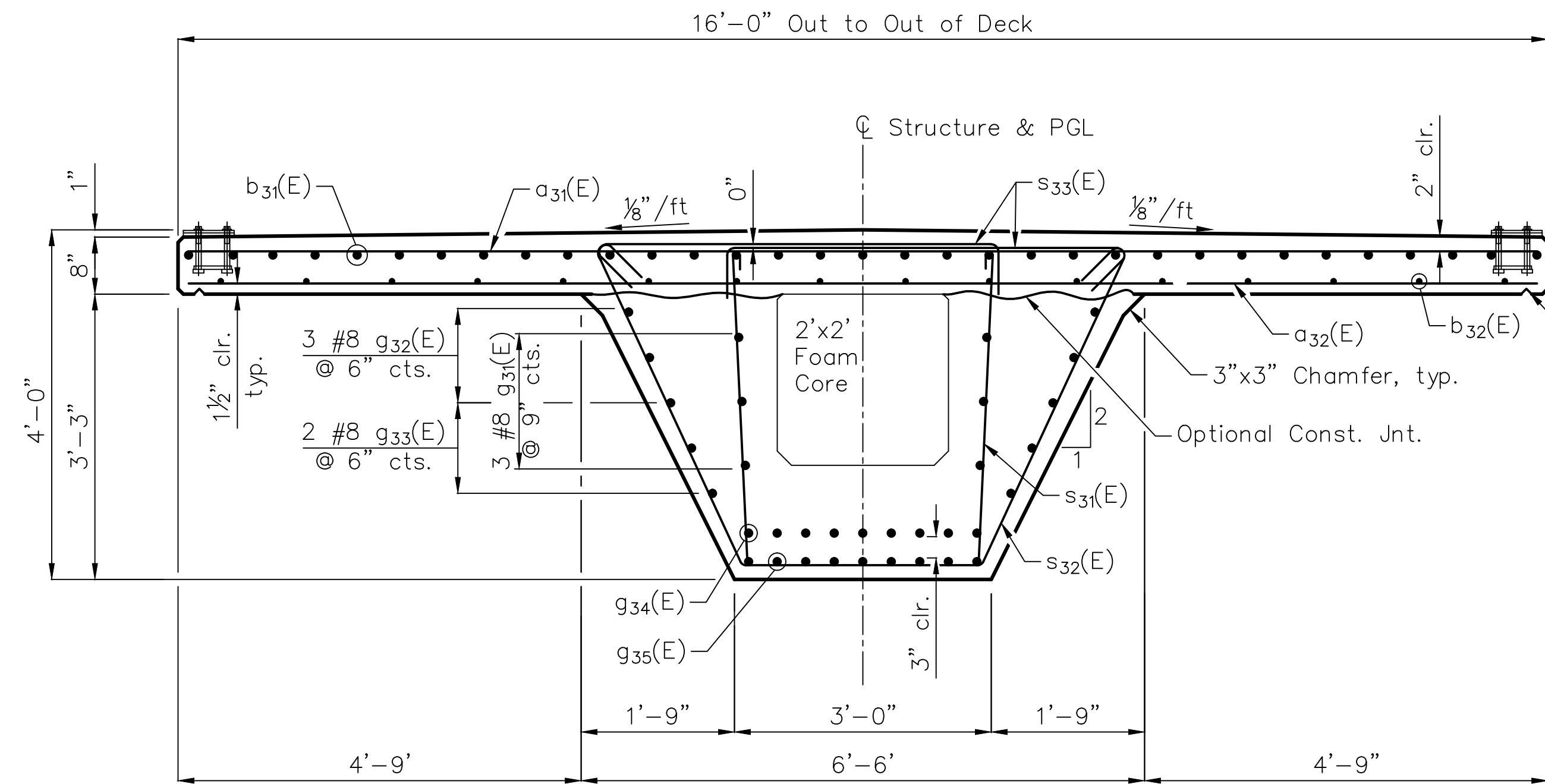


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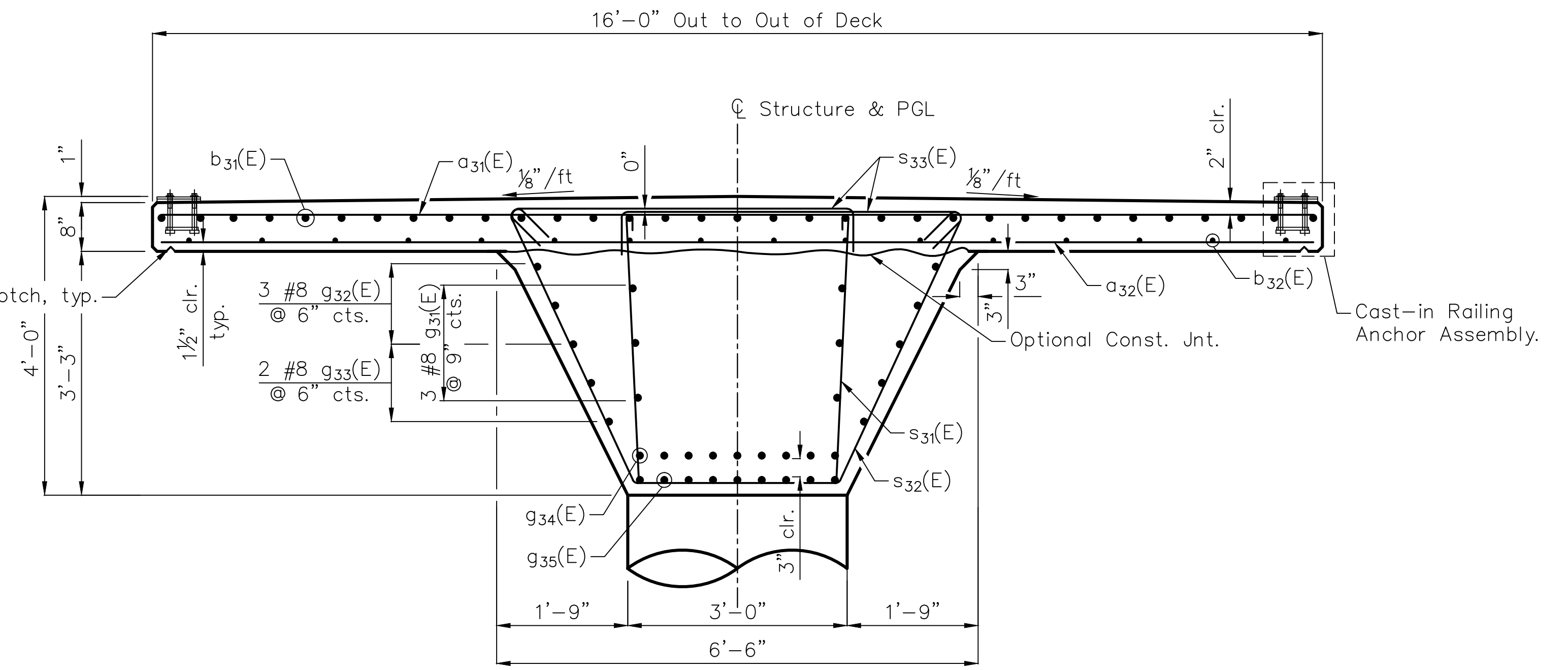


ADDISON UNDERBRIDGE TRAIL CONNECTION FROM
 CLARK PARK TO CALIFORNIA PARK
 STRUCTURE NO. 016-P025
 SUPERSTRUCTURE PLAN AND ELEVATION - SEGMENT C

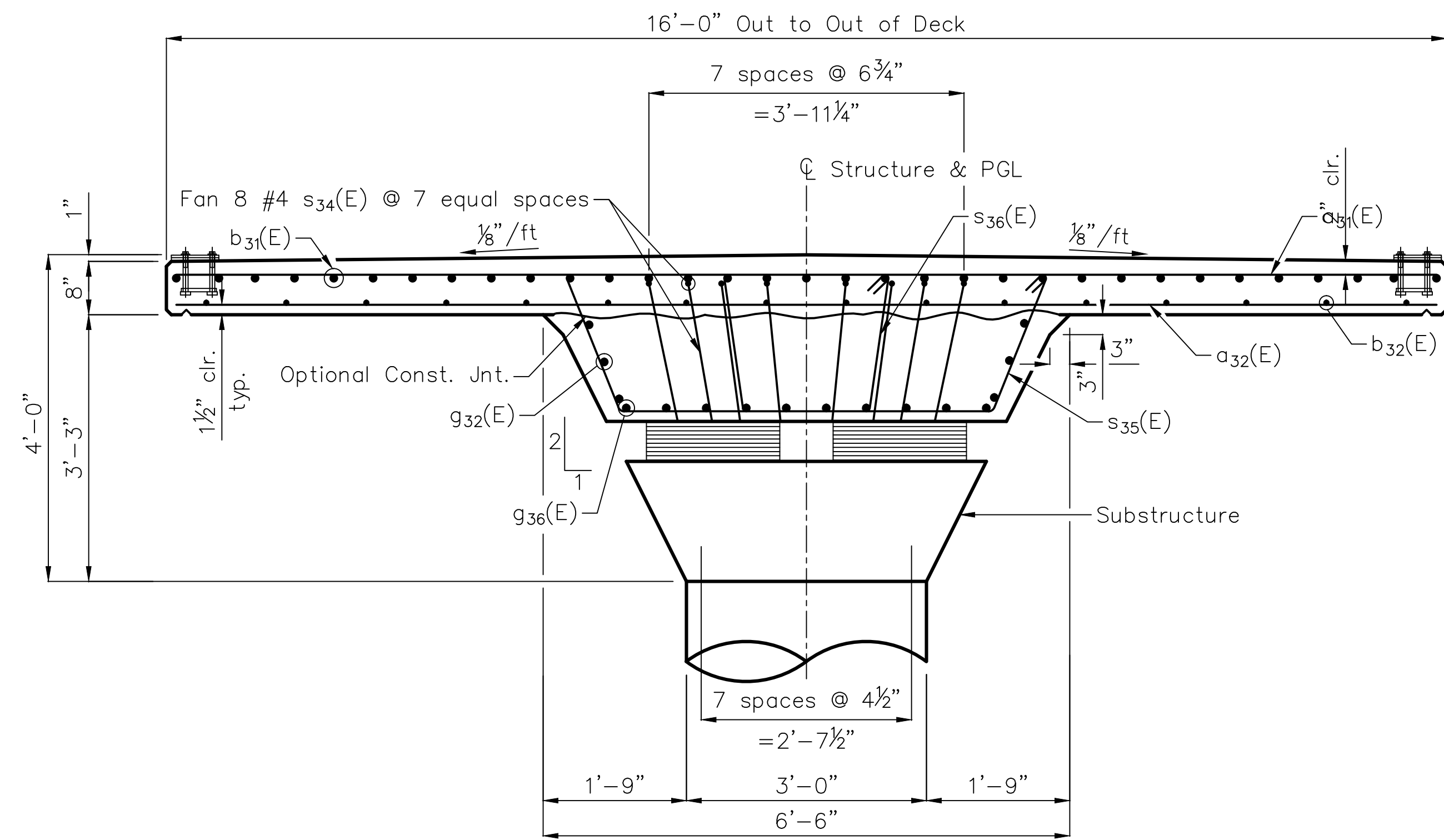
SHEET NO. S-15
 SHEET 62 OF 158
 REVISION



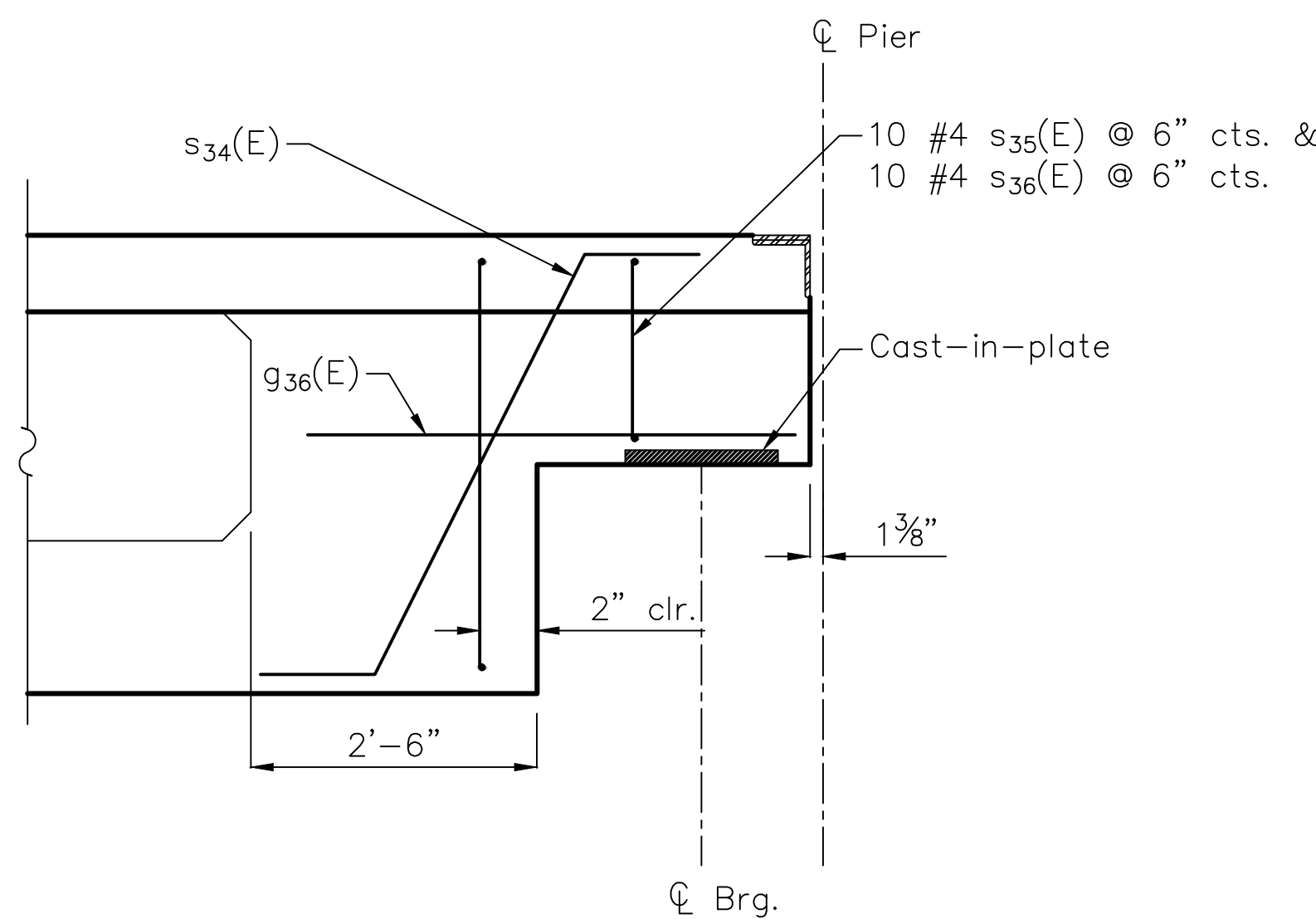
SECTION A-A



SECTION B-B

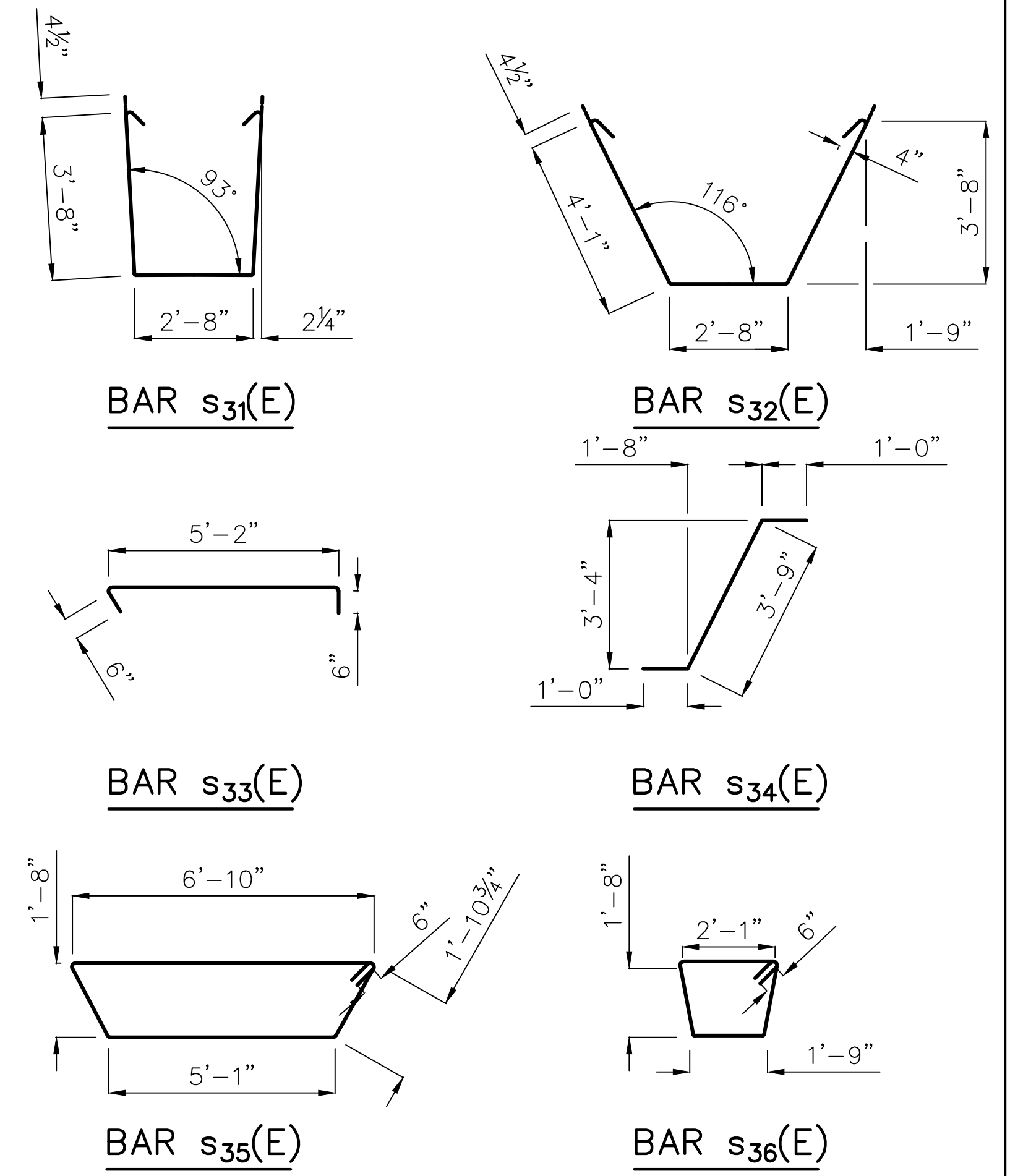


SECTION C-C



DETAIL A

* Similar for all Dapped End



Notes:
1. For Cast-in Railing Anchor Assembly, see sheet S-24 of S-87.

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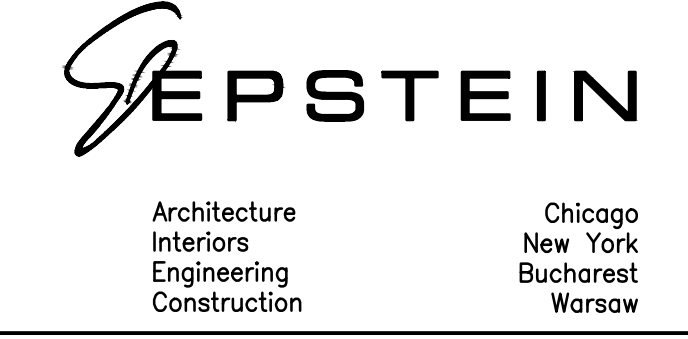
SECTION NO.: 12-E9121-00-BT
 DRAWN BY: N. FIRNBACH
 CHECKED BY: R. VARMA
 APPROVED BY: P. DOMBROWSKI
 DATE: 8/12/2016
 SCALE: N.T.S.

NO.	DATE	REVISIONS/ISSUANCES
1	8/12/16	ISSUED FOR FINAL
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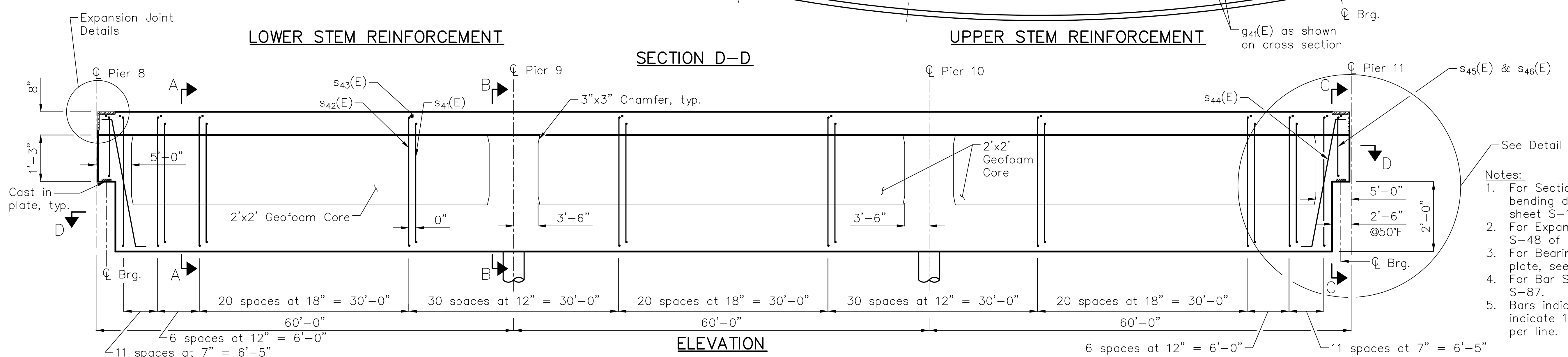
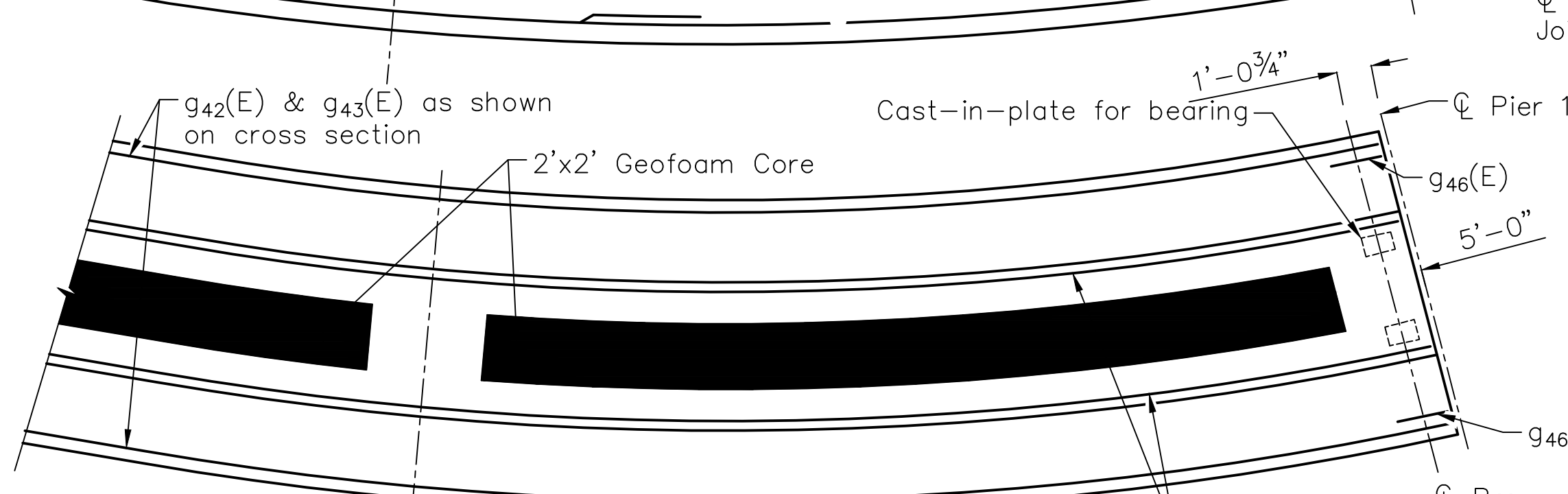
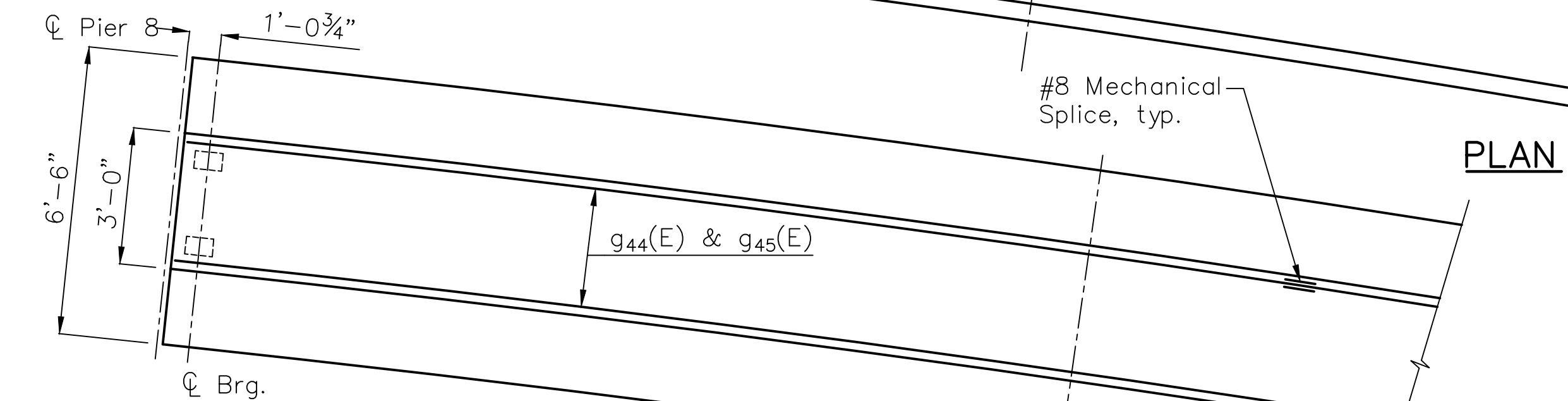
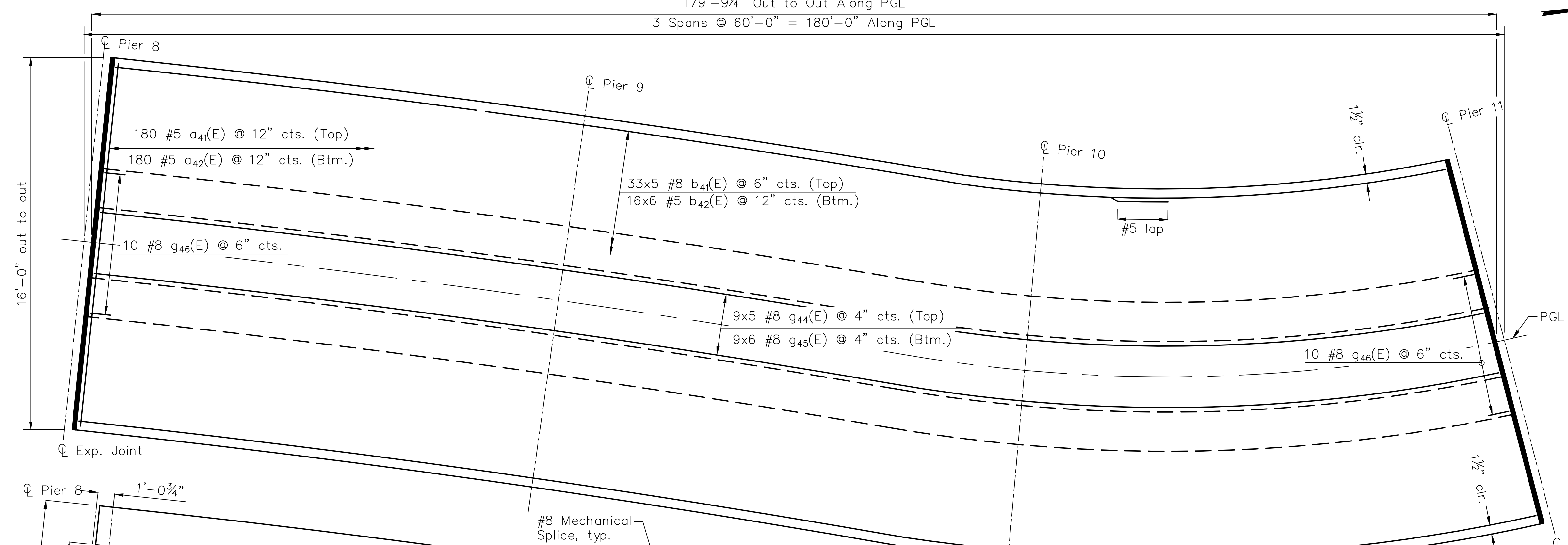
ADDISON UNDERBRIDGE TRAIL CONNECTION FROM CLARK PARK TO CALIFORNIA PARK
 STRUCTURE NO. 016-P025
 SUPERSTRUCTURE CROSS SECTIONS - SEGMENT C

SHEET NO. S-16	REVISION
SHEET 63 OF 158	

179'-9 1/4" Out to Out Along PGL
3 Spans @ 60'-0" = 180'-0" Along PGL

SEGMENT D
BILL OF MATERIALS

Bar	No.	Size	Length	Shape
a ₄₁ (E)	180	#5	15'-9"	—
a ₄₂ (E)	180	#5	15'-9"	—
b ₄₁ (E)	165	#8	35'-10"	—
b ₄₂ (E)	96	#5	31'-0"	—
g ₄₁ (E)	36	#8	29'-1"	—
g ₄₂ (E)	30	#8	35'-10"	—
g ₄₃ (E)	20	#8	34'-11"	—
g ₄₄ (E)	45	#8	34'-11"	—
g ₄₅ (E)	54	#8	29'-1"	—
g ₄₆ (E)	10	#8	4'-7"	—
s ₄₁ (E)	155	#4	10'-11"	U
s ₄₂ (E)	155	#4	11'-9"	U
s ₄₃ (E)	310	#4	6'-2"	U
s ₄₄ (E)	16	#4	5'-9"	U
s ₄₅ (E)	20	#4	16'-9"	U
s ₄₆ (E)	20	#4	8'-2"	U
Reinforcement Bars, Epoxy Coated			Pound	44,940
High Strength High Performance Concrete Superstructures			Cu. Yd.	150
Geofoam (Polystyrene Fill)			Cu. Yd.	23.2



Minimum Lap Lengths	
bar Size to be Spliced	Length
#4**	1'-1"
#5	2'-1"

** #4 spiral bars shall have a minimum lap length of 3'-0".

- Notes:
- For Sections A-A, B-B, & C-C bar bending diagrams and Detail A, see sheet S-14 of S-87.
 - For Expansion Joint Details, see sheet S-48 of S-87.
 - For Bearing Details & Cast-in-steel plate, see sheet S-49 of S-87.
 - For Bar Splicers, see sheet S-67 of S-87.
 - Bars indicated thus 100 x 2 - #5, etc. indicate 100 lines of bars with 2 lengths per line.

SECTION NO.: 12-E9121-00-BT
DRAWN BY: N. FIRNBACH
CHECKED BY: R. VARMA
APPROVED BY: P. DOMBROWSKI
DATE: 8/12/2016
SCALE: N.T.S.

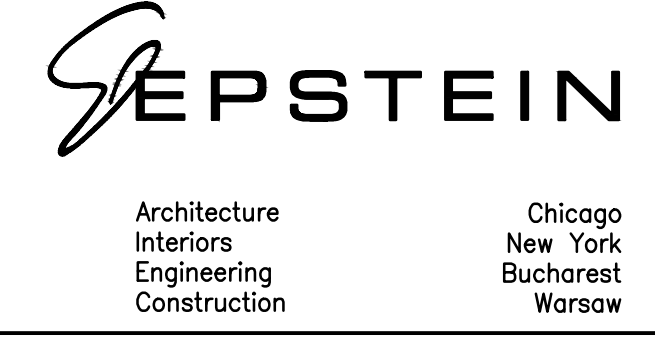
NO. DATE REVISIONS/ISSUANCES

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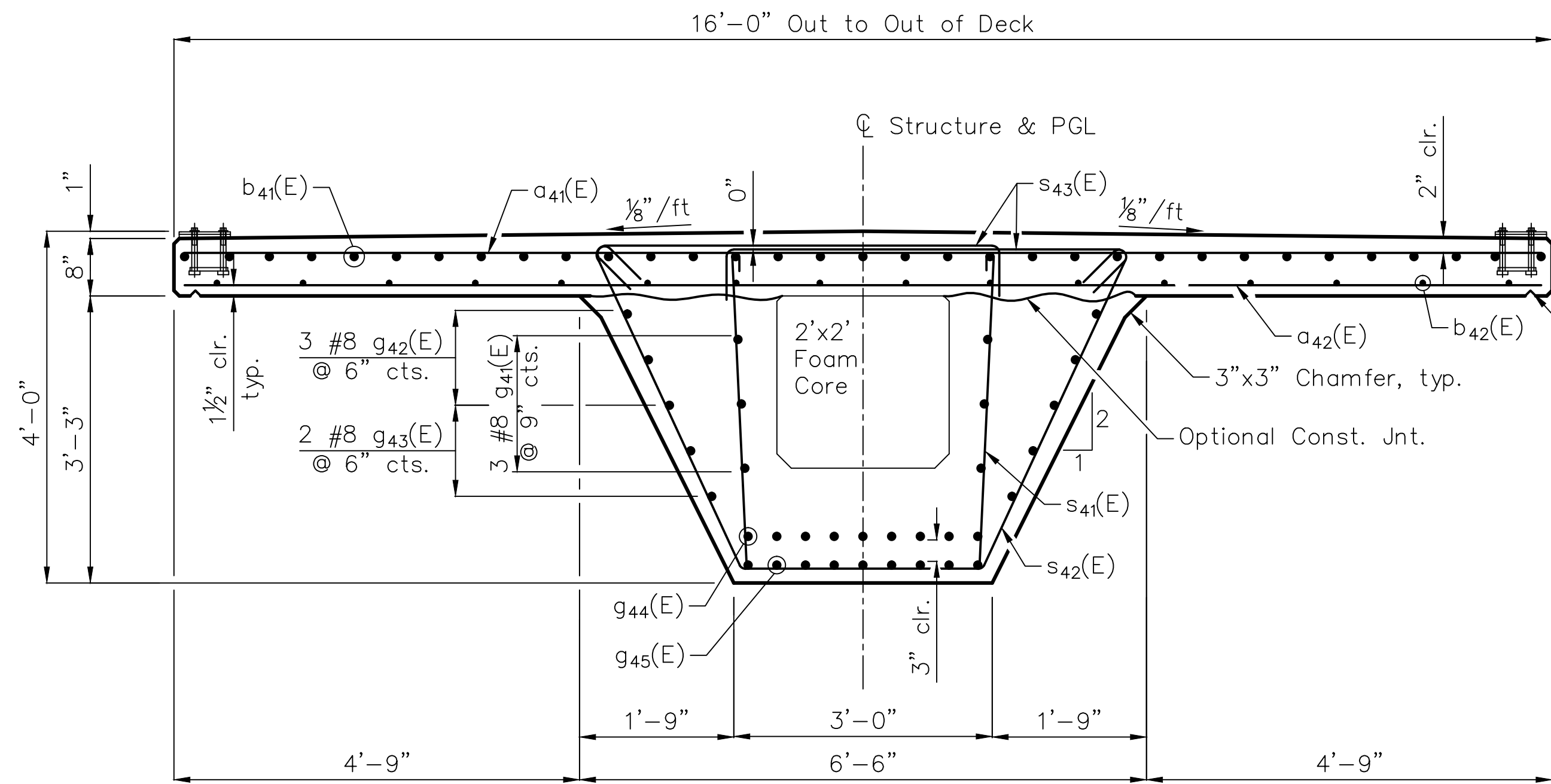


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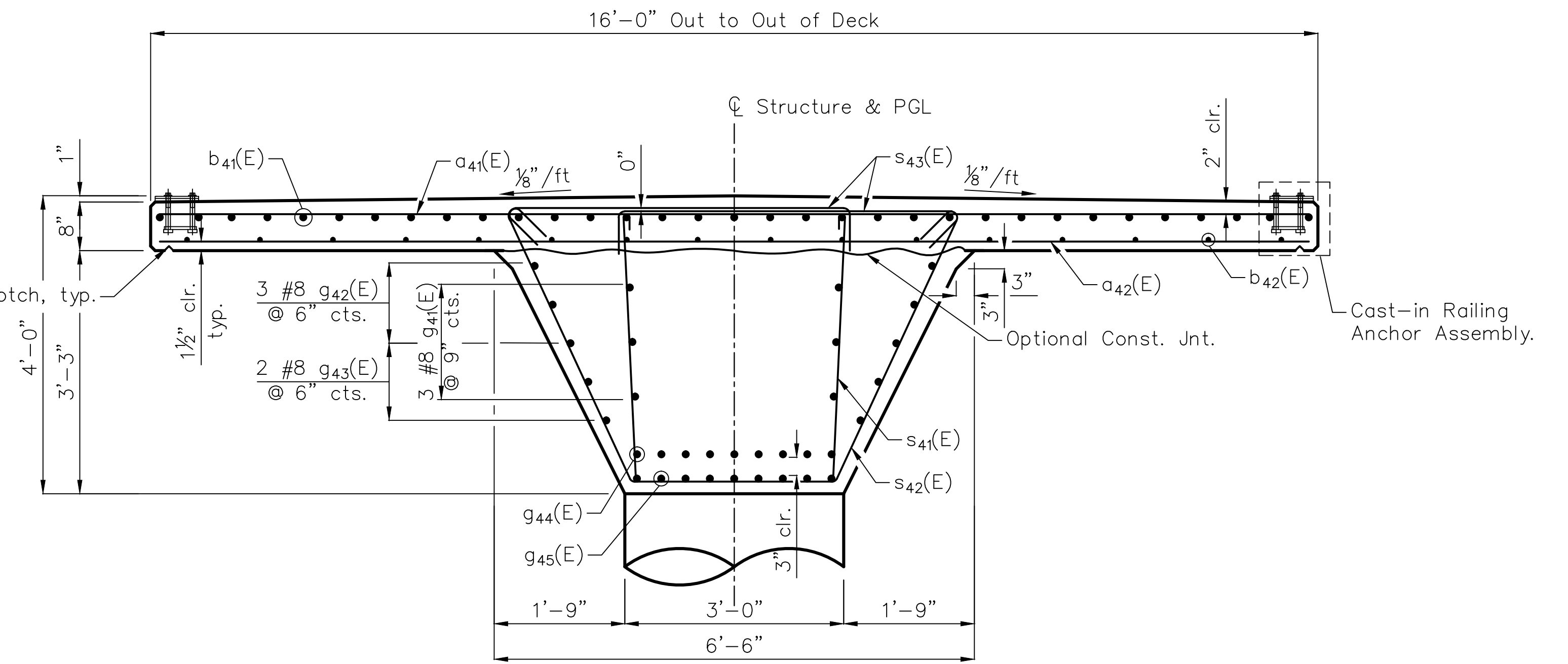


ADDISON UNDERBRIDGE TRAIL CONNECTION FROM CLARK PARK TO CALIFORNIA PARK STRUCTURE NO. 016-P025
SUPERSTRUCTURE PLAN AND ELEVATION - SEGMENT D

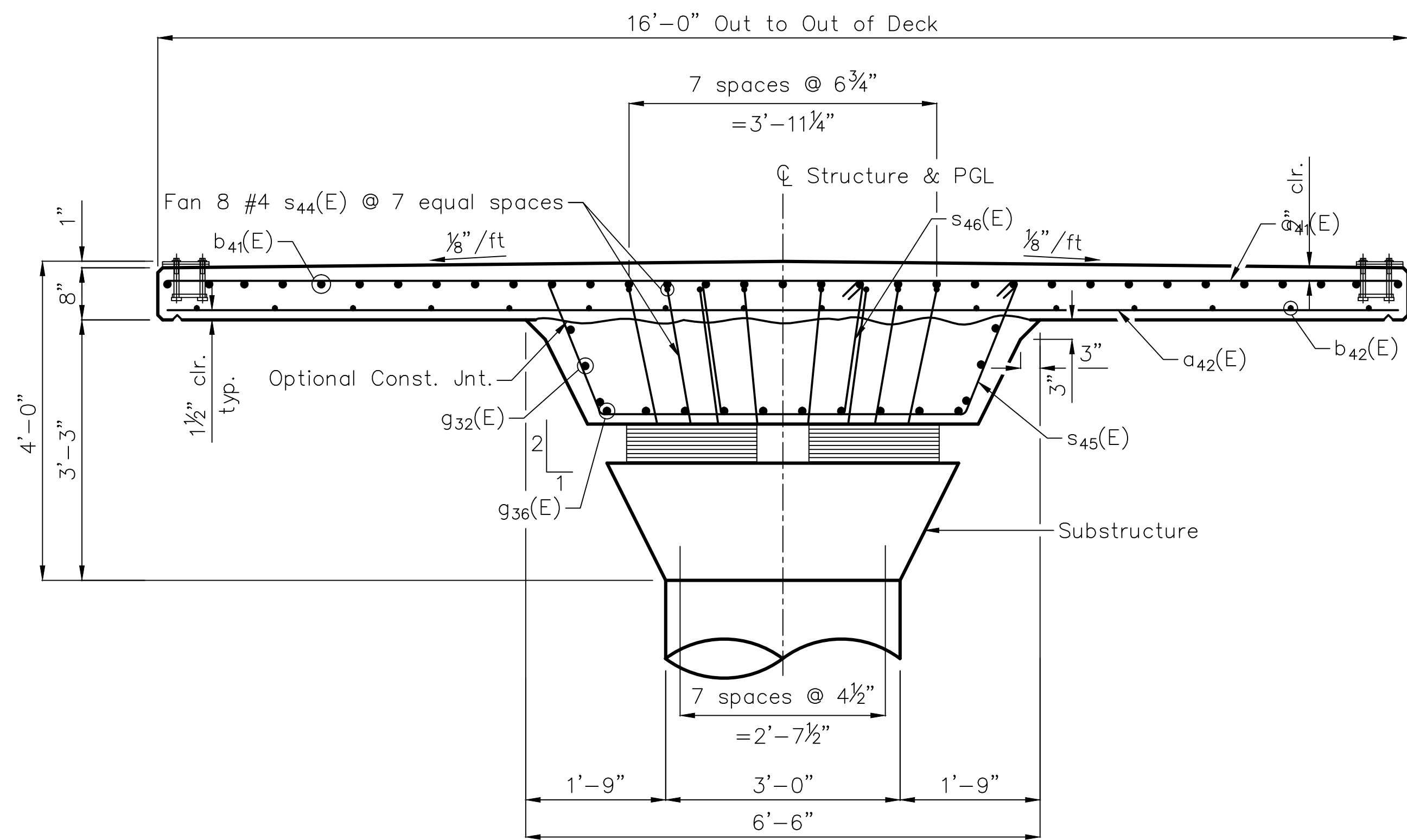
SHEET NO. S-17
SHEET 64 OF 158



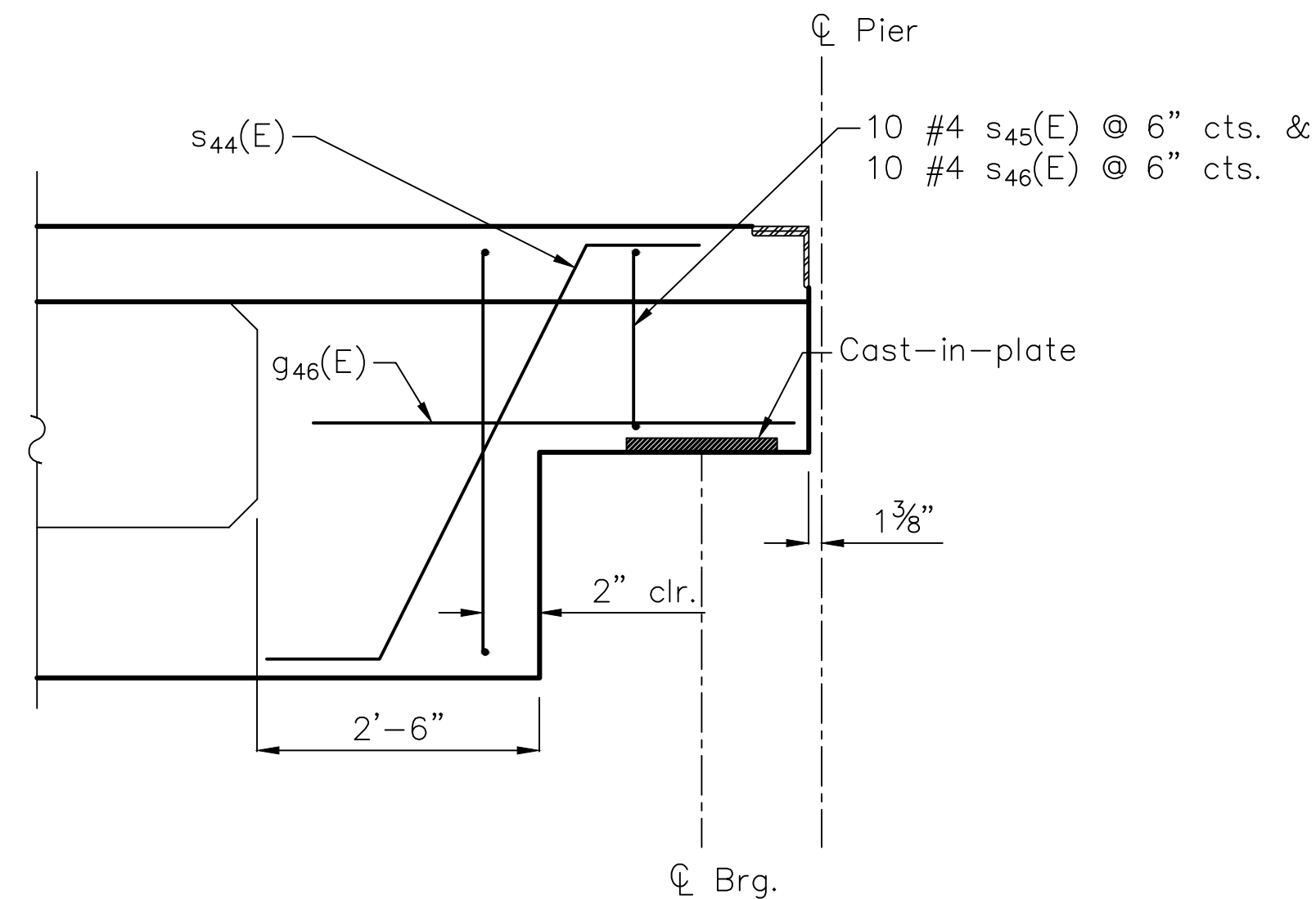
SECTION A-A



SECTION B-B

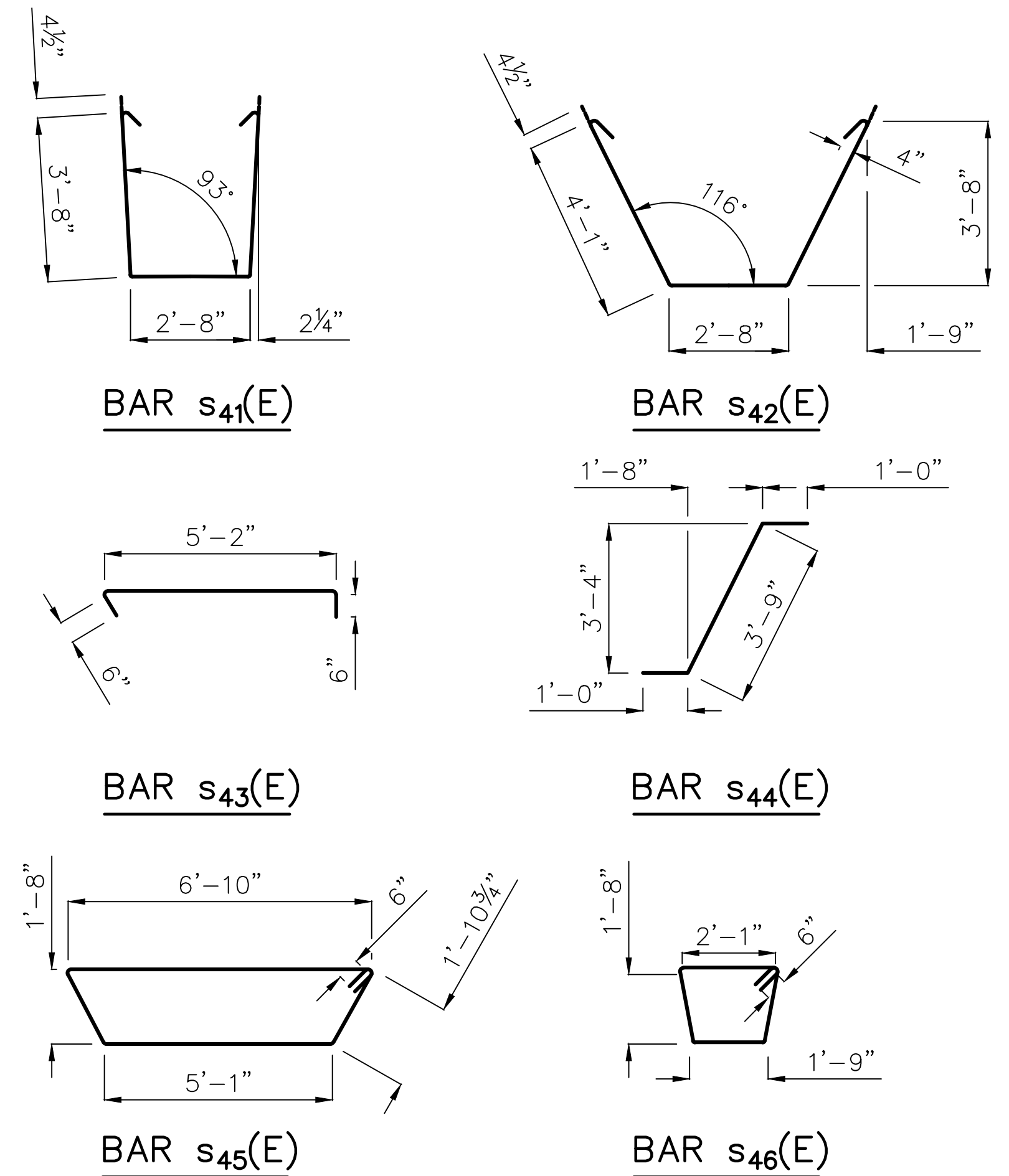


SECTION C-C



DETAIL A

* Similar for all Dapped End



Notes:

1. For Cast-in Railing Anchor Assembly, see sheet S-24 of S-87.

SECTION NO.: 12-E9121-00-BT
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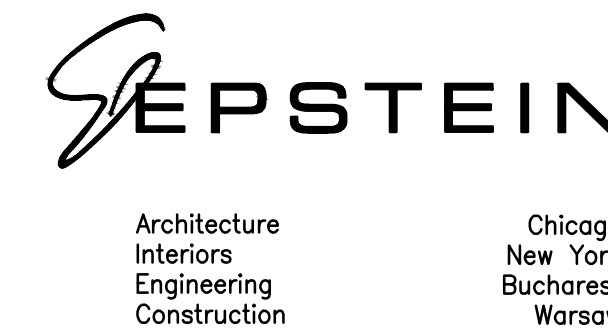
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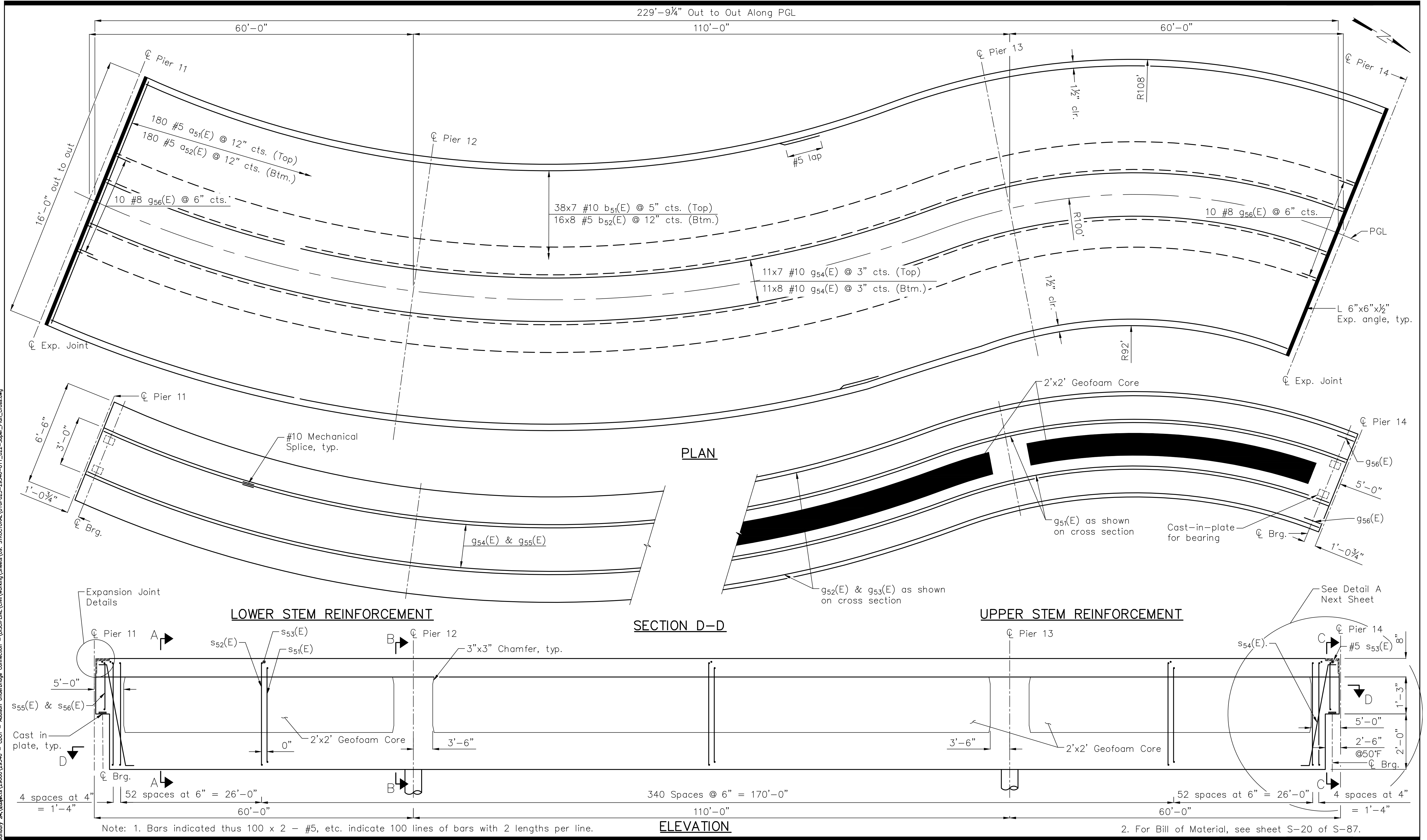
ADDISON UNDERBRIDGE TRAIL CONNECTION FROM
 CLARK PARK TO CALIFORNIA PARK
 STRUCTURE NO. 016-P025

SUPERSTRUCTURE CROSS SECTIONS - SEGMENT D

SHEET NO.
 S-18

SHEET 65 OF 158

REVISION



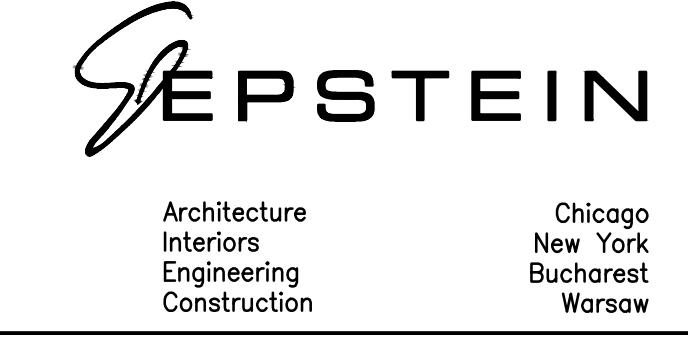
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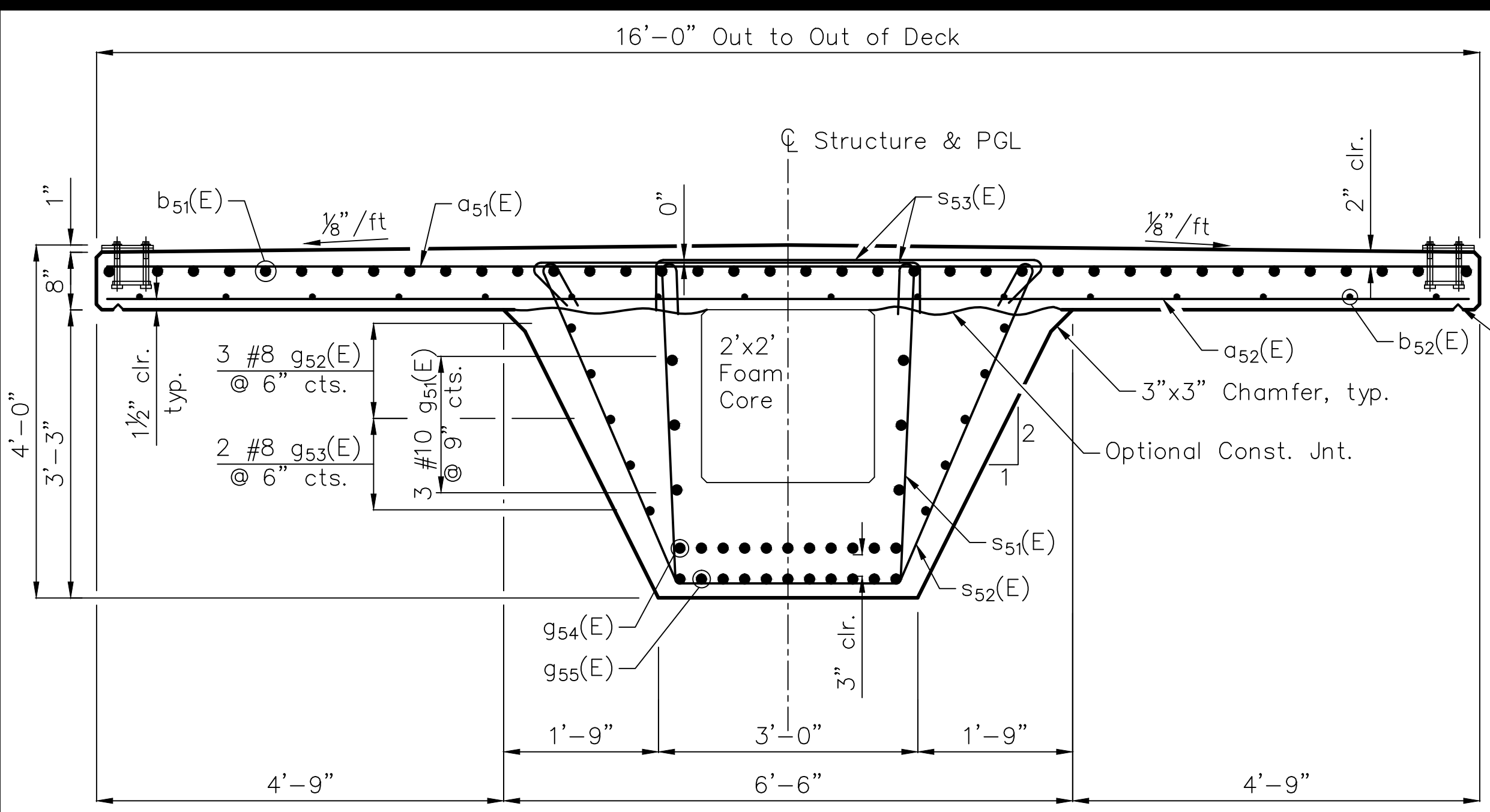
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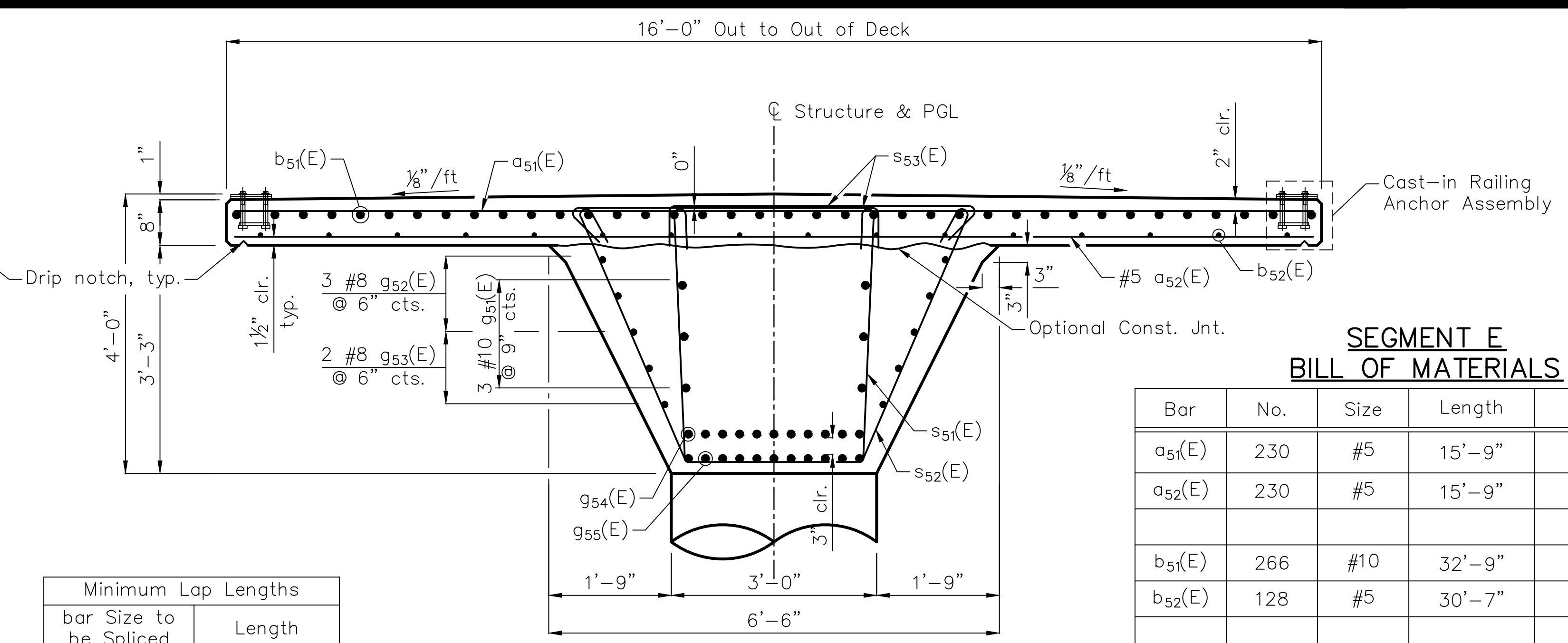
ADDISON UNDERBRIDGE TRAIL CONNECTION FROM
 CLARK PARK TO CALIFORNIA PARK
 STRUCTURE NO. 016-P025
 SUPERSTRUCTURE PLAN AND ELEVATION - SEGMENT E

SHEET NO. S-19
 SHEET 66 OF 158
 REVISION

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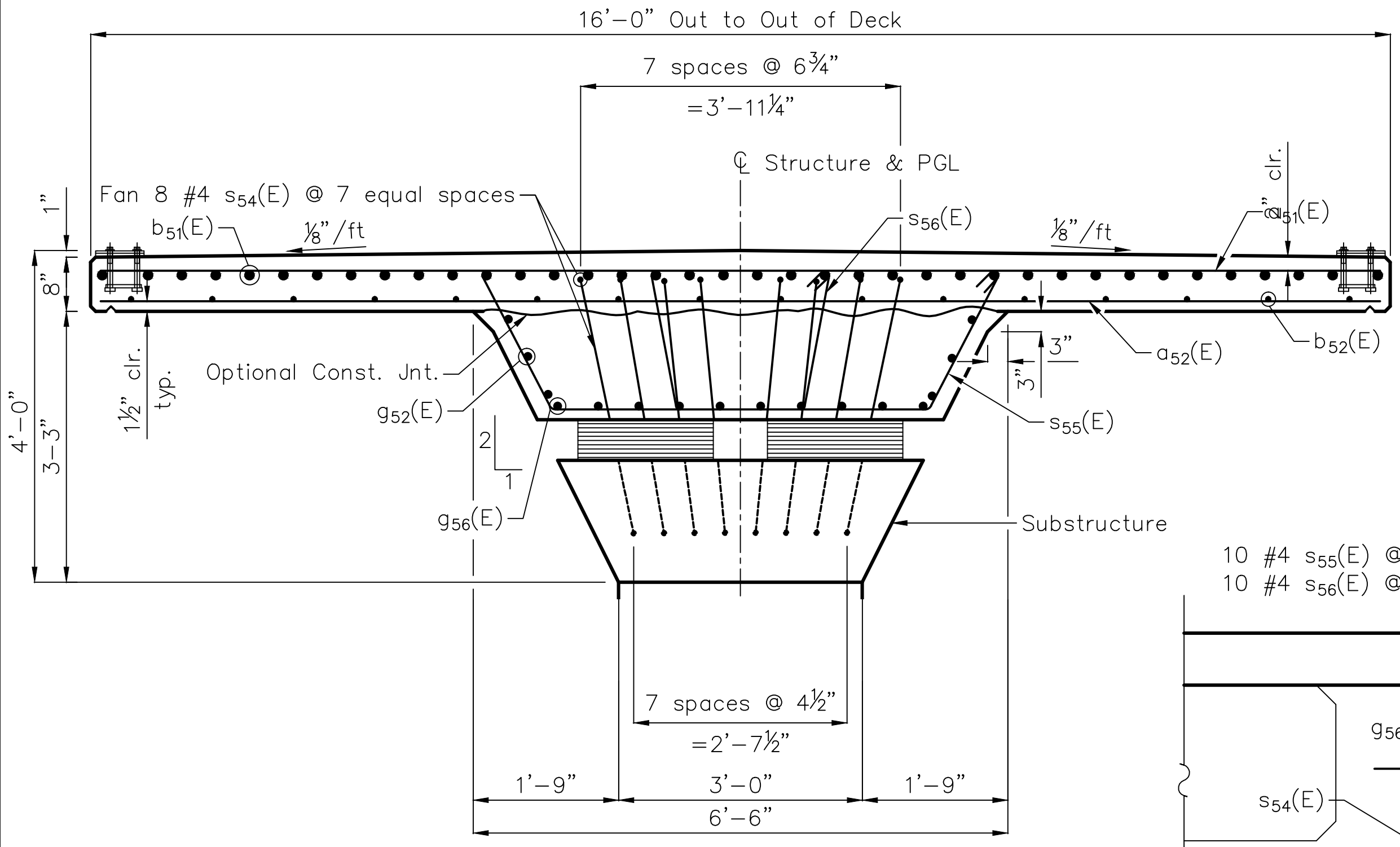
SECTION A-A



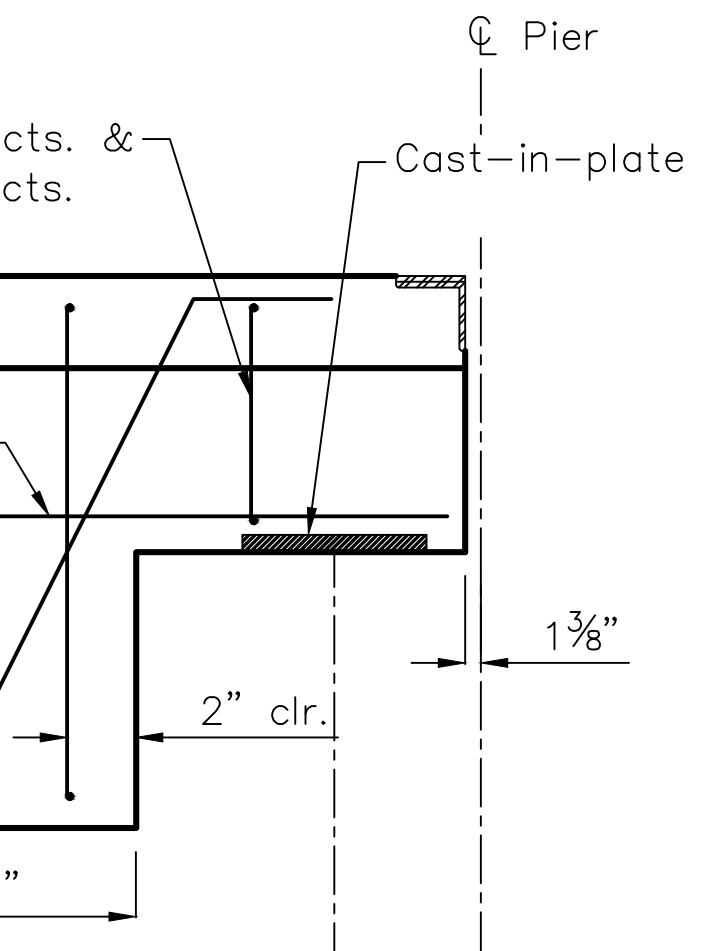
SECTION B-B

Minimum Lap Lengths	
bar Size to be Spliced	Length
#4**	1'-1"
#5	2'-1"

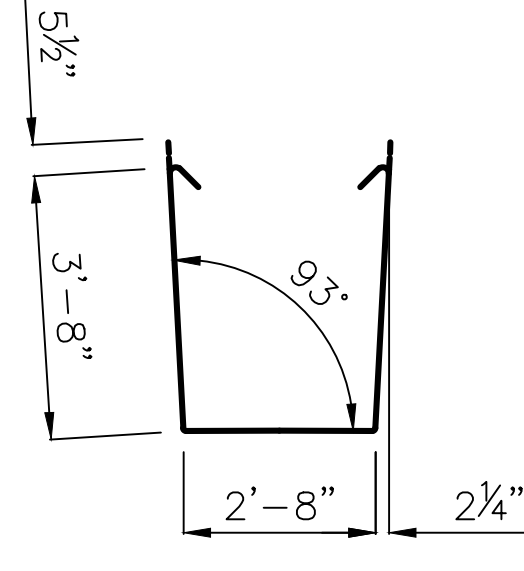
** #4 spiral bars shall have a minimum lap length of 3'-0".



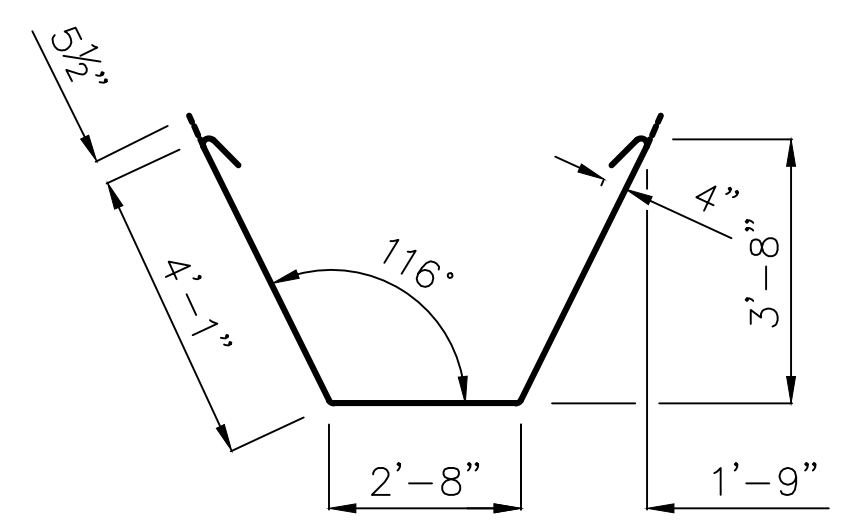
SECTION C-C



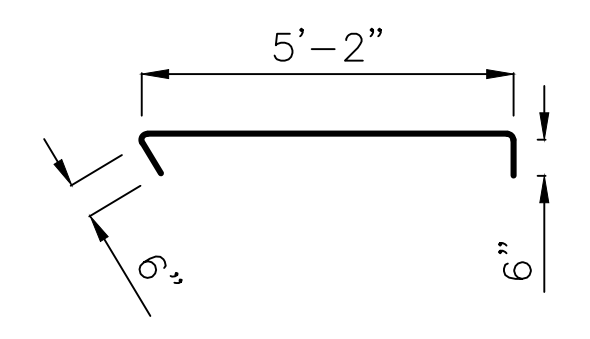
DETAIL A
* Similar for all Dapped End



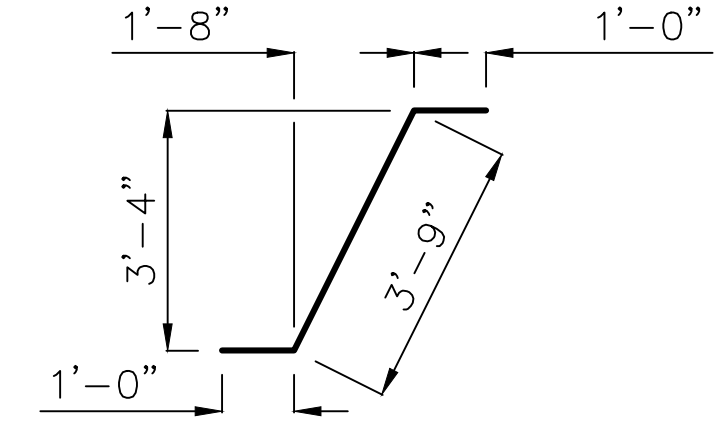
BAR s51(E)



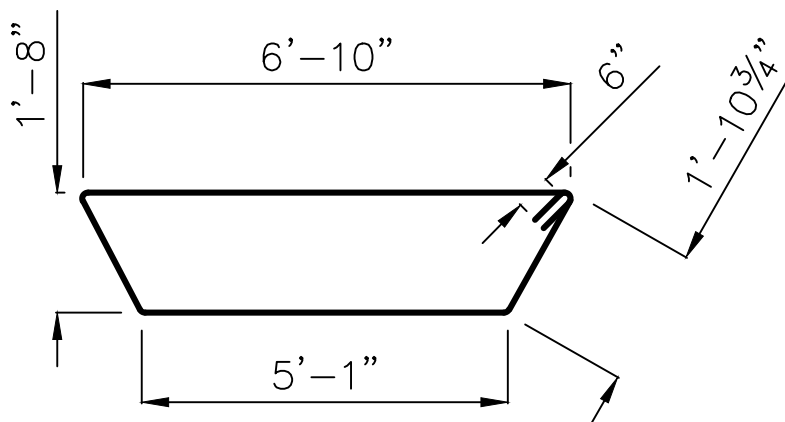
BAR s52(E)



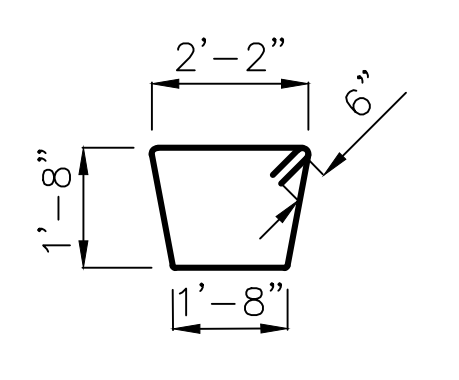
BAR s53(E)



BAR s54(E)



BAR s55(E)



BAR s56(E)

**SEGMENT E
BILL OF MATERIALS**

Bar	No.	Size	Length	Shape
a51(E)	230	#5	15'-9"	—
a52(E)	230	#5	15'-9"	—
b51(E)	266	#10	32'-9"	—
b52(E)	128	#5	30'-7"	—
g51(E)	48	#10	28'-1"	—
g52(E)	42	#10	32'-9"	—
g53(E)	28	#10	32'-1"	—
g54(E)	77	#10	32'-1"	—
g55(E)	88	#10	28'-1"	—
g56(E)	10	#10	4'-7"	—
s51(E)	452	#5	10'-11"	U
s52(E)	452	#5	11'-9"	U
s53(E)	904	#5	6'-2"	U
s54(E)	16	#5	5'-9"	U
s55(E)	20	#5	16'-9"	U
s56(E)	20	#5	8'-2"	U
Reinforcement Bars, Epoxy Coated			Pound	103,540
High Strength High Performance Concrete Superstructures			Cu. Yd.	191.1
Geofoam (Polystyrene Fill)			Cu. Yd.	30.6

- Notes:**
- For Sections A-A, B-B, & C-C bar bending diagrams and Detail A, see sheet S-14 of S-87.
 - For Expansion Joint Details, see sheet S-48 of S-87.
 - For Bearing Details & Cast-in-steel plate, see sheet S-49 of S-87.
 - For Bar Splacers, see sheet S-67 of S-87.

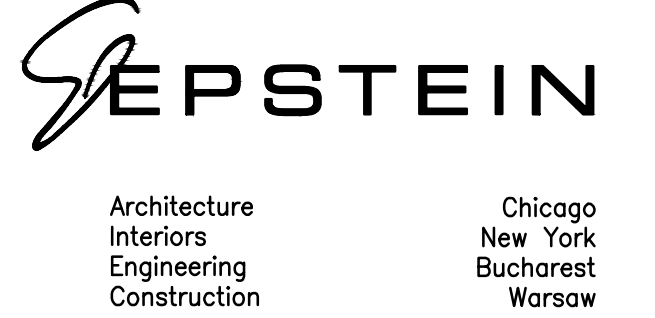
SECTION NO.: 12-E9121-00-BT
 DRAWN BY: N. FIRNBACH
 CHECKED BY: R. VARMA
 APPROVED BY: P. DOMBROWSKI
 DATE: 8/12/2016
 SCALE: N.T.S.

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ADDISON UNDERBRIDGE TRAIL CONNECTION FROM CLARK PARK TO CALIFORNIA PARK STRUCTURE NO. 016-P025
 SUPERSTRUCTURE CROSS SECTIONS - SEGMENT E

SHEET NO. S-20
 SHEET 67 OF 158
 REVISION

**SEGMENT F
BILL OF MATERIALS**

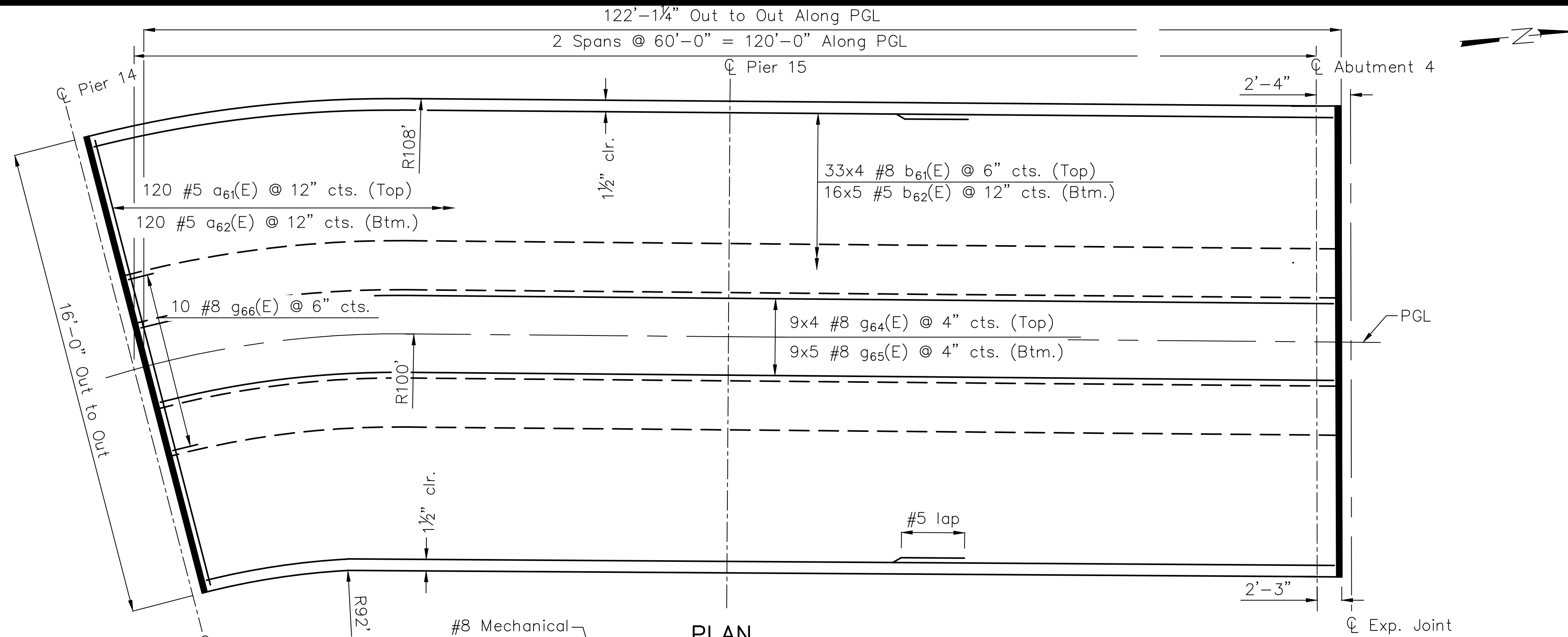
Bar	No.	Size	Length	Shape
a ₆₁ (E)	120	#5	15'-9"	—
a ₆₂ (E)	120	#5	15'-9"	—
b ₆₁ (E)	132	#8	30'-5"	—
b ₆₂ (E)	80	#5	32'-7"	—
g ₆₁ (E)	30	#8	23'-10"	—
g ₆₂ (E)	24	#8	30'-5"	—
g ₆₃ (E)	16	#8	29'-9"	—
g ₆₄ (E)	36	#8	29'-9"	—
g ₆₅ (E)	45	#8	23'-10"	—
g ₆₆ (E)	10	#8	4'-7"	—
s ₆₁ (E)	105	#4	10'-11"	U
s ₆₂ (E)	105	#4	11'-9"	U
s ₆₃ (E)	210	#4	6'-2"	U
s ₆₄ (E)	8	#4	5'-9"	U
s ₆₅ (E)	10	#4	16'-9"	U
s ₆₆ (E)	10	#4	8'-2"	U
Reinforcement Bars, Epoxy Coated			Pound	31,040
High Strength High Performance Concrete Superstructures			Cu. Yd.	102.1
Geofoam (Polystyrene Fill)			Cu. Yd.	15.9

Minimum Lap Lengths	
bar Size to be Spliced	Length
#4**	1'-1"
#5	2'-1"

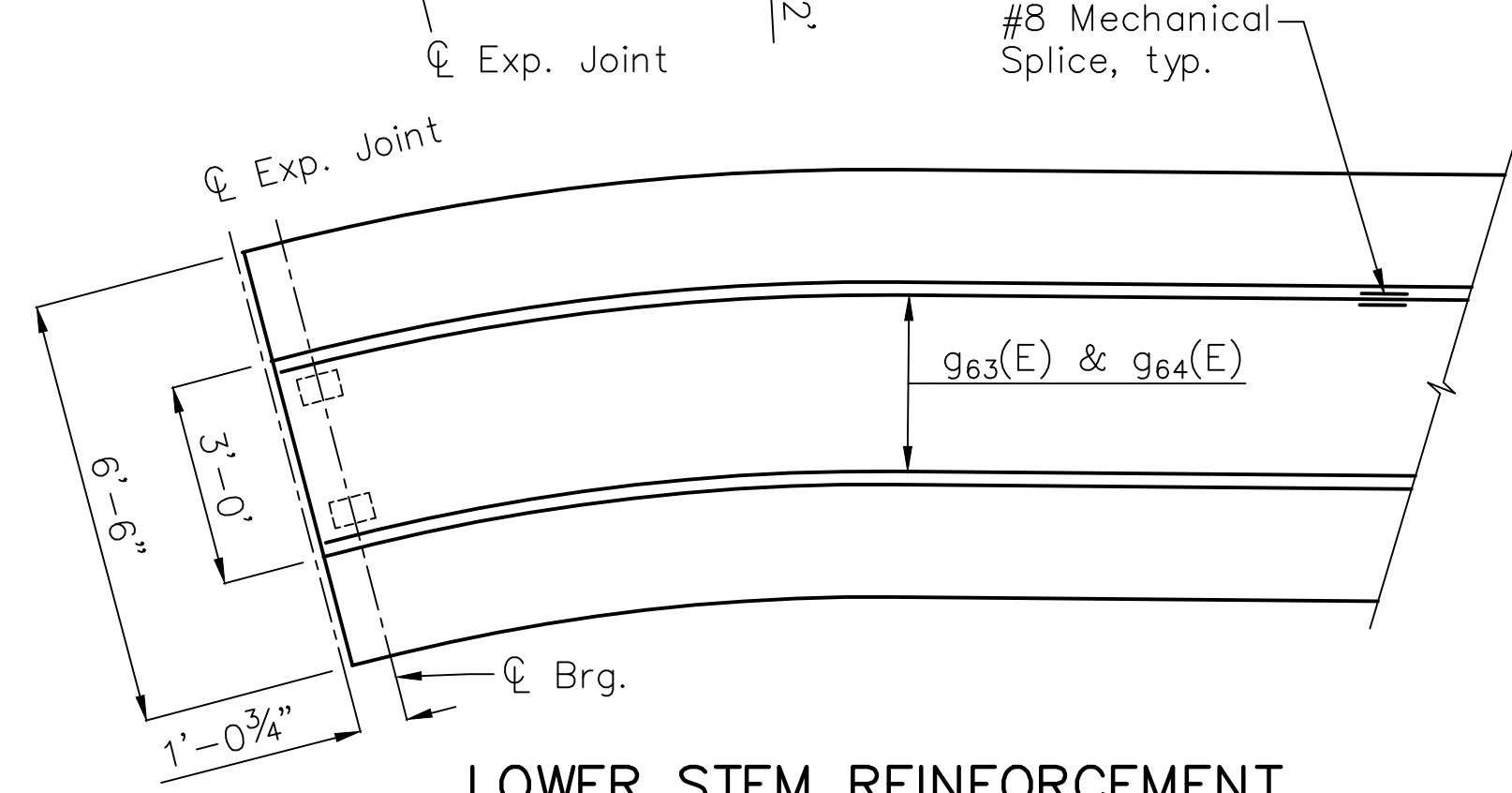
** #4 spiral bars shall have a minimum lap length of 3'-0".

Notes:

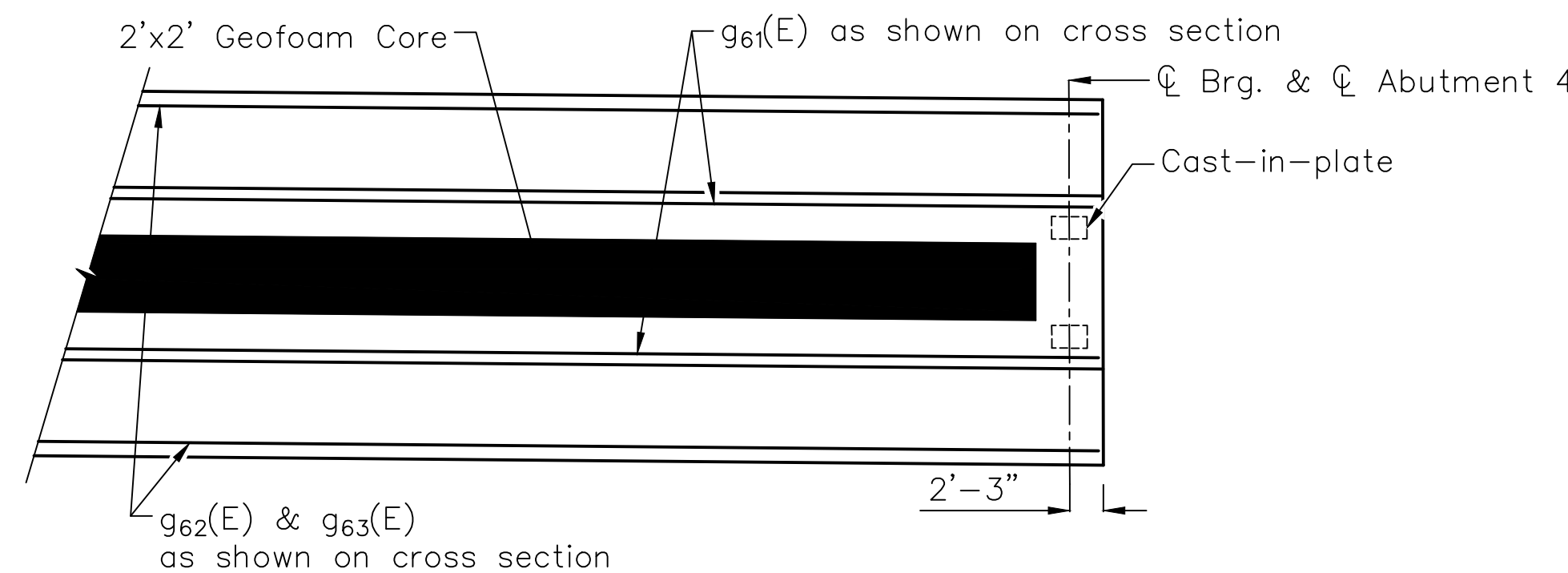
- For Sections A-A, B-B, & C-C bar bending diagrams and Detail A, see sheet S-14 of S-87.
- For Expansion Joint Details, see sheet S-48 of S-87.
- For Bearing Details & Cast-in-steel plate, see sheet S-49 of S-87.
- For Bar Splicers, see sheet S-67 of S-87.
- Bars indicated thus 100 x 2 - #5, etc. indicate 100 lines of bars with 2 lengths per line.



PLAN

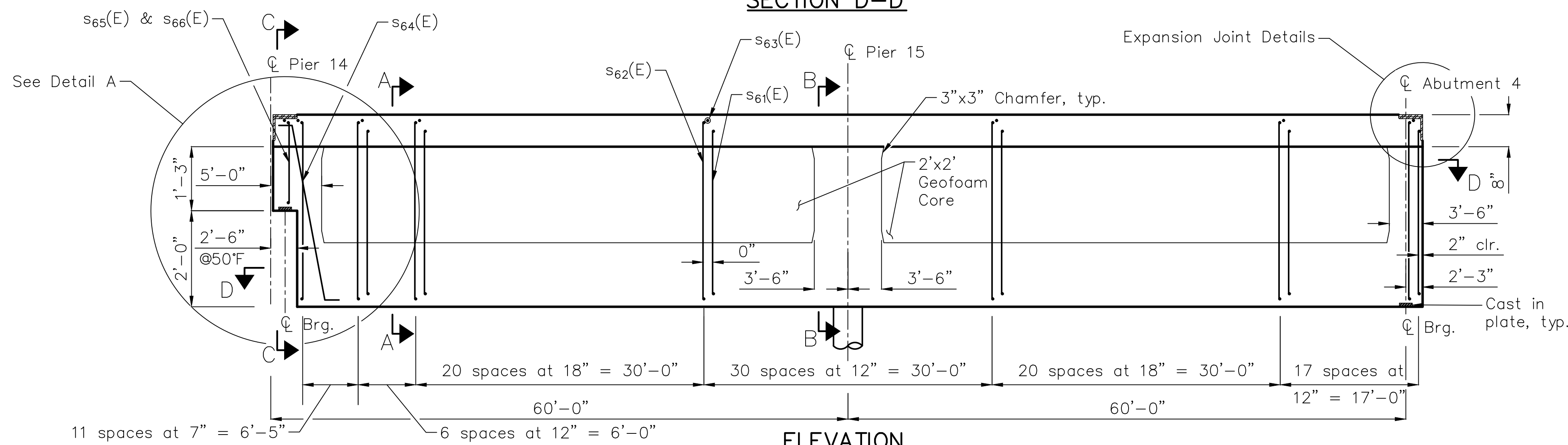


LOWER STEM REINFORCEMENT



UPPER STEM REINFORCEMENT

SECTION D-D



ELEVATION

SECTION NO.: 12-E9121-00-BT
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 CHECKED BY: R. VARMA
 APPROVED BY: P. DOMBROWSKI
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 SCALE: N.T.S.

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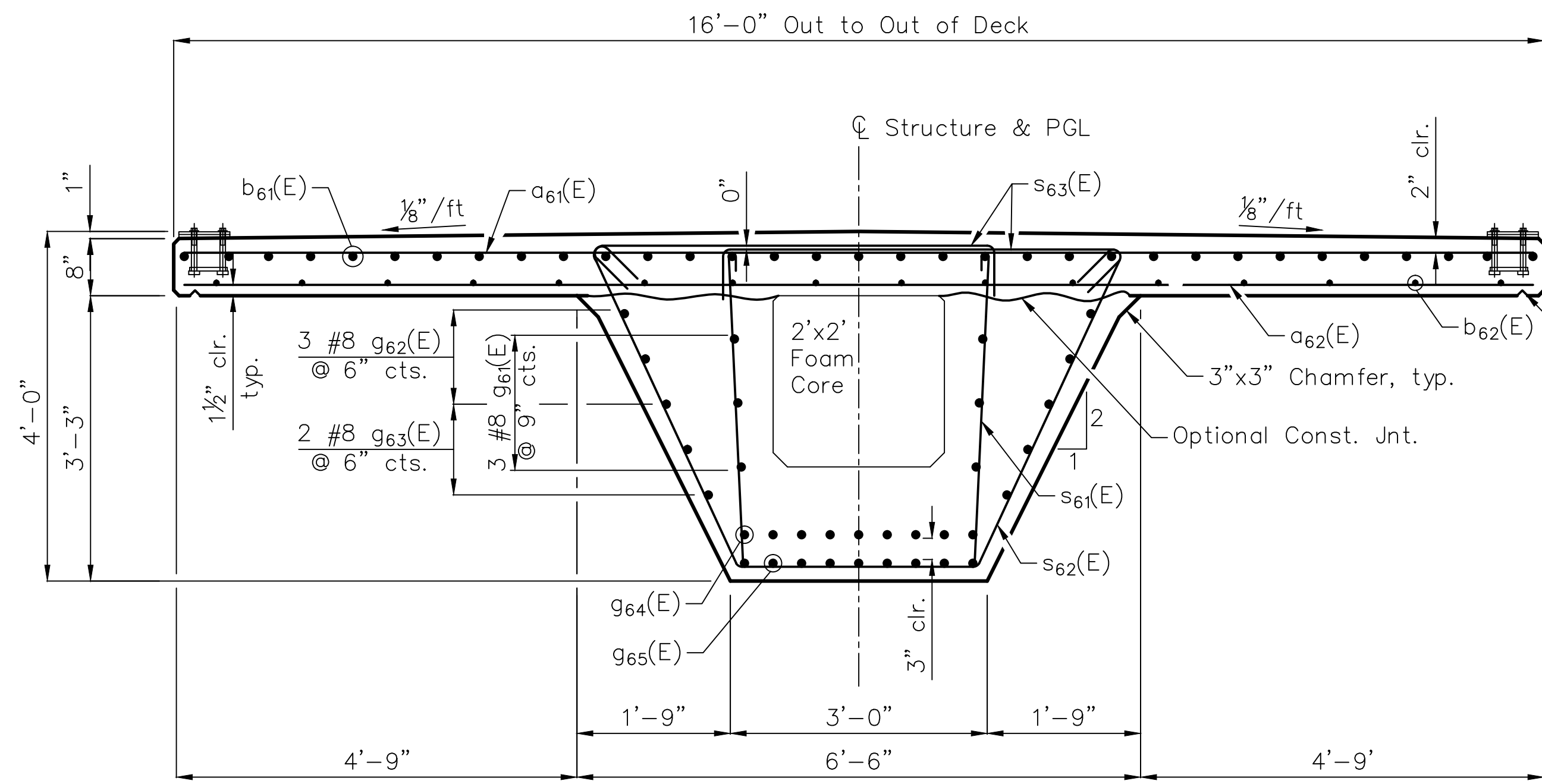
ADDISON UNDERBRIDGE TRAIL CONNECTION FROM
 CLARK PARK TO CALIFORNIA PARK
 STRUCTURE NO. 016-P025

SUPERSTRUCTURE PLAN AND ELEVATION - SEGMENT F

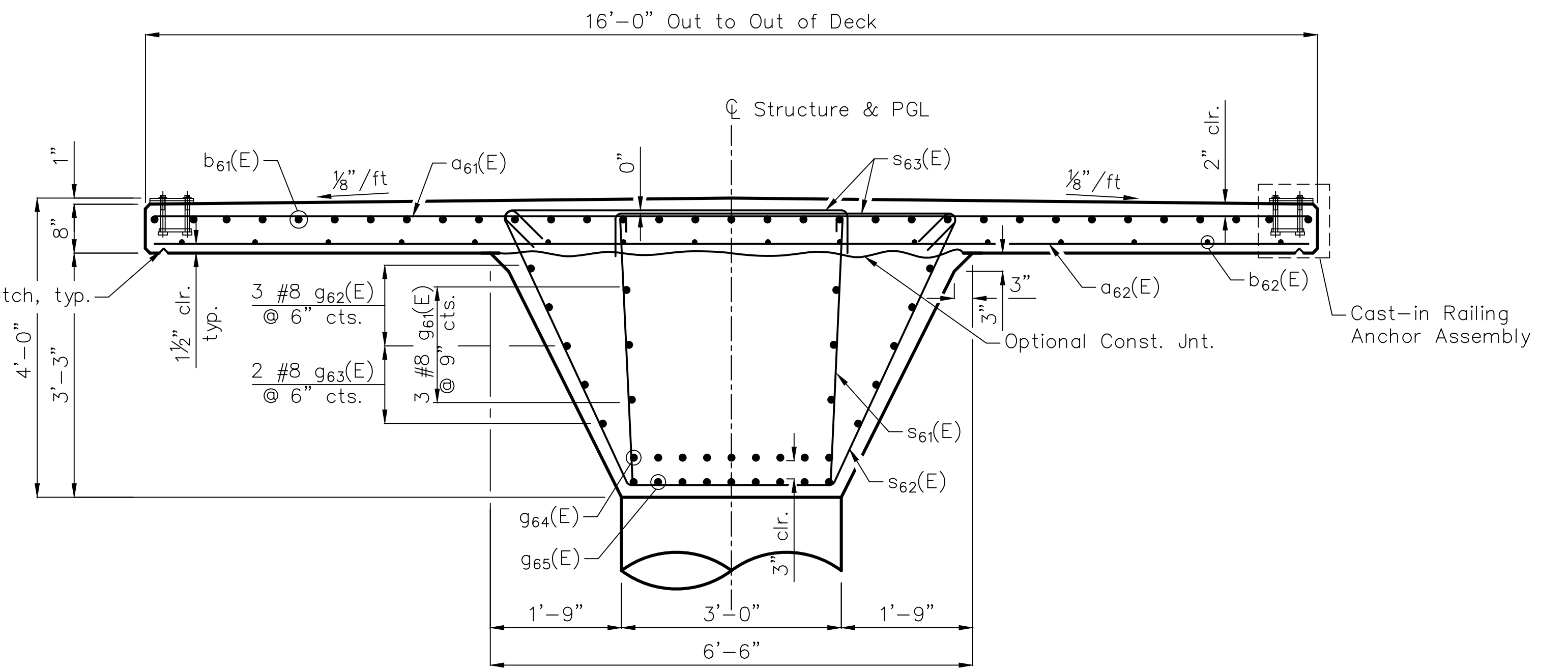
SHEET NO.
S-21

SHEET 68 OF 158

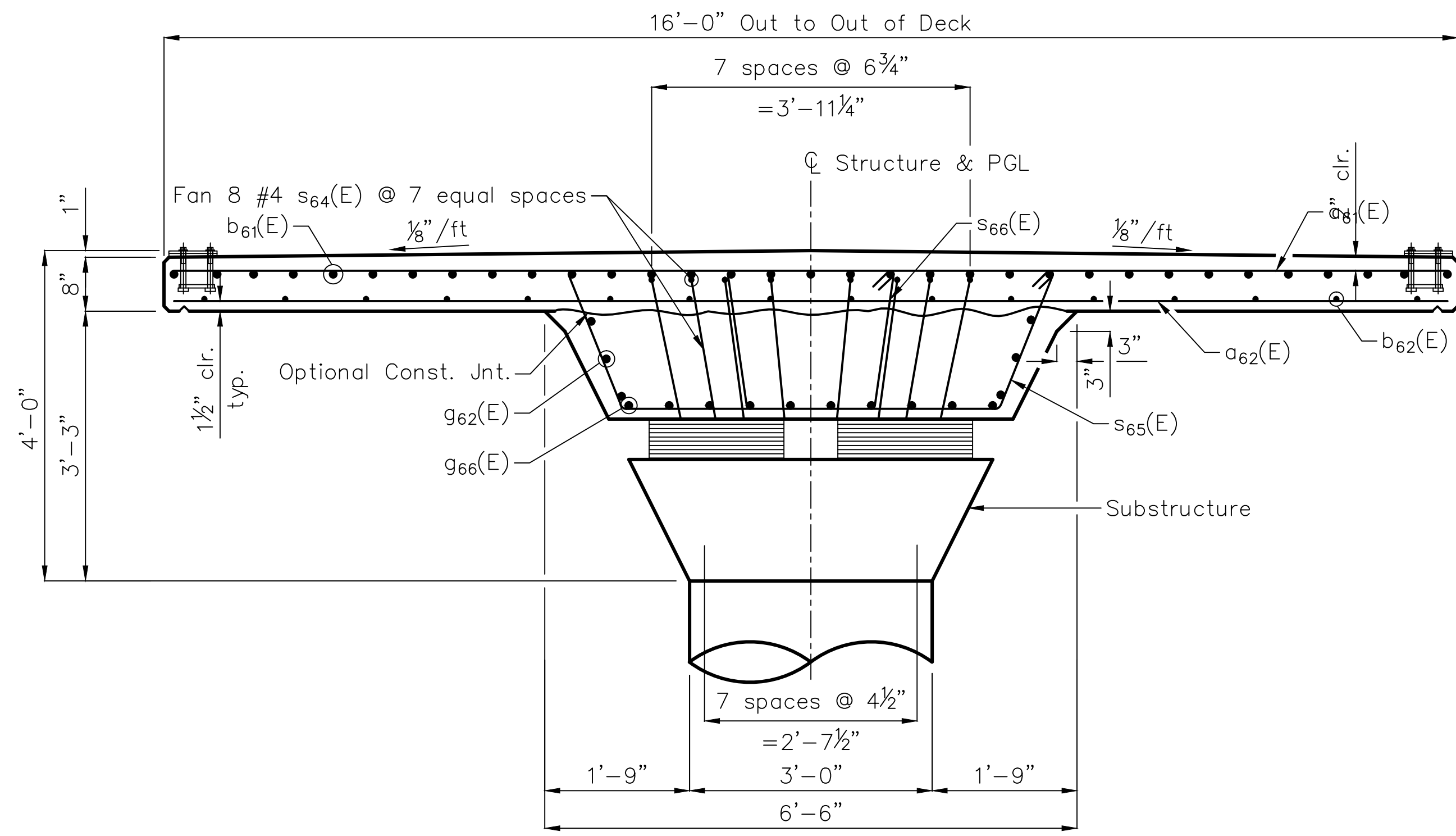
REVISION



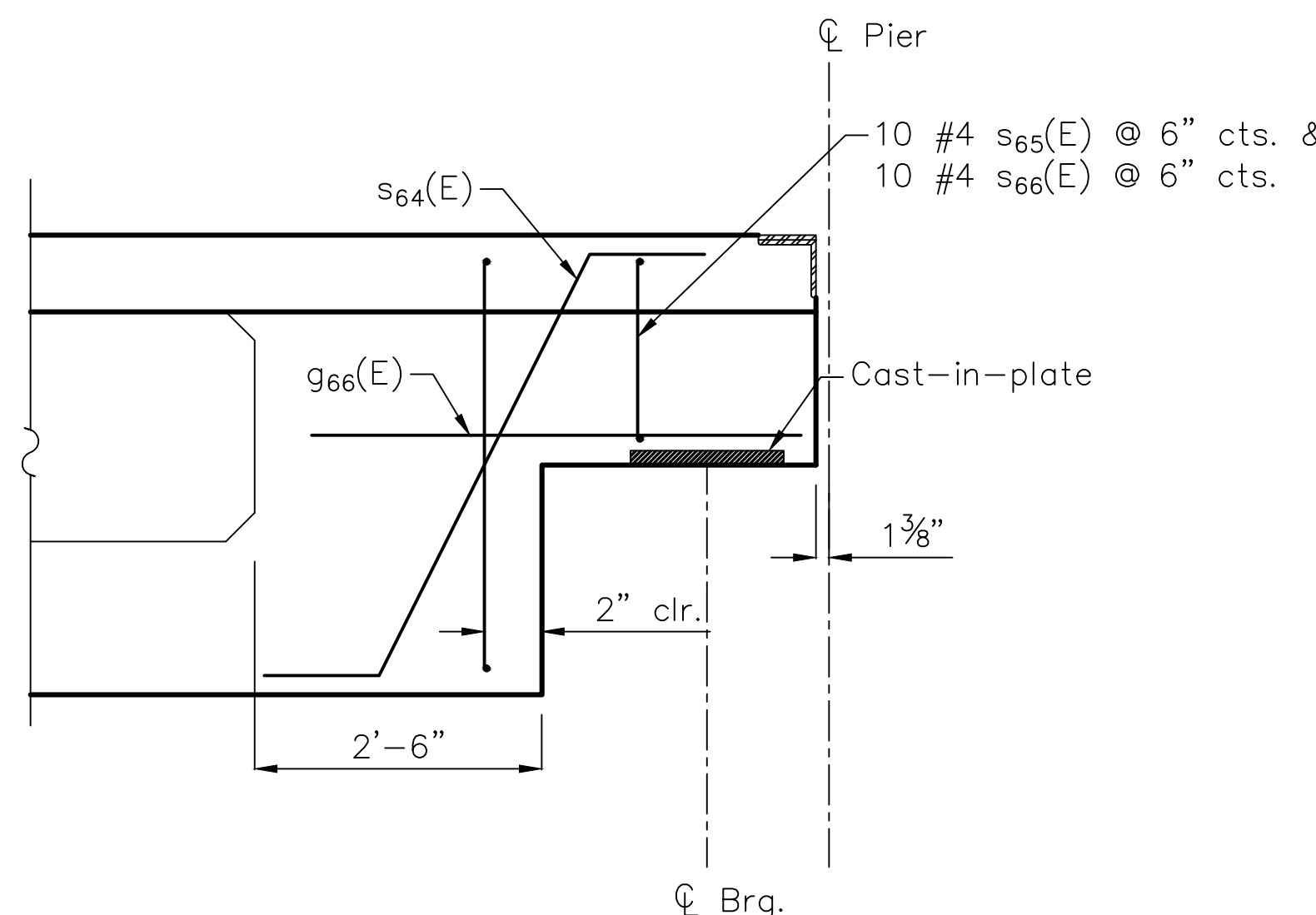
SECTION A-A



SECTION B-B

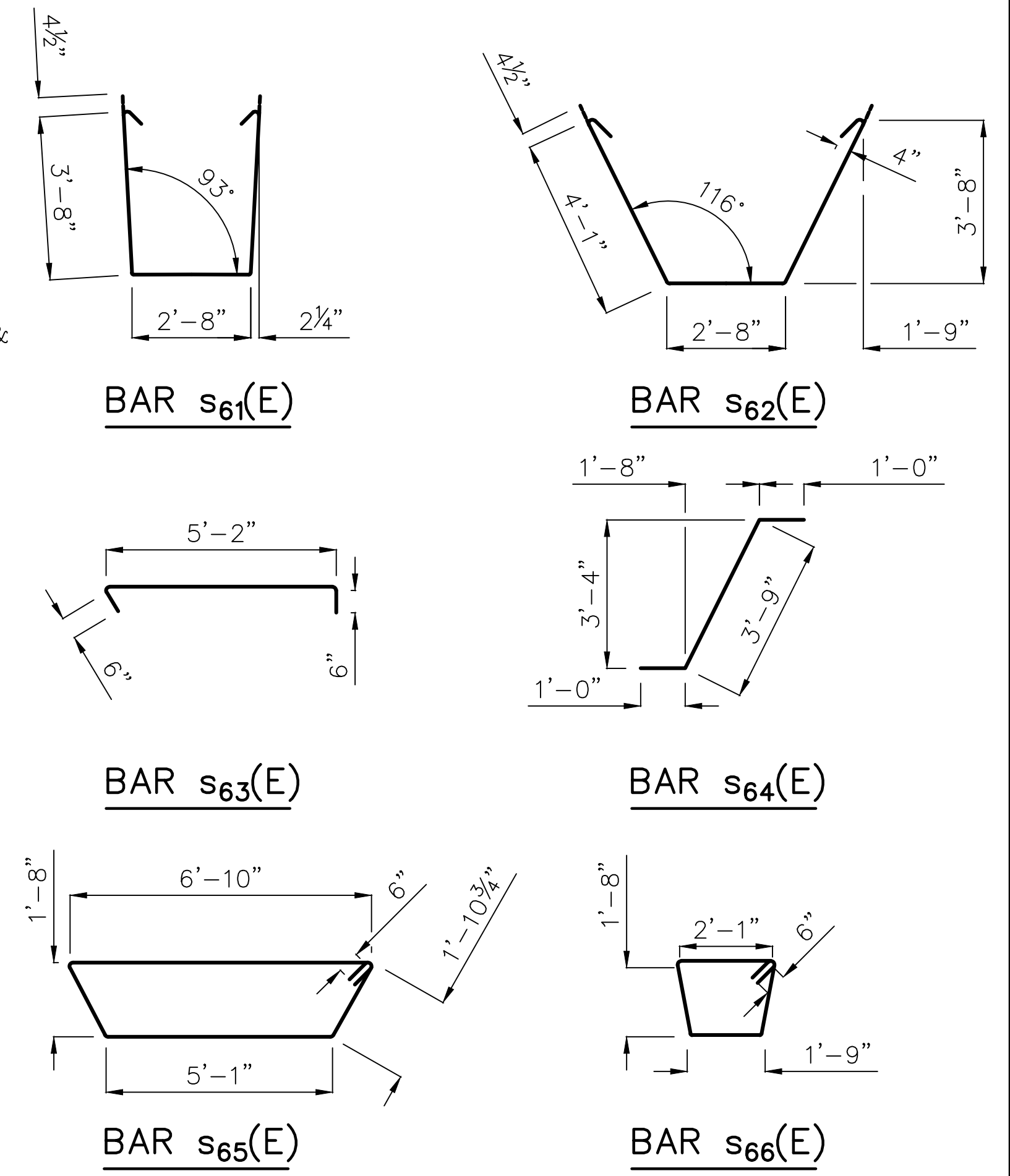


SECTION C-C



DETAIL A

* Similar for all Dapped End



Notes:
1. For Cast-in Railing Anchor Assembly, see sheet S-24 of S-87.

SECTION NO.: 12-E9121-00-BT
 DRAWN BY: N. FIRNBACH
 CHECKED BY: R. VARMA
 APPROVED BY: P. DOMBROWSKI
 DATE: 8/12/2016
 SCALE: N.T.S.

NO.	DATE	REVISIONS/ISSUANCES
1	8/12/16	ISSUED FOR FINAL
2	5/6/16	ISSUED FOR FINAL
3	6/05/15	ISSUED FOR FINAL REVIEW

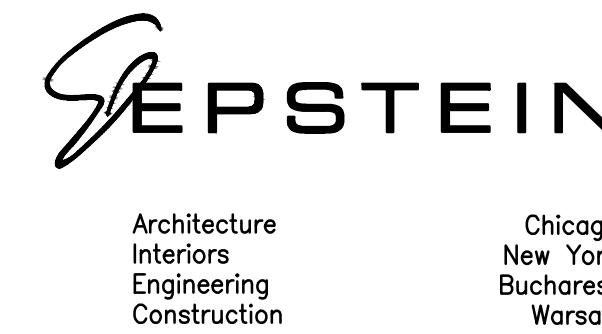
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 30 N LaSalle St. Suite 1100
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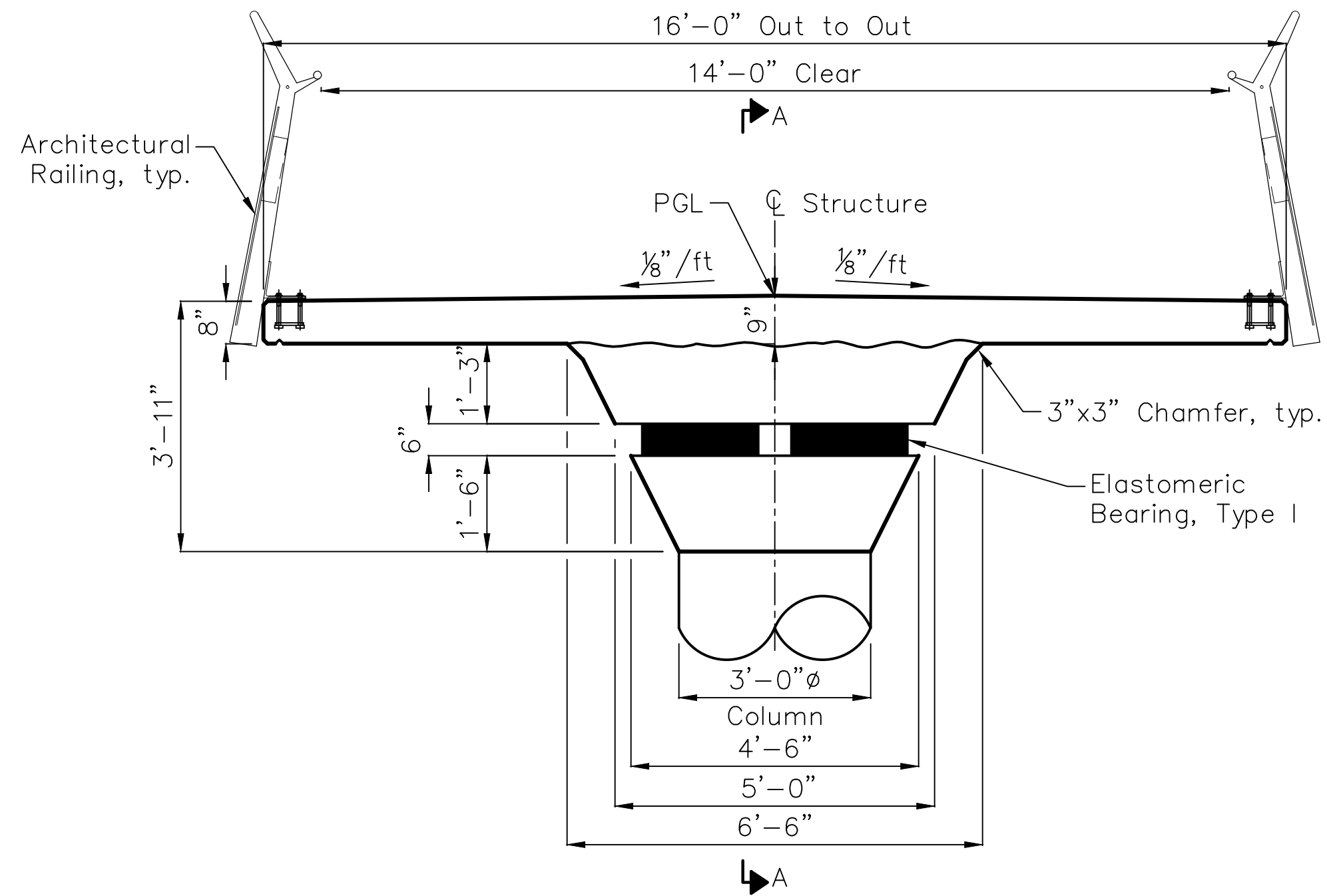
ADDISON UNDERBRIDGE TRAIL CONNECTION FROM
 CLARK PARK TO CALIFORNIA PARK
 STRUCTURE NO. 016-P025

SUPERSTRUCTURE CROSS SECTIONS - SEGMENT F

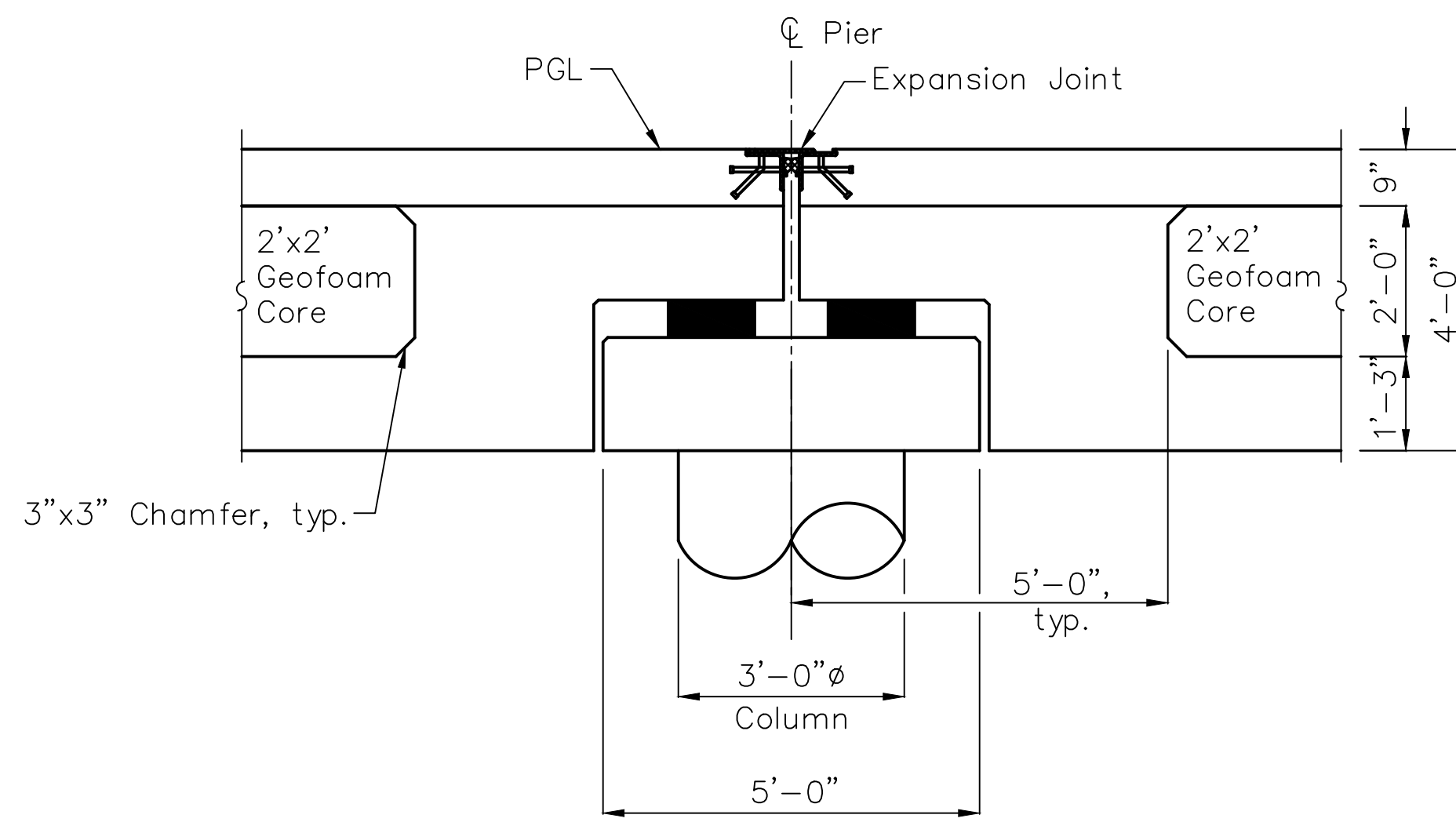
SHEET NO.
S-22

SHEET 69 OF 158

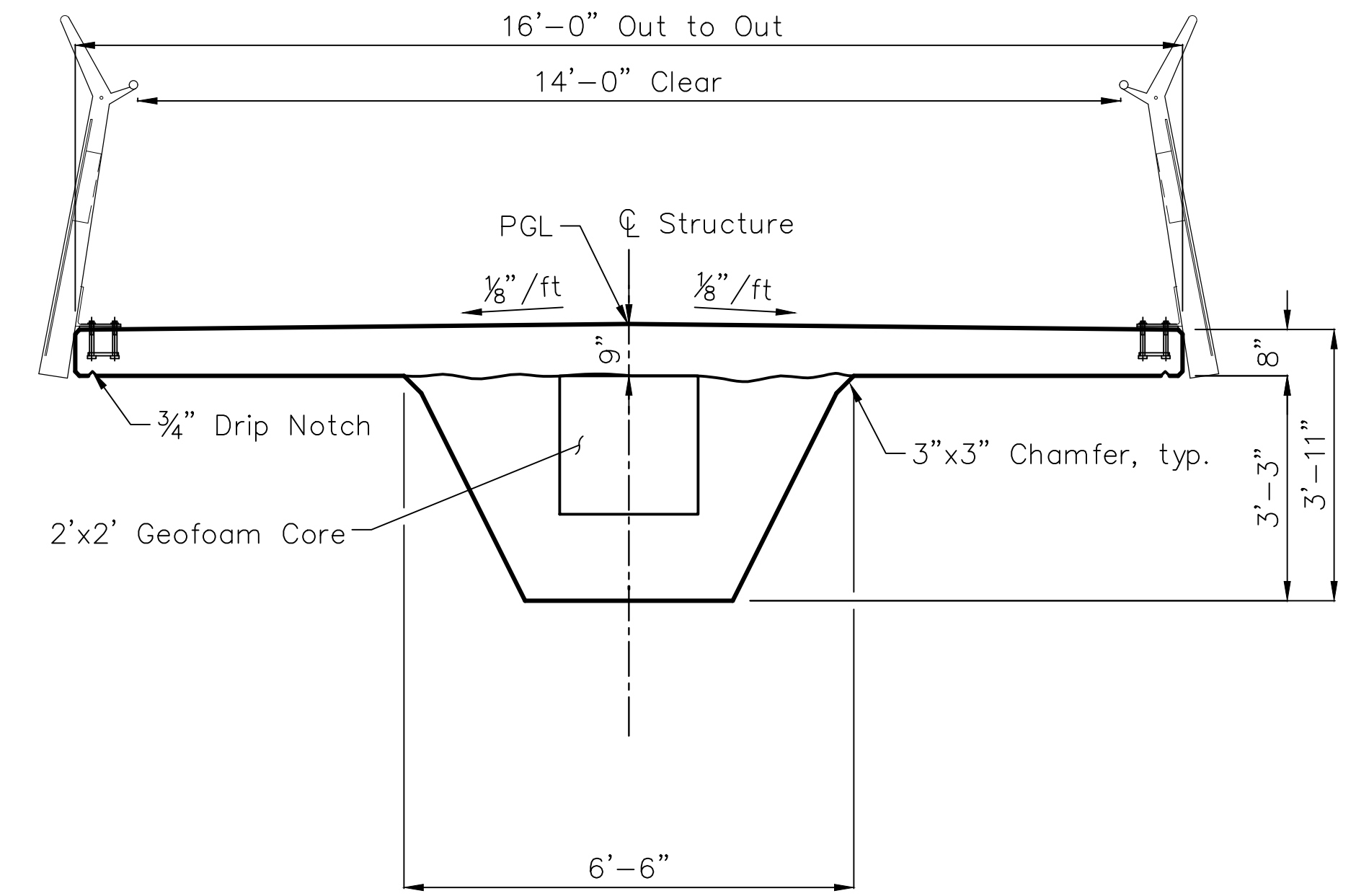
REVISION



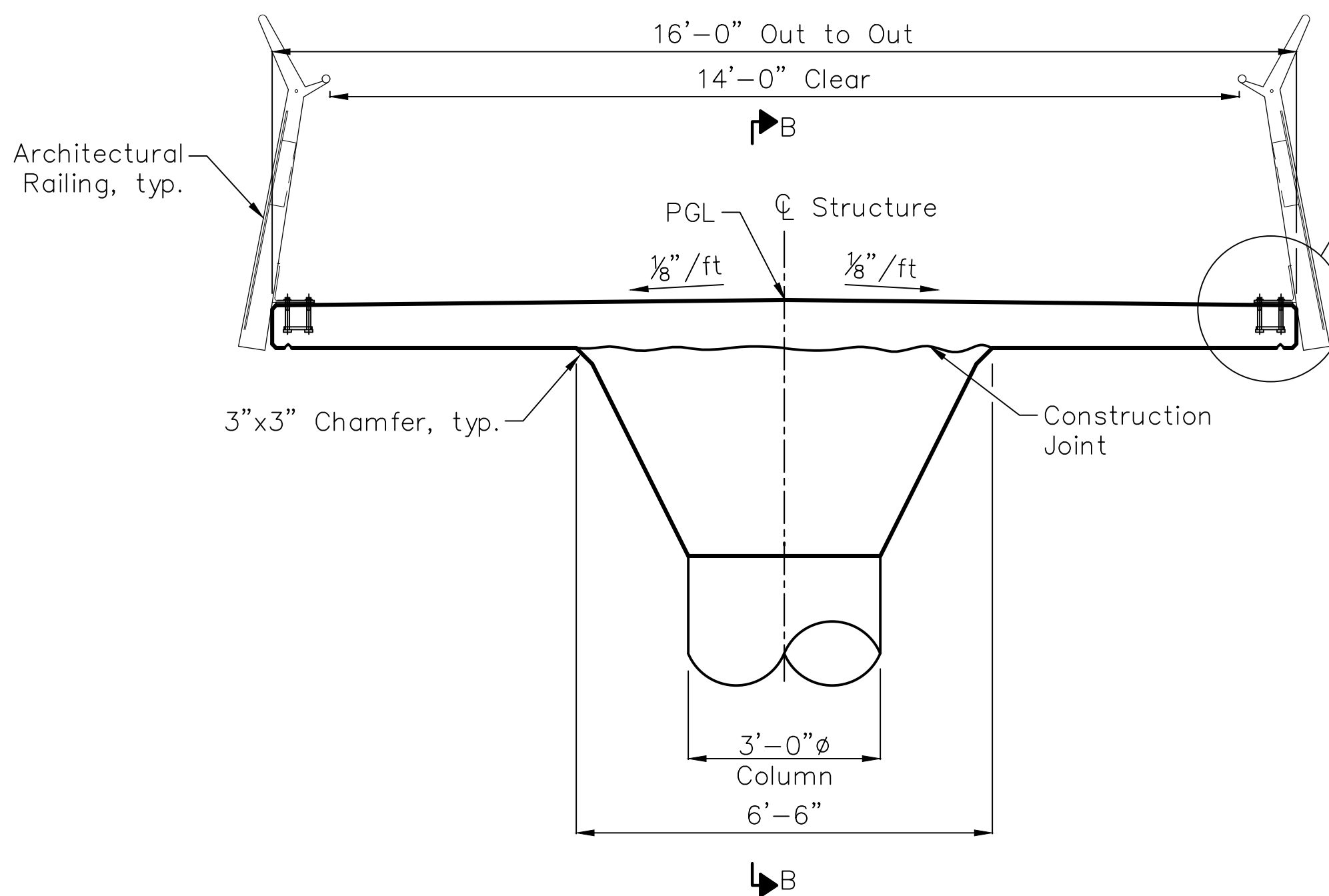
DOUBLE EXPANSION PIER CROSS SECTION



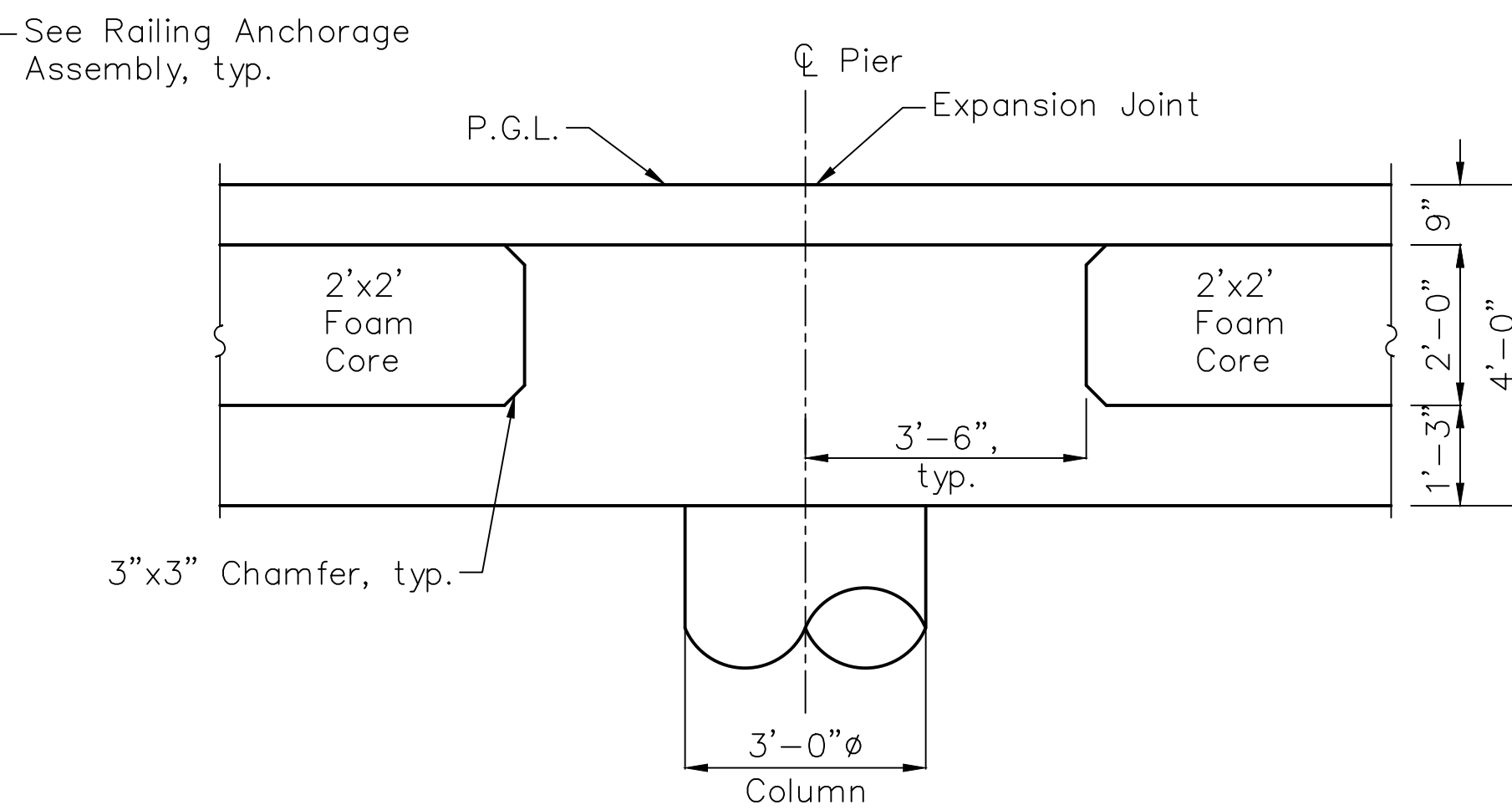
SECTION A-A



TYPICAL CROSS SECTION



FIXED PIER CROSS SECTION



SECTION B-B

- Notes:**
1. For Railing Anchorage Assembly Details, see sheet S-24 of S-87.
 2. For Expansion Joint Details, see sheet S-48 of S-87.
 3. For Bearing Details, see sheet S-49 of S-87.

SECTION NO.: 12-E9121-00-BT
 DRAWN BY: J. MASI
 CHECKED BY: R. VARMA
 APPROVED BY: P. DOMBROWSKI
 DATE: 8/12/2016
 SCALE: N.T.S.

NO.	DATE	REVISIONS/ISSUANCES
1	8/12/16	ISSUED FOR FINAL
2	5/6/16	ISSUED FOR FINAL
3	6/05/15	ISSUED FOR FINAL REVIEW

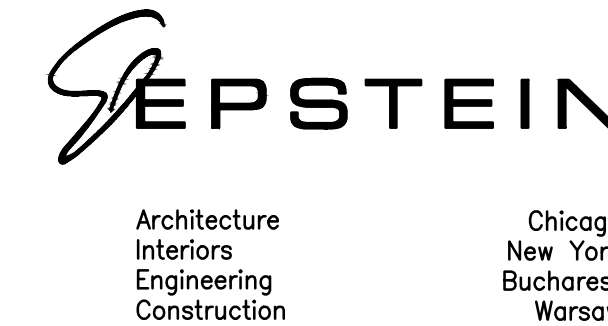
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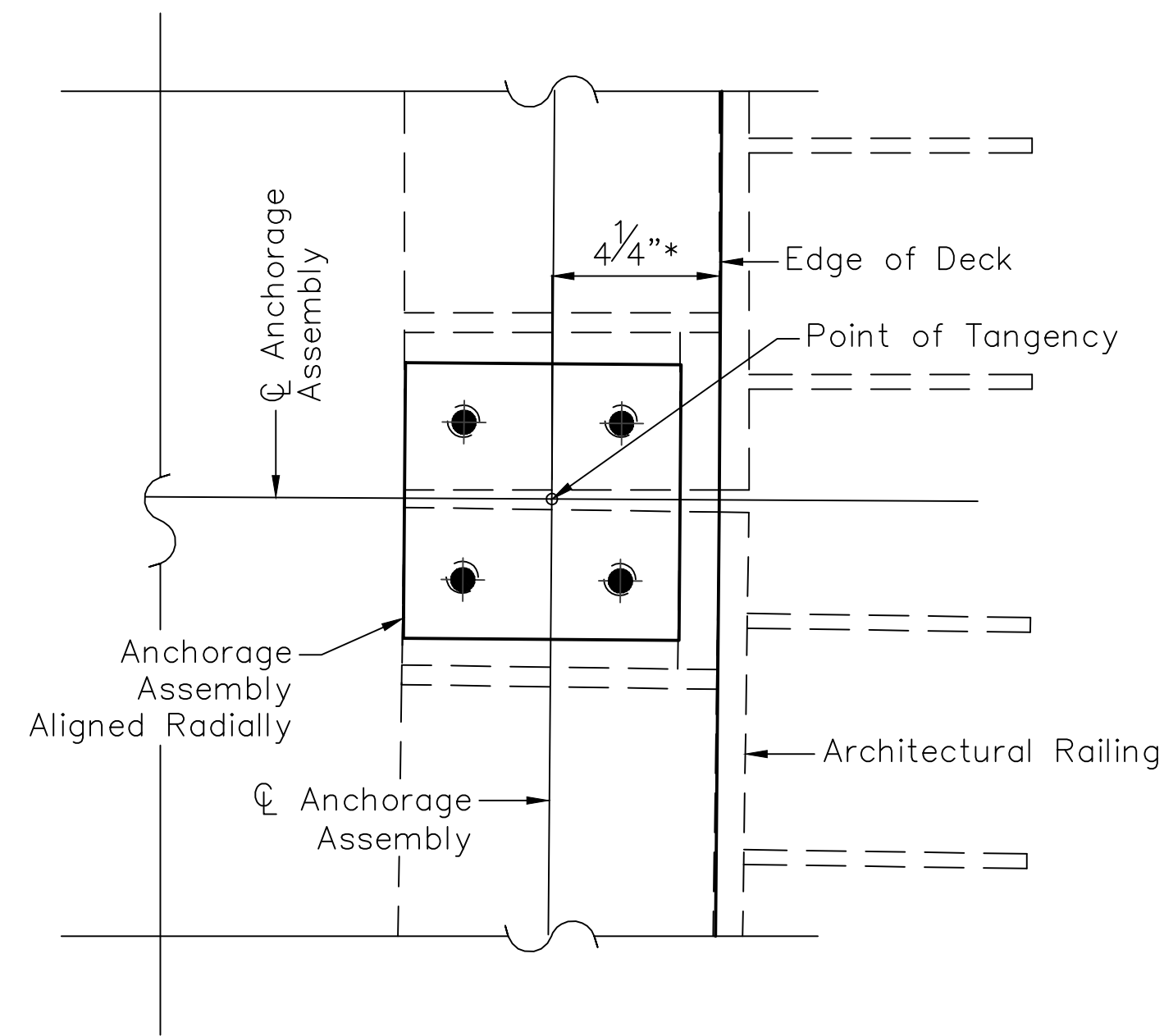
ADDISON UNDERBRIDGE TRAIL CONNECTION FROM
 CLARK PARK TO CALIFORNIA PARK
 STRUCTURE NO. 016-P025

SUPERSTRUCTURE DETAILS

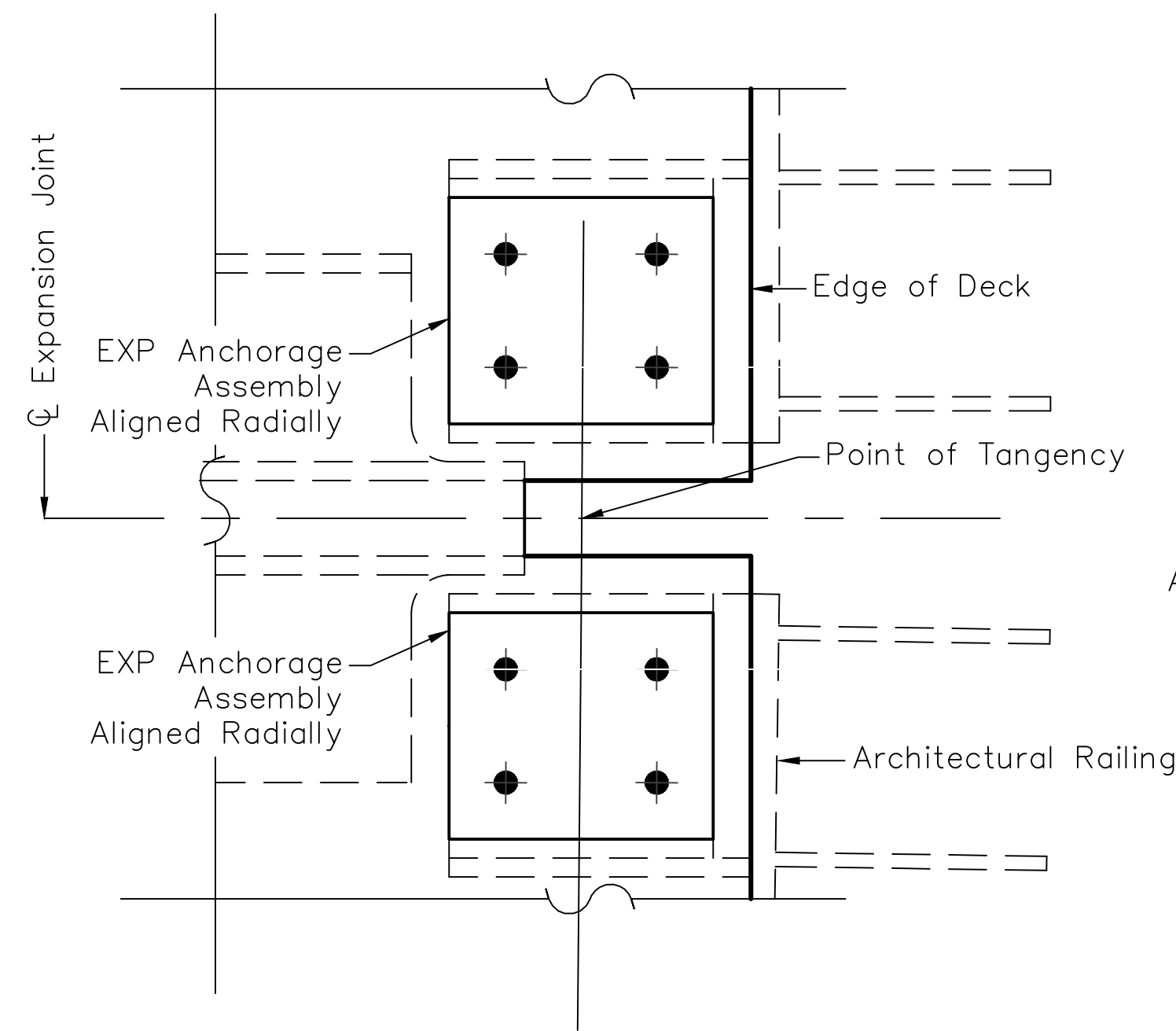
SHEET NO.
S-23

SHEET 70 OF 158

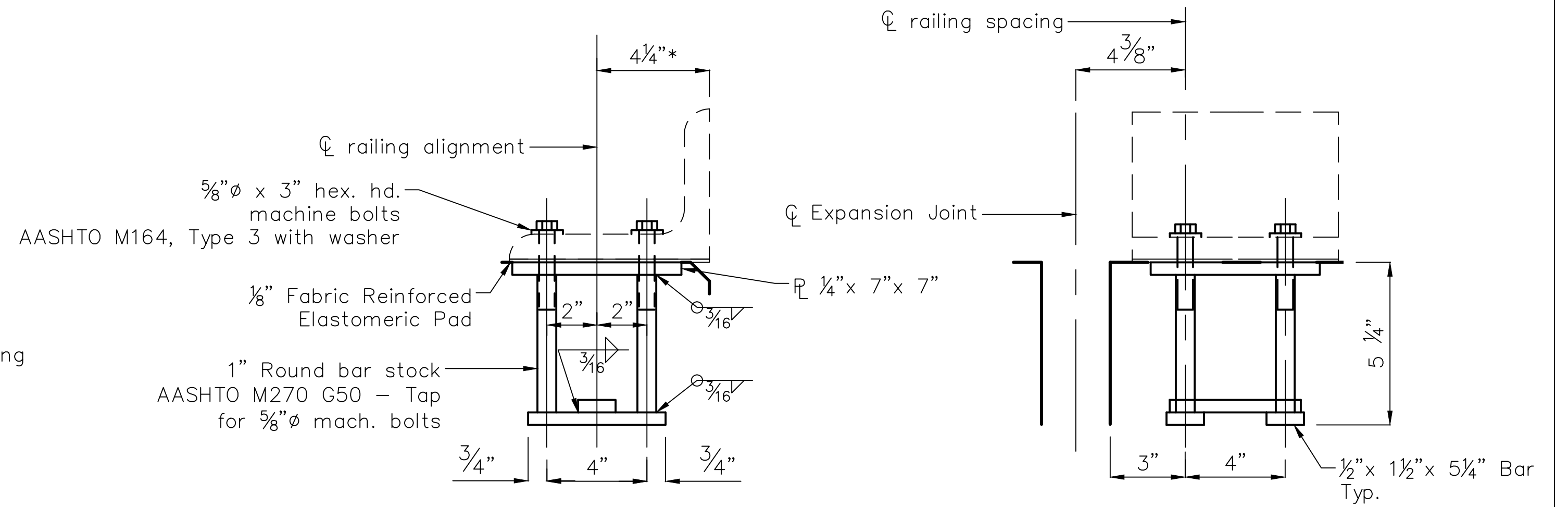
REVISION



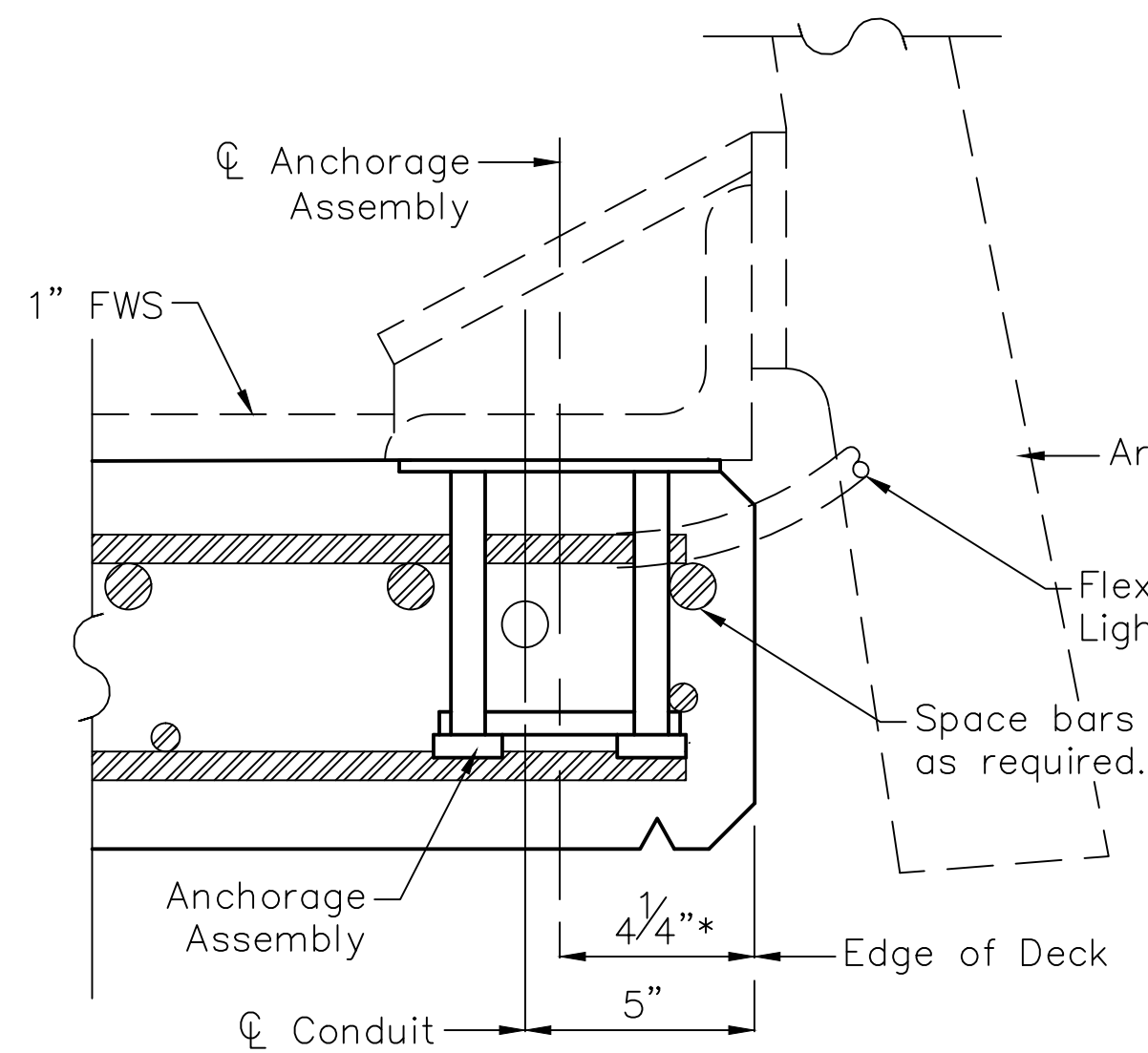
HANDRAIL ANCHORAGE ASSEMBLY



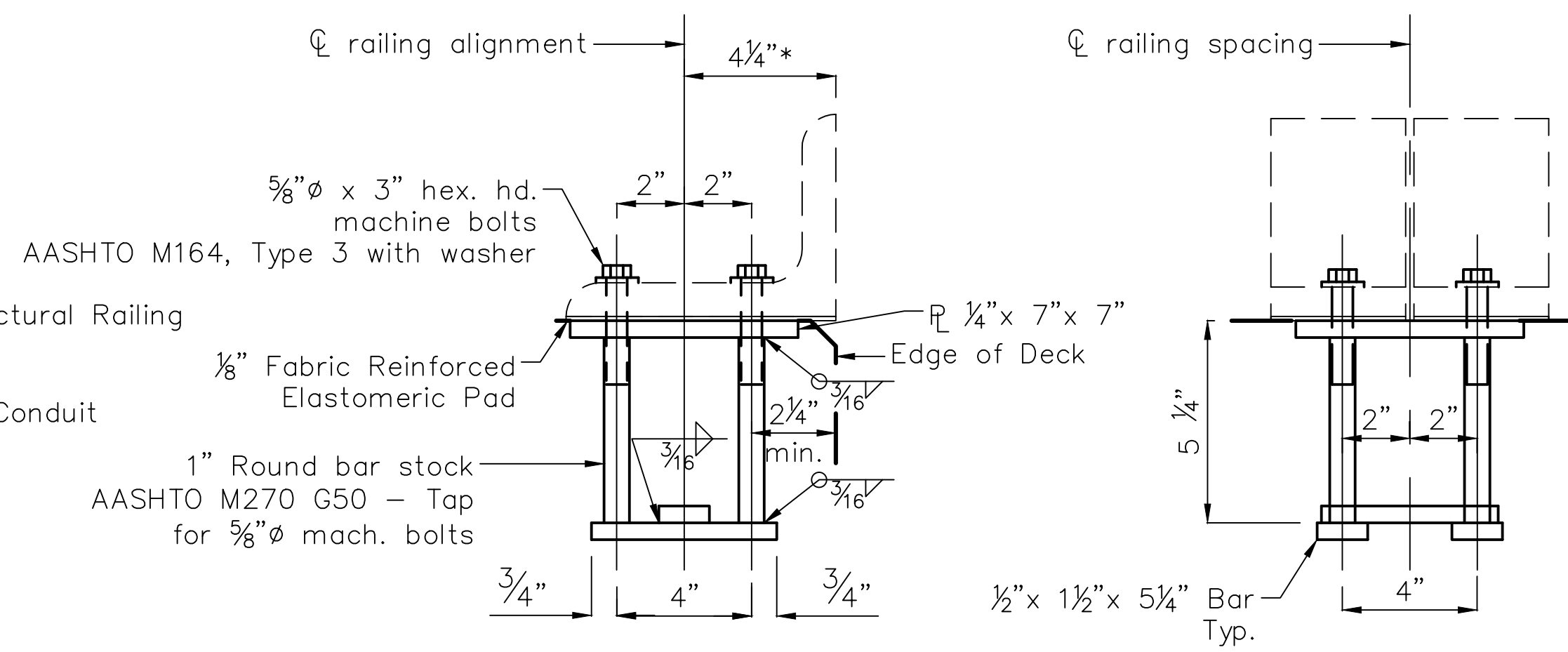
HANDRAIL ANCHORAGE ASSEMBLY AT EXP JOINT



RAILING EXP ANCHORAGE ASSEMBLY - TYPE B
(38 Required)



SECTION A-A



RAILING ANCHORAGE ASSEMBLY - TYPE A
(497 Required)

Cost of anchorage assembly included in pay item "Bicycle Railing, Special."

* Typ., variations noted in "Anchorage Locations" tables, see sheets S-25 thru S-62 of S-87.

SECTION NO.: 12-E9121-00-BT
 DRAWN BY: P. DOMBROWSKI
 CHECKED BY: R. VARMA
 APPROVED BY: P. DOMBROWSKI
 DATE: 8/12/2016
 SCALE: N.T.S.

NO.	DATE	REVISIONS/ISSUANCES
1	8/12/16	ISSUED FOR FINAL
2	5/6/16	ISSUED FOR FINAL
3	6/05/15	ISSUED FOR FINAL REVIEW

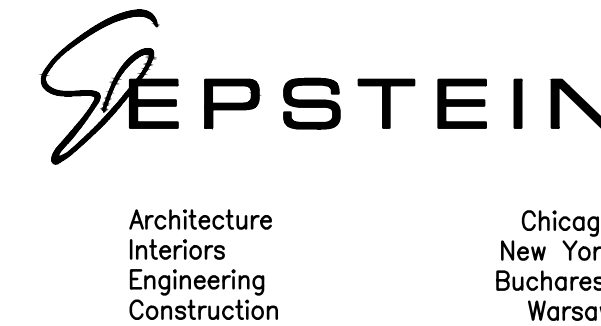
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ADDISON UNDERBRIDGE TRAIL CONNECTION FROM
 CLARK PARK TO CALIFORNIA PARK
 STRUCTURE NO. 016-P025

RAILING ANCHORAGE DETAILS

SHEET NO.
 S-24

SHEET 71 OF 158

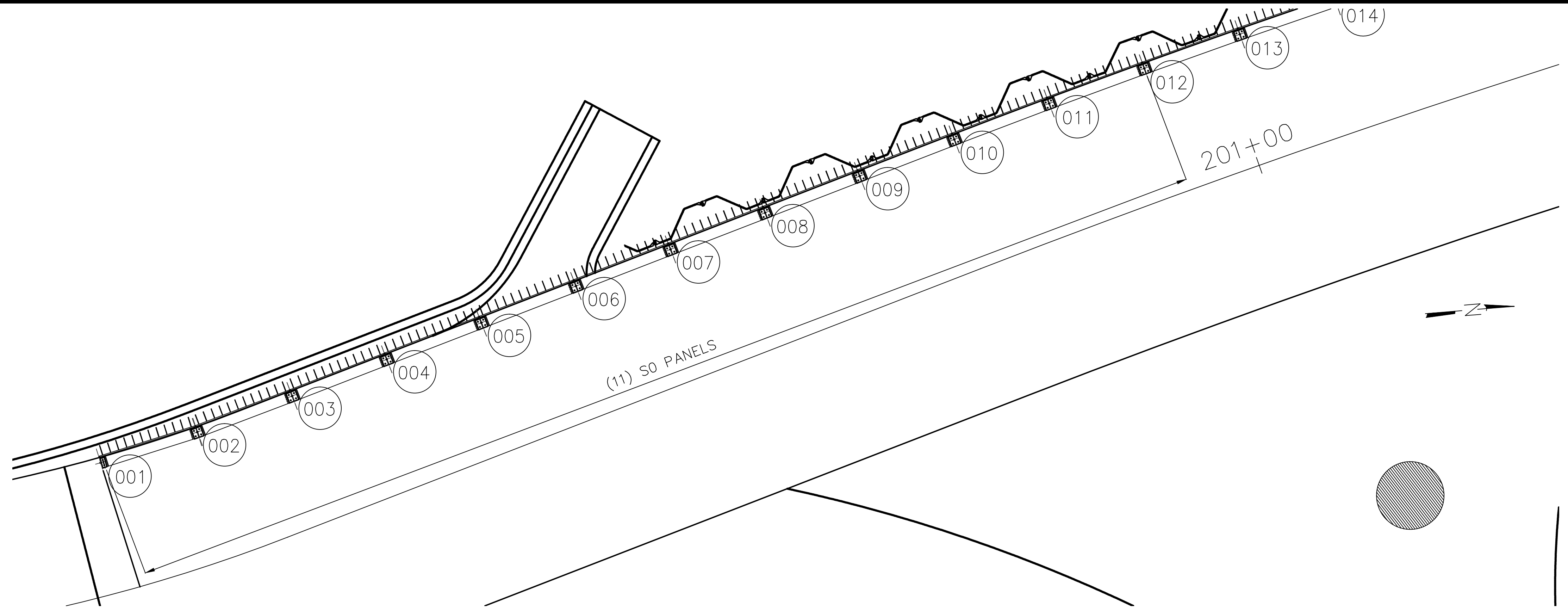
REVISION

**ANCHORAGE LOCATIONS
STA. 200+00 TO STA. 201+00**

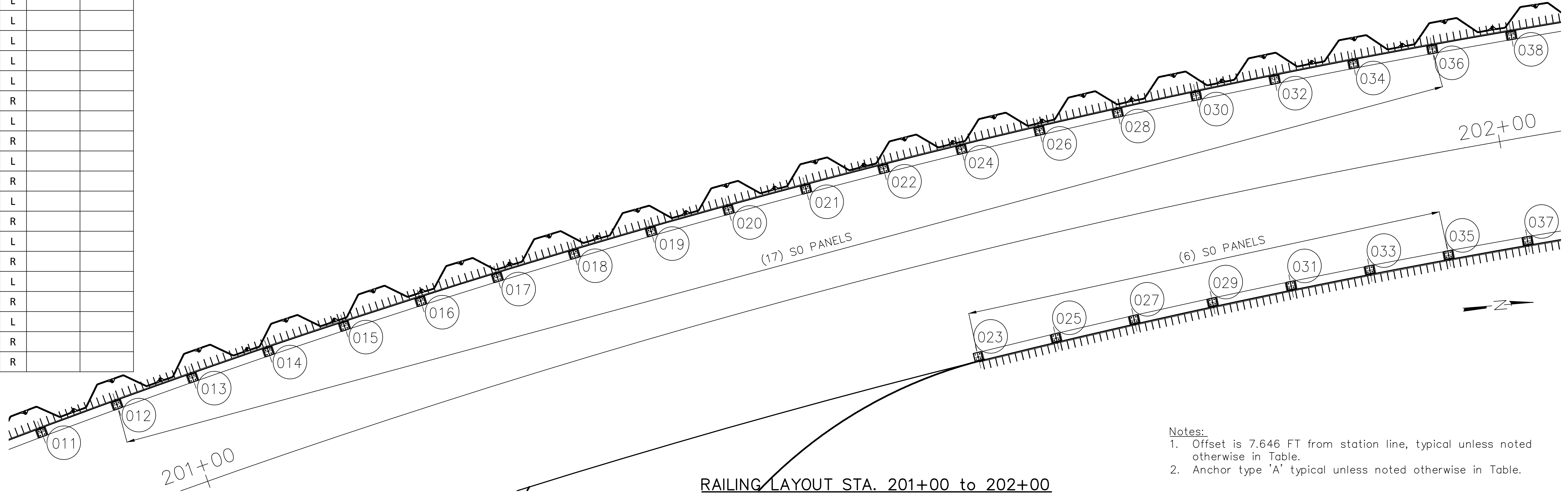
TAG	STATION	R/L	OFFSET (FT)	ANCHOR TYPE
001	200+29.18	L		
002	200+35.50	L		
003	200+41.73	L		
004	200+47.73	L		
005	200+53.73	L		
006	200+59.73	L		
007	200+65.73	L		
008	200+71.73	L		
009	200+77.73	L		
010	200+83.71	L		
011	200+89.64	L		
012	200+95.56	L		

**ANCHORAGE LOCATIONS
STA. 201+00 TO STA. 202+00**

TAG	STATION	R/L	OFFSET (FT)	ANCHOR TYPE
013	201+01.49	L		
014	201+07.41	L		
015	201+13.33	L		
016	201+19.26	L		
017	201+25.18	L		
018	201+31.11	L		
019	201+37.03	L		
020	201+42.95	L		
021	201+48.88	L		
022	201+47.30	L		
023	201+58.24	R		
024	201+60.73	L		
025	201+64.32	R		
026	201+66.65	L		
027	201+70.39	R		
028	201+72.58	L		
029	201+76.47	R		
030	201+78.50	L		
031	201+82.55	R		
032	201+84.42	L		
033	201+88.63	R		
034	201+90.35	L		
035	201+94.70	R		
036	201+96.27	R		



RAILING LAYOUT STA. 200+00 to 201+00



RAILING LAYOUT STA. 201+00 to 202+00

- Notes:**
1. Offset is 7.646 FT from station line, typical unless noted otherwise in Table.
 2. Anchor type 'A' typical unless noted otherwise in Table.

SECTION NO.: 12-E9121-00-BT
 DRAWN BY: A. PIBURN
 CHECKED BY: R. VARMA
 APPROVED BY: P. DOMBROWSKI
 DATE: 8/12/2016
 SCALE: 1/4" = 1'-0"

NO.	DATE	REVISIONS/ISSUANCES
1	8/12/16	ISSUED FOR FINAL
2	5/6/16	ISSUED FOR FINAL
3	6/05/15	ISSUED FOR FINAL REVIEW

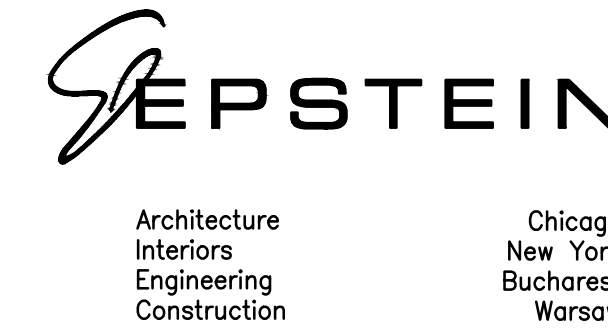
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ADDISON UNDERBRIDGE TRAIL CONNECTION FROM
 CLARK PARK TO CALIFORNIA PARK
 STRUCTURE NO. 016-P025

RAILING ANCHORAGE LAYOUT

SHEET NO.
 S-25

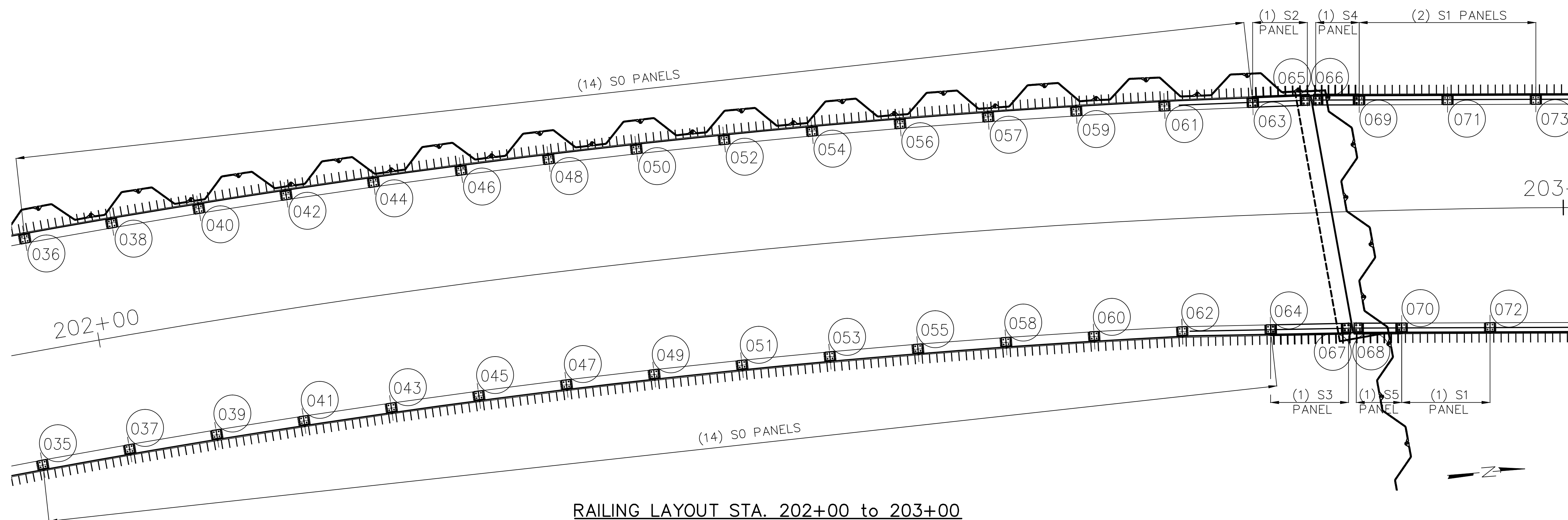
SHEET 72 OF 158

REVISION

P:\Projects\20000\03346 - CDOT - Addison Underbridge Connection - \DISCIPLINE\Civil\Working\Sheets\08_STRUCTURE\016_P025-23346-025_038-Railing_Anchorage_Layouts.dwg
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 1/24/2017 10:52:28 PM

ANCHORAGE LOCATIONS
STA. 202+00 TO STA. 203+00

TAG	STATION	R/L	OFFSET (FT)	ANCHOR TYPE
037	202+00.78	R		
038	202+02.20	L		
039	202+06.86	R		
040	202+08.12	L		
041	202+12.94	R		
042	202+14.04	L		
043	202+19.02	R		
044	202+19.97	L		
045	202+25.09	R		
046	202+25.89	L		
047	202+31.17	R		
048	202+31.82	L		
049	202+37.25	R		
050	202+37.74	L		
051	202+43.33	R		
052	202+43.66	L		
053	202+49.41	R		
054	202+49.59	L		
055	202+55.48	R		
056	202+55.51	L		
057	202+61.44	L		
058	202+61.56	R		
059	202+67.36	L		
060	202+67.64	R		
061	202+73.29	L		
062	202+73.72	R		
063	202+79.21	L	7.611	
064	202+79.79	R	7.788	
065	202+82.88	L	7.613	B
066	202+83.45	L	7.608	B
067	202+85.16	R	7.878	B
068	202+85.69	R	7.882	B
069	202+86.36	L	7.518	
070	202+88.82	R	7.962	
071	202+92.28	L	7.392	
072	202+94.90	R	8.063	
073	202+98.21	L	7.324	



RAILING LAYOUT STA. 202+00 to 203+00

Notes:

1. Offset is 7.646 FT from station line, typical unless noted otherwise in Table.
2. Anchor type 'A' typical unless noted otherwise in Table.

SECTION NO.: 12-E9121-00-BT
 DRAWN BY: A. PIBURN
 CHECKED BY: R. VARMA
 APPROVED BY: P. DOMBROWSKI
 DATE: 8/12/2016
 SCALE: 1/4" = 1'-0"

NO.	DATE	REVISIONS/ISSUANCES
1	8/12/16	ISSUED FOR FINAL
2	5/6/16	ISSUED FOR FINAL
3	6/05/15	ISSUED FOR FINAL REVIEW

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ADDISON UNDERBRIDGE TRAIL CONNECTION FROM
 CLARK PARK TO CALIFORNIA PARK
 STRUCTURE NO. 016-P025

RAILING ANCHORAGE LAYOUT

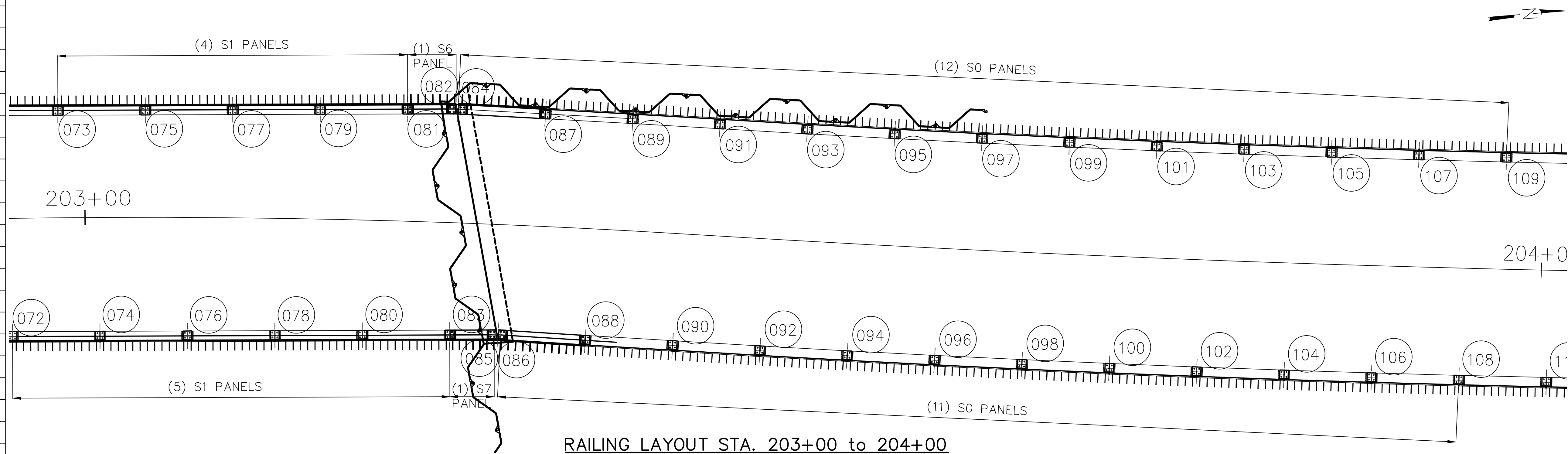
SHEET NO.
 S-26

SHEET 73 OF 158

REVISION

ANCHORAGE LOCATIONS
STA. 203+00 TO STA. 204+00

TAG	STATION	R/L	OFFSET (FT)	ANCHOR TYPE
074	203+00.98	R	8.104	
075	203+04.14	L	7.316	
076	203+07.07	R	8.084	
077	203+10.06	L	7.367	
078	203+13.15	R	8.003	
079	203+15.99	L	7.477	
080	203+19.23	R	7.862	
081	203+21.91	L		
082	203+25.03	L	7.760	B
083	203+25.30	R	7.658	
084	203+25.50	L	7.760	B
085	203+28.39	R	7.533	B
086	203+28.82	R	7.522	B
087	203+31.25	L	7.674	
088	203+34.73	R	7.624	
089	203+37.18	L		
090	203+40.80	R		
091	203+43.14	L		
092	203+46.78	R		
093	203+49.17	L		
094	203+52.75	R		
095	203+55.20	L		
096	203+58.72	R		
097	203+61.23	L		
098	203+64.69	R		
099	203+67.26	L		
100	203+70.66	R		
101	203+73.29	L		
102	203+76.63	R		
103	203+79.32	L		
104	203+82.59	R		
105	203+85.36	L		
106	203+88.56	R		
107	203+91.39	L		
108	203+94.53	R		
109	203+97.42	L		



RAILING LAYOUT STA. 203+00 to 204+00

Notes:

1. Offset is 7.646 FT from station line, typical unless noted otherwise in Table.
2. Anchor type 'A' typical unless noted otherwise in Table.

SECTION NO.: 12-E9121-00-BT
 DRAWN BY: A. PIBURN
 CHECKED BY: R. VARMA
 APPROVED BY: P. DOMBROWSKI
 DATE: 8/12/2016
 SCALE: 1/4" = 1'-0"

NO.	DATE	REVISIONS/ISSUANCES
1	8/12/16	ISSUED FOR FINAL
2	5/6/16	ISSUED FOR FINAL
3	6/05/15	ISSUED FOR FINAL REVIEW

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ADDISON UNDERBRIDGE TRAIL CONNECTION FROM
 CLARK PARK TO CALIFORNIA PARK
 STRUCTURE NO. 016-P025

RAILING ANCHORAGE LAYOUT

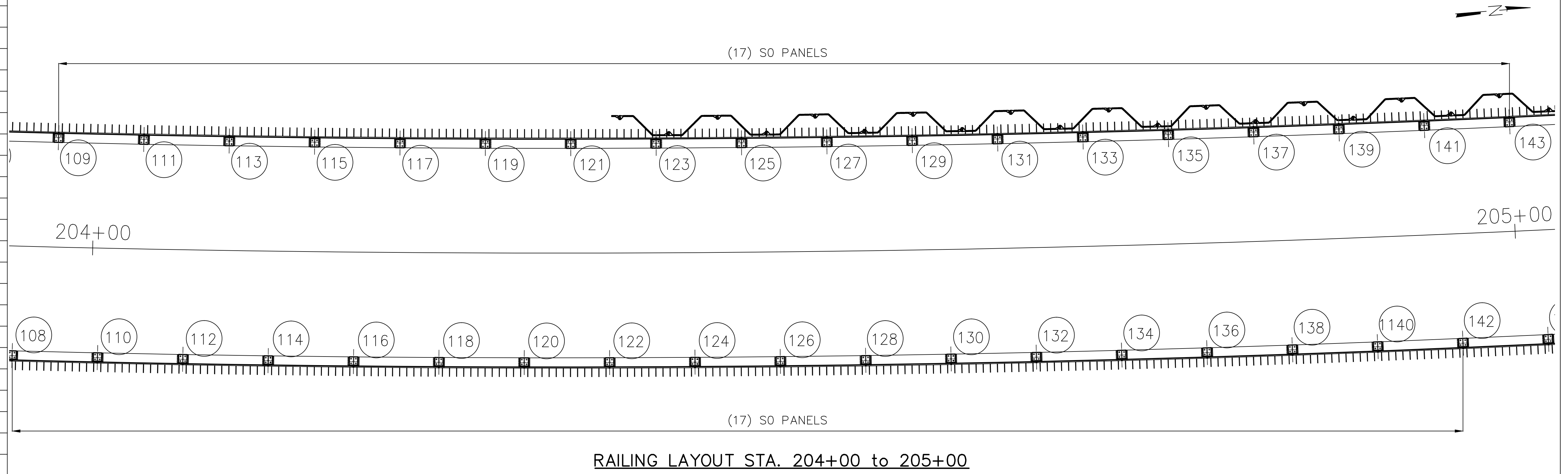
SHEET NO.
S-27

SHEET 74 OF 158

REVISION

ANCHORAGE LOCATIONS
STA. 204+00 TO STA. 205+00

TAG	STATION	R/L	OFFSET (FT)	ANCHOR TYPE
110	204+00.50	R		
111	204+03.45	L		
112	204+06.47	R		
113	204+09.48	L		
114	204+12.44	R		
115	204+15.51	L		
116	204+18.41	R		
117	204+21.54	L		
118	204+24.38	R		
119	204+27.58	L		
120	204+30.35	R		
121	204+33.61	L		
122	204+36.31	R		
123	204+39.64	L		
124	204+42.28	R		
125	204+45.67	L		
126	204+48.25	R		
127	204+51.70	L		
128	204+54.22	R		
129	204+57.73	L		
130	204+60.19	R		
131	204+63.76	L		
132	204+66.16	R		
133	204+69.80	L		
134	204+72.13	R		
135	204+75.83	L		
136	204+78.10	R		
137	204+81.86	L		
138	204+84.07	R		
139	204+87.89	L		
140	204+90.03	R		
141	204+93.92	L		
142	204+96.00	R		
143	204+99.95	L		



RAILING LAYOUT STA. 204+00 to 205+00

Notes:

1. Offset is 7.646 FT from station line, typical unless noted otherwise in Table.
2. Anchor type 'A' typical unless noted otherwise in Table.

SECTION NO.: 12-E9121-00-BT
 DRAWN BY: A. PIBURN
 CHECKED BY: R. VARMA
 APPROVED BY: P. DOMBROWSKI
 DATE: 8/12/2016
 SCALE: 1/4" = 1'-0"

NO.	DATE	REVISIONS/ISSUANCES
1	8/12/16	ISSUED FOR FINAL
2	5/6/16	ISSUED FOR FINAL
3	6/05/15	ISSUED FOR FINAL REVIEW

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ADDISON UNDERBRIDGE TRAIL CONNECTION FROM
 CLARK PARK TO CALIFORNIA PARK
 STRUCTURE NO. 016-P025

RAILING ANCHORAGE LAYOUT

SHEET NO.
 S-28

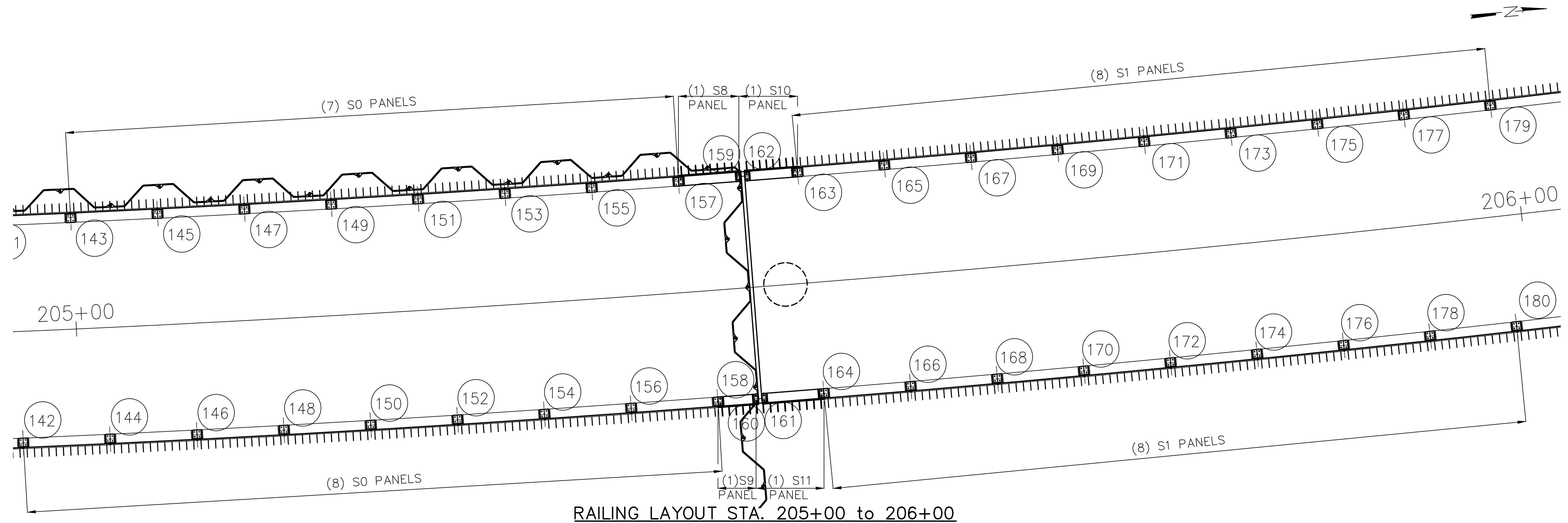
SHEET 75 OF 158

REVISION

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 January 28, 2016 10:52:46 AM

ANCHORAGE LOCATIONS
STA. 205+00 TO STA. 206+00

TAG	STATION	R/L	OFFSET (FT)	ANCHOR TYPE
144	205+01.97	R		
145	205+05.98	L		
146	205+07.94	R		
147	205+12.02	L		
148	205+13.91	R		
149	205+18.05	L		
150	205+19.88	R		
151	205+24.08	L		
152	205+25.85	R		
153	205+30.11	L		
154	205+31.82	R		
155	205+36.14	L		
156	205+37.79	R		
157	205+42.17	L		
158	205+43.75	R		
159	205+46.37	L	B	
160	205+46.37	R	B	
161	205+46.93	R	B	
162	205+46.94	L	B	
163	205+50.45	L		
164	205+51.05	R		
165	205+56.49	L		
166	205+57.02	R		
167	205+62.52	L		
168	205+62.99	R		
169	205+68.55	L		
170	205+68.96	R		
171	205+74.58	L		
172	205+74.93	R		
173	205+80.61	L		
174	205+80.89	R		
175	205+86.64	L		
176	205+86.86	R		
177	205+92.67	L		
178	205+92.83	R		
179	205+98.71	L		
180	205+98.80	R		



RAILING LAYOUT STA. 205+00 to 206+00

- Notes:
1. Offset is 7.646 FT from station line, typical unless noted otherwise in Table.
 2. Anchor type 'A' typical unless noted otherwise in Table.

P:\projects\20000\03346 - CDOT - Addison Underbridge Connection - DISCIPLINE (Civil\Working) Sheets\03 - STRUCTURAL\01\EP025-2346-025_03B-Railing_Anchorage_Layout.dwg
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SECTION NO.: 12-E9121-00-BT
 DRAWN BY: A. PIBURN
 CHECKED BY: R. VARMA
 APPROVED BY: P. DOMBROWSKI
 DATE: 8/12/2016
 SCALE: 1/4" = 1'-0"

NO.	DATE	REVISIONS/ISSUANCES
1	8/12/16	ISSUED FOR FINAL
2	5/6/16	ISSUED FOR FINAL
3	6/05/15	ISSUED FOR FINAL REVIEW

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ADDISON UNDERBRIDGE TRAIL CONNECTION FROM
 CLARK PARK TO CALIFORNIA PARK
 STRUCTURE NO. 016-P025

RAILING ANCHORAGE LAYOUT

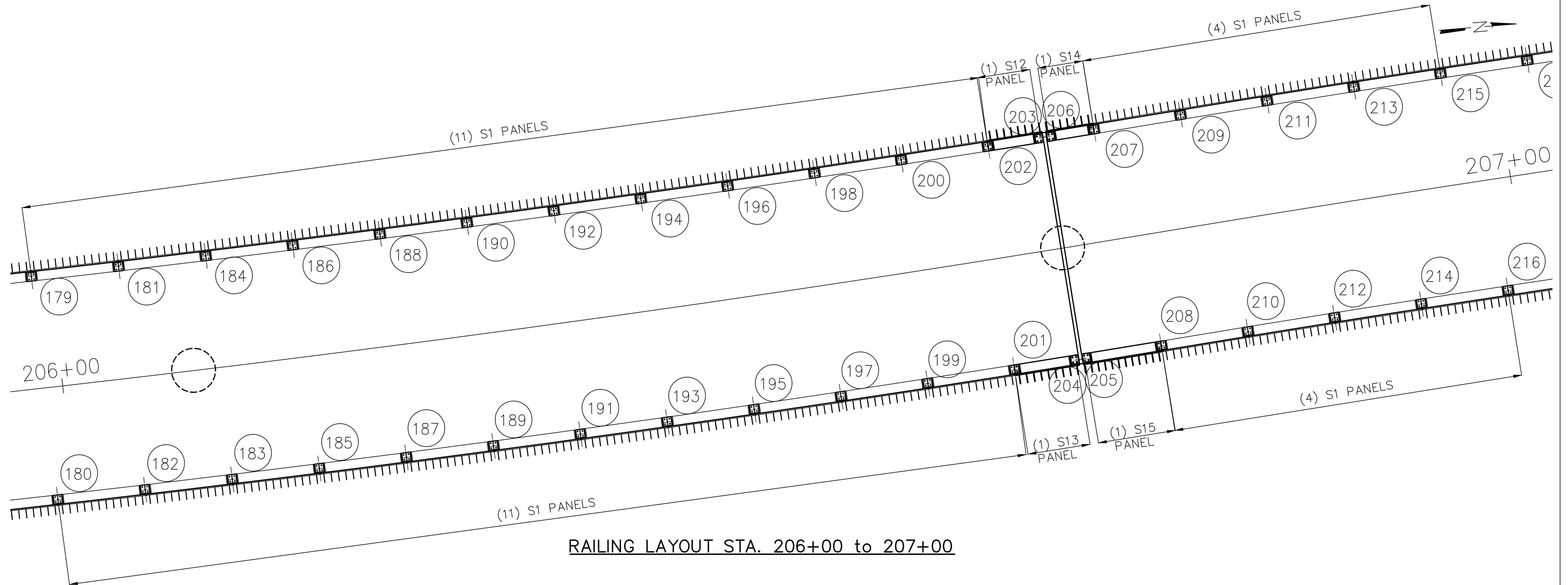
SHEET NO.
 S-29

SHEET 76 OF 158

REVISION

ANCHORAGE LOCATIONS
STA. 206+00 TO STA. 207+00

TAG	STATION	R/L	OFFSET (FT)	ANCHOR TYPE
181	206+04.74	L		
182	206+04.77	R		
183	206+10.74	R		
184	206+10.77	L		
185	206+16.71	R		
186	206+16.80	L		
187	206+22.68	R		
188	206+22.83	L		
189	206+28.65	R		
190	206+28.86	L		
191	206+34.61	R		
192	206+34.89	L		
193	206+40.58	R		
194	206+40.93	L		
195	206+46.55	R		
196	206+46.96	L		
197	206+52.52	R		
198	206+52.99	L		
199	206+58.49	R		
200	206+59.02	L		
201	206+64.46	R		
202	206+65.05	L		
203	206+68.72	L		B
204	206+68.73	R		B
205	206+69.29	R		B
206	206+69.29	L		B
207	206+72.42	L		
208	206+74.67	R		
209	206+78.43	L		
210	206+80.59	R		
211	206+84.40	L		
212	206+86.62	R		
213	206+90.37	L		
214	206+92.65	R		
215	206+96.34	L		
216	206+98.68	R		



RAILING LAYOUT STA. 206+00 to 207+00

Notes:

- Offset is 7.646 FT from station line, typical unless noted otherwise in Table.
- Anchor type 'A' typical unless noted otherwise in Table.

SECTION NO.: 12-E9121-00-BT
 DRAWN BY: A. PIBURN
 CHECKED BY: R. VARMA
 APPROVED BY: P. DOMBROWSKI
 DATE: 8/12/2016
 SCALE: 1/4" = 1'-0"

NO.	DATE	REVISIONS/ISSUANCES
1	8/12/16	ISSUED FOR FINAL
2	5/6/16	ISSUED FOR FINAL
3	6/05/15	ISSUED FOR FINAL REVIEW

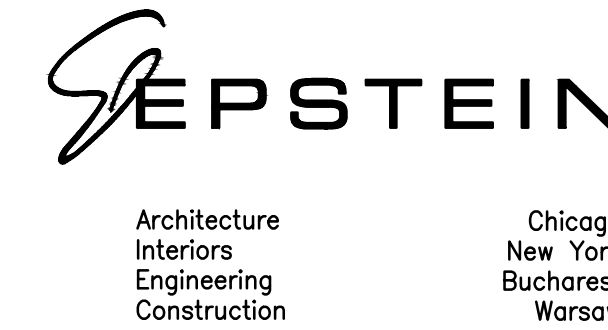
CITY OF CHICAGO

DEPARTMENT OF TRANSPORTATION
 30 N LaSalle St. Suite 1100
 Chicago, IL 60602
 312.744.3600



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ADDISON UNDERBRIDGE TRAIL CONNECTION FROM
 CLARK PARK TO CALIFORNIA PARK
 STRUCTURE NO. 016-P025

RAILING ANCHORAGE LAYOUT

SHEET NO.
S-30

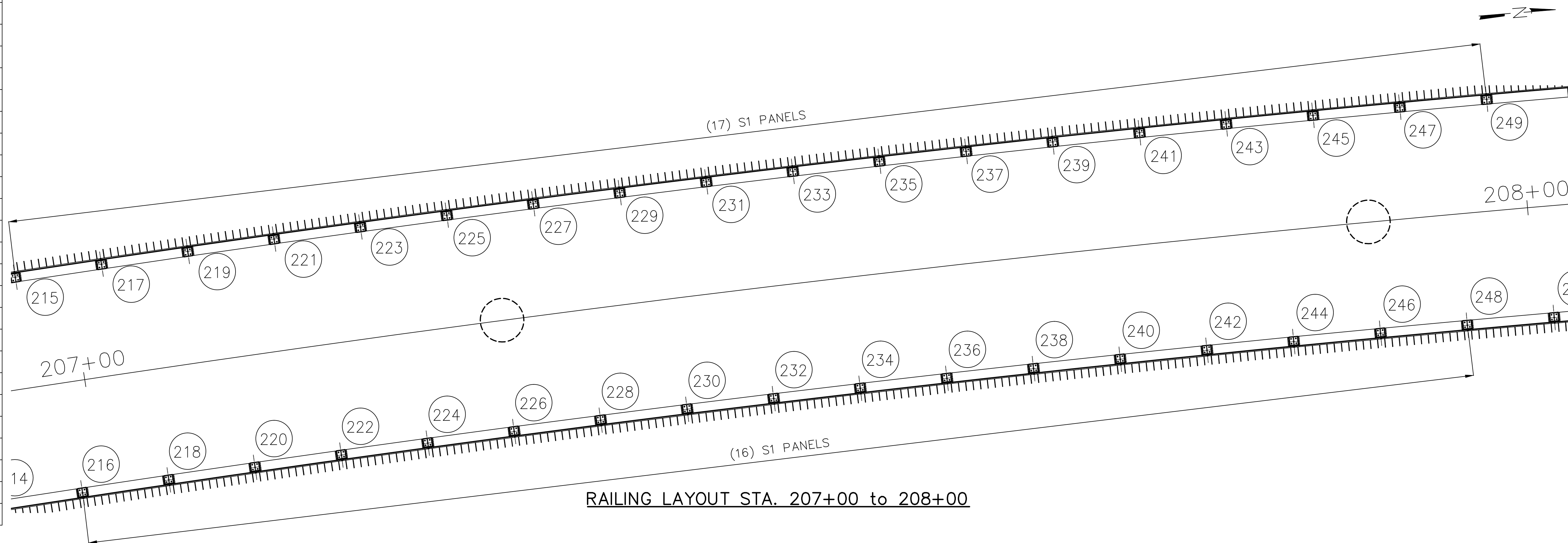
SHEET 77 OF 158

REVISION

P:\Projects\20000\03346 - CDOT - Addison Underbridge Connection - DISCIPLINE\Civil\Working\Sheets\08_STRUCTURAL\016P025-2346-025_039-Railing_Anchorage_Layout.dwg
 January 28, 2016 10:52:58 AM

ANCHORAGE LOCATIONS
STA. 207+00 TO STA. 208+00

TAG	STATION	R/L	OFFSET (FT)	ANCHOR TYPE
217	207+02.31	L		
218	207+04.71	R		
219	207+08.29	L		
220	207+10.74	R		
221	207+14.26	L		
222	207+16.77	R		
223	207+20.23	L		
224	207+22.80	R		
225	207+26.20	L		
226	207+28.83	R		
227	207+32.17	L		
228	207+34.86	R		
229	207+38.14	L		
230	207+40.88	R		
231	207+44.11	L		
232	207+46.91	R		
233	207+50.08	L		
234	207+52.94	R		
235	207+56.06	L		
236	207+58.97	R		
237	207+62.03	L		
238	207+65.00	R		
239	207+68.00	L		
240	207+71.03	R		
241	207+73.97	L		
242	207+77.06	R		
243	207+79.94	L		
244	207+83.09	R		
245	207+85.91	L		
246	207+89.12	R		
247	207+91.89	L		
248	207+95.15	R		
249	207+97.86	L		



- Notes:**
1. Offset is 7.646 FT from station line, typical unless noted otherwise in Table.
 2. Anchor type 'A' typical unless noted otherwise in Table.

SECTION NO.: 12-E9121-00-BT
 DRAWN BY: A. PIBURN
 CHECKED BY: R. VARMA
 APPROVED BY: P. DOMBROWSKI
 DATE: 8/12/2016
 SCALE: 1/4" = 1'-0"

NO.	DATE	REVISIONS/ISSUANCES
1	8/12/16	ISSUED FOR FINAL
2	5/6/16	ISSUED FOR FINAL
3	6/05/15	ISSUED FOR FINAL REVIEW

CITY OF CHICAGO

DEPARTMENT OF TRANSPORTATION
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 Chicago, IL 60602
 312.744.3600



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ADDISON UNDERBRIDGE TRAIL CONNECTION FROM
 CLARK PARK TO CALIFORNIA PARK
 STRUCTURE NO. 016-P025

RAILING ANCHORAGE LAYOUT

SHEET NO.
S-31

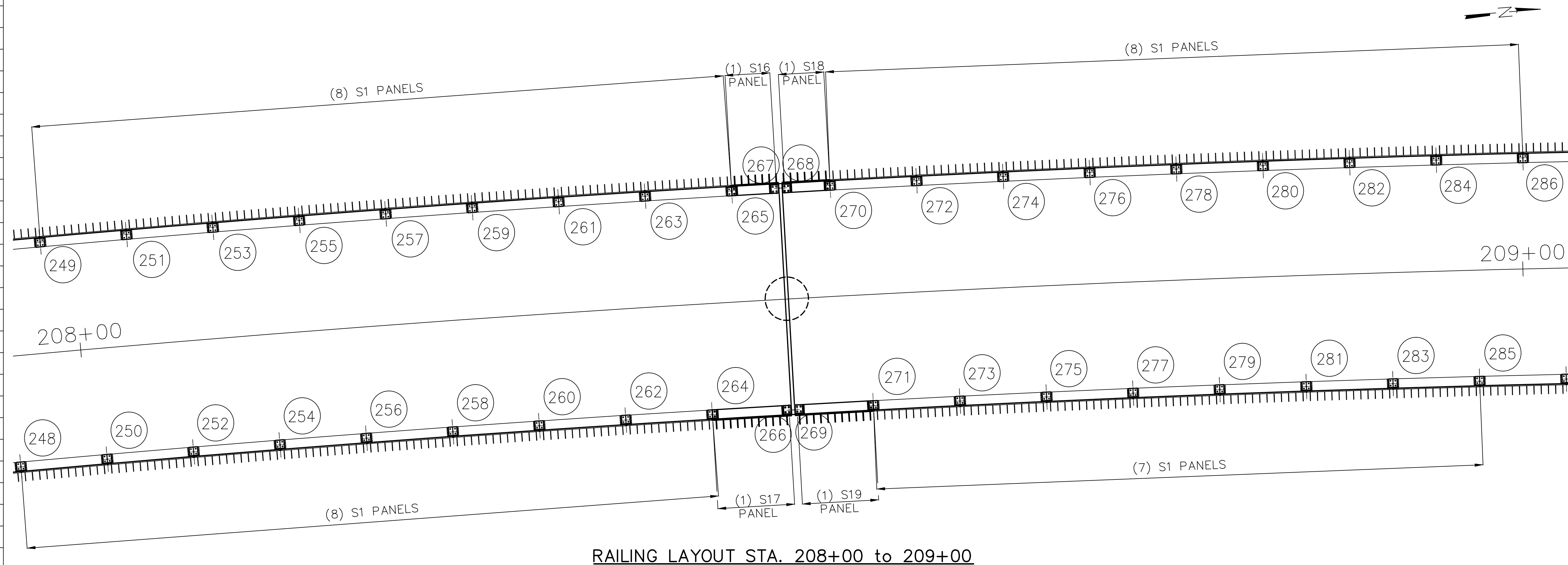
SHEET 78 OF 158

REVISION

P:\projects\20000\03146 - CDOT - Addison Underbridge Connection - DISCIPLINE\20000\3146 - CDOT - Addison Underbridge Connection - DISCIPLINE\Civil\Working Sheets\08 STRUCTURAL\016P025-23146-025_039-Railing_Anchorage_Layouts.dwg 1/24/2017 10:53:04 PM

ANCHORAGE LOCATIONS
STA. 208+00 TO STA. 209+00

TAG	STATION	R/L	OFFSET (FT)	ANCHOR TYPE
250	208+01.17	R		
251	208+03.83	L		
252	208+07.20	R		
253	208+09.80	L		
254	208+13.23	R		
255	208+15.77	L		
256	208+19.26	R		
257	208+21.74	L		
258	208+25.29	R		
259	208+27.72	L		
260	208+31.32	R		
261	208+33.69	L		
262	208+37.35	R		
263	208+39.66	L		
264	208+43.38	R		
265	208+45.63	L		
266	208+48.72	R		B
267	208+48.72	L		B
268	208+49.28	L		B
269	208+49.29	R		B
270	208+52.42	L		
271	208+54.60	R		
272	208+58.39	L		
273	208+60.63	R		
274	208+64.36	L		
275	208+66.66	R		
276	208+70.33	L		
277	208+72.68	R		
278	208+76.30	L		
279	208+78.71	R		
280	208+82.28	L		
281	208+84.74	R		
282	208+88.25	L		
283	208+90.77	R		
284	208+94.22	L		
285	208+96.80	R		



RAILING LAYOUT STA. 208+00 to 209+00

Notes:

1. Offset is 7.646 FT from station line, typical unless noted otherwise in Table.
2. Anchor type 'A' typical unless noted otherwise in Table.

SECTION NO.: 12-E9121-00-BT
 DRAWN BY: A. PIBURN
 CHECKED BY: R. VARMA
 APPROVED BY: P. DOMBROWSKI
 DATE: 8/12/2016
 SCALE: 1/4" = 1'-0"

NO.	DATE	REVISIONS/ISSUANCES
1	8/12/16	ISSUED FOR FINAL
2	5/6/16	ISSUED FOR FINAL
3	6/05/15	ISSUED FOR FINAL REVIEW

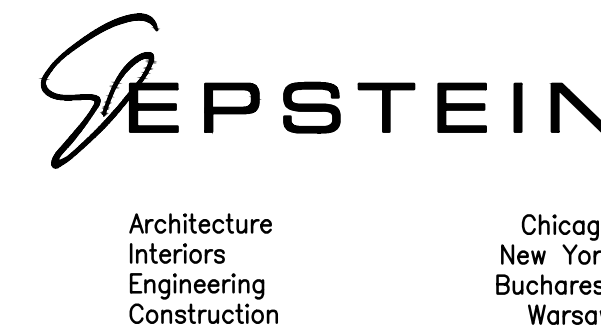
CITY OF CHICAGO

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ADDISON UNDERBRIDGE TRAIL CONNECTION FROM
 CLARK PARK TO CALIFORNIA PARK
 STRUCTURE NO. 016-P025

RAILING ANCHORAGE LAYOUT

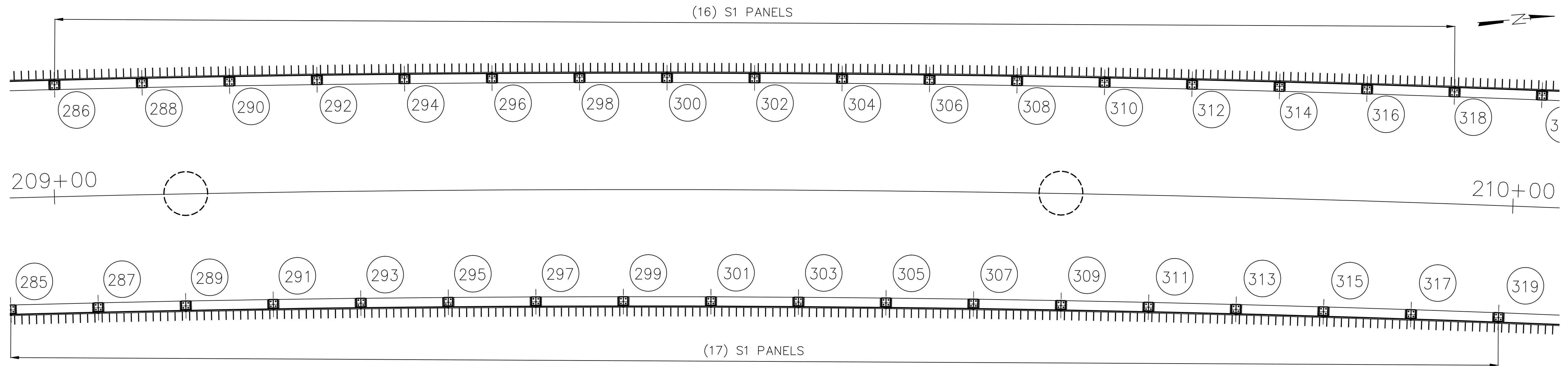
SHEET NO.
S-32

SHEET 79 OF 158

REVISION

ANCHORAGE LOCATIONS
STA. 209+00 TO STA. 210+00

TAG	STATION	R/L	OFFSET (FT)	ANCHOR TYPE
286	209+00.19	L		
287	209+02.83	R		
288	209+06.16	L		
289	209+08.86	R		
290	209+12.13	L		
291	209+14.89	R		
292	209+18.10	L		
293	209+20.92	R		
294	209+24.07	L		
295	209+26.95	R		
296	209+30.05	L		
297	209+32.97	R		
298	209+36.02	L		
299	209+39.00	R		
300	209+41.99	L		
301	209+45.03	R		
302	209+47.96	L		
303	209+51.06	R		
304	209+53.93	L		
305	209+57.09	R		
306	209+59.90	L		
307	209+63.12	R		
308	209+65.87	L		
309	209+69.15	R		
310	209+71.85	L		
311	209+75.18	R		
312	209+77.82	L		
313	209+81.21	R		
314	209+83.79	L		
315	209+87.24	R		
316	209+89.76	L		
317	209+93.26	R		
318	209+95.73	L		
319	209+99.29	R		



RAILING LAYOUT STA. 209+00 to 210+00

Notes:

1. Offset is 7.646 FT from station line, typical unless noted otherwise in Table.
2. Anchor type 'A' typical unless noted otherwise in Table.

SECTION NO.: 12-E9121-00-BT
 DRAWN BY: A. PIBURN
 CHECKED BY: R. VARMA
 APPROVED BY: P. DOMBROWSKI
 DATE: 8/12/2016
 SCALE: 1/4" = 1'-0"

NO.	DATE	REVISIONS/ISSUANCES
1	8/12/16	ISSUED FOR FINAL
2	5/6/16	ISSUED FOR FINAL
3	6/05/15	ISSUED FOR FINAL REVIEW

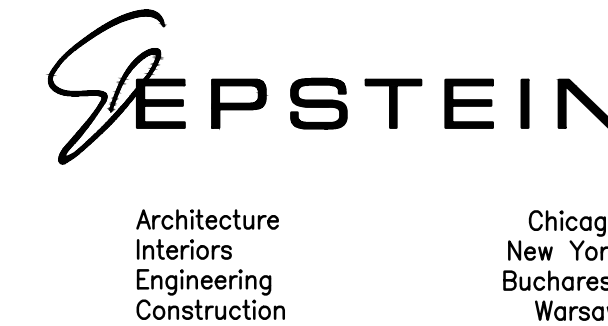
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 Chicago, IL 60602
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ADDISON UNDERBRIDGE TRAIL CONNECTION FROM
 CLARK PARK TO CALIFORNIA PARK
 STRUCTURE NO. 016-P025

RAILING ANCHORAGE LAYOUT

SHEET NO.
S-33

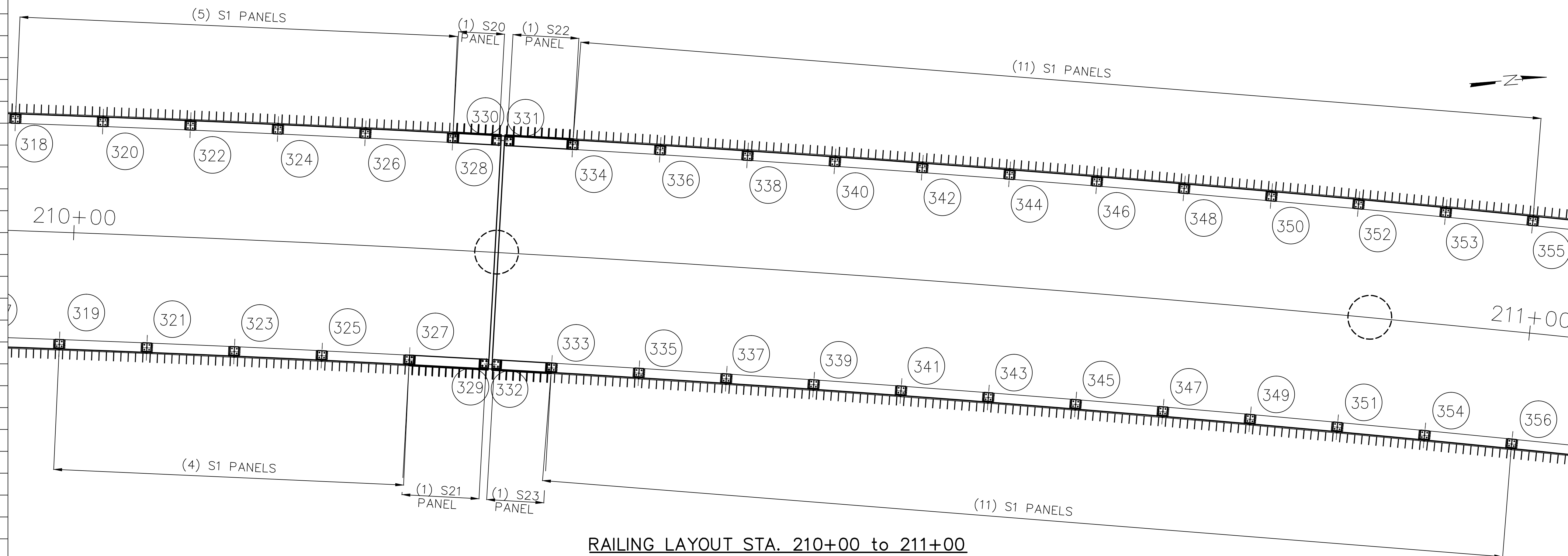
SHEET 80 OF 158

REVISION

P:\projects\200000\03346 - CDOT - Addison Underbridge Connection - DISCIPLINE\20000\03346 - CDOT - Addison Underbridge Connection - DISCIPLINE\Civil\Working\Sheets\08_STRUCTURAL\016P025-2346-025_039-Railing_Anchorage_Layout.dwg 1/24/2017 10:53:17 PM

ANCHORAGE LOCATIONS
STA. 210+00 TO STA. 211+00

TAG	STATION	R/L	OFFSET (FT)	ANCHOR TYPE
320	210+01.70	L		
321	210+05.32	R		
322	210+07.67	L		
323	210+11.35	R		
324	210+13.65	L		
325	210+17.38	R		
326	210+19.62	L		
327	210+23.41	R		
328	210+25.59	L		
329	210+28.72	R		B
330	210+28.72	L		B
331	210+29.28	L		B
332	210+29.28	R		B
333	210+33.22	R		
334	210+33.78	L		
335	210+39.25	R		
336	210+39.75	L		
337	210+45.28	R		
338	210+45.72	L		
339	210+51.30	R		
340	210+51.69	L		
341	210+57.33	R		
342	210+57.66	L		
343	210+63.36	R		
344	210+63.64	L		
345	210+69.39	R		
346	210+69.61	L		
347	210+75.42	R		
348	210+75.58	L		
349	210+81.45	R		
350	210+81.55	L		
351	210+87.48	R		
352	210+87.52	L		
353	210+93.49	L		
354	210+93.51	R		
355	210+99.46	L		
356	210+99.54	R		



RAILING LAYOUT STA. 210+00 to 211+00

Notes:

1. Offset is 7.646 FT from station line, typical unless noted otherwise in Table.
2. Anchor type 'A' typical unless noted otherwise in Table.

SECTION NO.: 12-E9121-00-BT
 DRAWN BY: A. PIBURN
 CHECKED BY: R. VARMA
 APPROVED BY: P. DOMBROWSKI
 DATE: 8/12/2016
 SCALE: 1/4" = 1'-0"

NO.	DATE	REVISIONS/ISSUANCES
1	8/12/16	ISSUED FOR FINAL
2	5/6/16	ISSUED FOR FINAL
3	6/05/15	ISSUED FOR FINAL REVIEW

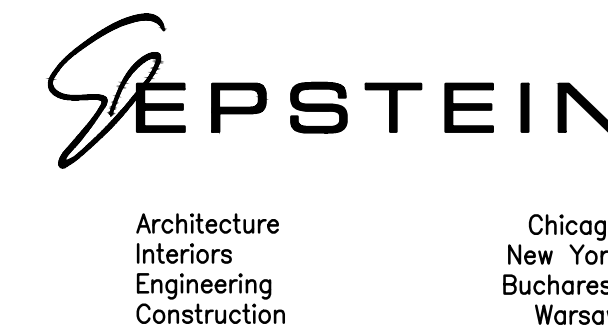
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ADDISON UNDERBRIDGE TRAIL CONNECTION FROM
 CLARK PARK TO CALIFORNIA PARK
 STRUCTURE NO. 016-P025

RAILING ANCHORAGE LAYOUT

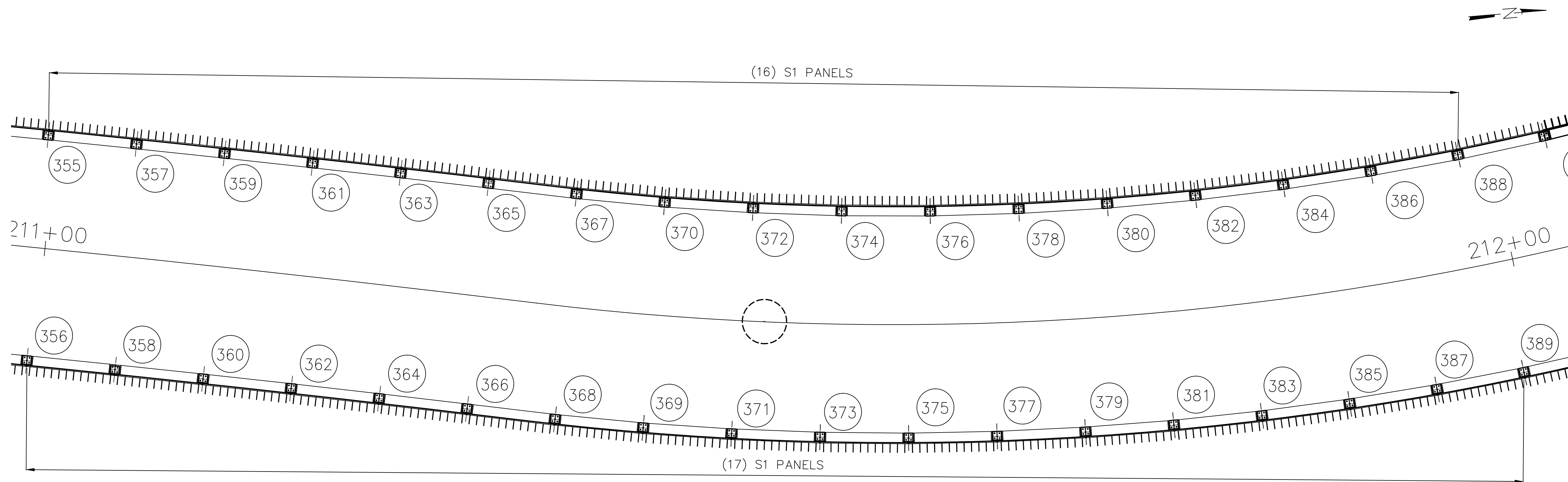
SHEET NO.
S-34

SHEET 81 OF 158

REVISION

**ANCHORAGE LOCATIONS
STA. 211+00 TO STA. 212+00**

TAG	STATION	R/L	OFFSET (FT)	ANCHOR TYPE
357	211+05.44	L		
358	211+05.57	R		
359	211+11.41	L		
360	211+11.59	R		
361	211+17.38	L		
362	211+17.62	R		
363	211+23.35	L		
364	211+23.65	R		
365	211+29.32	L		
366	211+29.68	R		
367	211+35.37	L		
368	211+35.62	R		
369	211+41.40	R		
370	211+41.61	L		
371	211+47.17	R		
372	211+47.85	L		
373	211+52.95	R		
374	211+54.09	L		
375	211+58.73	R		
376	211+60.33	L		
377	211+64.51	R		
378	211+66.57	L		
379	211+70.29	R		
380	211+72.81	L		
381	211+76.06	R		
382	211+79.05	L		
383	211+81.84	R		
384	211+85.29	L		
385	211+87.62	R		
386	211+91.53	L		
387	211+93.40	R		
388	211+97.77	L		
389	211+99.18	R		



RAILING LAYOUT STA. 211+00 to 212+00

Notes:

1. Offset is 7.646 FT from station line, typical unless noted otherwise in Table.
2. Anchor type 'A' typical unless noted otherwise in Table.

SECTION NO.: 12-E9121-00-BT
 DRAWN BY: A. PIBURN
 CHECKED BY: R. VARMA
 APPROVED BY: P. DOMBROWSKI
 DATE: 8/12/2016
 SCALE: 1/4" = 1'-0"

NO.	DATE	REVISIONS/ISSUANCES
1	8/12/16	ISSUED FOR FINAL
2	5/6/16	ISSUED FOR FINAL
3	6/05/15	ISSUED FOR FINAL REVIEW

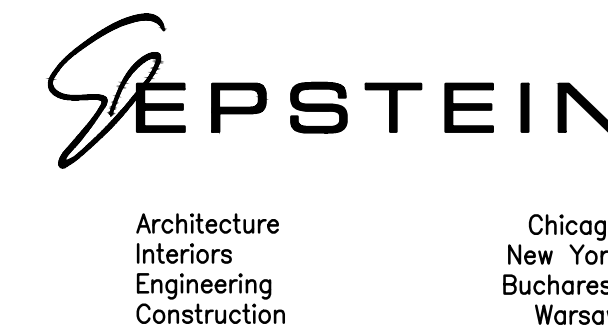
CITY OF CHICAGO

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ADDISON UNDERBRIDGE TRAIL CONNECTION FROM
 CLARK PARK TO CALIFORNIA PARK
 STRUCTURE NO. 016-P025

RAILING ANCHORAGE LAYOUT

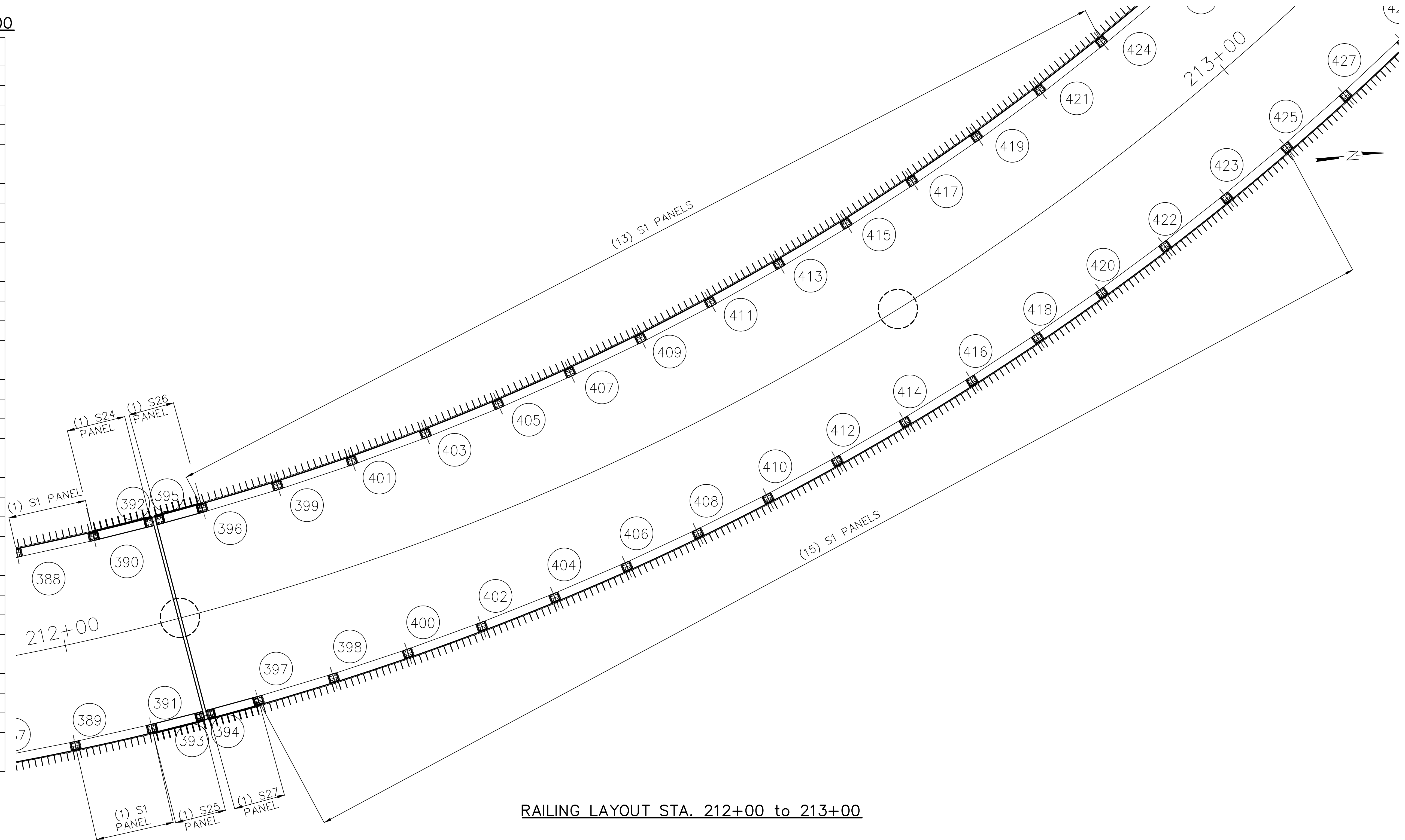
SHEET NO.
S-35

SHEET 82 OF 158

REVISION

**ANCHORAGE LOCATIONS
STA. 212+00 TO STA. 213+00**

TAG	STATION	R/L	OFFSET (FT)	ANCHOR TYPE
390	212+04.01	L		
391	212+04.96	R		
392	212+08.71	L		B
393	212+08.73	R		B
394	212+09.27	R		B
395	212+09.29	L		B
396	212+12.94	L		
397	212+13.07	R		
398	212+18.85	R		
399	212+19.18	L		
400	212+24.62	R		
401	212+25.42	L		
402	212+30.40	R		
403	212+31.66	L		
404	212+36.18	R		
405	212+37.90	L		
406	212+41.96	R		
407	212+44.14	L		
408	212+47.74	R		
409	212+50.38	L		
410	212+53.51	R		
411	212+56.62	L		
412	212+59.29	R		
413	212+62.86	L		
414	212+65.07	R		
415	212+69.10	L		
416	212+70.85	R		
417	212+75.34	L		
418	212+76.63	R		
419	212+81.58	L		
420	212+82.40	R		
421	212+87.82	L		
422	212+88.18	R		
423	212+93.96	R		
424	212+94.06	L		
425	212+99.74	R		



RAILING LAYOUT STA. 212+00 to 213+00

- Notes:**
1. Offset is 7.646 FT from station line, typical unless noted otherwise in Table.
 2. Anchor type 'A' typical unless noted otherwise in Table.

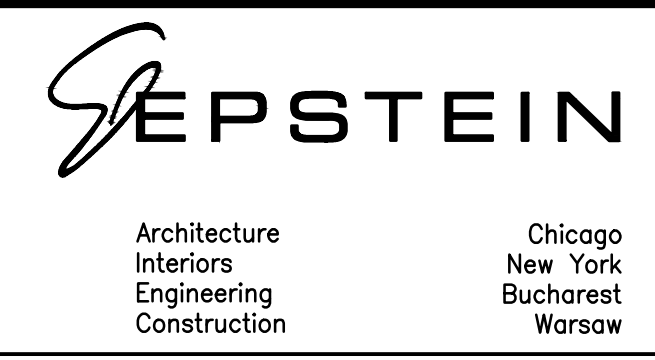
SECTION NO.: 12-E9121-00-BT
 DRAWN BY: A. PIBURN
 CHECKED BY: R. VARMA
 APPROVED BY: P. DOMBROWSKI
 DATE: 8/12/2016
 SCALE: 1/4" = 1'-0"

NO.	DATE	REVISIONS/ISSUANCES
1	8/12/16	ISSUED FOR FINAL
2	5/6/16	ISSUED FOR FINAL
3	6/05/15	ISSUED FOR FINAL REVIEW

CITY OF CHICAGO
 DEPARTMENT OF TRANSPORTATION
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 Chicago, IL 60602
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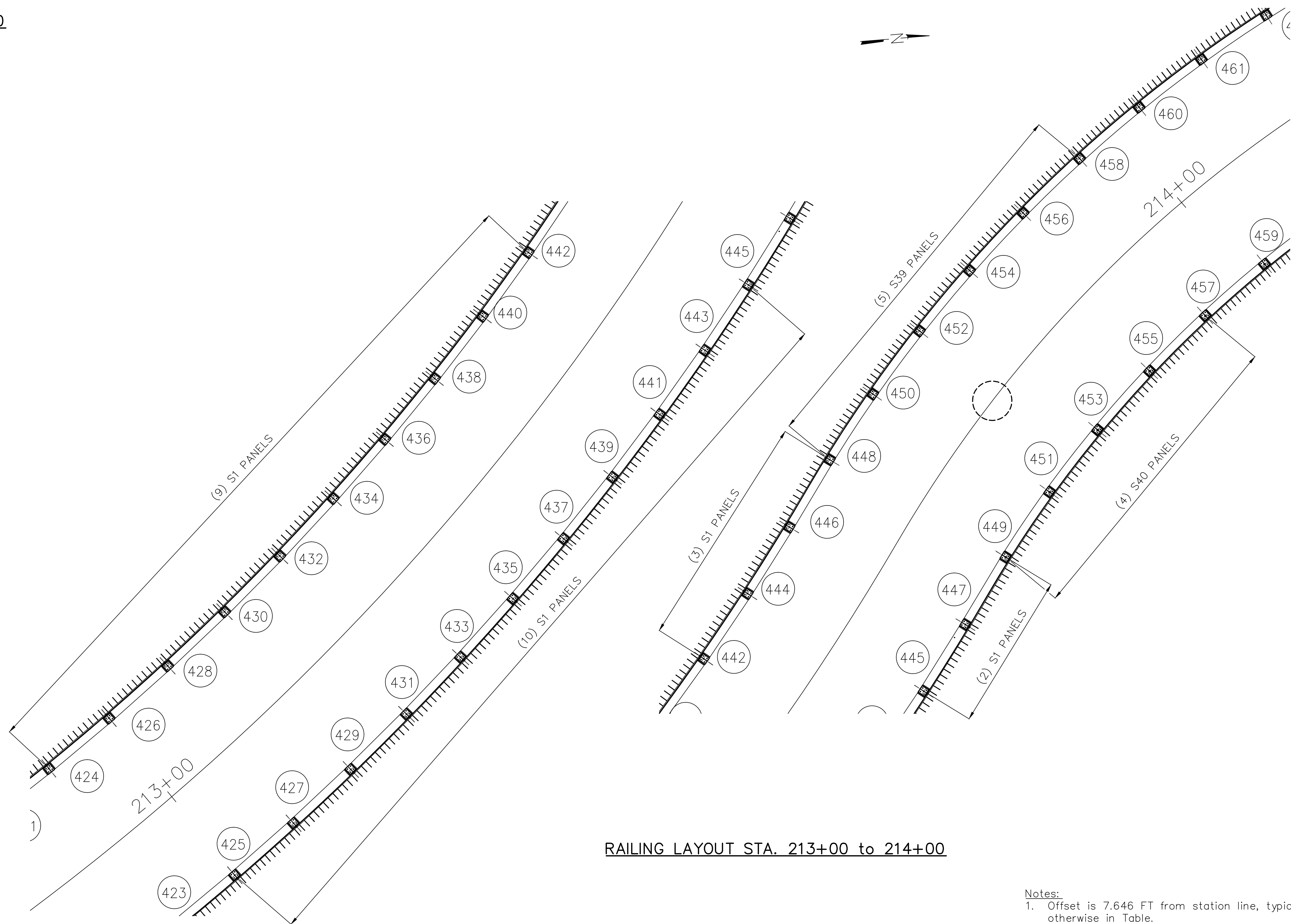
ADDISON UNDERBRIDGE TRAIL CONNECTION FROM
 CLARK PARK TO CALIFORNIA PARK
 STRUCTURE NO. 016-P025

RAILING ANCHORAGE LAYOUT

SHEET NO. S-36
 SHEET 83 OF 158

**ANCHORAGE LOCATIONS
STA. 213+00 TO STA. 214+00**

TAG	STATION	R/L	OFFSET (FT)	ANCHOR TYPE
426	213+00.30	L		
427	213+05.52	R		
428	213+06.54	L		
429	213+11.29	R		
430	213+12.78	L		
431	213+17.07	R		
432	213+19.02	L		
433	213+22.85	R		
434	213+25.26	L		
435	213+28.63	R		
436	213+31.50	L		
437	213+34.41	R		
438	213+37.74	L		
439	213+40.18	R		
440	213+43.98	L		
441	213+45.96	R		
442	213+50.21	L		
443	213+51.74	R		
444	213+56.46	L		
445	213+57.52	R		
446	213+62.70	L		
447	213+63.30	R		
448	213+68.65	L		
449	213+69.44	R		
450	213+74.22	L		
451	213+75.94	R		
452	213+79.79	L		
453	213+82.44	R		
454	213+85.36	L		
455	213+88.94	R		
456	213+90.94	L		
457	213+95.44	R		
458	213+96.51	L		



RAILING LAYOUT STA. 213+00 to 214+00

Notes:

1. Offset is 7.646 FT from station line, typical unless noted otherwise in Table.
2. Anchor type 'A' typical unless noted otherwise in Table.

SECTION NO.: 12-E9121-00-BT
 DRAWN BY: A. PIBURN
 CHECKED BY: R. VARMA
 APPROVED BY: P. DOMBROWSKI
 DATE: 8/12/2016
 SCALE: 1/4" = 1'-0"

NO.	DATE	REVISIONS/ISSUANCES
1	8/12/16	ISSUED FOR FINAL
2	5/6/16	ISSUED FOR FINAL
3	6/05/15	ISSUED FOR FINAL REVIEW

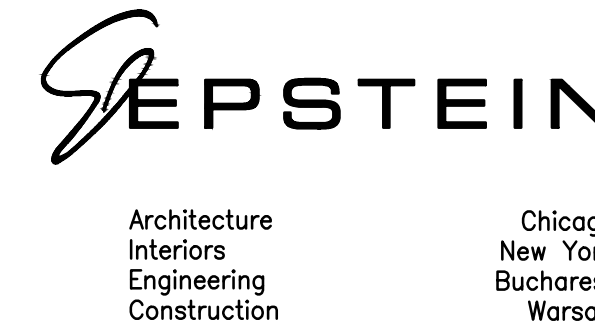
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ADDISON UNDERBRIDGE TRAIL CONNECTION FROM
 CLARK PARK TO CALIFORNIA PARK
 STRUCTURE NO. 016-P025

RAILING ANCHORAGE LAYOUT

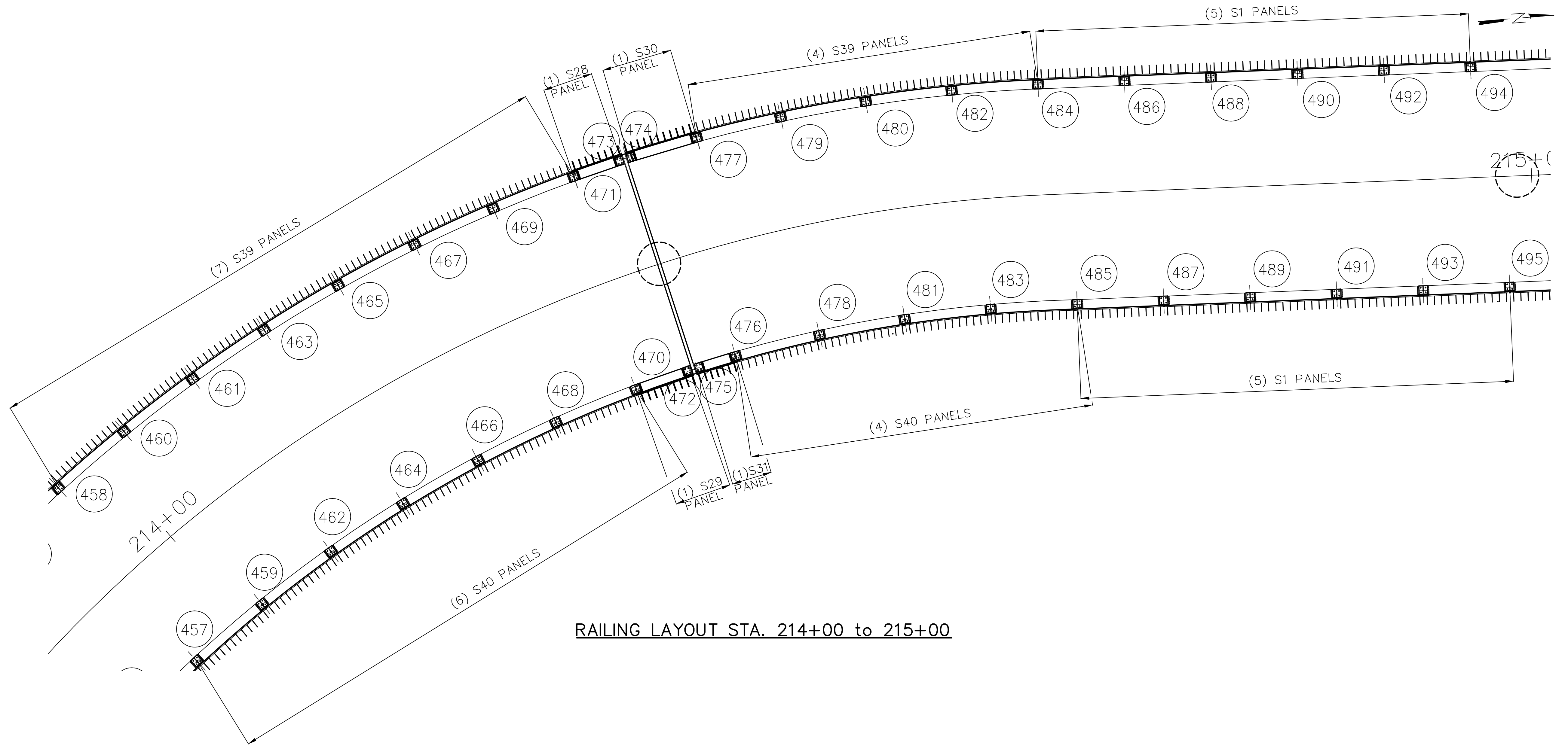
SHEET NO.
S-37

SHEET 84 OF 158

REVISION

ANCHORAGE LOCATIONS
STA. 214+00 TO STA. 215+00

TAG	STATION	R/L	OFFSET (FT)	ANCHOR TYPE
459	214+01.94	R		
460	214+02.08	L		
461	214+07.65	L		
462	214+08.44	R		
463	214+13.22	L		
464	214+14.94	R		
465	214+18.80	L		
466	214+21.44	R		
467	214+24.37	L		
468	214+27.94	R		
469	214+29.94	L		
470	214+34.44	R		
471	214+35.51	L		
472	214+38.70	R		B
473	214+38.75	L		B
474	214+39.27	L		B
475	214+39.31	R		B
476	214+42.33	R		
477	214+43.81	L		
478	214+48.83	R		
479	214+49.38	L		
480	214+54.96	L		
481	214+55.33	R		
482	214+60.53	L		
483	214+61.83	R		
484	214+66.10	L		
485	214+68.18	R		
486	214+72.08	L		
487	214+74.18	R		
488	214+78.08	L		
489	214+80.18	R		
490	214+84.08	L		
491	214+86.18	R		
492	214+90.08	L		
493	214+92.18	R		
494	214+92.99	L		
495	214+98.18	R		



RAILING LAYOUT STA. 214+00 to 215+00

Notes:

1. Offset is 7.646 FT from station line, typical unless noted otherwise in Table.
2. Anchor type 'A' typical unless noted otherwise in Table.

SECTION NO.: 12-E9121-00-BT
 DRAWN BY: A. PIBURN
 CHECKED BY: R. VARMA
 APPROVED BY: P. DOMBROWSKI
 DATE: 8/12/2016
 SCALE: 1/4" = 1'-0"

NO.	DATE	REVISIONS/ISSUANCES
1	8/12/16	ISSUED FOR FINAL
2	5/6/16	ISSUED FOR FINAL
3	6/05/15	ISSUED FOR FINAL REVIEW

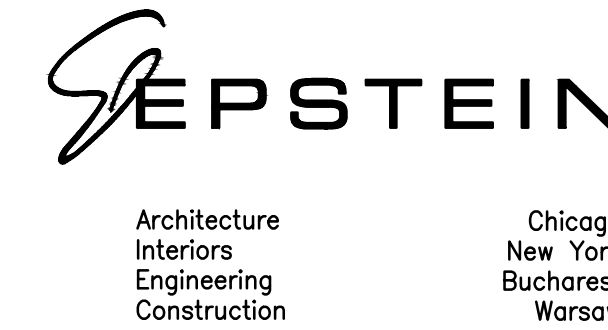
CITY OF CHICAGO

DEPARTMENT OF TRANSPORTATION
 30 N LaSalle St. Suite 1100
 Chicago, IL 60602
 312.744.3600



CONSULTANT

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 600 W. Fulton Street
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ADDISON UNDERBRIDGE TRAIL CONNECTION FROM
 CLARK PARK TO CALIFORNIA PARK
 STRUCTURE NO. 016-P025

RAILING ANCHORAGE LAYOUT

SHEET NO.
S-38

SHEET 85 OF 158

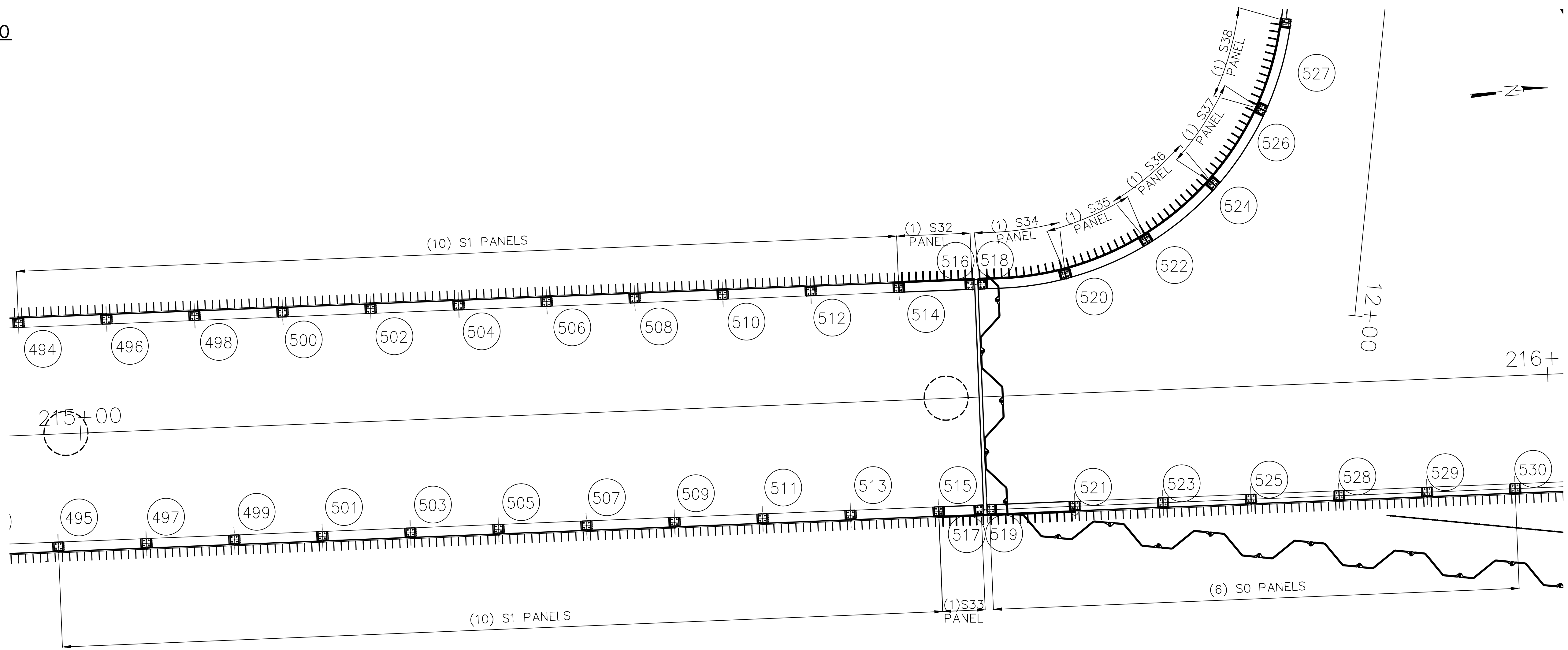
REVISION

**ANCHORAGE LOCATIONS
STA. 215+00 TO STA. 216+00**

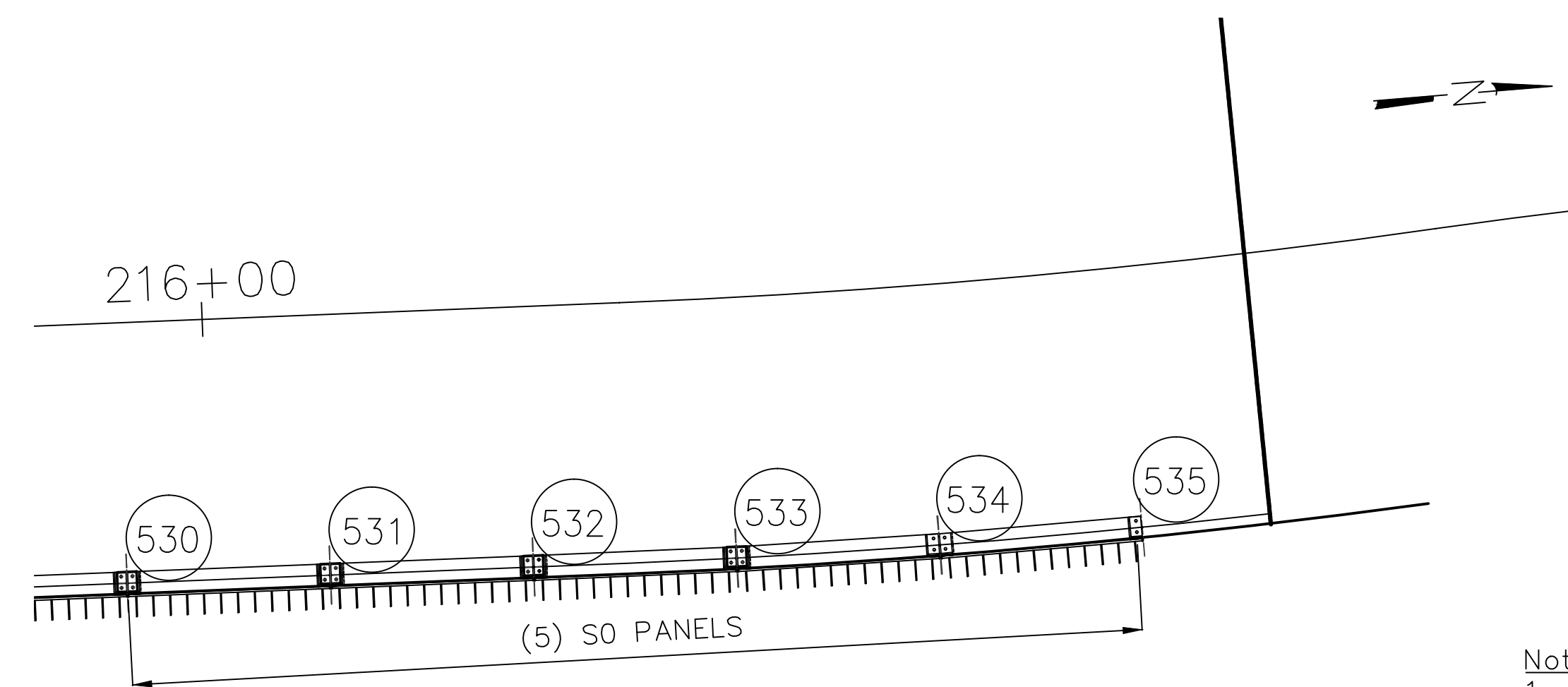
TAG	STATION	R/L	OFFSET (FT)	ANCHOR TYPE
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497	214+04.18	R		
498	214+08.08	L		
499	214+10.18	R		
500	214+14.08	L		
501	214+16.18	R		
502	214+20.08	L		
503	214+22.18	R		
504	214+26.08	L		
505	214+28.18	R		
506	214+32.08	L		
507	214+34.18	R		
508	214+38.08	L		
509	214+40.18	R		
510	214+44.08	L		
511	214+46.18	R		
512	214+50.08	L		
513	214+52.18	R		
514	214+56.08	L		
515	214+58.18	R		
516	214+61.08	L		B
517	214+61.09	R		B
518	214+61.65	L		B
519	214+61.65	R		B
520	214+67.44	L	8.117	
521	214+67.48	R		
522	214+73.02	L	10.238	
523	214+73.48	R		
524	214+77.73	L	13.874	
525	214+79.48	R		
526	214+81.23	L	18.813	
527	214+83.18	L	24.758	B
528	214+85.48	R		
529	214+91.48	R		
530	214+97.48	R		

**ANCHORAGE LOCATIONS
STA. 216+00 TO STA. 216+26.85**

TAG	STATION	R/L	OFFSET (FT)	ANCHOR TYPE
531	216+03.48	R		
532	216+09.44	R		
533	216+15.38	R		
534	216+21.21	R		
535	216+26.85	R	7.657	B



RAILING LAYOUT STA. 215+00 to 216+00



RAILING LAYOUT STA. 216+00 to 216+26.85

- Notes:**
1. Offset is 7.646 FT from station line, typical unless noted otherwise in Table.
 2. Anchor type 'A' typical unless noted otherwise in Table.

SECTION NO.: 12-E9121-00-BT
 DRAWN BY: A. PIBURN
 CHECKED BY: R. VARMA
 APPROVED BY: P. DOMBROWSKI
 DATE: 8/12/2016
 SCALE: 1/4" = 1'-0"

NO.	DATE	REVISIONS/ISSUANCES
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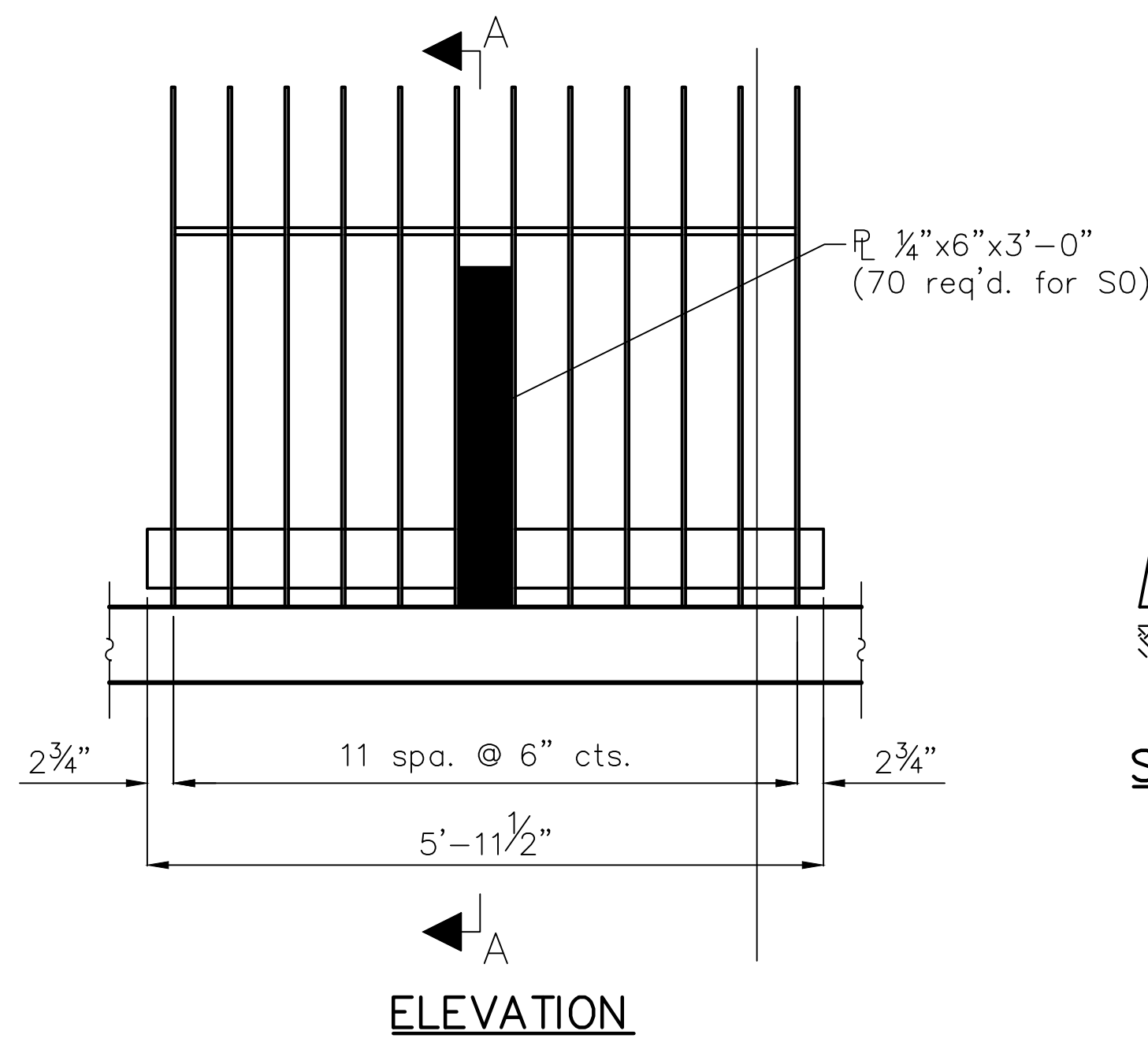
ADDISON UNDERBRIDGE TRAIL CONNECTION FROM
 CLARK PARK TO CALIFORNIA PARK
 STRUCTURE NO. 016-P025

RAILING ANCHORAGE LAYOUT

SHEET NO.
S-39

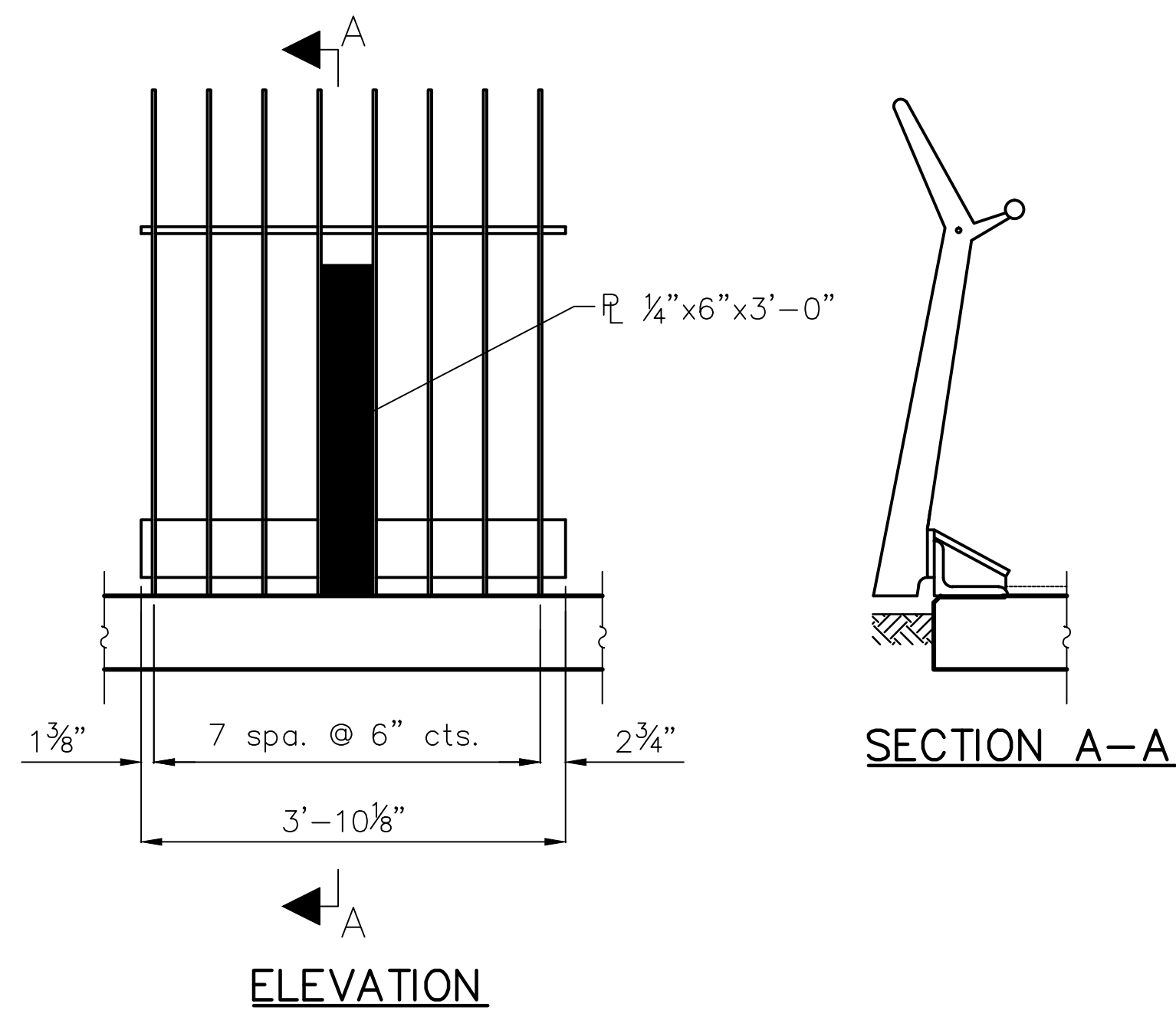
SHEET 86 OF 158

REVISION



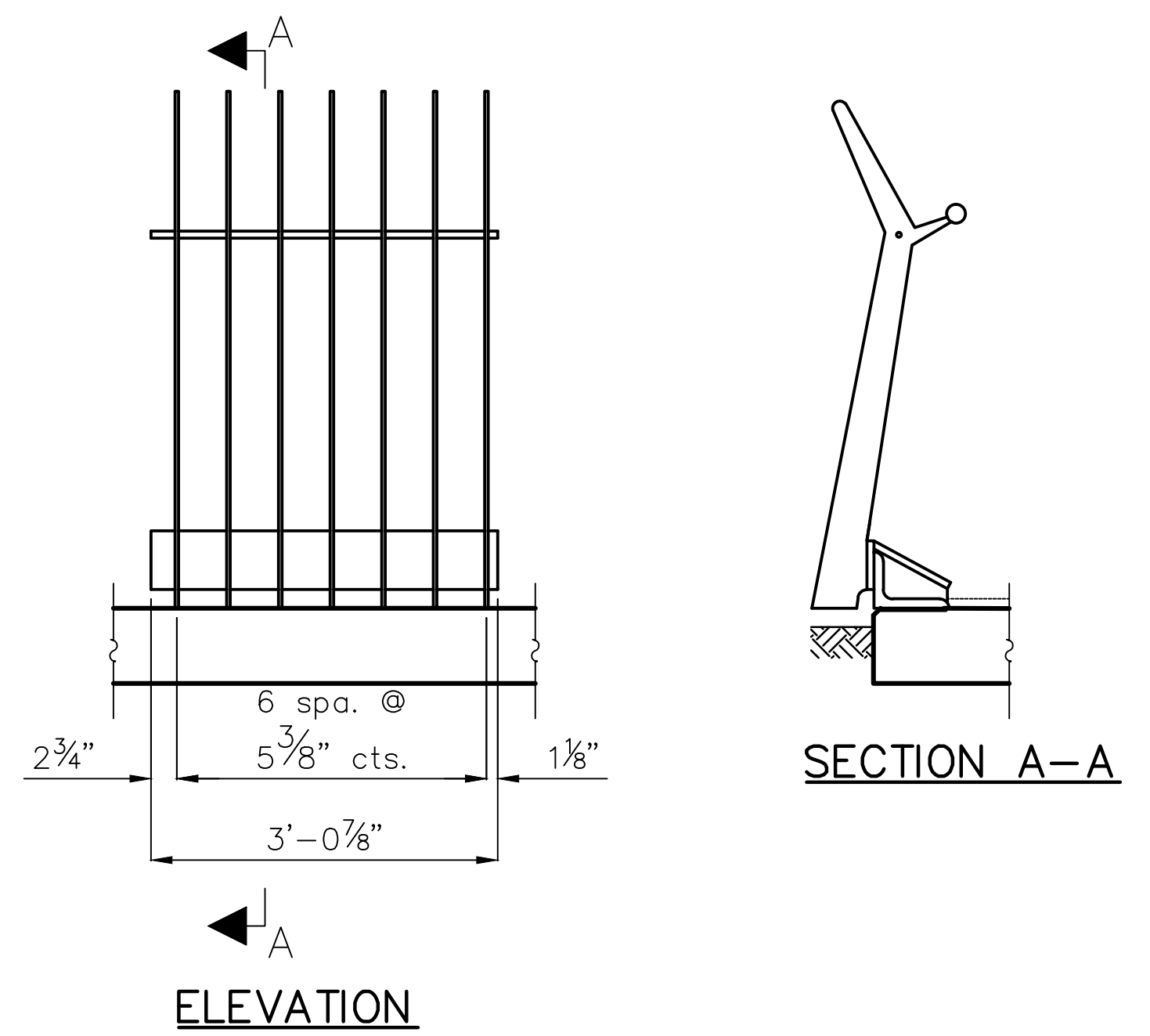
ELEVATION

RAILING PANEL TYPE S0
(140 required)
(Exterior Elevation Shown Typical)



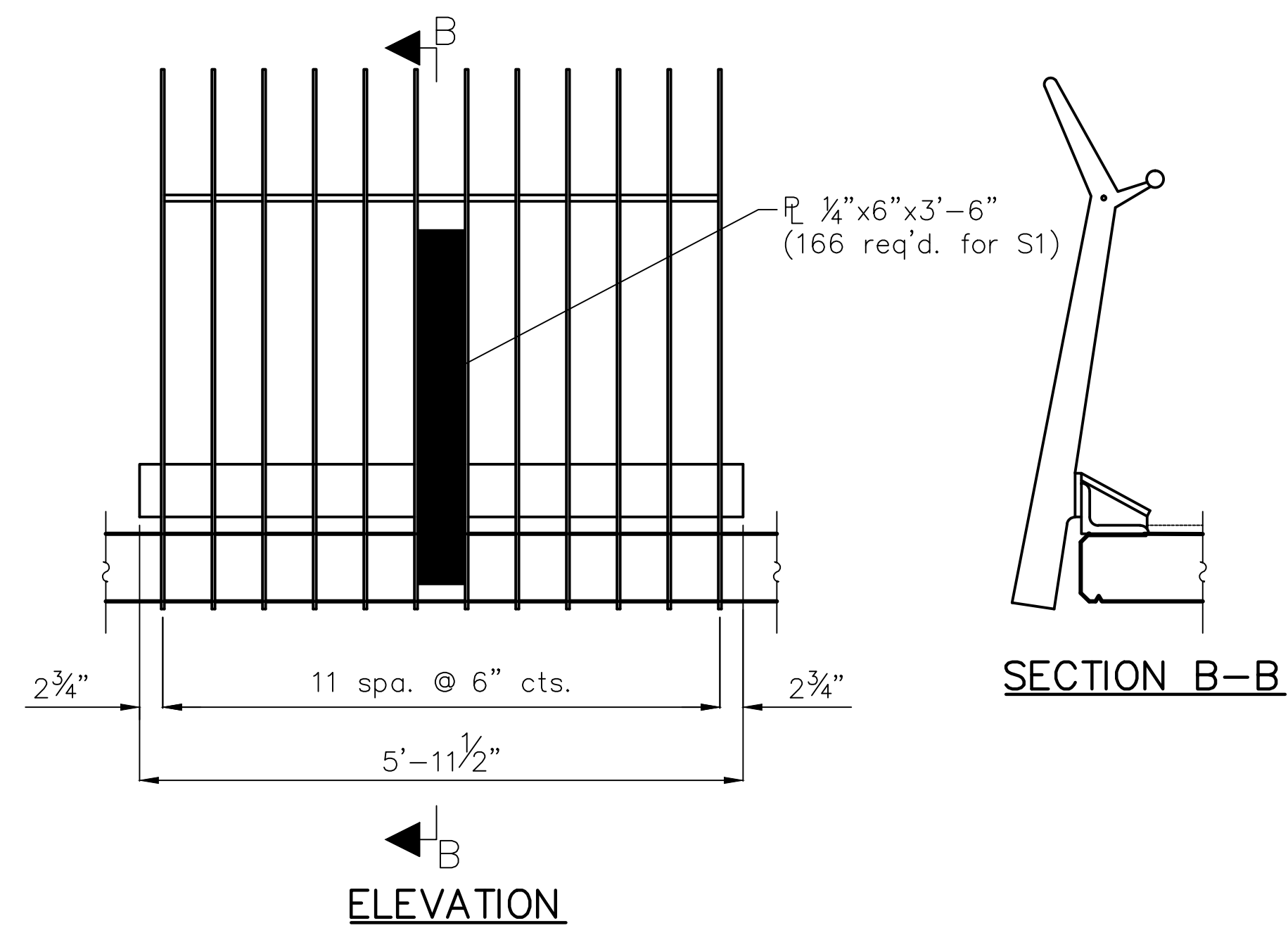
ELEVATION

RAILING PANEL TYPE S2



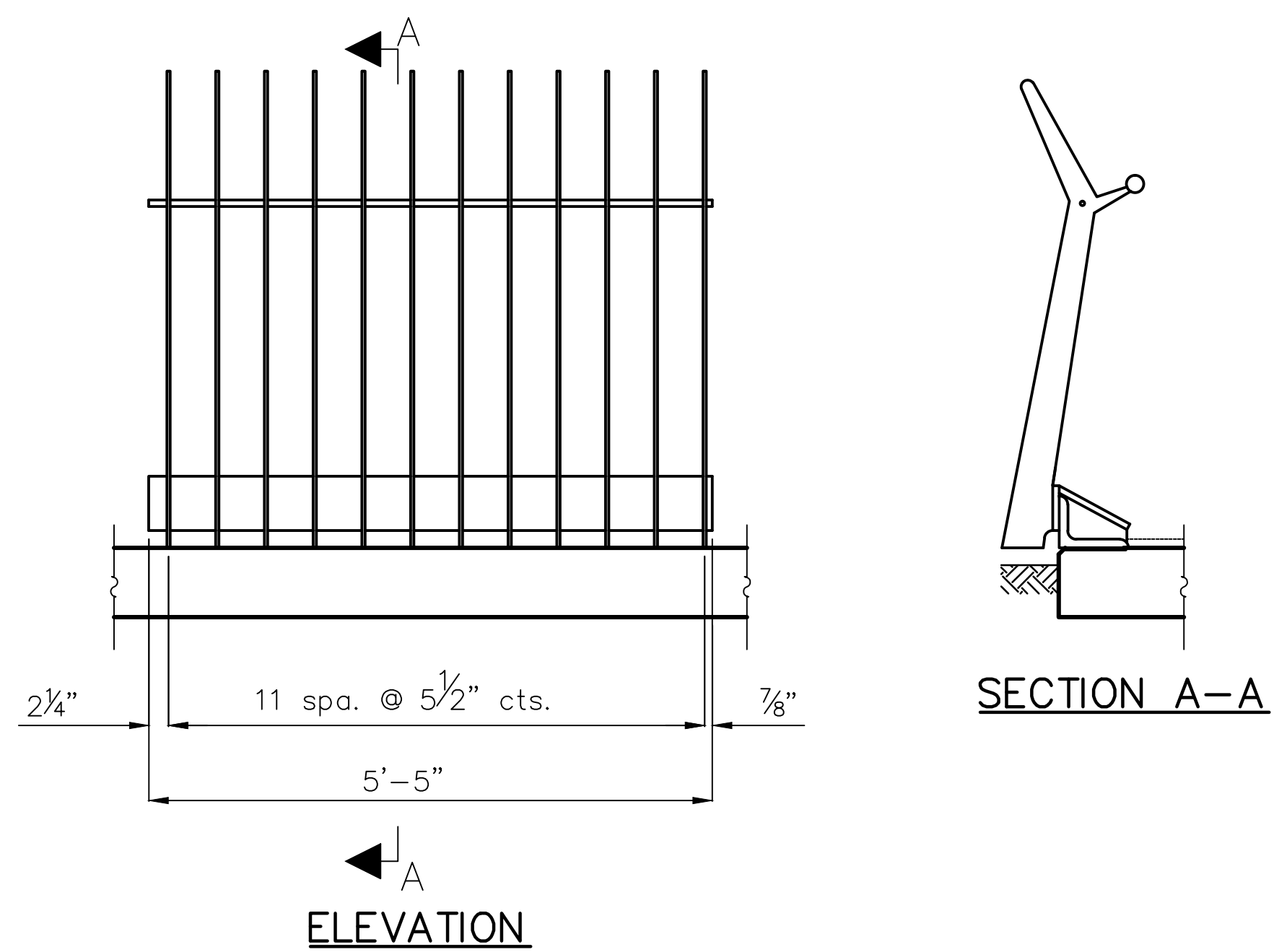
ELEVATION

RAILING PANEL TYPE S4



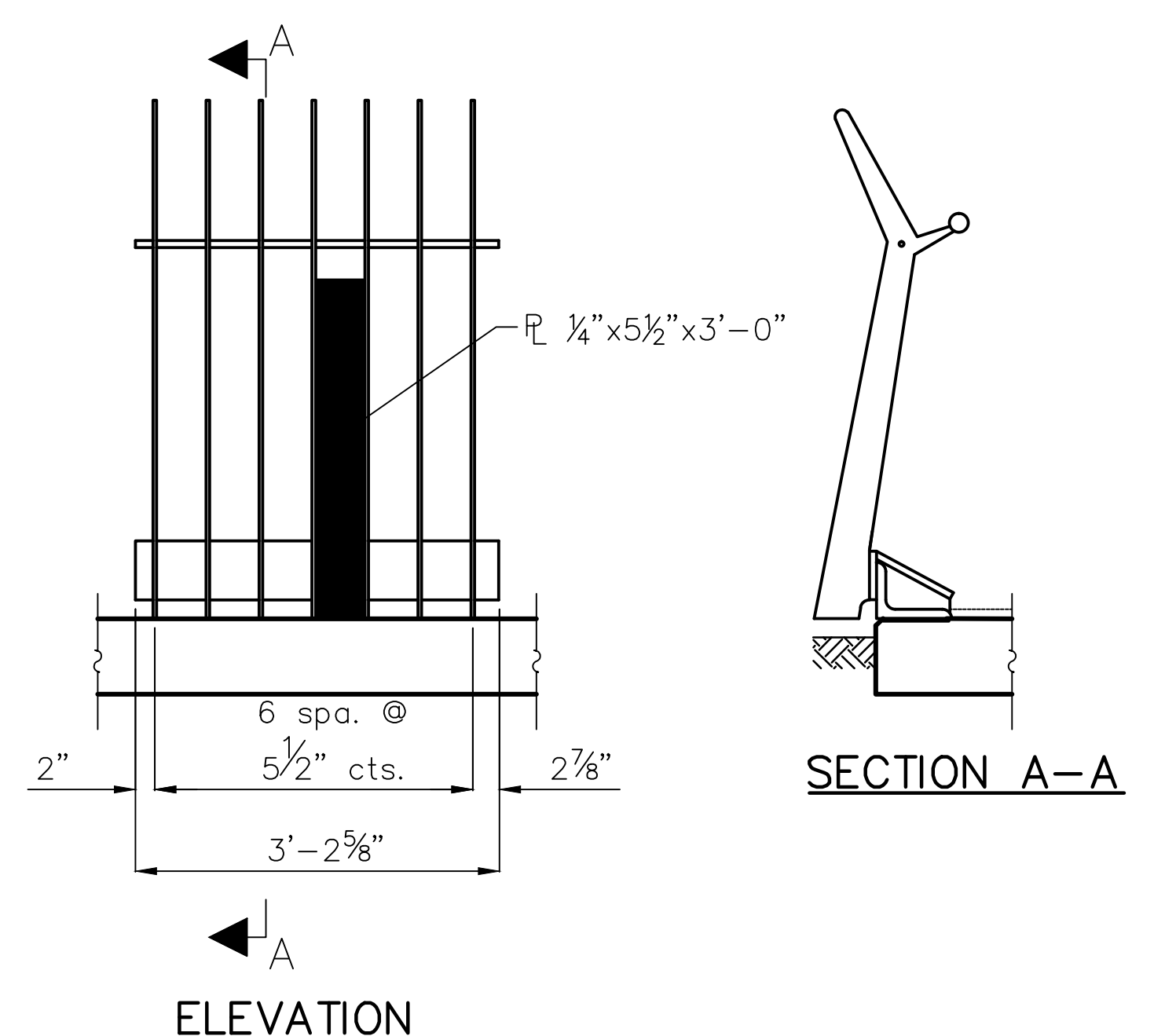
ELEVATION

RAILING PANEL TYPE S1
(333 required)



ELEVATION

RAILING PANEL TYPE S3



ELEVATION

RAILING PANEL TYPE S5

- Notes:
1. For Railing Anchorage Details, see sheet S-24 of S-87.
 2. For Railing Panel Details, see sheet A-1 of A-1.

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SECTION NO.: 12-E9121-00-BT
 DRAWN BY: J. MASI
 CHECKED BY: R. VARMA
 APPROVED BY: P. DOMBROWSKI
 DATE: 8/12/2016
 SCALE: 3/4" = 1'-0"

NO.	DATE	REVISIONS/ISSUANCES
1	8/12/16	ISSUED FOR FINAL
2	5/6/16	ISSUED FOR FINAL
3	6/05/15	ISSUED FOR FINAL REVIEW

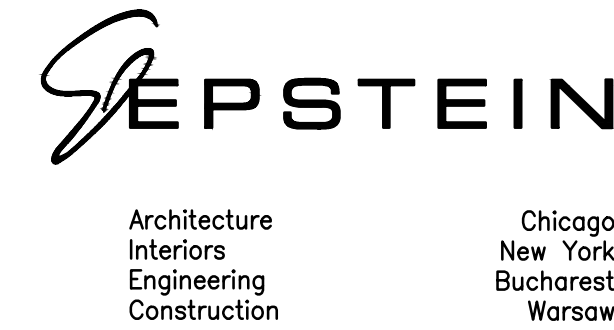
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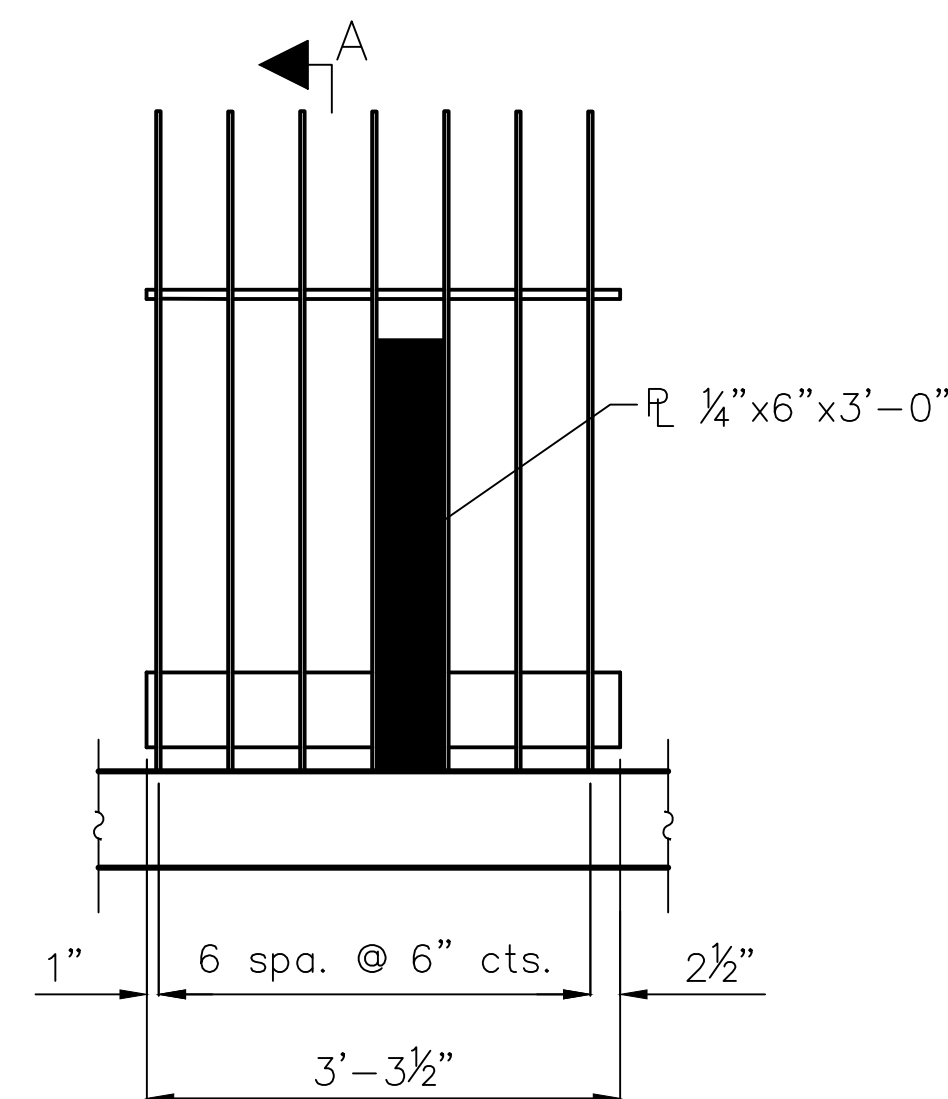
ADDISON UNDERBRIDGE TRAIL CONNECTION FROM
 CLARK PARK TO CALIFORNIA PARK
 STRUCTURE NO. 016-P025

RAILING PANEL DETAILS

SHEET NO.
S-40

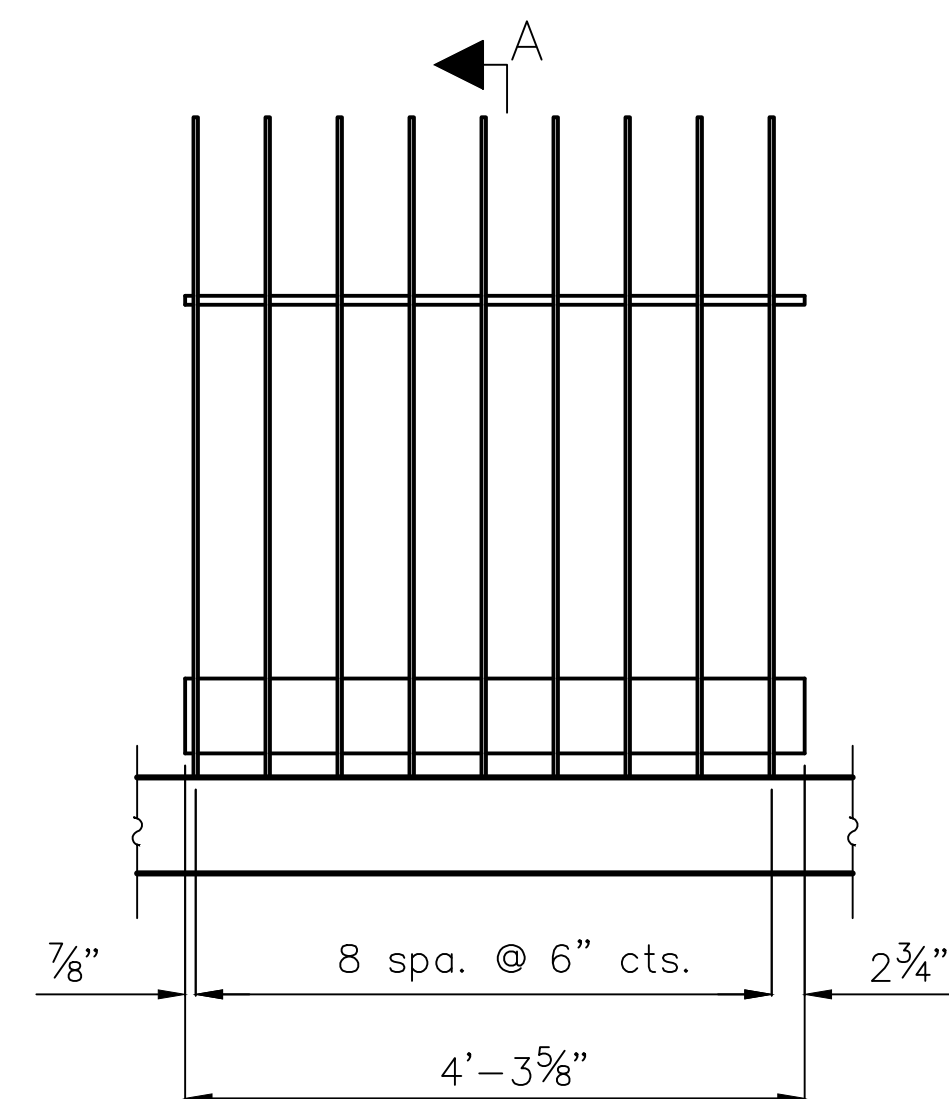
SHEET 87 OF 158

REVISION



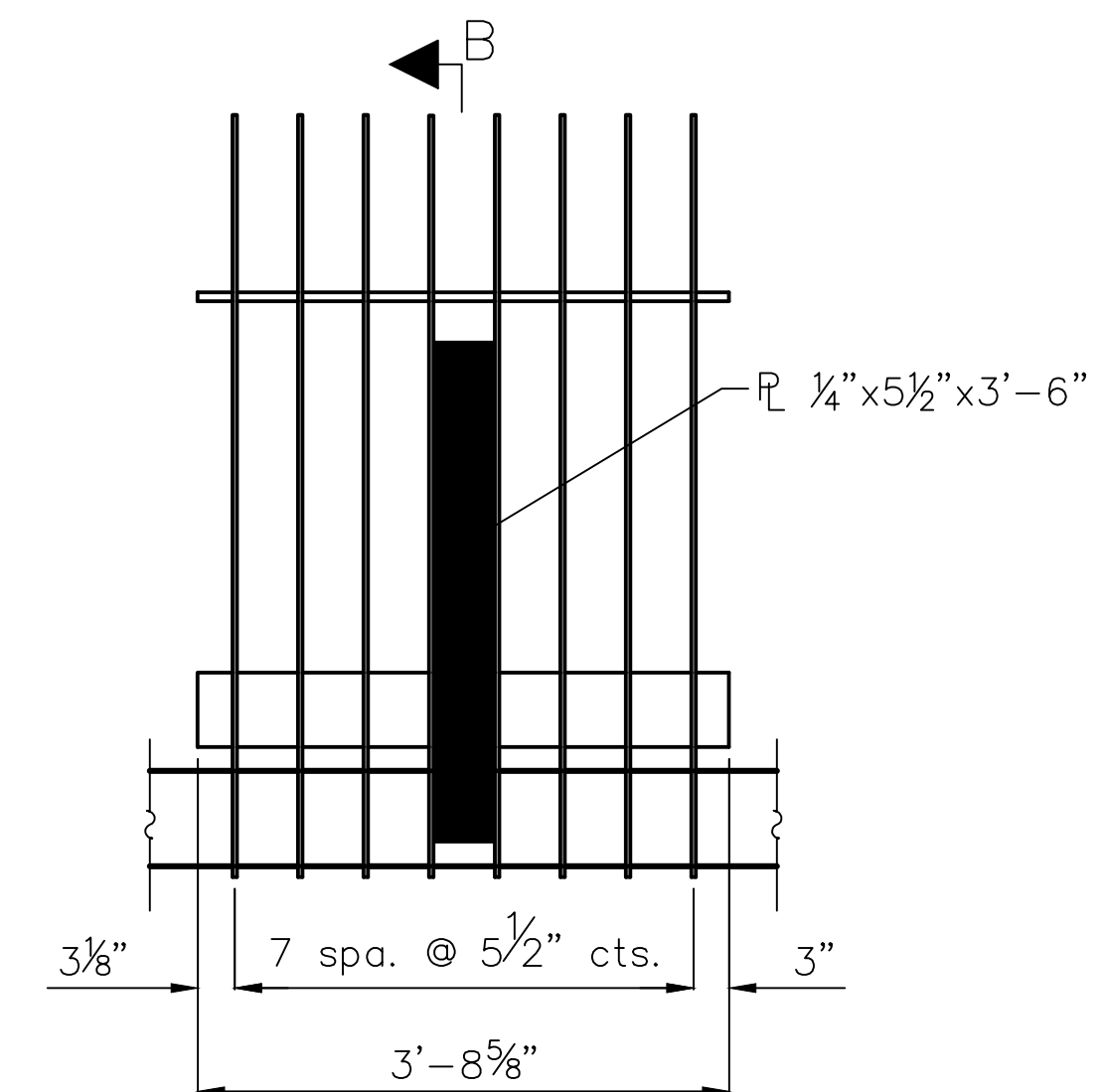
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RAILING PANEL TYPE S6



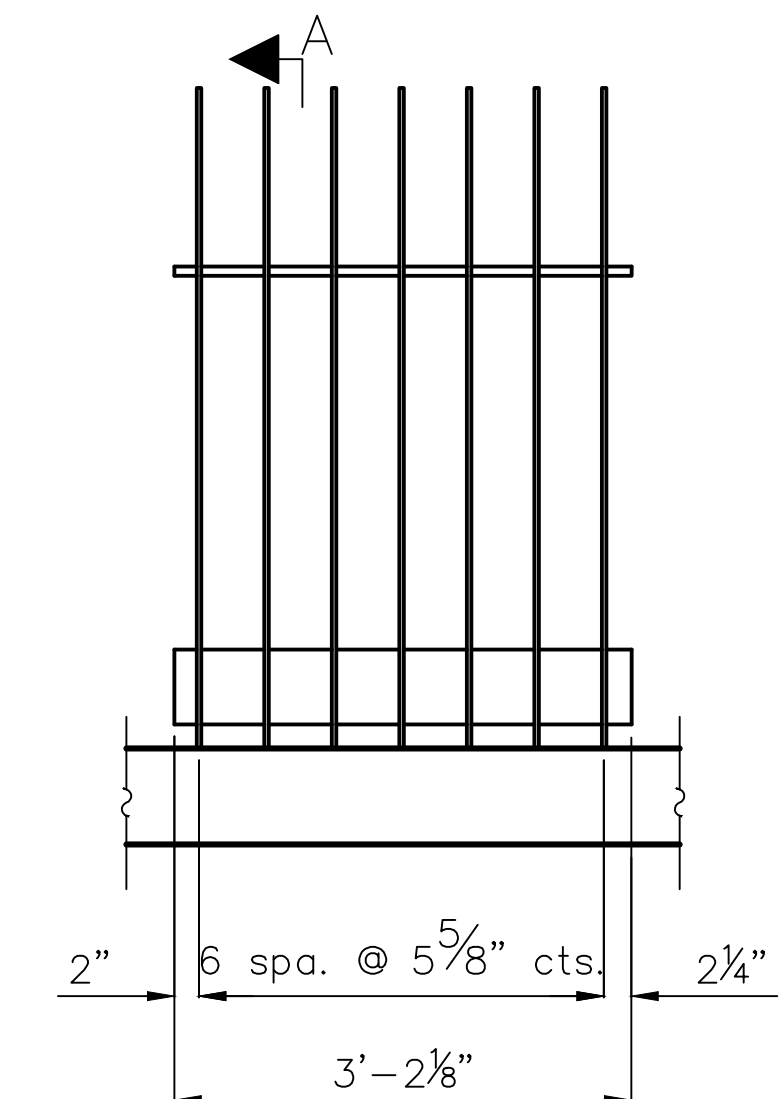
ELEVATION

RAILING PANEL TYPE S8



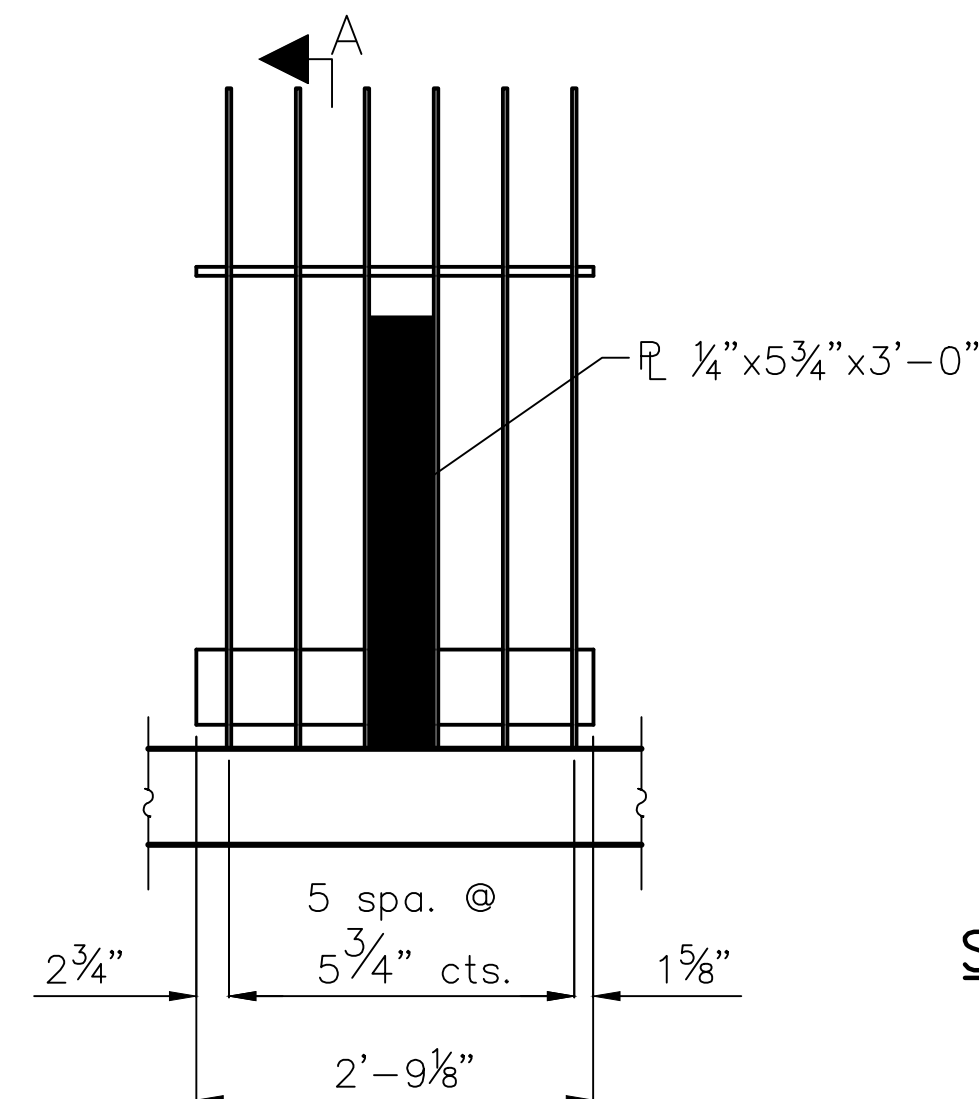
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RAILING PANEL TYPE S10



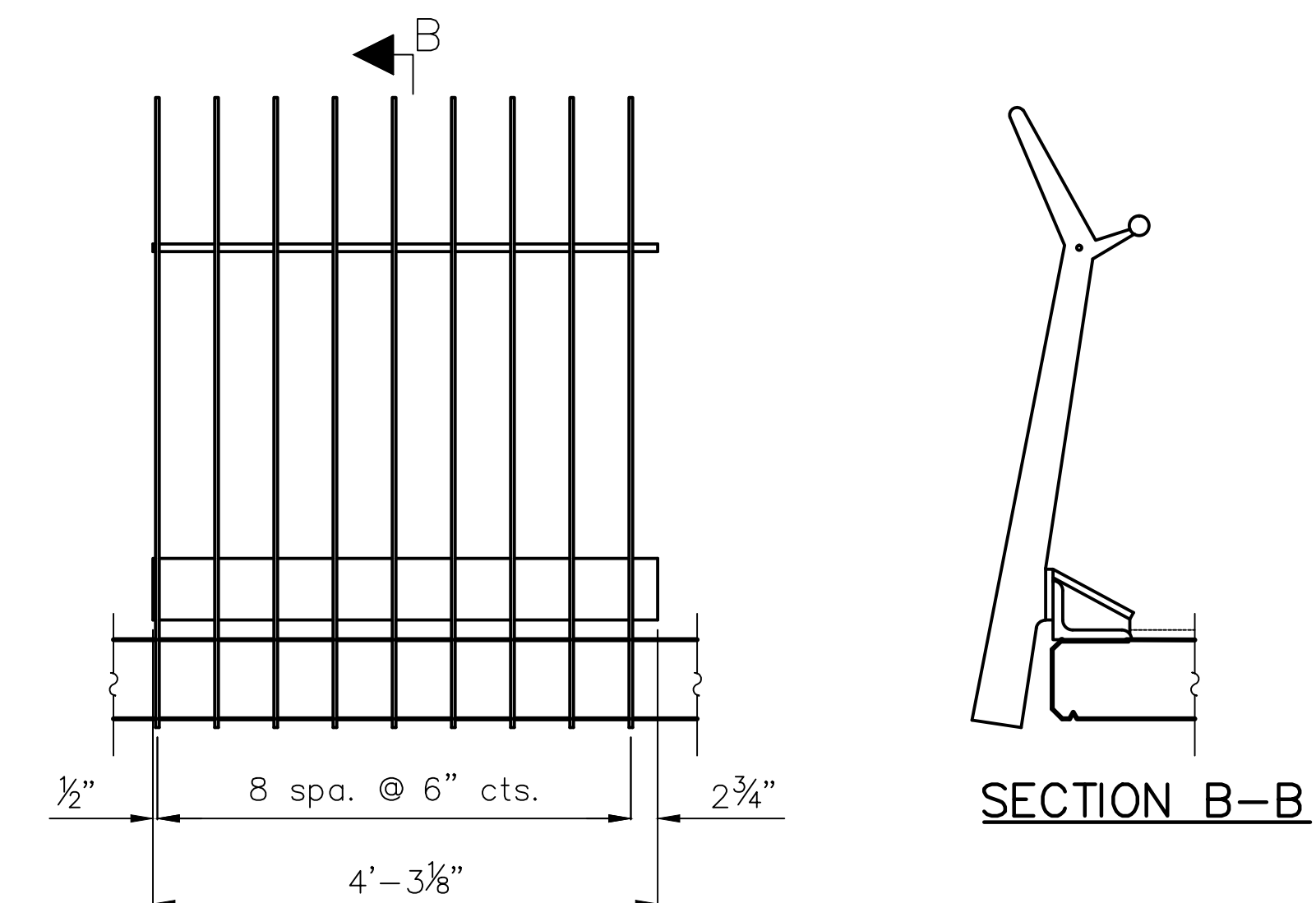
ELEVATION

RAILING PANEL TYPE S7



ELEVATION

RAILING PANEL TYPE S9



ELEVATION

RAILING PANEL TYPE S11

- Notes:
1. For Railing Anchorage Details, see sheet S-24 of S-87.
 2. For Railing Panel Details, see sheet A-1 of A-1.

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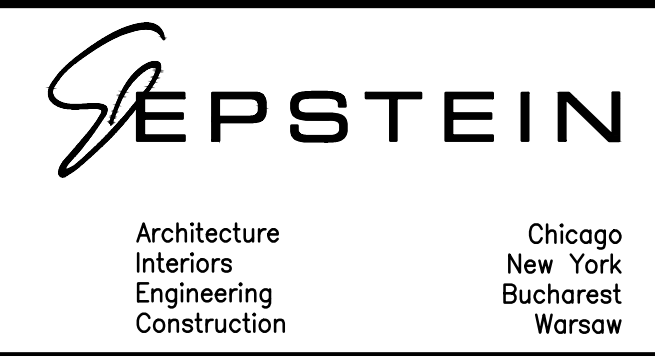
SECTION NO.: 12-E9121-00-BT
 DRAWN BY: J. MASI
 CHECKED BY: R. VARMA
 APPROVED BY: P. DOMBROWSKI
 DATE: 8/12/2016
 SCALE: 3/4" = 1'-0"

NO.	DATE	REVISIONS/ISSUANCES
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3	6/05/15	ISSUED FOR FINAL REVIEW

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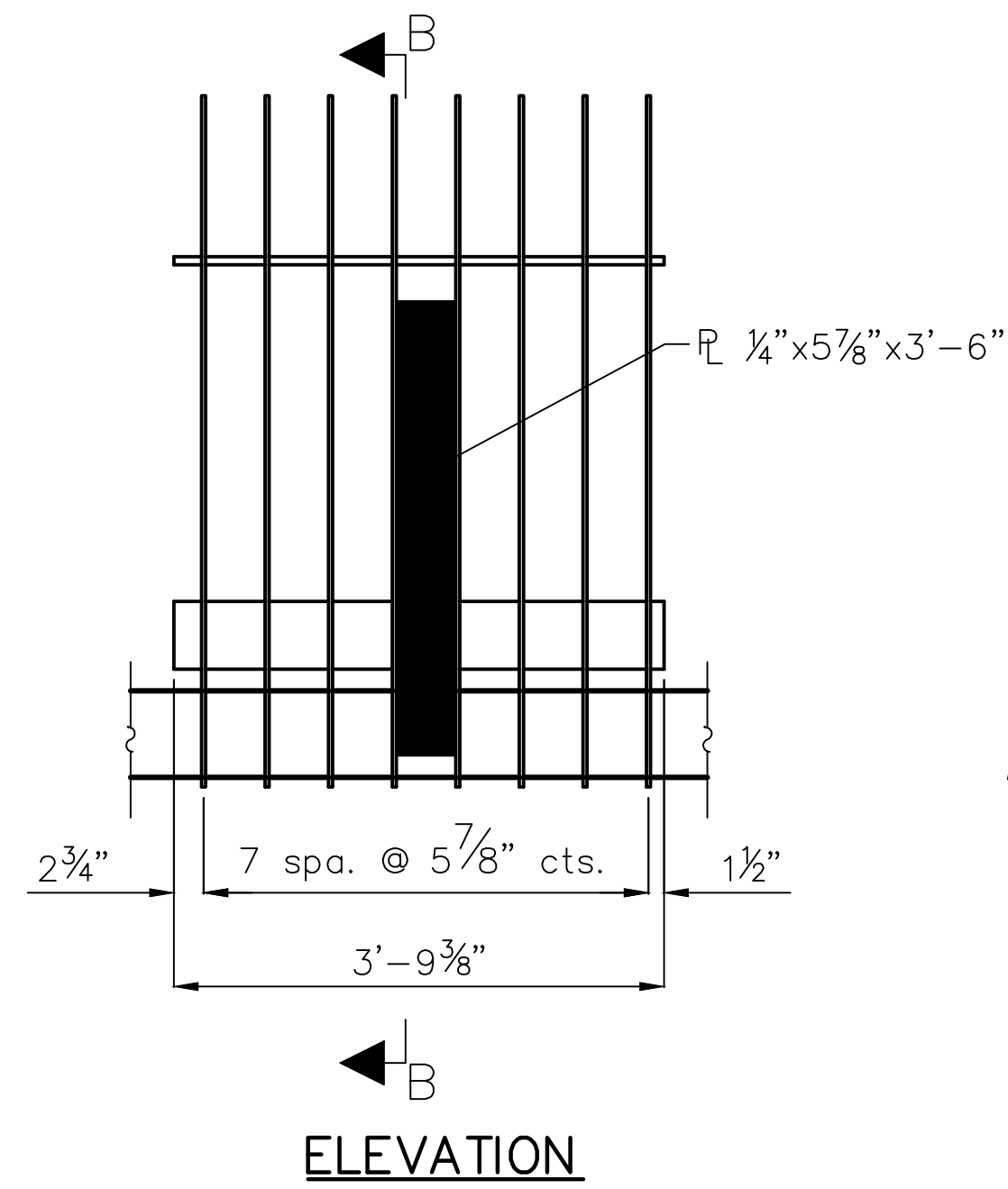
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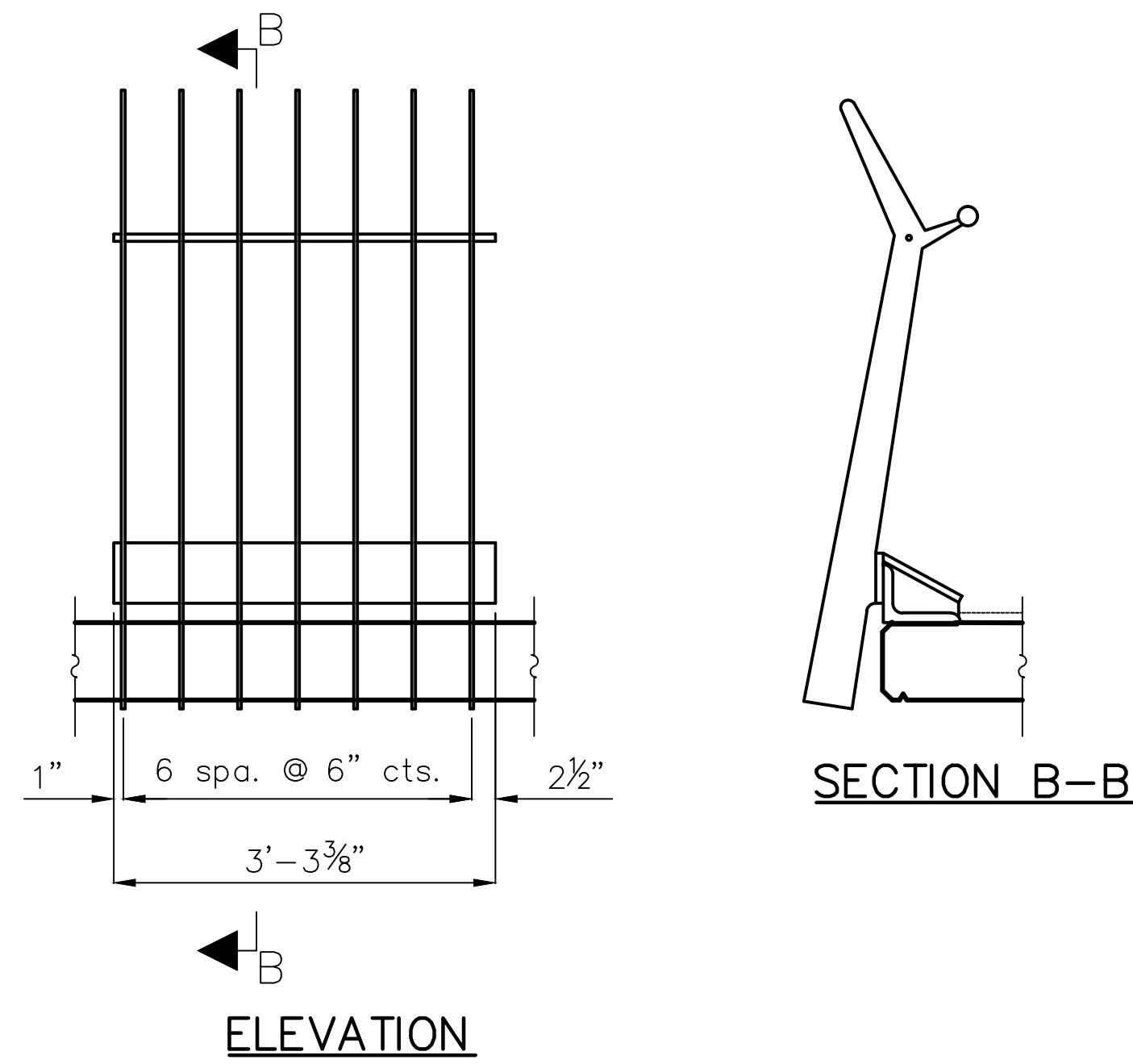
ADDISON UNDERBRIDGE TRAIL CONNECTION FROM
 CLARK PARK TO CALIFORNIA PARK
 STRUCTURE NO. 016-P025

RAILING PANEL DETAILS

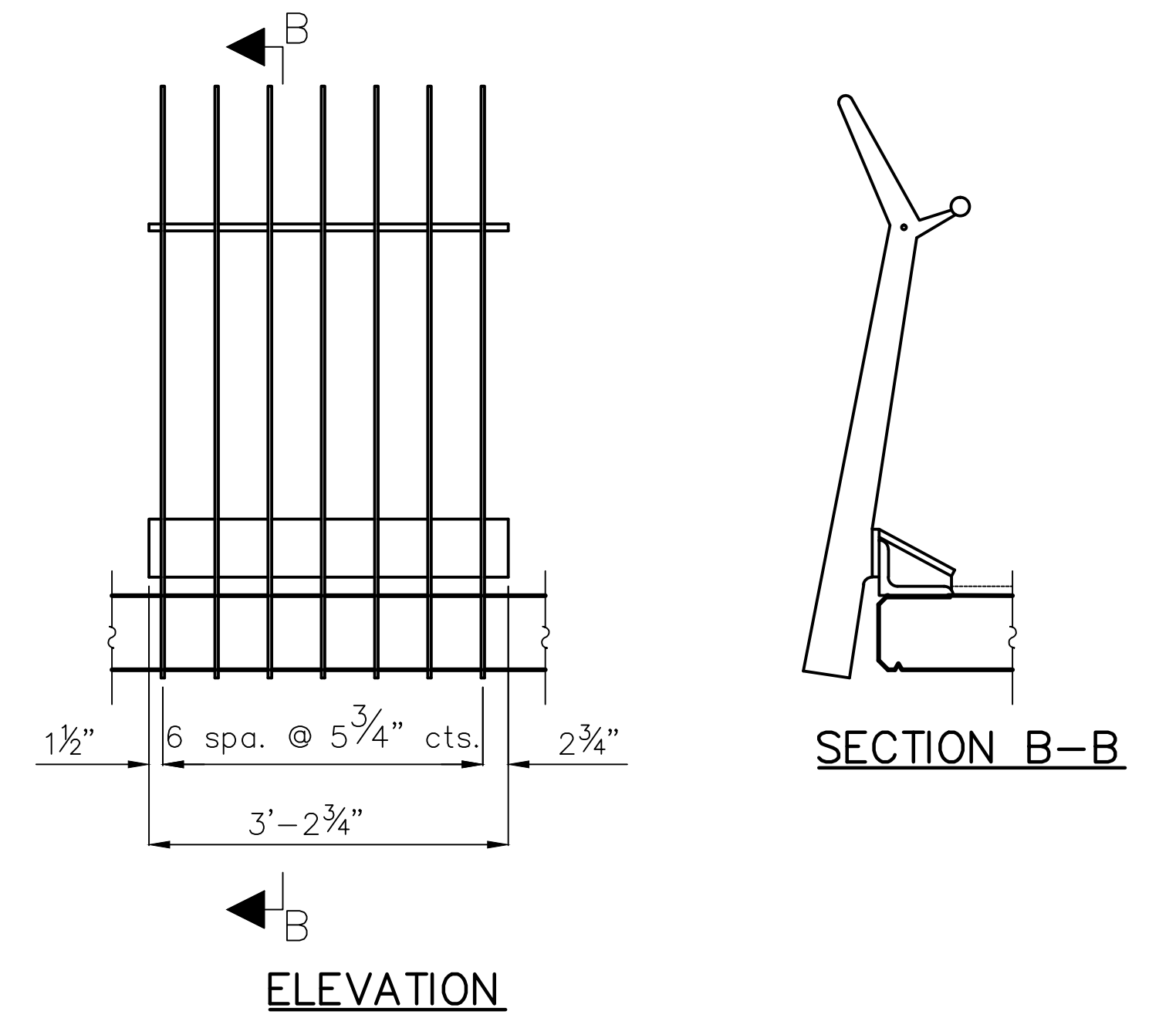
SHEET NO. S-41
 SHEET 88 OF 158
 REVISION



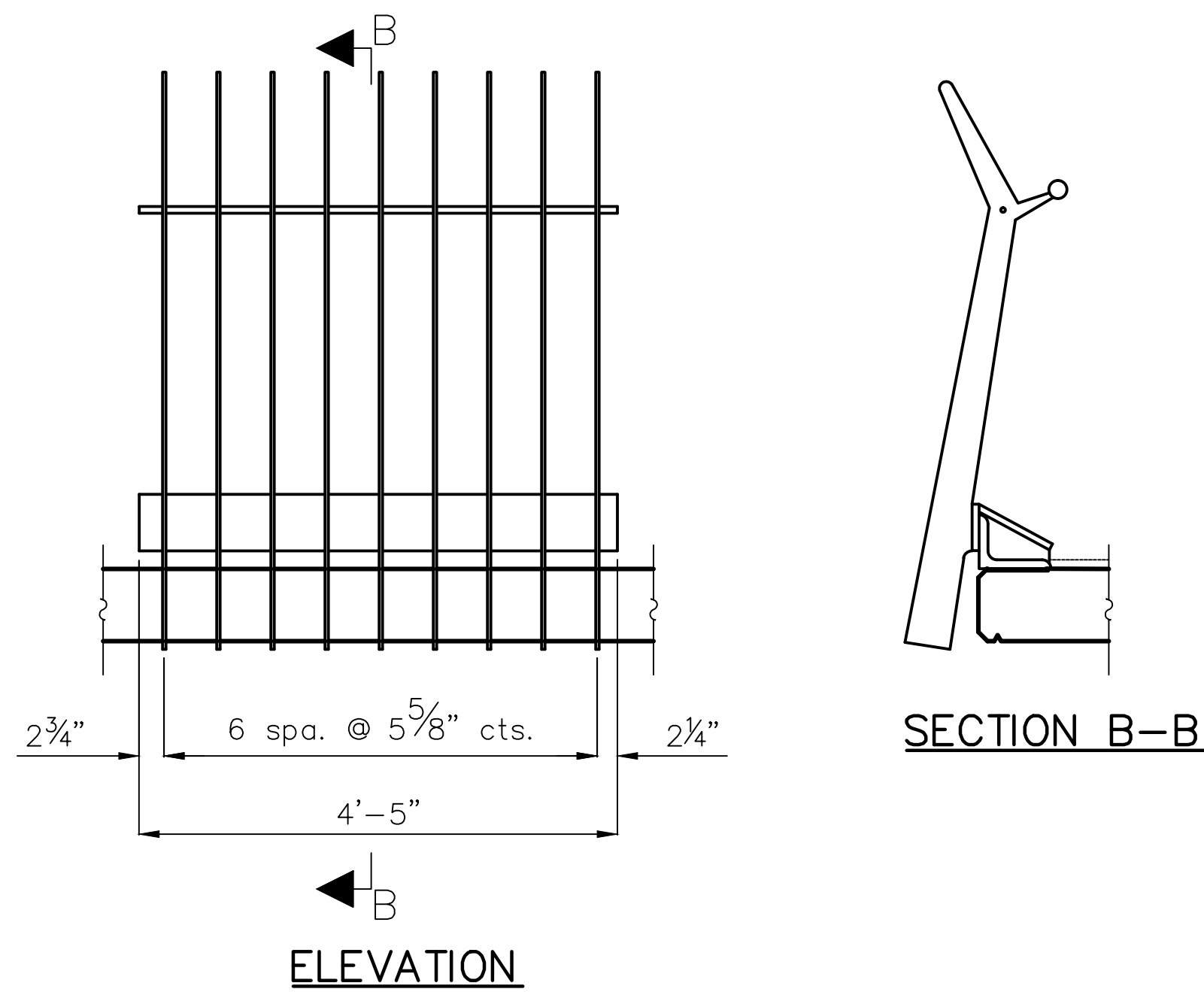
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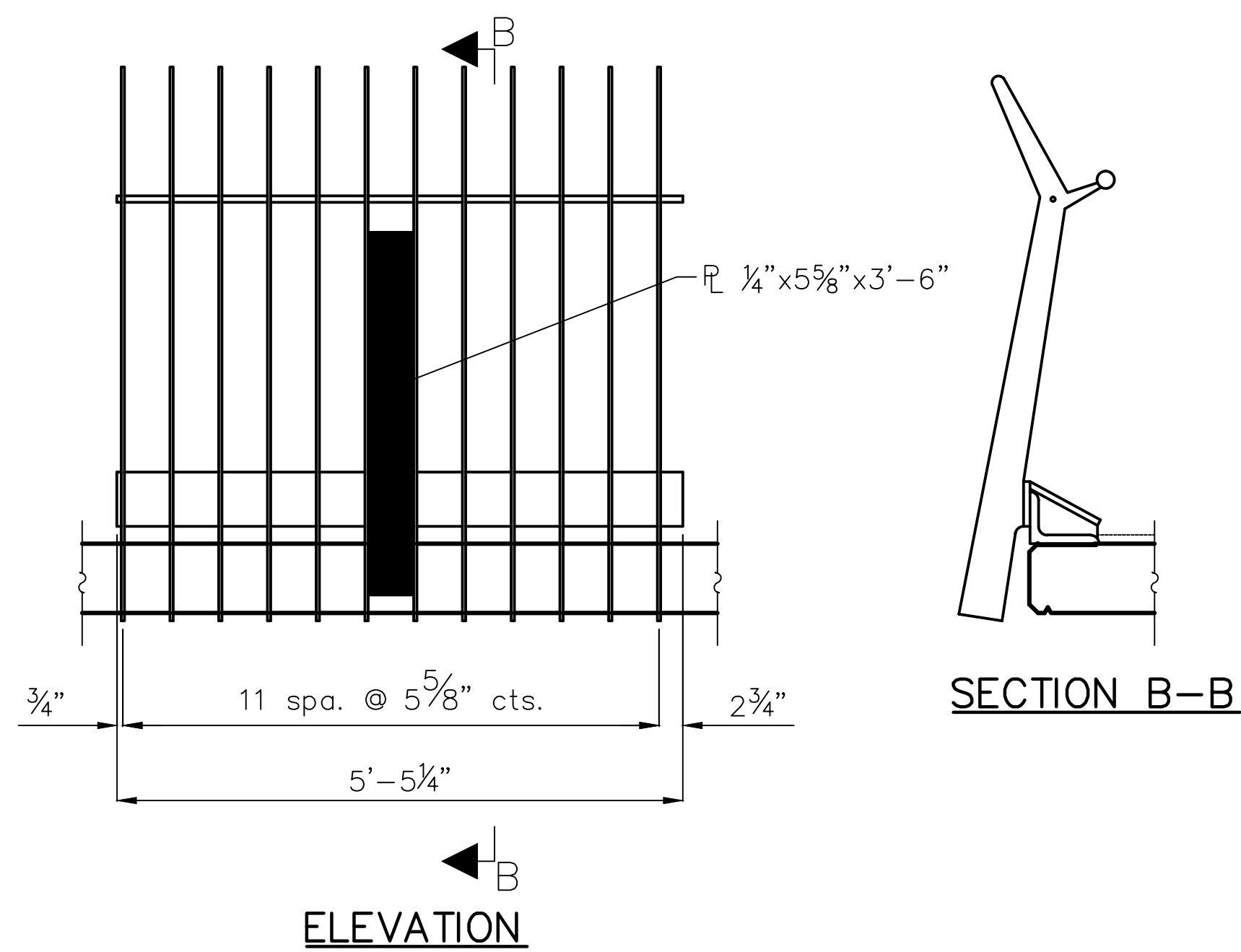
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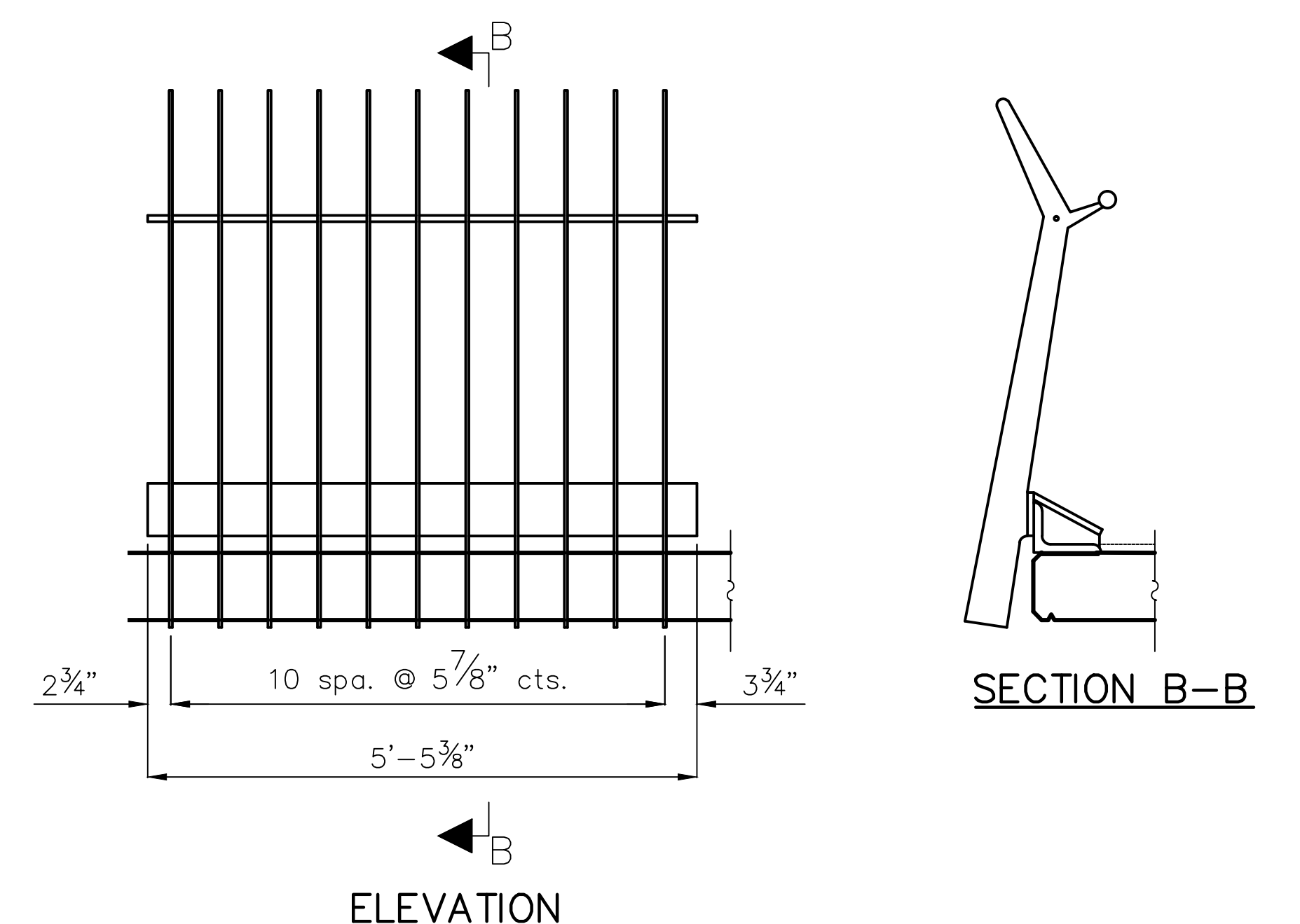
RAILING PANEL TYPE S16



RAILING PANEL TYPE S13



RAILING PANEL TYPE S15



RAILING PANEL TYPE S17

- Notes:
1. For Railing Anchorage Details, see sheet S-24 of S-87.
 2. For Railing Panel Details, see sheet A-1 of A-1.

SECTION NO.: 12-E9121-00-BT
 DRAWN BY: J. MASI
 CHECKED BY: R. VARMA
 APPROVED BY: P. DOMBROWSKI
 DATE: 8/12/2016
 SCALE: 3/4" = 1'-0"

NO.	DATE	REVISIONS/ISSUANCES
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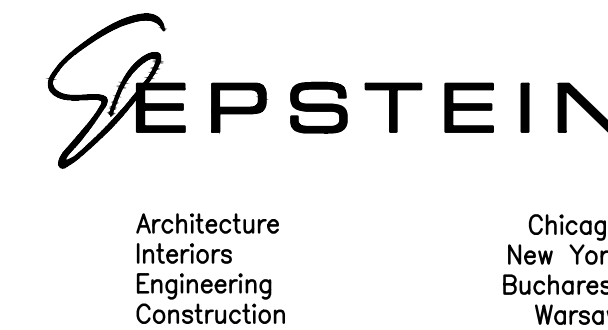
CITY OF CHICAGO

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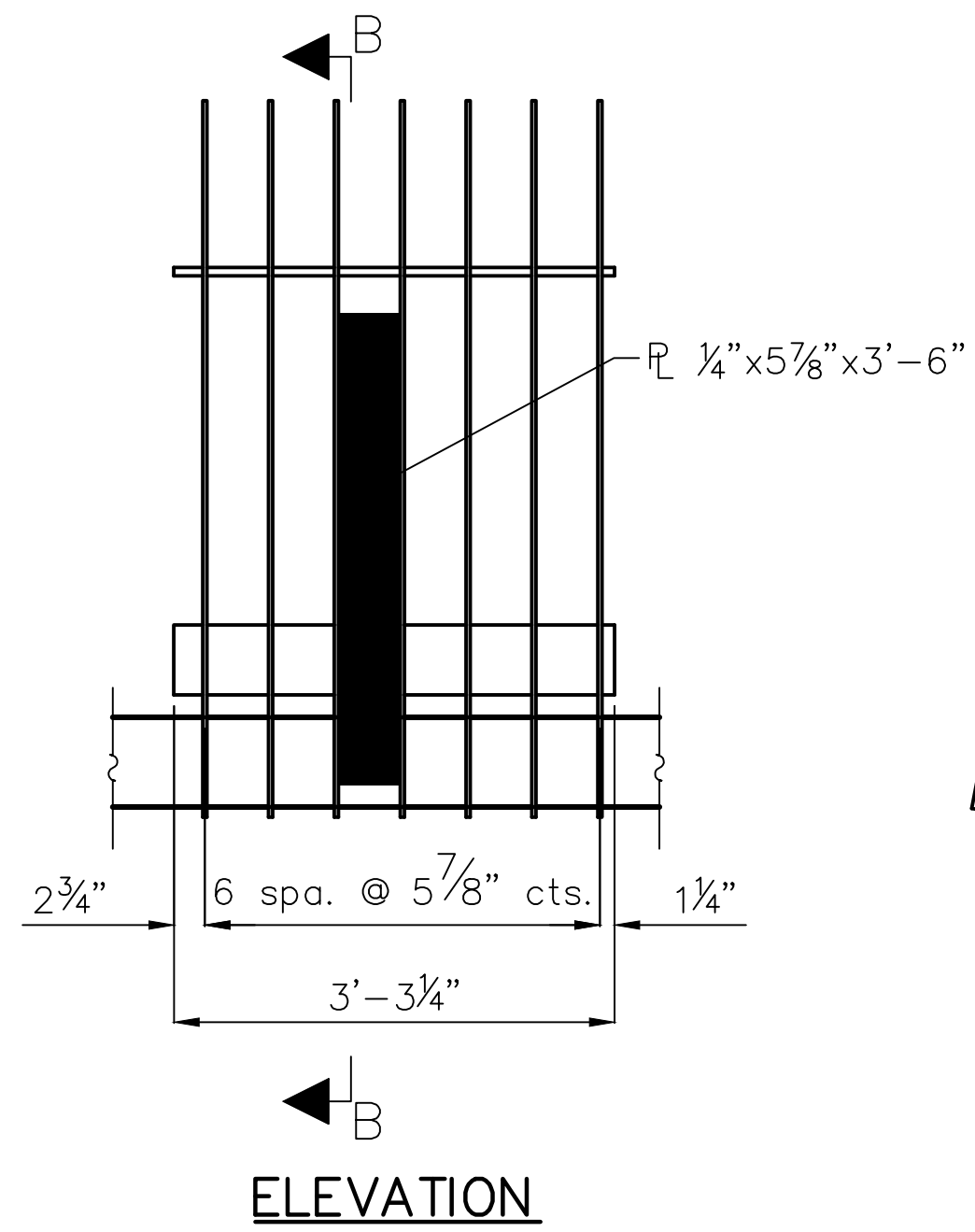
ADDISON UNDERBRIDGE TRAIL CONNECTION FROM
 CLARK PARK TO CALIFORNIA PARK
 STRUCTURE NO. 016-P025

RAILING PANEL DETAILS

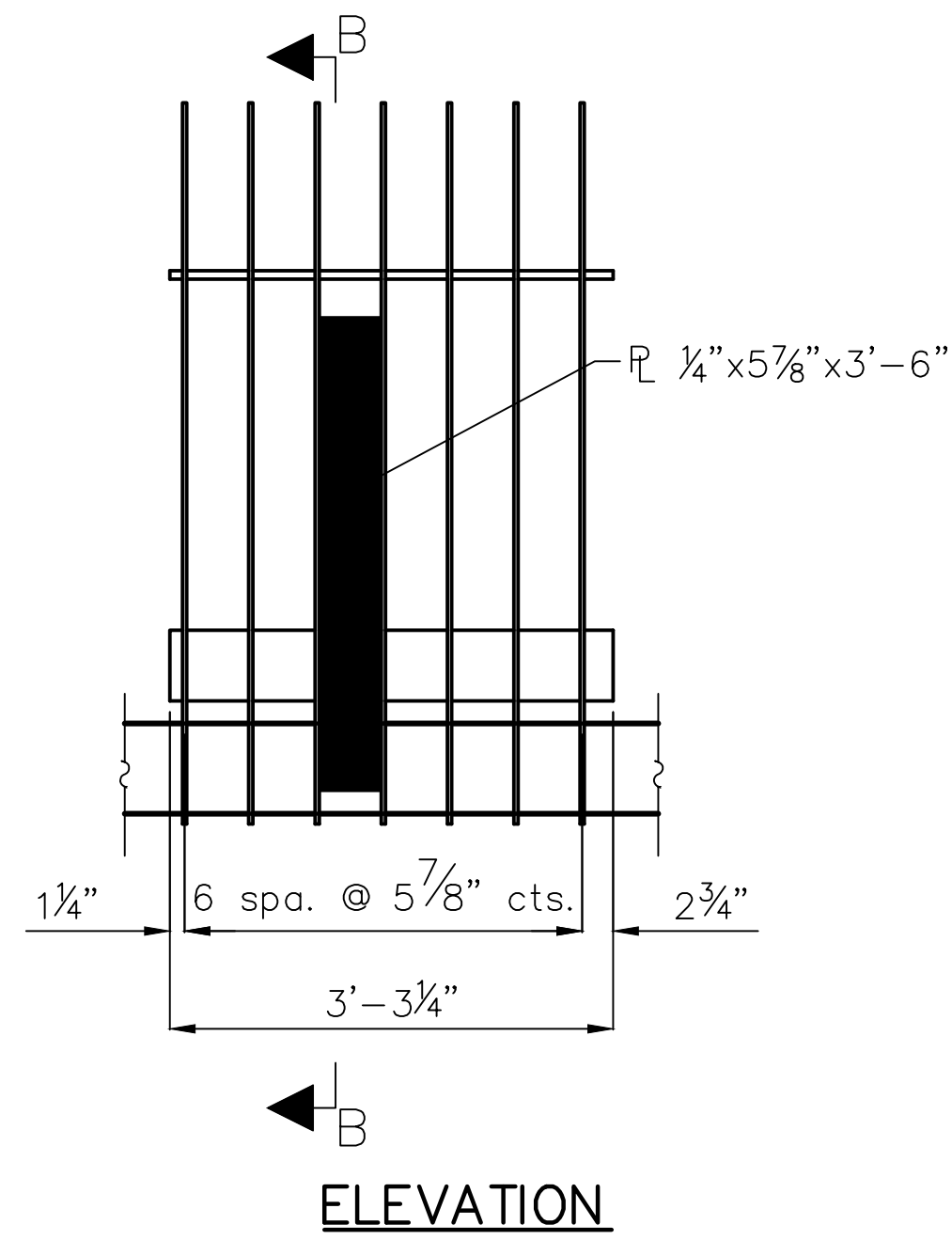
SHEET NO.
 S-42

SHEET 89 OF 158

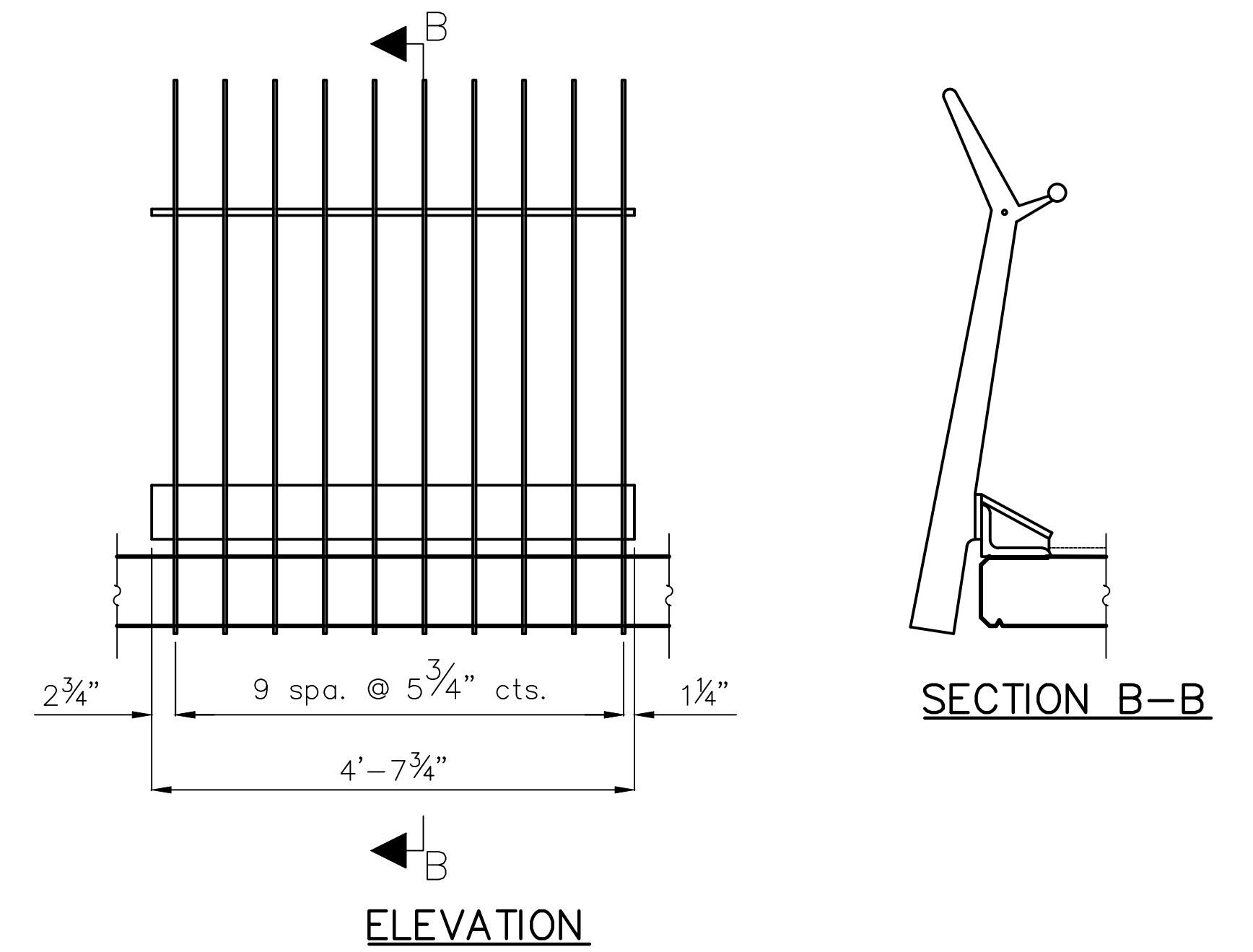
REVISION



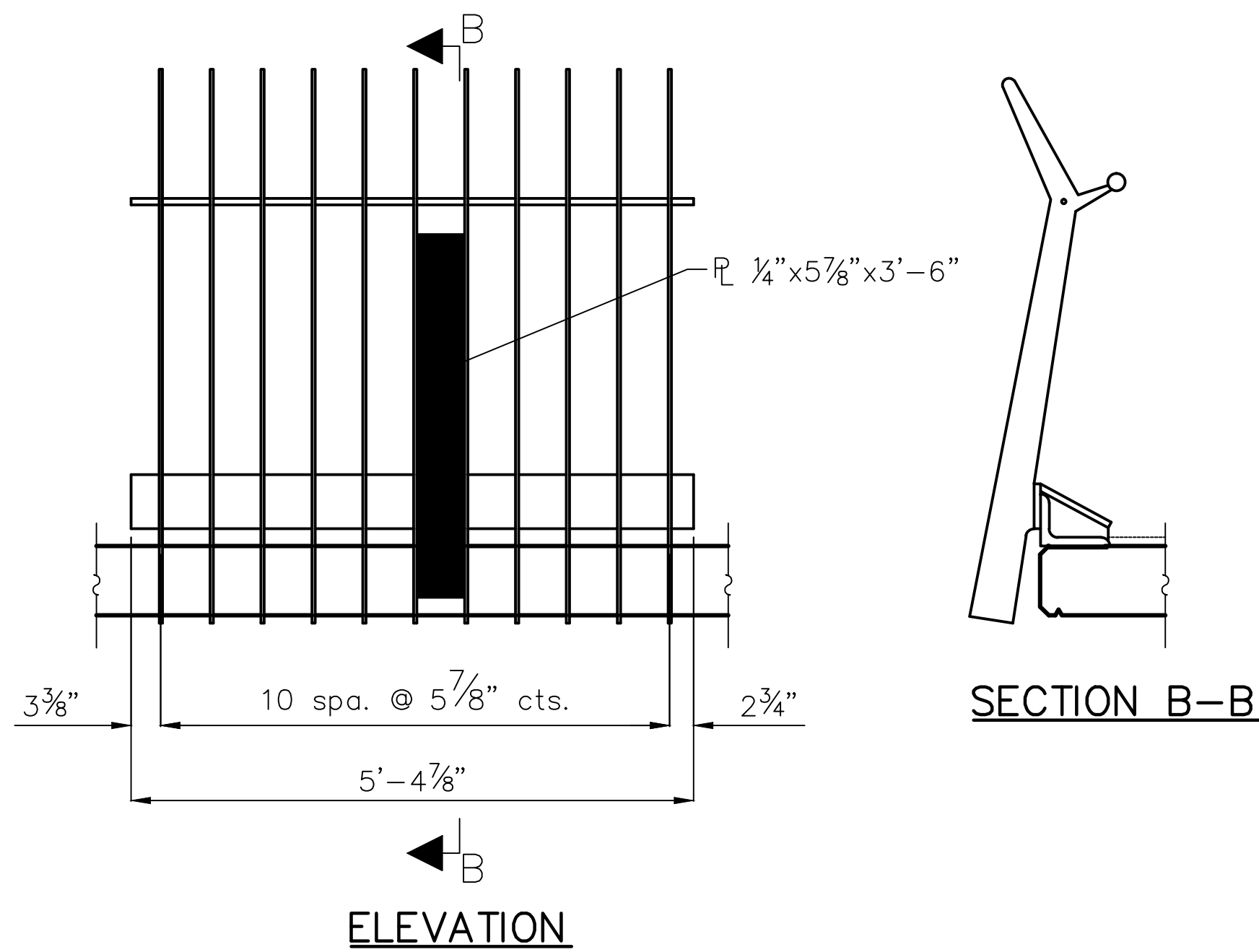
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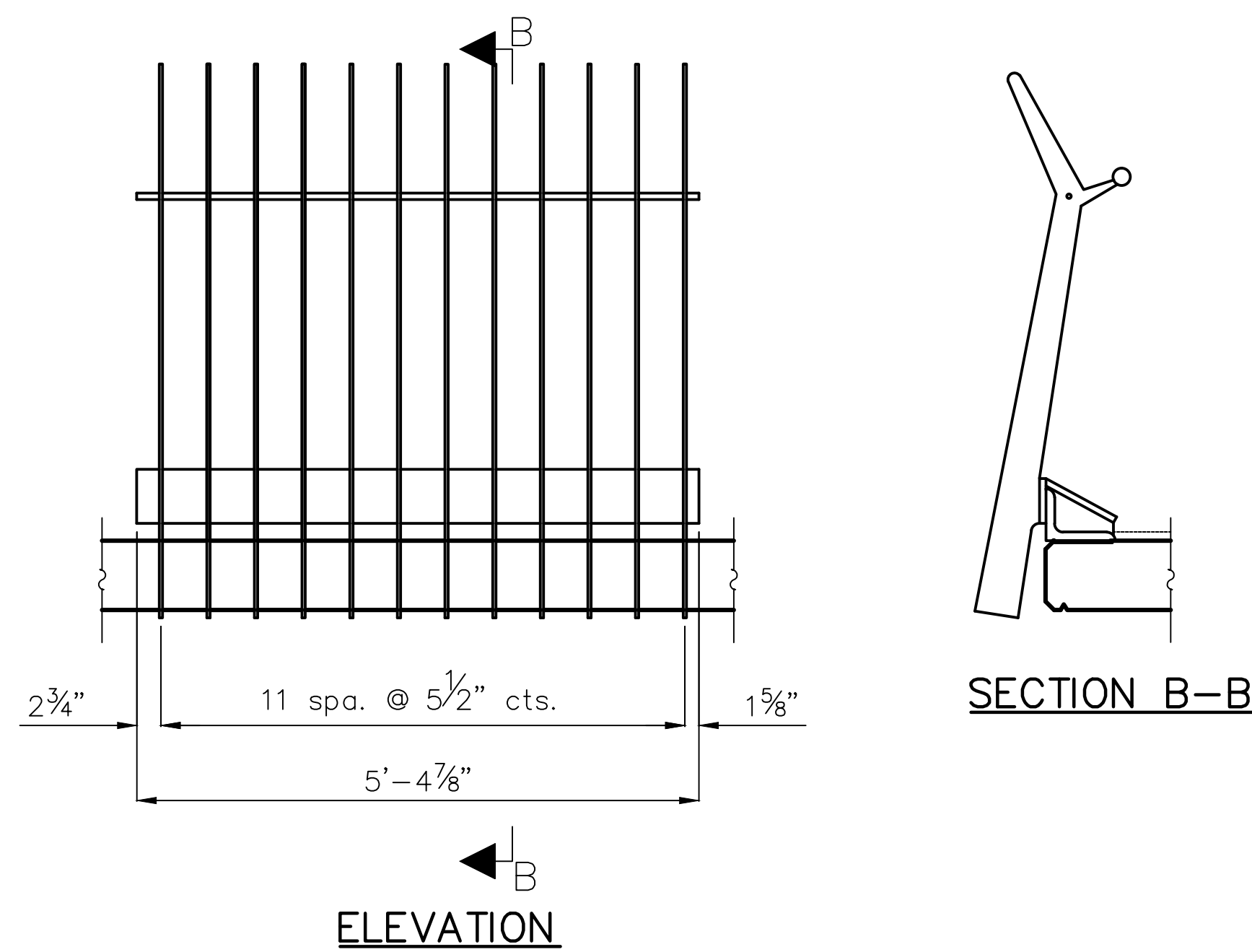
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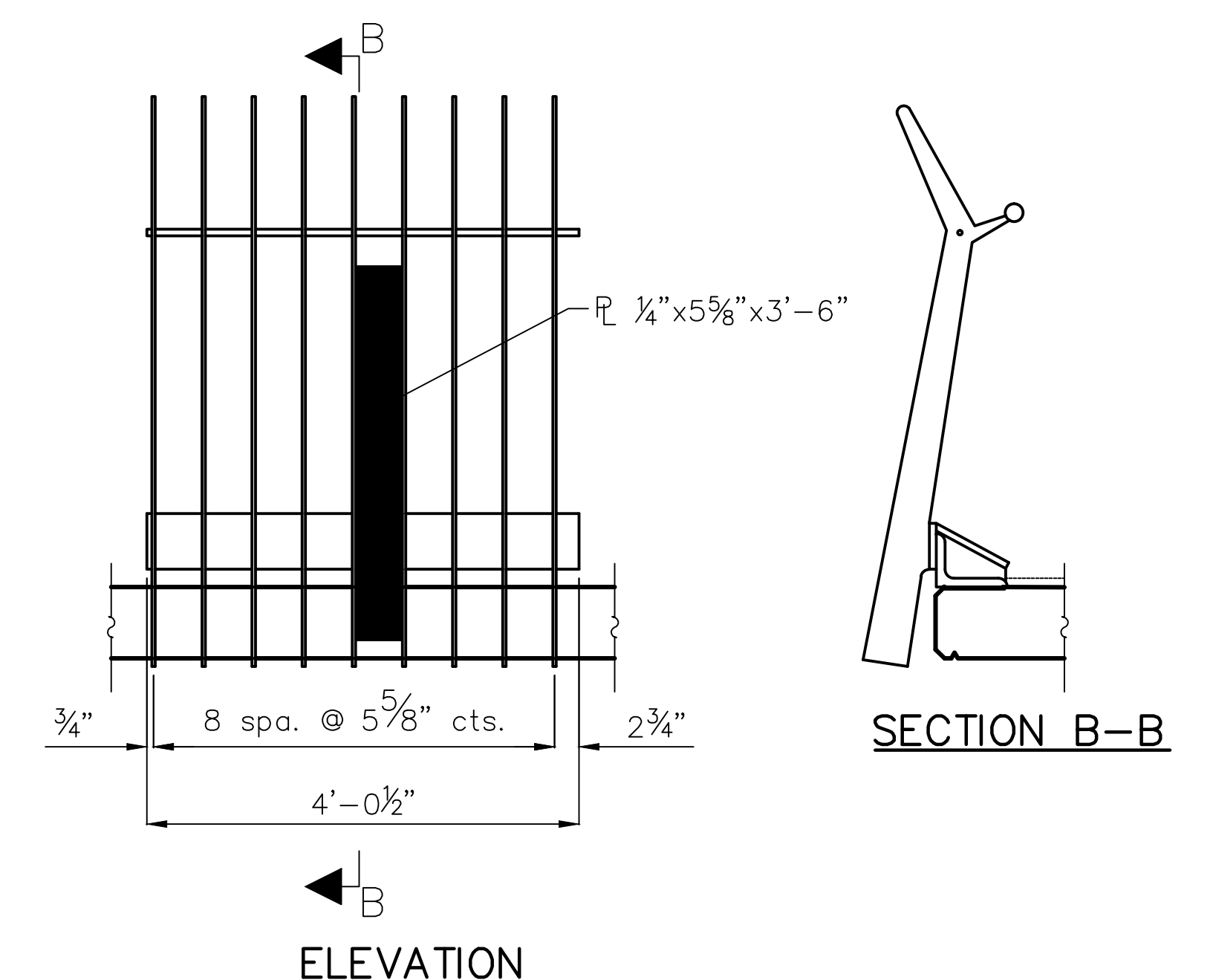
RAILING PANEL TYPE S22



RAILING PANEL TYPE S19



RAILING PANEL TYPE S21



RAILING PANEL TYPE S23

- Notes:
1. For Railing Anchorage Details, see sheet S-24 of S-87.
 2. For Railing Panel Details, see sheet A-1 of A-1.

SECTION NO.: 12-E9121-00-BT
 DRAWN BY: J. MASI
 CHECKED BY: R. VARMA
 APPROVED BY: P. DOMBROWSKI
 DATE: 8/12/2016
 SCALE: 3/4" = 1'-0"

NO.	DATE	REVISIONS/ISSUANCES
1	8/12/16	ISSUED FOR FINAL
2	5/6/16	ISSUED FOR FINAL
3	6/05/15	ISSUED FOR FINAL REVIEW

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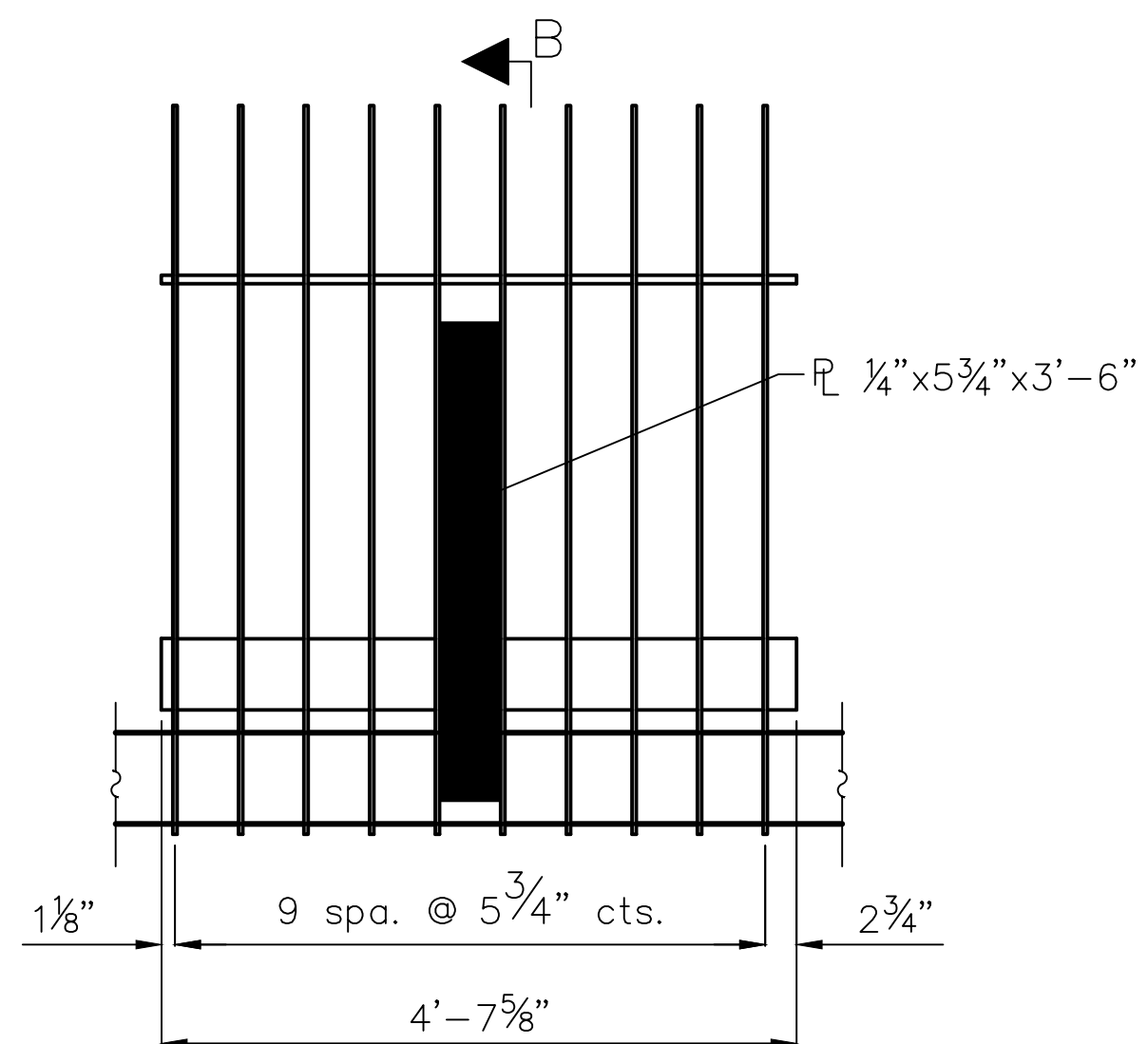
ADDISON UNDERBRIDGE TRAIL CONNECTION FROM
 CLARK PARK TO CALIFORNIA PARK
 STRUCTURE NO. 016-P025

RAILING PANEL DETAILS

SHEET NO.
 S-43

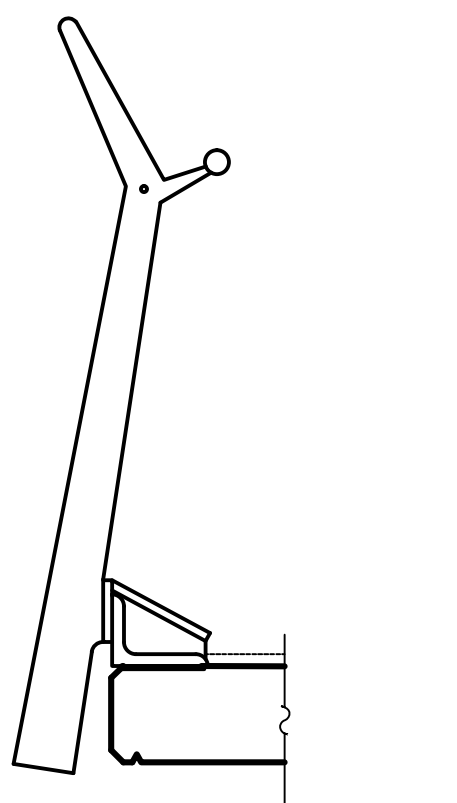
SHEET 90 OF 158

REVISION

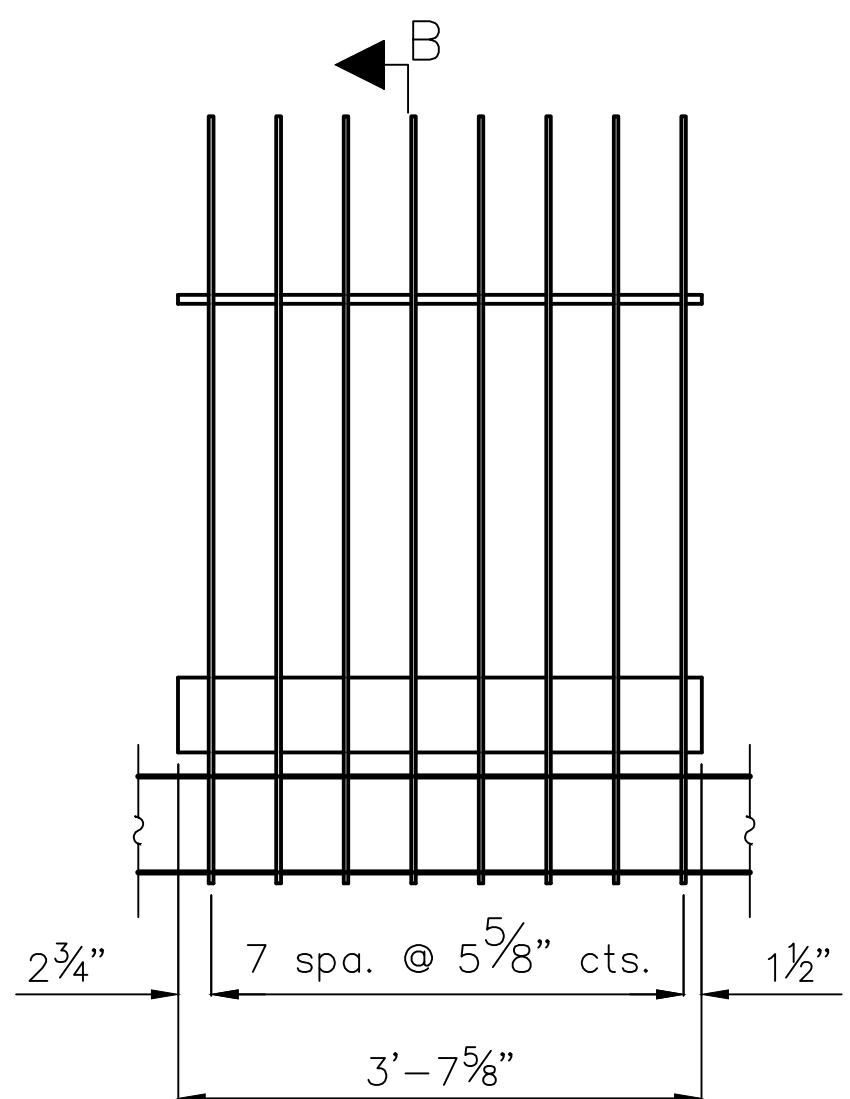


ELEVATION

RAILING PANEL TYPE S24

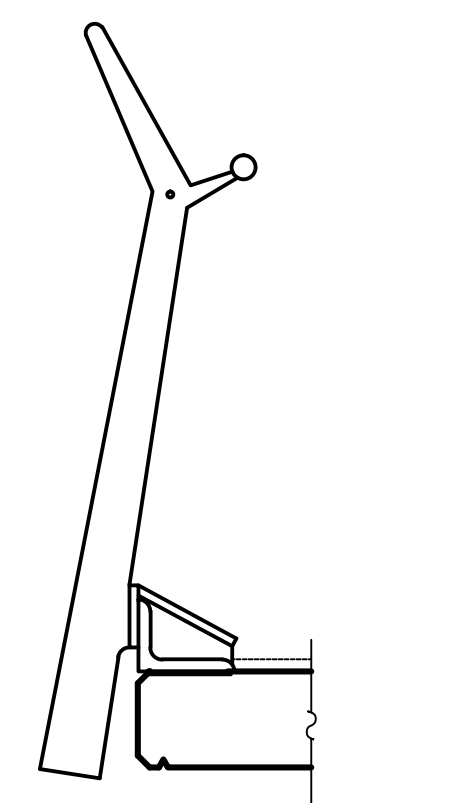


SECTION B-B

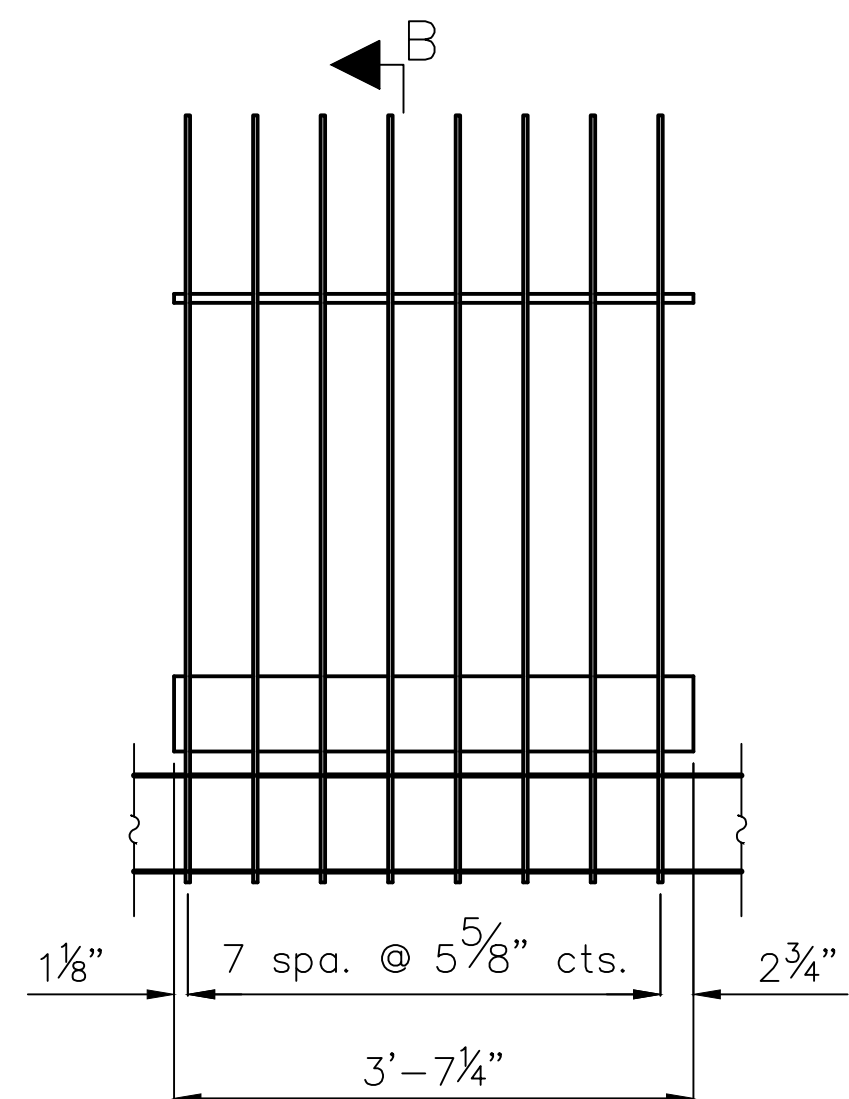


ELEVATION

RAILING PANEL TYPE S26

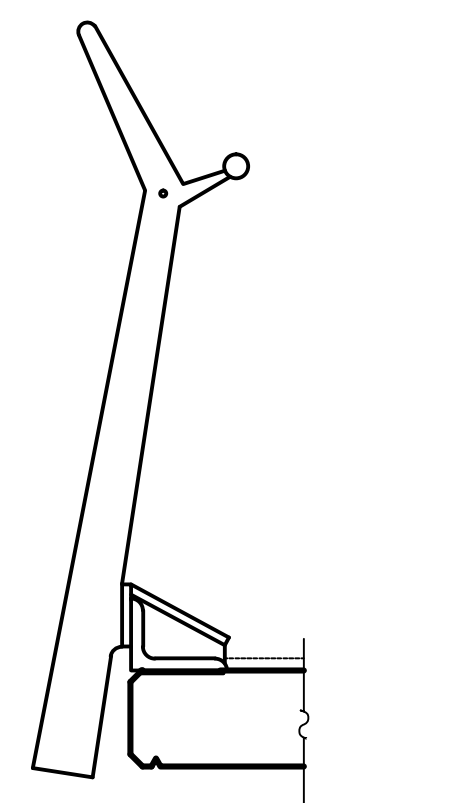


SECTION B-B

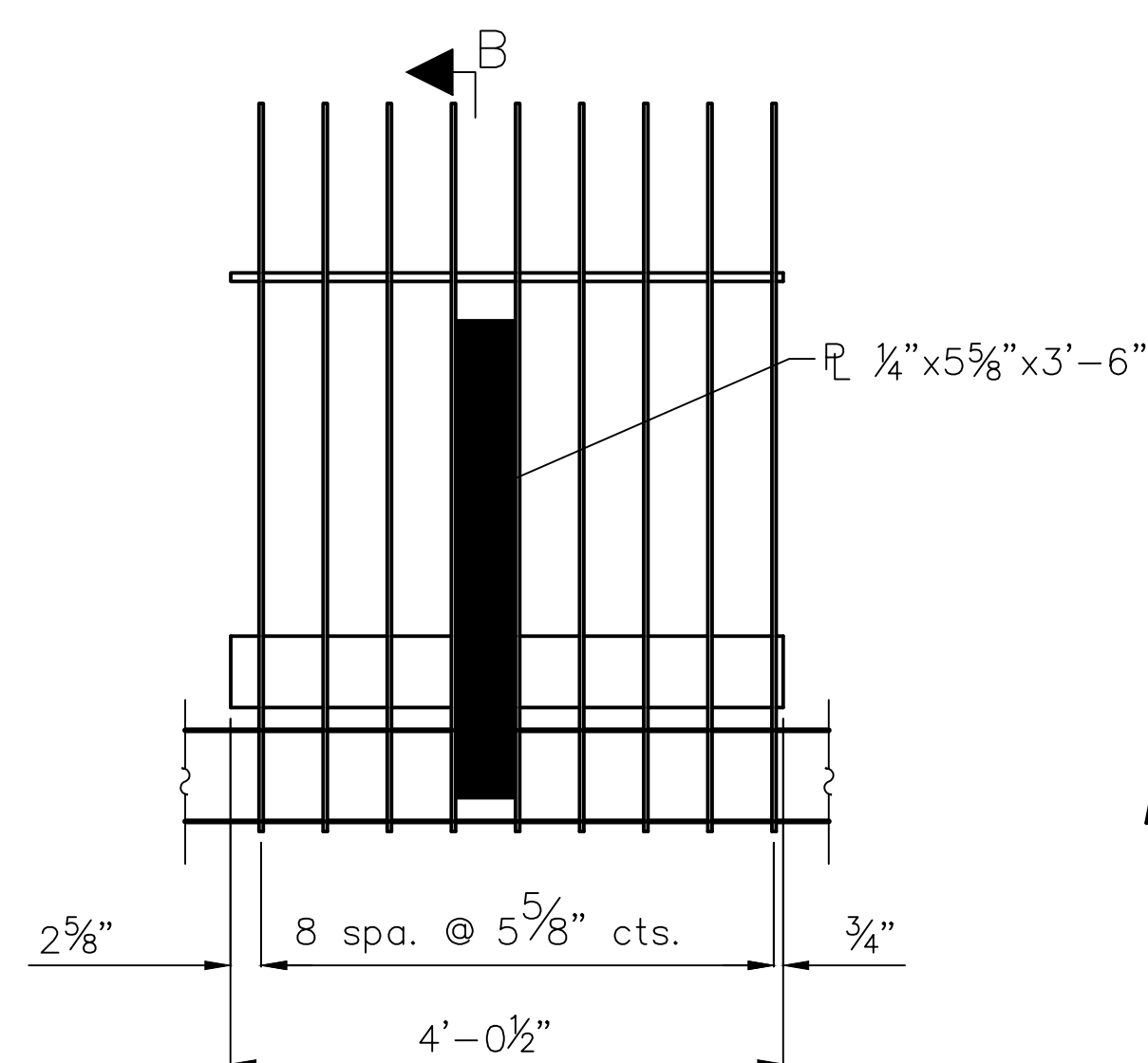


ELEVATION

RAILING PANEL TYPE S28

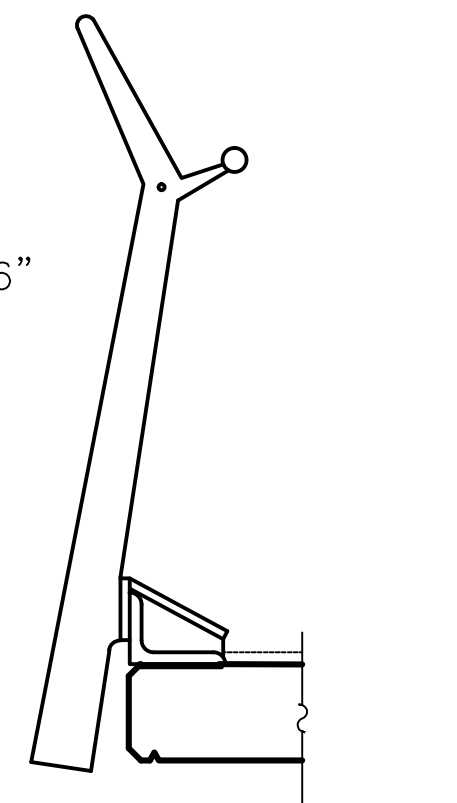


SECTION B-B

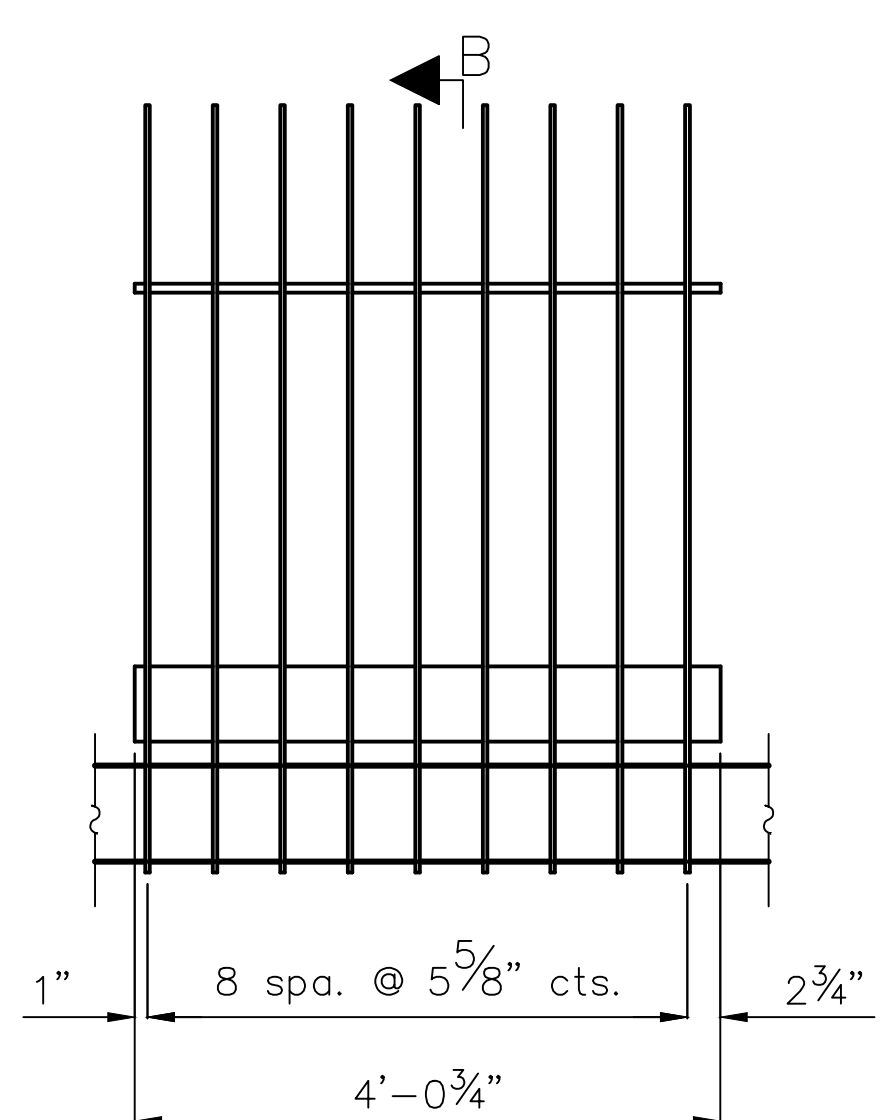


ELEVATION

RAILING PANEL TYPE S25

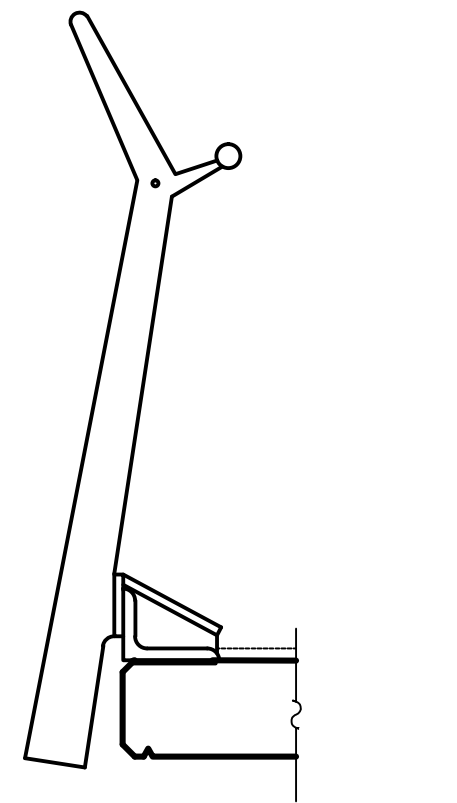


SECTION B-B

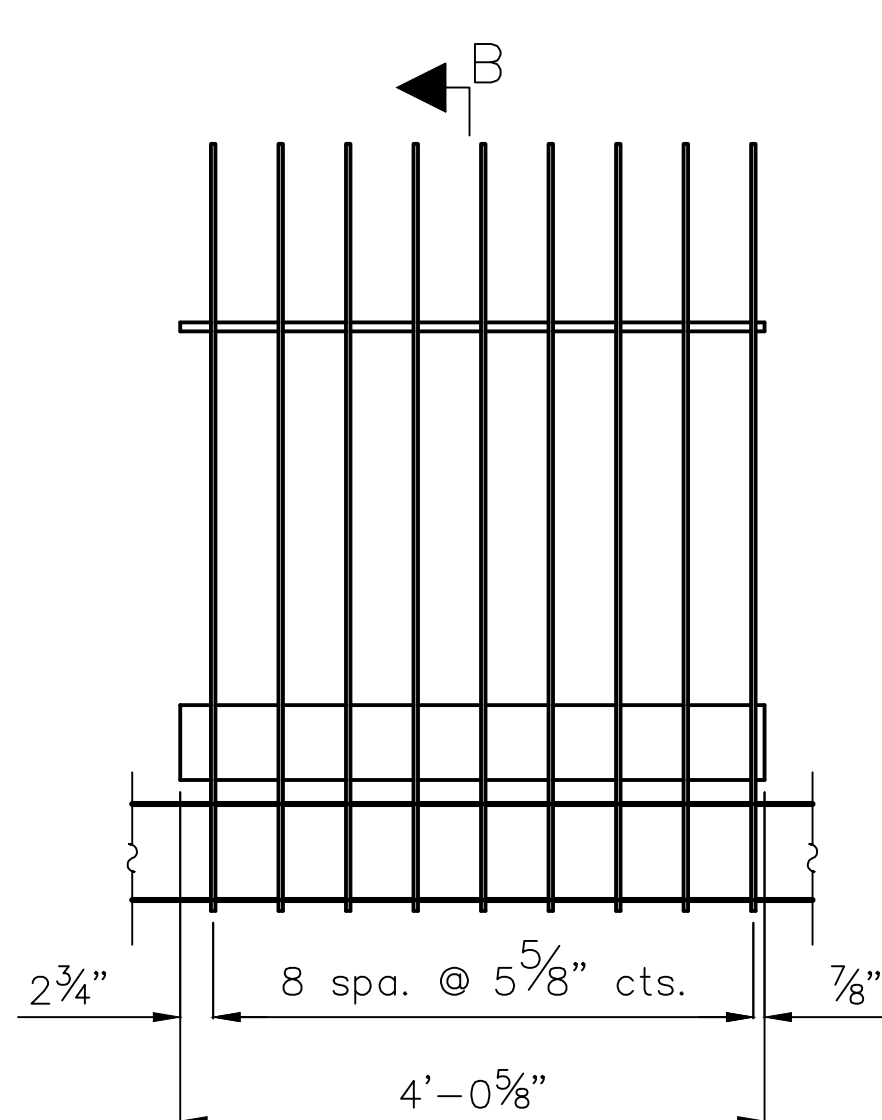


ELEVATION

RAILING PANEL TYPE S27

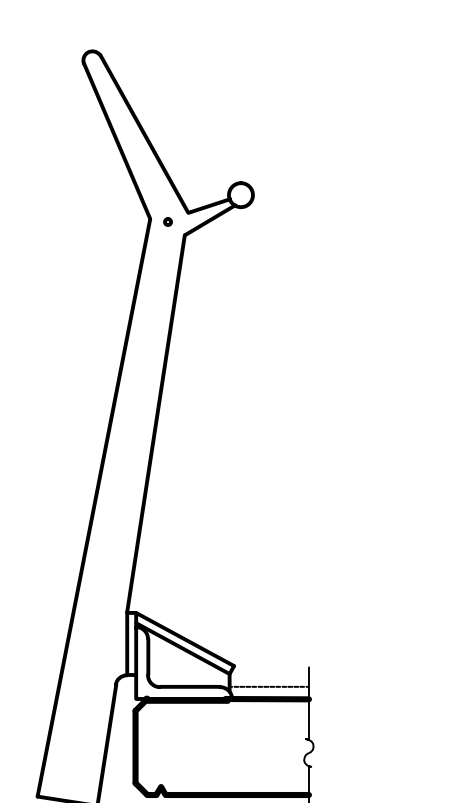


SECTION B-B



ELEVATION

RAILING PANEL TYPE S29



SECTION B-B

- Notes:
1. For Railing Anchorage Details, see sheet S-24 of S-87.
 2. For Railing Panel Details, see sheet A-1 of A-1.

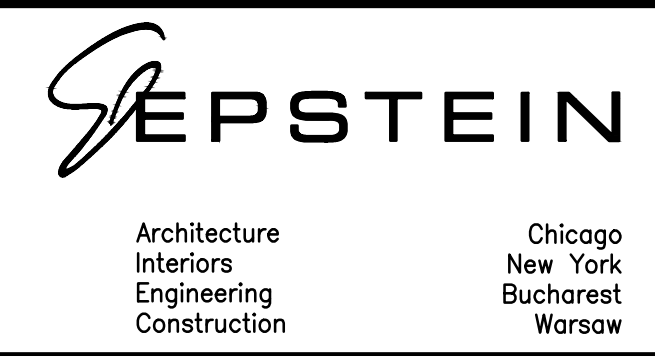
SECTION NO.: 12-E9121-00-BT
 DRAWN BY: J. MASI
 CHECKED BY: R. VARMA
 APPROVED BY: P. DOMBROWSKI
 DATE: 8/12/2016
 SCALE: 3/4" = 1'-0"

NO.	DATE	REVISIONS/ISSUANCES
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2	5/6/16	ISSUED FOR FINAL
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CITY OF CHICAGO
 DEPARTMENT OF TRANSPORTATION
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 Chicago, IL 60602
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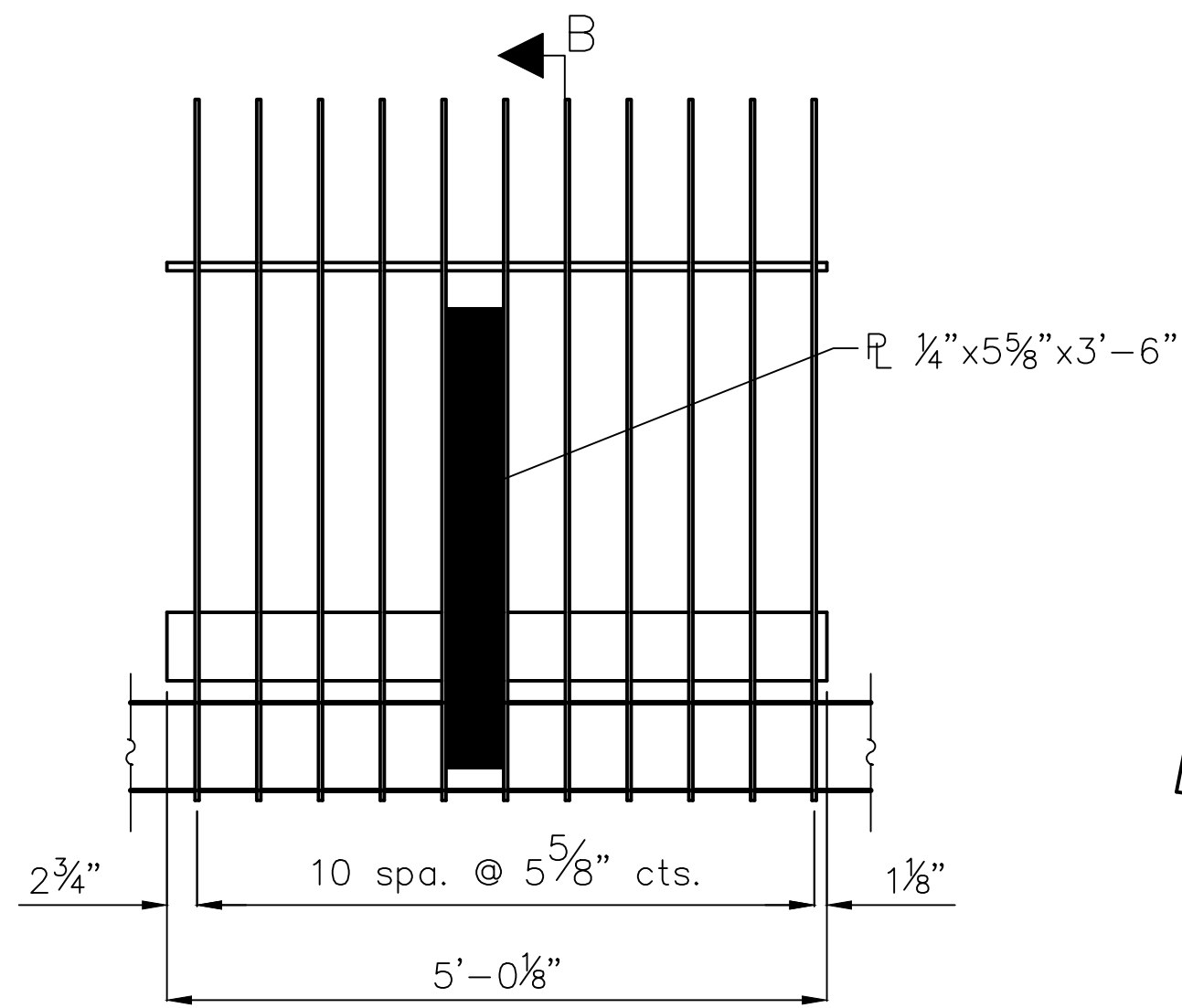
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ADDISON UNDERBRIDGE TRAIL CONNECTION FROM
 CLARK PARK TO CALIFORNIA PARK
 STRUCTURE NO. 016-P025

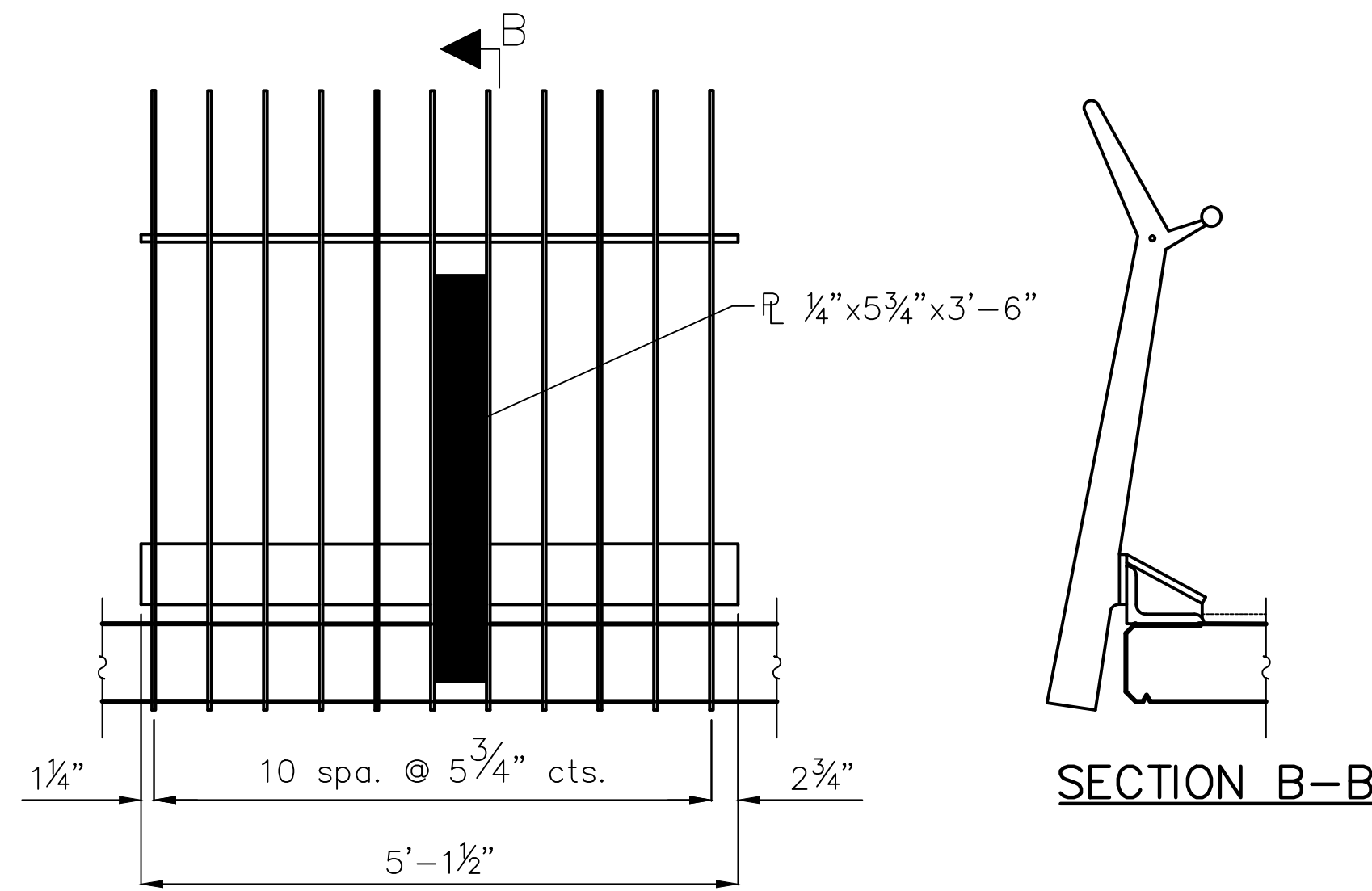
RAILING PANEL DETAILS

SHEET NO.
S-44
 SHEET 91 OF 158



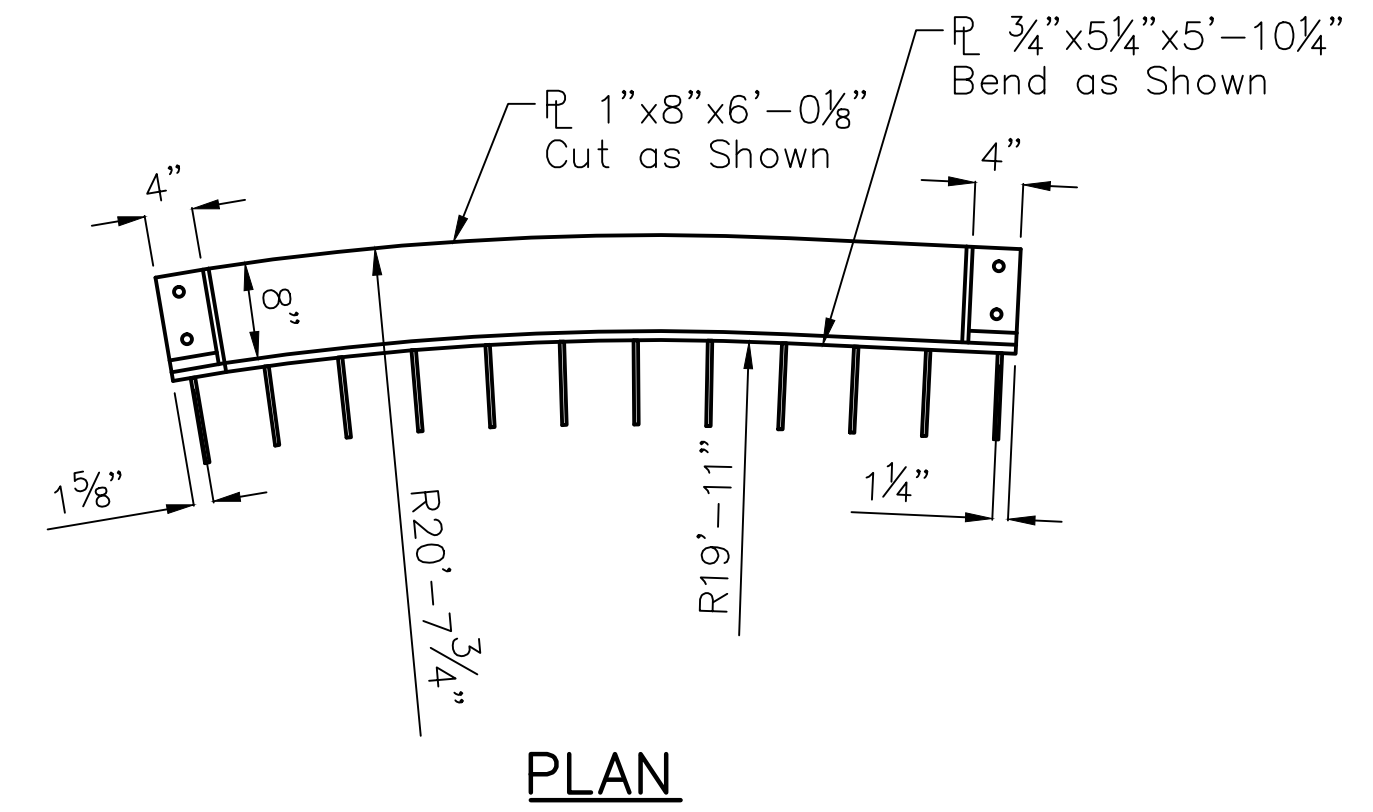
ELEVATION

RAILING PANEL TYPE S30

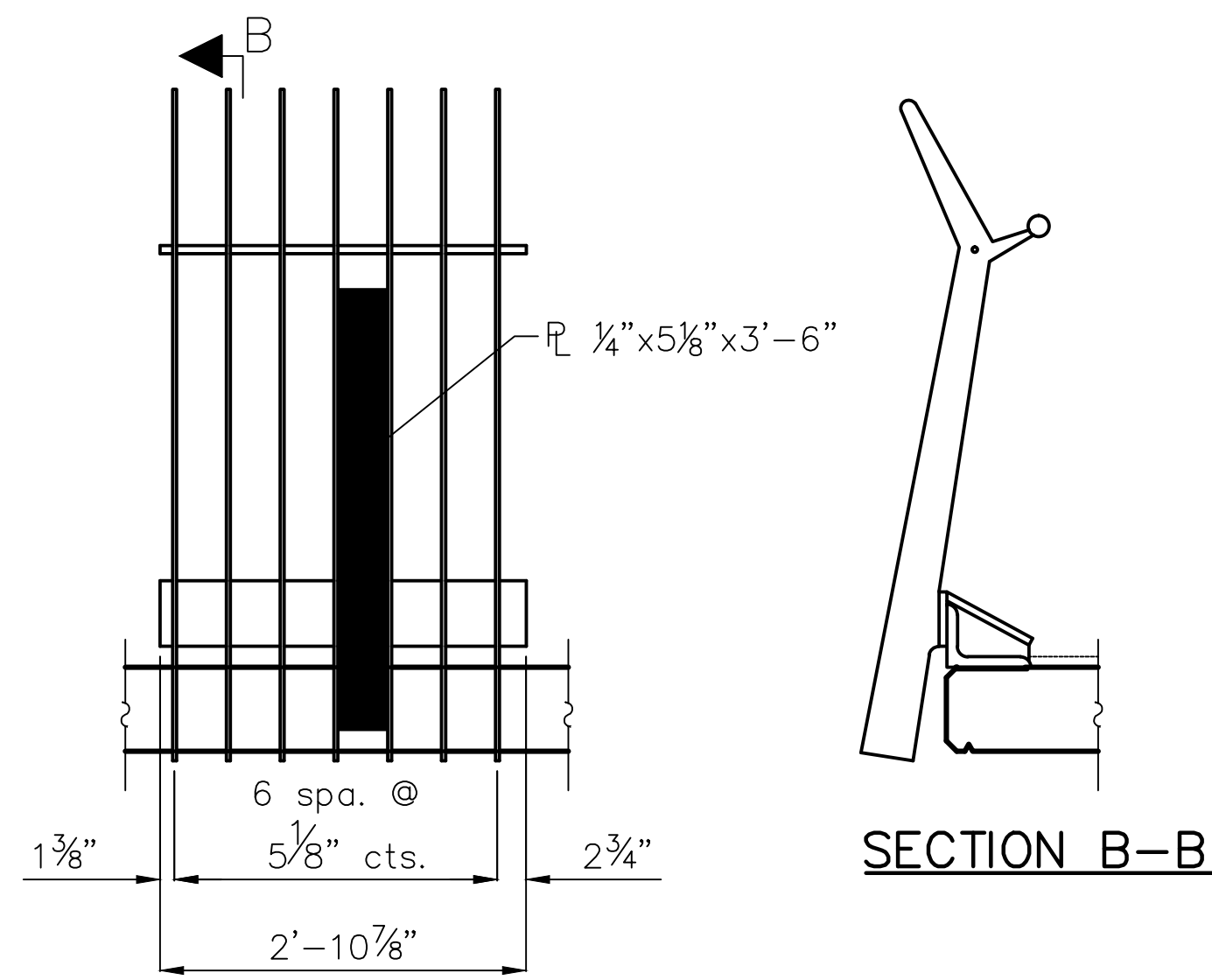


ELEVATION

RAILING PANEL TYPE S32

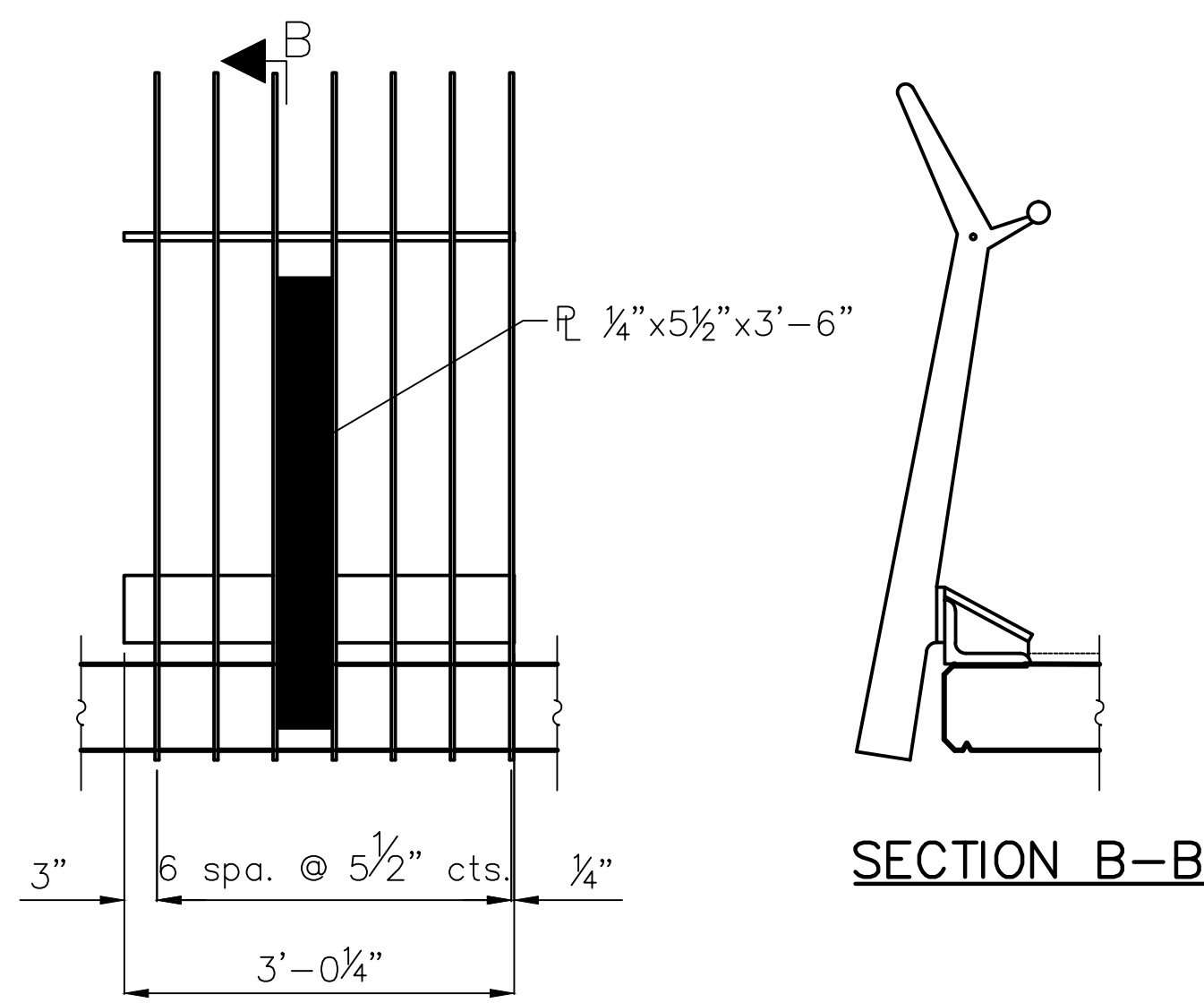


PLAN



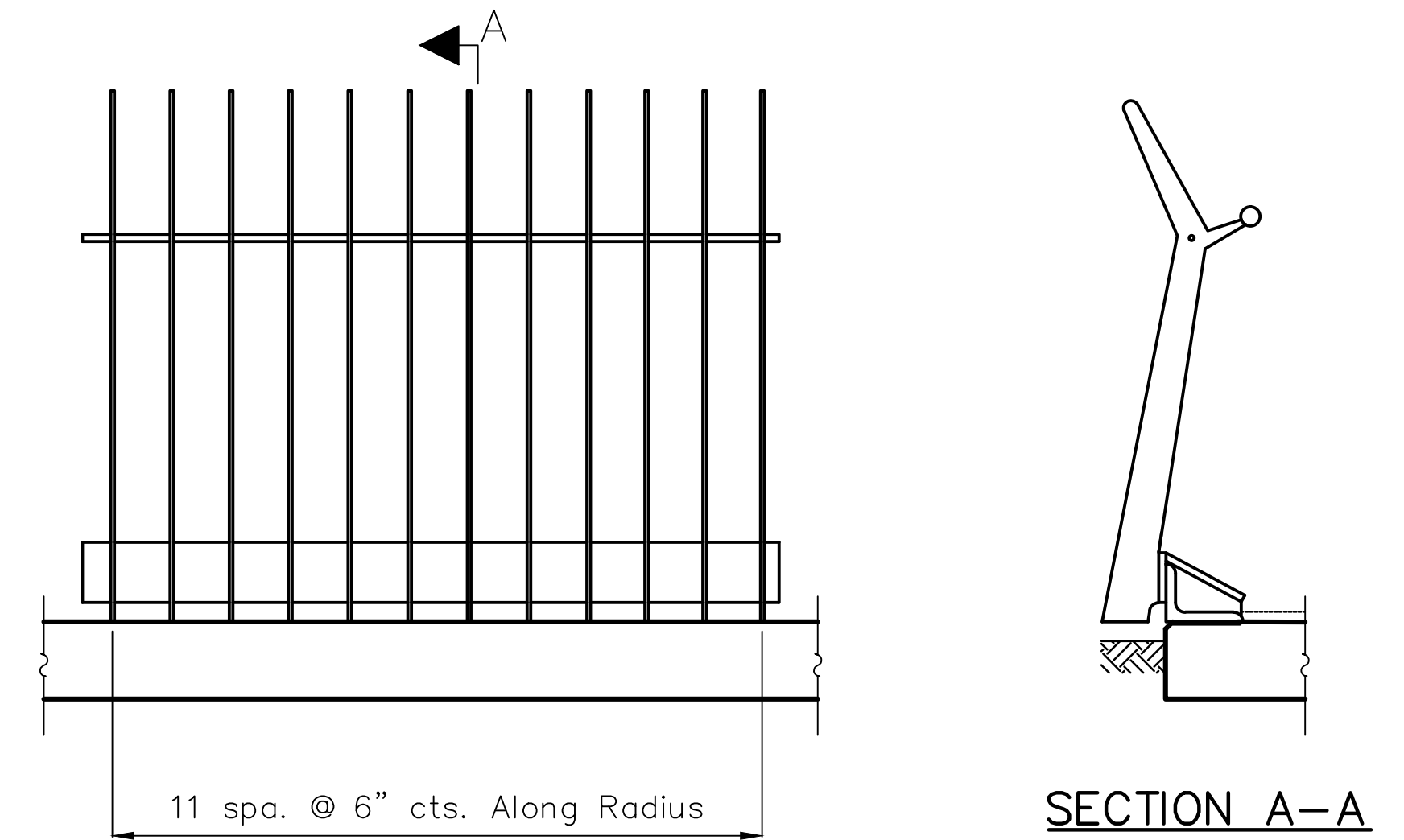
ELEVATION

RAILING PANEL TYPE S31



ELEVATION

RAILING PANEL TYPE S33



ELEVATION

RAILING PANEL TYPE S34

SECTION A-A

- Notes:
1. For Railing Anchorage Details, see sheet S-24 of S-87.
 2. For Railing Panel Details, see sheet A-1 of A-1.

SECTION NO.: 12-E9121-00-BT
 DRAWN BY: J. MASI
 CHECKED BY: R. VARMA
 APPROVED BY: P. DOMBROWSKI
 DATE: 8/12/2016
 SCALE: 3/4" = 1'-0"

NO.	DATE	REVISIONS/ISSUANCES
1	8/12/16	ISSUED FOR FINAL
2	5/6/16	ISSUED FOR FINAL
3	6/05/15	ISSUED FOR FINAL REVIEW

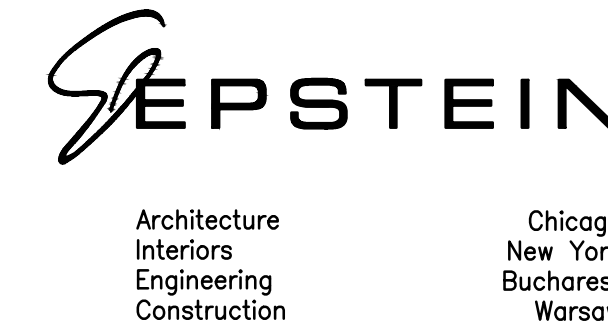
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 Chicago, IL 60602
 312.744.3600



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 600 W. Fulton Street
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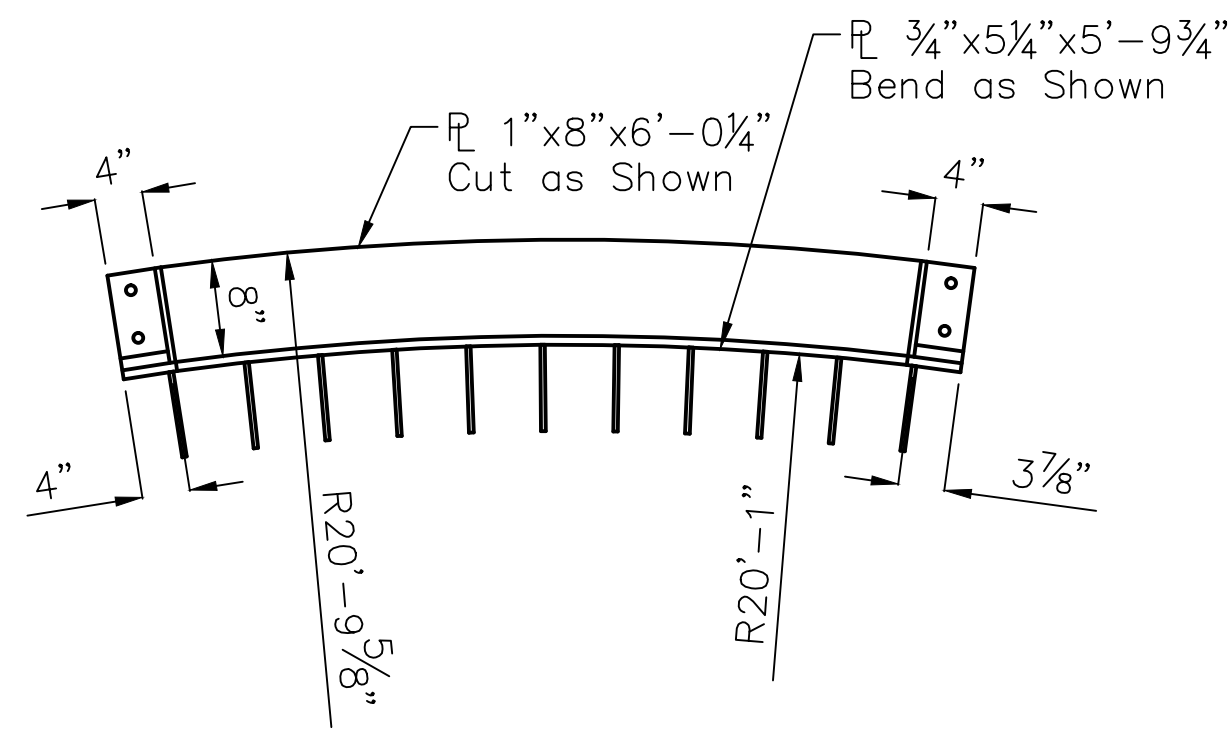
ADDISON UNDERBRIDGE TRAIL CONNECTION FROM
 CLARK PARK TO CALIFORNIA PARK
 STRUCTURE NO. 016-P025

RAILING PANEL DETAILS

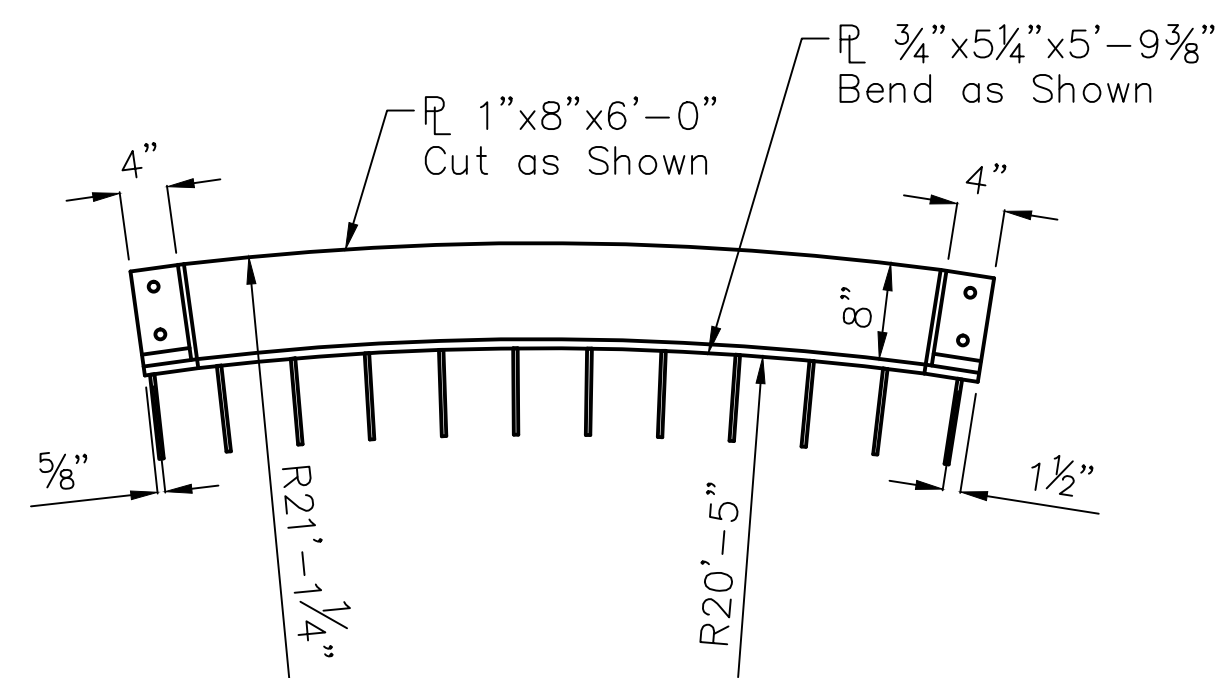
SHEET NO.
 S-45

SHEET 92 OF 158

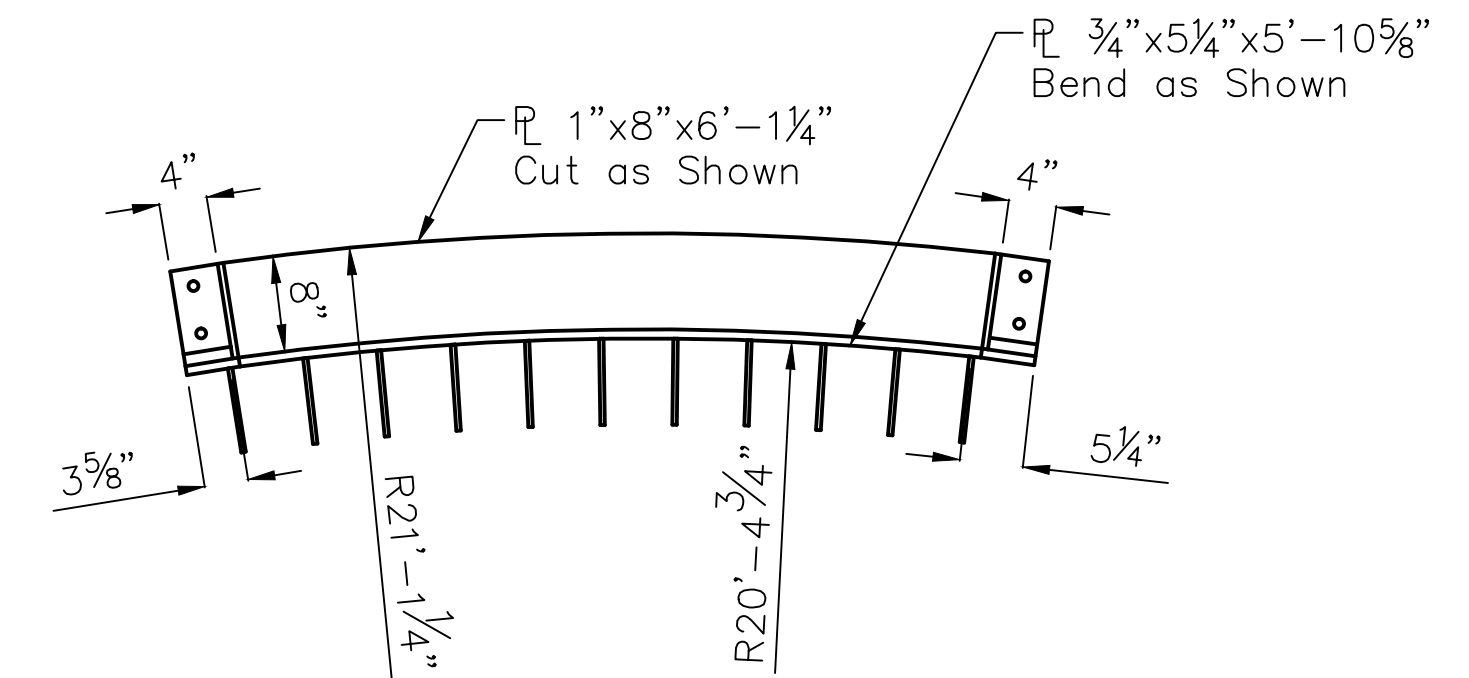
REVISION



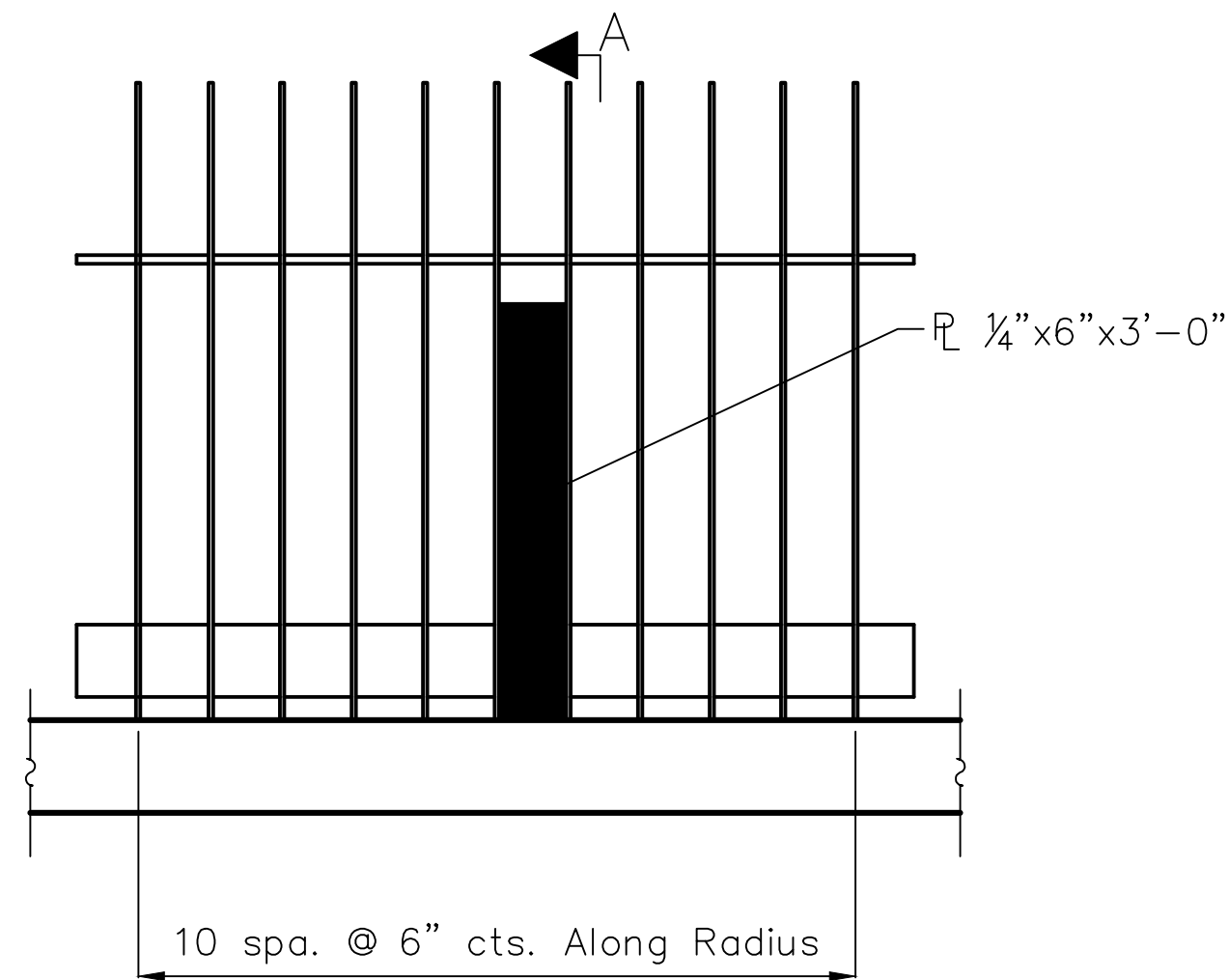
PLAN



PLAN

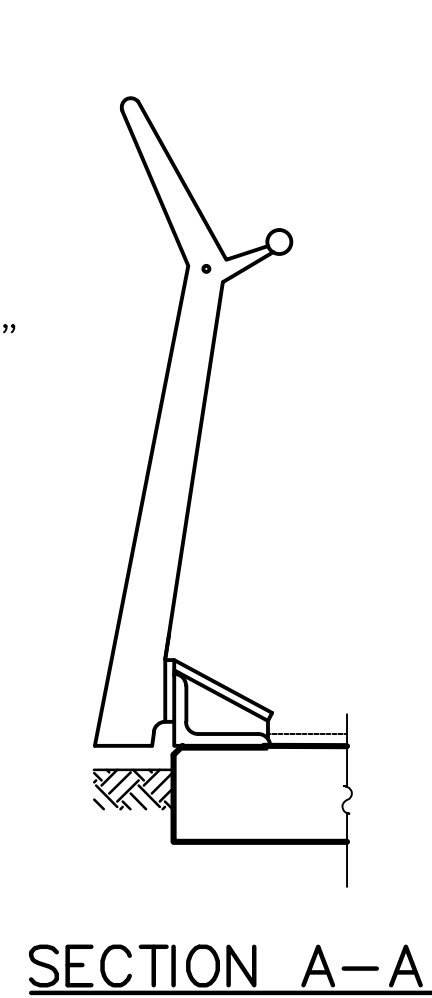


PLAN

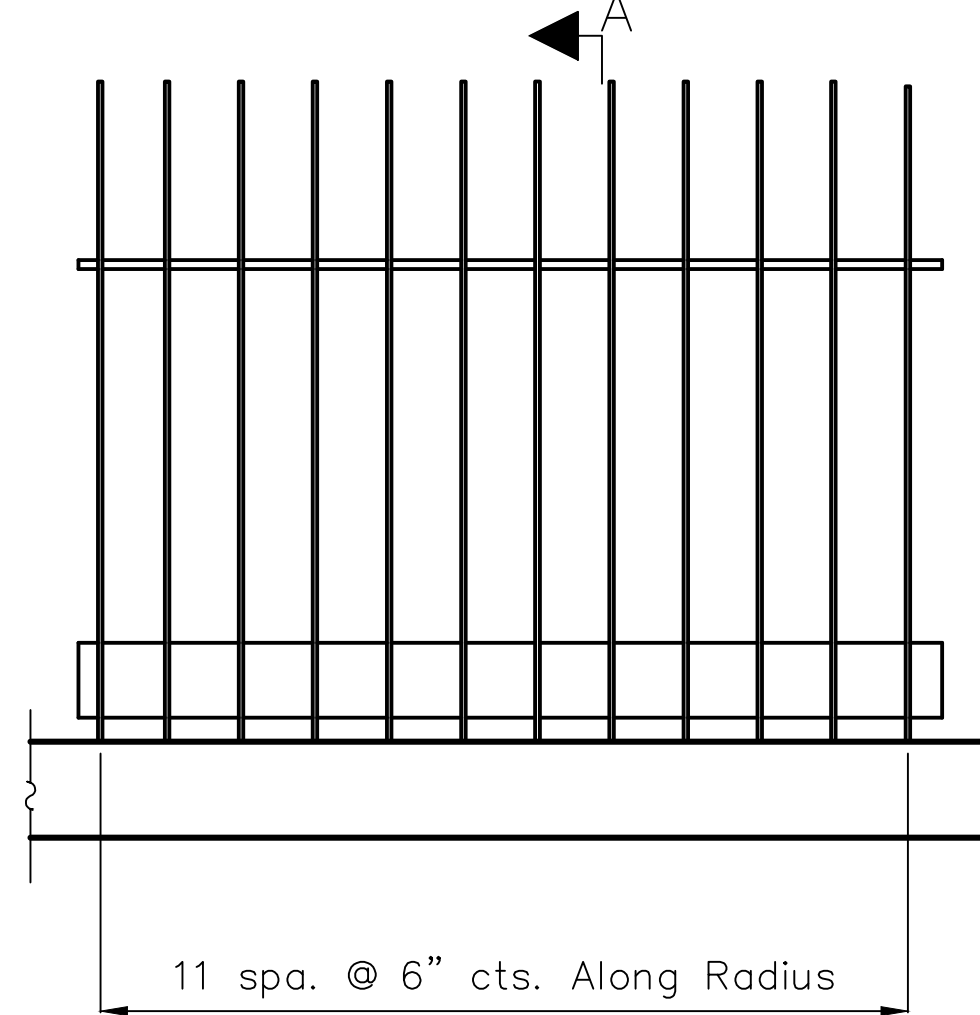


ELEVATION

RAILING PANEL TYPE S35

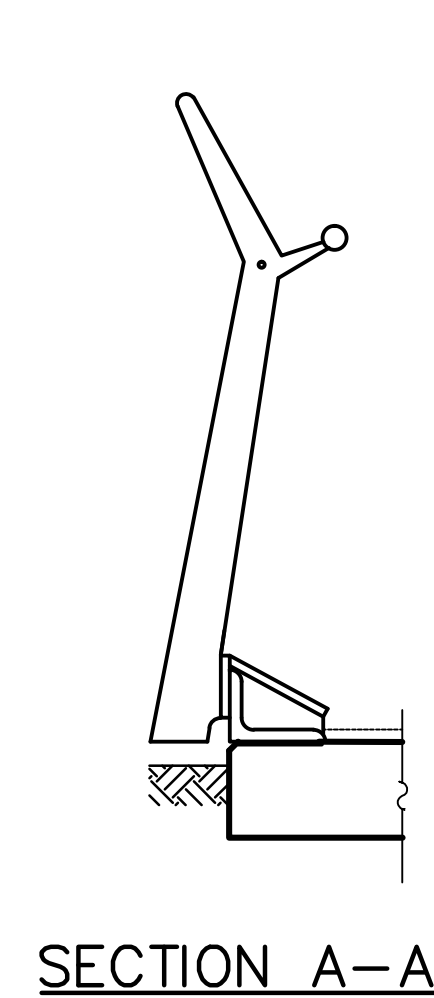


SECTION A-A

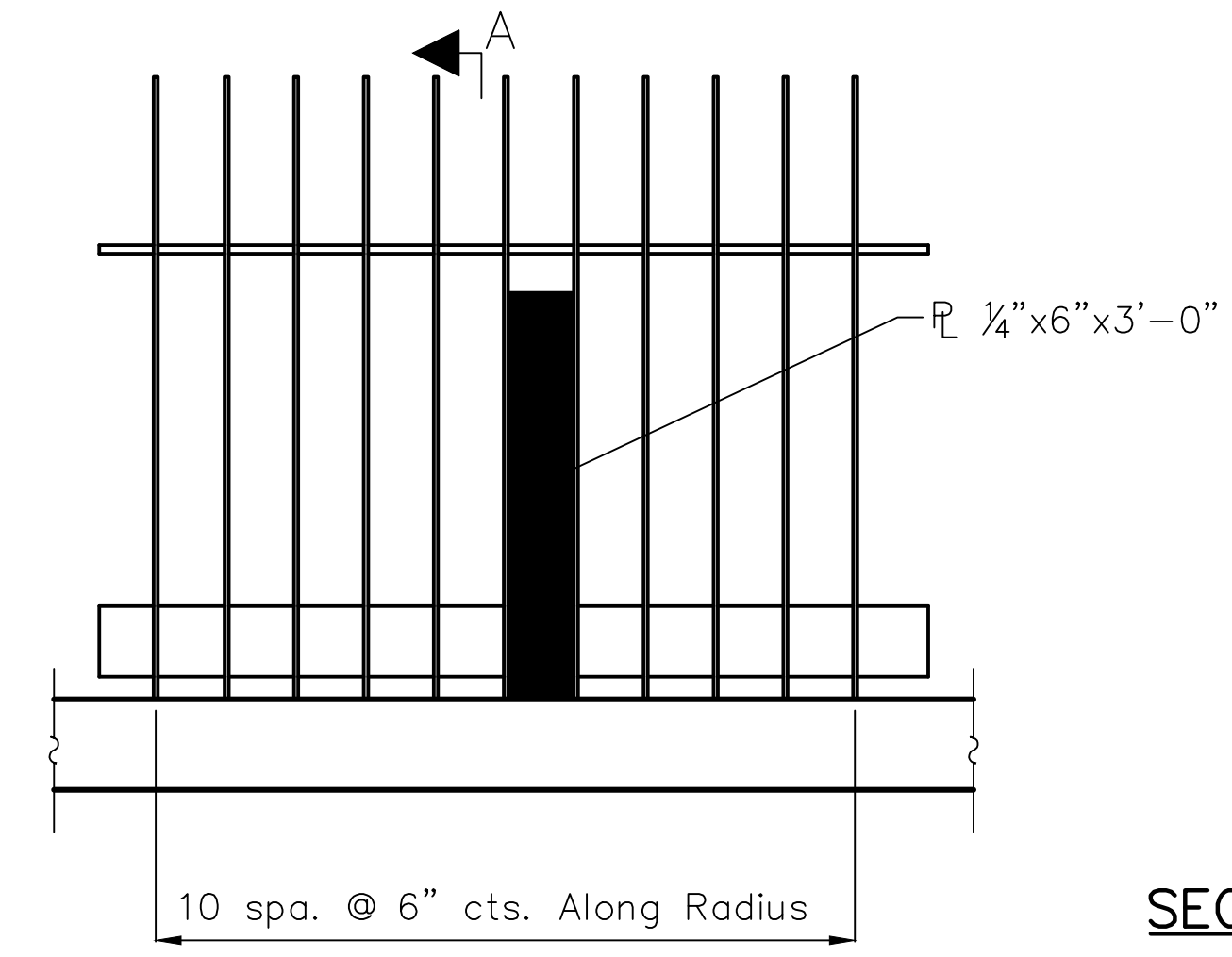


ELEVATION

RAILING PANEL TYPE S36

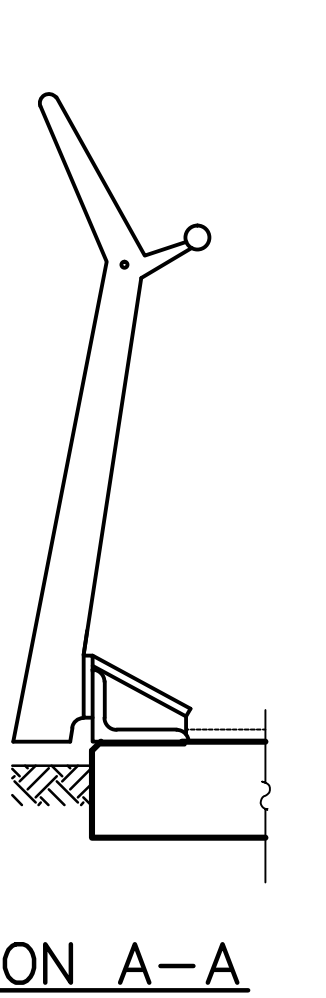


SECTION A-A



ELEVATION

RAILING PANEL TYPE S37



SECTION A-A

- Notes:
1. For Railing Anchorage Details, see sheet S-24 of S-87.
 2. For Railing Panel Details, see sheet A-1 of A-1.

SECTION NO.: 12-E9121-00-BT
 DRAWN BY: J. MASI
 CHECKED BY: R. VARMA
 APPROVED BY: P. DOMBROWSKI
 DATE: 8/12/2016
 SCALE: 3/4" = 1'-0"

NO.	DATE	REVISIONS/ISSUANCES
1	8/12/16	ISSUED FOR FINAL
2	5/6/16	ISSUED FOR FINAL
3	6/05/15	ISSUED FOR FINAL REVIEW

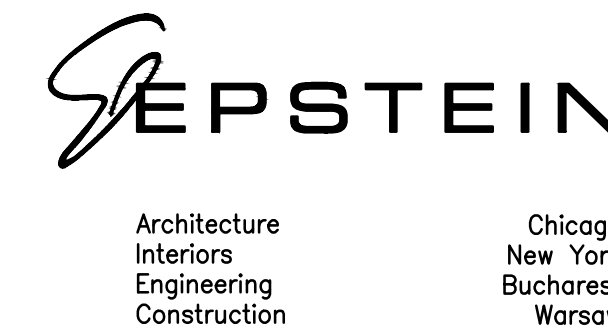
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 Chicago, IL 60602
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CONSULTANT

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ADDISON UNDERBRIDGE TRAIL CONNECTION FROM
 CLARK PARK TO CALIFORNIA PARK
 STRUCTURE NO. 016-P025

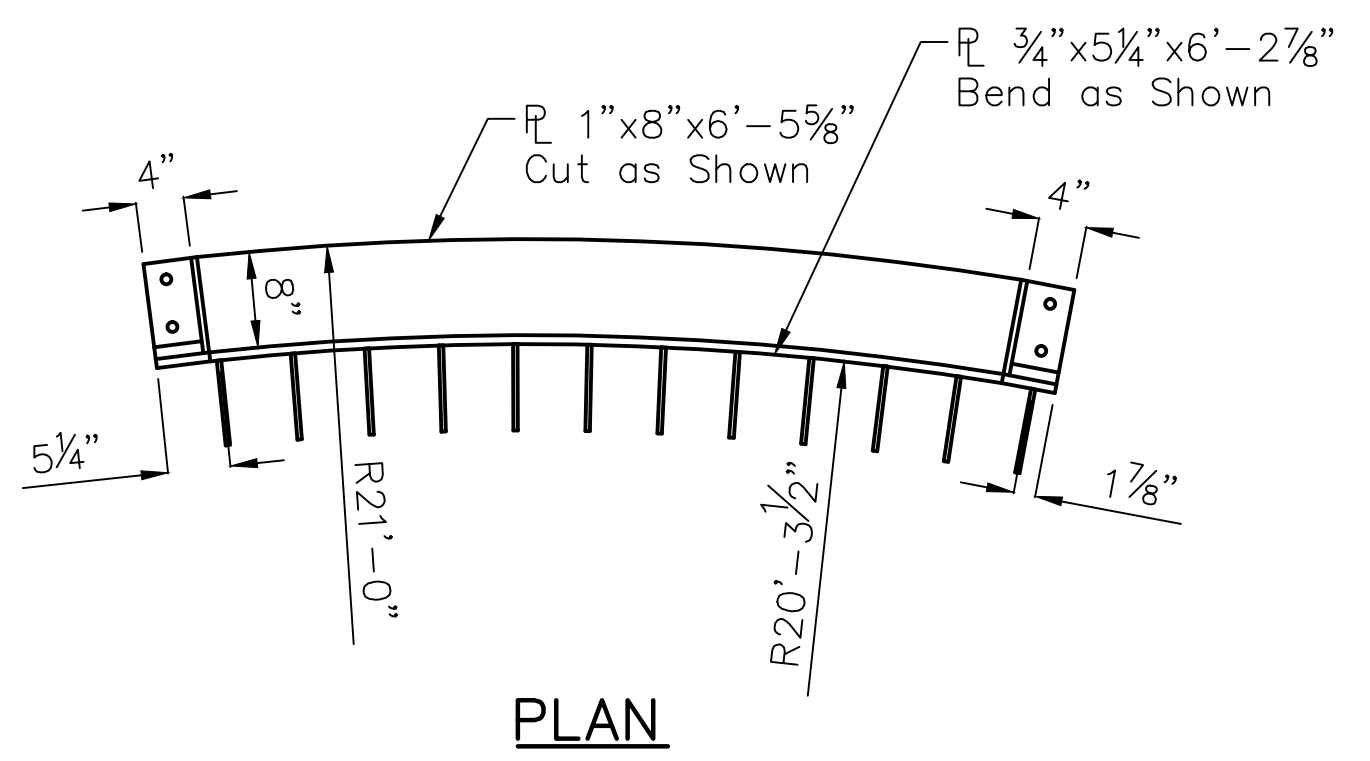
RAILING PANEL DETAILS

SHEET NO.
 S-46

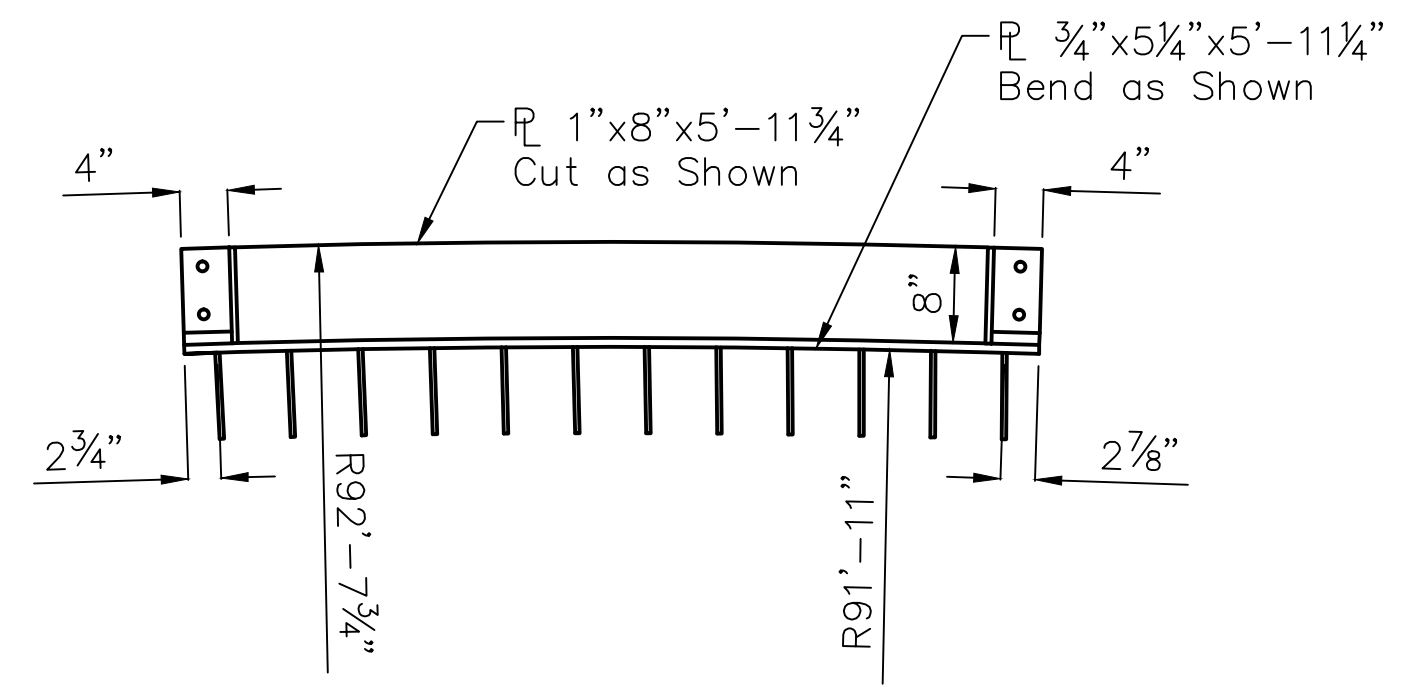
SHEET 93 OF 158

REVISION

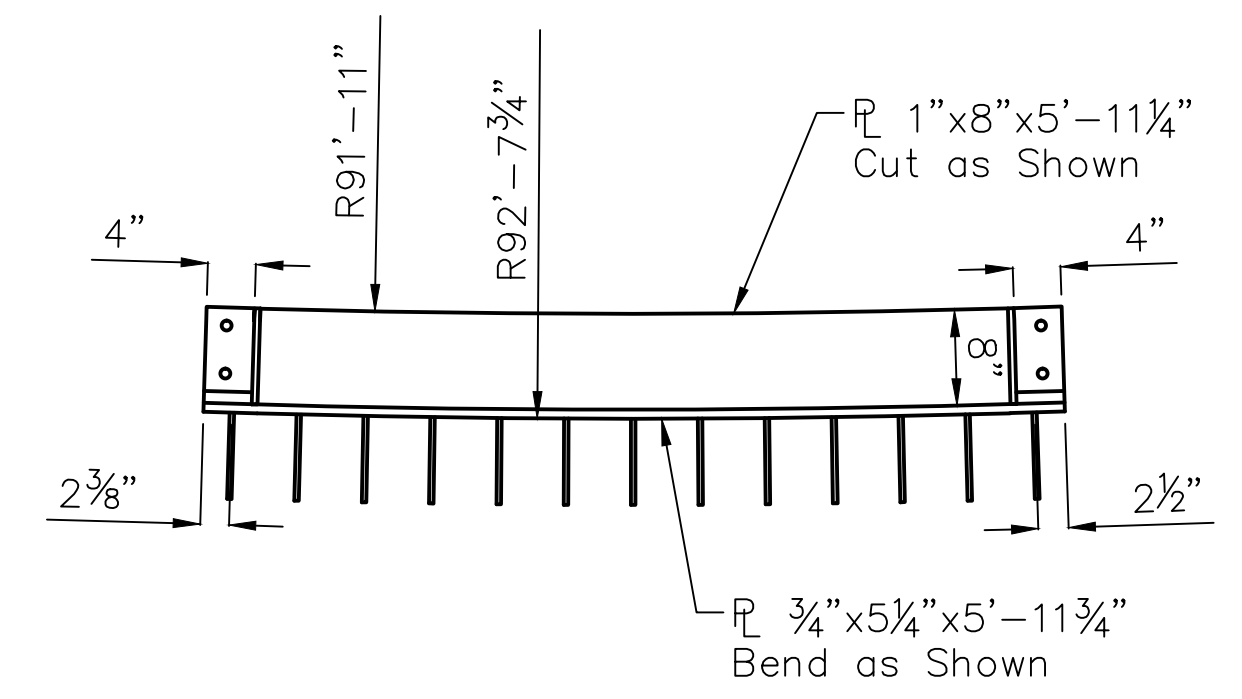
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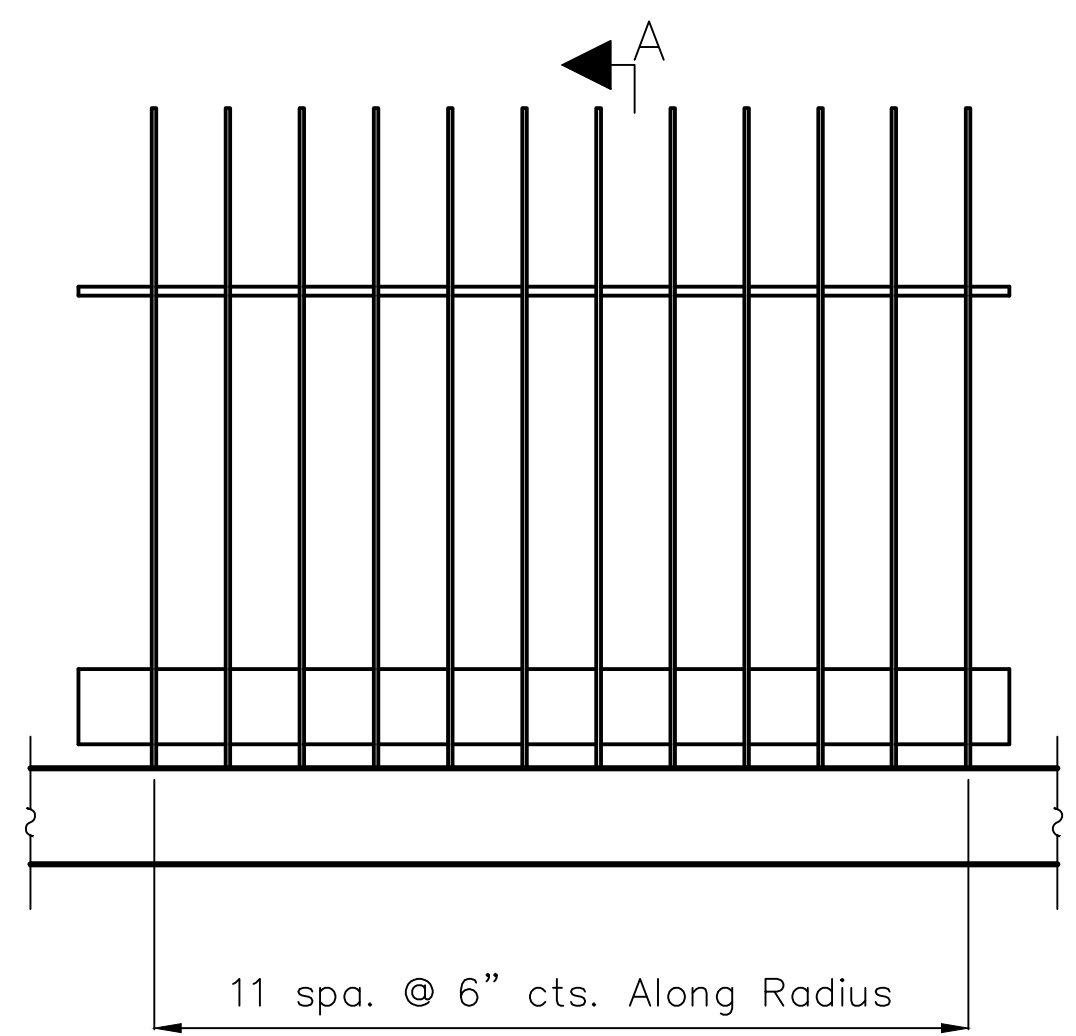
PLAN



ELEVATION

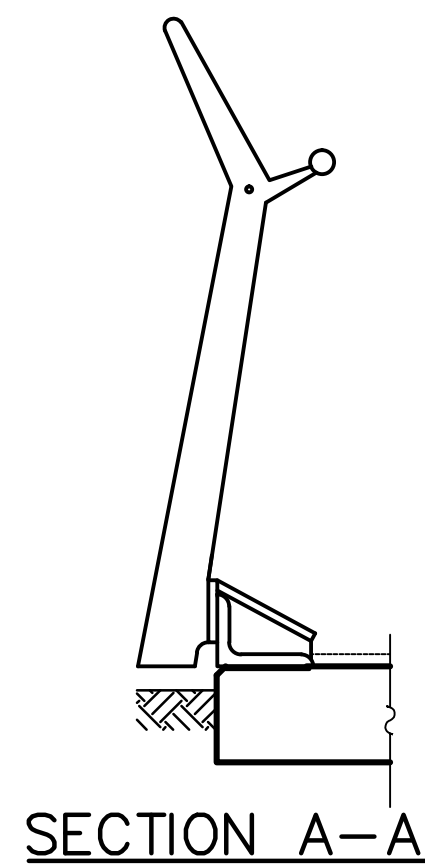


ELEVATION

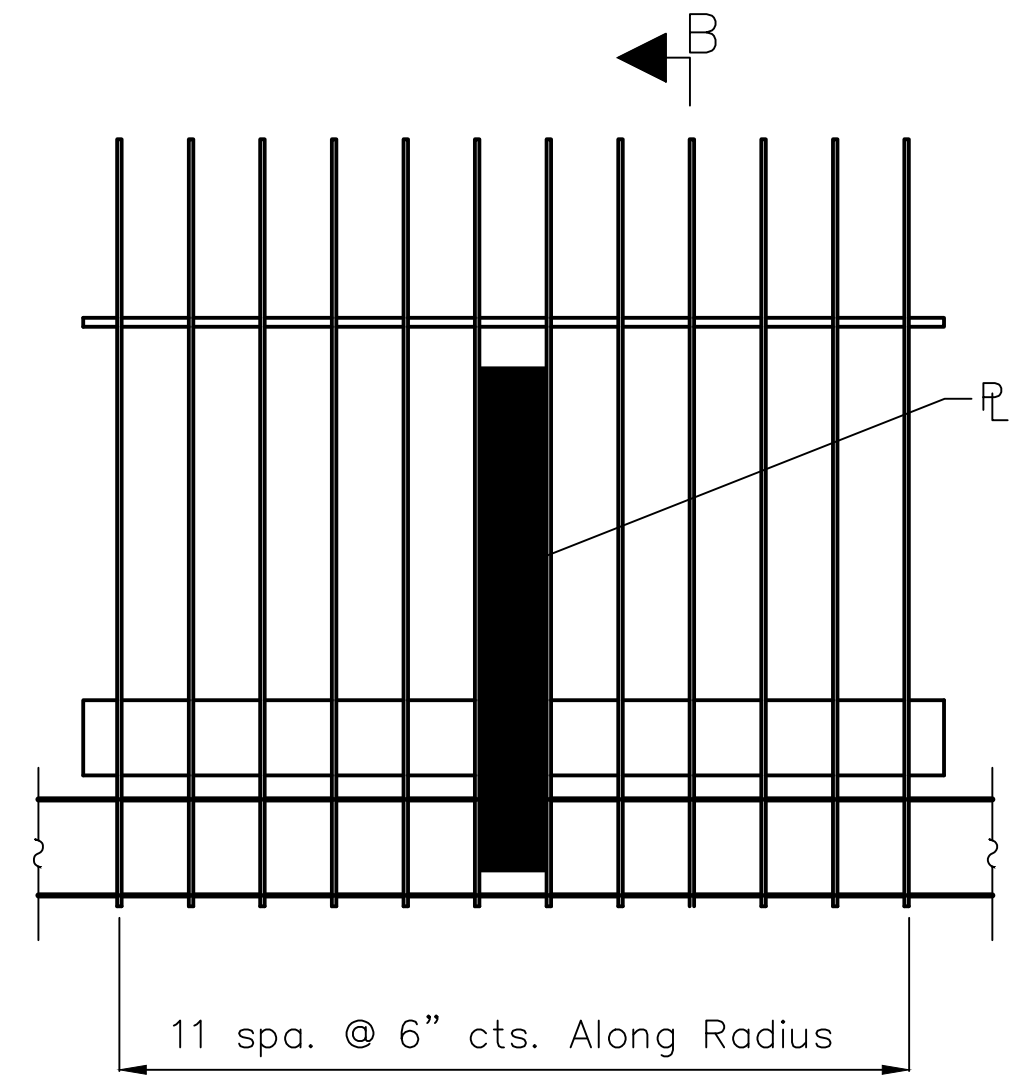


ELEVATION

RAILING PANEL TYPE S38

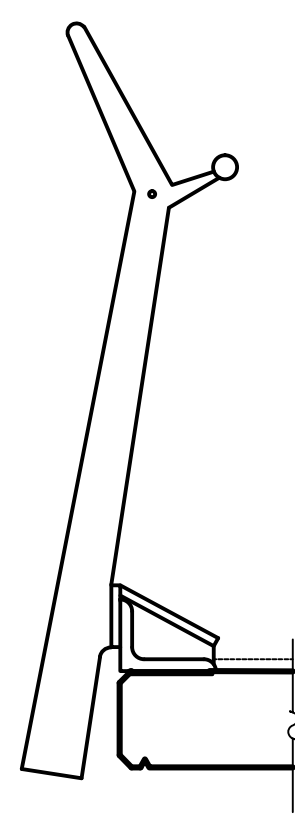


SECTION A-A

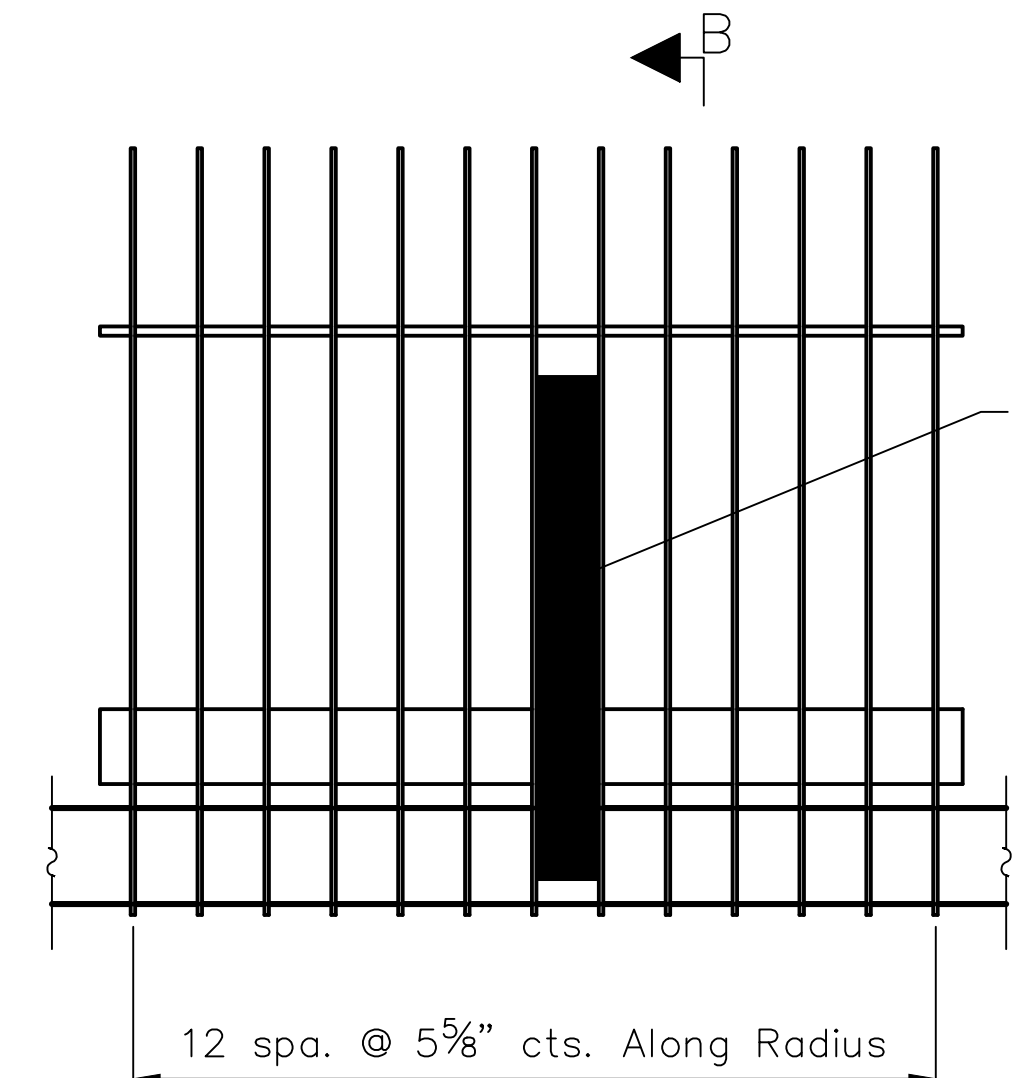


ELEVATION

RAILING PANEL TYPE S39

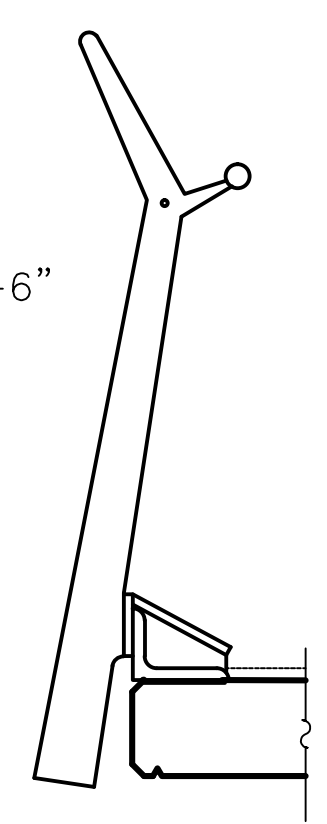


SECTION B-B



ELEVATION

RAILING PANEL TYPE S40



SECTION B-B

- Notes:
1. For Railing Anchorage Details, see sheet S-24 of S-87.
 2. For Railing Panel Details, see sheet A-1 of A-1.

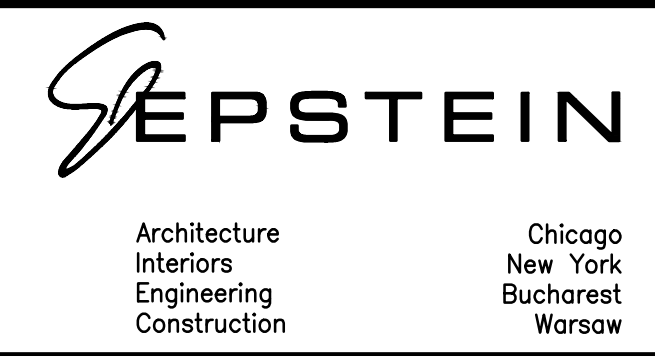
SECTION NO.: 12-E9121-00-BT
 DRAWN BY: J. MASI
 CHECKED BY: R. VARMA
 APPROVED BY: P. DOMBROWSKI
 DATE: 8/12/2016
 SCALE: 3/4" = 1'-0"

NO.	DATE	REVISIONS/ISSUANCES
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2	5/6/16	ISSUED FOR FINAL
3	6/05/15	ISSUED FOR FINAL REVIEW

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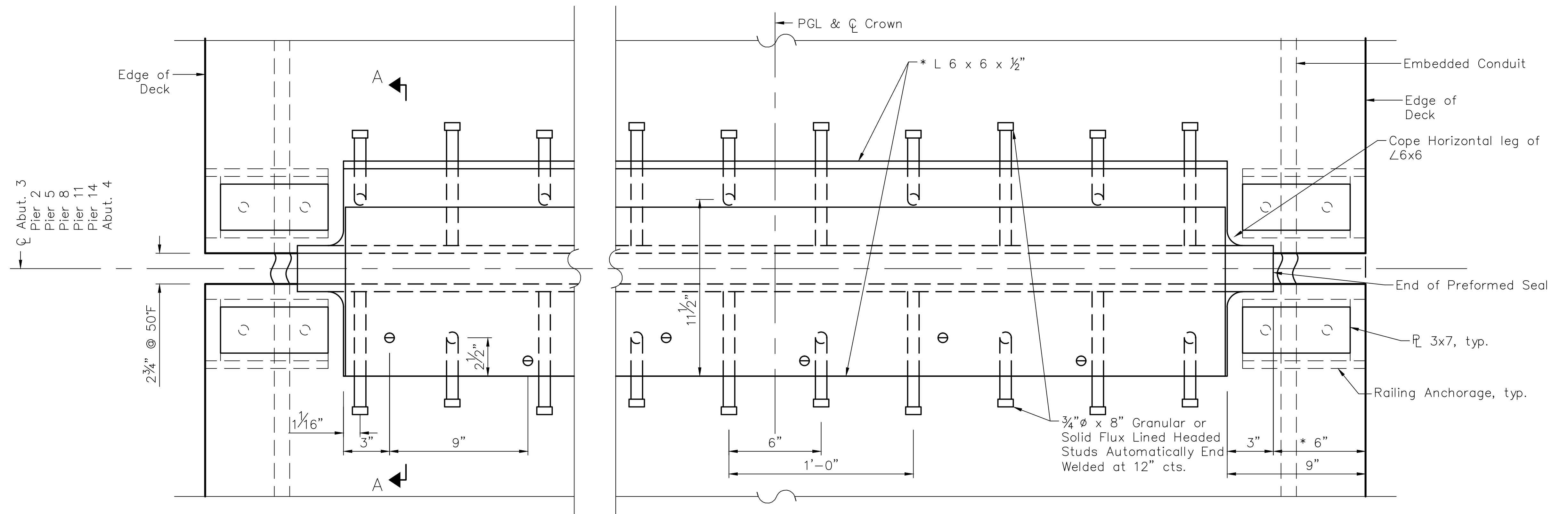
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ADDISON UNDERBRIDGE TRAIL CONNECTION FROM
 CLARK PARK TO CALIFORNIA PARK
 STRUCTURE NO. 016-P025

RAILING PANEL DETAILS

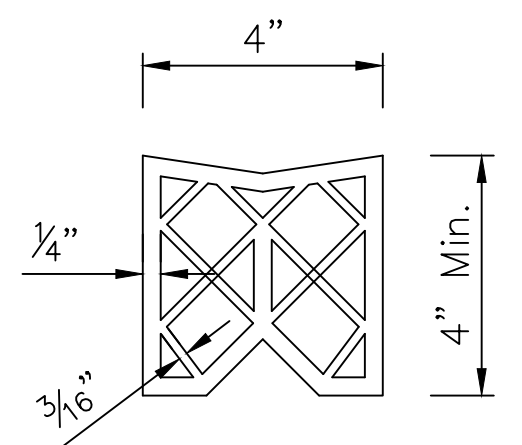
SHEET NO. S-47
 SHEET 94 OF 158
 REVISION



* Cut retainer bars (1/2" x 1/2") in deck 6" short of deck face

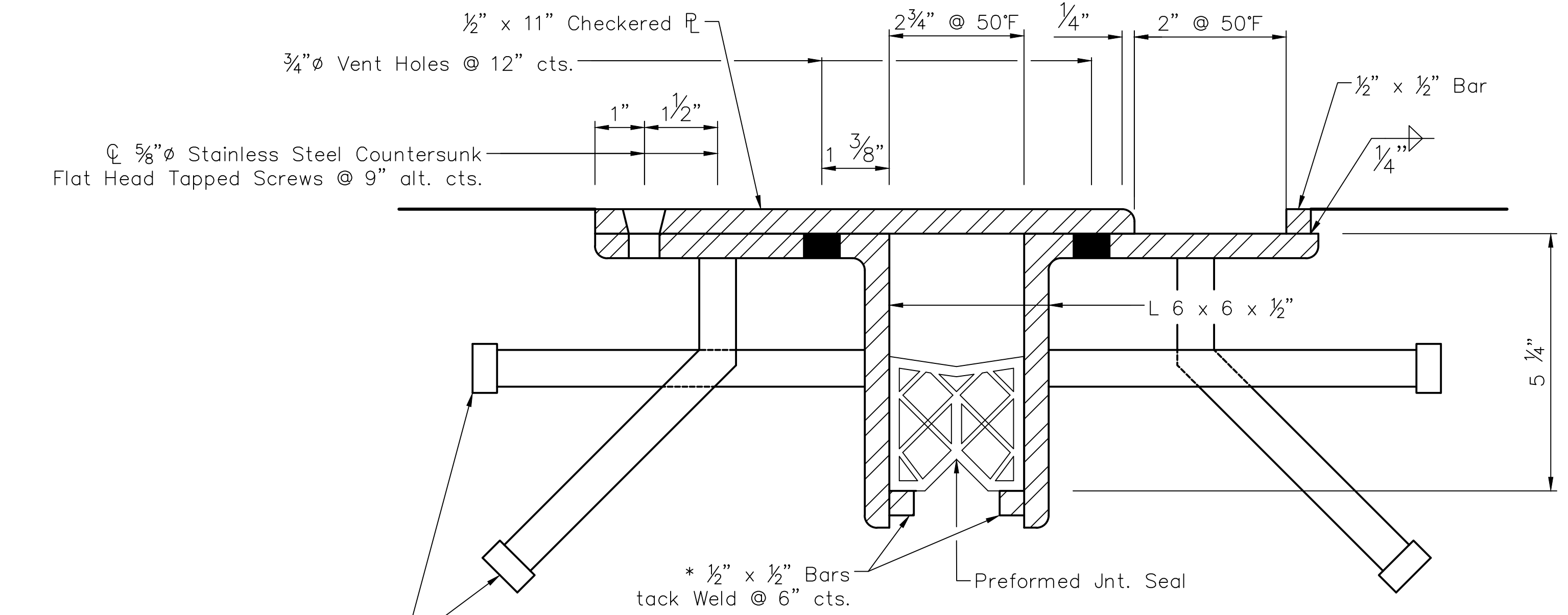
BILL OF MATERIAL

Item	Units	Quantity
Preformed Joint Seal, 4"	Foot	112



PREFORMED JOINT SEAL

PLAN



SECTION A-A

- Notes:**
1. Joint shall be fabricated to match cross section and crown.
 2. All steel members to be stainless steel (ASTM A240). Welding in accordance with provisions of AWS D1.6.

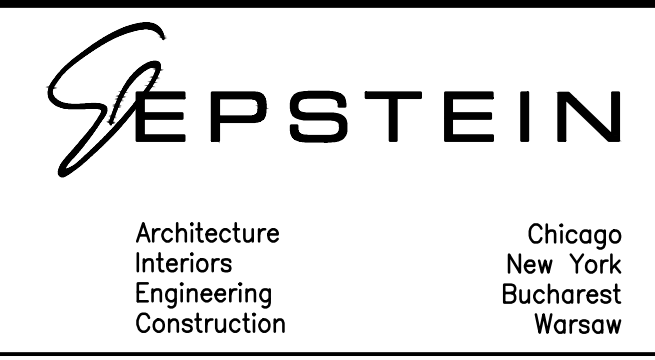
SECTION NO.: 12-E9121-00-BT
 DRAWN BY: J. MASI
 CHECKED BY: R. VARMA
 APPROVED BY: P. DOMBROWSKI
 DATE: 8/12/2016
 SCALE: N.T.S.

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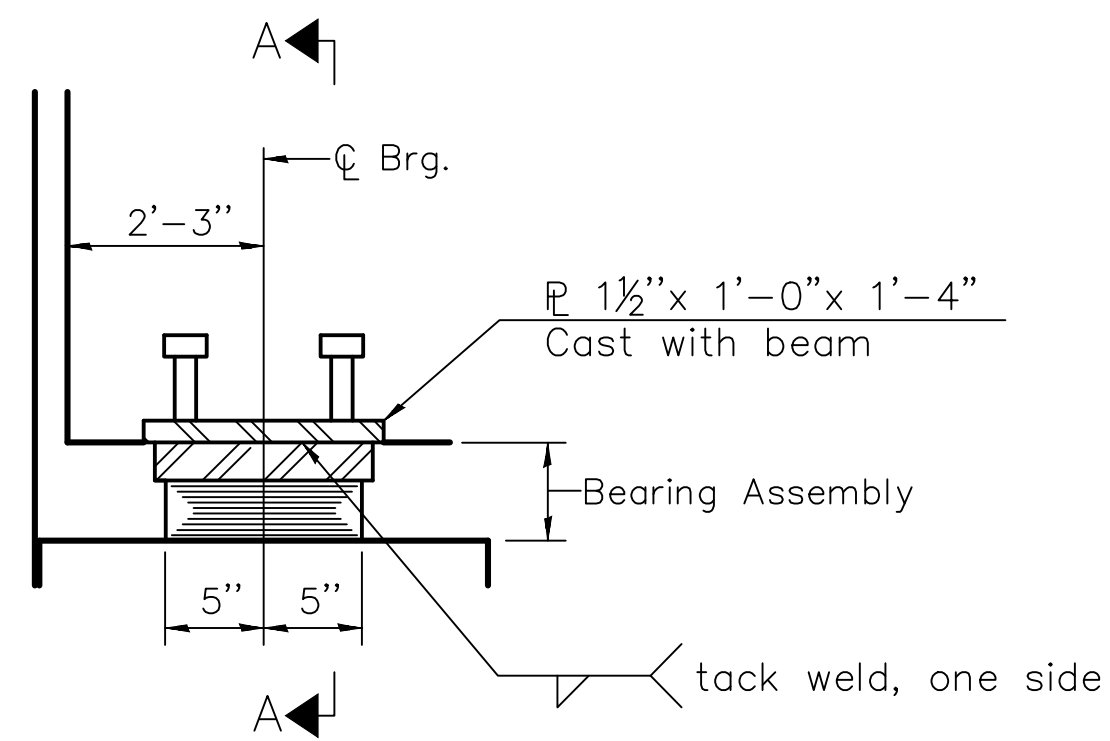
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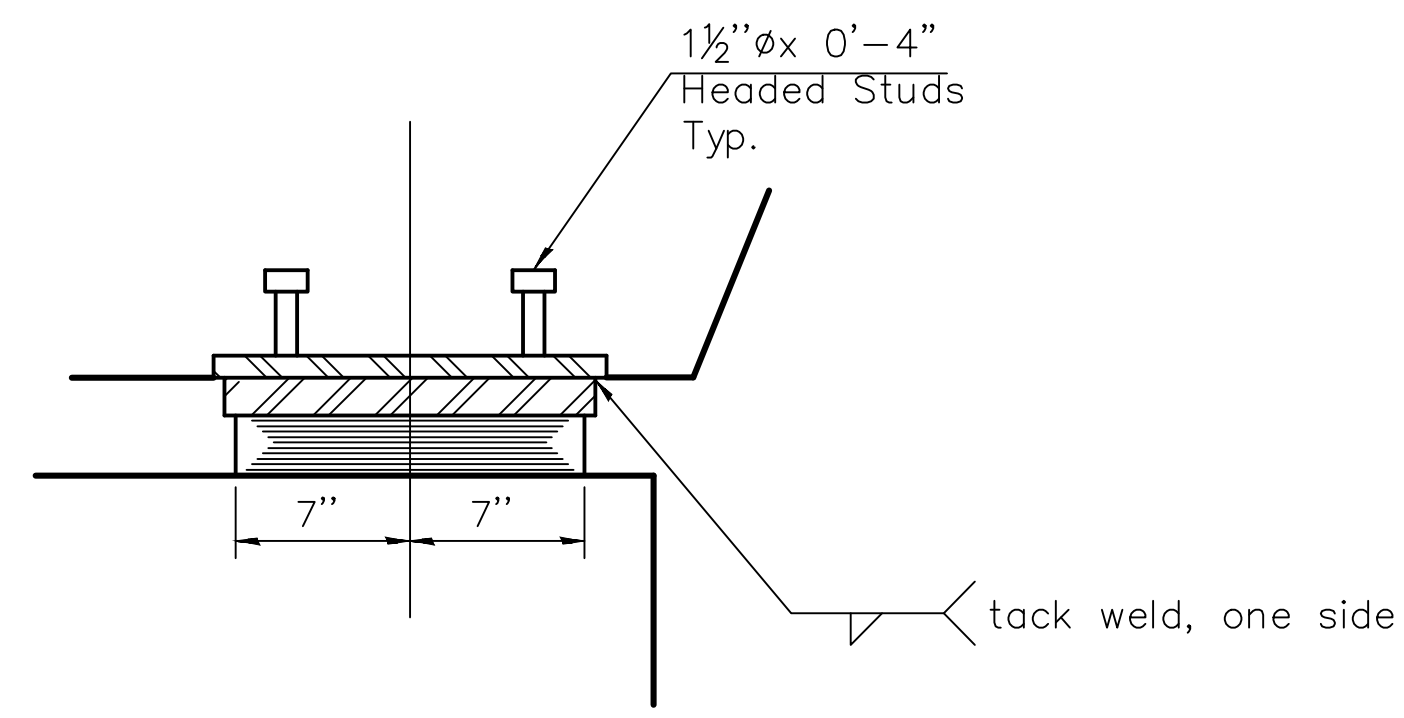
ADDISON UNDERBRIDGE TRAIL CONNECTION FROM
 CLARK PARK TO CALIFORNIA PARK
 STRUCTURE NO. 016-P025

EXPANSION JOINT DETAILS

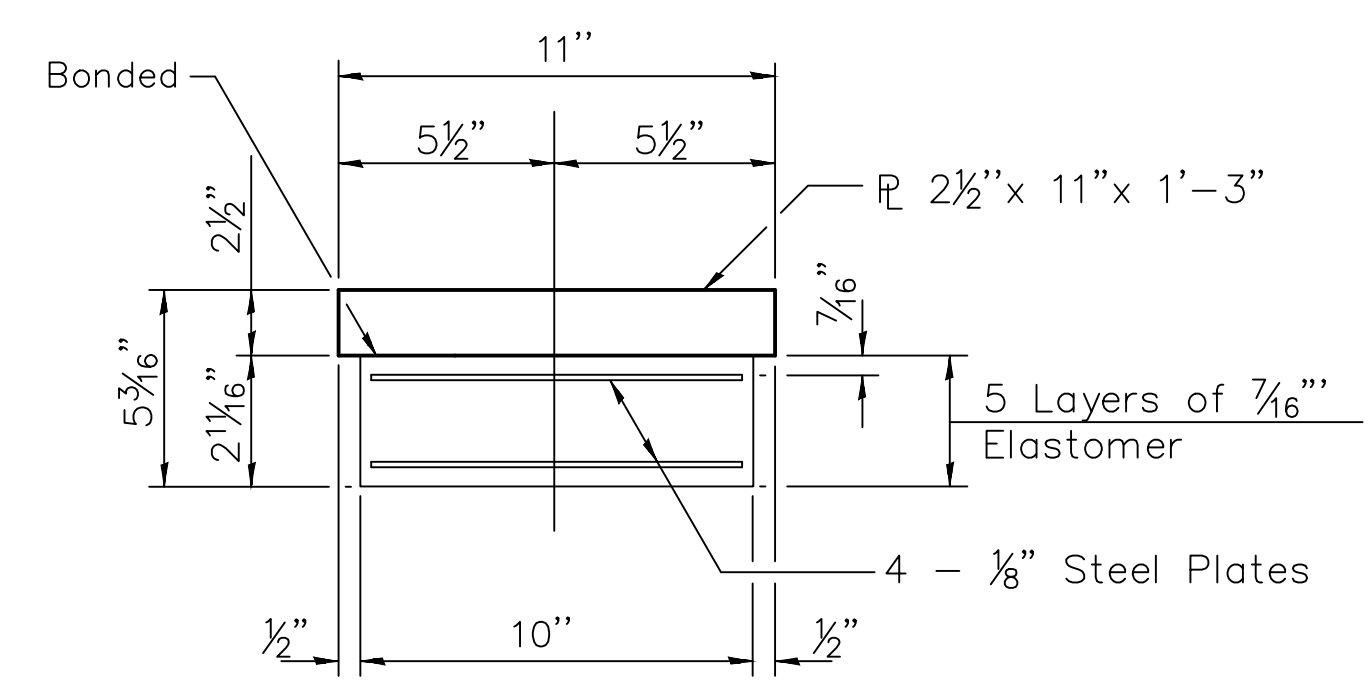
SHEET NO. S-48
 SHEET 95 OF 158



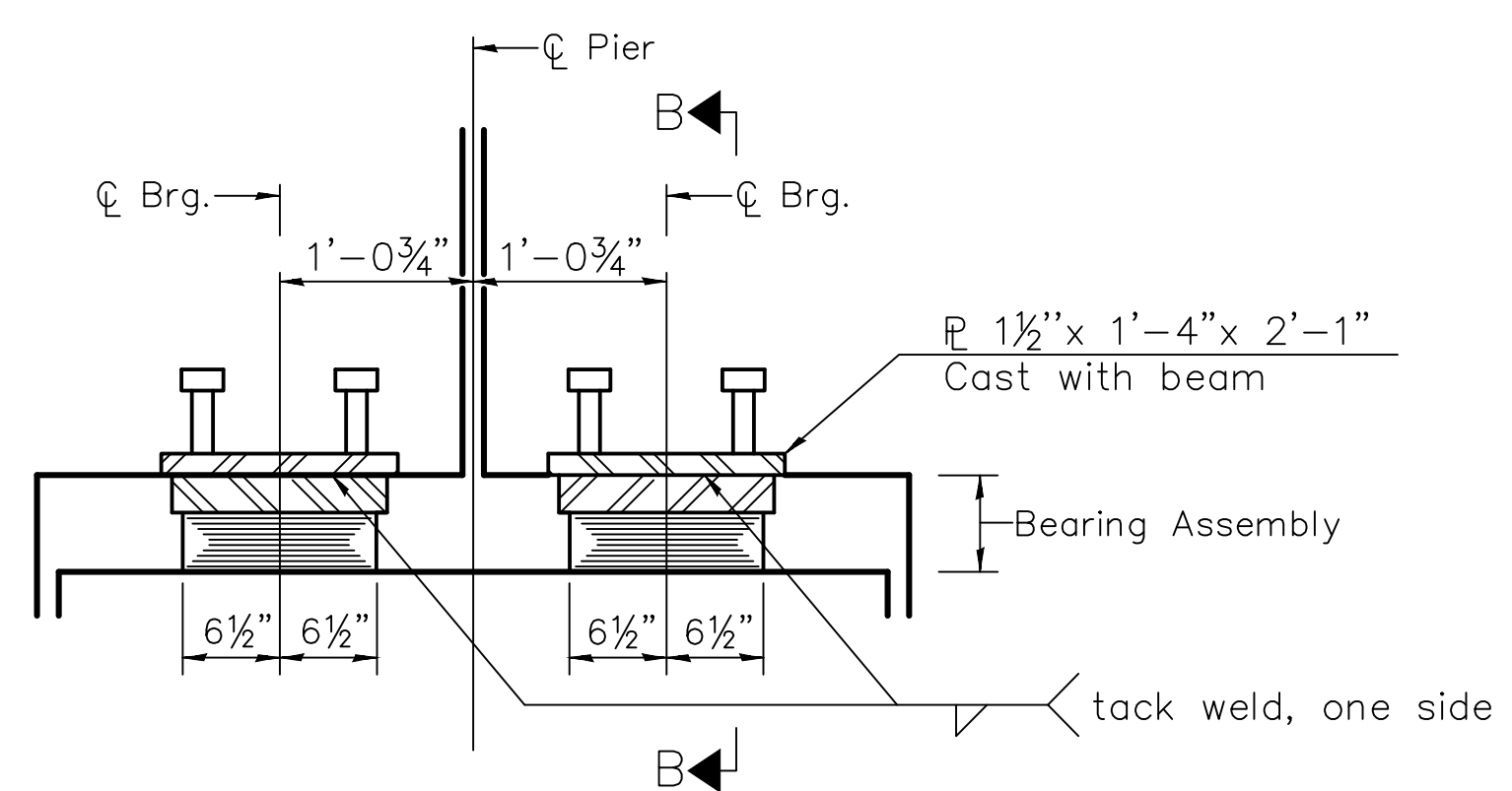
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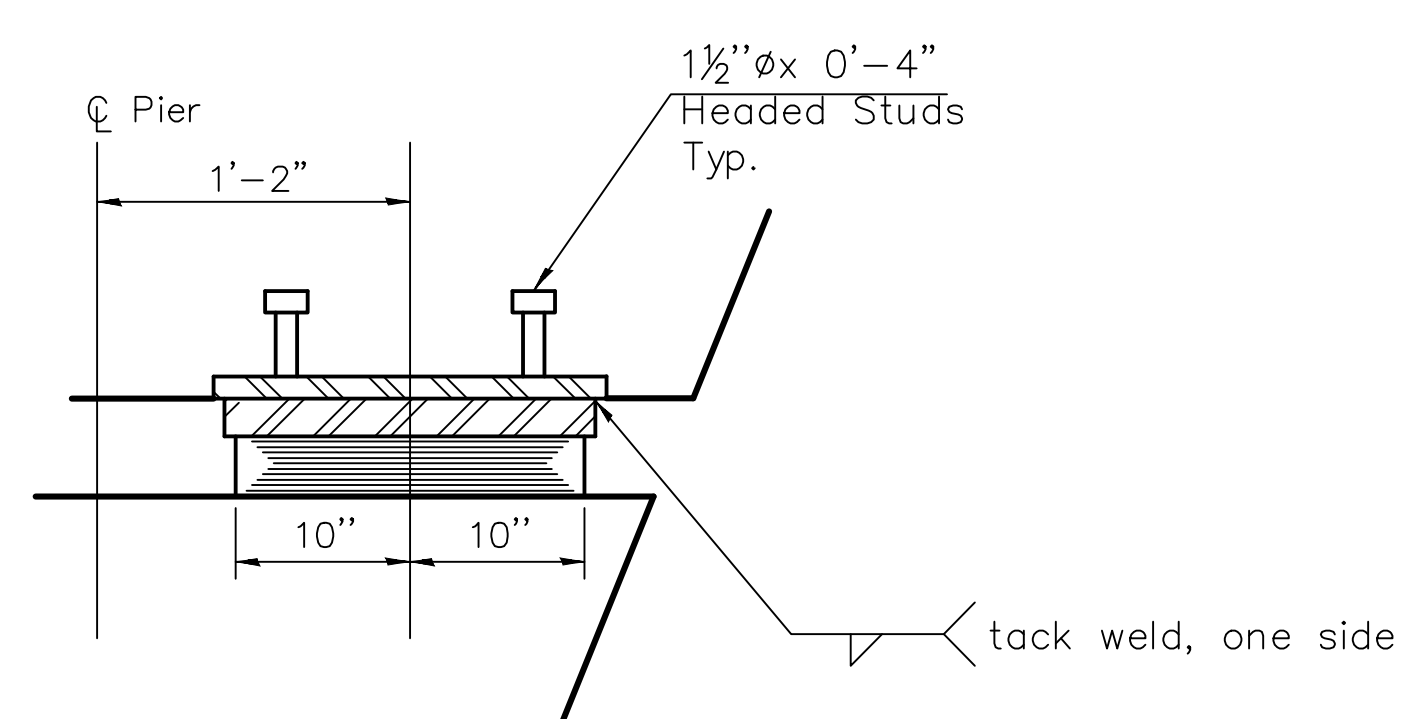
SECTION A-A



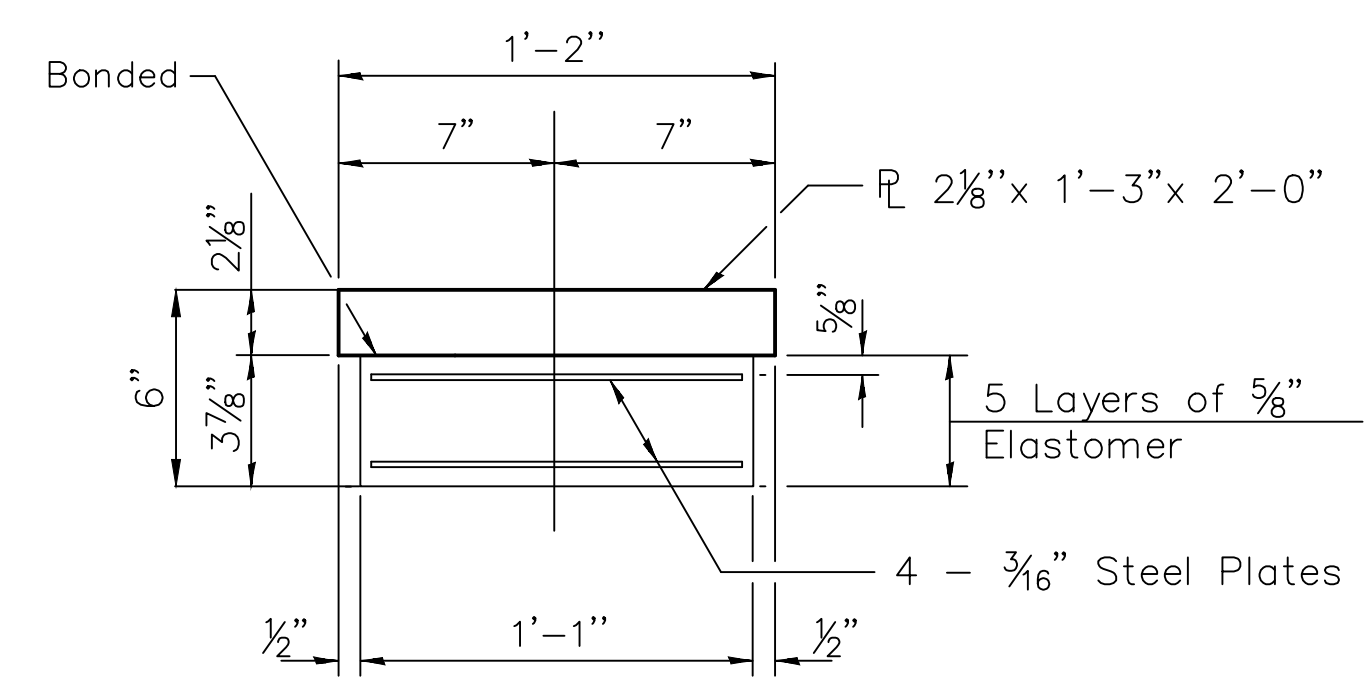
ABUTMENT BEARING ASSEMBLY
(4 Required)



SECTION AT PIER



SECTION B-B



PIER BEARING ASSEMBLY
(20 Required)

TYPE I ELASTOMERIC EXP. BRG.

UNFACTORED BEARING REACTIONS

(Typical for all bearings)
 Total Dead Load (DC+DW) = 100 kips
 Pedestrian Live Load (PL) = 40 kips

BILL OF MATERIAL

Item	Unit	Total
Elastomeric Bearing Assembly, Type I	Each	24

Notes:

- Tack weld one side of Top Plate of Bearing Assembly to plate cast with slab. Contractor is responsible to ensure that the tack welded side is accessible.
- To replace bearing:
 Install blocking at the end, break weld, remove and replace existing bearing. Do not jack beam ends.
 For bearing placement and orientation, see sheets S-59 thru S-61 of S-87.

P:\Projects\20000\03346 - CDOT - Addison Underbridge Connection - \DISCIPLINE\Civil\Working\Sheets\08 - STRUCTURAL\01\EP025-23346-049-Bearing_Details.dwg
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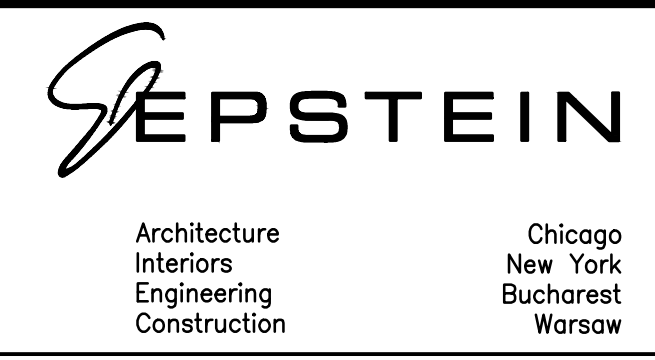
SECTION NO.: 12-E9121-00-BT
 DRAWN BY: P. DOMBROWSKI
 CHECKED BY: R. VARMA
 APPROVED BY: P. DOMBROWSKI
 DATE: 8/12/2016
 SCALE: N.T.S.

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3	6/05/15	ISSUED FOR FINAL REVIEW

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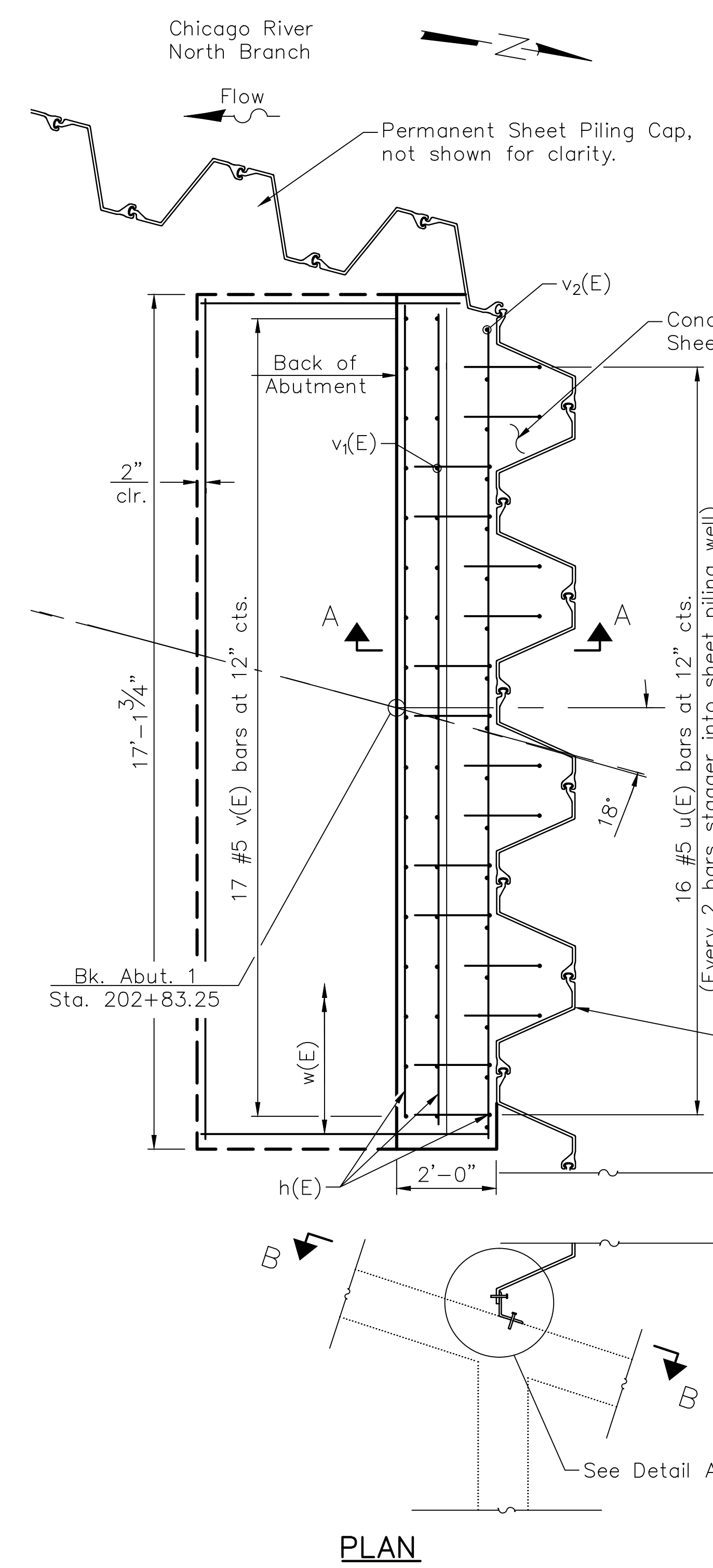
ADDISON UNDERBRIDGE TRAIL CONNECTION FROM
 CLARK PARK TO CALIFORNIA PARK
 STRUCTURE NO. 016-P025

BEARING DETAILS

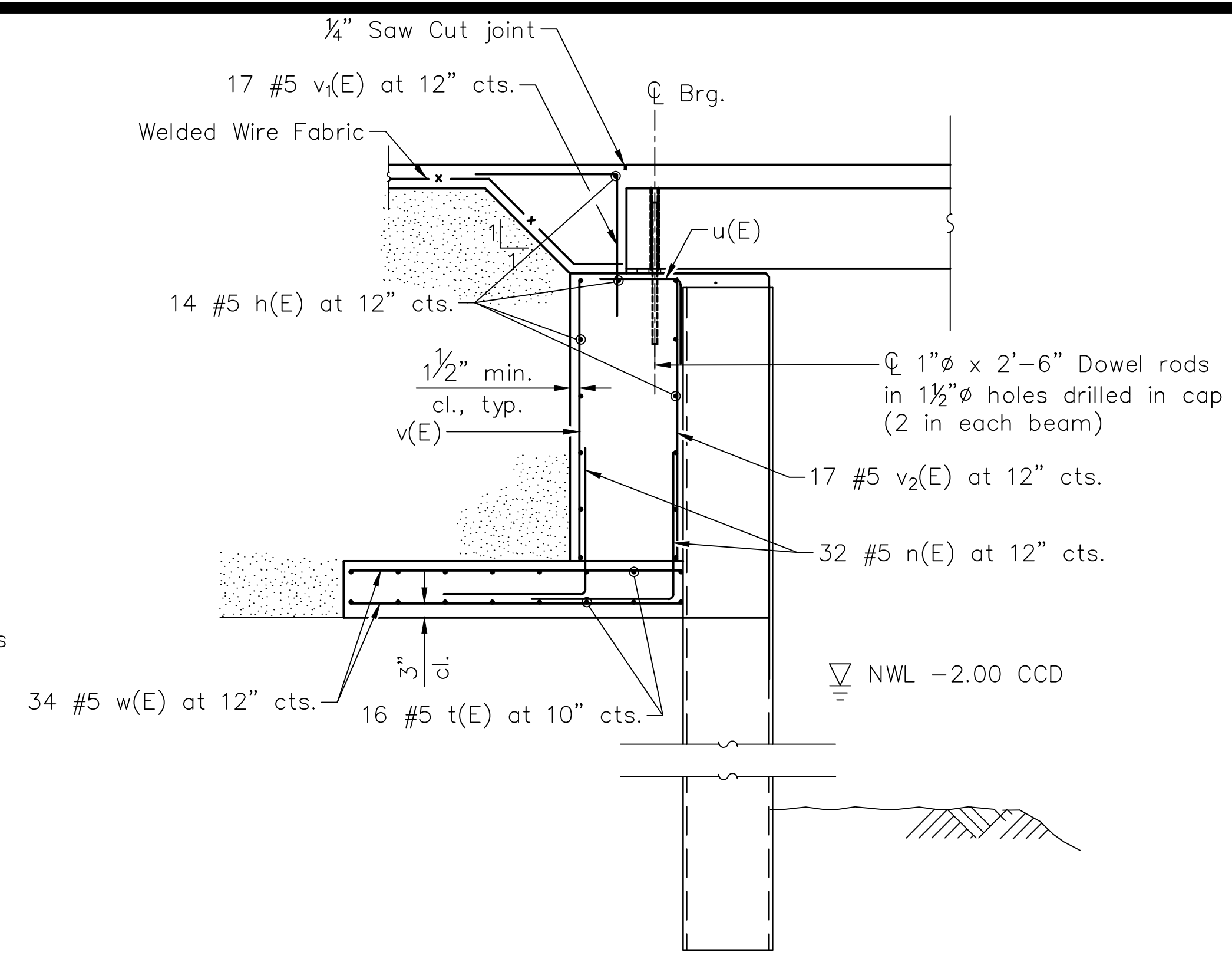
SHEET NO.
S-49

SHEET 96 OF 158

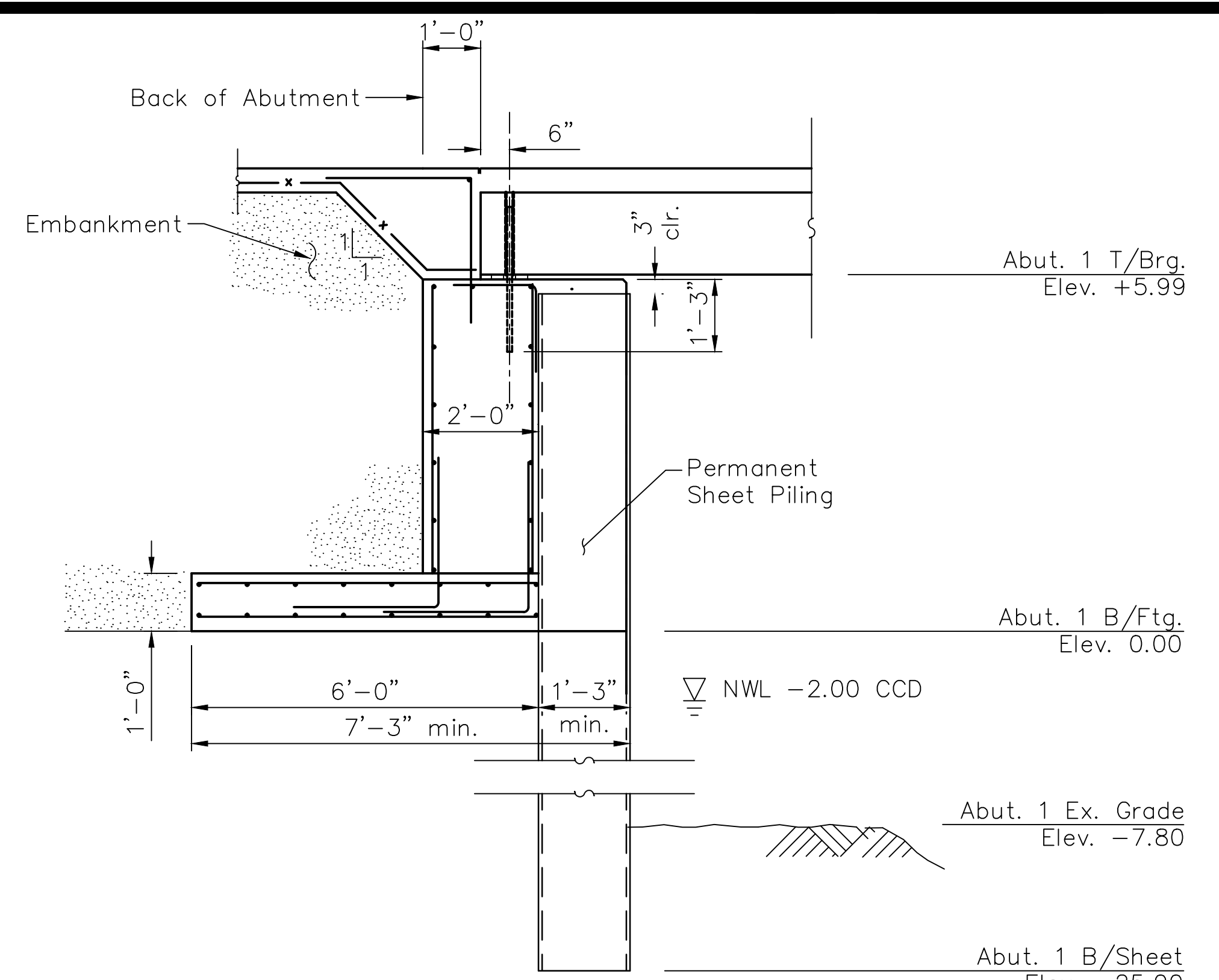
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PLAN



SECTION A-A
Abutment 1
(Showing Reinforcement)



SECTION A-A
Abutment 1
(Showing Dimensions)

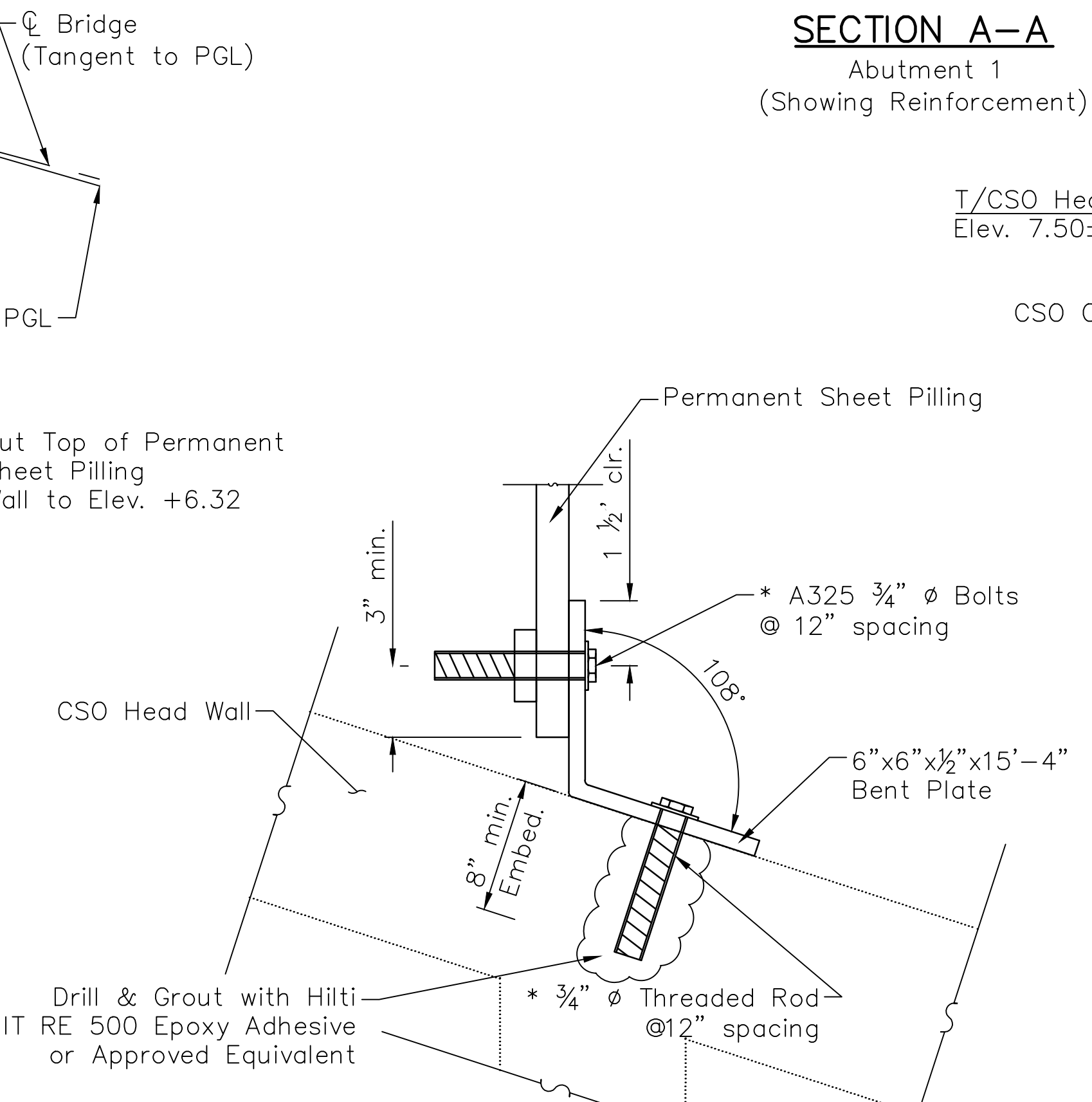
* Cost included with Pay Item: Pipe Underdrains for Structures 4"

ABUTMENT 1
BILL OF MATERIALS

Bar	No.	Size	Length	Shape
v(E)	17	#5	5'-0"	—
v ₁ (E)	17	#5	5'-0"	—
v ₂ (E)	17	#5	5'-0"	—
h(E)	14	#5	16'-10"	—
w(E)	34	#5	5'-10"	—
t(E)	16	#5	15'-8"	—
u(E)	16	#5	1'-6"	└
n(E)	34	#5	2'-6"	└
High Strength High Performance Concrete Structures			Cu Yd	19.7
Reinforcement Bars, Epoxy Coated			Pound	1,100

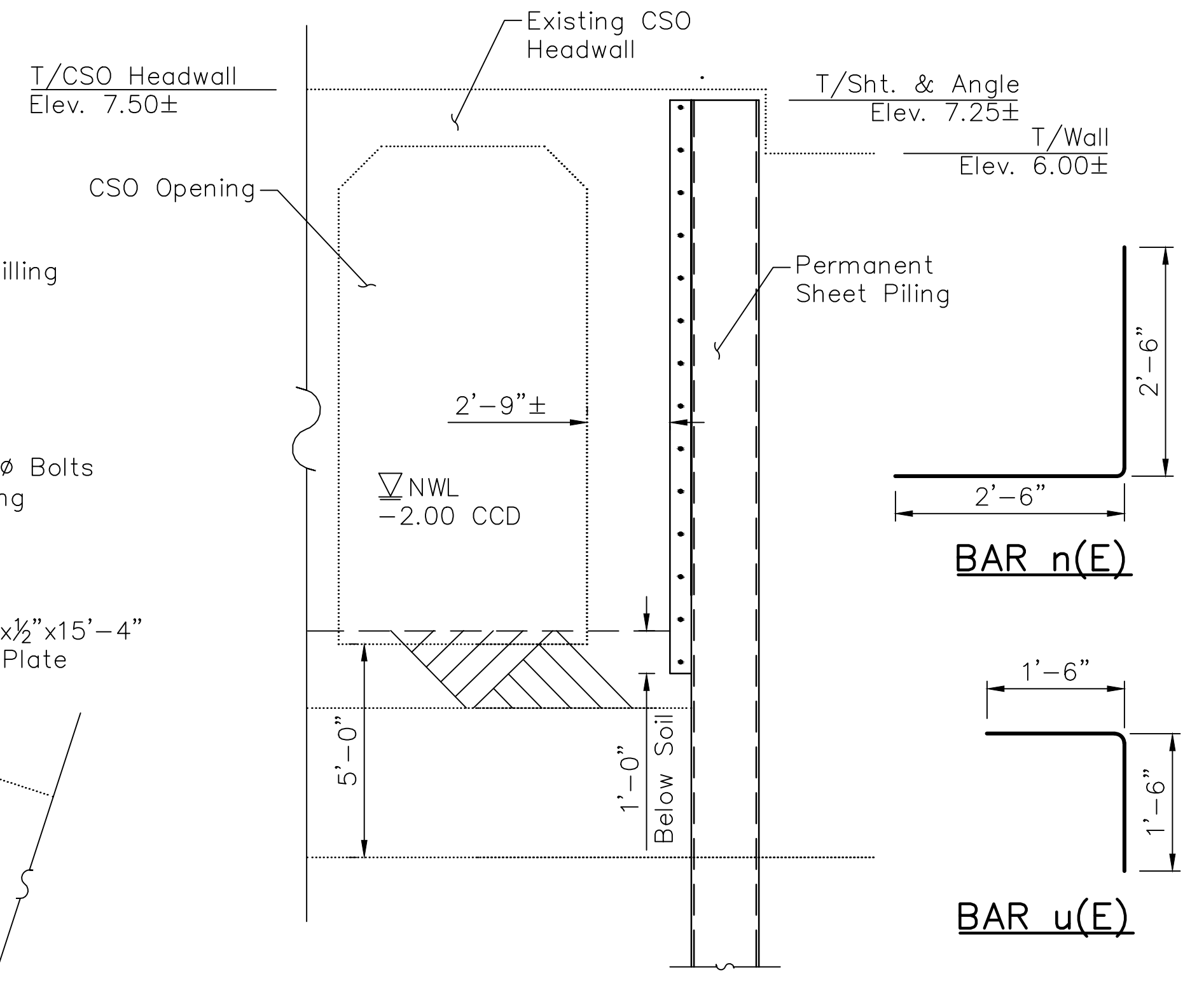
Maximum Footing Pressure = 3.25 ksf

Note:
1. For Offset sketch, see sheet S-09 of S-87.



DETAIL A

* Cost included with Permanent Sheet Piling



SECTION B-B

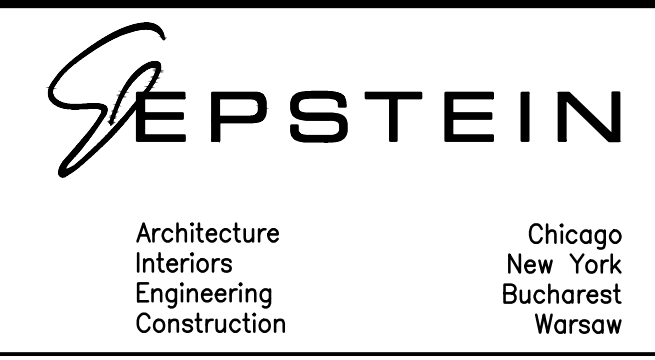
SECTION NO.: 12-E9121-00-BT
 DRAWN BY: J. MASI
 CHECKED BY: R. VARMA
 APPROVED BY: P. DOMBROWSKI
 DATE: 8/12/2016
 SCALE: N.T.S.

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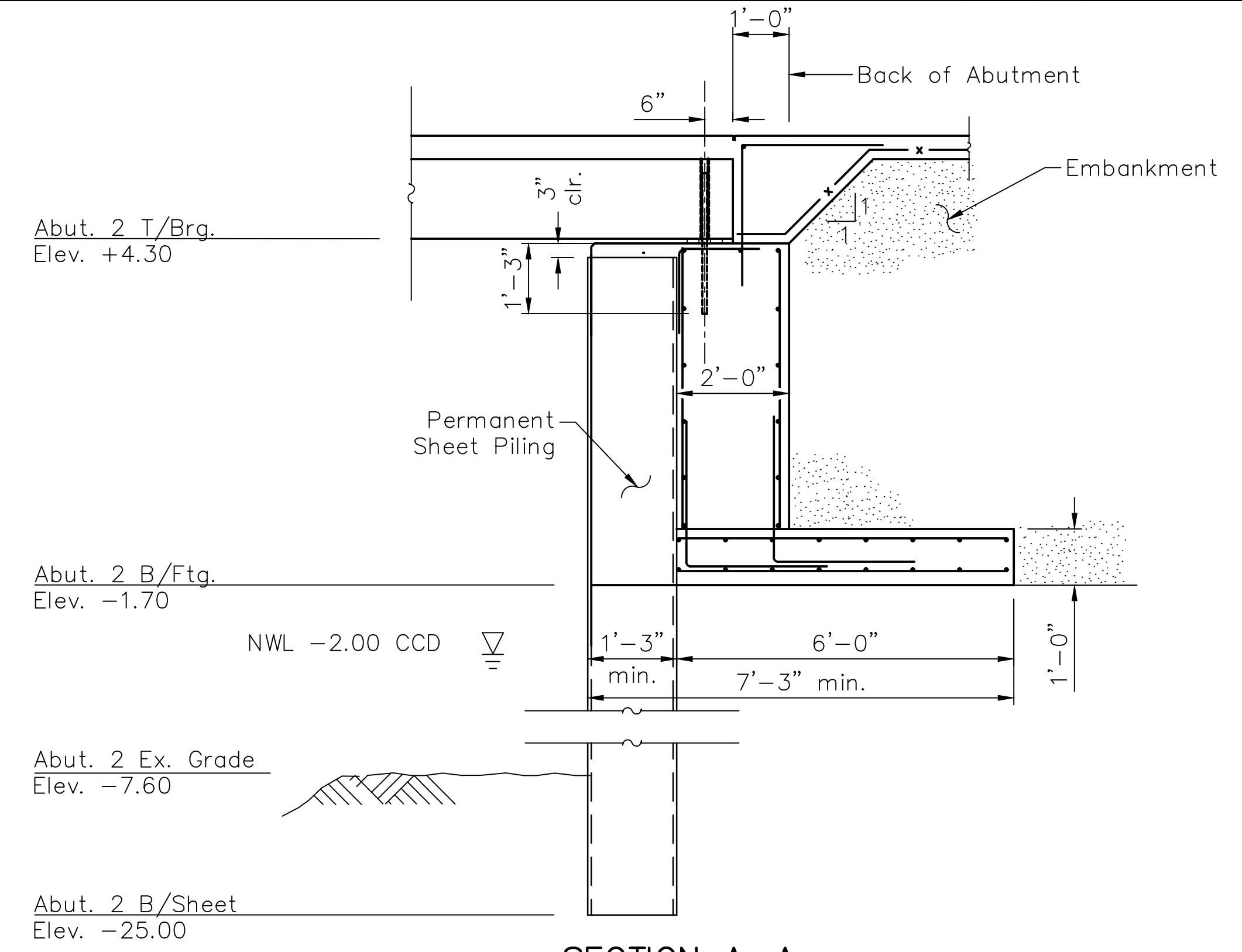
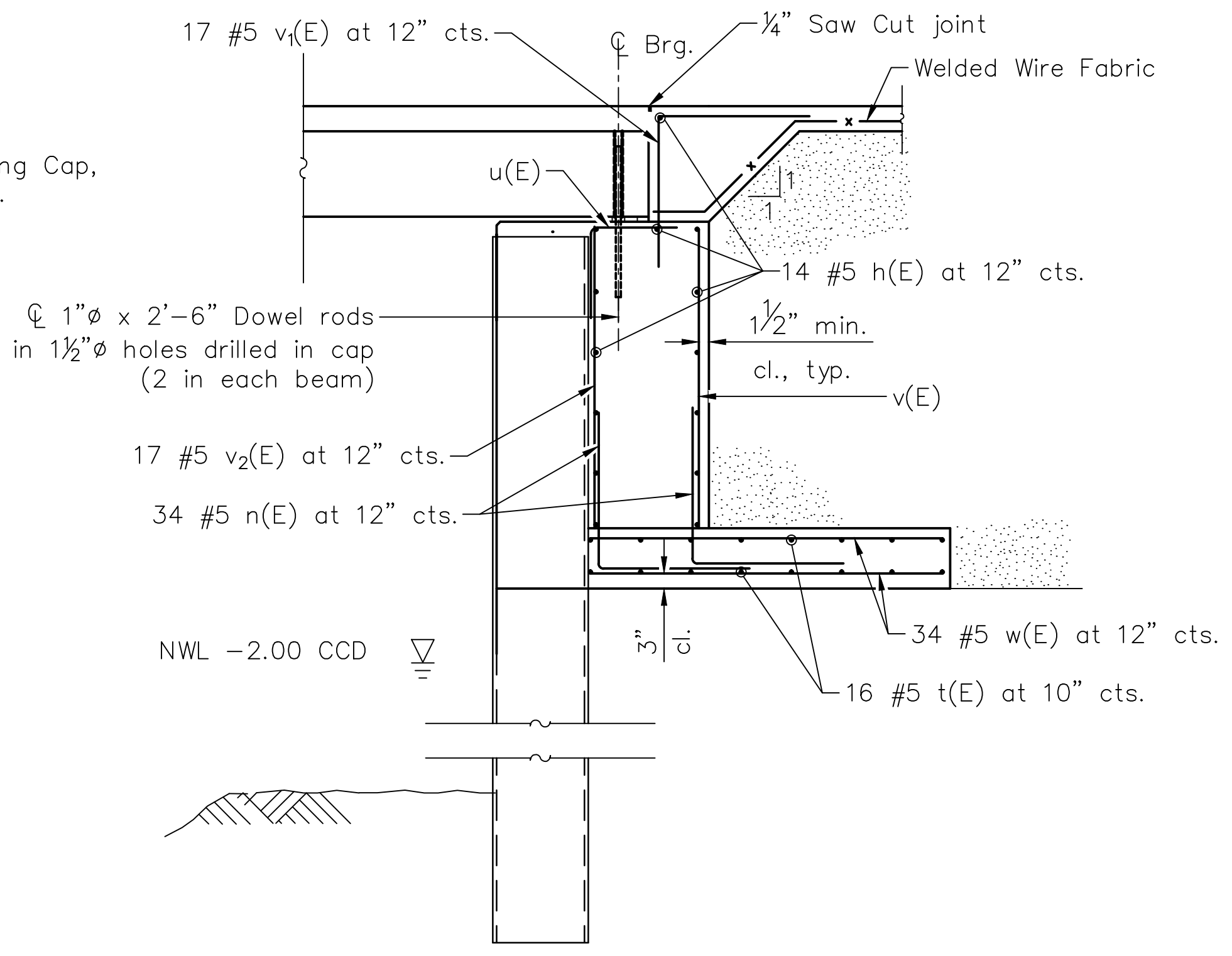
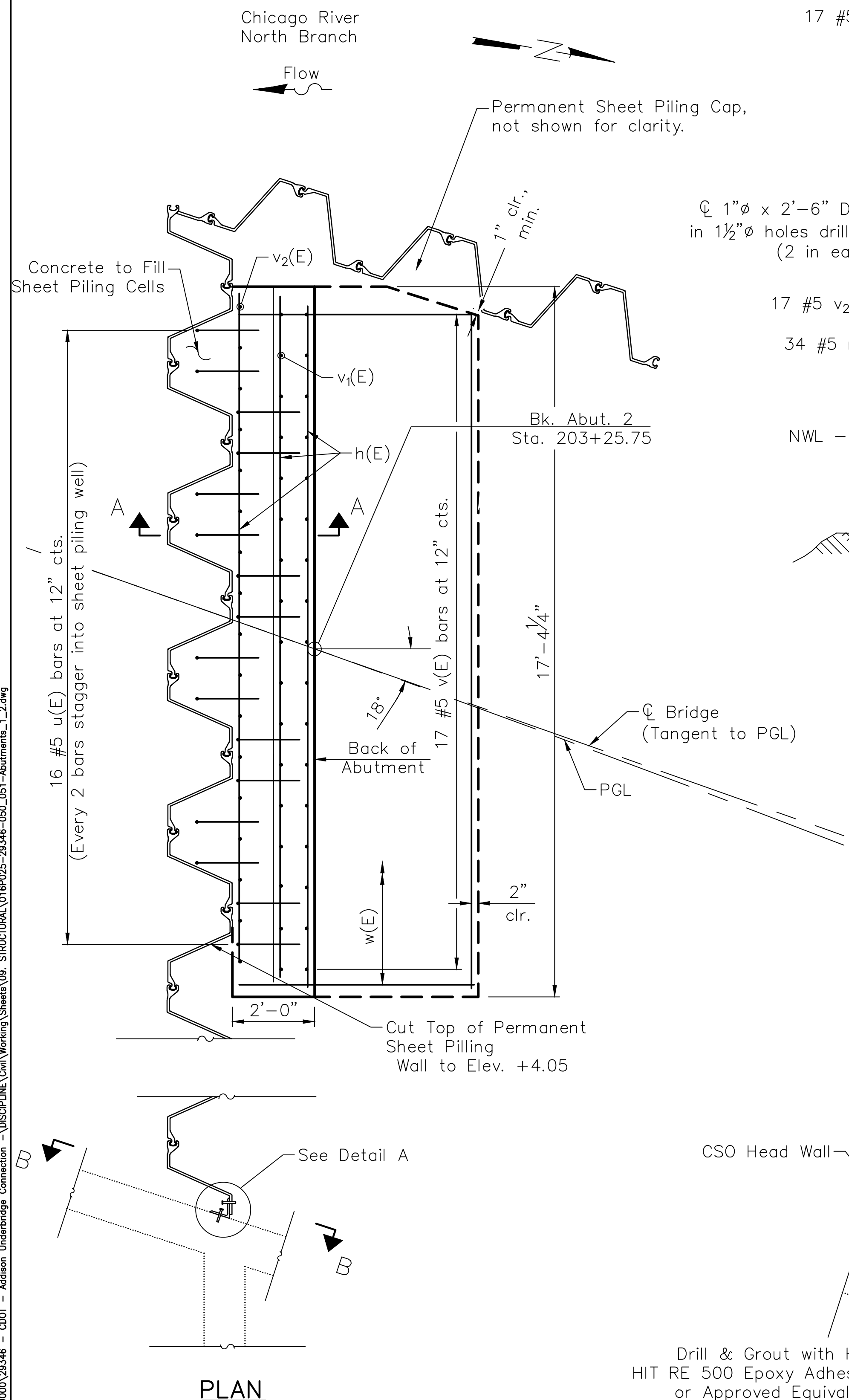


ADDISON UNDERBRIDGE TRAIL CONNECTION FROM CLARK PARK TO CALIFORNIA PARK VALUE
 SUBSTRUCTURE - ABUTMENT 1

SHEET NO. S-50
 SHEET 97 OF 158

REVISION

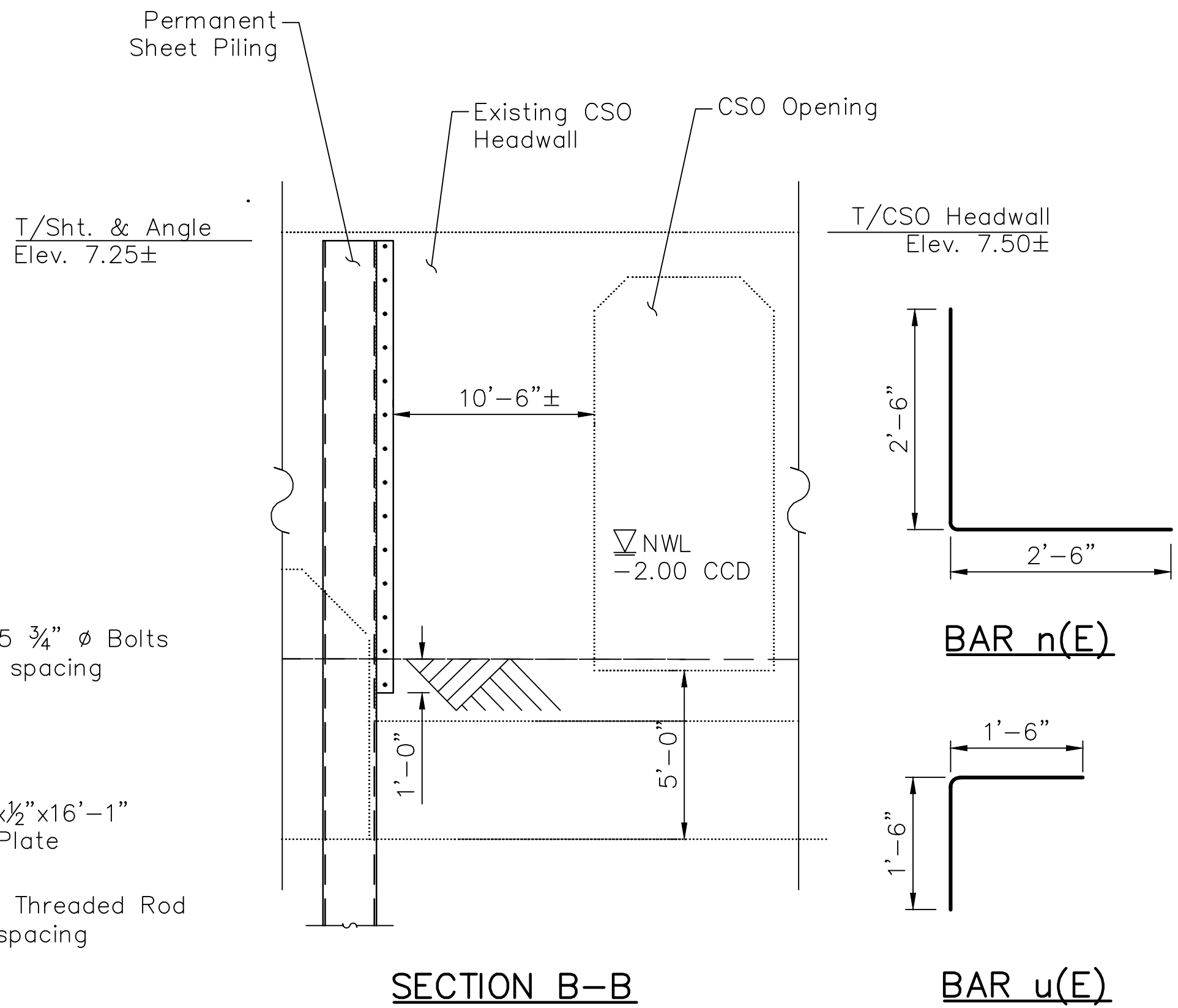
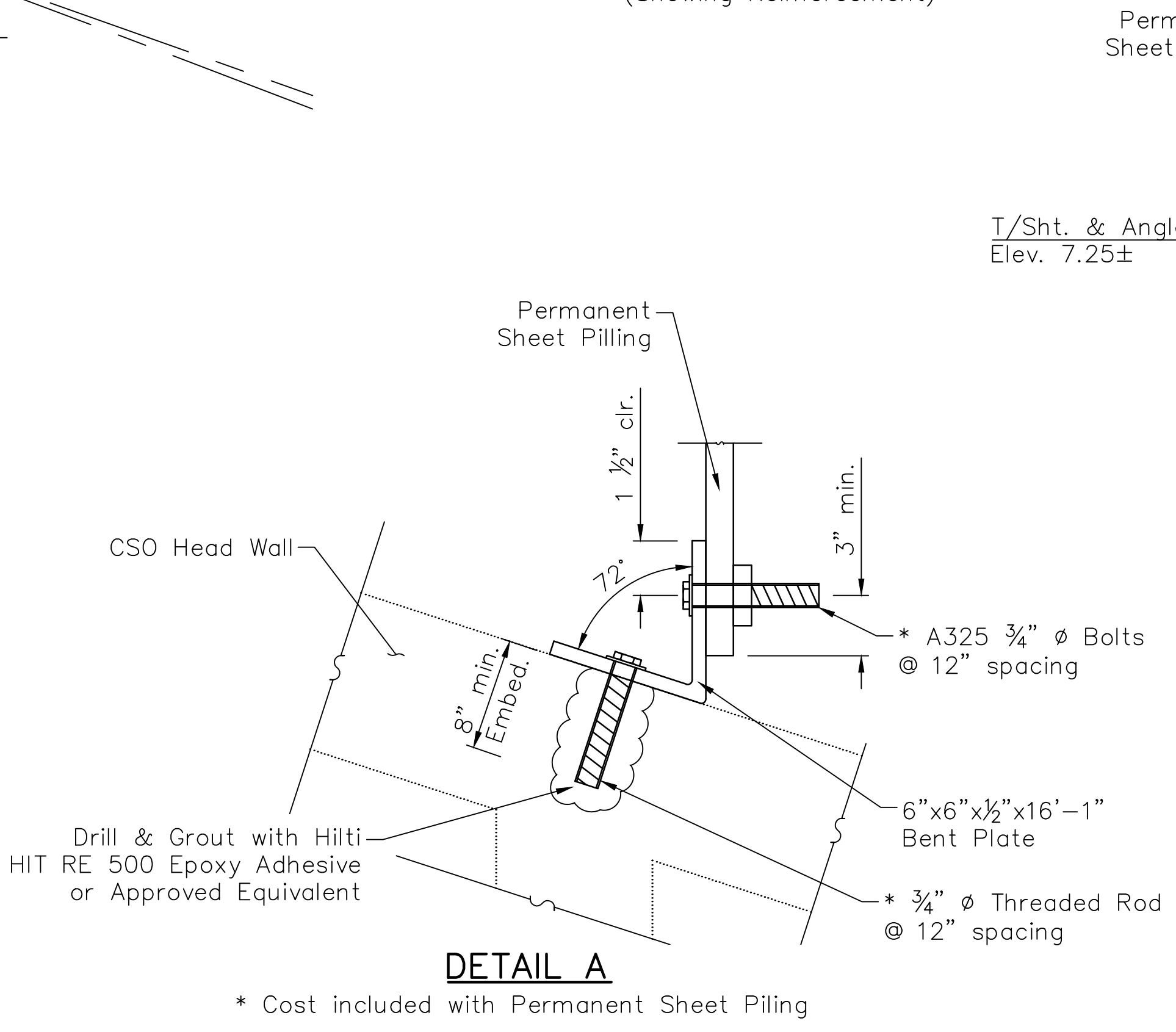
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**ABUTMENT 2
BILL OF MATERIALS**

Bar	No.	Size	Length	Shape	
v(E)	17	#5	6'-10"	—	
v ₁ (E)	17	#5	5'-0"	—	
v ₂ (E)	17	#5	5'-0"	—	
h(E)	14	#5	17'-2 1/4"	—	
w(E)	34	#5	5'-10"	—	
t(E)	16	#5	15'-8"	—	
u(E)	16	#5	1'-6"	└	
n(E)	34	#5	2'-6"	└	
High Strength High Performance Concrete Structures				Cu Yd	19.7
Reinforcement Bars, Epoxy Coated				Pound	1,100

Maximum Footing Pressure = 3.25 ksf



BAR n(E)

BAR u(E)

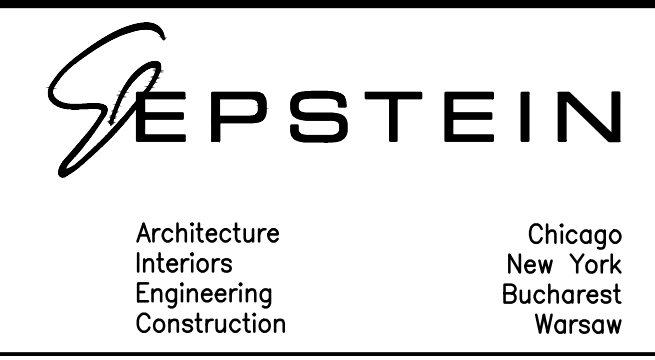
Note:
1. For Offset sketch, see sheet S-09 of S-87.

SECTION NO.: 12-E9121-00-BT	NO. DATE	REVISIONS/ISSUANCES
DRAWN BY: J. MASI	8/12/16	ISSUED FOR FINAL
CHECKED BY: R. VARMA	5/6/16	ISSUED FOR FINAL
APPROVED BY: P. DOMBROWSKI	6/05/15	ISSUED FOR FINAL REVIEW
DATE: 8/12/2016		
SCALE: N.T.S.		

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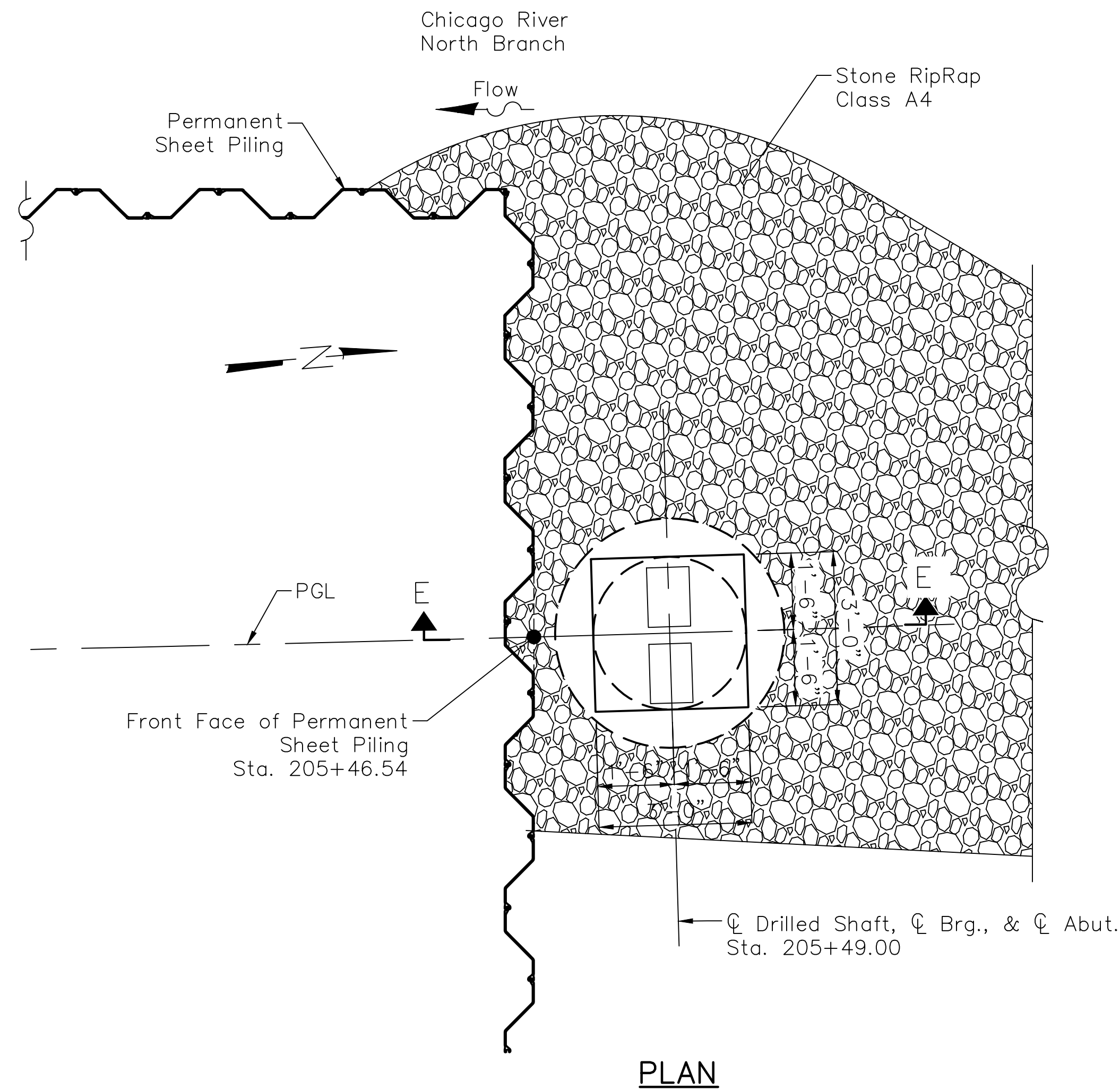


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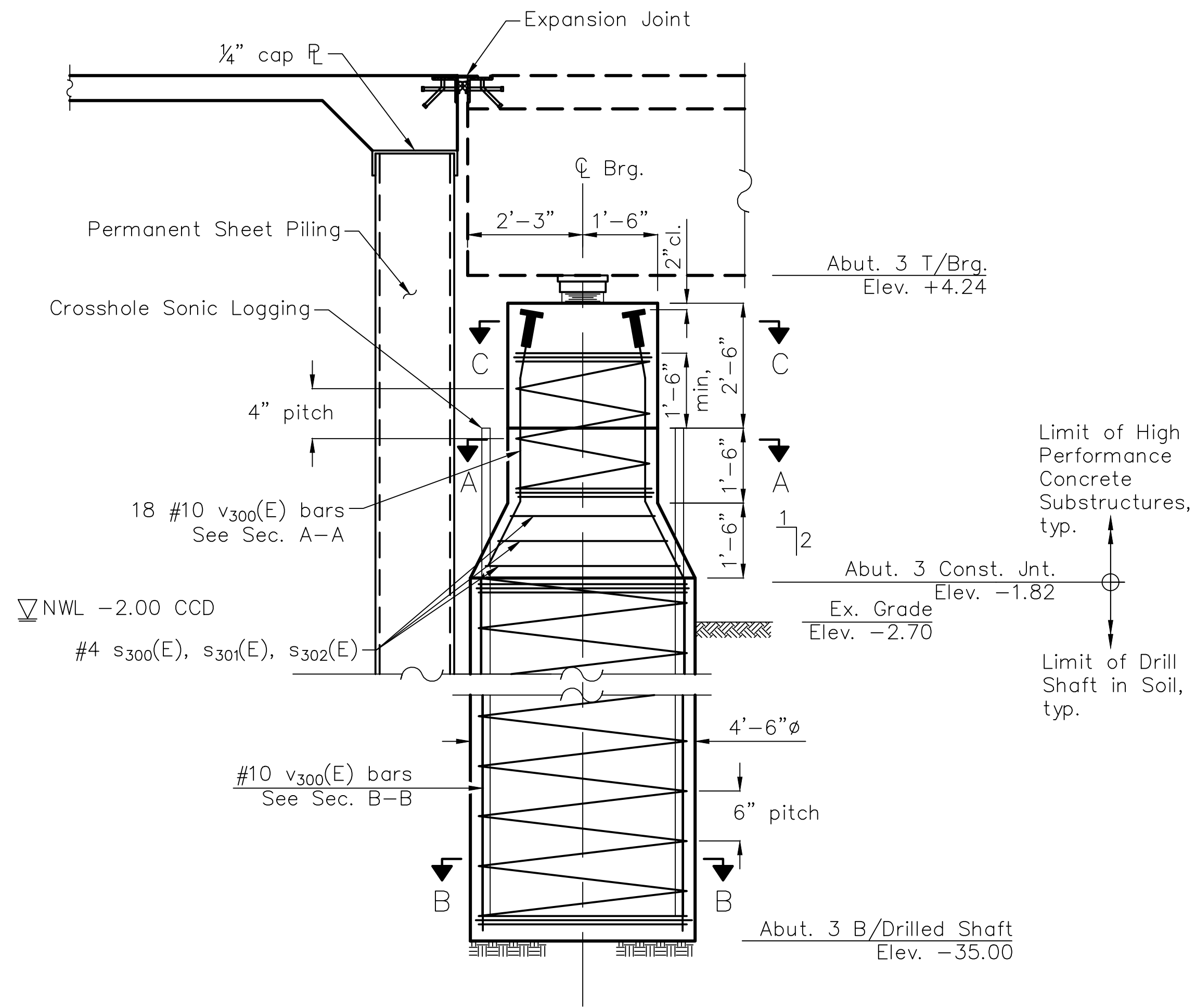


ADDISON UNDERBRIDGE TRAIL CONNECTION FROM CLARK PARK TO CALIFORNIA PARK VALUE
SUBSTRUCTURE - ABUTMENT 2

SHEET NO. S-51	REVISION
SHEET 98 OF 158	



PLAN



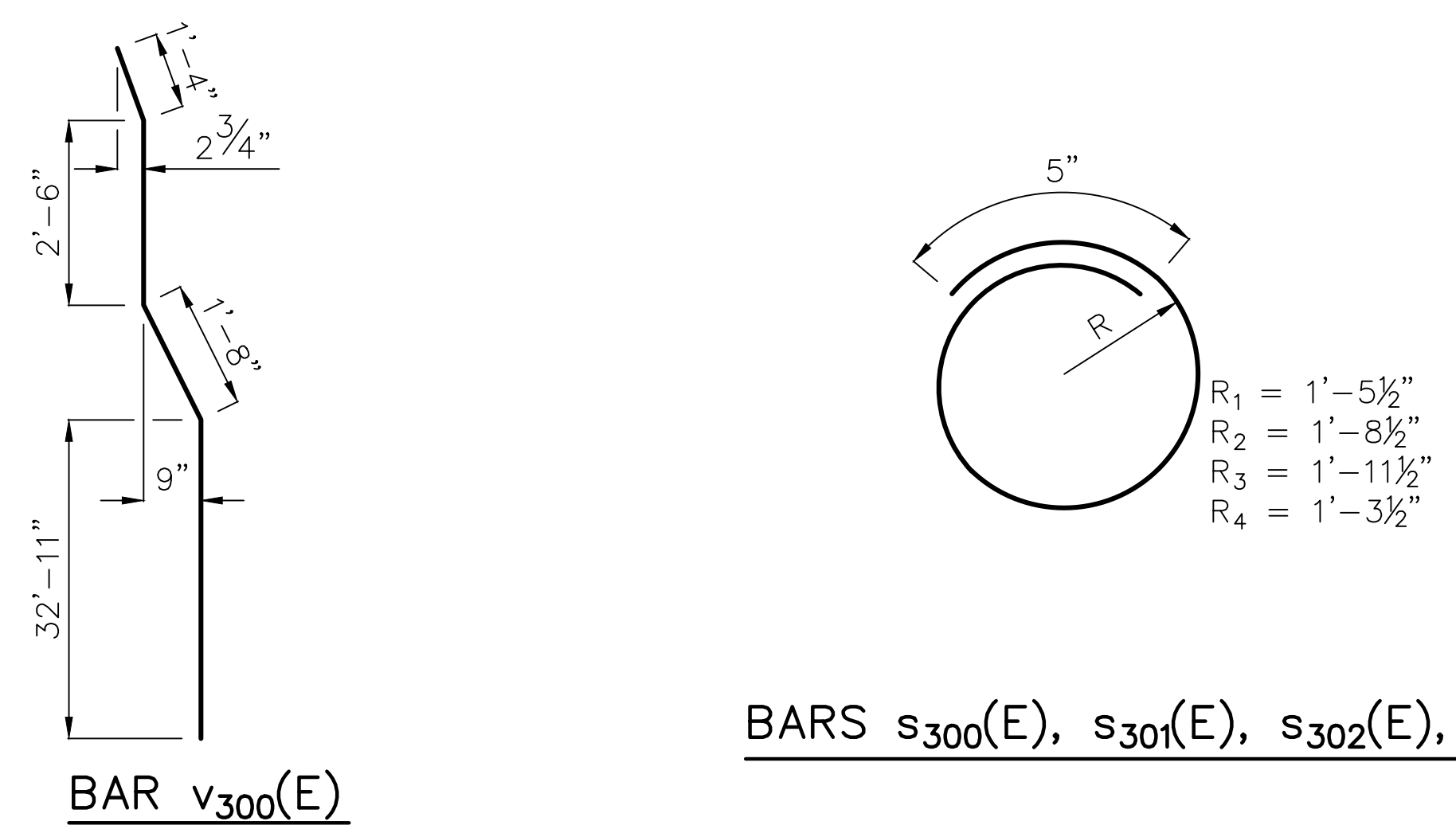
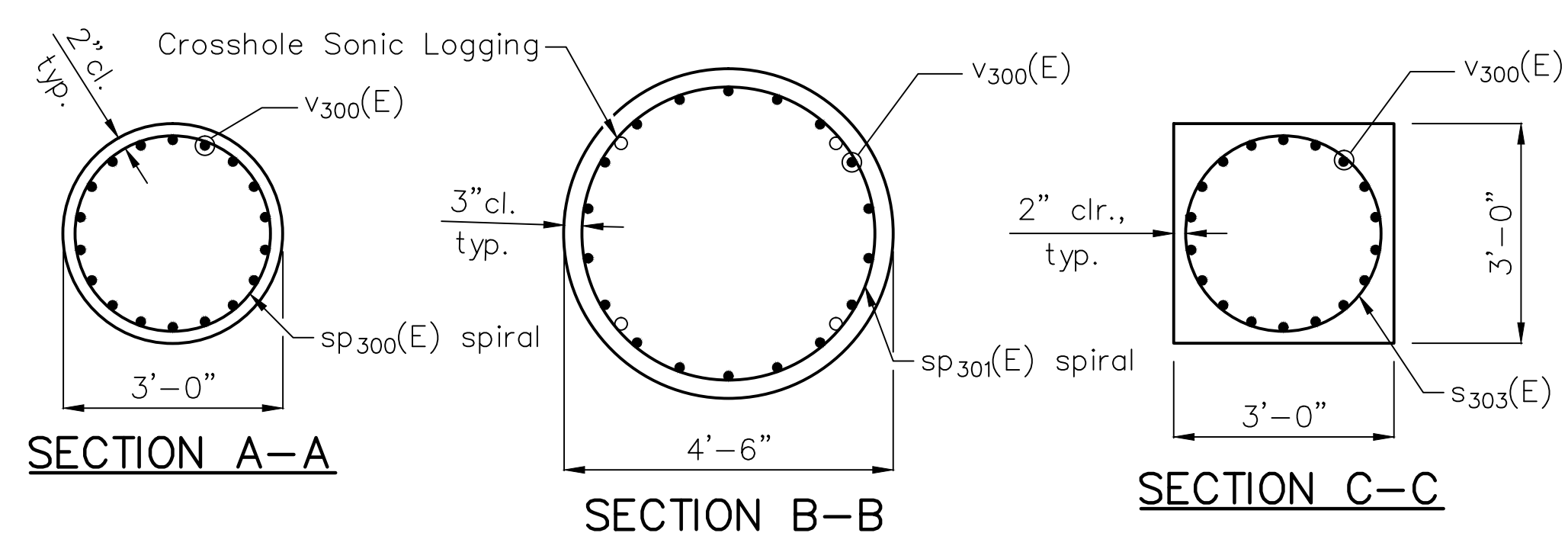
SECTION E-E

ABUTMENT 3 BILL OF MATERIALS

Bar	No.	Size	Length	Shape
v300(E)	18	#10	38'-5"	—
s300(E)	1	#4	9'-7"	○
s301(E)	1	#4	11'-2"	○
s302(E)	1	#4	12'-9"	○
* sP300(E)	1	#4	3'-0"	⋈
* sP30(E)	1	#4	37'-0"	⋈
Reinforcement Bars, Epoxy Coated		Pound	3,680	
High Strength High Performance Concrete Structures		Cu Yd	2.9	
Permanent Casing		Foot	34	
Drilled Shaft in Soil		Cu Yd	22	

* Length is height of spiral.

Maximum Bearing Pressure = 12.6 ksf



- Notes:
- For permanent steel sheet piling, see sheets S-62 thru S-66 of S-87.
 - For expansion joint, see sheet S-48 of S-87.
 - When splicing of spiral reinforcement is necessary, the spirals shall be provided with 1 1/2 extra turns at the ends to be spliced. These additional turns shall either be welded together according to AWS D1.4, or shall both terminate with a 135° standard hook.
 - For Crosshole Sonic Logging details, see sheet S-61 of S-87.

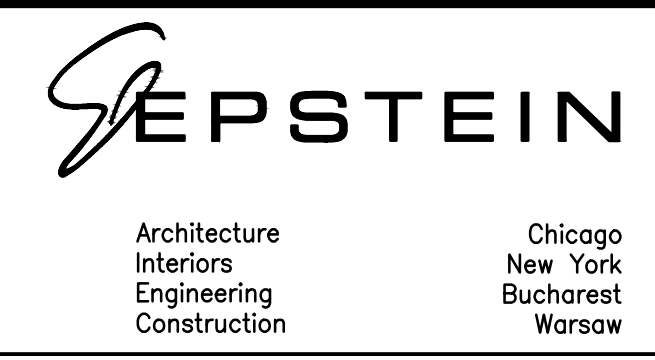
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 DRAWN BY: J. MASI
 CHECKED BY: R. VARMA
 APPROVED BY: P. DOMBROWSKI
 DATE: 8/12/2016
 SCALE: N.T.S.

NO.	DATE	REVISIONS/ISSUANCES
1	8/12/16	ISSUED FOR FINAL
2	5/6/16	ISSUED FOR FINAL
3	6/05/15	ISSUED FOR FINAL REVIEW

CITY OF CHICAGO
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 312.744.3600



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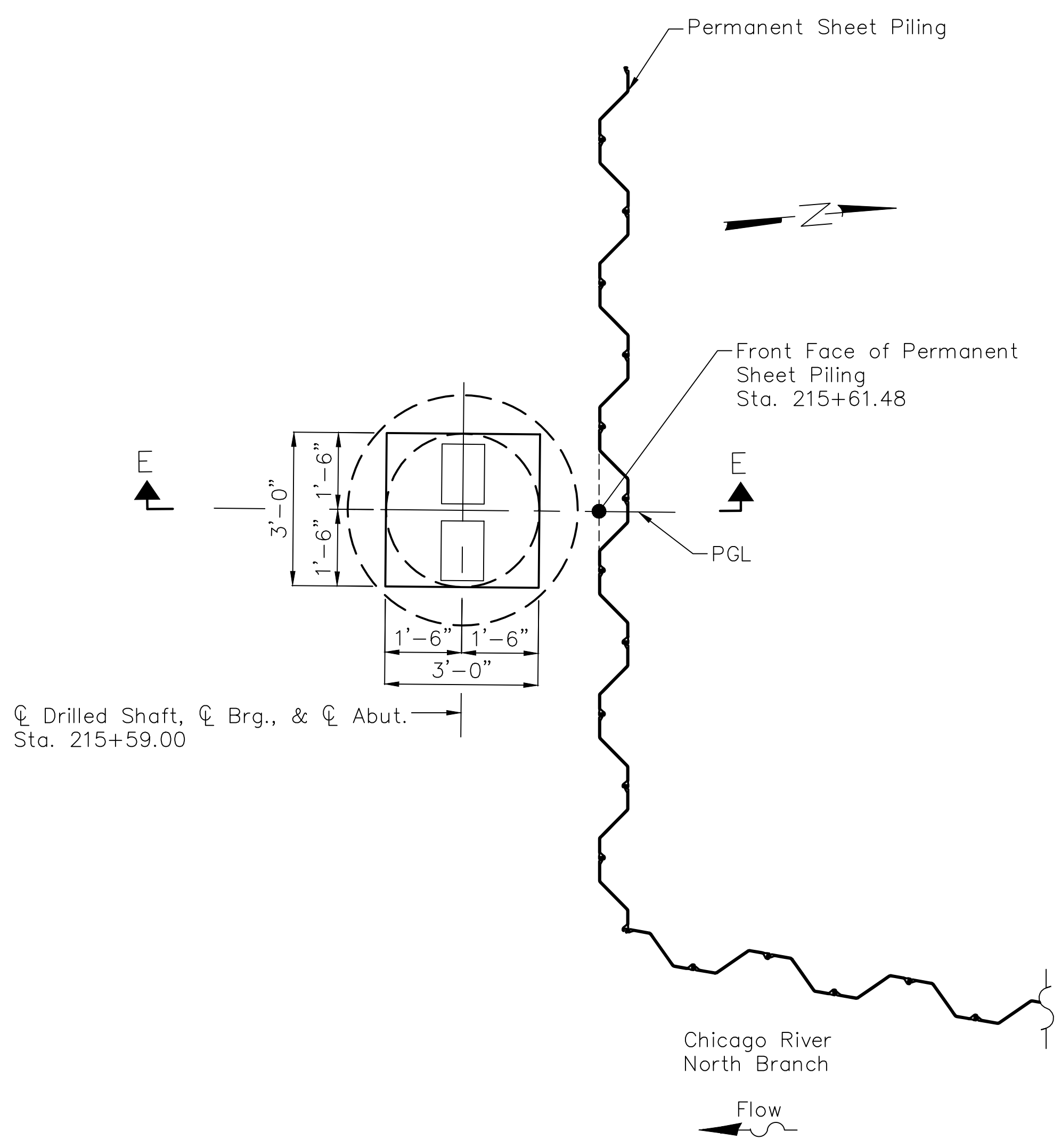


ADDISON UNDERBRIDGE TRAIL CONNECTION FROM
 CLARK PARK TO CALIFORNIA PARK
 STRUCTURE NO. 016-P025

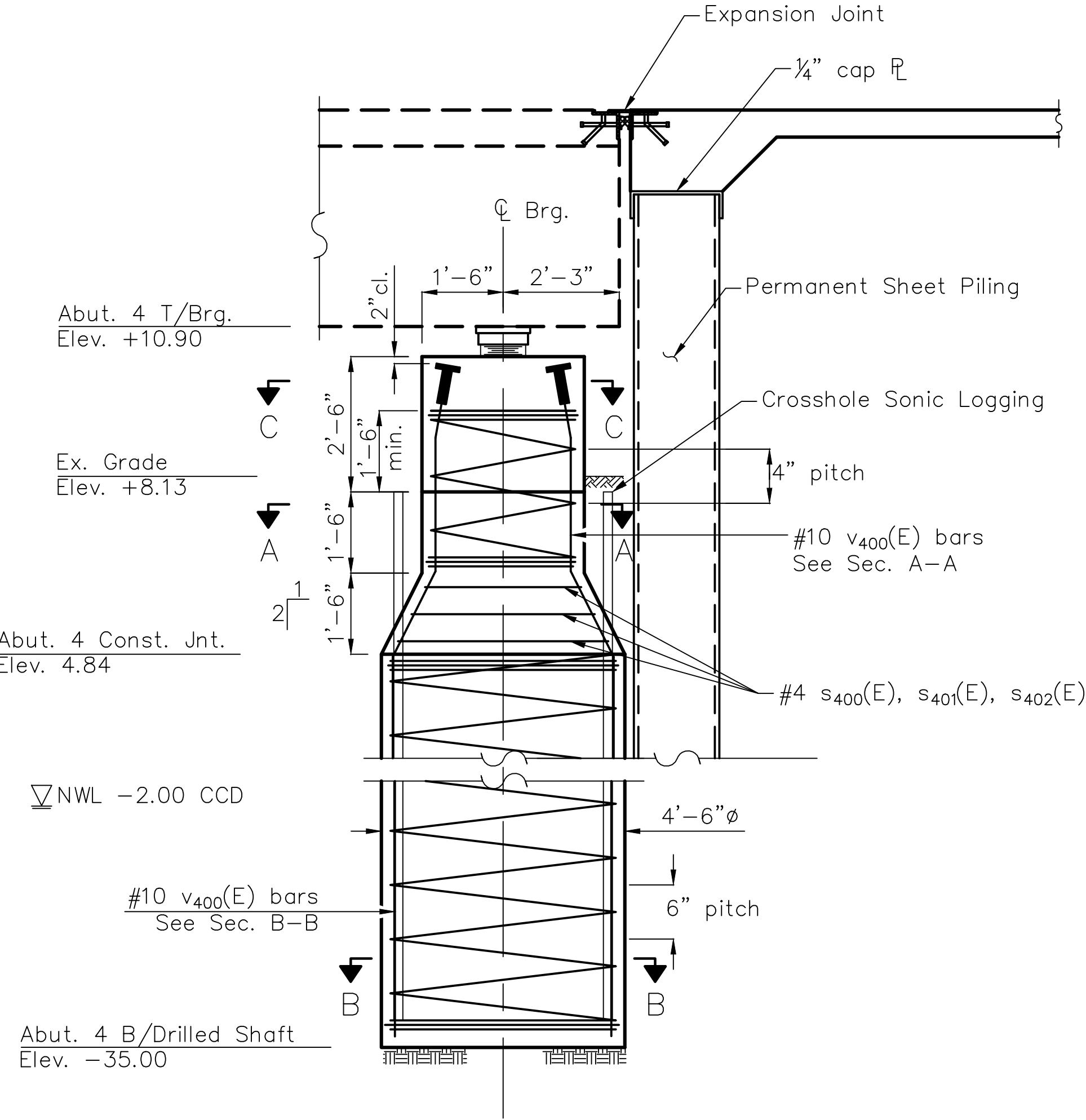
SUBSTRUCTURE - ABUTMENT 3

SHEET NO.
S-52
 SHEET 99 OF 158

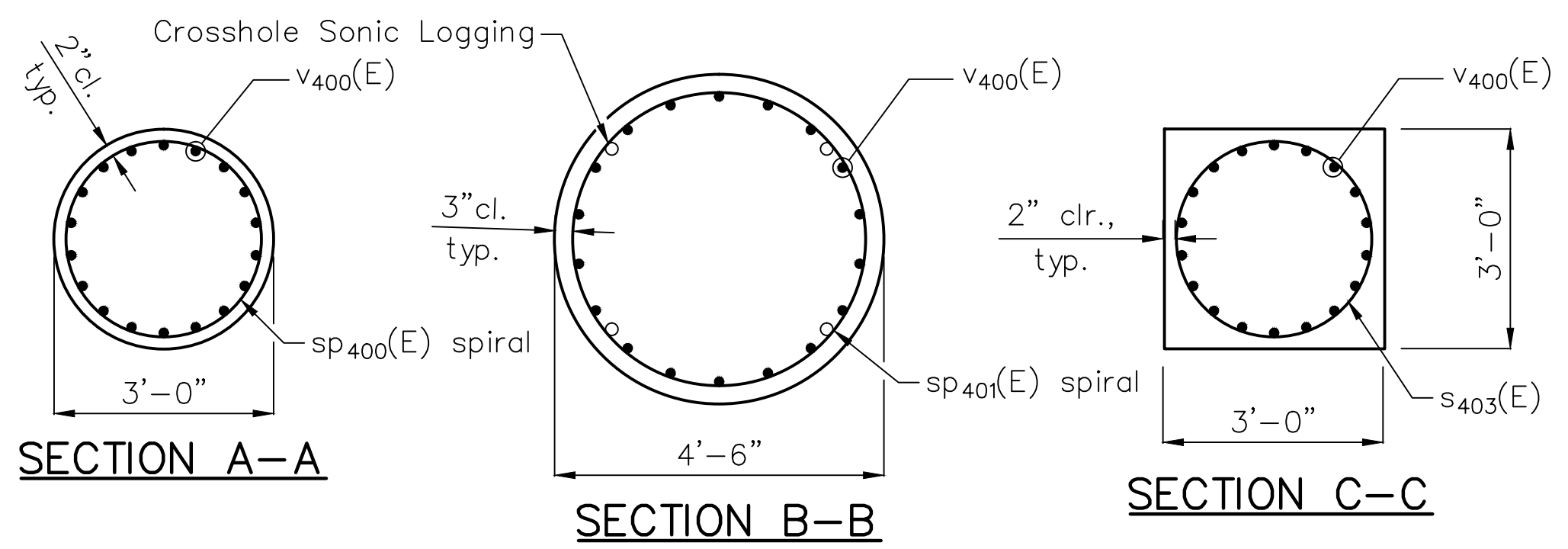
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PLAN



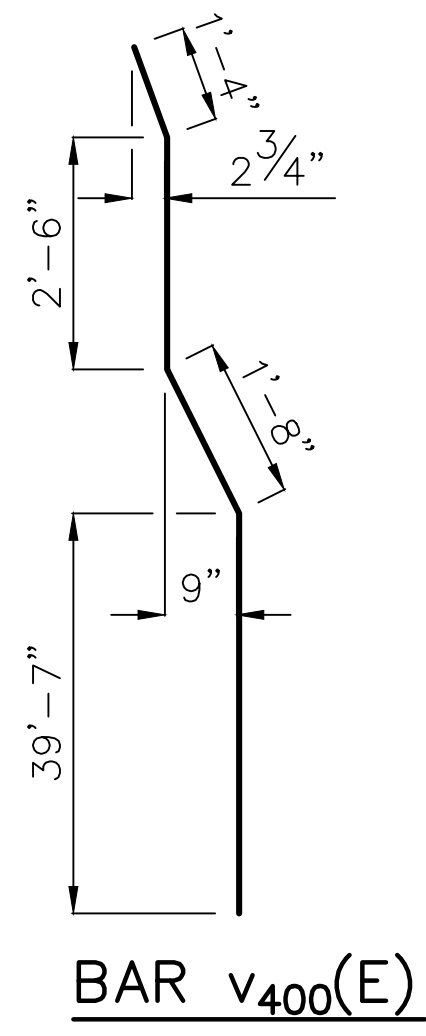
SECTION E-E



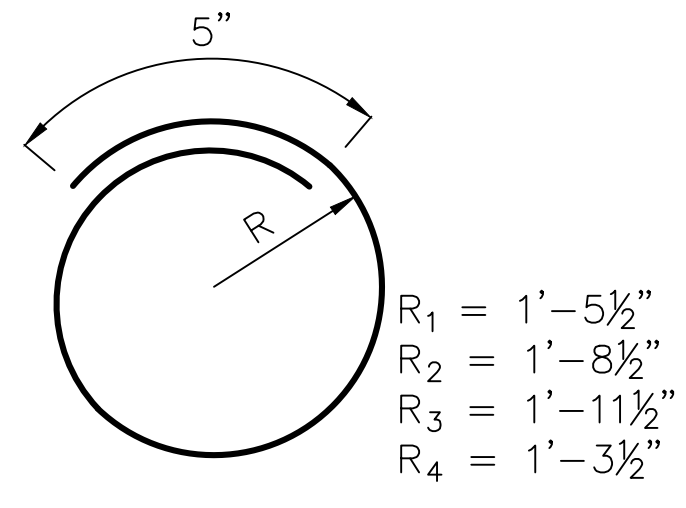
SECTION A-A

SECTION B-B

SECTION C-C



BAR v400(E)



BARS s400(E), s401(E), s402(E), s403(E)

**ABUTMENT 4
BILL OF MATERIALS**

Bar	No.	Size	Length	Shape
v400(E)	18	#10	45'-1"	—
s400(E)	1	#4	9'-7"	○
s401(E)	1	#4	11'-2"	○
s402(E)	1	#4	12'-9"	○
* SP400(E)	1	#4	3'-0"	⋈
* SP401(E)	1	#4	40'-0"	⋈
Reinforcement Bars, Epoxy Coated		Pound	4,310	
High Strength High Performance Concrete Structures		Cu Yd	2.9	
Permanent casing		Foot	40	
Drilled Shaft in Soil		Cu Yd	24	

* Length is height of spiral.
Maximum Bearing Pressure = 12.6 ksf

- Notes:
- For permanent steel sheet piling, see sheets S-62 thru S-66 of S-87.
 - For expansion joint, see sheet S-48 of S-87.
 - When splicing of spiral reinforcement is necessary, the spirals shall be provided with 1 1/2 extra turns at the ends to be spliced. These additional turns shall either be welded together according to AWS D1.4, or shall both terminate with a 135° standard hook.
 - For Crosshole Sonic Logging details, see sheet S-61 of S-87.

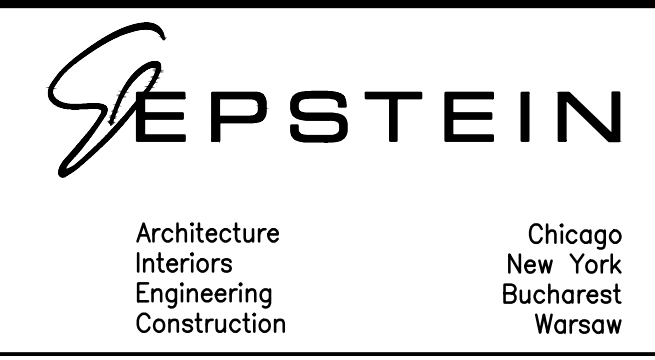
SECTION NO.: 12-E9121-00-BT
 DRAWN BY: J. MASI
 CHECKED BY: R. VARMA
 APPROVED BY: P. DOMBROWSKI
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ADDISON UNDERBRIDGE TRAIL CONNECTION FROM
 CLARK PARK TO CALIFORNIA PARK
 STRUCTURE NO. 016-P025

SUBSTRUCTURE - ABUTMENT 4

SHEET NO.
S-53
 SHEET 100 OF 158